

PROPOSED TAKE 5 OIL CHANGE

RTE 291 @ SE LANGSFORD RD
LEE'S SUMMIT, MISSOURI



VICINITY MAP
N.T.S

- NOTE:
- THIS PROJECT IS TO BE CONSTRUCTED WITHIN THE JURISDICTIONAL BOUNDARIES OF THE CITY OF LEE'S SUMMIT, MO.
 - UNLESS OTHERWISE NOTED ALL IMPROVEMENTS SHALL COMPLY WITH THE CITY OF LEE'S SUMMIT, MO STANDARD SPECIFICATIONS AND DRAWINGS.

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DEVELOPER

DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302



REVISION	BY
1 10/24/2024 REVISED PER CITY	KRG

HIGH TIDE

CONSULTANTS LLC

434 N. COLUMBIA ST, SUITE 200A

COVINGTON, LA 70433

www.hightidela.com

SIGNATURE

OCTOBER 24, 2024

DATE

STAMP

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 COVER

SHEET
COVER

TOPOGRAPHIC SURVEY

SURVEY PREPARED FOR
DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO. 80302
TODD MINIS, MANAGING PARNTER
todd@drivenassets.com

SURVEY NOTES

1. INFORMATION AS SHOWN FOR UNDERGROUND UTILITIES HAS BEEN COMPILED FROM THE RECORDS OF VARIOUS UTILITY COMPANIES CONCERNED, AND AS MARKED IN THE FIELD BY THE MISSOURI ONE CALL SYSTEM, 1 (800) 344-7483. WHEN PRECISE LOCATIONS OF UNDERGROUND UTILITIES ARE NEEDED PRIOR TO EXCAVATION OR CONNECTIONS, THE VARIOUS UTILITY COMPANIES CONCERNED ARE TO FURNISH A CREW TO POINT OUT THE LOCATIONS AT THE JOB SITE. Missouri One Call Ticket #232351453.
3. CONTOURS SHOWN HEREON ARE 1 FOOT INTERVALS AND BASED NAVD88.
4. BASIS OF BEARINGS: MISSOURI STATE PLANE, WEST ZONE.
5. CLASS OF SURVEY: URBAN
6. WE HAVE REVIEWED THE "FLOOD INSURANCE RATE MAP", COMMUNITY PANEL NO. 29095C0436G HAVING AN EFFECTIVE JANUARY 20, 2017 AS PUBLISHED BY FEDERAL EMERGENCY MANAGEMENT AGENCY. OUR REVIEW OF THIS MAP INDICATES THAT THIS PARCEL OF LAND LIES WITHIN ZONE X OTHER FLOOD AREAS, WHICH IS "AREAS OF 0.2%; AREAS OF 1% ANNUAL CHANCE FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE; AND AREAS PROTECTED BY LEVEES FORM 1% ANNUAL CHANCE FLOOD."

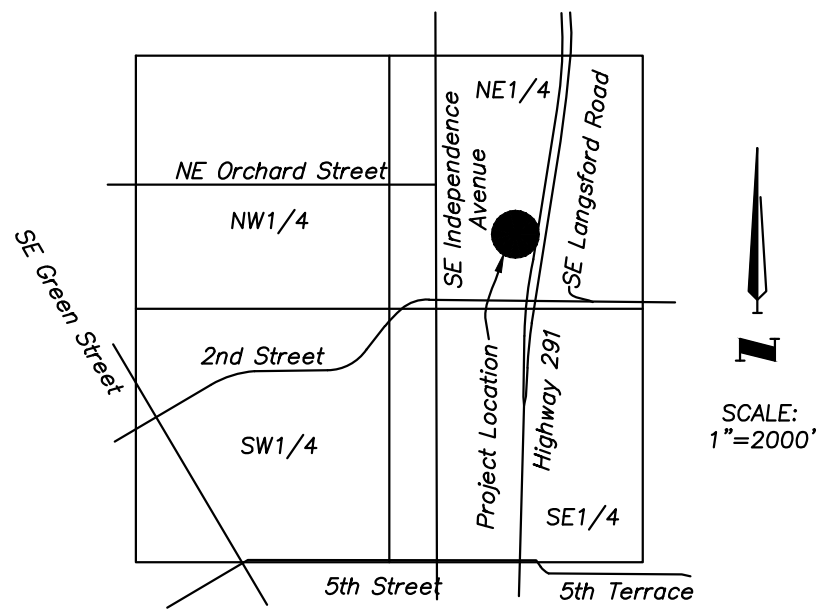
LEGAL DESCRIPTION PER TITLE COMMITMENT

TRACT I:

THE SOUTH 150 FEET OF THE EAST 150 FEET OF THE NORTH 300 FEET OF THE SOUTH 902 FEET OF THE EAST 880 FEET OF THE WEST ONE HALF OF THE NORTHEAST QUARTER OF SECTION 5, TOWNSHIP 47, RANGE 31, IN LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHWEST CORNER OF THE NORTHEAST QUARTER OF SAID SECTION 5, TOWNSHIP 47, RANGE 31; THENCE SOUTH 89 DEGREES, 22 MINUTES, 09 SECONDS EAST ALONG THE SOUTH LINE OF THE WEST ONE HALF OF SAID QUARTER SECTION, A DISTANCE OF 1321.26 FEET TO THE SOUTHEAST CORNER OF SAID WEST ONE HALF; THENCE NORTH 00 DEGREES, 40 MINUTES, 00 SECONDS EAST ALONG THE EAST LINE OF SAID WEST ONE HALF, A DISTANCE OF 602.00 FEET TO THE POINT OF BEGINNING; THENCE NORTH 89 DEGREES, 22 MINUTES, 09 SECONDS EAST, PARALLEL WITH THE SOUTH LINE OF SAID WEST ONE HALF, A DISTANCE OF 150.00 FEET; THENCE NORTH 00 DEGREES, 40 MINUTES, 00 SECONDS EAST, PARALLEL WITH THE EAST LINE OF SAID WEST ONE HALF, A DISTANCE OF 150.00 FEET; THENCE SOUTH 89 DEGREES, 22 MINUTES, 09 SECONDS EAST, PARALLEL WITH THE SOUTH LINE OF SAID WEST ONE HALF, A DISTANCE OF 150.00 FEET TO A POINT ON THE EAST LINE OF SAID WEST ONE HALF, THENCE SOUTH 00 DEGREES, 40 MINUTES, 00 SECONDS WEST ALONG SAID EAST LINE, A DISTANCE OF 150.00 FEET TO THE POINT OF BEGINNING.

TRACT II:

COMMENCING AT THE SOUTHWEST CORNER OF THE EAST 1/2 OF THE NORTHEAST 1/4 OF SECTION 5, TOWNSHIP 47, RANGE 31, LEE'S SUMMIT, JACKSON COUNTY, MISSOURI; THENCE ALONG THE WEST LINE OF SAID 1/2 OF 1/4 SECTION NORTH 2 DEGREES, 33 MINUTES, 49 SECONDS EAST 602 FEET TO THE TRUE POINT OF BEGINNING OF THIS TRACT; THENCE ALONG SAID WEST LINE NORTH 2 DEGREES, 33 MINUTES, 49 SECONDS EAST 478.79 FEET; THENCE SOUTH 88 DEGREES, 27 MINUTES, 48 SECONDS EAST 152.53 FEET TO A POINT ON THE WEST LINE OF THE RIGHT-OF-WAY OF M-291; THENCE ALONG SAID RIGHT-OF-WAY LINE AS FOLLOWS: SOUTH 15 DEGREES, 32 MINUTES, 36 SECONDS WEST 73.03 FEET TO A POINT 135 FEET OPPOSITE CENTER LINE STATION 117+00; THENCE SOUTH 11 DEGREES, 23 MINUTES, 57 SECONDS WEST 200 FEET TO A POINT 135 FEET OPPOSITE CENTER LINE STATION 119+00; THENCE SOUTH 1 DEGREE, 11 MINUTES, 44 SECONDS WEST 101.61 FEET TO A POINT 117 FEET OPPOSITE CENTER LINE STATION 120+00; THENCE PARALLEL TO SAID CENTER LINE SOUTH 11 DEGREES, 23 MINUTES, 57 SECONDS WEST 50 FEET; THENCE SOUTH 64 DEGREES, 21 MINUTES, 40 SECONDS WEST 66.40 FEET TO A POINT 170 FEET OPPOSITE CENTER LINE STATION 120+90; THENCE PARALLEL TO SAID CENTER LINE SOUTH 11 DEGREES, 23 MINUTES, 57 SECONDS WEST 31.03 FEET; THENCE LEAVING SAID RIGHT-OF-WAY NORTH 87 DEGREES, 26 MINUTES, 43 SECONDS WEST 35.61 FEET TO THE TRUE POINT OF BEGINNING



VICINITY MAP
5-47-31

LEGEND

- GA = GUY ANCHOR
- MS = METAL SIGN
- PP = POWER POLE
- SSMH = SANITARY SEWER MANHOLE
- SC = SECTION CORNER
- SCO = SEWER CLEAN OUT
- W = WATER MARKER
- WV = WATER VALVE
- WS = WOOD SIGN
- B = BOLLARD
- PM = GAS METER
- LP = LIGHT POLE w/CONC. BASE

- CP 50
1/2" IRON BAR WITH SKW TRAVERSE CAP
1. NORTH 3.0 FEET TO THE SOUTH EDGE OF AN ASPHALT ENTRANCE.
2. SOUTHWEST 8.5 FEET TO THE WEST CURB RETURN.
3. SOUTHEAST 24.0 FEET TO THE SOUTH END OF AN 18" CMP.

- CP 51
1/2" IRON BAR WITH SKW CAP
1. EAST 10.50 FEET TO THE WEST EDGE OF HIGHWAY 291.
2. WEST 15.5 FEET TO THE EAST EDGE OF A CONCRETE FLUME.

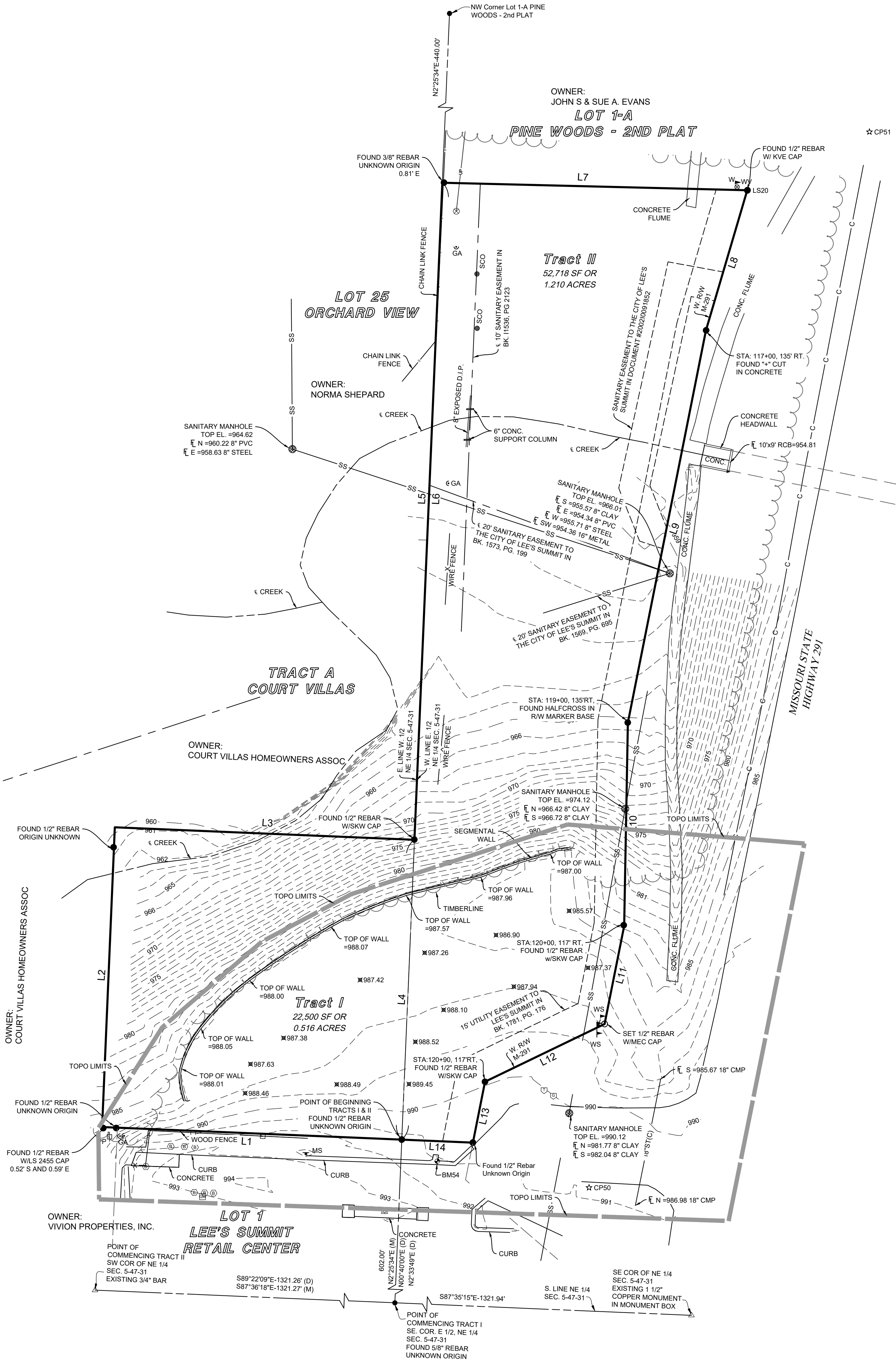
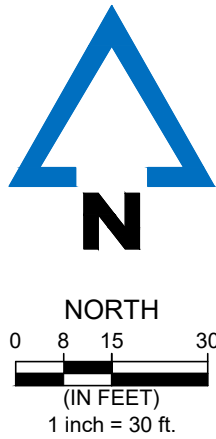
- CP 52
MAG & SHINER IN THE TOP OF A CURB AT THE NORTH END OF YELLOW NO PARKING.
1. WEST 28.0 FEET TO THE EAST END OF AN ISLAND.
2. EAST 41.8 FEET TO THE WEST EDGE OF HIGHWAY 291.
3. SOUTH 36.8 FEET TO A FIRE HYDRANT.

- BENCHMARK 53
ELEV.: 994.99
SET + CUT THE NORTH BOLT TOP FLANGE OF A FIRE HYDRANT ±150' SOUTHEAST OF THE SOUTHEAST CORNER OF THIS SURVEY.

- BENCHMARK 54
ELEV.: 991.62
EXISTING SQUARE CUT ON BACK OF CURB 50'± EAST NORTHEAST OF THE NORTHEAST CORNER OF MEINEKE ON THE NORTH SIDE DRIVE AT THE FLUME.

PROJECT CONTROL POINTS TABLE				
CP#	NORTHING	EASTING	ELEV.	DESCRIPTION
50	1002321.14'	2828279.79'	990.78'	CP 50
51	1002848.95'	2828419.99'	979.14'	CP 51
52	1001994.38'	2828211.00'	997.15'	CP 52
53	1002195.40'	2828248.41'	994.99'	BM 53
54	1002334.35'	2828203.90'	991.68'	BM 54

LINE TABLE		
LINE NO.	BEARING	DISTANCE
L1	N87°36'18"W (M) N89°22'09"E (D)	150.00'
L2	N2°25'34"E (M) N00°40'00"E (D)	150.00'
L3	S89°22'09"E (D) S87°36'18"E (M)	150.00'
L4	N00°40'00"E (D) N2°25'34"E (M)	150.00'
L5	N2°33'48"E (D) N2°25'34"E (M)	328.79'
L6	N2°33'48"E (D) N2°25'34"E (M)	478.79'
L7	S88°27'48"E (D) S89°38'03"E (M)	152.53'
L8	S16°24'21"W (M) S16°32'38"W (D)	73.03'
L9	N11°15'42"W (M) S11°23'57"W (D)	200.00'
L10	S11°11'44"W (D) S1°03'29"W (D)	101.61'
L11	S11°15'42"W (M) S11°23'57"W (D)	50.00'
L12	S64°13'25"W (M) S64°21'40"W (D)	66.40'
L13	S11°15'42"W (M) S11°23'57"W (D)	31.03'
L14	N87°34'58"W (M) N87°26'43"W (D)	35.61'



UTILITY WARNING

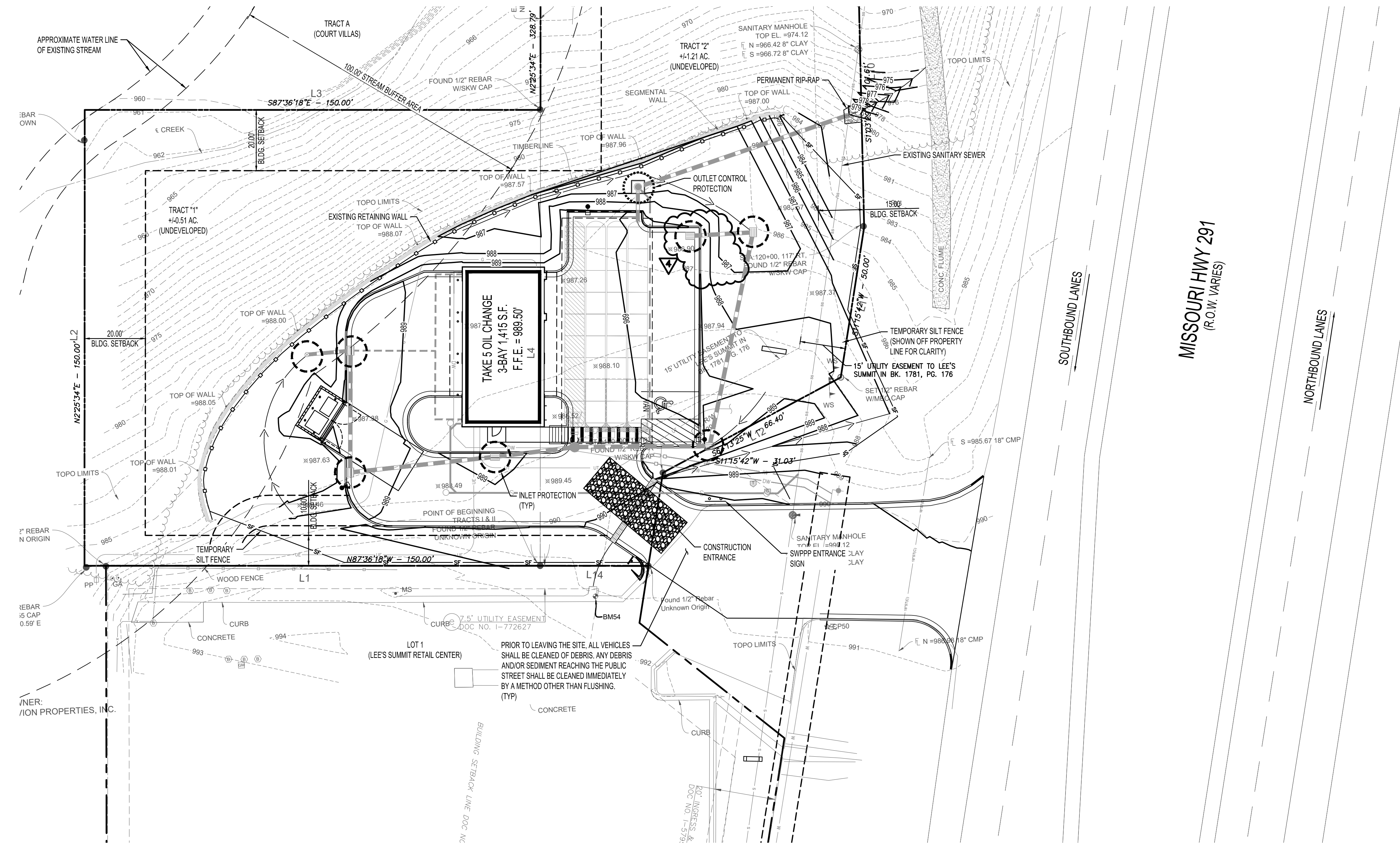
THE UTILITIES DEPICTED ON THIS DOCUMENT HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND/OR RECORDS OBTAINED. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES OR SUBSURFACE FEATURES SHOWN COMPRISE ALL SUCH ITEMS IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES OR SUBSURFACE FEATURES SHOWN ARE IN THE EXACT LOCATION INDICATED EXCEPT WHERE NOTED AS QUALITY LEVEL A.

TOPOGRAPHIC SURVEY

ENGINEER: J. BURNETTE
SURVEYOR: S. WHITAKER
DRAWN BY: J. BURNETTE
CREW CHIEF: S. WHITAKER
REVISIONS: LEE'S SUMMIT, JACKSON CO. MISSOURI
SEC. 5-747-R31
2023001249-000
9/13/2023
SHEET NO. 01/01



ACCORDING TO THE MISSOURI DEPARTMENT OF NATURAL RESOURCES GIS MAP, NO EXISTING OIL AND GAS WELLS ARE LOCATED ON SITE.



EROSION CONTROL NOTES:

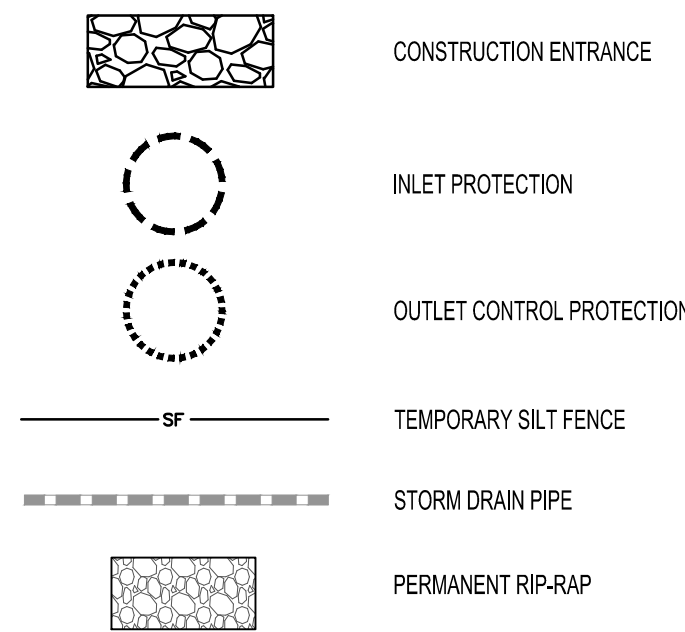
- SEDIMENT AND EROSION CONTROL FACILITIES AND STORM DRAINAGE FACILITIES SHALL BE INSTALLED PRIOR TO ANY OTHER CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN EROSION CONTROL FACILITIES DURING THE ENTIRE CONSTRUCTION PERIOD. FACILITIES ARE NOT TO BE REMOVED UNTIL COMPLETION OF THE PROJECT.
- ADDITIONAL EROSION CONTROL BMP'S MAY BE REQUIRED AS DEEMED NECESSARY BY GOVERNING AUTHORITIES.
- SILT FENCES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO $\frac{1}{2}$ THE HEIGHT OF THE FENCE.
- EROSION CONTROL MEASURES ARE TO BE INSPECTED WEEKLY AND AFTER EACH RAINFALL AND REPAIRED AS NECESSARY.
- ALL GRADED AREAS SHALL BE STABILIZED WITH A PERMANENT FAST GROWING COVER AND/OR MULCH UPON COMPLETION OF GRADING OPERATIONS. COMPLETION OF GRADING OPERATIONS DOES NOT MEAN AT THE END OF THE PROJECT, AS SOON AS FINAL GRADES ARE ESTABLISHED IN AN UNPAVED AREA, THE CONTRACTOR SHALL STABILIZE WITH A TEMPORARY GRASS OR PERMANENT SOD. IF A TEMPORARY GRASS IS APPLIED, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO APPLY A PERMANENT SEED OR SOD AT THE PROPER TIME OF YEAR.
- FILL SLOPES SHOULD BE PLANTED AS SOON AS AN AREA OF THE SITE IS BROUGHT TO FINAL GRADE. SURFACE RUNOFF SHALL BE INTERCEPTED AT THE TOP OF TEMPORARY AND PERMANENT SLOPES DURING CONSTRUCTION SO THAT WATER IS NOT ALLOWED TO FLOW OVER THE SLOPE FACE.
- THE GENERAL CONTRACTOR AND THE GRADING CONTRACTOR SHALL REVIEW THEIR GRADING SEQUENCE TO INSURE THAT THE LEAST AMOUNT OF LAND POSSIBLE AT ANY ONE TIME IS DISTURBED WITHOUT PERMANENT STABILIZATION.
- CONTRACTOR SHALL INSTALL TEMPORARY CONSTRUCTION ENTRANCES PRIOR TO ANY EARTHWORK OPERATIONS.
- CONTRACTOR SHALL MAINTAIN SILT FENCE FOR THE DURATION OF THE PROJECT UNTIL ACCEPTED BY THE OWNER AT NO EXPENSE TO OWNER.
- CONTRACTOR SHALL INSPECT ON A DAILY BASIS FOR NEEDED REMOVAL OF ANY ACCUMULATED SILTS, DEBRIS, OR REPAIR OF DAMAGED SILT FENCE AT NO ADDITIONAL EXPENSE TO OWNER.
- PRIOR TO CONSTRUCTION, THE EROSION AND SEDIMENT CONTROL MEASURES SHOWN HEREON SHALL BE IN PLACE. CLEARING AND GRUBBING OPERATIONS WILL BE ENGAGED IN ONLY AS NECESSARY TO ALLOW THE PLACEMENT OF EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN HEREON UNTIL ALL SUCH MEASURES ARE IN PLACE.
- THE CONTRACTOR SHALL OBTAIN AND COMPLY WITH ALL MISSOURI DEPT. OF NATURAL RESOURCES NPDES STORM WATER GENERAL PERMIT REQUIREMENTS THROUGHOUT THE DURATION OF CONSTRUCTION.
- CONTRACTOR SHALL UPDATE MAP AS NEEDED DURING CONSTRUCTION TO INDICATE LOCATIONS OF PORTABLE TOILETS, MATERIAL STORAGE AREAS, CONCRETE WASHOUTS, ETC. AND TO DOCUMENT BMP INSTALLATION AND CHANGES.

BMP MAINTENANCE EROSION NOTES:

ALL MEASURES STATED ON THIS SITE MAP SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. ALL EROSION & SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED PERSON IN ACCORDANCE WITH THE CONTRACT DOCUMENTS OR THE APPLICABLE PERMIT, WHICHEVER IS MORE STRINGENT, & REPAIRED IN ACCORDANCE WITH THE FOLLOWING:

- INLET PROTECTION DEVICES & BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING OR DETERIORATION.
- ALL SEEDED AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED. AREAS SHOULD BE FERTILIZED, WATERED, & RESEED AS NEEDED.
- SILT FENCES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM THE SILT FENCES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE SILT FENCE.
- THE CONSTRUCTION ENTRANCE / EXIT SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING OF THE CONSTRUCTION EXITS AS CONDITIONS DEMAND.
- THE TEMPORARY PARKING & STORAGE AREA PROVIDED BY CONTRACTOR SHALL BE KEPT IN GOOD CONDITION (SUITABLE FOR PARKING & STORAGE). THIS MAY REQUIRE PERIODIC TOP DRESSING OF THE TEMPORARY PARKING AREA AS CONDITIONS DEMAND.
- PRIOR TO LEAVING THE SITE, ALL VEHICLES SHALL BE CLEANED OF DEBRIS. ANY DEBRIS &/OR SEDIMENT REACHING THE PUBLIC STREET SHALL BE CLEANED IMMEDIATELY BY A METHOD OTHER THAN FLUSHING.

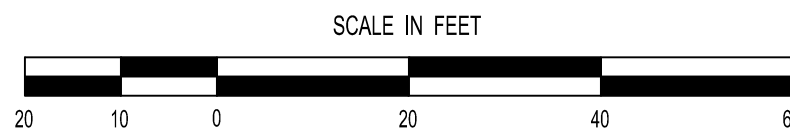
EROSION CONTROL LEGEND



REFER TO SURVEY SHEETS FOR LEGEND OF EXISTING FEATURES

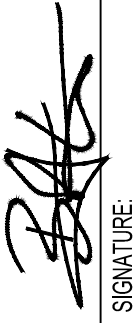


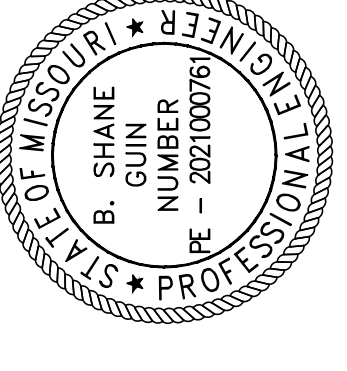
EROSION CONTROL PLAN



REVISION		BY
1	10/24/2024 REVISED PER CITY	KRG
2	12/16/2024 REVISED PER CITY	KRG
3	1/17/2025 REVISED PER CITY	KRG
4	1/31/2025 REVISED PER CITY	KRG

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com

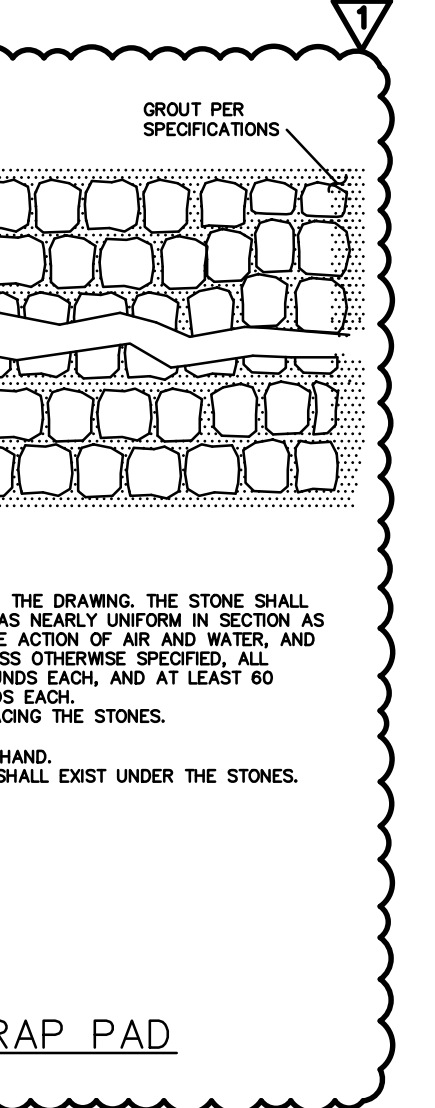
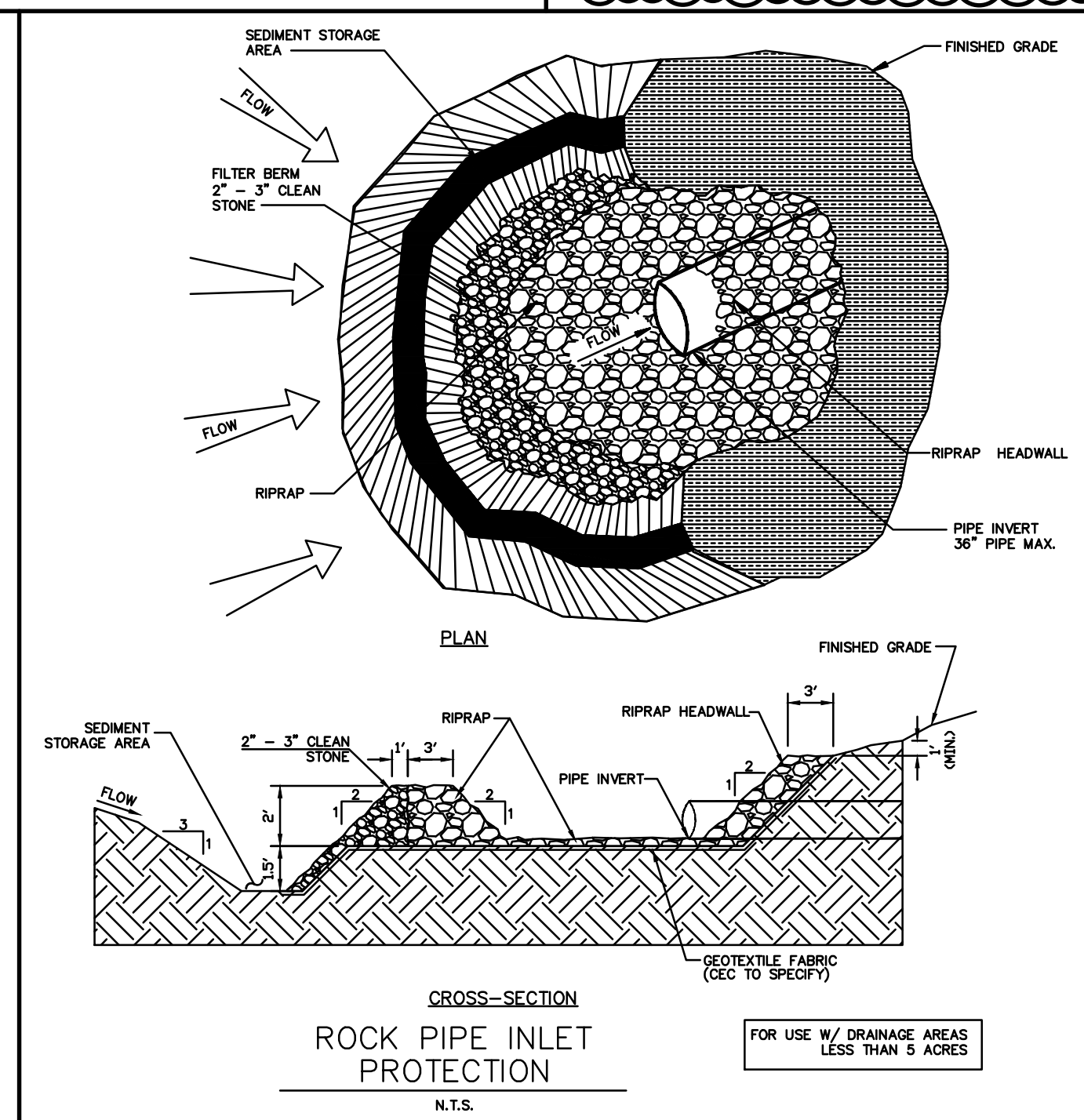
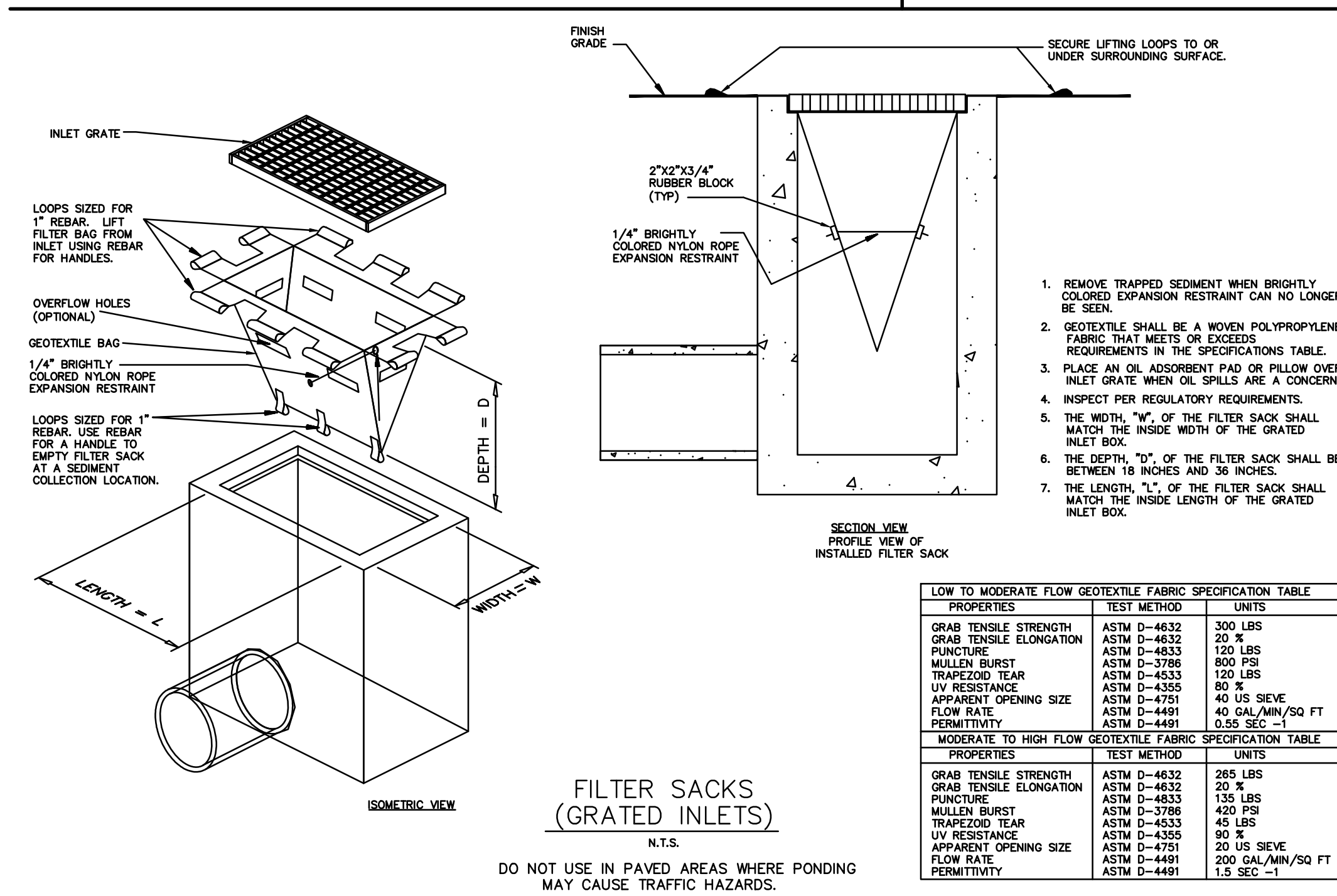
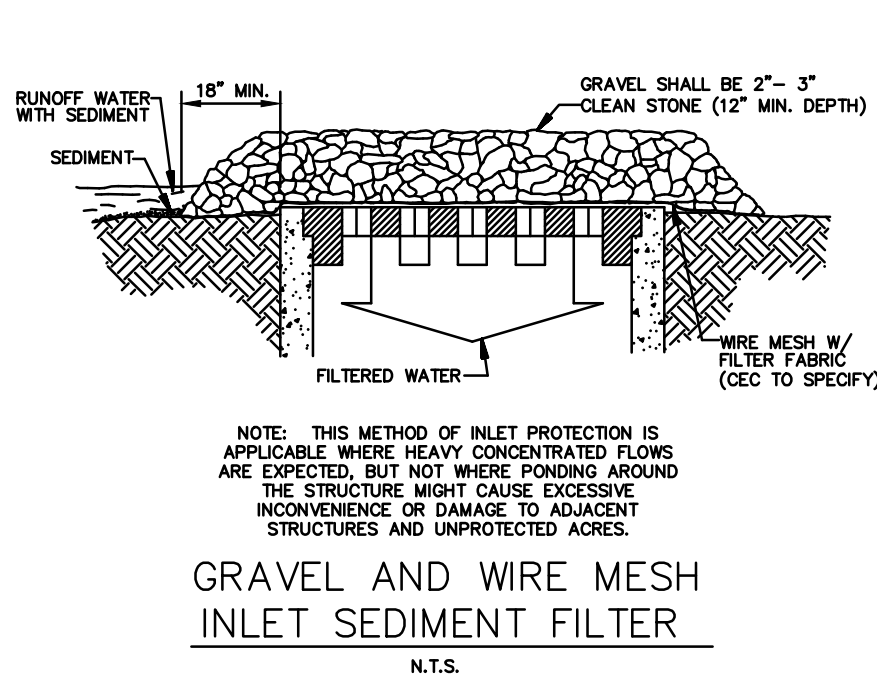
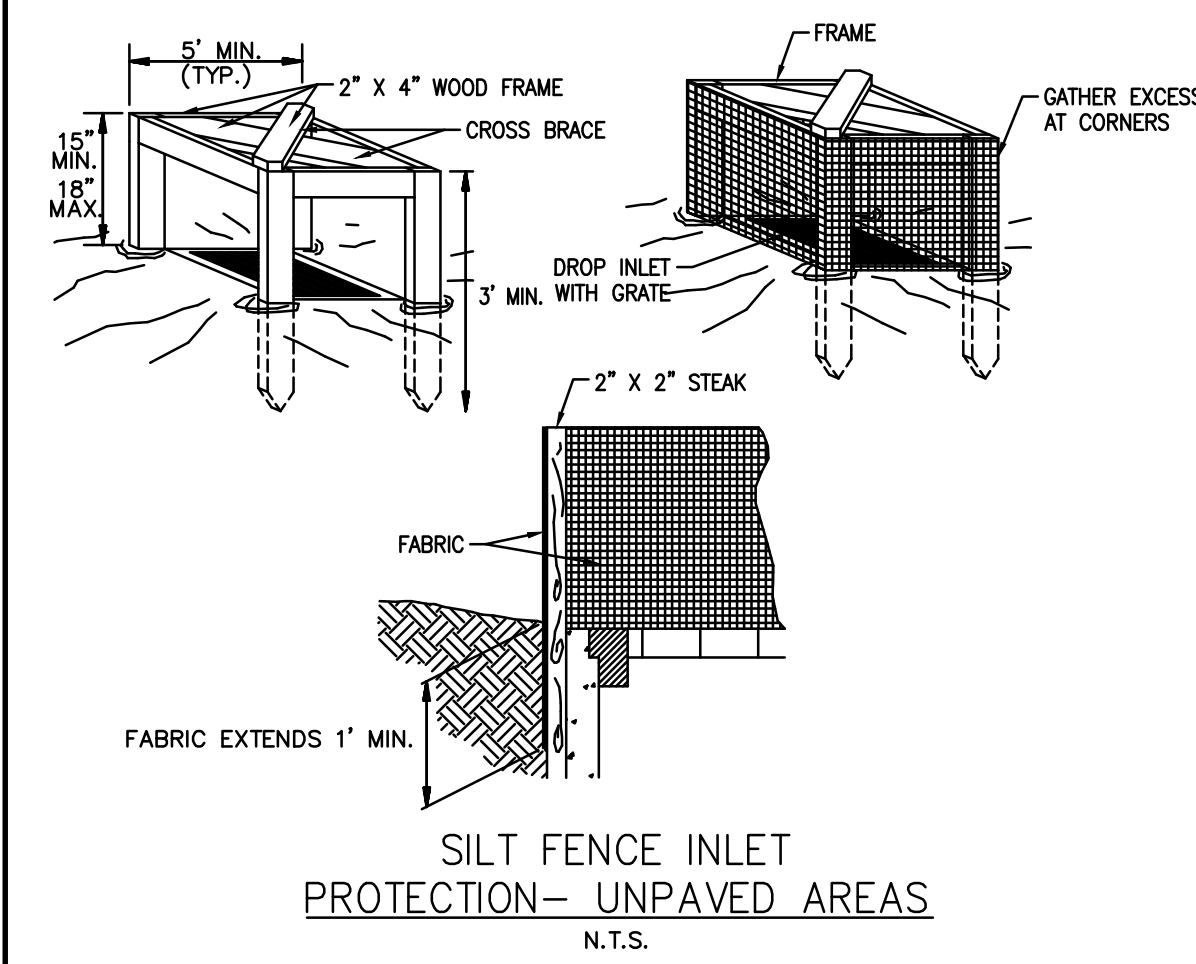
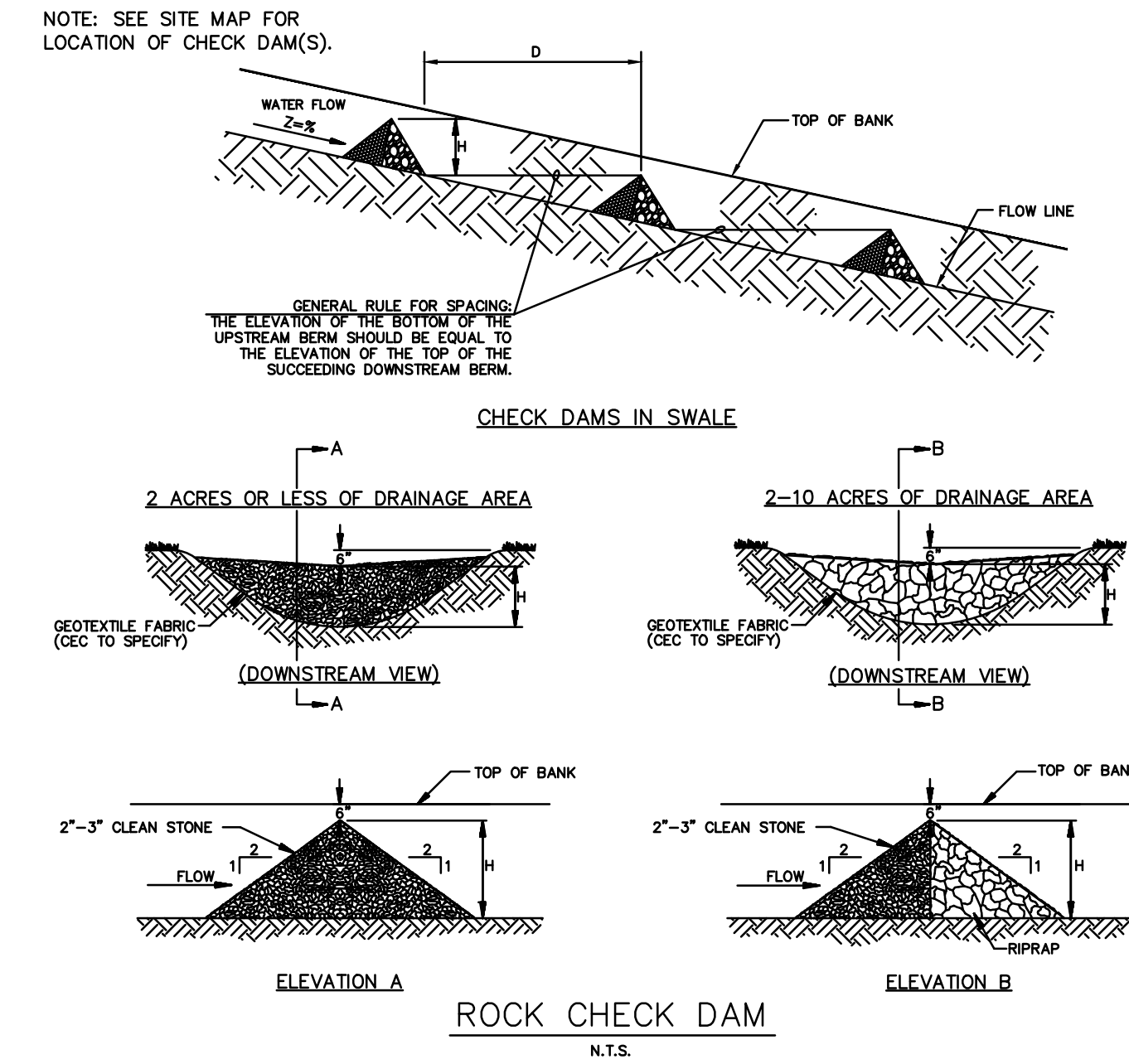
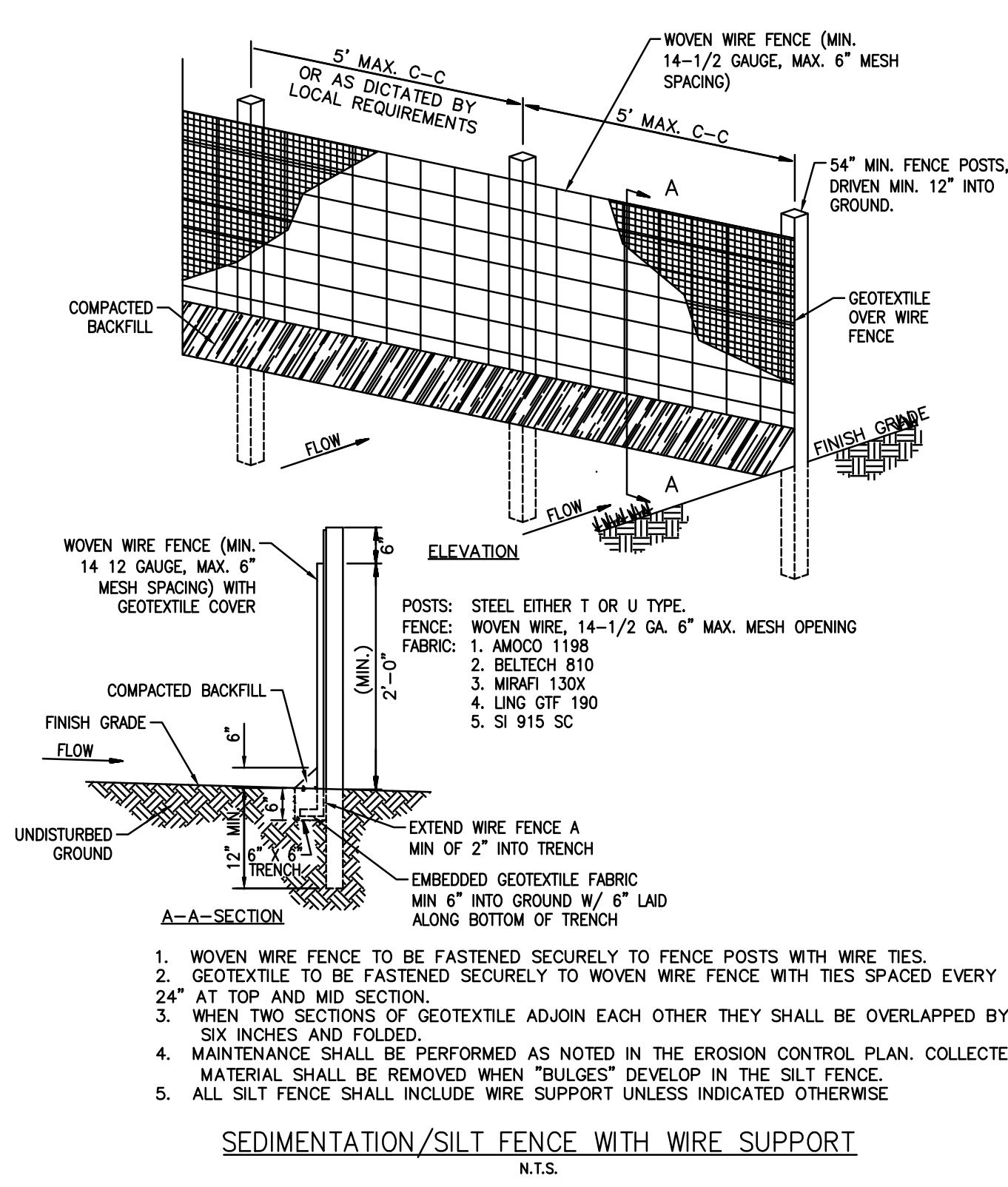
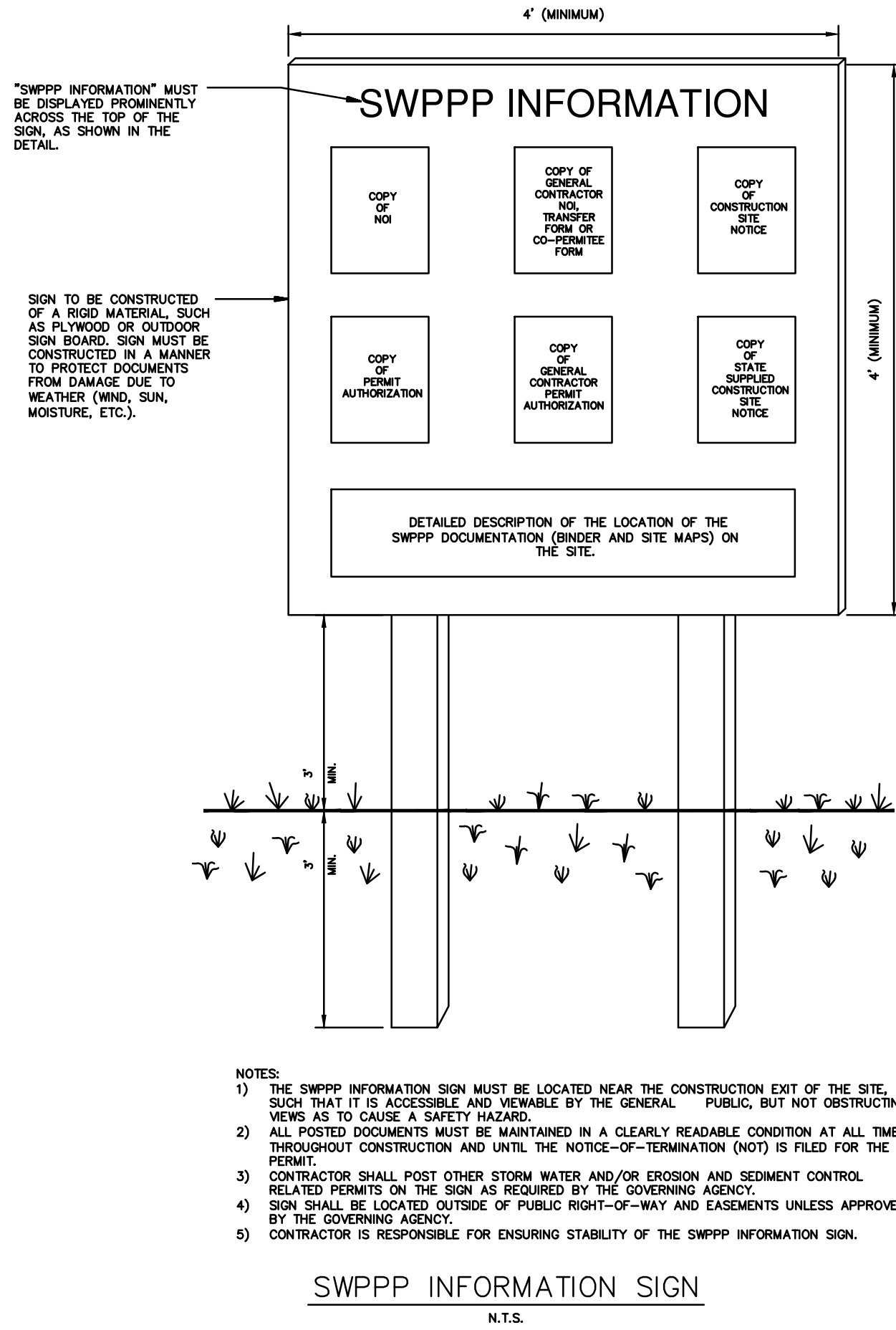
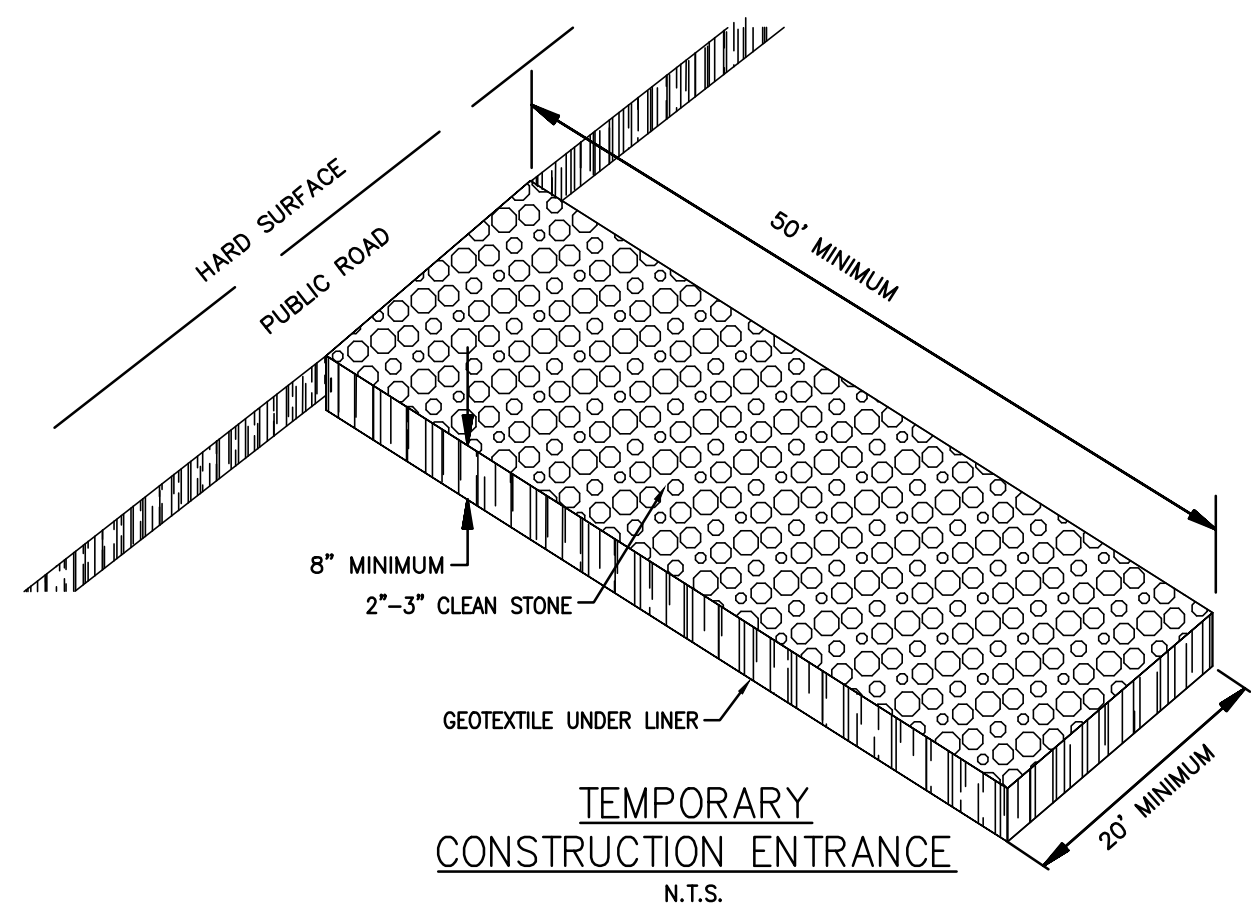

SIGNATURE
JANUARY 31, 2025
DATE


B. SHANE
ENGINEER
NO. 202100076
STATE OF MISSOURI

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 ES-1 Erosion Control Plan
SHEET ES-1



EROSION CONTROL DETAILS

REVISION	BY
1	KRG
10/24/2024	
REVISED PER CITY	

HIGH TIDE CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com

STATE OF MISSOURI
B. SHANE GUIN
ENGINEER
NUMBER 202100076
DATE: OCTOBER 24, 2024

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER CO 80302

ES-2
DRAWN: KRG
CHECKED: RCG
ISSUED DATE: 07/30/2024
ISSUED FOR: PERMITTING
PROJECT NO.: 22-218
FILE: 22-218 C-4 Details
SHEET: 22-218 C-4 Details

ACCORDING TO THE MISSOURI DEPARTMENT OF NATURAL RESOURCES GIS MAP, NO EXISTING OIL AND GAS WELLS ARE LOCATED ON SITE.

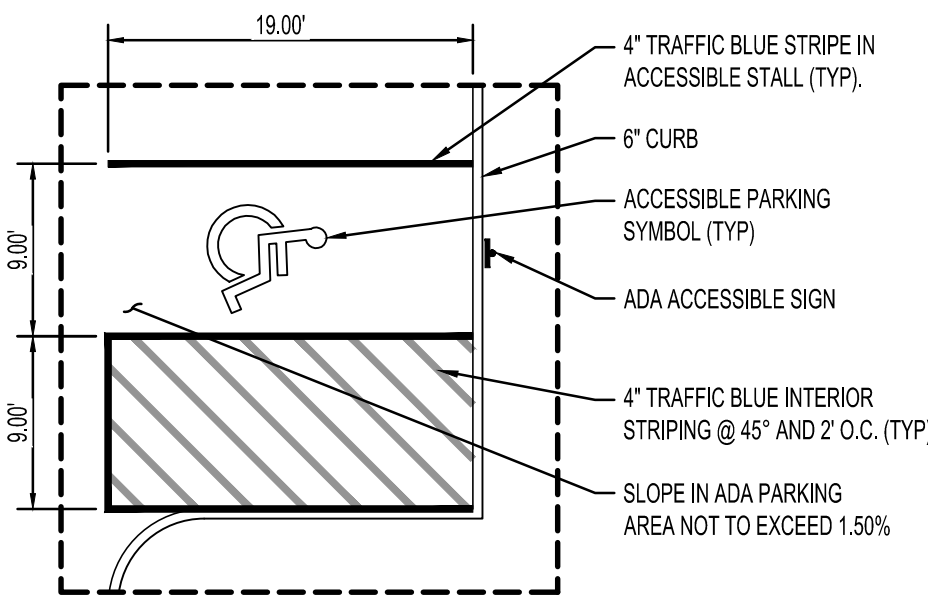
FLOOD NOTE:
THIS PROPERTY LIES WITHIN FLOOD ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE LIMITS OF THE 0.2% ANNUAL CHANCE FLOOD PLANE, AND OTHER FLOOD AREAS AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, COMMUNITY NO. 29 174 0436G AND DATED JANUARY 20, 2017

SITE DATA SUMMARY

ZONING	CP-2
PROPOSED USE	AUTOMOBILE SERVICE
BUILDING AREA (GROSS SQUARE FOOTAGE)	1,415 S.F.
LAND AREA	21,501.56 S.F.
IMPERVIOUS SURFACE	10,042.20 S.F.
FLOOR AREA RATIO (FAR)	$1,415/20,501 = 0.07 < 0.55 \text{ MAX}$

ADA DETAIL

N.T.S.



SITE PLAN



PARKING ANALYSIS

TAKE 5 OIL	+/- 1,415 S.F.
PARKING REQUIRED	10 SPACES
PARKING PROVIDED	10 SPACES (INCLUDING 3 BAYS)

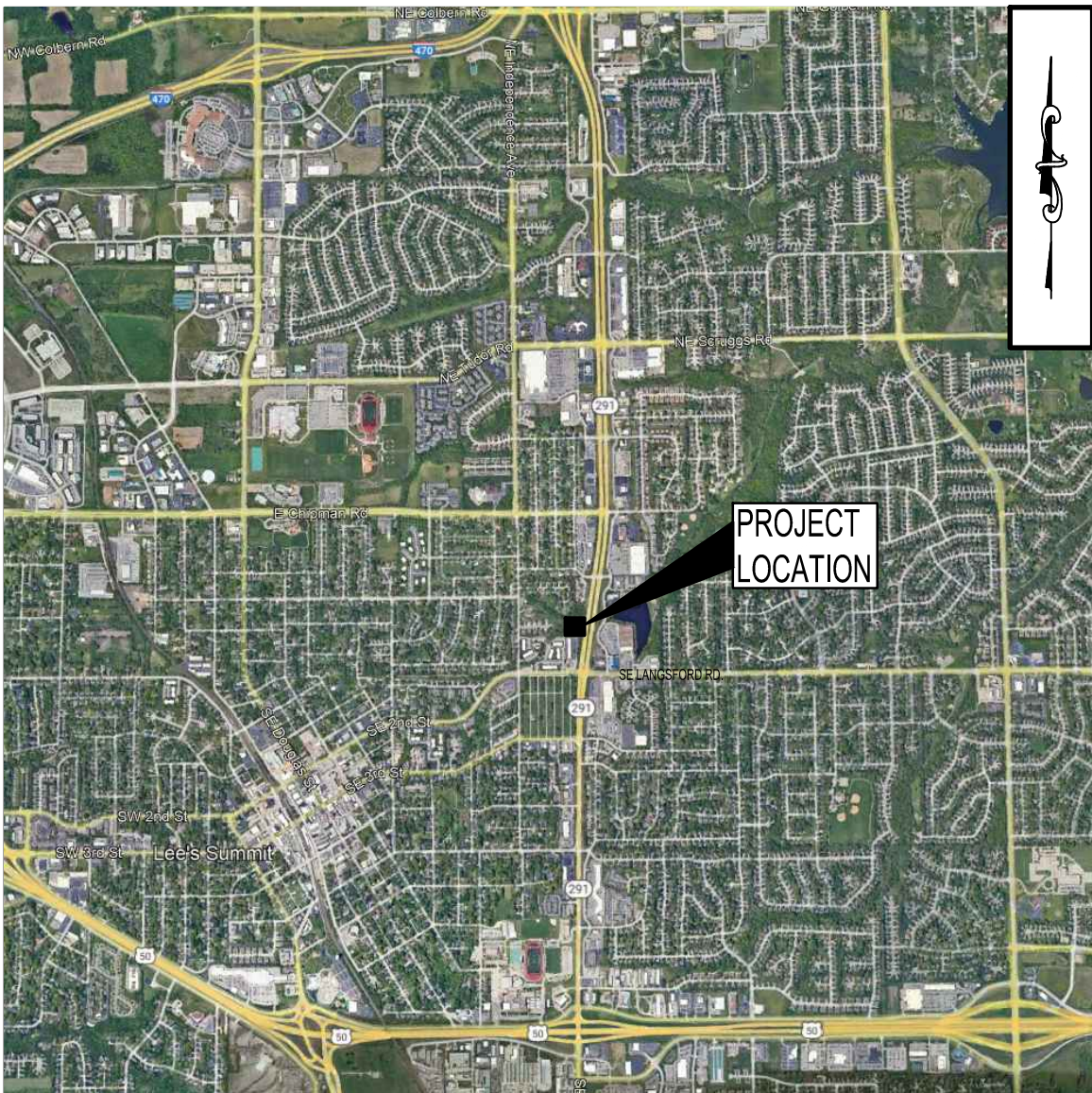
*CITY PARKING REQUIREMENT
3 SPACES PER BAY, BAYS MAY BE COUNTED AS A PARKING SPACE

LEGEND

	PROPERTY LINE		STANDARD DUTY CONCRETE PAVEMENT
	PROPOSED BUILDING		HEAVY DUTY CONCRETE PAVEMENT
	CG-1 CONCRETE CURB		HEAVY DUTY REINFORCED CONCRETE DUMPSTER PAD
	CG-1 CONCRETE CURB w/ PAVEMENT TURN DOWN		STAINED CONCRETE
	PARKING COUNT		SIDEWALK
	SITE LIGHTING FIXTURE		
REFER TO SURVEY SHEETS FOR LEGEND OF EXISTING FEATURES.			

VICINITY MAP

NTS



OWNER
VIVION PROPERTIES, LLC
5350 W. 94th TERRACE,
SUITE 104
PRAIRIE VILLAGE, KS 66207

PLAN PREPARED/CIVIL ENGINEER
HIGH TIDE CONSULTANTS, LLC
434 N. COLUMBIA ST., SUITE 200A
COVINGTON, LA 70435
ATTN: RICHARD C. GALLOWAY, PE
EMAIL: ricky@hightidelc.com

ARCHITECT
FUSION ARCHITECTS
3488 BRENTWOOD DRIVE
BATON ROUGE, LA 70806
ATTN: MATT DAIGREPOINT, AIA
PHONE: 225-766-4848

LANDSCAPE ARCHITECT
MCKNIGHT LANDSCAPE ARCHITECTS
668 S. FOSTER DRIVE, STE 101
BOULDER, CO 80302
ATTN: WES WILKERSON
EMAIL: wes@mcknightla.com

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2101 PEARL STREET
BOULDER, CO 80302
ATTN: TODD MINNIS
EMAIL: todd@drivenassets.com
PHONE: 214-587-5088

MULTI-USE DEVELOPMENT
ZONING: CP-2

REVISION	BY
1 10/24/2024 REVISED PER CITY	KRG
2 12/17/2024 REVISED PER CITY	KRG
3 2/10/2025 REVISED PER CITY	KRG

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST., SUITE 200A
COVINGTON, LA 70433
www.hightidelc.com



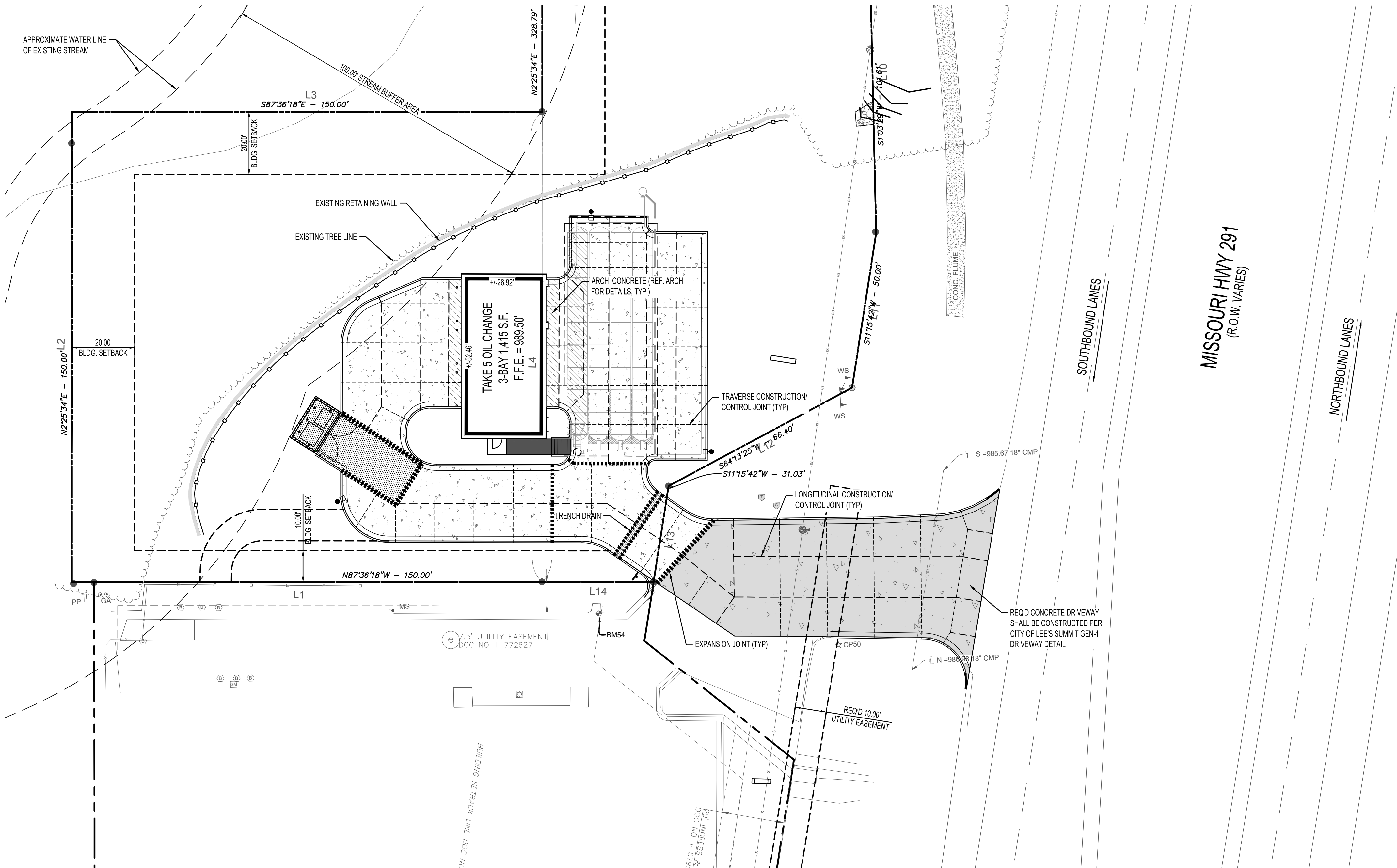
Stamp area containing the signature of B. Shane, the Professional Engineer's seal for the State of Missouri (No. 202100076), and the date FEBRUARY 10, 2025.

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 C-1 Site Plan

SHEET

C-1



GENERAL PAVING NOTES

- ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY OF LEE'S SUMMIT AND MCDOT REGULATIONS AND CODES AND O.S.H.A. STANDARDS.
- CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING BENCHMARK.
- ALL NECESSARY PERMITS AND APPROVALS FROM AGENCIES GOVERNING THE CONSTRUCTION OF THIS WORK SHALL BE SECURED PRIOR TO BEGINNING CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL AREAS INDICATED TO REMAIN UNDISTURBED OR TO REMAIN AS BUFFERS, ALL PROPERTY CORNERS, AND REPLACING ALL PINS DAMAGED OR ELIMINATED DURING CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRS OR DAMAGE TO ANY EXISTING IMPROVEMENTS DURING CONSTRUCTION SUCH AS, BUT NOT LIMITED TO, DRAINAGE, UTILITIES, PAVEMENT, STRIPING, CURB, ETC. REPAIRS SHALL BE EQUAL TO OR BETTER THAN EXISTING CONDITIONS.
- ANY WORK IN THE RIGHT-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH MISSOURI DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT'S STANDARD DRAWINGS AND SPECIFICATIONS.
- PAVEMENT JOINTS SHOWN HEREIN ARE PROVIDED FOR REFERENCE ONLY. CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH THE PAVEMENT JOINT DETAILS PROVIDED IN THE DETAIL SHEETS.
- PAVEMENT JOINTS SHALL BE SAWCUT AS SOON AS THE CONCRETE HAS REACHED SUFFICIENT STRENGTH TO SUPPORT THE SAWING EQUIPMENT AND TEARING OF CONCRETE DOES NOT OCCUR.
- JOINTS SHALL BE SEALED WITH APPROVED EXTERIOR PAVEMENT JOINT SEALANTS AND SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. JOINTS SHALL BE CELAN AND DRY PRIOR TO THE APPLICATION OF THE SEALANT.
- CONSTRUCT EXPANSION, WEAKENED PLANE CONTROL (CONTRACTION), AND CONSTRUCTION JOINTS STRAIGHT WITH FACE PERPENDICULAR TO THE CONCRETE SURFACE.

JOINT LAYOUT NOTES

- A SUGGESTED JOINT LAYOUT PLAN HAS BEEN PROVIDED. THE CONTRACTOR SHALL ADJUST THE JOINT LAYOUT AS REQUIRED TO MEET ALL SPACING REQUIREMENTS AND ADJUST TO CONSTRUCTION SEQUENCING.
- TRANSVERSE JOINTS SHALL BE PLACED PERPENDICULAR TO THE LONGITUDINAL JOINTS SHOWN AT A SPACING NO LARGER THAN 1.3 TIMES THE SPACING OF THE LONGITUDINAL JOINT. FOR EXAMPLE, IF THE SPACING BETWEEN THE LONGITUDINAL JOINTS IS 10 FEET, THE MAXIMUM SPACING FOR THE ASSOCIATED TRANSVERSE JOINT SHALL BE 13 FEET.
- IN THE AREAS WITH HORIZONTAL CURVES, THE LONGITUDINAL JOINTS SHALL BE TRUE OFFSETS OF THE CURVES. THE TRANSVERSE JOINTS SHALL BE RADIAL TO THAT HORIZONTAL CURVE. SEE THE LAYOUT SHOWN AND APPLY THE SAME PRINCIPAL TO THE REMAINDER OF THE SITE.
- WHERE JOINTS DO NOT FALL PERPENDICULAR TO A PAVEMENT EDGE, A TWO FOOT MINIMUM LENGTH OF JOINT SHALL BE TURNED PERPENDICULAR TO THE PAVEMENT EDGE TO AVOID "POINTS" IN PANELS.
- SMOOTH BARS IN RACKS SHALL BE HORIZONTAL AND PERPENDICULAR TO THE JOINT.
- CRACKED PANELS OR SPALLS AT JOINT OR PAVEMENT EDGES SHALL BE REPAIRED OR THE PANEL COMPLETELY REMOVED AND REPLACED AT THE COMPLETE DISCRETION OF THE ENGINEER. THE GENERAL LOGIC TO BE USED BY THE ENGINEER FOLLOWS FPA ADVISORY CIRCULAR 180-5370-10F DATED 9/30/2011 UNDER SECTIONS 501.4.19 AND 501.4.20 BUT THE FINAL CRITERIA IS LEFT UP TO THE ENGINEER. ANY REQUIRED REPAIRS INCLUDING COMPLETE REMOVAL OF THE PANEL SHALL BE AT NO ADDITIONAL COST TO THE OWNER.
- REFER TO PAVING DETAIL SHEET FOR JOINT DETAILS.
- JOINTS SHALL BE 90° AT THE FACE OF CURB.
- NO PANEL SHALL BE SMALLER THAN 4' IN ANY DIRECTION. FOR RECTANGULAR PANELS, THE LENGTH OF THE LONG SIDE SHALL NOT EXCEED TWICE THE LENGTH OF THE SHORT SIDE.
- AVOID IRREGULAR SHAPES.
- USE SELF-LEVELING SILICONE SEALANT. COLOR TO MATCH PAVEMENT.
- ALL EARTHWORK, SITE PREPARATION, AND PAVEMENT CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE GEOTECHNICAL REPORT RECOMMENDATIONS.
- JOINT LAYOUT SHALL BE ADJUSTED IN THE FIELD AS NECESSARY TO ACCOMMODATE EXACT LOCATIONS OF STRUCTURES INCLUDING MANHOLES, CATCH BASINS, FLUMES, STOOPS, ETC.

LEGEND

---	PROPERTY LINE		STANDARD DUTY CONCRETE PAVEMENT (12.5' MAX JOINT SPACING)
	PROPOSED BUILDING		HEAVY DUTY CONCRETE PAVEMENT (15.0' MAX JOINT SPACING)
CG-1	CG-1 CONCRETE CURB		HEAVY DUTY REINFORCED CONCRETE DUMPSTER PAD
CG-1	CG-1 CONCRETE CURB w/ PAVEMENT TURN DOWN		STAINED CONCRETE
■	SITE LIGHTING FIXTURE		SIDEWALK
---		---	LONGITUDINAL CONSTRUCTION/CONTROL JOINT
---		---	TRAVERSE CONSTRUCTION/CONTROL JOINT
.....		EXPANSION JOINT

REFER TO SURVEY SHEETS FOR
LEGEND OF EXISTING FEATURES.



JOINT LAYOUT PLAN

REVISION	BY

HIGH TIDE
CONSULTANTS LLC
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SIGNATURE: DATE: JANUARY 31, 2025

PROPOSED TAKE 5
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FOR DRIVEN ASSETS, LLC
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BOULDER, CO 80302

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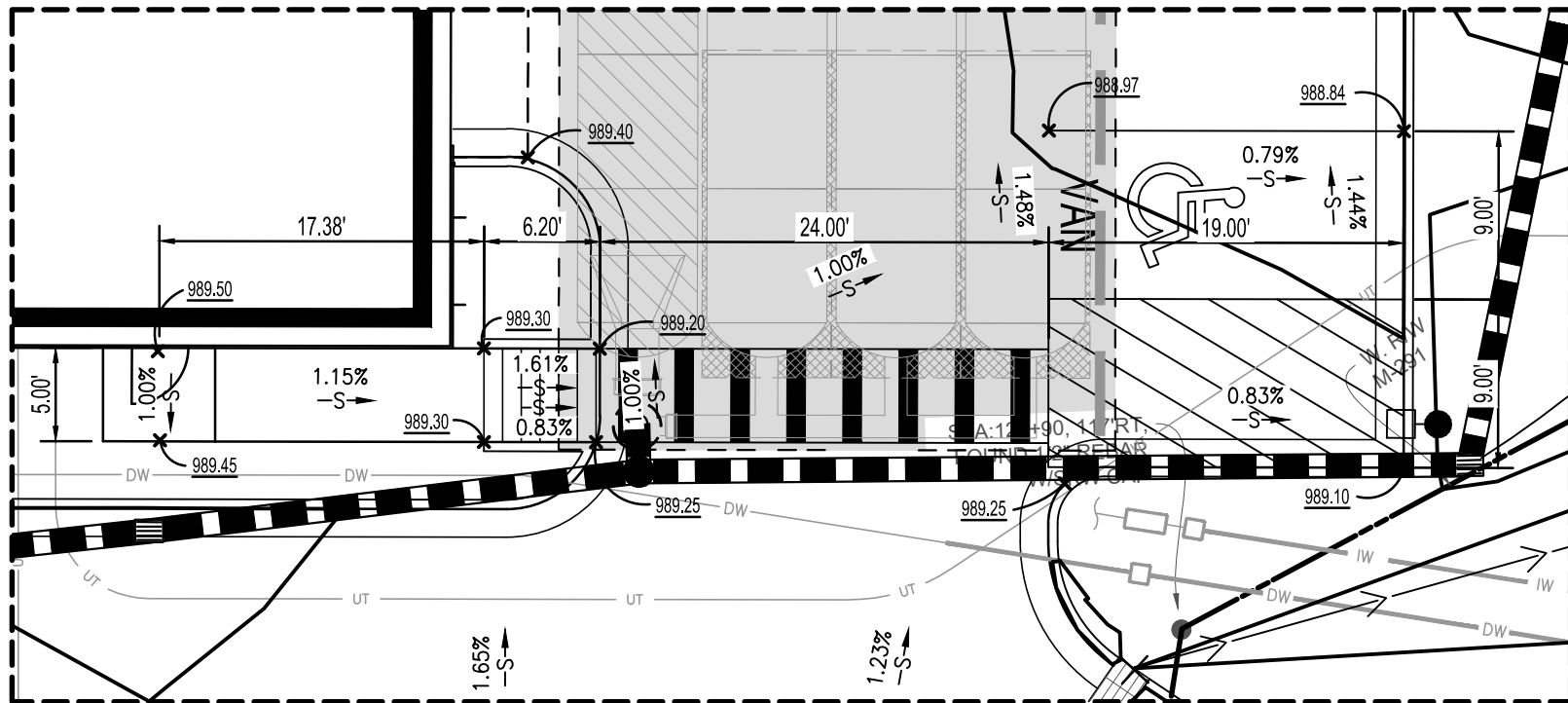
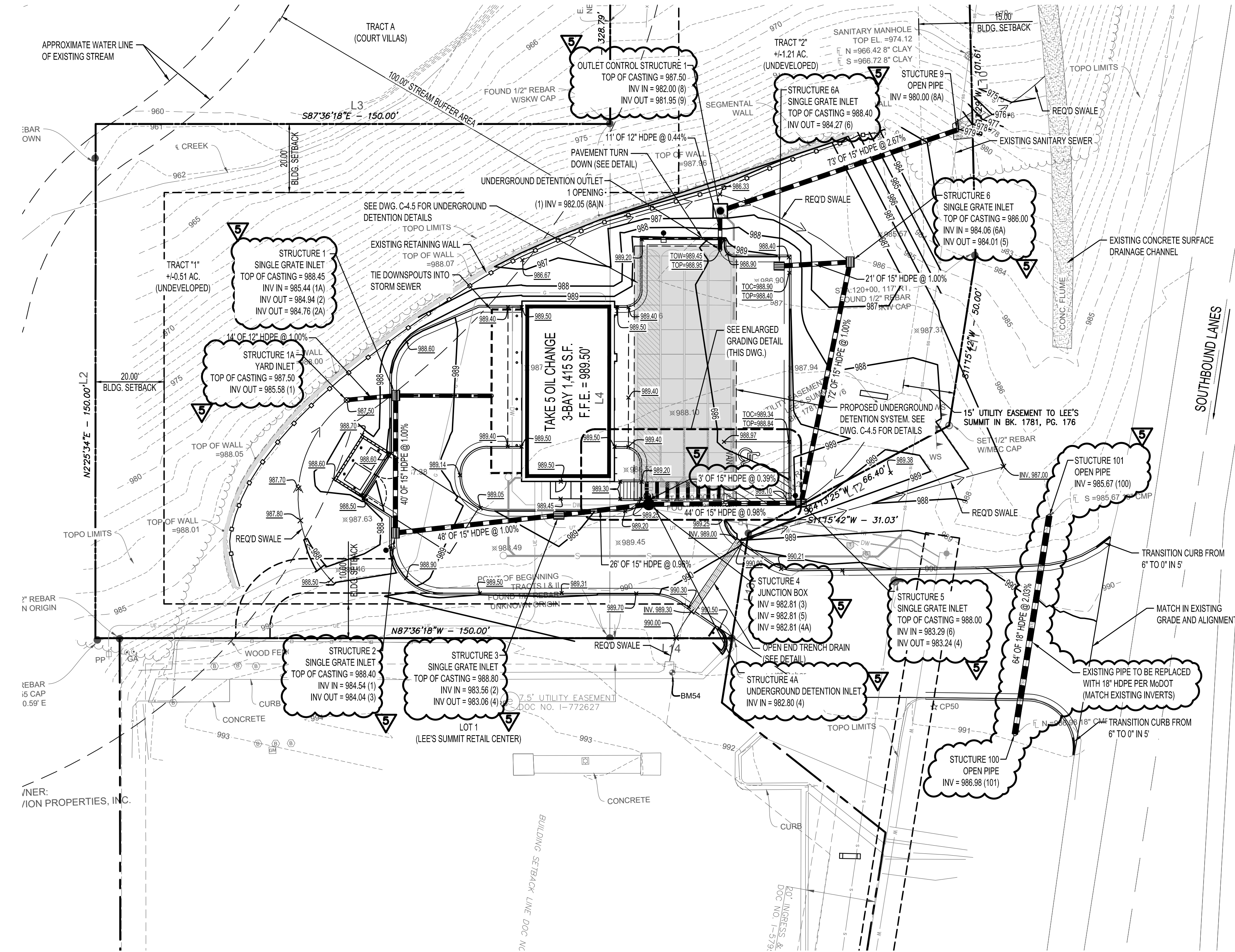
ISSUED DATE
07/30/2024

ISSUED FOR
PERMITTING

PROJECT NO.
22-218

FILE
22-218 C-1.1 Joint Layout Plan

SHEET
C-1.1



ENLARGED GRADING DETAIL
N.T.S.

ACCORDING TO THE MISSOURI DEPARTMENT OF
NATURAL RESOURCES GIS MAP, NO EXISTING OIL
AND GAS WELLS ARE LOCATED ON SITE.

GRADING NOTES

- CONTRACTOR IS RESPONSIBLE FOR DEMOLITION OF EXISTING FEATURES, INCLUDING REMOVAL OF ANY EXISTING UTILITIES UNLESS OTHERWISE NOTED ON THE PLANS. UTILITIES ARE TO BE REMOVED TO THE RIGHT-OF-WAY LIMITS.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- ALL CUT OR FILL SLOPES SHALL BE 3:1 OR FLATTER UNLESS OTHERWISE NOTED.
- PRECAST STRUCTURES MAY BE USED AT CONTRACTORS OPTION.
- EXISTING PIPES TO BE CLEANED OUT TO REMOVE ALL SILT AND DEBRIS.
- IF ANY EXISTING STRUCTURES SHOWN TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
- ALL STORM PIPE ENTERING STRUCTURES SHALL BE GROUTED TO ASSURE CONNECTION AT STRUCTURE IS WATERTIGHT.
- ALL STORM SEWER MANHOLES IN PAVED AREAS SHALL BE FLUSH WITH PAVEMENT AND SHALL HAVE TRAFFIC BEARING RING & COVERS. MANHOLES IN UNPAVED AREAS SHALL BE 6" ABOVE FINISH GRADE. LIDS SHALL BE LABELED "STORM SEWER".
- CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
- TOPOGRAPHIC INFORMATION IS TAKEN FROM A TOPOGRAPHIC SURVEY BY MCCLURE. IF THE CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR TO THE OWNER FOR REVIEW.
- ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3H:1V OR STEEPER. CONTRACTOR SHALL STABILIZE DISTURBED AREAS IN ACCORDANCE WITH GOVERNING SPECIFICATIONS UNTIL A HEALTHY STAND OF VEGETATION IS ESTABLISHED.
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME.
- CONTRACTOR SHALL COMPLY TO THE FULLEST EXTENT WITH THE LATEST STANDARD OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES.
- ALL STORM STRUCTURES SHALL HAVE A SMOOTH UNIFORM POURED MORTAR INVERT FROM INVERT IN TO INVERT OUT.
- ALL PIPES AND STRUCTURES WITHIN THE STREET RIGHT-OF-WAY SHALL BE PER MISSOURI DEPARTMENT OF TRANSPORTATION STANDARDS & SPECIFICATIONS, LATEST EDITION AND MODOT STANDARD DETAILS.
- DRAINAGE STRUCTURES SHALL BE PRECAST OR CAST-IN-PLACE CONCRETE IN ACCORDANCE WITH SPECIFICATIONS PROVIDED. REFER TO DETAIL SHEETS FOR DETAILS OF AREA INLETS AND MANHOLES. ALL INLET FRAMES AND GRATES SHALL BE VULCAN FOUNDRY CORP. CATALOG # V-4863 OR EQUAL.

SITE AND PAD PREPARATION NOTES

- ALL EARTHWORK, PAD AND SITE PREPARATION SHALL BE DONE IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEERING SERVICES REPORT PREPARED BY TERRACON CONSULTANTS, INC., DATED NOVEMBER 2, 2023, TERRACON PROJECT NO. 0222528.
- THE CONTRACTOR SHALL OBTAIN A COPY OF THE GEOTECHNICAL REPORT FROM THE OWNER, BECOME FAMILIAR WITH THE REPORT AND RECOMMENDATIONS AND SHALL FOLLOW THE RECOMMENDATIONS AND REQUIREMENTS OF THE REPORT.
- SITE PREPARATION SHALL INCLUDE THE STRIPPING OF EXISTING PAVEMENT SECTIONS, VEGETATION, ORGANICS, SILTY SOILS, AND LOOSE, SOFT OR OTHERWISE UNSUITABLE MATERIAL. COMPLETE STRIPPING OF THE ROOT MAT SHALL BE PERFORMED IN THE PROPOSED BUILDING AND PAVEMENT AREAS. STRIPPED MATERIALS CONSISTING OF VEGETATION AND ORGANIC MATERIALS SHALL BE WASTED OFF SITE. THE ACTUAL STRIPPING AND UNDERCUTTING DEPTHS SHALL BE DETERMINED BY A REPRESENTATIVE OF THE GEOTECHNICAL ENGINEER AT THE TIME OF CONSTRUCTION.
- THE SOILS WITHIN THE PLANNED BUILDING AREA SHALL BE FURTHER UNDERCUT AS NECESSARY TO ACCOMMODATE PLACEMENT OF THE RECOMMENDED 24-INCH THICK LVC LAYER BELOW FLOOR SLABS. THE UNDERCUT AREAS SHALL EXTEND A MINIMUM OF 5 FEET LATERALLY OUTSIDE THE BUILDING WALL LINES.
- FOLLOWING INITIAL STRIPPING AND ANY NECESSARY UNDERCUTTING, THE EXPOSED SOILS SHALL BE PROOFROLLED. A TERRACON REPRESENTATIVE SHOULD OBSERVE THE PROOFROLLING. PROOFROLLING CAN BE ACCOMPLISHED USING A LOADED TANDEM-AXLE DUMP TRUCK WITH A GROSS WEIGHT OF AT LEAST 20 TONS, OR SIMILARLY LOADED EQUIPMENT. AREAS THAT DISPLAY EXCESSIVE DEFLECTION (PUMPING) OR RUTTING DURING PROOFROLL OPERATIONS SHOULD BE IMPROVED BY SCARIFICATION/COMPACTION OR BY REMOVAL AND REPLACEMENT WITH ENGINEERED FILL.
- SEE GEOTECHNICAL REPORT FOR FILL MATERIAL TYPES, ACCEPTABLE LOCATION FOR PLACEMENT, AND FILL COMPACTION REQUIREMENTS.
- ALL GRADES MUST PROVIDE EFFECTIVE DRAINAGE AWAY FROM THE BUILDING DURING AND AFTER CONSTRUCTION AND SHOULD BE MAINTAINED THROUGHOUT THE LIFE OF THE STRUCTURE.
- THE CONSTRUCTION PHASE DRAINAGE SHOULD BE CONSIDERED IN THE DEVELOPMENT OF THE PROJECT OVERALL GRADING AND DRAINAGE PLAN. THE POSSIBLE POOR DRAINAGE CONDITIONS CAN LEAD TO INSTABILITY IN THE AREAS AROUND THE BUILDING AND HAMPER CONSTRUCTION PROGRESS. THE SITE GRADING AND GENERAL CONTRACTOR SHOULD CONSIDER THEIR MEANS AND METHODS TO MAINTAIN DRAINAGE DURING THE CONSTRUCTION PHASE.
- EXPOSED SUBGRADES MUST BE SLOPED TO PROVIDE POSITIVE DRAINAGE SO THAT SATURATION OF THE SUBGRADES IS AVOIDED. SURFACE WATER SHALL NOT BE PERMITTED TO ACCUMULATE ON THE SITE.
- UPON COMPLETION OF FILLING AND GRADING, CARE SHOULD BE TAKEN TO MAINTAIN THE SUBGRADE WATER CONTENT PRIOR TO CONSTRUCTION OF FLOOR SLABS.
- CONSTRUCTION TRAFFIC OVER THE COMPLETED SUBGRADES SHOULD BE AVOIDED TO THE EXTENT PRACTICAL.
- THE SITE SHOULD ALSO BE GRADED TO PREVENT PONDING OF SURFACE WATER ON THE PREPARED SUBGRADES OR IN EXCAVATIONS. WATER COLLECTING OVER, OR ADJACENT TO, CONSTRUCTION AREAS SHOULD BE REMOVED. IF THE SUBGRADE RESATURATES, SATURATES, OR IS DISTURBED, THE AFFECTED MATERIAL SHOULD BE REMOVED, OR THE MATERIALS SHOULD BE SCARIFIED, MOISTURE CONDITIONED, AND RECOMPACTED, PRIOR TO FLOOR SLAB CONSTRUCTION.
- AS A MINIMUM, EXCAVATIONS SHOULD BE PERFORMED IN ACCORDANCE WITH OSHA 29 CFR, PART 1926, SUBPART P, "EXCAVATIONS" AND ITS APPENDICES, AND IN ACCORDANCE WITH ANY APPLICABLE LOCAL, AND/OR STATE REGULATIONS.

SEE TERRACON GEOTECHNICAL REPORT
DATED 11/2/2023 FOR ALL SITE AND PAD
PREPARATION REQUIREMENTS.

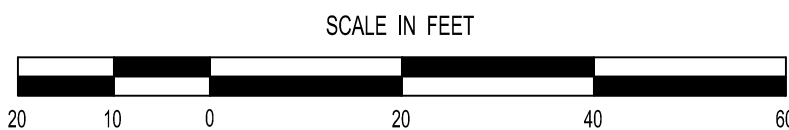
LEGEND - PROPOSED IMPROVEMENTS

PROPERTY LINE	975	CONTOUR
PROPOSED BUILDING	X 900.00'	SPOT ELEVATION
CG-1 CONCRETE CURB	TOP = TOP OF PAVEMENT TOW = TOP OF WALL TOC = TOP OF CURB	SLOPE ARROW
CG-1 CONCRETE CURB w/ PAVEMENT TURN DOWN	DW	DOMESTIC WATER LINE
STORM DRAIN PIPE	S	SANITARY SEWER LINE
DOWNSPOUT COLLECTOR N-12 PIPE (12" TRUNK W/ 8" LATERAL CONNECTIONS)	G	GAS LINE
SINGLE GRATE INLET	UT	UNDERGROUND TELEPHONE LINE
HP / GB	UE	UNDERGROUND ELECTRICAL LINE
HIGH POINT / GRADE BREAK		

REFER TO SURVEY SHEETS FOR
LEGEND OF EXISTING FEATURES



GRADING PLAN



NOTE:
THIS SITE IS LOCATED WITHIN THE WEST PRAIRIE LEE WATERSHED

FLOOD NOTE:
THIS PROPERTY LIES WITHIN FLOOD ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE LIMITS OF THE 0.2% ANNUAL CHANCE FLOOD PLANE, AND OTHER FLOOD AREAS AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, COMMUNITY NO. 29 174 0436G AND DATED JANUARY 20, 2017

REVISION	BY
1	10/24/2024 REVISED PER CITY
2	12/16/2024 REVISED PER CITY
3	1/17/2025 REVISED PER CITY
4	1/31/2025 REVISED PER CITY
5	2/14/2025 REVISED PER CITY

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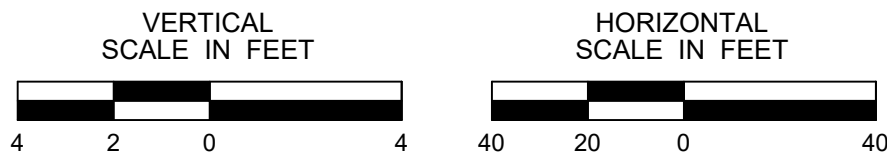
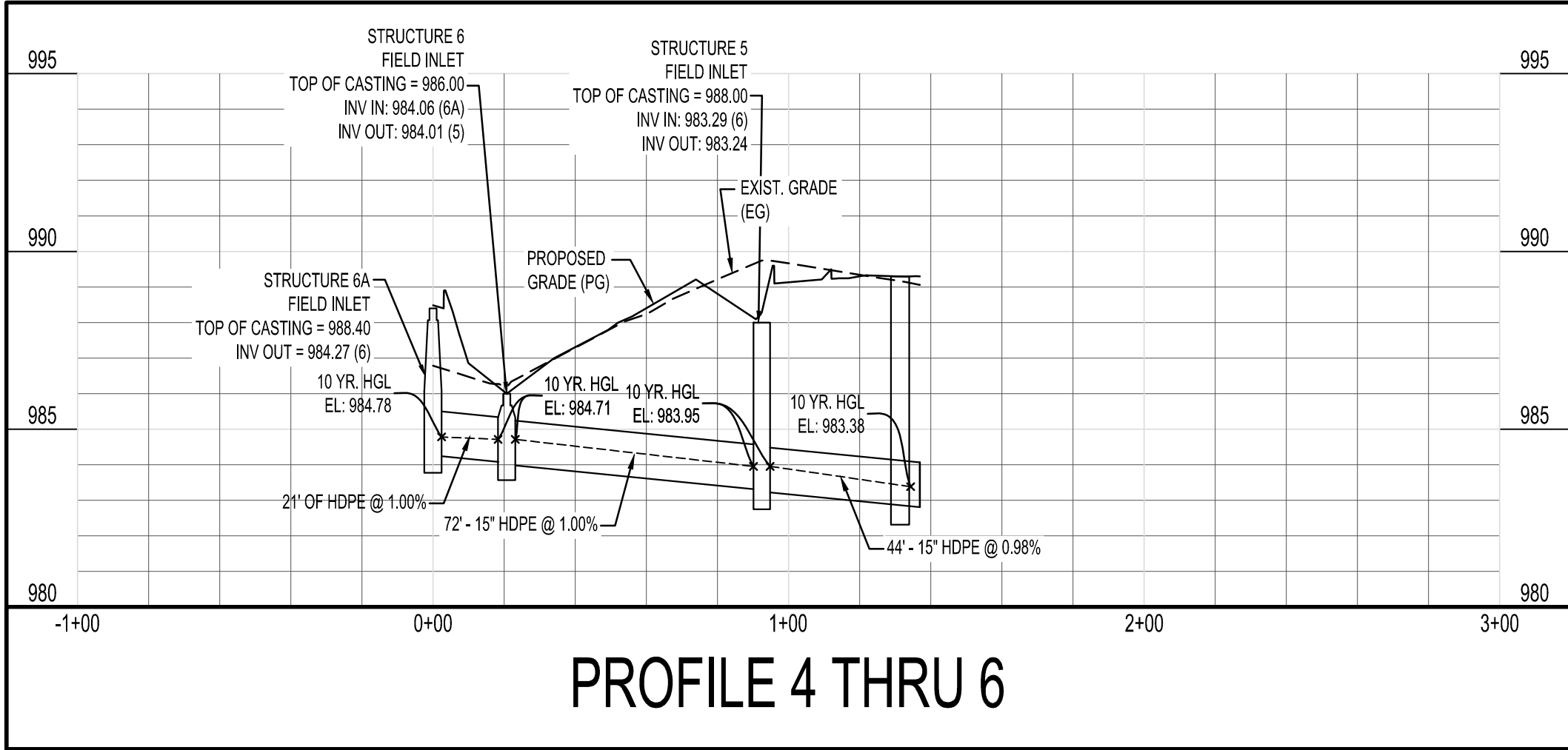
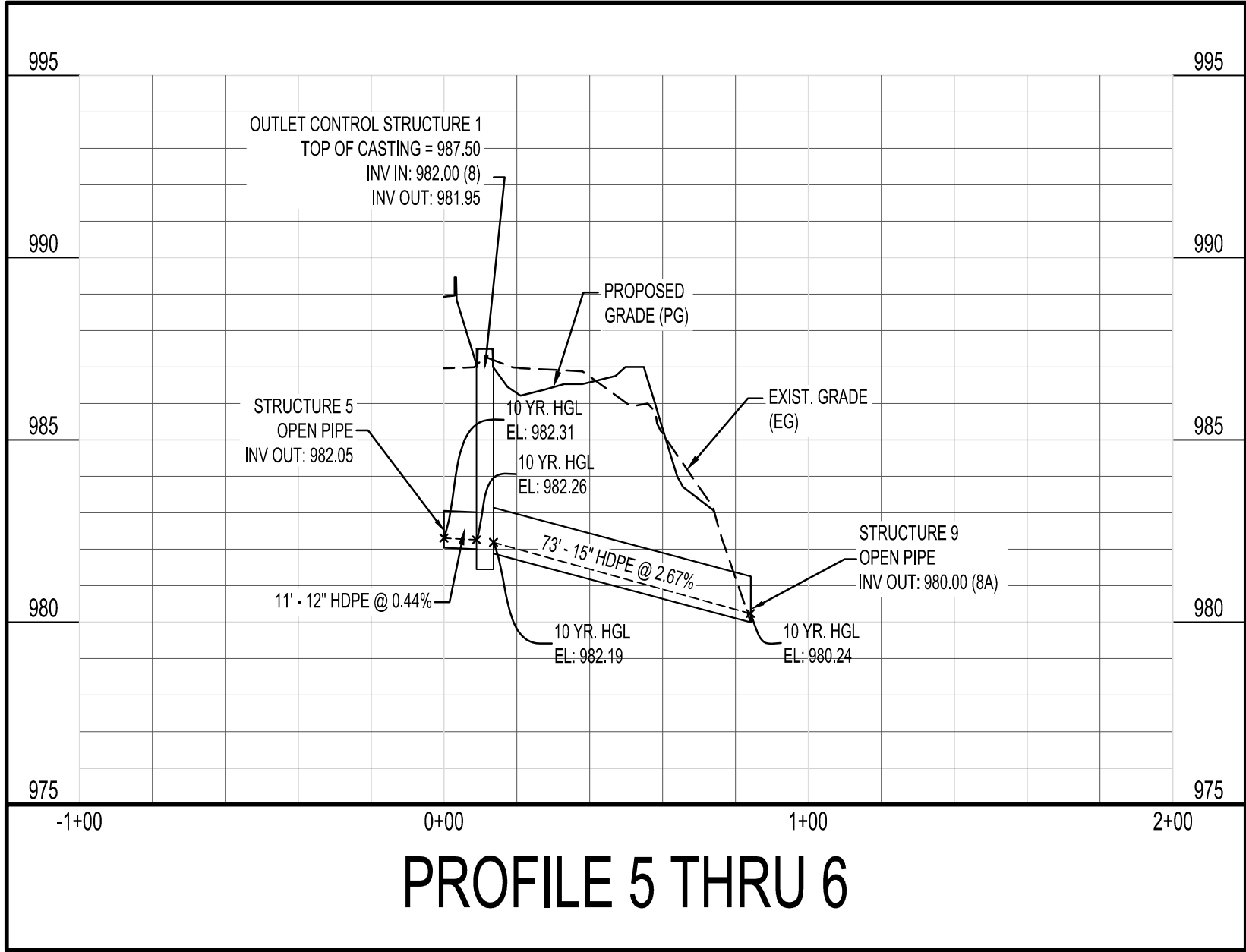
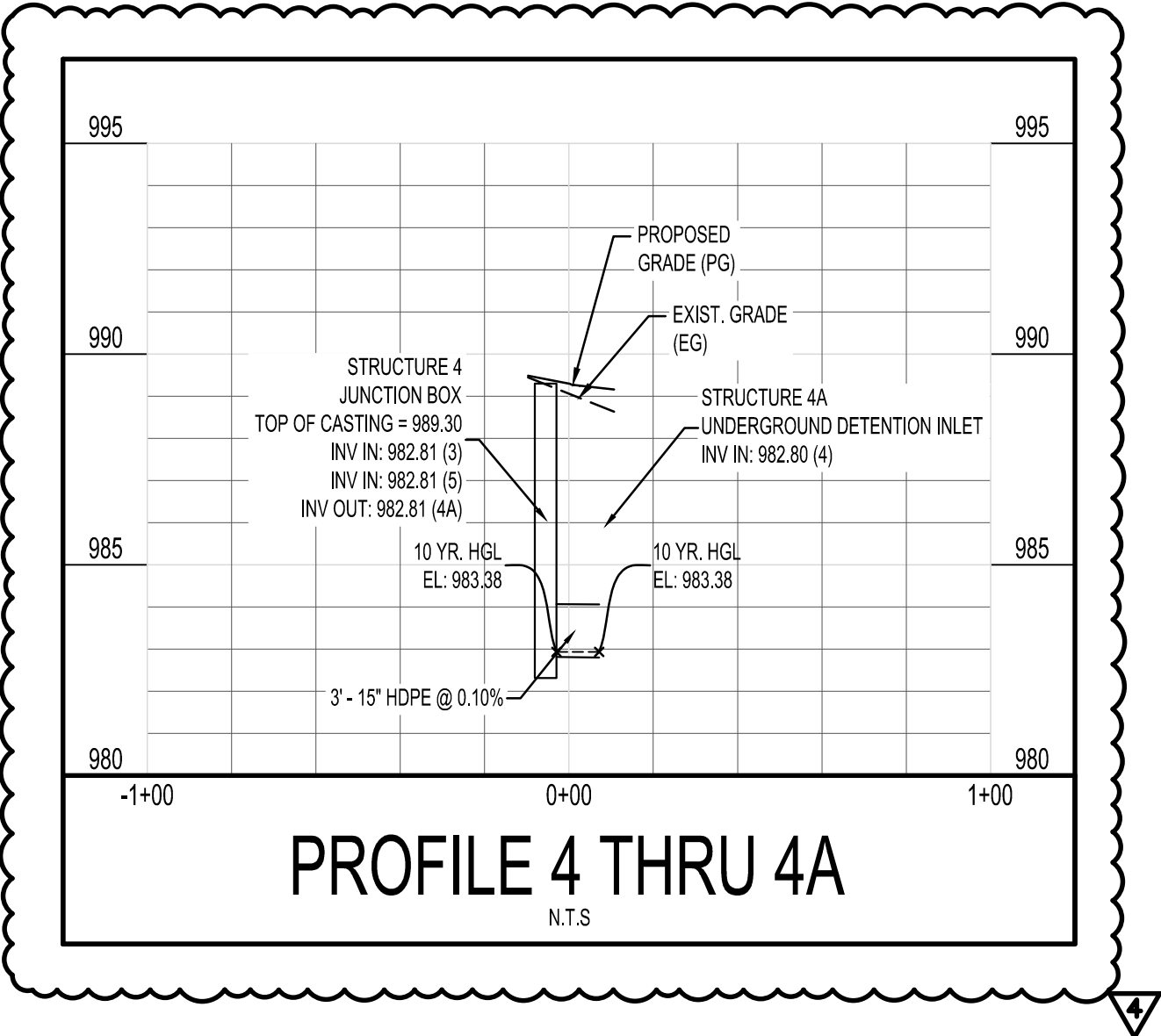
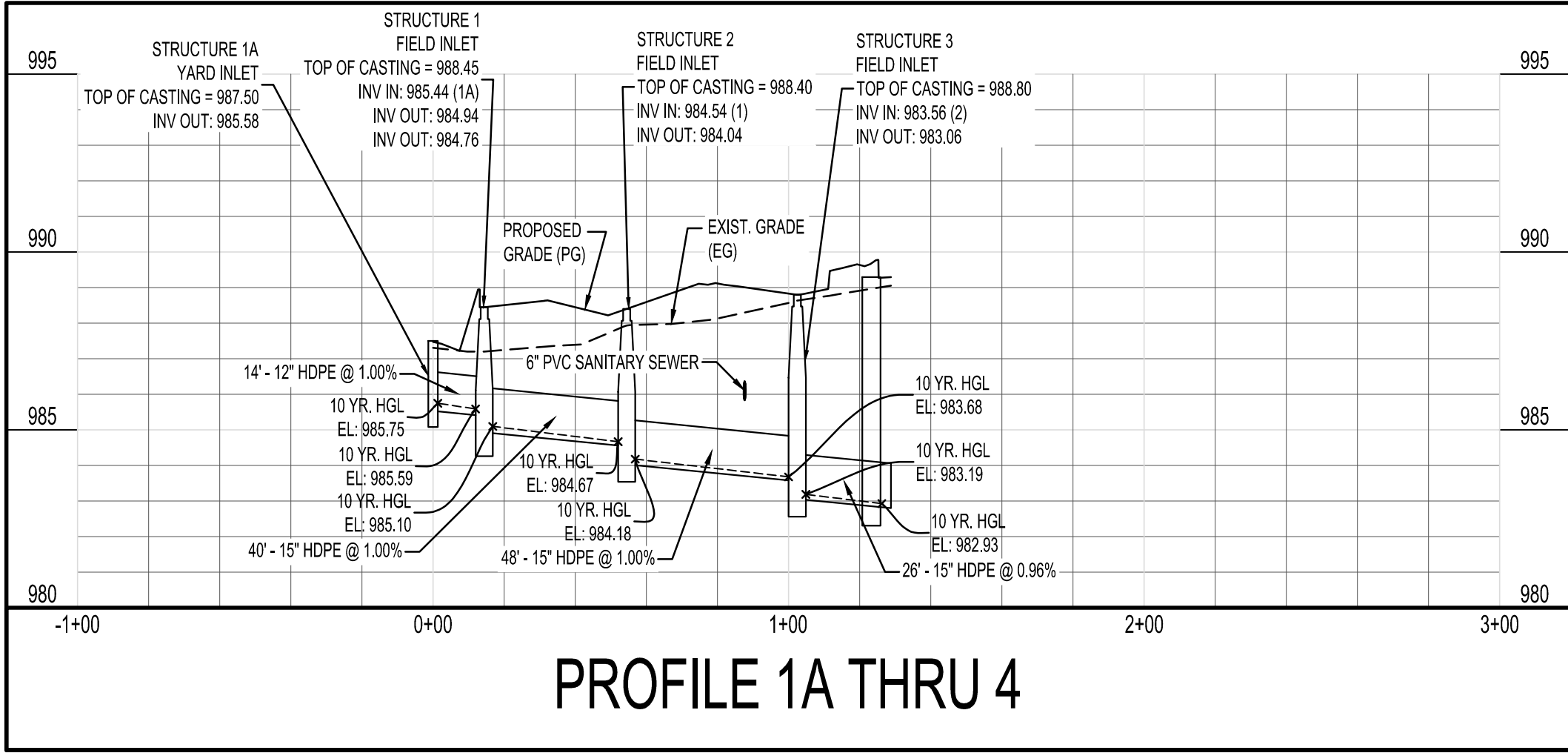


B. SHANE
SIGNATURE
FEBRUARY 14, 2025
DATE
B. SHANE
ENGINEER
NO. 202100076
MISSOURI
PROFESSIONAL

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN
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RCG
ISSUED DATE
07/30/2024
ISSUED FOR
PERMITTING
PROJECT NO.
22-218
FILE
22-218 C-2 Grading Plan

SHEET
C-2



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HIGH TIDE

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SIGNATURE

FEBRUARY 14, 2025

DATE

STATE OF MISSOURI

B. SHANE GUIN

NUMBER

202100076

PROFESSIONAL ENGINEER

STAMP

PROPOSED TAKE 5

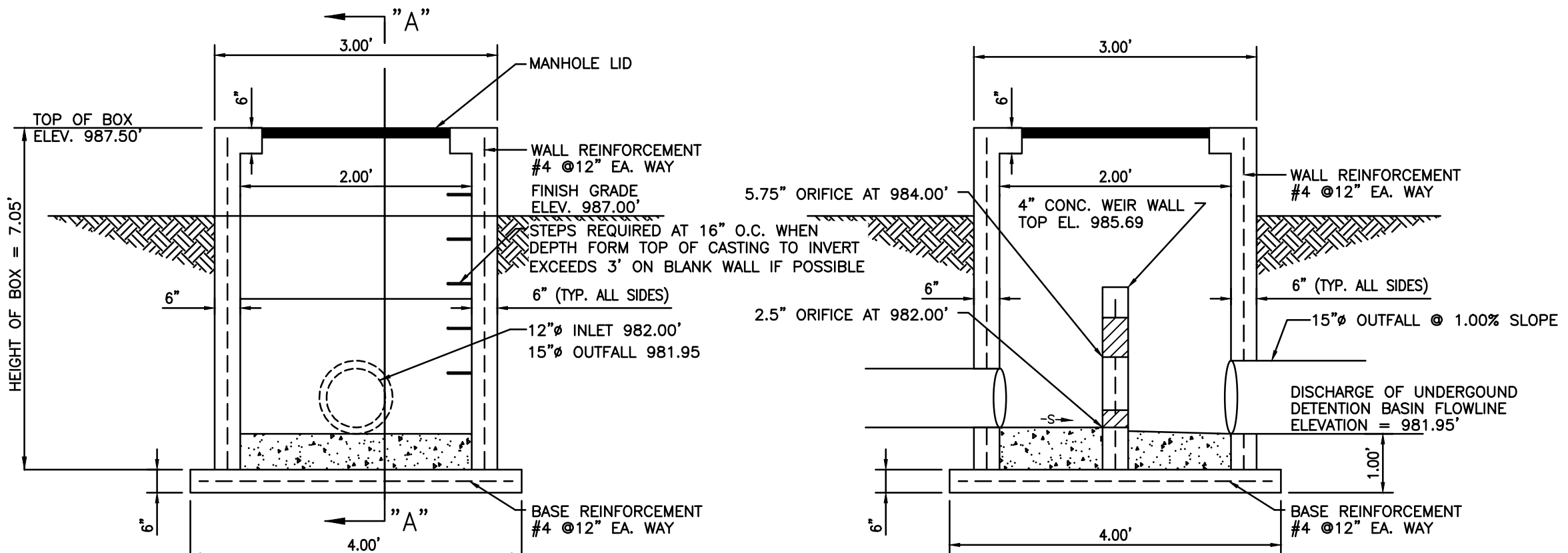
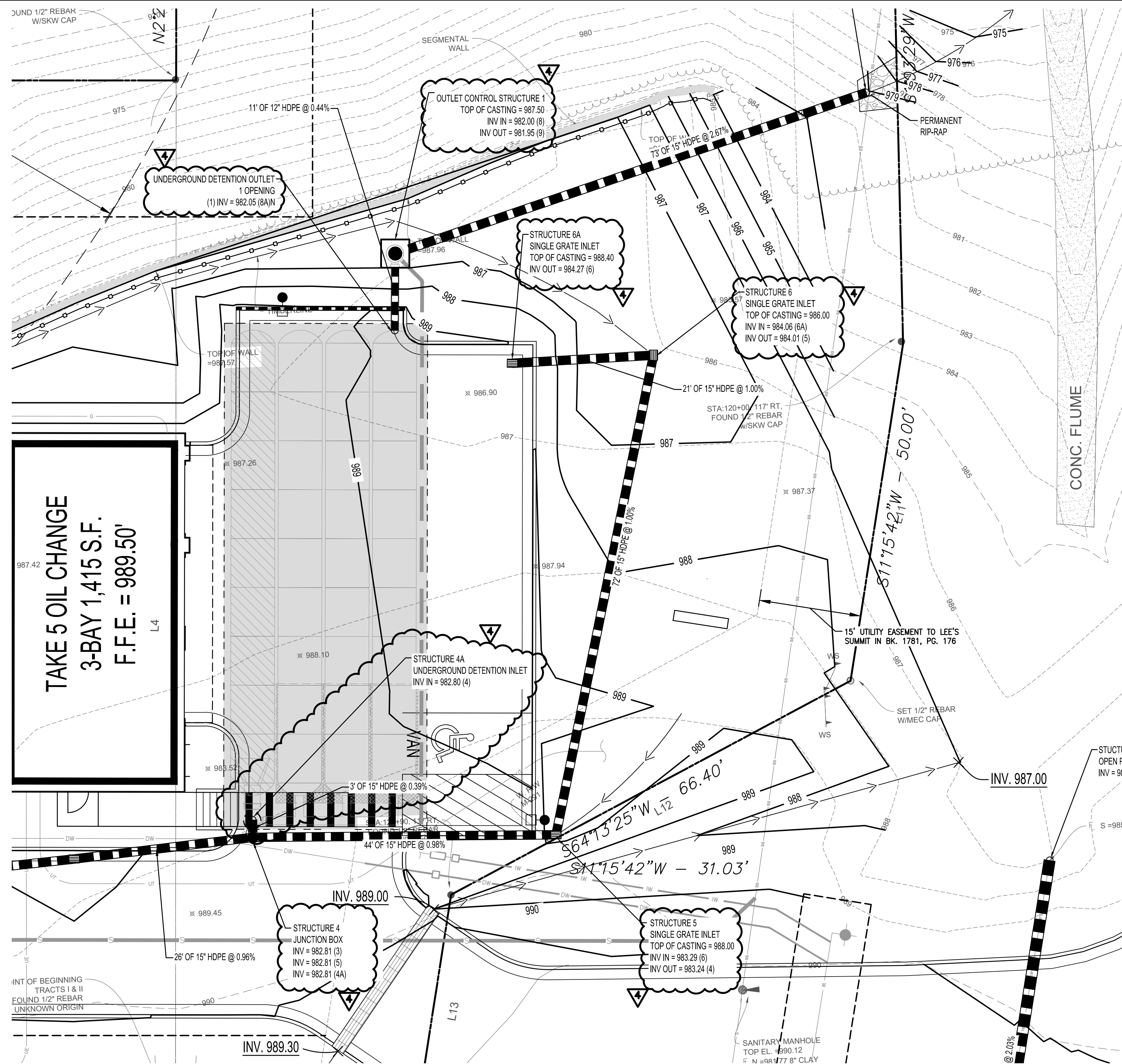
LEE'S SUMMIT, MISSOURI

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2101 PEARL STREET

BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 10/24/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 X-Layout
SHEET C-2.1



OUTLET CONTROL STRUCTURE 1
N.T.S.

ACCORDING TO THE MISSOURI DEPARTMENT OF NATURAL RESOURCES GIS MAP, NO EXISTING OIL AND GAS WELLS ARE LOCATED ON SITE.

NOTE:
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FLOOD NOTE:
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100 YEAR DESIGN STORAGE
VOLUME = 5,488 CF

REFER TO ADS STORM TECH DETAILS ON
SHEET C-4.5 FOR UNDERGROUND
DETENTION SECTIONS AND DETAILS

NOTE:
DETENTION BASIN SHALL BE CONSTRUCTED
PRIOR TO ALL OTHER WORK EXCEPT FOR
EROSION AND SEDIMENT CONTROL

NOTE:
AN AS-GRADED AND AS BUILT RECORD
DRAWINGS SHALL BE SUBMITTED AND
ACCEPTED BY THE CITY PRIOR TO ANY
CERTIFICATE OF OCCUPANCY BY THE CITY

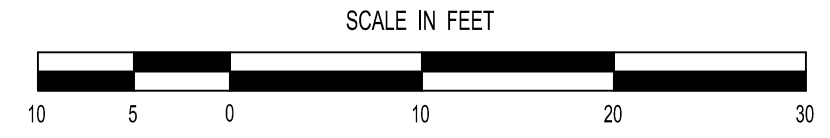
LEGEND - PROPOSED IMPROVEMENTS

- PROPERTY LINE
- PROPOSED BUILDING
- CG-1 CONCRETE CURB
- CG-1 CONCRETE CURB
w/ PAVEMENT TURN DOWN
- STORM DRAIN PIPE
- DOWNSPOUT COLLECTOR
N-12 PIPE (12" TRUNK W/ 8"
LATERAL CONNECTIONS)
SINGLE
GRATE
INLET
- 975 CONTOUR
- SLOPE ARROW

REFER TO SURVEY SHEETS FOR
LEGEND OF EXISTING FEATURES



DETENTION
BASIN PLAN



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1	12/16/2024 REVISED PER CITY	KRG
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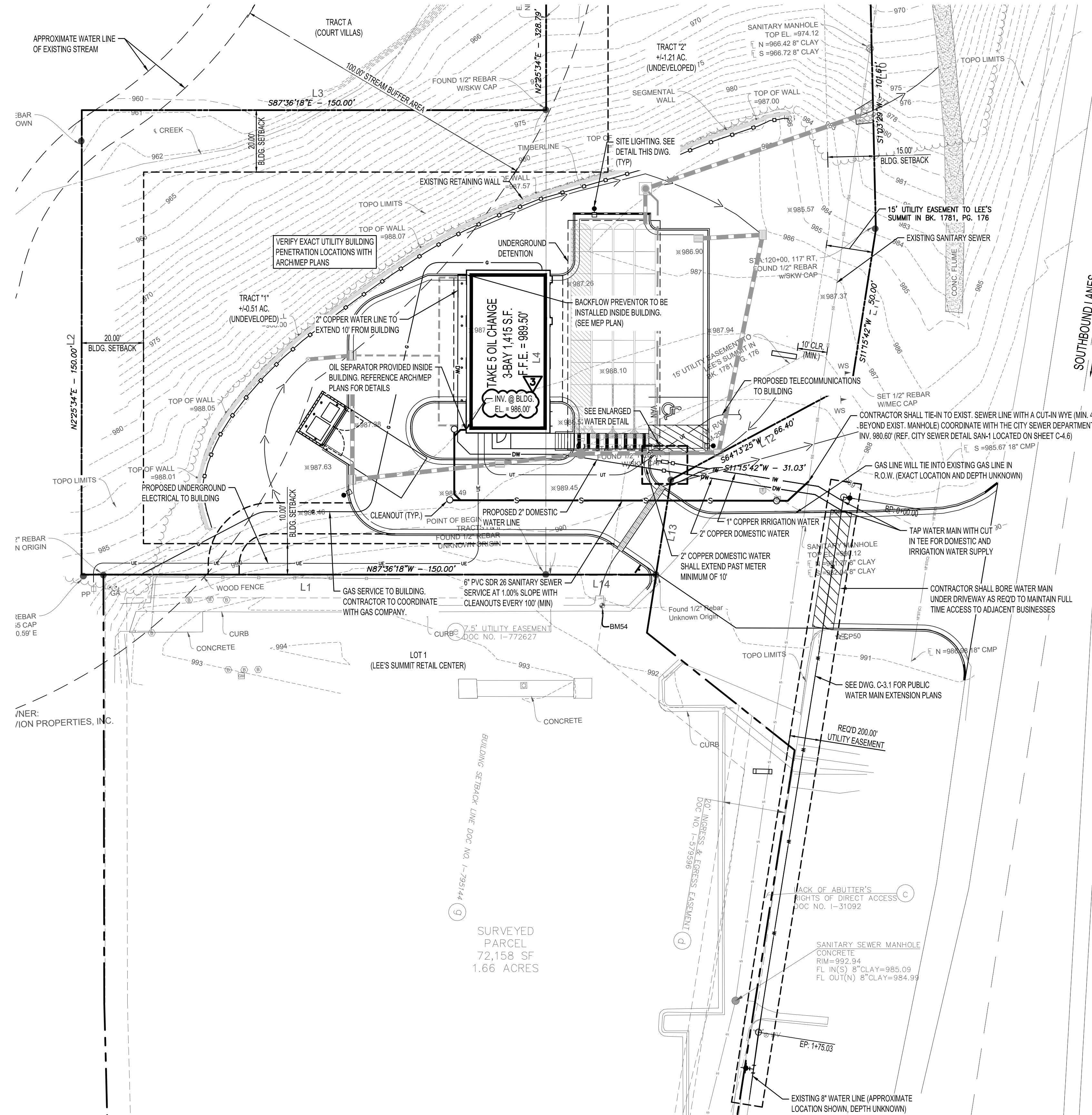
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SIGNATURE: [Signature]
DATE: FEBRUARY 14, 2025
STAMP: B. SHANE GUN NUMBER 202100076 PROFESSIONAL ENGINEER STATE OF MISSOURI

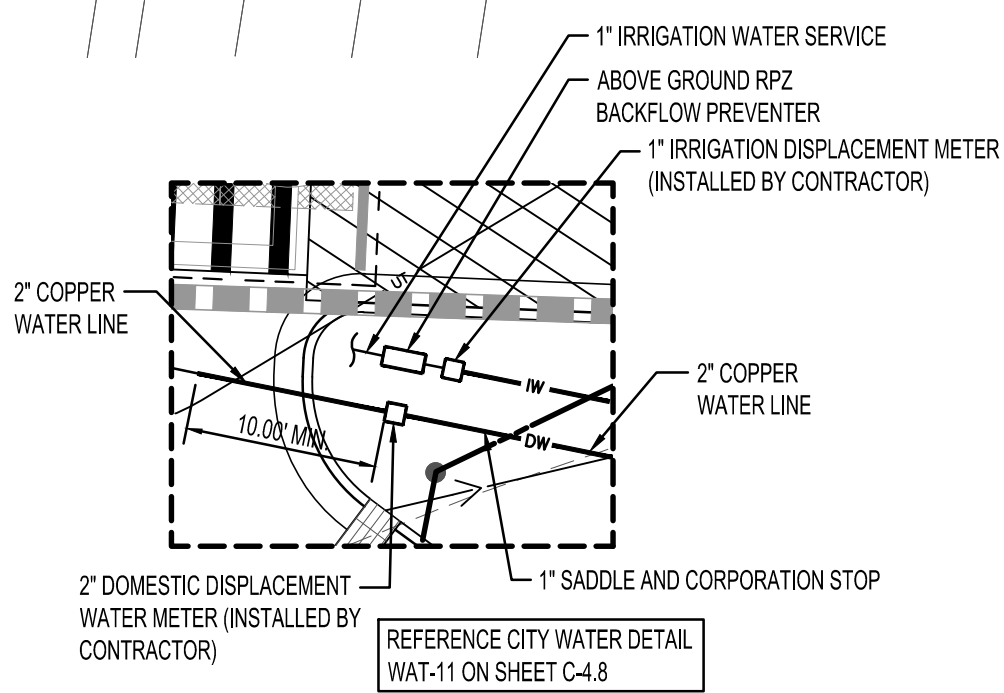
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SHEET
C-2.2



ACCORDING TO THE MISSOURI DEPARTMENT OF NATURAL RESOURCES GIS MAP, NO EXISTING OIL AND GAS WELLS ARE LOCATED ON SITE.



ENLARGED WATER DETAIL
N.T.S.

UTILITY NOTES

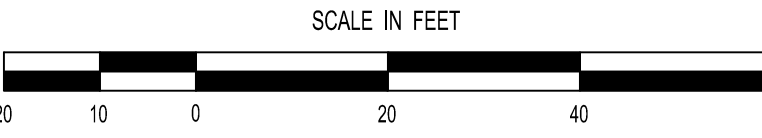
- ALL FILL MATERIAL IS TO BE IN PLACE AND COMPACTED BEFORE INSTALLATION OF PROPOSED UTILITIES.
- ALL SEWER UTILITY WORK SHALL BE DONE TO THE CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATIONS. ALL WATER UTILITY WORK SHALL BE DONE TO LEE'S SUMMIT WATER UTILITIES STANDARDS AND SPECIFICATIONS. ALL ELECTRICAL UTILITY WORK SHALL BE DONE IN ACCORDANCE WITH LOCAL STANDARDS AND SPECIFICATIONS. ALL GAS UTILITY WORK SHALL BE DONE TO LOCAL GAS STANDARDS AND SPECIFICATIONS.
- CONTRACTOR SHALL NOTIFY THE UTILITY AUTHORITIES INSPECTORS 72 HOURS BEFORE CONNECTING TO ANY EXISTING LINE. CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
- SANITARY SEWER PIPE SHALL BE AS FOLLOWS:
 - 6\"/>
- WATER LINES SHALL BE AS FOLLOWS:
 - WATER SERVICE LINES GREATER THAN 1\", BUT LESS THAN 4\"/>
 - FROM THE CITY'S MAIN TO THE CURB VALVE SHALL BE FLEXIBLE OR RIGID TYPE \"K\" COPPER.
 - FROM THE CURB VALVE TO 10' BEYOND THE METER WELL SHALL BE FLEXIBLE OR RIGID TYPE \"K\" OR \"L\" COPPER. ALSO, COPPER MUST BE USED OUTSIDE THE BUILDING WALL OF THE PREMISES SERVED, A MINIMUM OF 10'.
- MINIMUM TRENCH WIDTH SHALL BE 2 FEET.
- ON COPPER WATER SERVICE LINE, JOINTS (EXCLUDING JOINTS ON PRE-PURCHASED \"METER SETTER\") SHALL BE FLARED, COMPRESSION, OR BRAZED. USE OF ANY OTHER TYPE OF JOINT IS PROHIBITED, UNLESS SPECIFICALLY AUTHORIZED BY THE CITY.
- ALL WATER AND SANITARY SEWER UTILITIES SHOULD MAINTAIN A MINIMUM TEN (10') OF HORIZONTAL SEPARATION OR, WHEN CROSSING, 18\"/>
- CONTRACTOR SHALL MAINTAIN A MINIMUM OF 4'-0\"/>
- IN THE EVENT OF A VERTICAL CONFLICT BETWEEN WATER LINES, SANITARY LINES, STORM LINES AND GAS LINES (EXISTING AND PROPOSED), THE SANITARY LINE SHALL BE DUCTILE IRON PIPE WITH MECHANICAL JOINTS AT LEAST 10 FEET ON BOTH SIDES OF CROSSING. THE WATER LINE SHALL HAVE MECHANICAL JOINTS WITH APPROPRIATE THRUST BLOCKING AS REQUIRED TO PROVIDE A MINIMUM OF 18\"/>
- LINES UNDERGROUND SHALL BE INSTALLED, INSPECTED AND APPROVED BEFORE BACKFILLING.
- TOPS OF EXISTING MANHOLES SHALL BE ADJUSTED AS NECESSARY TO BE FLUSH WITH PROPOSED PAVEMENT ELEVATIONS. IN UNPAVED AREAS, EXISTING MANHOLE TOPS SHALL BE 6\"/>
- ALL CONCRETE FOR ENCASEMENTS SHALL HAVE A MINIMUM 28 DAY COMPRESSION STRENGTH AT 3000 P.S.I. REFER TO ARCHITECTURAL AND MEP DRAWINGS FOR EXACT BUILDING TIE-IN LOCATIONS OF ALL UTILITIES.
- CONTRACTOR IS RESPONSIBLE FOR COMPLYING TO THE SPECIFICATIONS OF THE LOCAL AUTHORITIES WITH REGARDS TO MATERIALS AND INSTALLATION OF THE WATER AND SEWER LINES.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES. THIS AND THE FINAL CONNECTIONS OF THE SERVICE SHALL BE COMPLETED 30 DAYS PRIOR TO BUILDING OCCUPANCY.
- CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR INSTALLATION REQUIREMENTS AND SPECIFICATIONS.
- REFER TO BUILDING PLANS FOR SITE LIGHTING ELECTRICAL PLAN.
- CONTRACTOR SHALL COORDINATE WITH THE CITY OF LEE'S SUMMIT PUBLIC WORKS FOR TIE-IN LOCATIONS TO PUBLIC UTILITIES PRIOR TO COMMENCEMENT OF WORK.
- EXISTING PUBLIC WATER LINE SHOWN IS BASED ON APPROXIMATE LOCATION PER MAP PROVIDED BY LEE'S SUMMIT WATER UTILITIES. EXACT SIZE AND LOCATION SHALL BE VERIFIED PRIOR TO START OF CONSTRUCTION. ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES.
- TOPOGRAPHIC INFORMATION IS TAKEN FROM A TOPOGRAPHIC SURVEY BY McCURE DATED 13 SEPTEMBER 2023. IF THE CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR TO THE OWNER FOR REVIEW.

LEGEND

- PROPERTY LINE
- PROPOSED BUILDING
- CG-1 CONCRETE CURB
- STORM DRAIN PIPE
- DOMESTIC WATER LINE
- SANITARY SEWER LINE
- GAS LINE
- UNDERGROUND TELEPHONE LINE (4\"/>
- UNDERGROUND ELECTRICAL LINE
- CLEANOUT
- SITE LIGHTING FIXTURE



UTILITY PLAN



REVISION		BY
1	10/24/2024 REVISED PER CITY	KRG
2	12/16/2024 REVISED PER CITY	KRG
3	1/16/2025 REVISED PER CITY	KRG

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com

SIGNATURE:
DATE: JANUARY 31, 2025

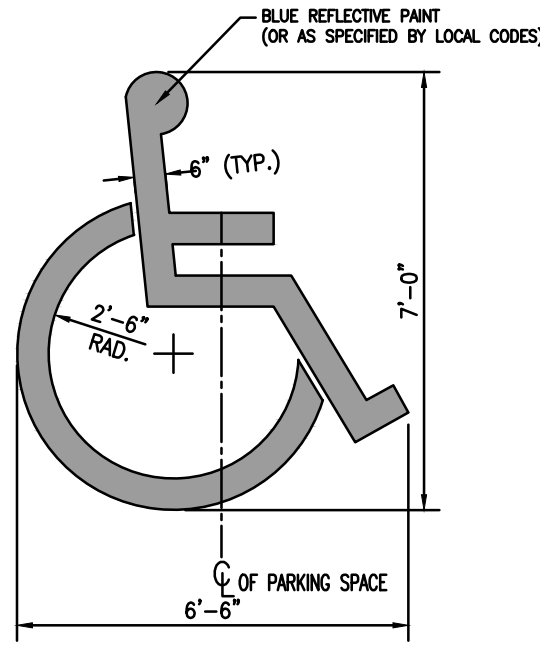
STATE OF MISSOURI
B. SHANE GINN
ENGINEER
NUMBER - 202100076
PROFESSIONAL

STAMP

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

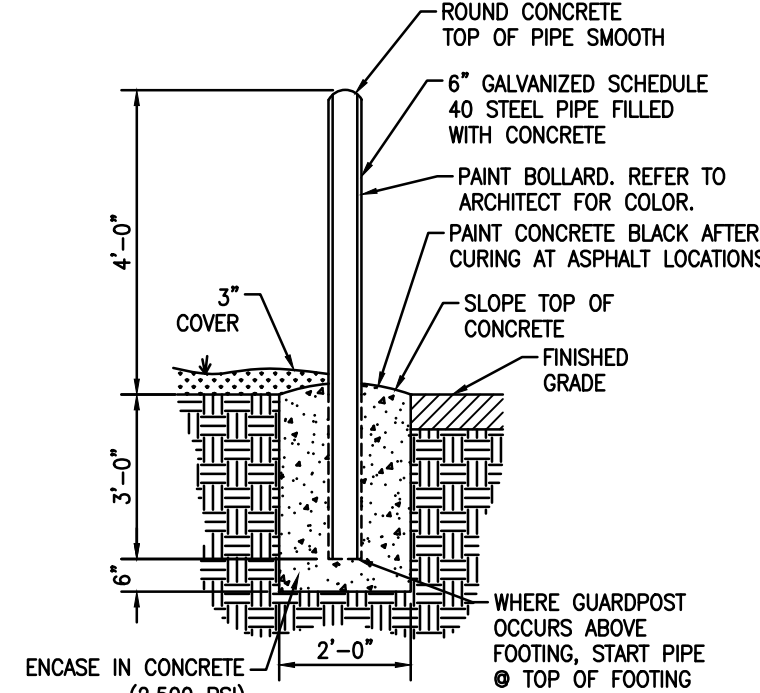
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN: KRG
CHECKED: RCG
ISSUED DATE: 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO.: 22-218
FILE: 22-218 C-3 Utility Plan
SHEET: C-3

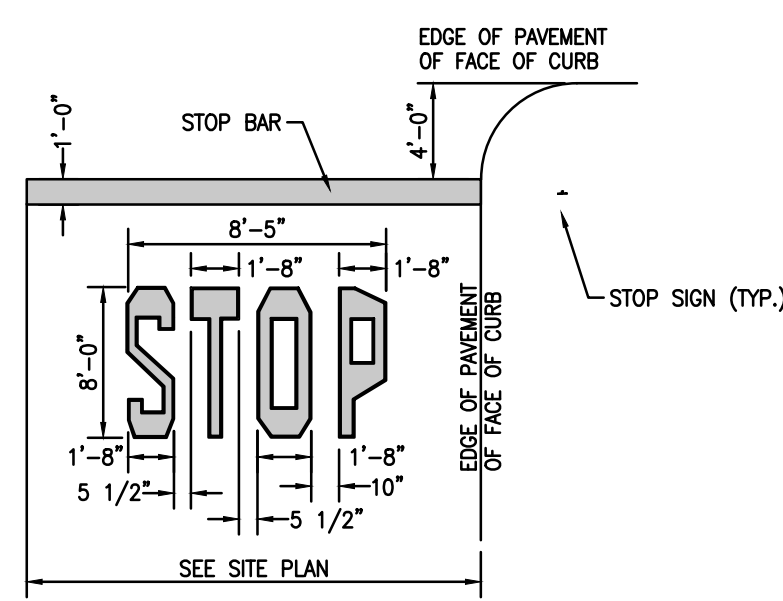


- NOTES:
1. VERIFY SYMBOL REQUIREMENTS WITH LOCAL JURISDICTION.
 2. ADD "VAN" LETTERING WHERE INDICATED ON SITE PLAN.
 3. LOCATE SYMBOL AT THE EDGE OF PARKING SPACE UNLESS ACCOMPANIED BY "VAN" LETTERING.

ACCESSIBLE PARKING SYMBOL DETAIL
N.T.S.

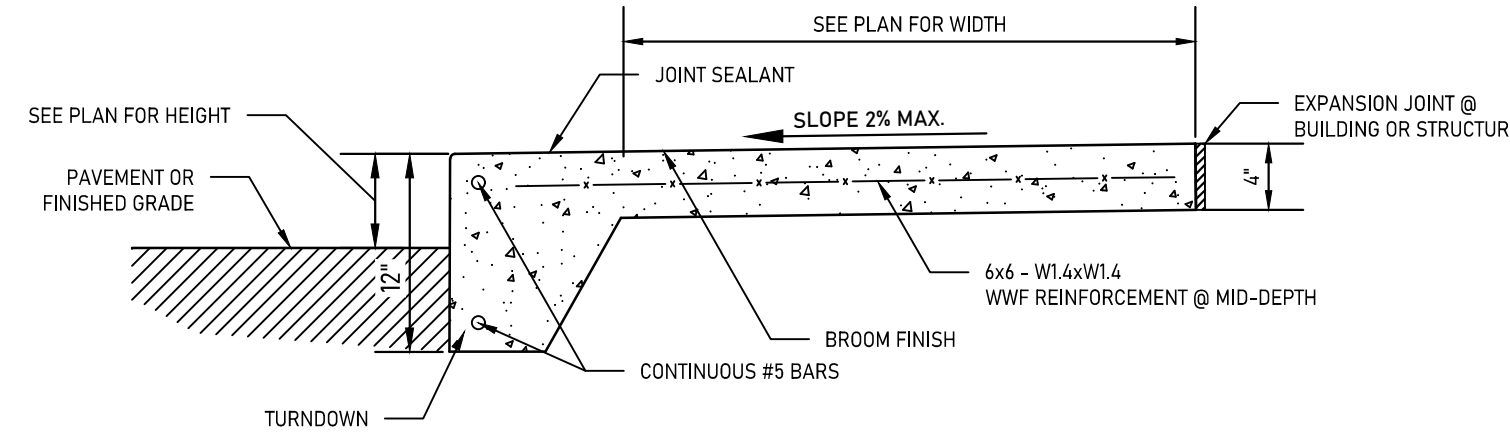


BOLLARD DETAIL
N.T.S.



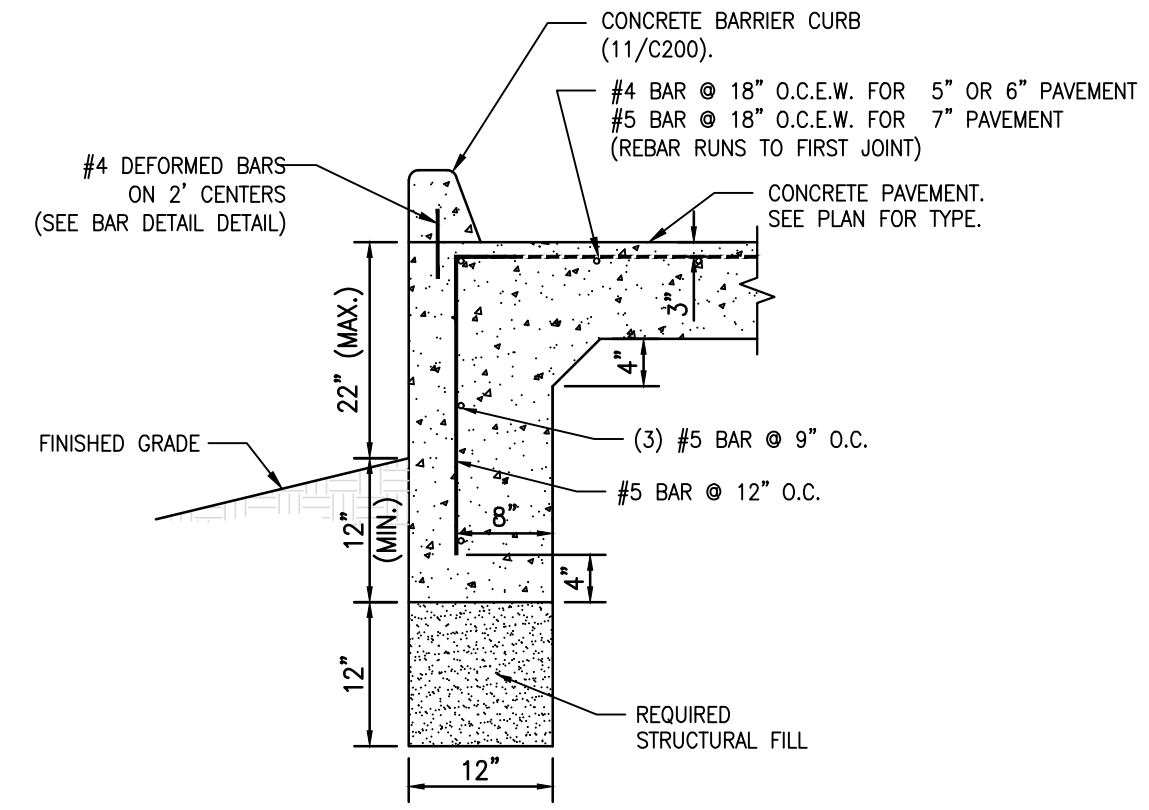
- NOTES:
1. WORDS AND ARROWS FOR DRIVEWAYS SHALL BE APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OUTLINED IN SECTION 3B OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.
 2. WORDS AND BAR TO BE PAINTED REFLECTIVE WHITE.
 3. ALL LETTERS AND SYMBOLS SHALL BE IN CONFORMANCE WITH THE STANDARD ALPHABETS FOR HIGHWAY SIGN AND PAVEMENT MARKINGS.

STOP BAR DETAIL
N.T.S.

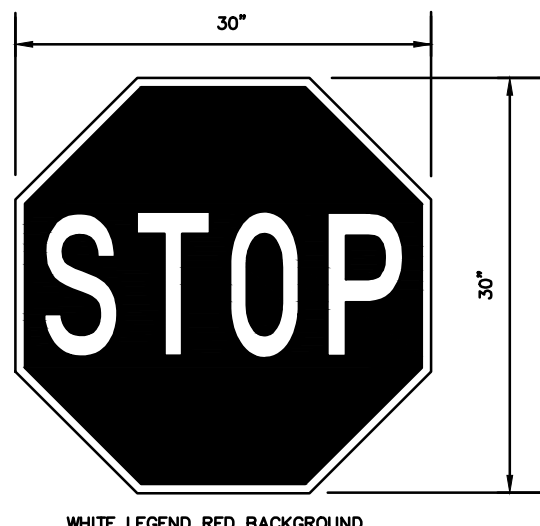


- CONCRETE SIDEWALK NOTES:
1. TOOLED JOINTS REQUIRED AT INTERVALS EQUAL TO WIDTH OF SIDEWALK.
 2. EXPANSION JOINTS REQUIRED AT 100' MAX. AND AT JUNCTIONS WITH CURBS, DRIVES, OR OTHER WALKS.
 3. SIDEWALK CROSS-SLOPES SHALL NOT EXCEED 2% (1:50).
 4. SIDEWALK LONGITUDINAL SLOPES SHALL NOT EXCEED 5% (1:20).

CONCRETE SIDEALK DETAIL
N.T.S.

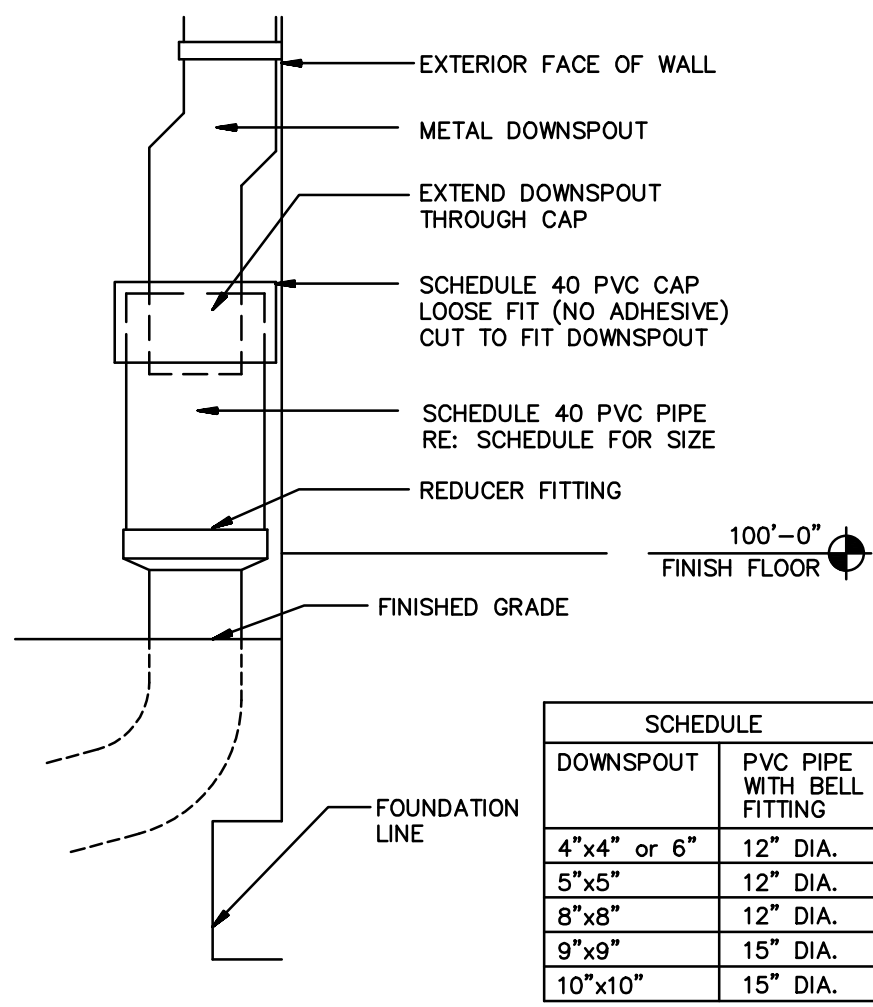


CONCRETE PAVEMENT TURN DOWN
N.T.S.

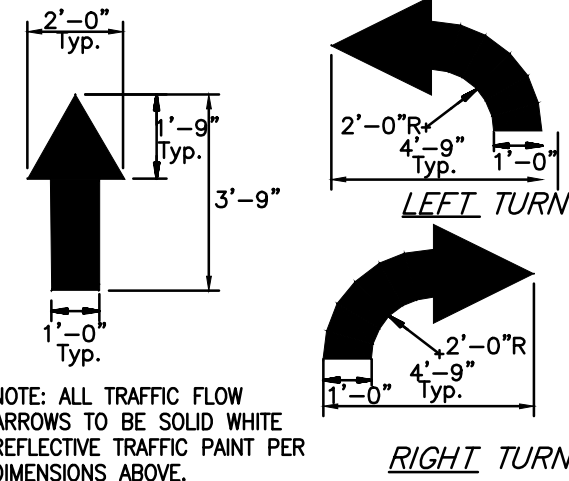


WHITE LEGEND RED BACKGROUND
R1-1

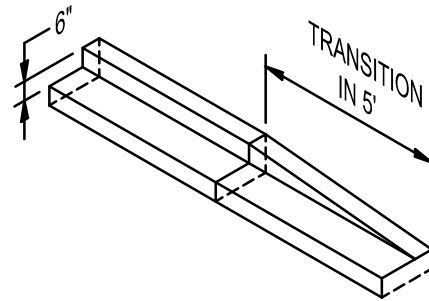
"STOP" SIGN
N.T.S.



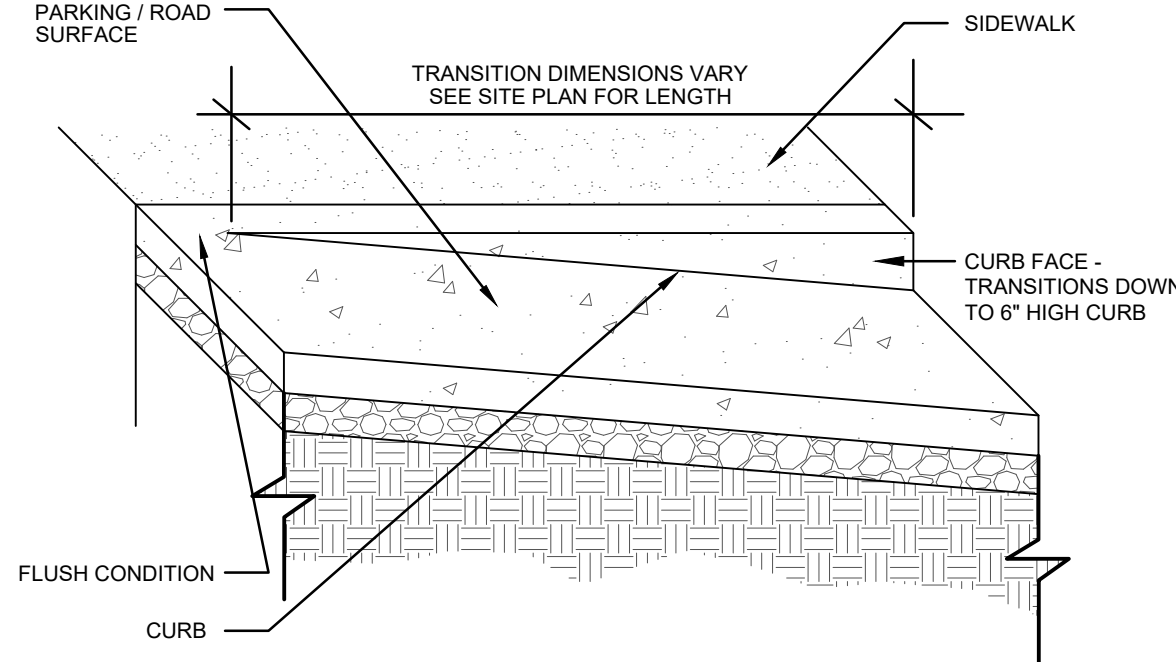
EXTERIOR DOWNSPOUT COLLECTOR
N.T.S.



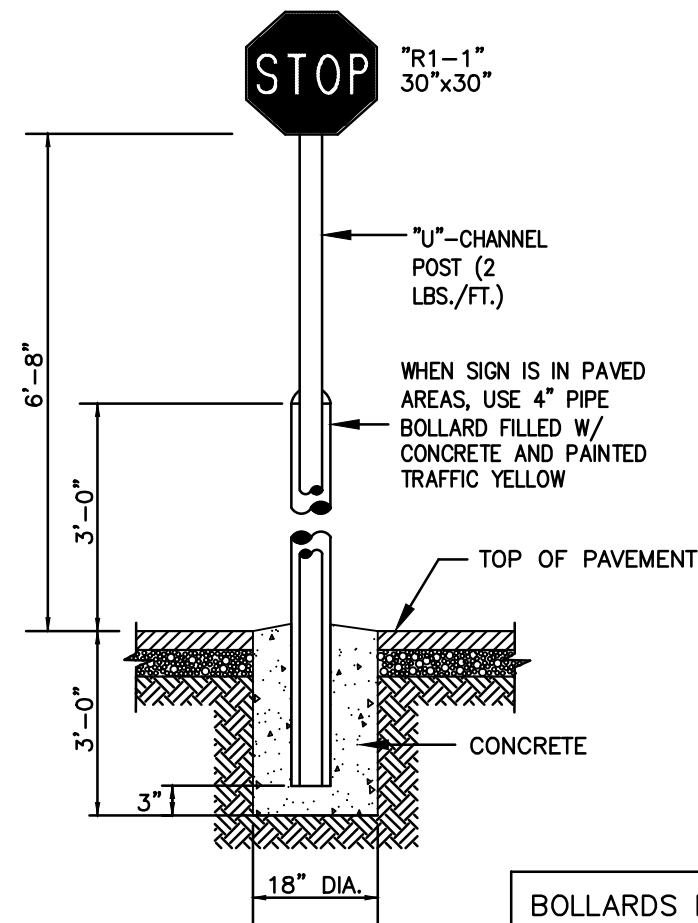
TRAFFIC FLOW ARROW
N.T.S.



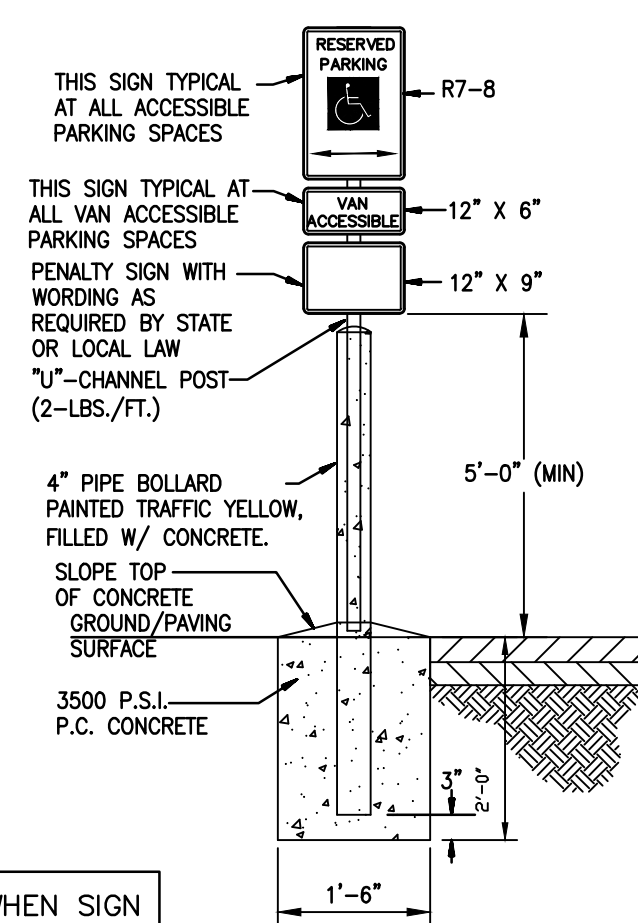
CURB TRANSITION DETAIL
N.T.S.



SIDEWALK TRANSITION DETAIL
N.T.S.

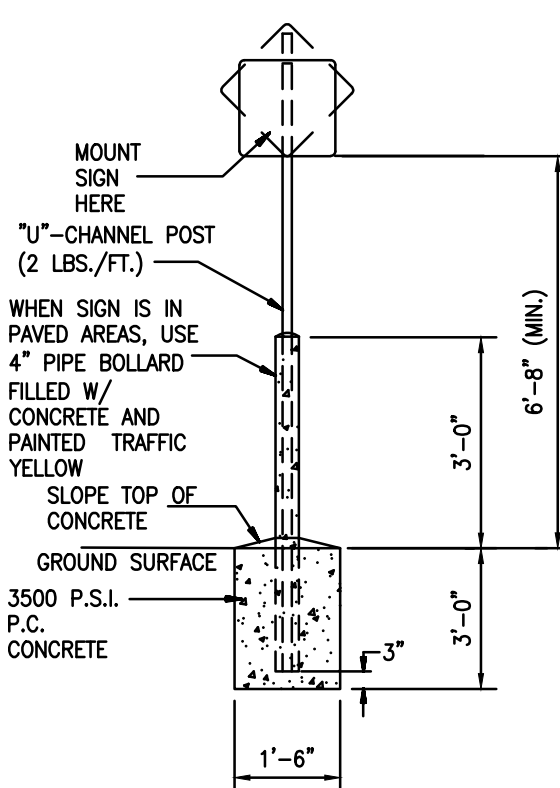


STOP SIGN W/ PIPE BOLLARD
N.T.S.



ACCESSIBLE PARKING SIGN
N.T.S.

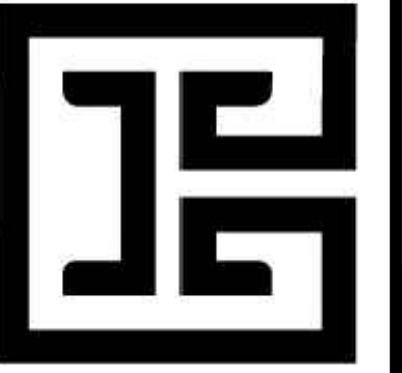
ALL SIGNS SHALL COMPLY WITH U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LOCAL CODES AND AS SPECIFIED. MOUNT SIGNS TO POST IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.



STANDARD SIGN BASE
N.T.S.

REVISION		BY
1	12/16/2024 REVISED PER CITY	KRG
2	1/17/2025 REVISED PER CITY	KRG

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com



Signature: [Signature]
DATE: JANUARY 31, 2025
Professional Engineer Seal: B. SHANE, GUN NUMBER 202100076, STATE OF MISSOURI

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 C-4 Details

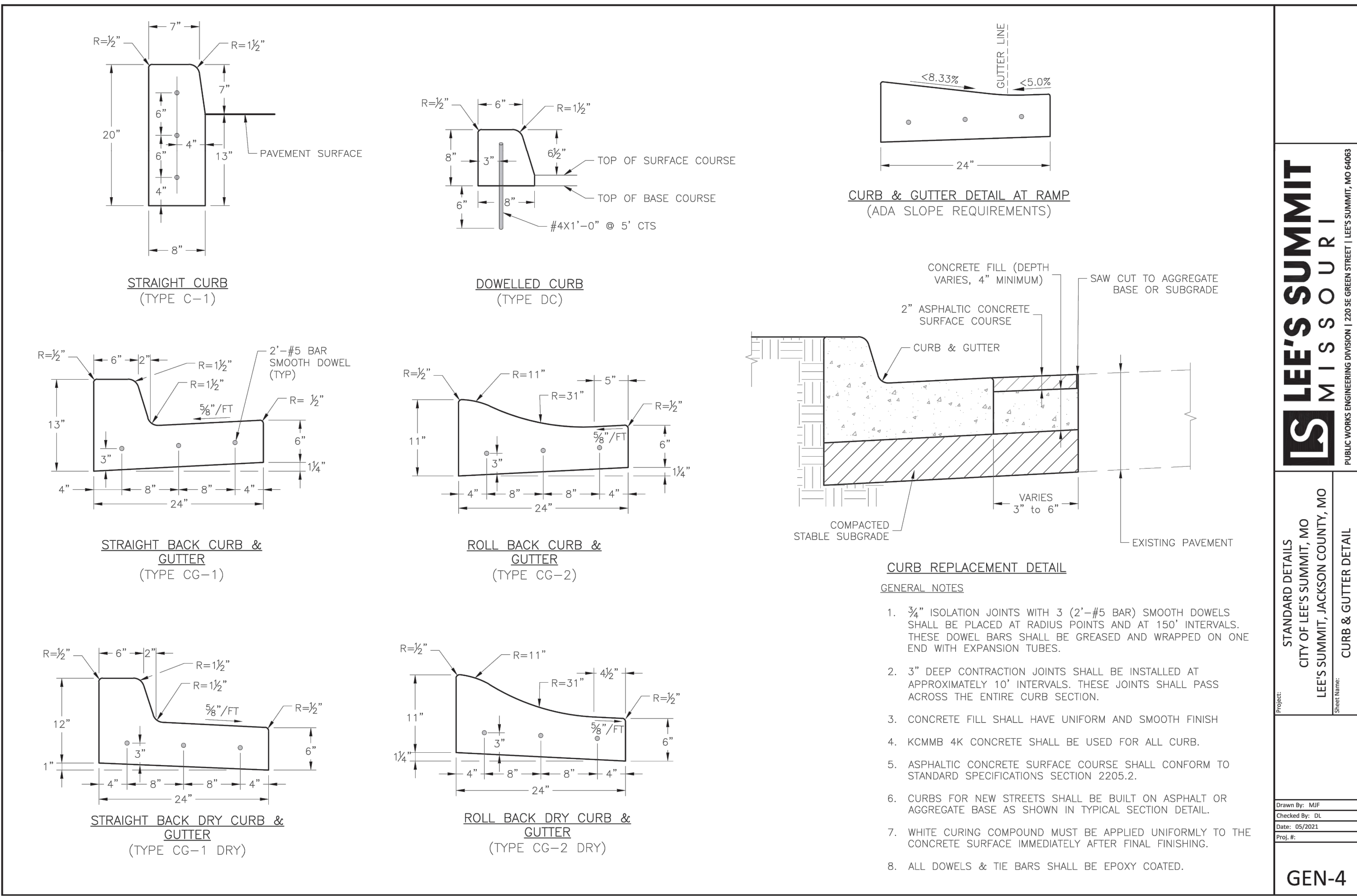
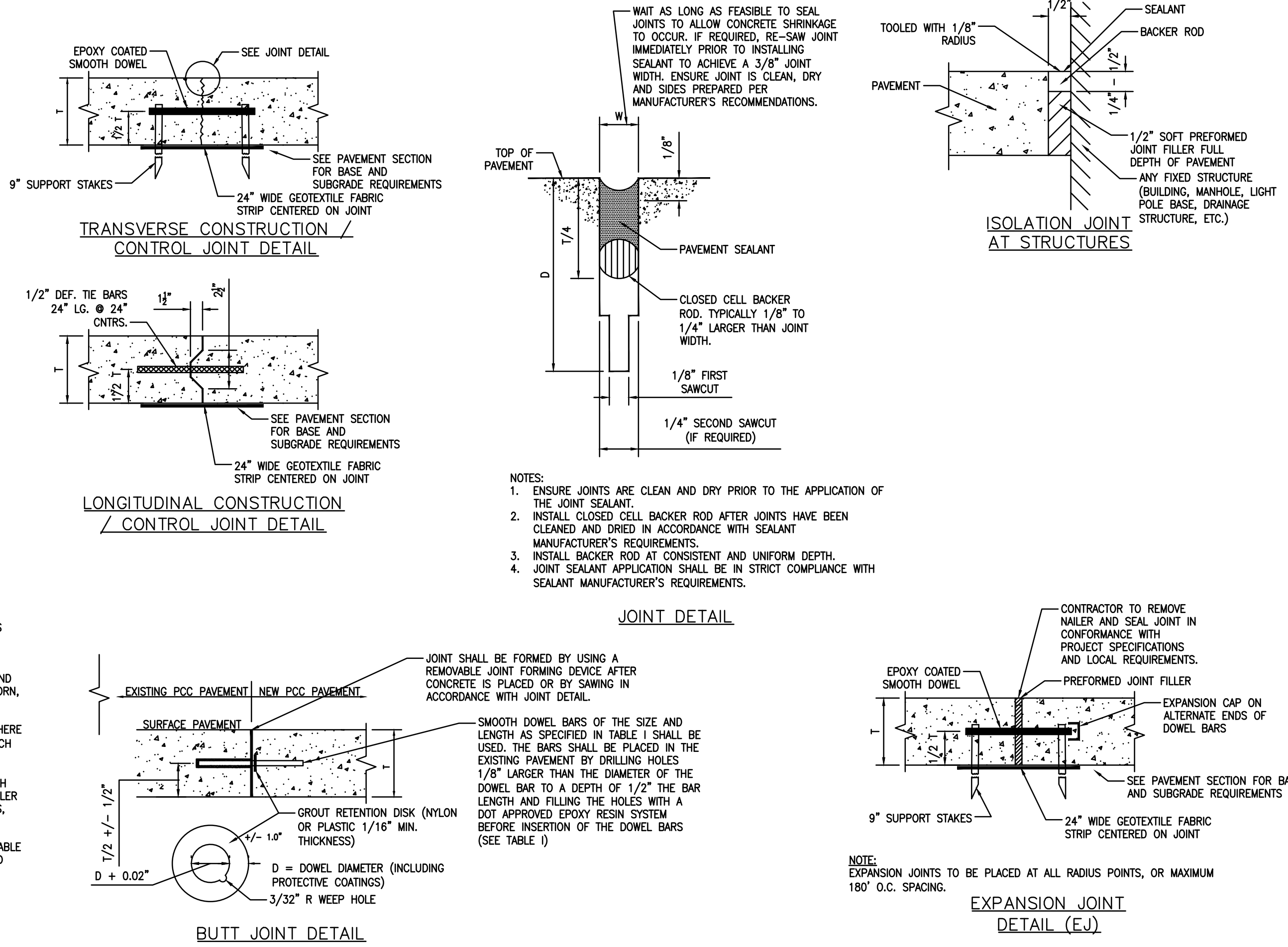


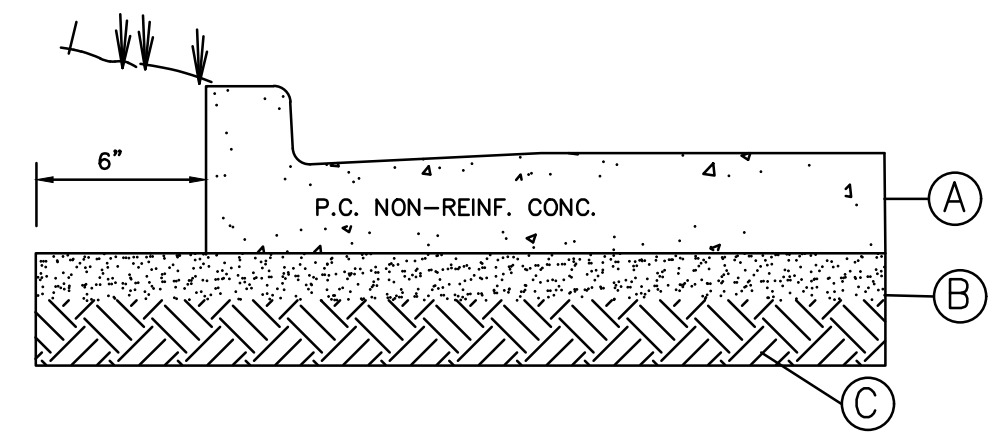
TABLE 1

PAVEMENT THICKNESS	SMOOTH DOWEL BARS				MINIMUM JOINT DEPTH
" T "	SIZE Ø	LENGTH	SPACING	" D "	" (IN)
5"	1 1/2"	12"	18"	1	1 1/2"
6"	3/4"	14"	12"	1	3/4"
7"	1"	16"	12"		2"
8"	1 1/4"	18"	12"		3"
9"	1 1/4"	18"	12"		3"
10"	1 1/4"	18"	12"	3	1 1/4"

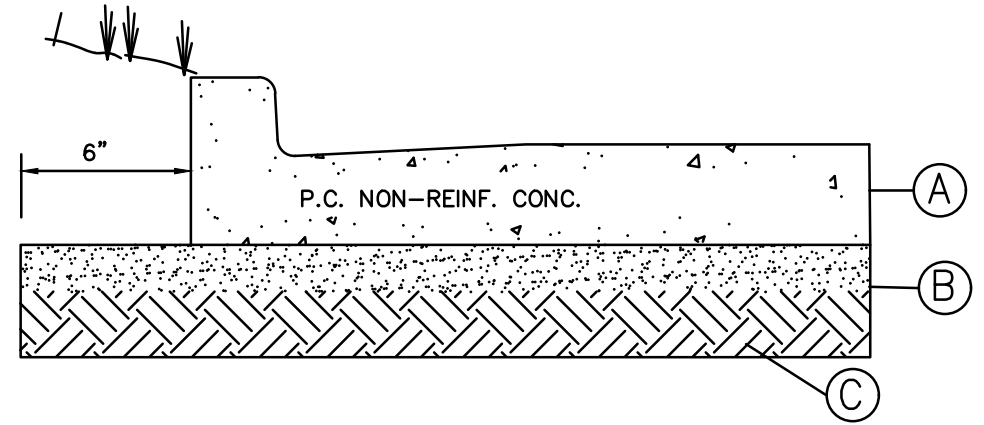
- NOTES:
- JOINTS SHALL BE SAWCUT AS SOON AS THE CONCRETE HAS REACHED SUFFICIENT STRENGTH TO SUPPORT THE SAWING EQUIPMENT AND TEARING OF CONCRETE DOES NOT OCCUR.
 - GEOTEXTILE FABRIC SHALL BE CONSTRUCTED OF NON-WOVEN POLYPROPYLENE FIBERS RESISTANT TO CHEMICAL ATTACK, MILDEW, AND ROT. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A GEOTEXTILE MATERIAL SUBMITTAL FOR APPROVAL.
 - CONSTRUCT EXPANSION, WEAKENED PLANE CONTROL (CONTRACTION), AND CONSTRUCTION JOINTS STRAIGHT WITH FACE PERPENDICULAR TO CONCRETE SURFACE.
 - CONSTRUCT CONTROL JOINTS FOR DEPTH EQUAL TO AT LEAST 1/4 OF THE CONCRETE THICKNESS AS FOLLOWS:
 - FORM TOOLED JOINTS IN FRESH CONCRETE BY GROOVING TOP WITH RECOMMENDED TOOL AND FINISHING EDGE WITH JOINTER.
 - FORM SAWED JOINTS USING POWERED SAWS EQUIPPED WITH SHATTERPROOF ABRASIVE OR DIAMOND RIMMED BLADES. CUT JOINTS INTO HARDENED CONCRETE AS SOON AS SURFACE WILL NOT BE TORN, ABRADED, OR OTHERWISE DAMAGED BY CUTTING ACTION.
 - CONSTRUCTION JOINTS: PLACE CONSTRUCTION JOINTS AT END OF PLACEMENTS AND AT LOCATIONS WHERE PLACEMENTS OPERATIONS ARE STOPPED FOR PERIOD OF MORE THAN 1/2 HOUR, EXCEPT WHERE SUCH PLACEMENTS TERMINATE AT EXPANSION JOINTS. CONSTRUCT JOINTS IN ACCORDANCE WITH DETAILS.
 - EXPANSION JOINTS: LOCATE EXPANSION JOINTS AT MAXIMUM OF 180'-0" ON CENTERS, MAXIMUM EACH WAY UNLESS OTHERWISE SHOWN ON THE CONSTRUCTION DRAWINGS. PROVIDE PRE-MOLDED JOINT FILLER FOR EXPANSION JOINTS ABUTTING CONCRETE CURBS, CATCH BASINS, MANHOLES, INLETS, STRUCTURES, SIDEWALKS, AND OTHER FIXED OBJECTS.
 - BUTT JOINTS: FOR JOINTS AGAINST EXISTING PAVEMENT, PLACE DOWELS OF THE SIZE INDICATED IN TABLE 1 INTO HOLES DRILLED INTO CENTER OF EXISTING SLAB. EPOXY DOWELS INTO HOLES WITH APPROVED EPOXY COMPOUND. PLACE DOWELS PRIOR TO CONCRETE PLACEMENT FOR NEW CONCRETE. DOWEL SPACING TO BE AS INDICATED IN TABLE 1. SAW JOINTS AND FILL WITH JOINT SEALER.
 - JOINT FILLERS: EXTEND JOINT FILLERS FULL WIDTH AND DEPTH OF JOINT, AND NOT LESS THAN 1/2-INCH OR MORE THAN 1-INCH BELOW FINISHED SURFACE WHERE JOINT SEALER IS INDICATED. FURNISH JOINT FILLERS IN 1 PIECE LENGTHS FOR FULL WIDTH BEING PLACED, WHEREVER POSSIBLE. WHERE MORE THAN 1 LENGTH IS REQUIRED LACE OR CLIP JOINT FILLER SECTIONS TOGETHER.
 - JOINT SEALANTS: JOINTS SHALL BE SEALED WITH APPROVED EXTERIOR PAVEMENT JOINT SEALANTS AND SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
 - DOWELS SHALL NOT BE PLACED CLOSER THAN 12" TO A JOINT INTERSECTION.



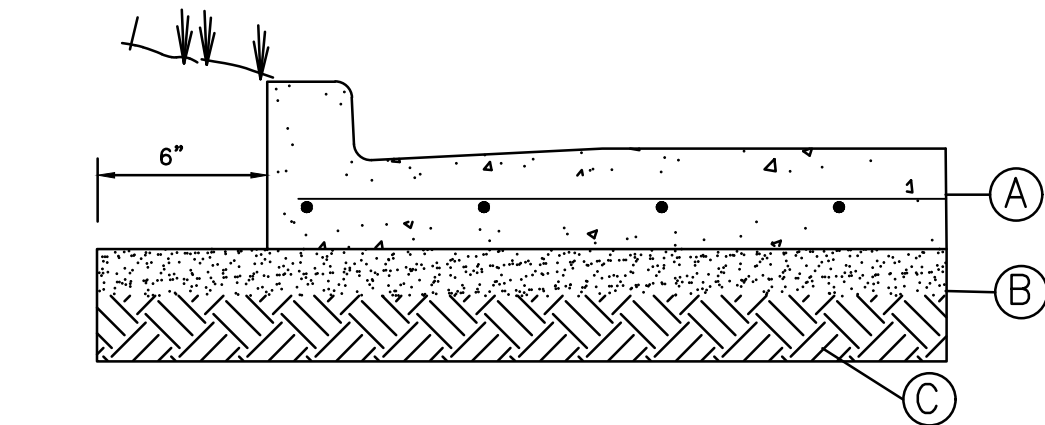
CONCRETE JOINT DETAILS
N.T.S.



STANDARD DUTY
CONCRETE PAVEMENT

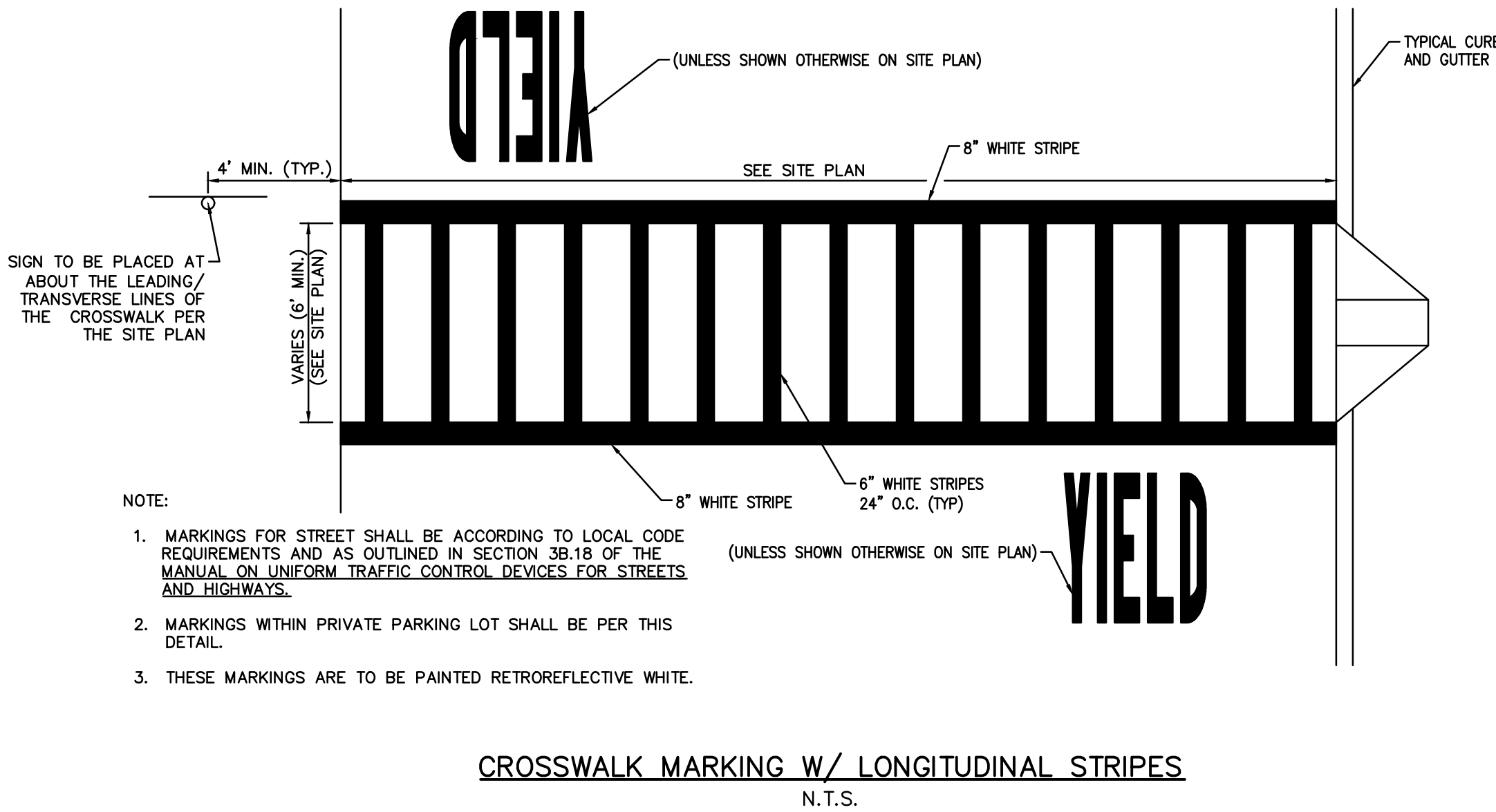


HEAVY DUTY
CONCRETE PAVEMENT



REINFORCED CONCRETE DUMPSTER PAD / APRON

- PAVEMENT NOTES (ALL SECTIONS):
- PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE KCMMB 4K MIX.
 - ALL PAVEMENT, BASE, FILL MATERIAL, AND SITE AND BUILDING PAD PREPARATION SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL REPORT RECOMMENDATIONS. REPORT PREPARED BY TERRACON ENGINEERING, LLC DATED 11/2/2023. TERRACON PROJECT NO. 0225253.
 - THE EXPOSED SUBGRADE SHOULD BE PROOFROLLED WITH A RUBBER TIERED VEHICLE WEIGHING 20 TONS. SOILS WHICH ARE OBSERVED TO RUT OR DEFLECT EXCESSIVELY UNDER THE MOVING LOAD SHALL BE UNDERCUT AND REPLACED WITH COMPACTED STRUCTURAL FILL.
 - AGGREGATE SHALL BE MODOT TYPE 5.
 - GRANULAR FILL SHALL BE PLACED IN MAXIMUM 8" LOOSE LIFTS AND COMPACTED TO 95% MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D 698 (STANDARD PROCTOR) WITHIN 3% OF OPTIMUM MOISTURE CONTENT.



CROSSWALK MARKING W/ LONGITUDINAL STRIPES
N.T.S.

SITE DETAILS

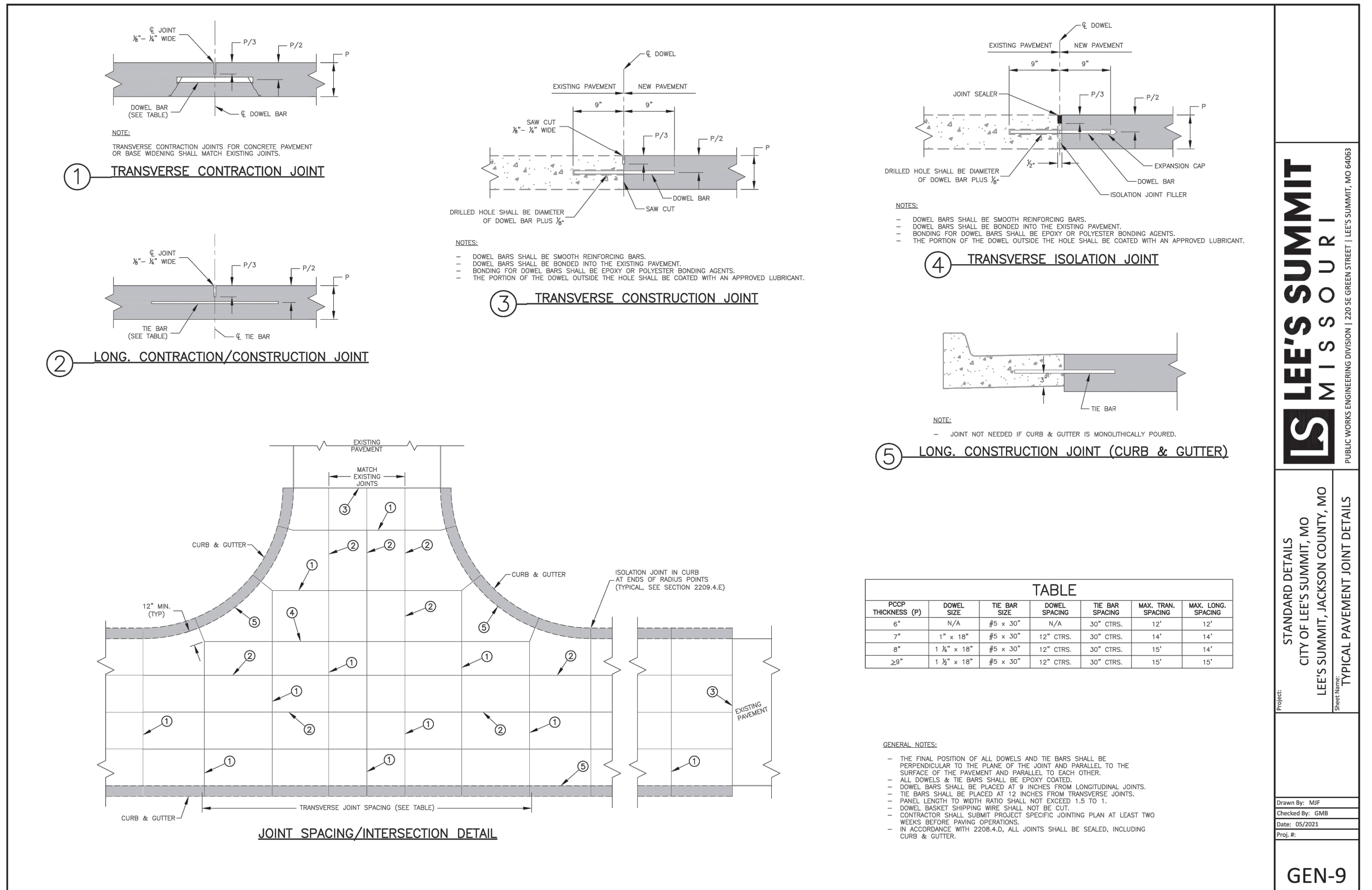
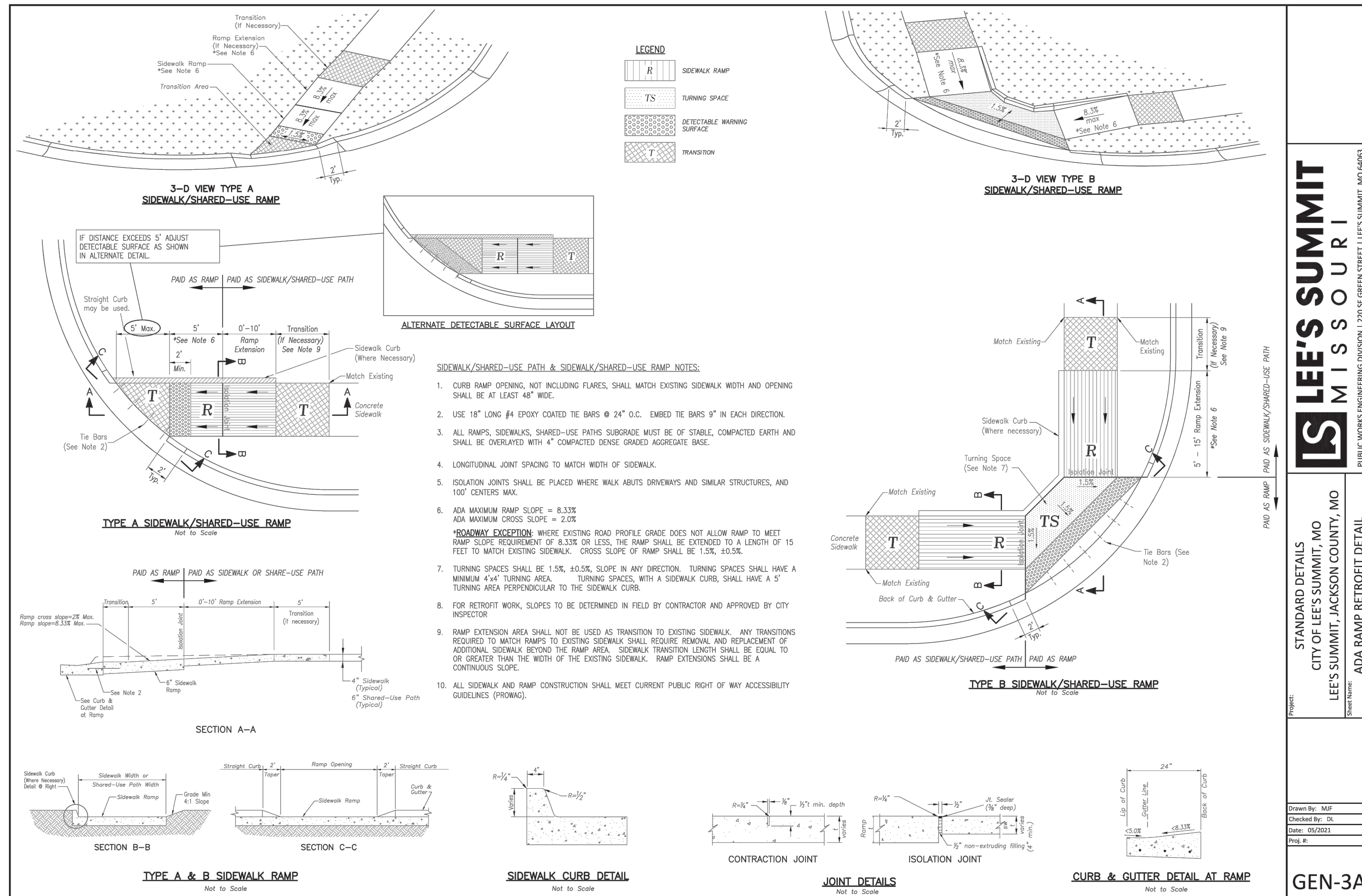
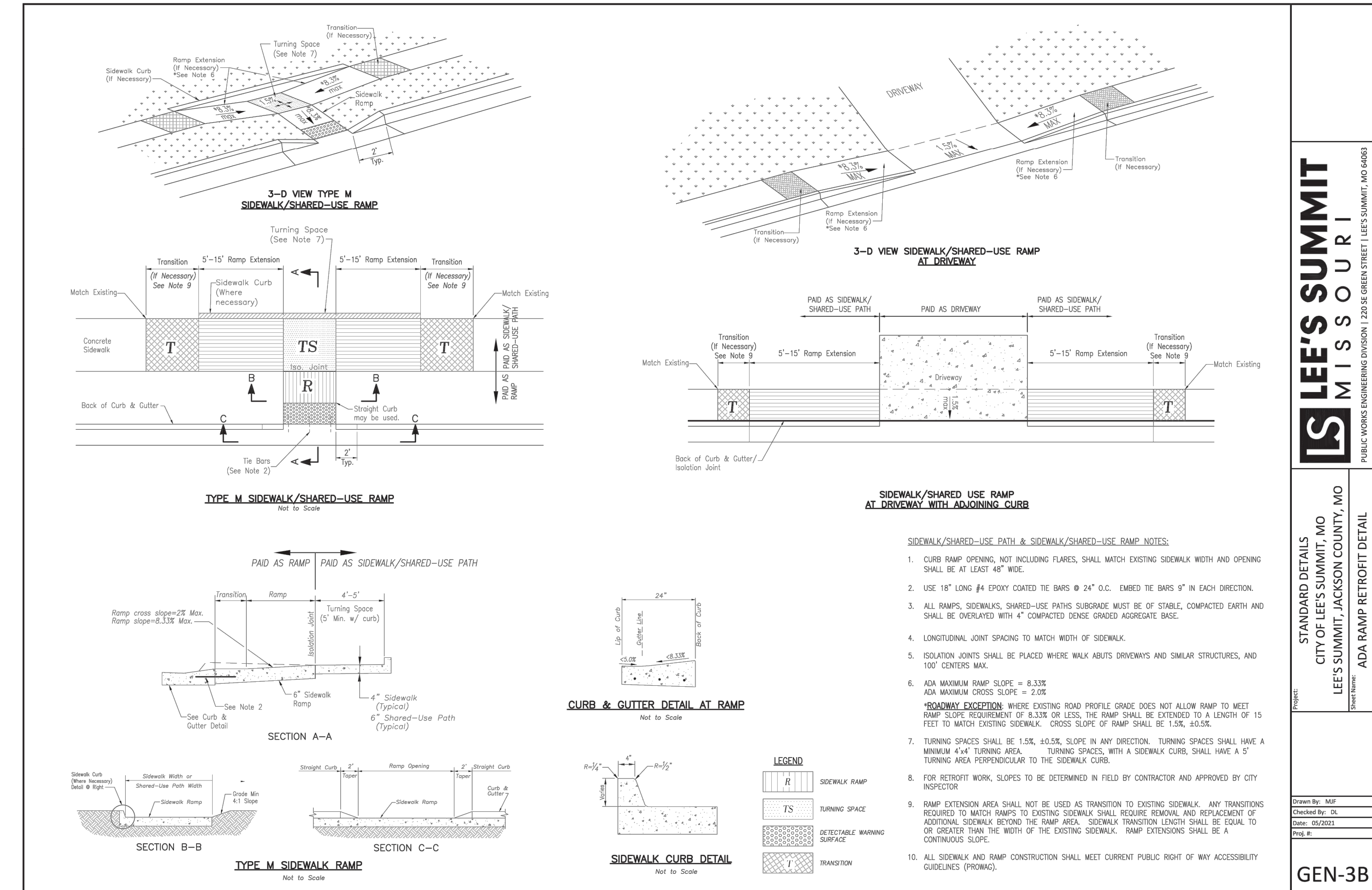
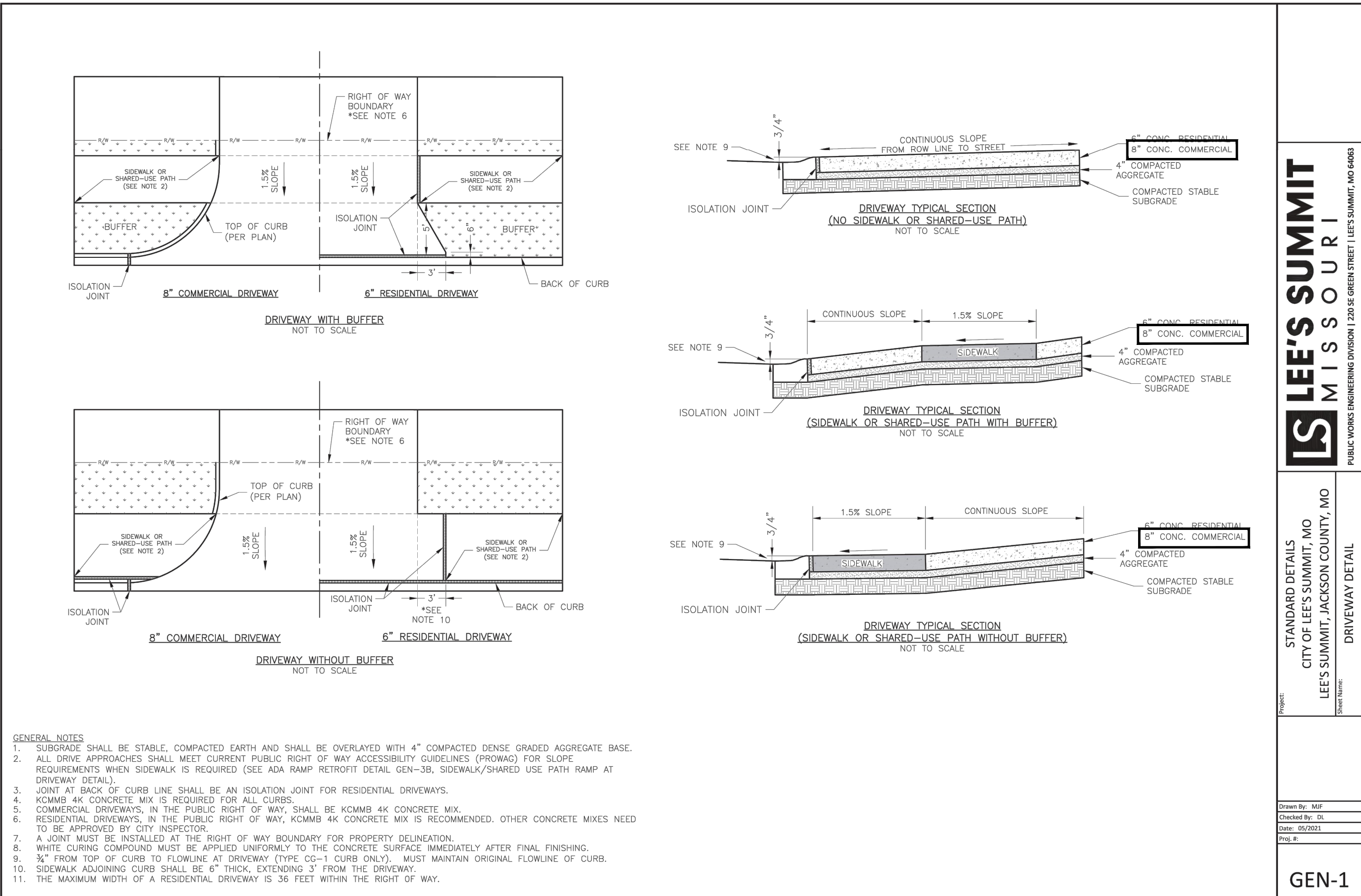
REVISION	BY

HIGH TIDE CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidelc.com

PROFESSIONAL ENGINEER
B. SHANE GUIN
NUMBER 202100076
STATE OF MISSOURI
DATE: FEBRUARY 10, 2025
SIGNATURE: [Signature]

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 C-4 Details
SHEET C-4.2



REVISION	BY

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com

LEE'S SUMMIT
MISSOURI
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
ADA RAMP RETROFIT DETAIL

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
ADA RAMP RETROFIT DETAIL

Drawn By: MGP
Checked By: JLS
Date: 06/2023
Proj #

GEN-3B

LEE'S SUMMIT
MISSOURI
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
TYPICAL PAVEMENT JOINT DETAILS

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
TYPICAL PAVEMENT JOINT DETAILS

Drawn By: MGP
Checked By: JLS
Date: 06/2023
Proj #

GEN-9

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN
KRG

CHECKED
RCG

ISSUED DATE
07/30/2024

ISSUED FOR
PERMITTING

PROJECT NO.
22-218

FILE
22-218 C-4 Details

SHEET
C-4.3

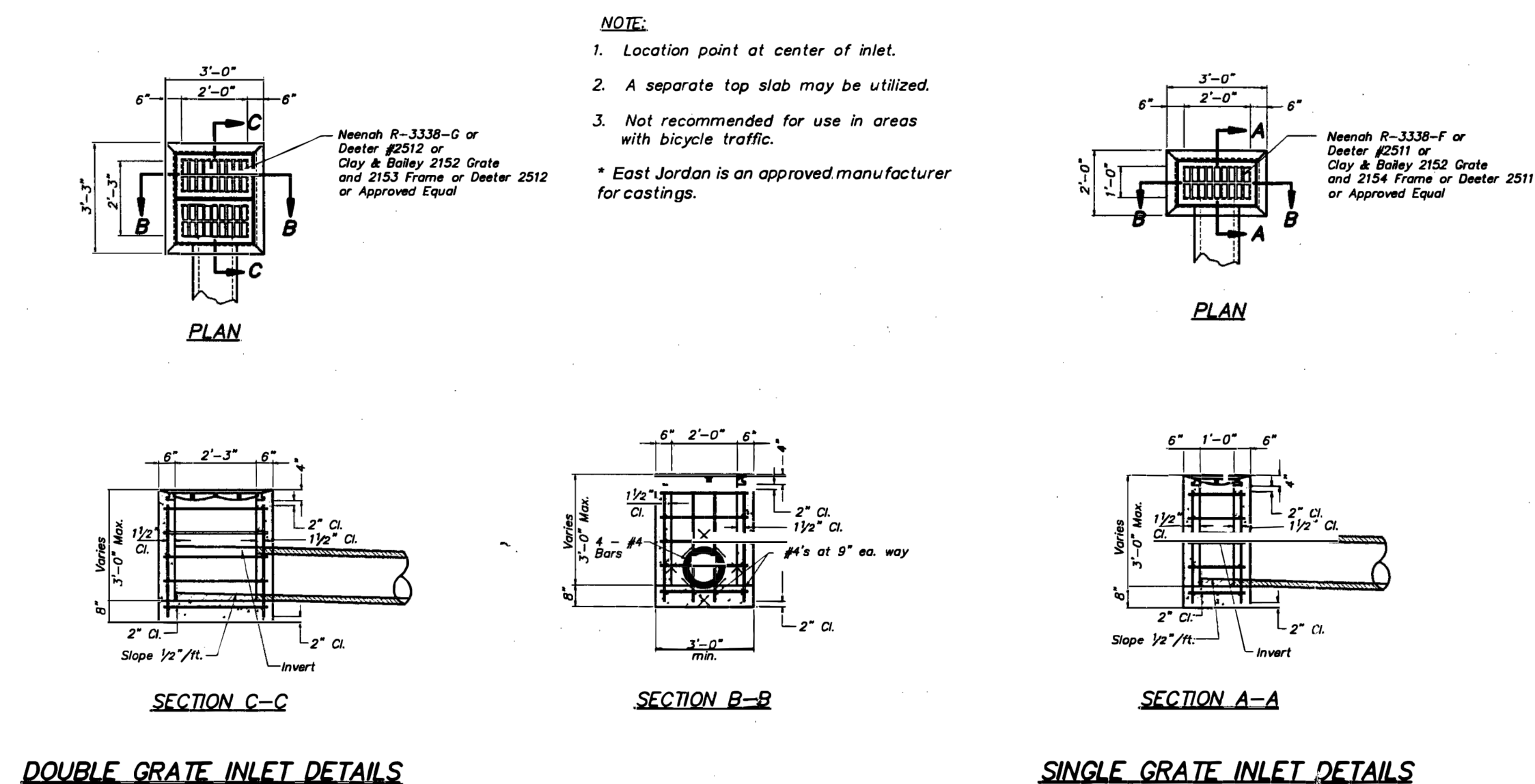



HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com

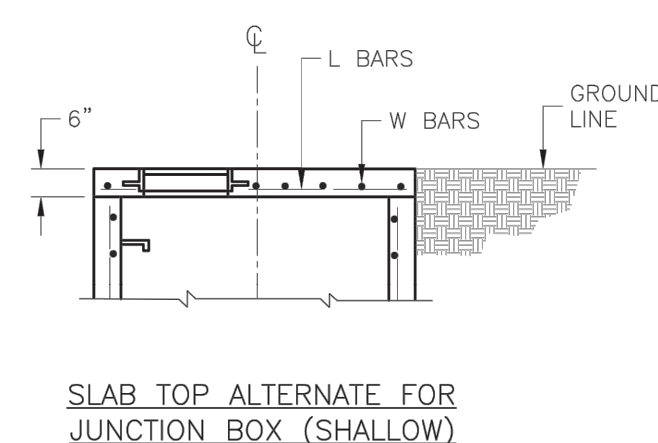
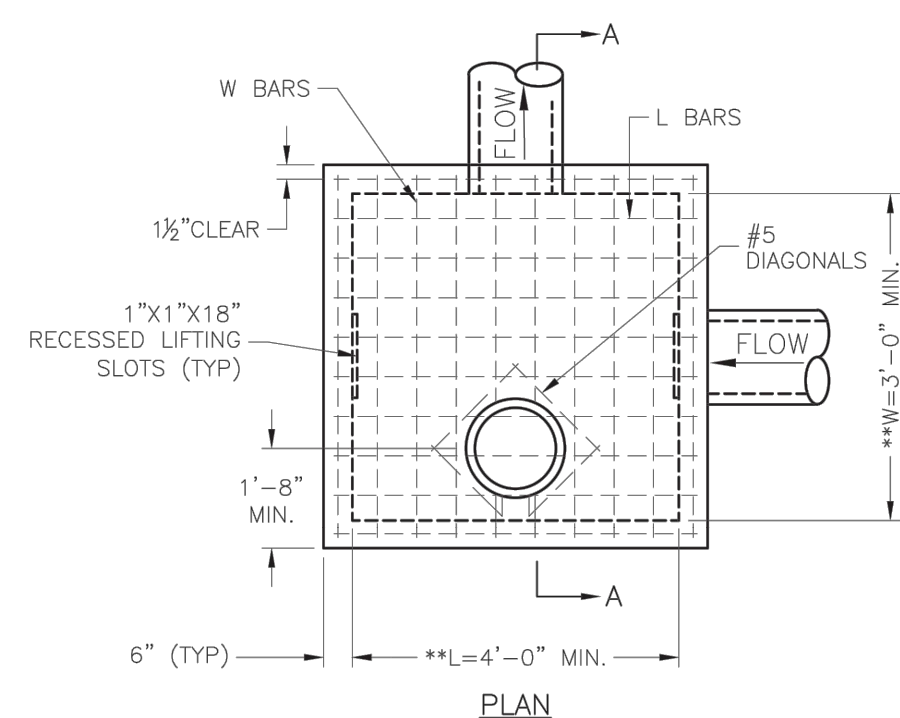
PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 C-4 Details
SHEET
C-4.4

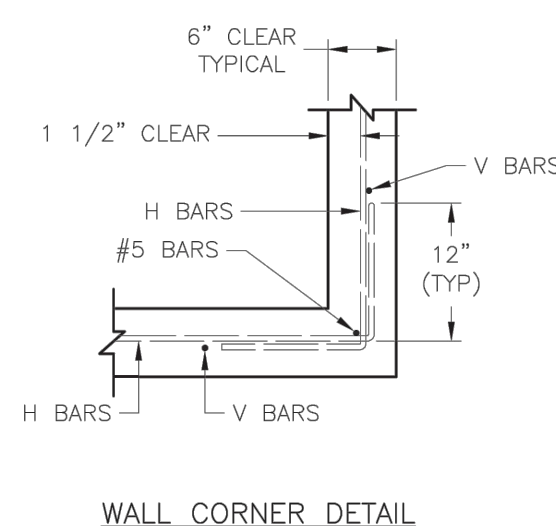
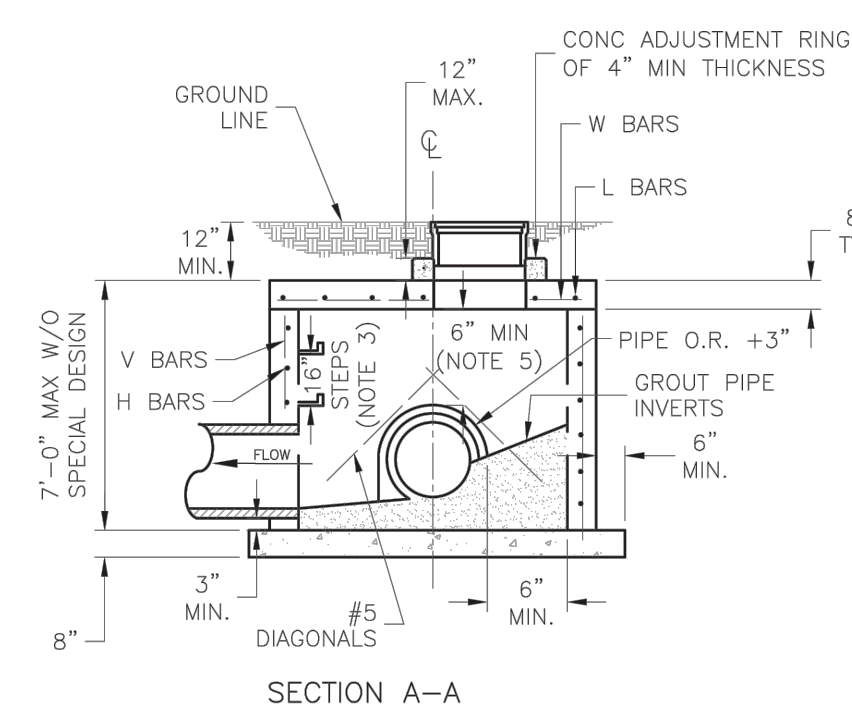


AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METROPOLITAN CHAPTER
GRATE INLET DETAILS	STANDARD DRAWING NUMBER GI-1 ADOPTED: APRIL 17, 1996



** INCREASE IN MULTIPLES OF 6"
(7'-0") MAX WITHOUT SPECIAL DESIGN
(SEE PROJECT PLANS FOR DETAILS)

REINFORCING		
BARS	BAR SIZE	SPACING (IN.)
H	4	12
V	4	12
L	5	6
W	5	6



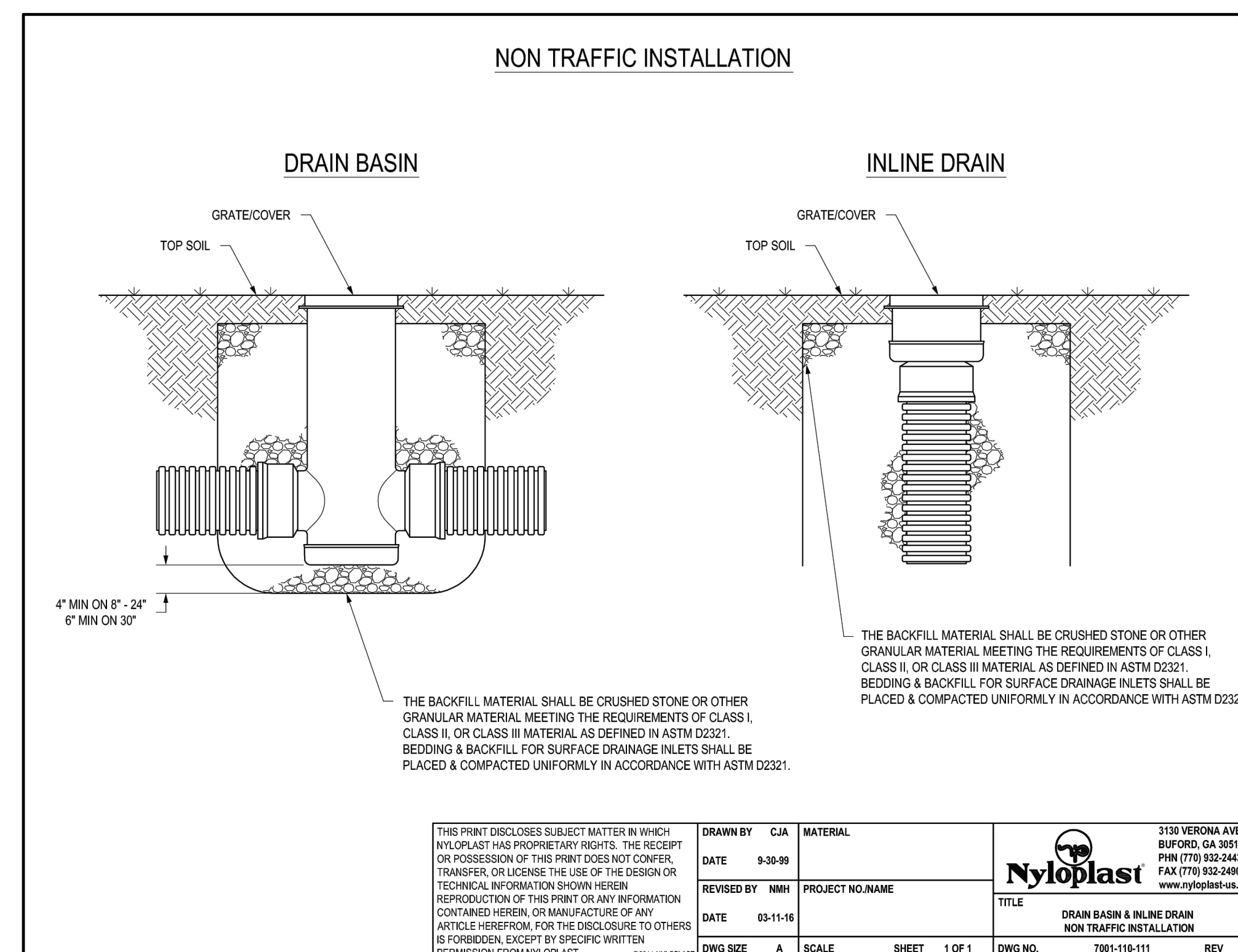
- GENERAL NOTE:
1. LOCATE RING AND COVER ON BLANK WALL.
2. USE $\frac{3}{4}$ " CHAMFER STRIP OR $\frac{1}{2}$ " R EDGER TOOL ON ALL EXPOSED CONCRETE CORNERS.
3. STEPS REQUIRED AT 16" O.C. WHEN DEPTH FROM TOP OF CASTING TO INVERT EXCEEDS 4' ON BLANK WALL IF POSSIBLE.
4. BOXOUTS WILL NOT BE ALLOWED TO PROJECT THROUGH THE JOINTS OF THE STRUCTURE AND THE MINIMUM DISTANCE BETWEEN BOXOUTS IS 6".
5. THE MINIMUM REINFORCING SHALL BE 1 H-BAR OVER A CAST-IN PLACE PIPE AND 2 H-BARS OVER A PRECAST BOXOUT.
6. PRECAST LIDS SHALL BE PINNED, SEALED WITH NON-SHRINKABLE GROUT AND REMOVABLE FOR FUTURE MAINTENANCE.
7. REINFORCING OF COVERS IN STREETS REQUIRE SPECIAL DESIGN.
10. FOR RING AND COVER SEE THE STORMWATER APPROVED PRODUCT LIST.

LS LEE'S SUMMIT
MISSOURI
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

<p>object:</p> <p>STANDARD DETAILS</p> <p>CITY OF LEE'S SUMMIT, MO</p> <p>LEE'S SUMMIT, JACKSON COUNTY</p> <p>sheet Name:</p>	<p>JUNCTION BOX DETAIL</p>
---	----------------------------

Drawn By: MUF
Checked By: DL
Date: 05/2021
Proj. #:

STM-3

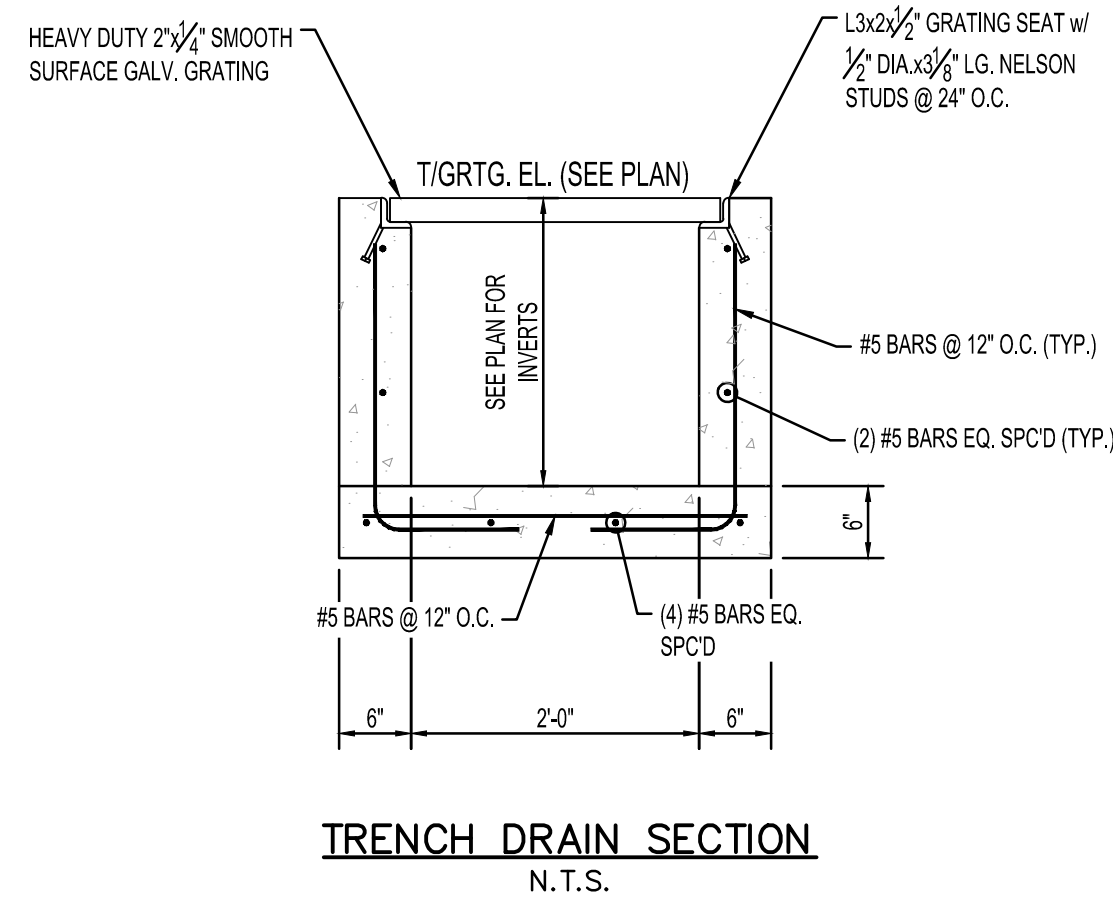


YARD INLET
N.T.S.

DRAINAGE DETAILS

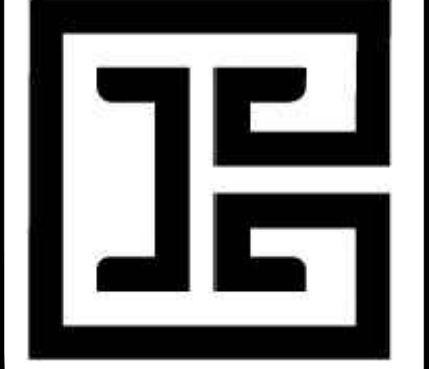
SHEET

C-4.4




REVISION		BY
1	10/24/2024 REVISED PER CITY	KRG
2	12/17/2024 REVISED PER CITY	KRG

**HIGHTIDE
CONSULTANTS LLC**
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com




STAMP

The seal is circular with a double-lined border. Inside the border, the words "STATE OF MISSOURI" are written along the top arc and "PROFESSIONAL ENGINEER" along the bottom arc, separated by two stars. In the center of the seal, the text "B. SHANE" is at the top, "G.U.N" is in the middle, and "No. - 202100076" is at the bottom.

DATE: _____

JANUARY 17, 2025

SIGNATURE _____

A stylized handwritten signature in black ink, appearing to read "B. SHANE G. N.", written over a horizontal line.

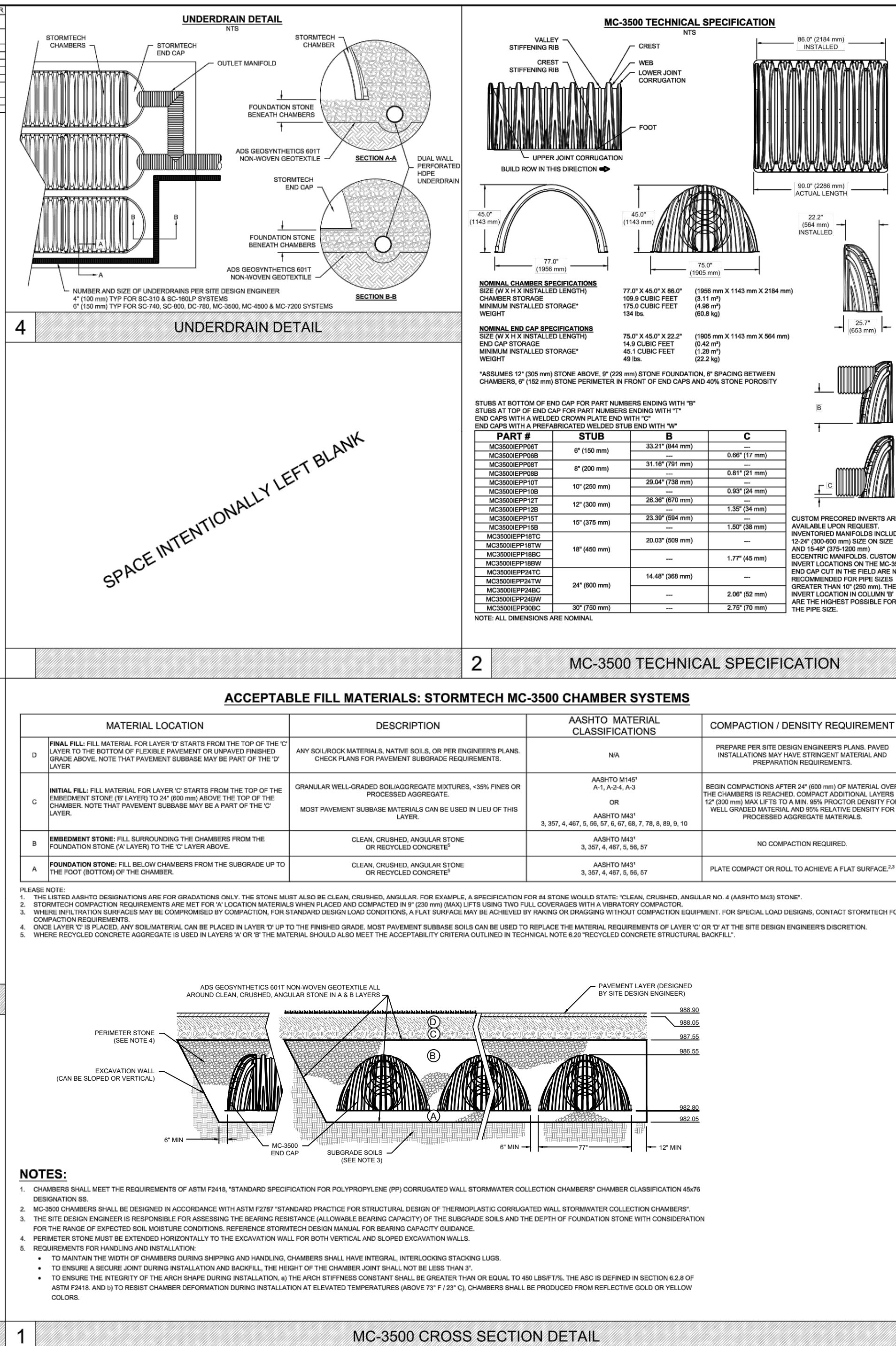
PROPOSED TAKE 5
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FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN KRG
CHECKED RCG
ISSUED DATE 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO. 22-218
FILE 22-218 C-4 Details

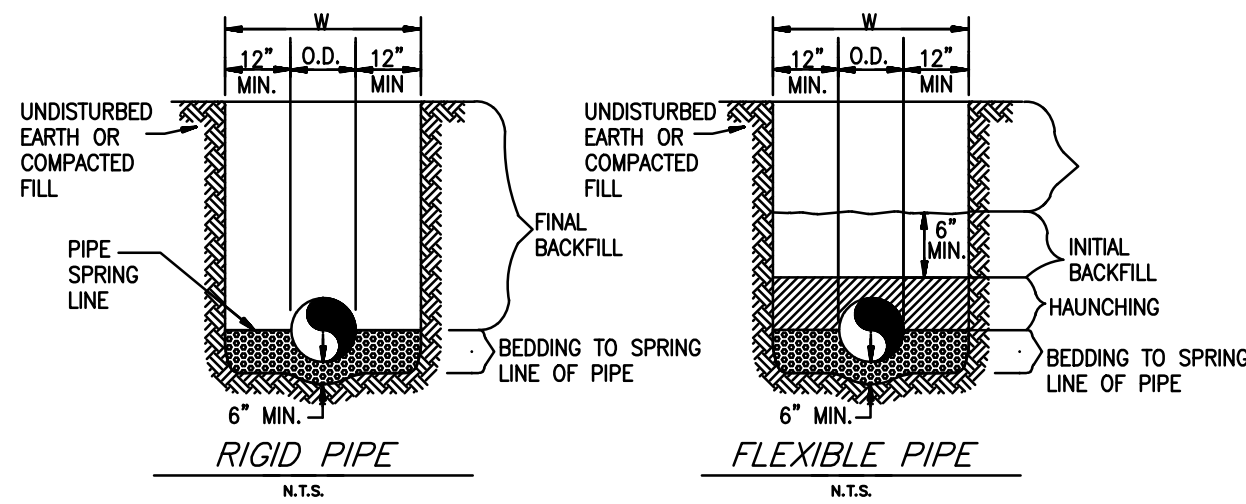
SHEET

C-4.5

[illegible]

 4640 TRUEMAN BLVD HILLIARD, OH 43026 1-800-735-4773	StormTech® Storm System 1-800-821-4710 WWW.STORMTECH.COM	TAKE 5 - LEE'S SUMMIT, MO		DRAWN: PMI	DATE: 10/23/2024
		LEE'S SUMMIT, MO, USA		CHECKED: N/A	PROJECT #:
				REV:	NOT TO SCALE
		THIS DRAWING HAS BEEN DRAWING BASED ON INFORMATION PROVIDED TO ADS/STORMTECH UNDER THE DIRECTION OF THE PROJECT'S ENGINEER OF RECORD (TEP) OR OTHER PROJECT REPRESENTATIVE. THIS DRAWING IS NOT INTENDED FOR USE IN BIDDING OR CONSTRUCTION WITHOUT THE ENGINEER'S PRIOR APPROVAL. FOR SUCH, THE USER HAS ASSUMED ALL LIABILITY. THE USER'S RESPONSIBILITY IS TO ENSURE THAT THE PROJECTS DESIGNED AND ALL ASSOCIATED LAWS, REGULATIONS AND ORDINANCES ARE COMPLIED WITH.			

DRAINAGE DETAILS



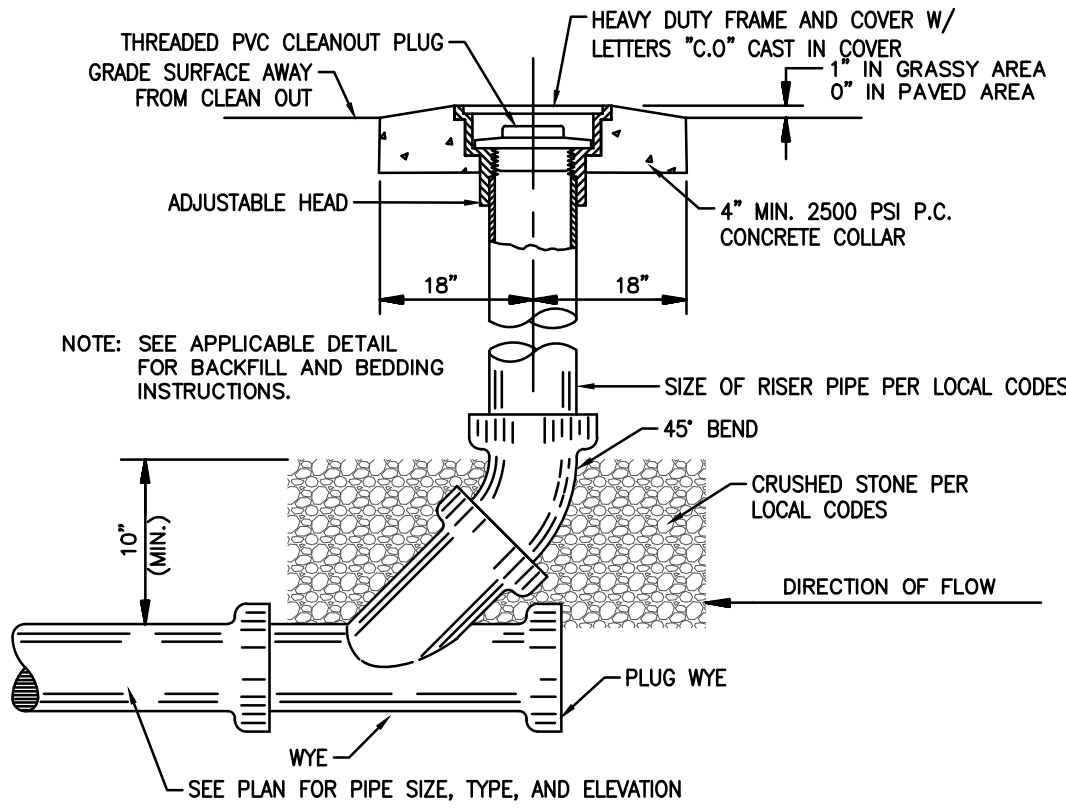
- GENERAL NOTES**
- BEDDING SHALL BE CLASS I-A WORKED BY HAND. IF GROUNDWATER IS ANTICIPATED, THEN BEDDING SHALL BE CLASS I-B COMPACTED TO 95% STANDARD PROCTOR.
 - HAUNCHING SHALL BE WORKED AROUND THE PIPE BY HAND TO ELIMINATE VOIDS AND SHALL BE CLASS I-A OR CLASS I-B OR CLASS II COMPACTED TO 95% PROCTOR.
 - INITIAL BACKFILL UNDER PAVED AREAS SHALL BE CLASS I-A WORKED BY HAND, OR CLASS I-B OR CLASS II COMPACTED TO 95% STANDARD PROCTOR.
 - INITIAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS II COMPACTED TO 90% STANDARD PROCTOR.
 - FINAL BACKFILL UNDER PAVED AREAS SHALL BE CLASS I, II, OR III COMPACTED AS NOTED IN NOTES 3. AND 4.
 - FINAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS IV-A COMPACTED TO 95% STANDARD PROCTOR.
 - ALL MATERIALS ARE CLASSIFIED IN ACCORDANCE WITH ASTM D 2321-LATEST EDITION.
 - ALL MATERIALS SHALL BE INSTALLED IN MAXIMUM 8" LOOSE LIFTS IN ACCORDANCE WITH ASTM D 698. CLASS III AND IV-A MATERIALS SHALL BE COMPACTED NEAR OPTIMUM MOISTURE CONTENT.
 - FILL SALVAGED FROM EXCAVATION SHALL BE FREE OF DEBRIS, ORGANICS AND ROCKS LARGER THAN 3".
 - ALL TRENCH EXCAVATIONS SHALL BE SLOPED, SHORED, SHEETED, BRACED, OR OTHERWISE SUPPORTED IN COMPLIANCE WITH OSHA REGULATIONS AND LOCAL ORDINANCES.
 - GEOTECH FABRIC TO BE USED UNDER BEDDING MATERIAL WHEN UNSUITABLE SOILS ARE ENCOUNTERED OR A STABLE NON-YIELDING FOUNDATION CANNOT BE OBTAINED.
 - HAUNCHING FOR FLEXIBLE PIPE SHALL BE WORKED A MINIMUM OF 6" ABOVE THE TOP OF PIPE OR PER THE MANUFACTURER'S RECOMMENDATION, WHICHEVER IS MORE STRINGENT.

*SEE CHART ON D-3 FILL TYPES

CLASS	TYPE	SOIL GROUP SYMBOL	DESCRIPTION
1A	MANUFACTURED AGGREGATES; OPEN-GRADED, CLEAN	NONE	ANGULAR, CRUSHED STONE OR ROCK, CRUSHED GRAVEL, BROKEN CORAL, CRUSHED SLAG, CINDERS OR SHELLS, LARGE VOID CONTENT, CONTAIN LITTLE OR NO FINES
1B	MANUFACTURED, PROCESSED AGGREGATES; DENSE-GRADED, CLEAN.	NONE	ANGULAR, CRUSHED STONE (OR OTHER CLASS 1A MATERIALS) AND STONE/SAND MIXTURES WITH GRADATIONS SELECTED TO MINIMIZE MIGRATION OF ADJACENT SOILS, CONTAIN LITTLE OR NO FINES (SEE X1.8)
II	COARSE-GRAINED SOILS CLEAN	GW	WELL-GRADED GRAVELS AND GRAVEL-SAND MIXTURES; LITTLE OR NO FINES
		GP	POORLY-GRADED GRAVELS AND GRAVEL-SAND MIXTURES; LITTLE OR NO FINES
		SW	WELL-GRADED SANDS AND GRAVELY SANDS; LITTLE OR NO FINES (NO. 200 SIEVE)
		SP	POORLY-GRADED SANDS AND GRAVELY SANDS; LITTLE OR NO FINES (NO. 200 SIEVE)
III	COARSE-GRAINED SOILS BORDERLINE CLEAN TO W/ FINES	e.g. GW-GC, SP-SM	SANDS AND GRAVELS WHICH ARE BORDERLINE BETWEEN CLEAN AND WITH FINES
		GM	SILTY GRAVELS, GRAVEL-SAND-SILT MIXTURES
		GC	CLAYEY GRAVELS, GRAVEL-SAND-CLAY MIXTURES
		SM	SILTY SANDS, SAND-SILT MIXTURES
IV-A	FINE-GRAINED SOILS (INORGANIC)	SC	CLAYEY SANDS, SAND-CLAY MIXTURES
		ML	INORGANIC SILTS AND VERY FINE SANDS, ROCK FLOUR, SILTY OR CLAYEY FINE SANDS, SILTS WITH SLIGHT PLASTICITY
		CL	INORGANIC CLAYS OF LOW TO MEDIUM PLASTICITY, GRAVELY CLAYS; SANDY CLAYS; SILTY CLAYS; LEAN CLAYS
		MH	INORGANIC SILTS, MICACEOUS OR DIATOMACEOUS FINE SANDY OR SILTY SOILS, ELASTIC SILTS
IV-B	FINE-GRAINED SOILS (INORGANIC)	CH	INORGANIC CLAYS OF HIGH PLASTICITY, FAT CLAYS
		OL	ORGANIC SILTS AND ORGANIC SILTY CLAYS OF LOW PLASTICITY
		OH	ORGANIC CLAYS OF MEDIUM TO HIGH PLASTICITY, ORGANIC SILTS
		PT	PEAT AND OTHER HIGH ORGANIC SOILS.
V	ORGANIC SOILS HIGHLY ORGANIC		

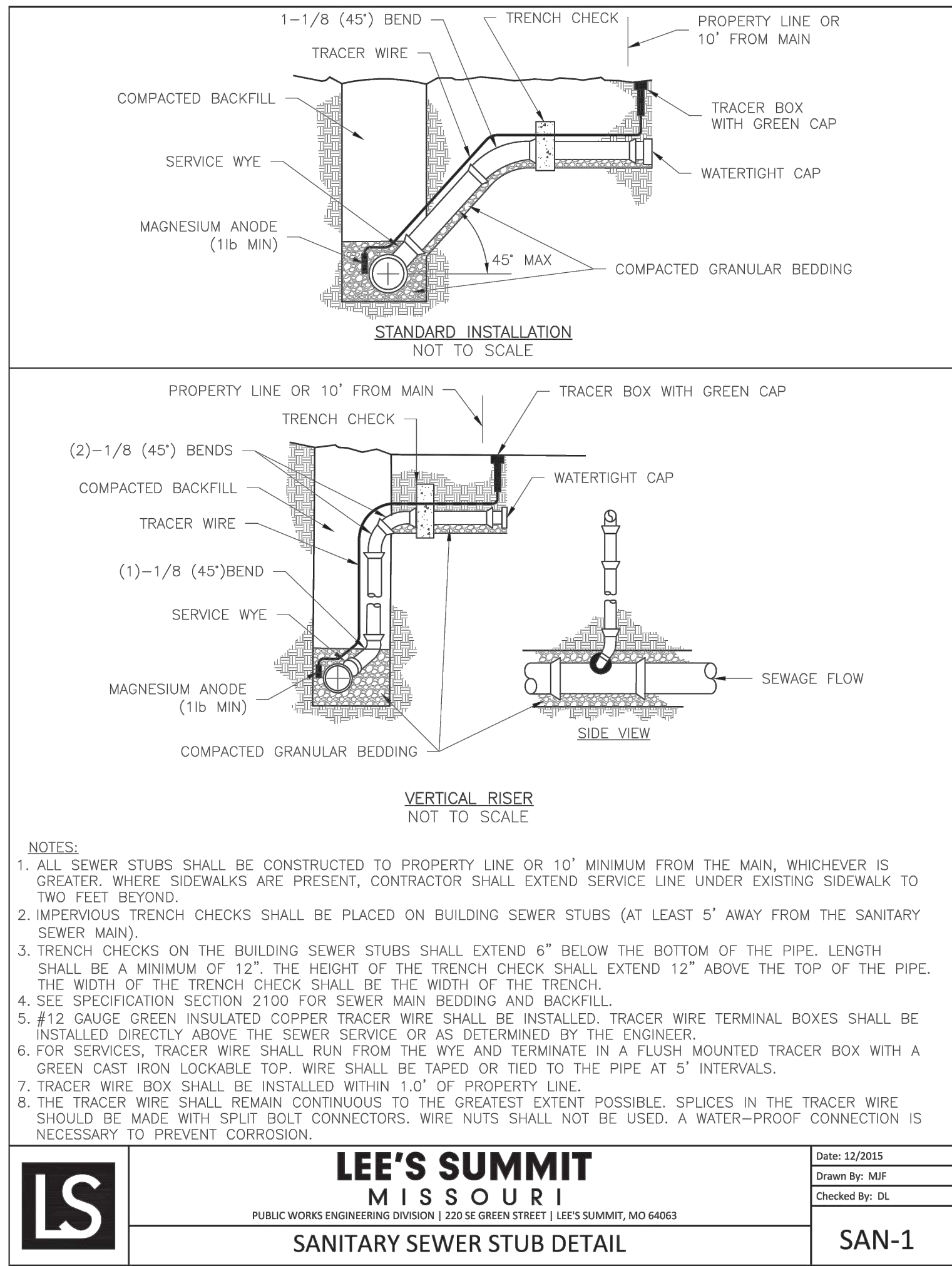
STORM SEWER TRENCH AND BEDDING

N.T.S.



SANITARY SEWER CLEAN-OUT

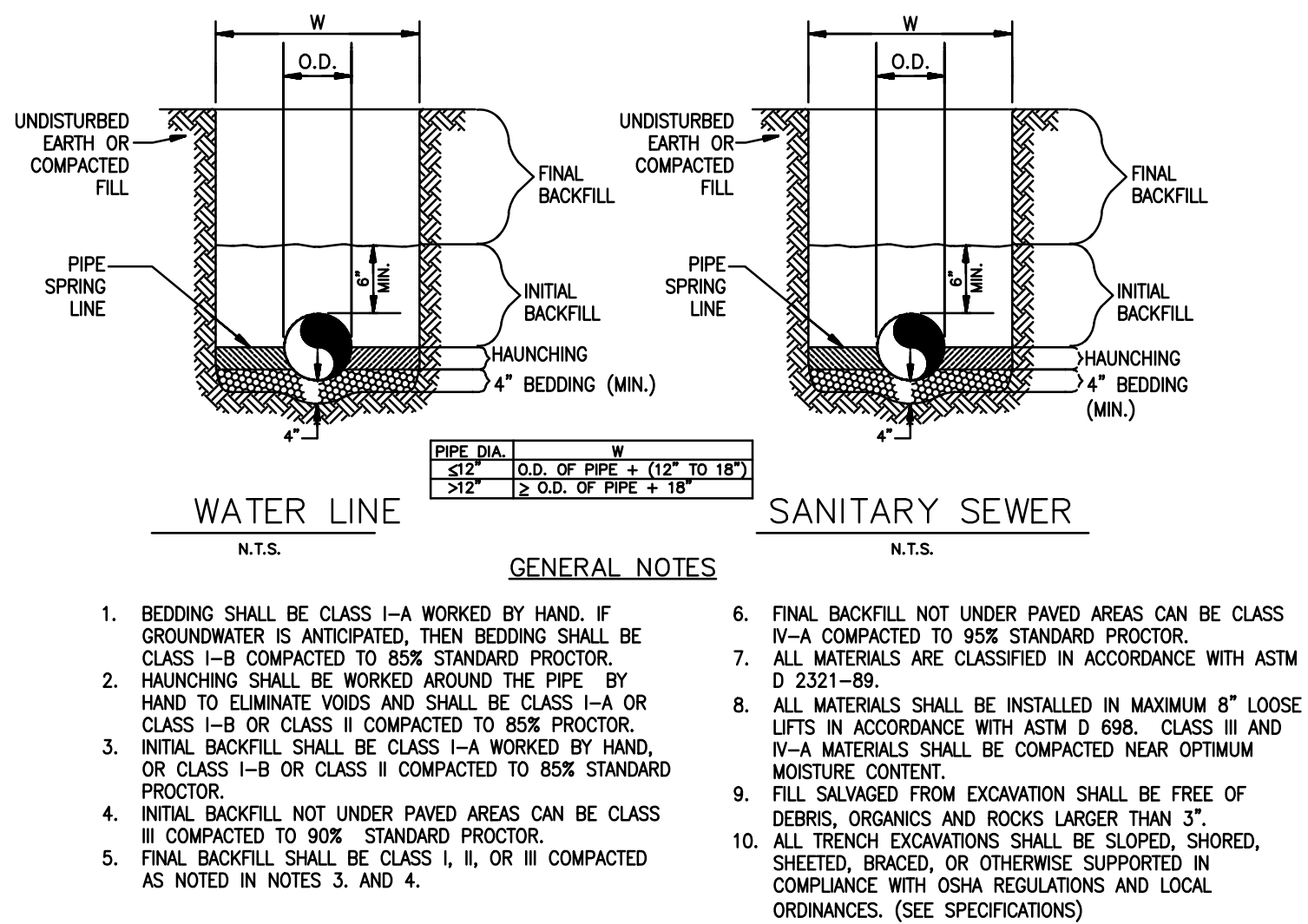
N.T.S.



LEE'S SUMMIT
MISSOURI
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

Date: 12/20/15
Drawn By: MEF
Checked By: DL

SAN-1



UTILITY TRENCH AND BEDDING

N.T.S.

SEWER DETAILS

REVISION	BY

HIGH TIDE
CONSULTANTS LLC
 434 N. COLUMBIA ST, SUITE 200A
 COVINGTON, LA 70433
www.hightidela.com

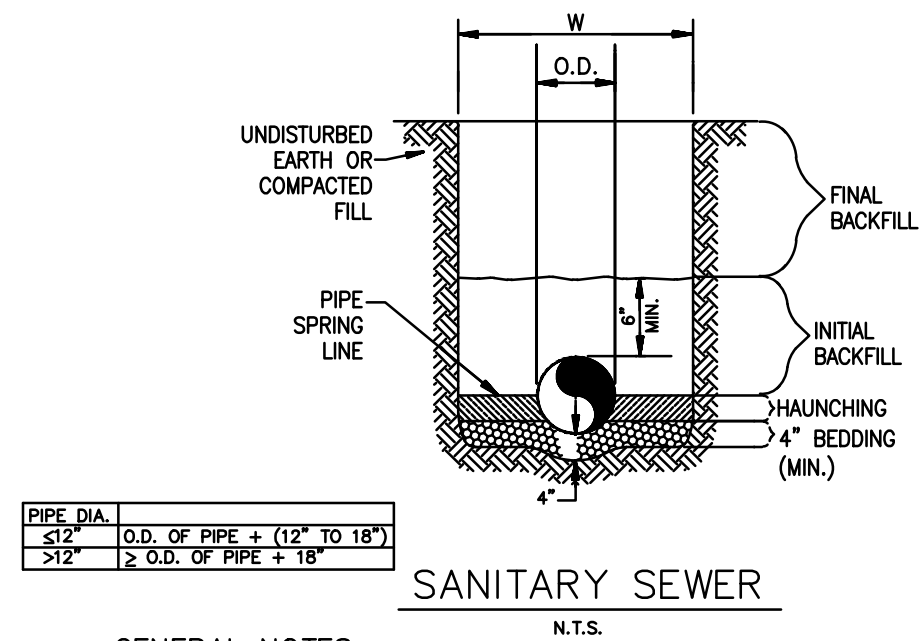
SIGNATURE
B. SHANE
ENGINEER

JULY 30, 2024
DATE

PROPOSED TAKE 5
 LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
 2101 PEARL STREET
 BOULDER, CO 80302

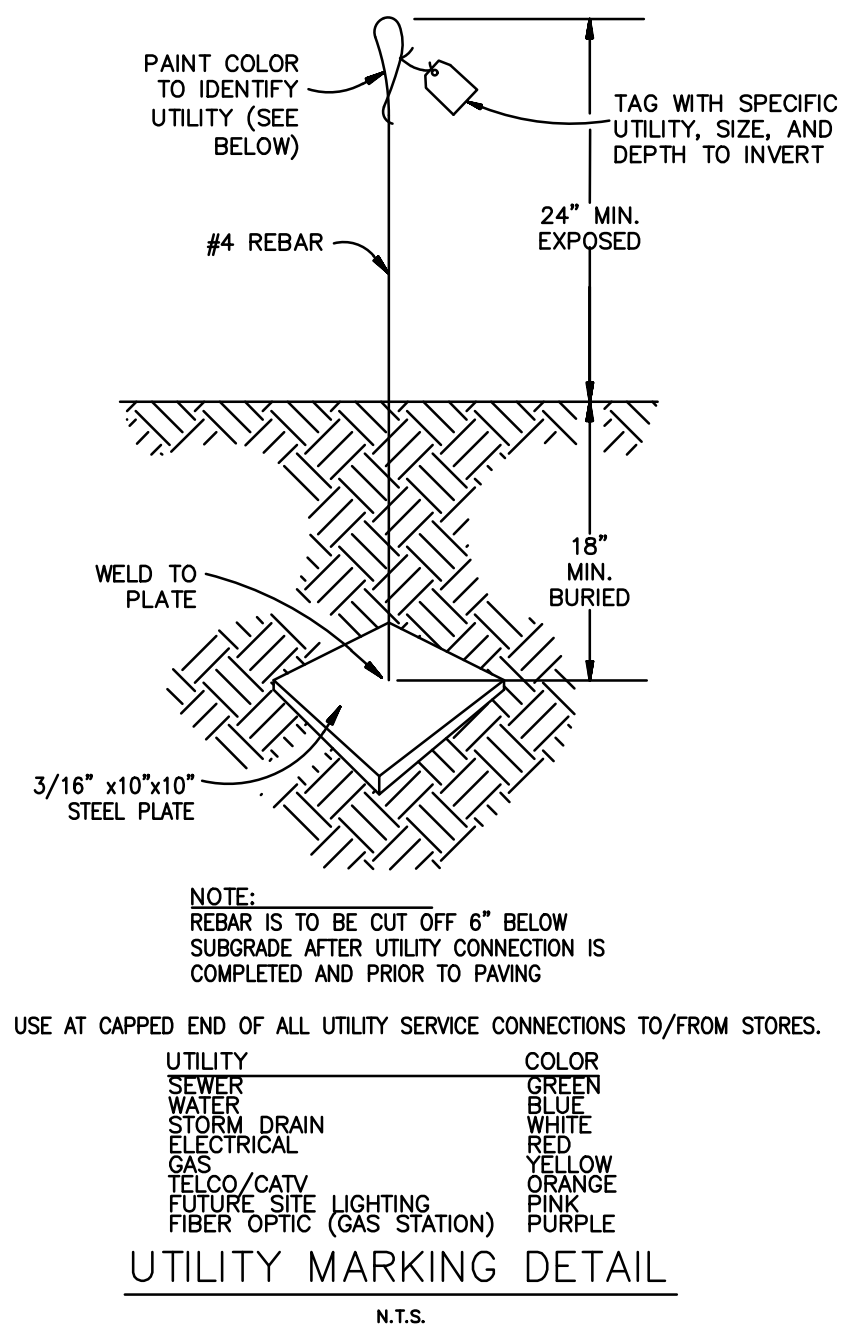
DRAWN	KRG
CHECKED	RCG
ISSUED DATE	07/30/2024
ISSUED FOR	PERMITTING
PROJECT NO.	22-218
FILE	22-218 C-4 Details
SHEET	C-4.6



- GENERAL NOTES**
- BEDDING SHALL BE CLASS I-A WORKED BY HAND, IF GROUNDWATER IS ANTICIPATED, THEN BEDDING SHALL BE CLASS I-B COMPACTED TO 85% STANDARD PROCTOR.
 - HAUNCHING SHALL BE WORKED AROUND THE PIPE BY HAND TO ELIMINATE VOIDS AND SHALL BE CLASS I-A OR CLASS I-B OR CLASS II COMPACTED TO 85% PROCTOR.
 - INITIAL BACKFILL SHALL BE CLASS I-A WORKED BY HAND, OR CLASS I-B OR CLASS II COMPACTED TO 85% STANDARD PROCTOR.
 - INITIAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS III COMPACTED TO 90% STANDARD PROCTOR.
 - FINAL BACKFILL SHALL BE CLASS I, II, OR III COMPACTED AS NOTED IN NOTES 3, AND 4.
 - FINAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS IV-A COMPACTED TO 95% STANDARD PROCTOR.
 - ALL MATERIALS ARE CLASSIFIED IN ACCORDANCE WITH ASTM D 2321-89.
 - ALL MATERIALS SHALL BE INSTALLED IN MAXIMUM 8" LOOSE LIFTS IN ACCORDANCE WITH ASTM D 656. CLASS III AND IV-A MATERIALS SHALL BE COMPACTED NEAR OPTIMUM MOISTURE CONTENT.
 - FILL SALVAGED FROM EXCAVATION SHALL BE FREE OF DEBRIS, ORGANICS AND ROCKS LARGER THAN 3".
 - ALL TRENCH EXCAVATIONS SHALL BE SLOPED, SHORED, SHEETED, BRACED, OR OTHERWISE SUPPORTED IN COMPLIANCE WITH OSHA REGULATIONS AND LOCAL ORDINANCES. (SEE SPECIFICATIONS)

UTILITY TRENCH AND BEDDING

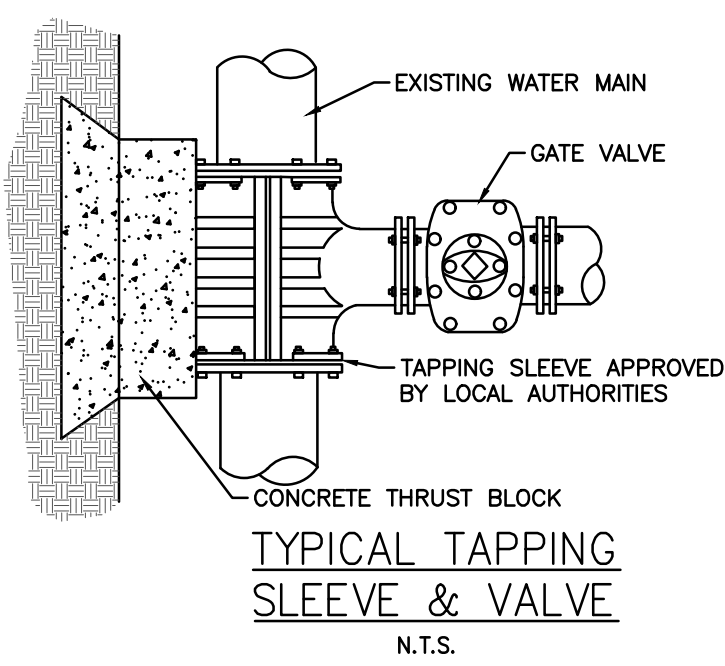
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UTILITY MARKING DETAIL

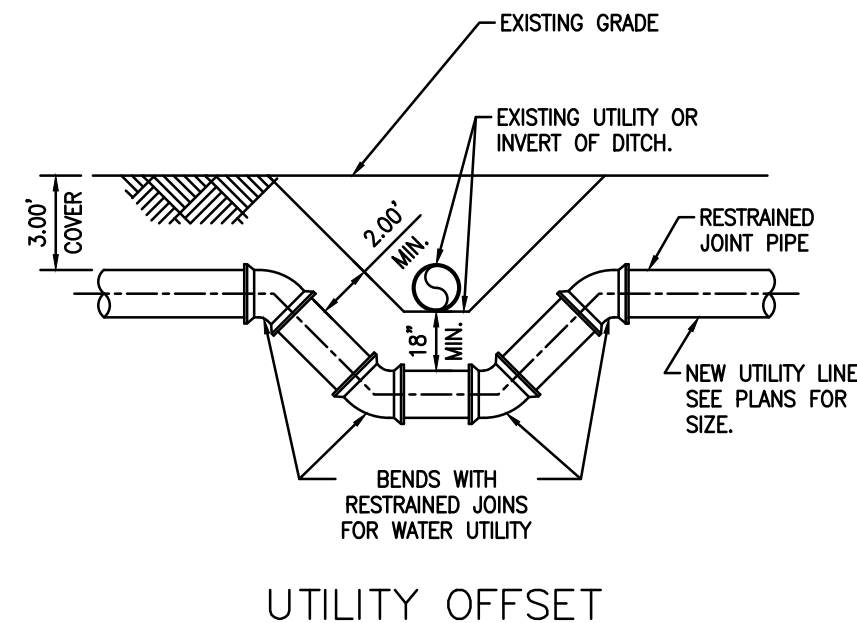
N.T.S.

CLASS	TYPE	SOIL GROUP SYMBOL D2487	DESCRIPTION
1A	MANUFACTURED AGGREGATES, OPEN-GRADED, CLEAN	NONE	ANGULAR, CRUSHED STONE OR ROCK, CRUSHED GRAVEL, BROKEN CORAL, CRUSHED SLAG, CINDERS OR SHELLS; LARGE VOID CONTENT, CONTAIN LITTLE OR NO FINES
1B	MANUFACTURED, PROCESSED AGGREGATES, DENSE-GRADED, CLEAN.	NONE	ANGULAR, CRUSHED STONE (OR OTHER CLASS 1A MATERIALS) AND STONE/SAND MIXTURES WITH GRADATIONS SELECTED TO MINIMIZE MIGRATION OF ADJACENT SOILS; CONTAIN LITTLE OR NO FINES (SEE 1.8)
II	COARSE-GRAINED SOILS CLEAN	GW	WELL-GRADED GRAVELS AND GRAVEL-SAND MIXTURES; LITTLE OR NO FINES
		GP	POORLY-GRADED GRAVELS AND GRAVEL-SAND MIXTURES; LITTLE OR NO FINES
		SW	WELL-GRADED SANDS AND GRAVELY SANDS; LITTLE OR NO FINES (NO. 200 SIEVE)
		SP	POORLY-GRADED SANDS AND GRAVELY SANDS; LITTLE OR NO FINES (NO. 200 SIEVE)
	COARSE-GRAINED SOILS BORDERLINE CLEAN TO W/ FINES	e.g. GW-GC, SP-SM	SANDS AND GRAVELS WHICH ARE BORDERLINE BETWEEN CLEAN AND WITH FINES
		GM	SILTY GRAVELS, GRAVEL-SAND-SILT MIXTURES
		GC	CLAYEY GRAVELS, GRAVEL-SAND-CLAY MIXTURES
		SM	SILTY SANDS, SAND-SILT MIXTURES
III	COARSE-GRAINED SOILS WITH FINES	SC	CLAYEY SANDS, SAND-CLAY MIXTURES
		ML	INORGANIC SILTS AND VERY FINE SANDS, ROCK FLOUR, SILTY OR CLAYEY FINE SANDS, SILTS WITH SLIGHT PLASTICITY
		CL	INORGANIC CLAYS OF LOW TO MEDIUM PLASTICITY, GRAVELY CLAYS, SANDY CLAYS, SILTY CLAYS, LEAN CLAYS
		MH	INORGANIC SILTS, MICACEOUS OR DIATOMACEOUS FINE SANDY OR SILTY SOILS, ELASTIC SILTS
IV-A	FINE-GRAINED SOILS (INORGANIC)	CH	INORGANIC CLAYS OF HIGH PLASTICITY, FAT CLAYS.
		OL	ORGANIC SILTS AND ORGANIC SILTY CLAYS OF LOW PLASTICITY
IV-B	FINE-GRAINED SOILS (INORGANIC)	OH	ORGANIC CLAYS OF MEDIUM TO HIGH PLASTICITY, ORGANIC SILTS
		PT	PEAT AND OTHER HIGH ORGANIC SOILS.
V	ORGANIC SOILS		
	HIGHLY ORGANIC		

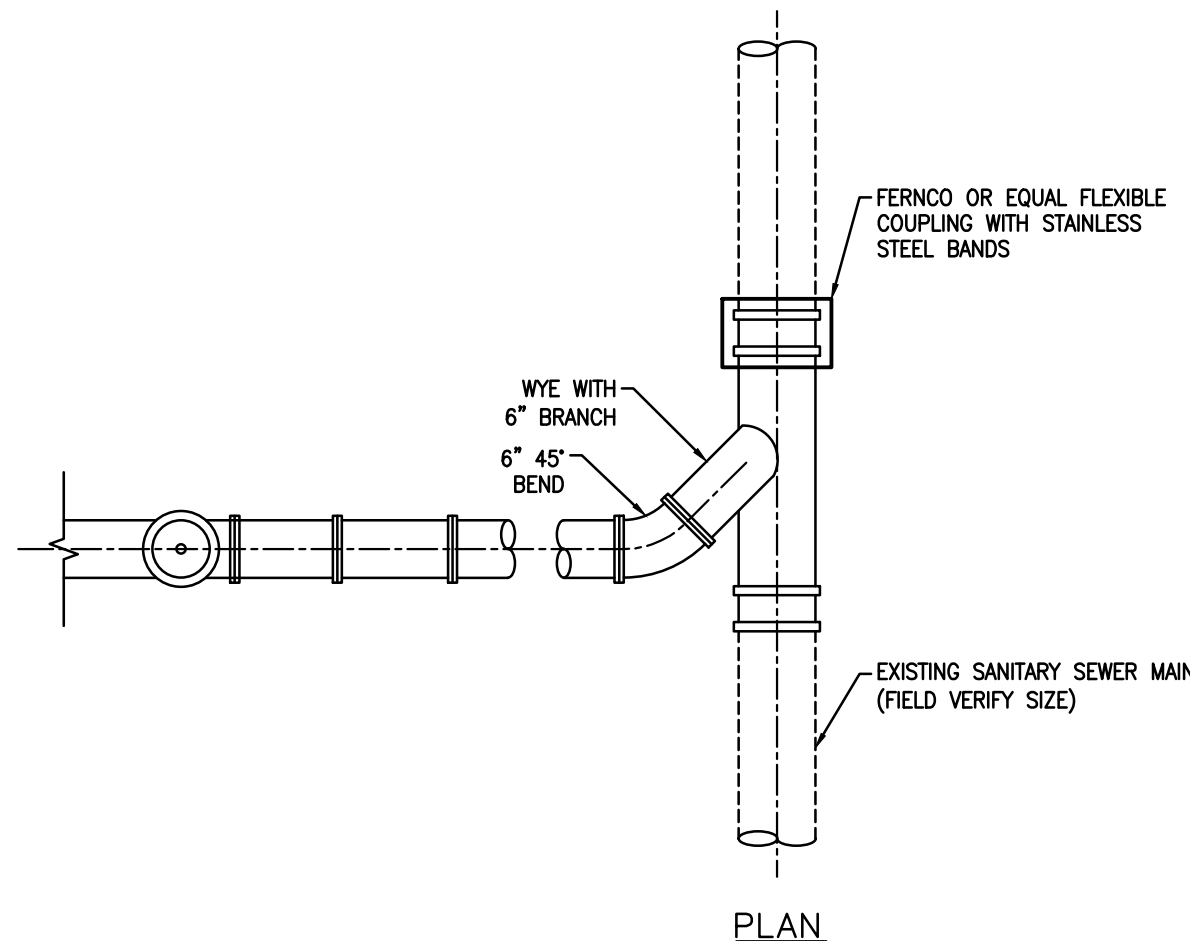


TYPICAL TAPPING SLEEVE & VALVE

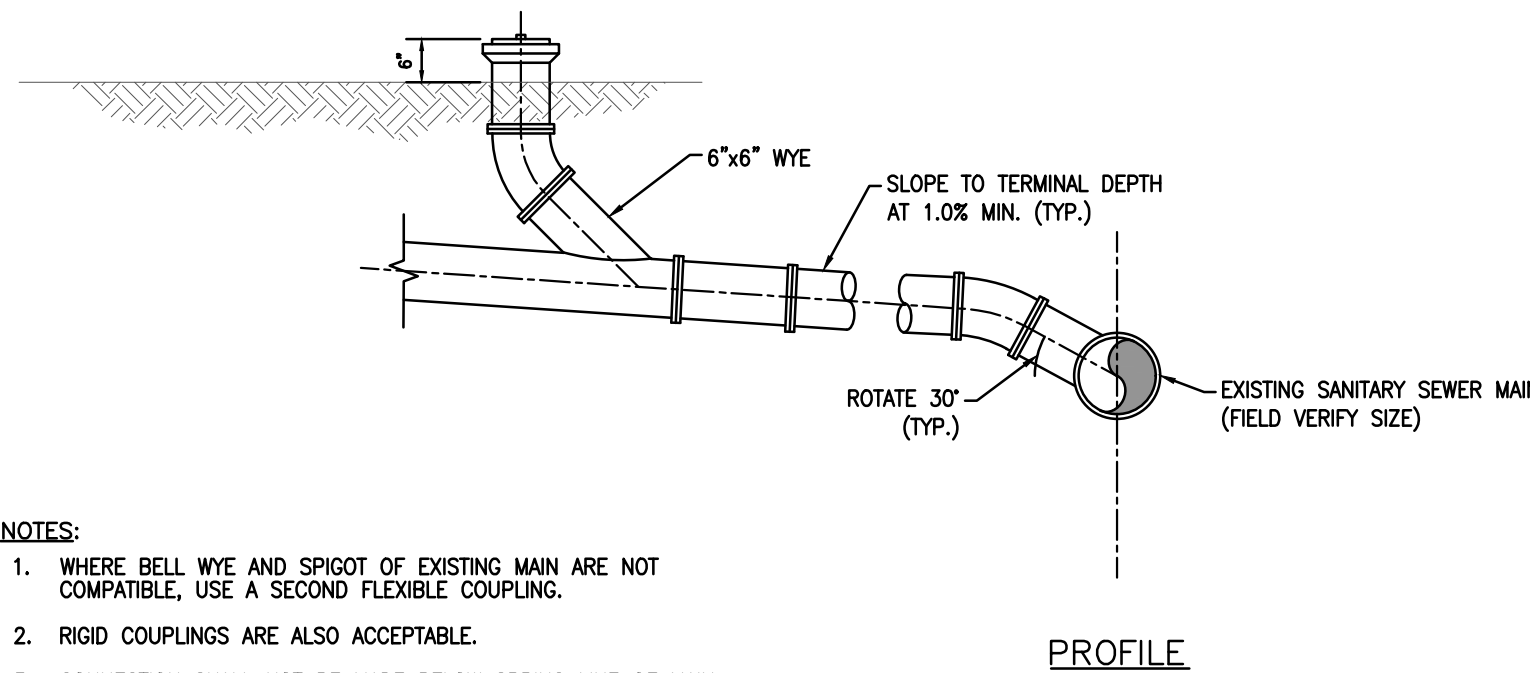
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UTILITY OFFSET



PLAN



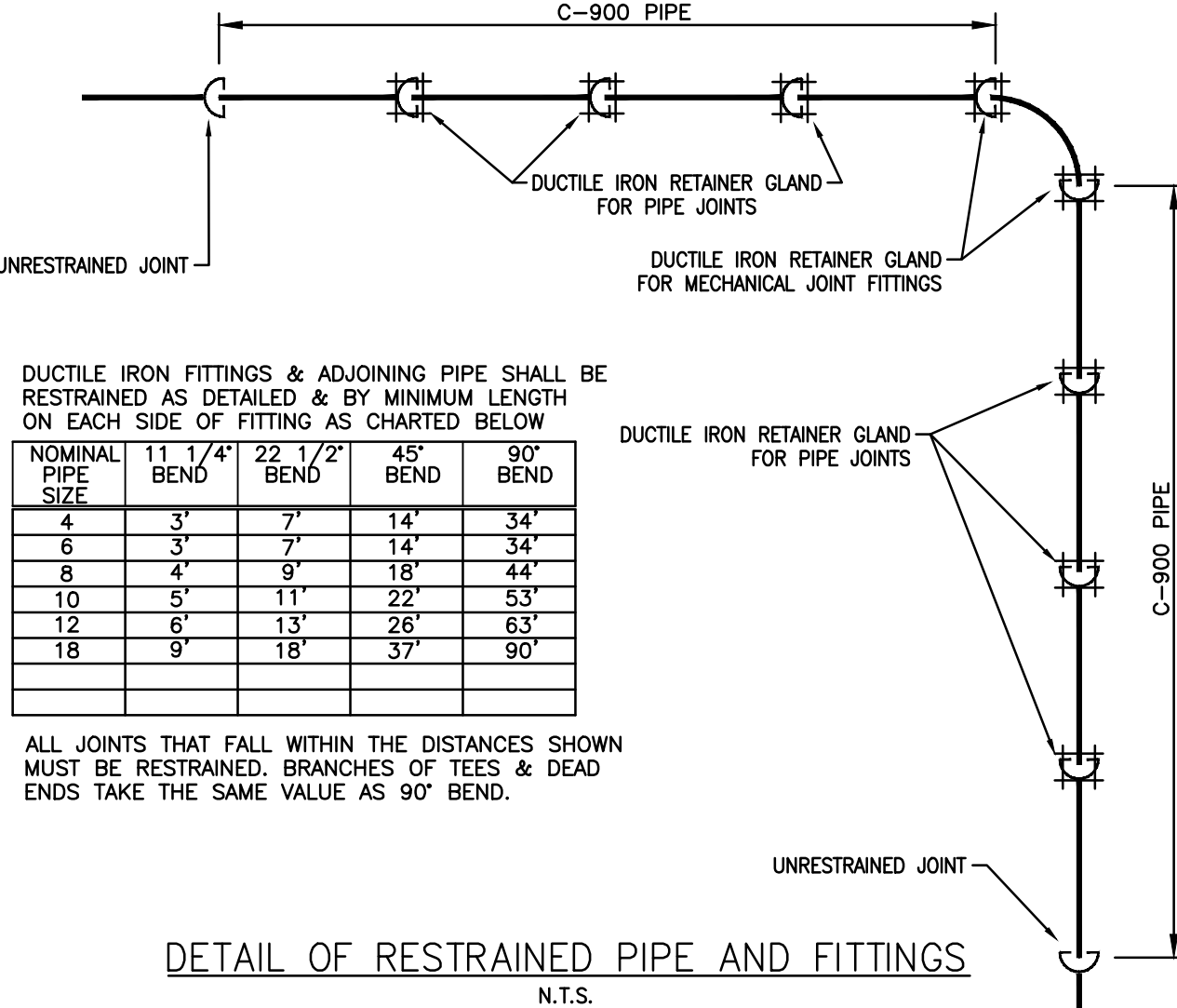
PROFILE

- NOTES:**
- WHERE BELL WYE AND SPIGOT OF EXISTING MAIN ARE NOT COMPATIBLE, USE A SECOND FLEXIBLE COUPLING.
 - RIGID COUPLINGS ARE ALSO ACCEPTABLE.
 - CONNECTION SHALL NOT BE MADE BELOW SPRING LINE OF MAIN.

SANITARY SEWER WYE CONNECTION DETAIL

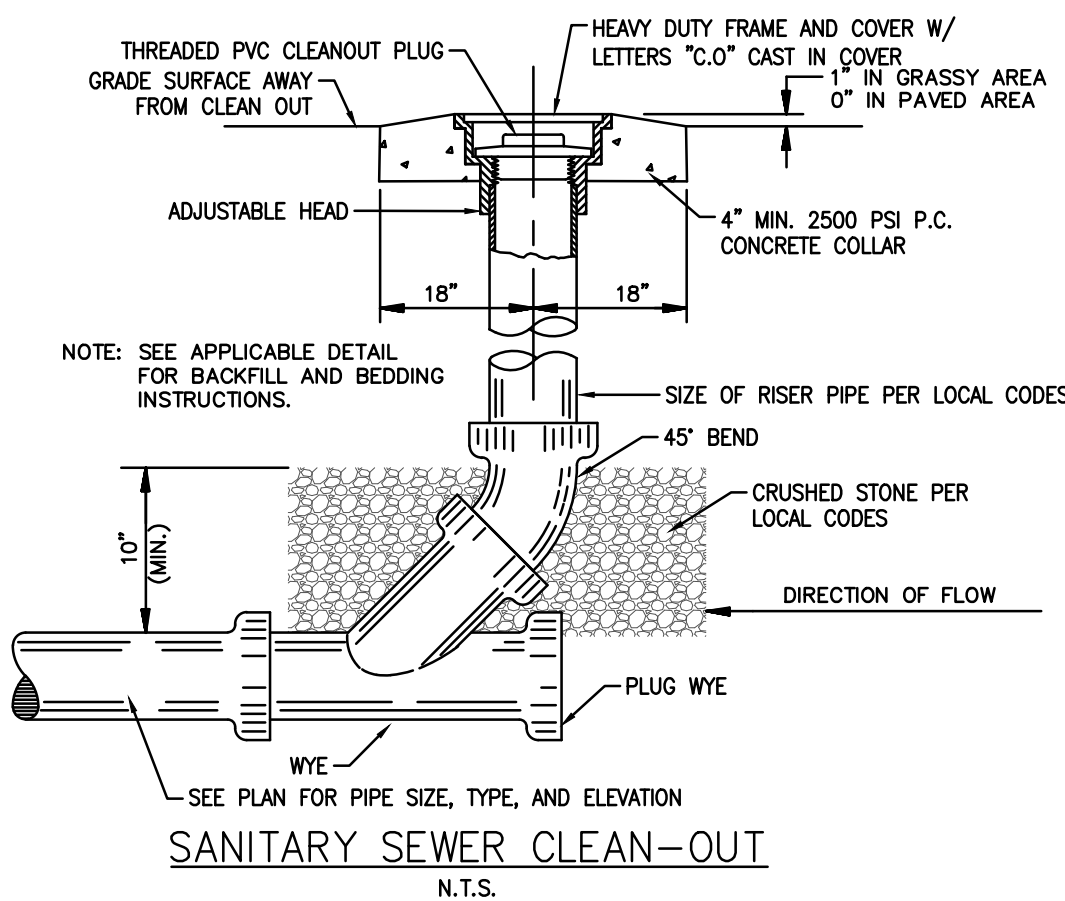
N.T.S.

- NOTES:**
- DISTANCES OF RESTRAINTS SHALL BE REQUIRED ON EVERY JOINT(S) ON EACH SIDE OF FITTING, IN ADDITION TO CONCRETE OR TIMBER BLOCKING.
 - RESTRAINTS NOT REQUIRED INSIDE OF CASING WHEN FITTING OR PIPE IS RODDED TO CASING ENDS.
 - RESTRAIN TEE WHEN USED AS A 90° BEND, DEAD END, OR AS NOTED ON PLANS.
 - AT ALL ROAD CROSSINGS AND WHERE NOTED ON PLANS, FITTINGS NOTED ON PLANS.
 - TIE-INS SHALL BE RESTRAINED AS IF A 90° BEND
 - RESTRAINT SYSTEM:
EITHER: (A) RODS FOR USE WITH MECHANICAL JOINT FITTINGS - 2 REQUIRED - 5/8" DIA STAINLESS STEEL FOR 8" PIPE & BELOW
(B) RETAINER GLAND: USE EBBA IRON, UNI-FLANGE OR EQUAL IN COMPLIANCE WITH PIPE MATERIAL USED.



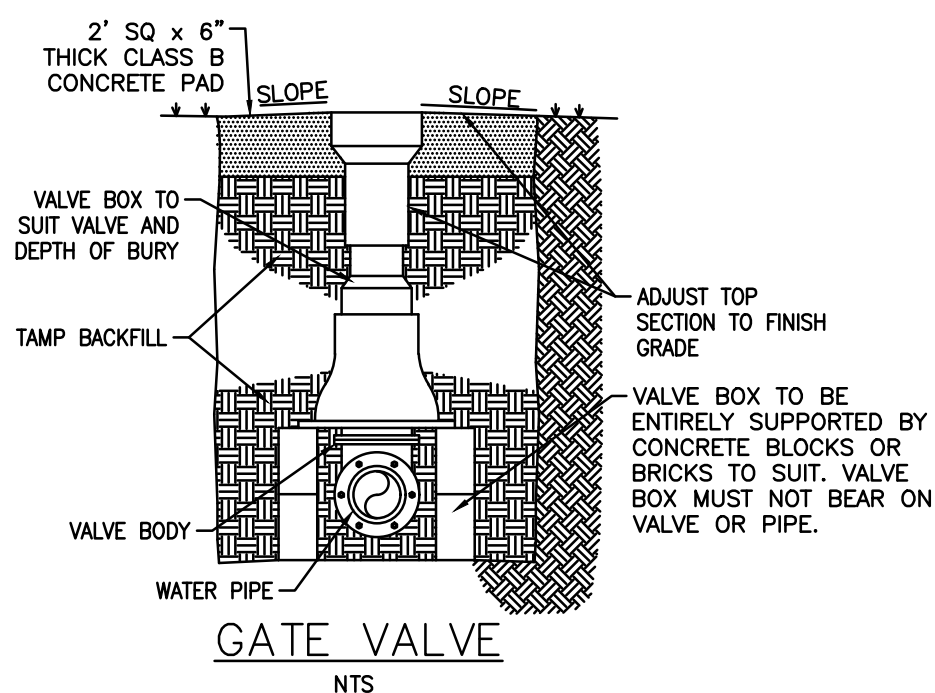
DETAIL OF RESTRAINED PIPE AND FITTINGS

N.T.S.



SANITARY SEWER CLEAN-OUT

N.T.S.



GATE VALVE

N.T.S.

UTILITY DETAILS

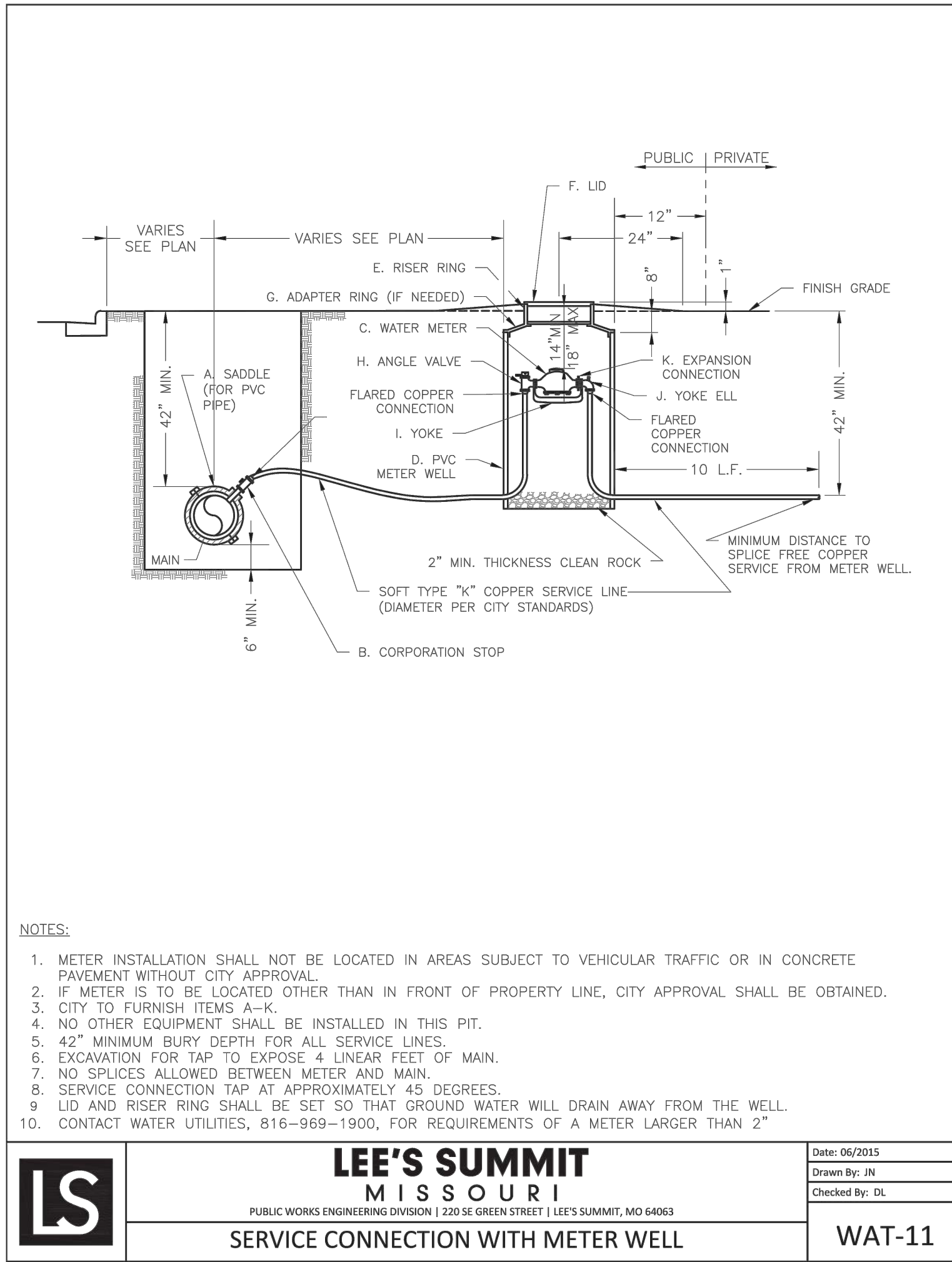
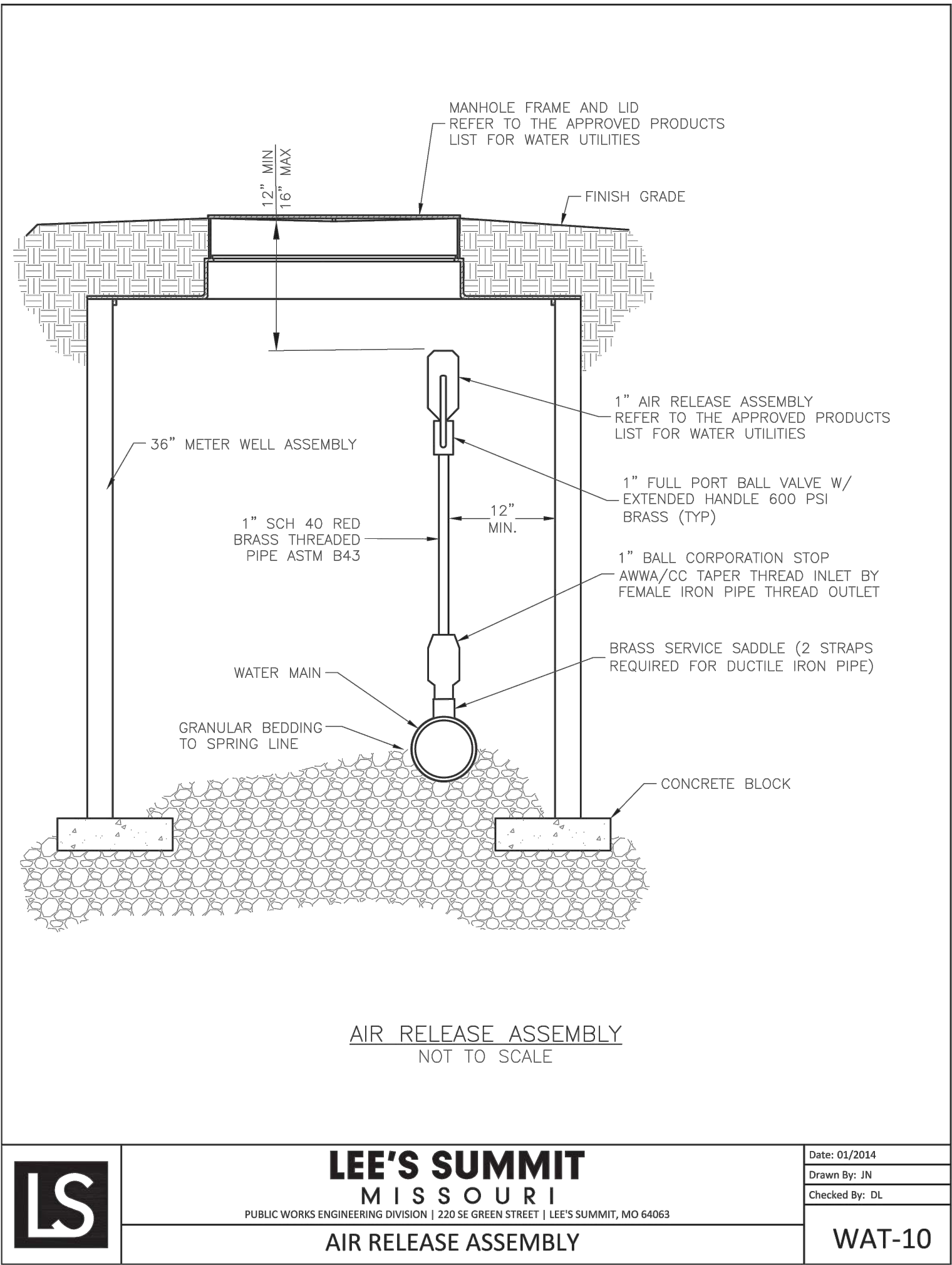
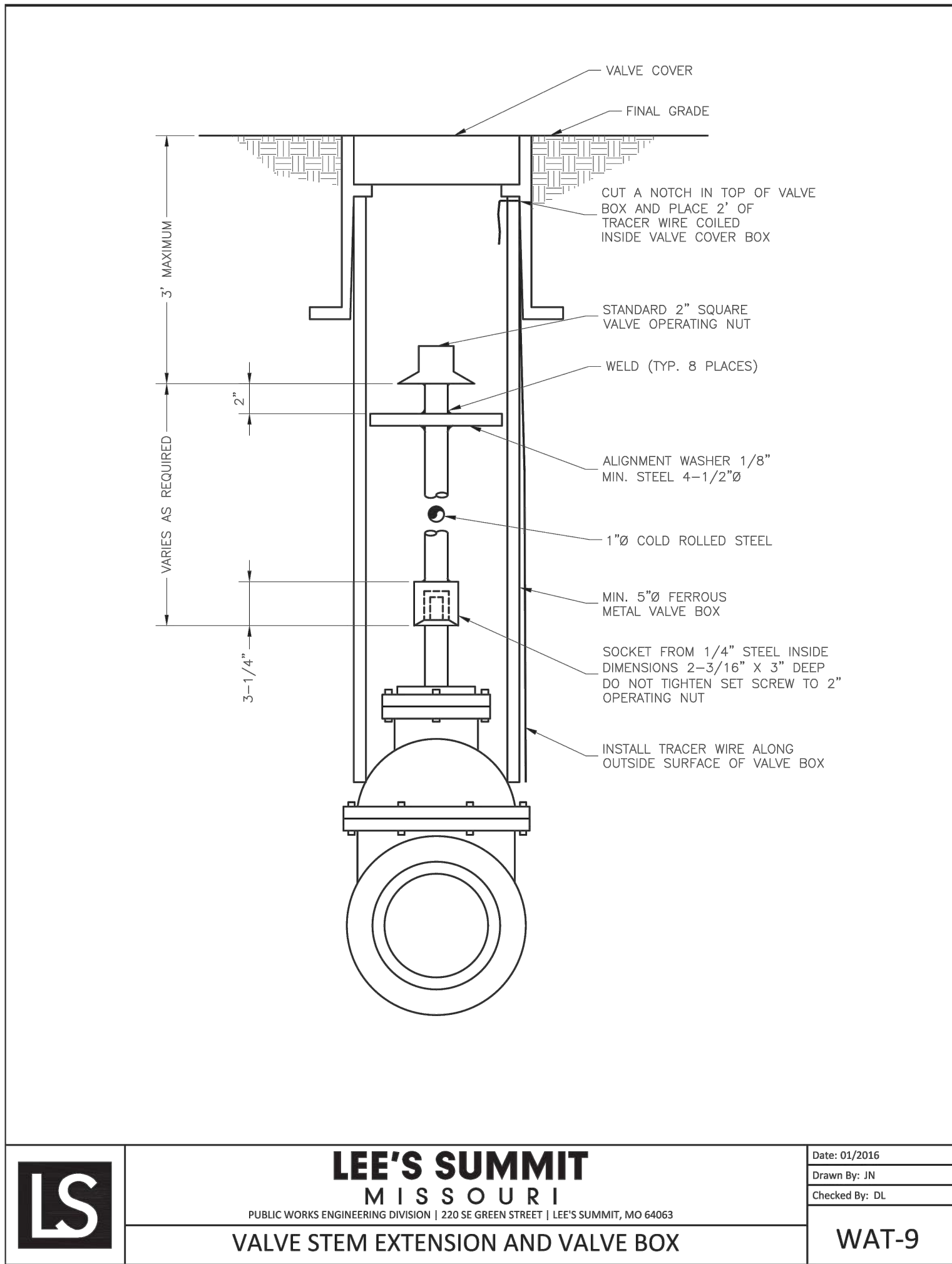
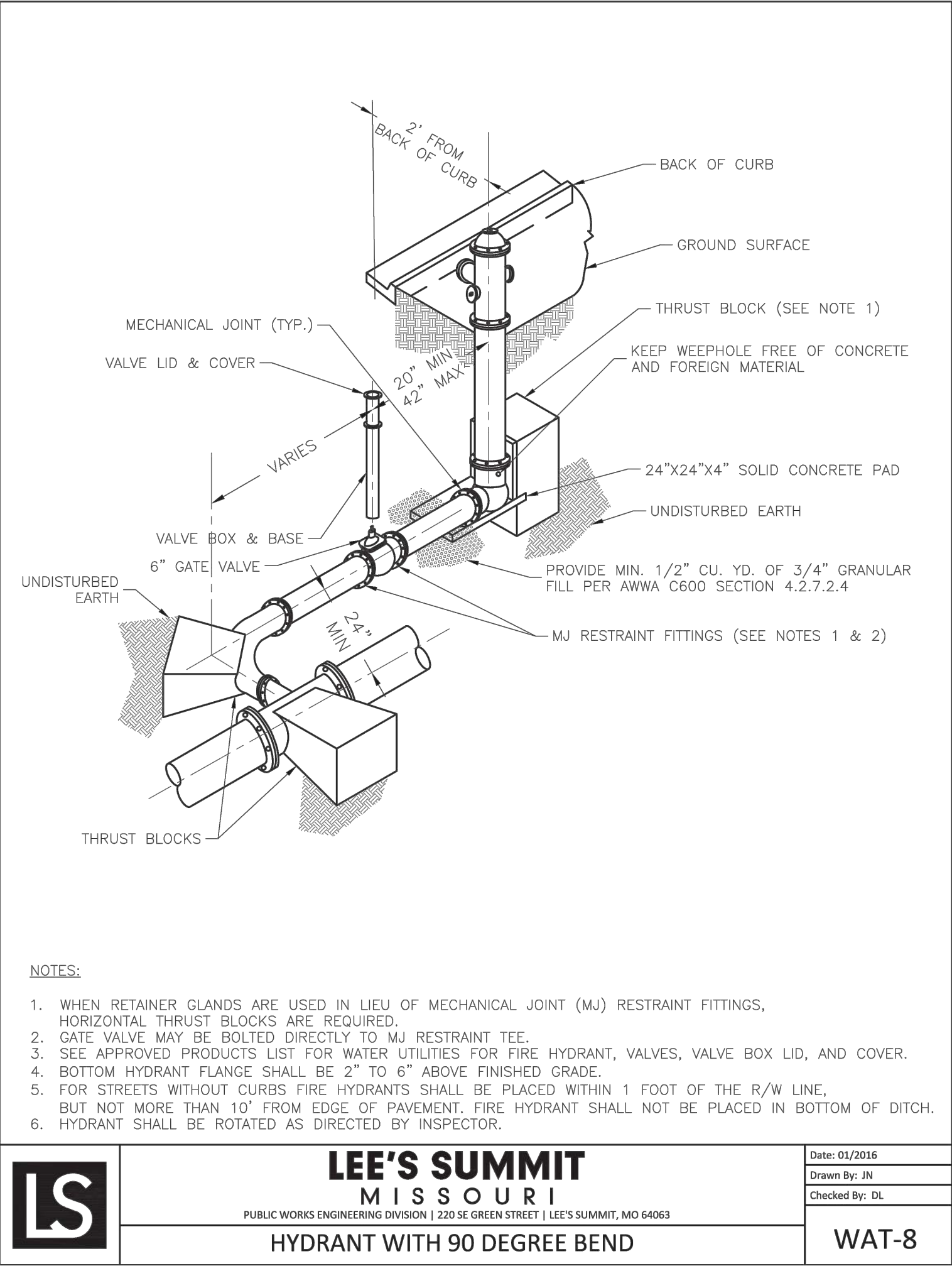
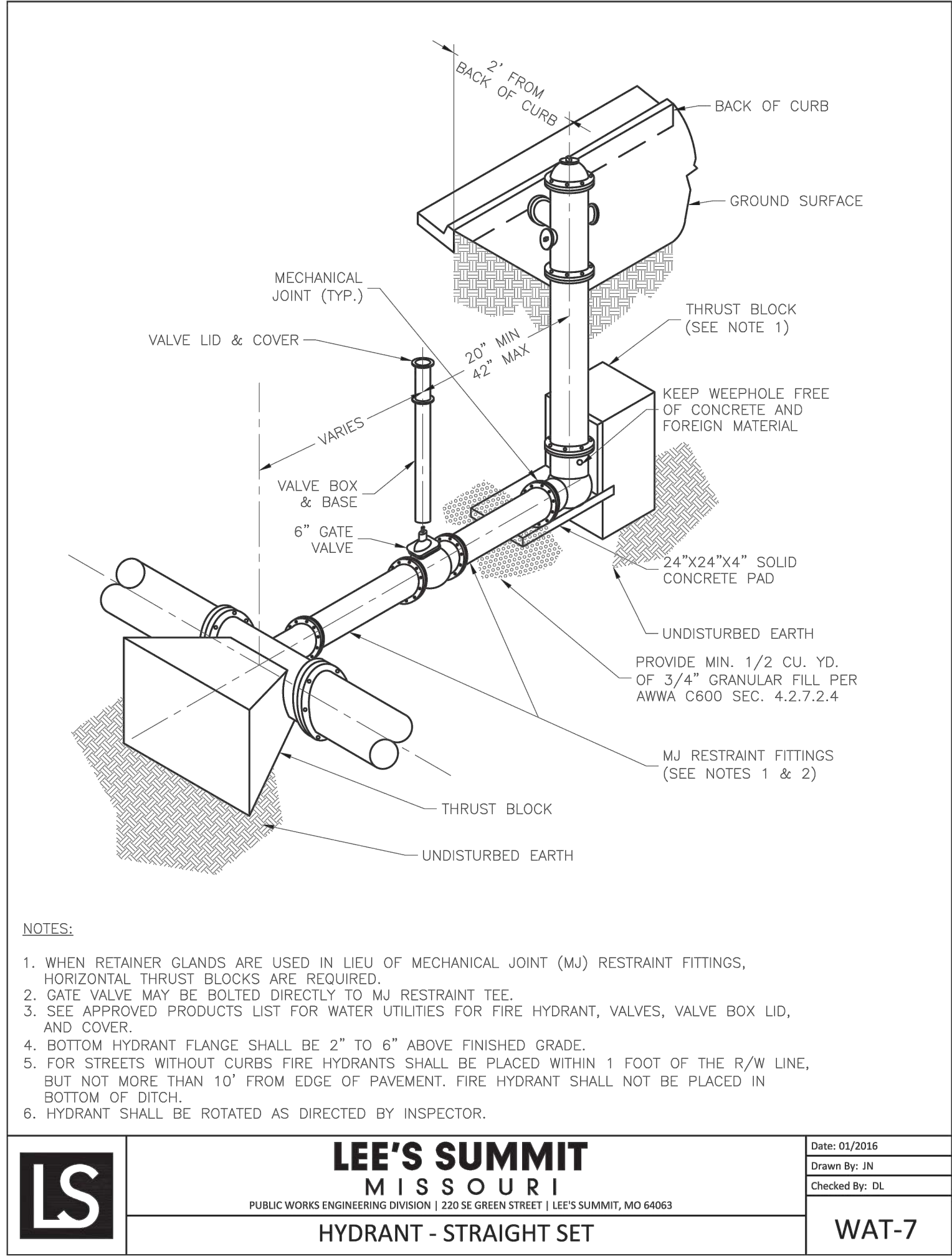
REVISION	BY

HIGH TIDE CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidelc.com

PROFESSIONAL ENGINEER
B. SHANE
GUILD NUMBER
PE - 202100076
JULY 30, 2024
DATE

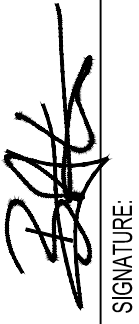
PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

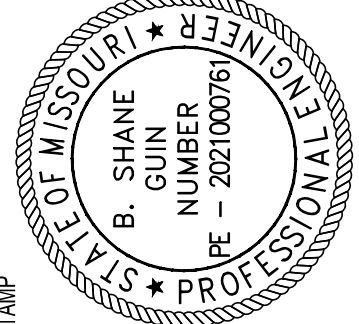
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KRG
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RCG
ISSUED DATE
07/30/2024
ISSUED FOR
PERMITTING
PROJECT NO.
22-218
FILE
22-218 C-4 Details
SHEET
C-4.7



REVISION	BY

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com


SIGNATURE
JULY 30, 2024
DATE


B. SHANE
GUIN
NUMBER
PE - 202100076
STATE OF MISSOURI
PROFESSIONAL ENGINEER

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN
KRG

CHECKED
RCG

ISSUED DATE
07/30/2024

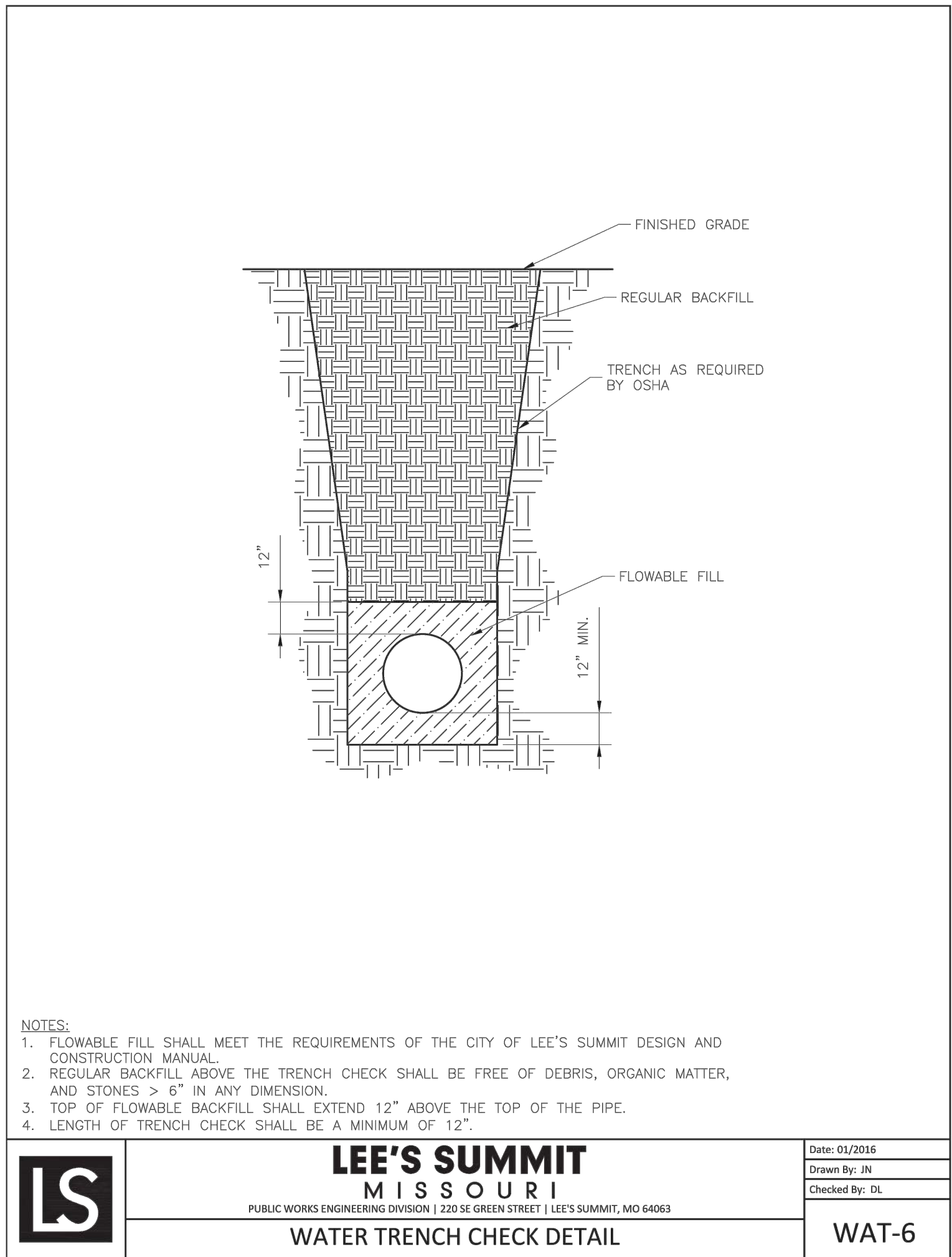
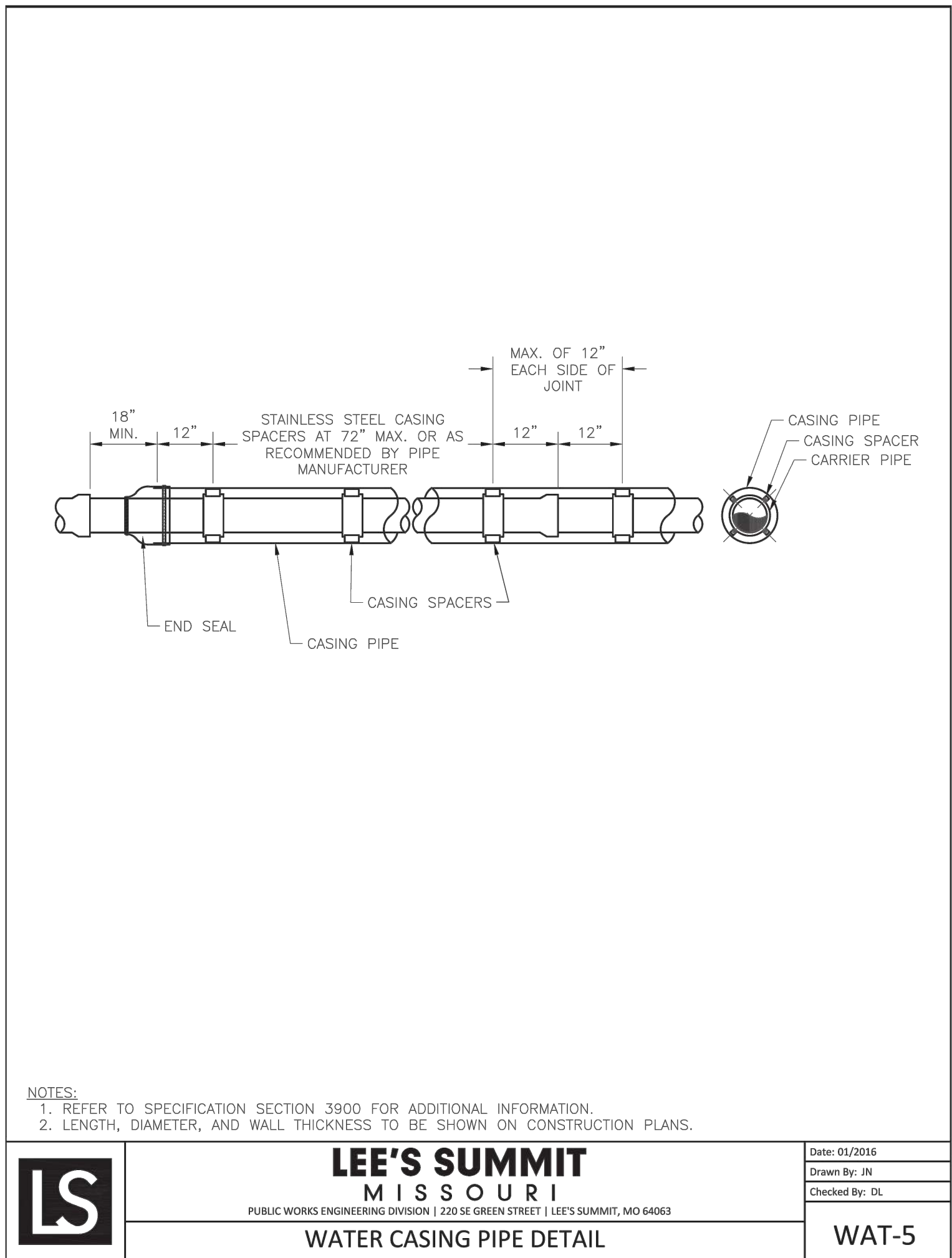
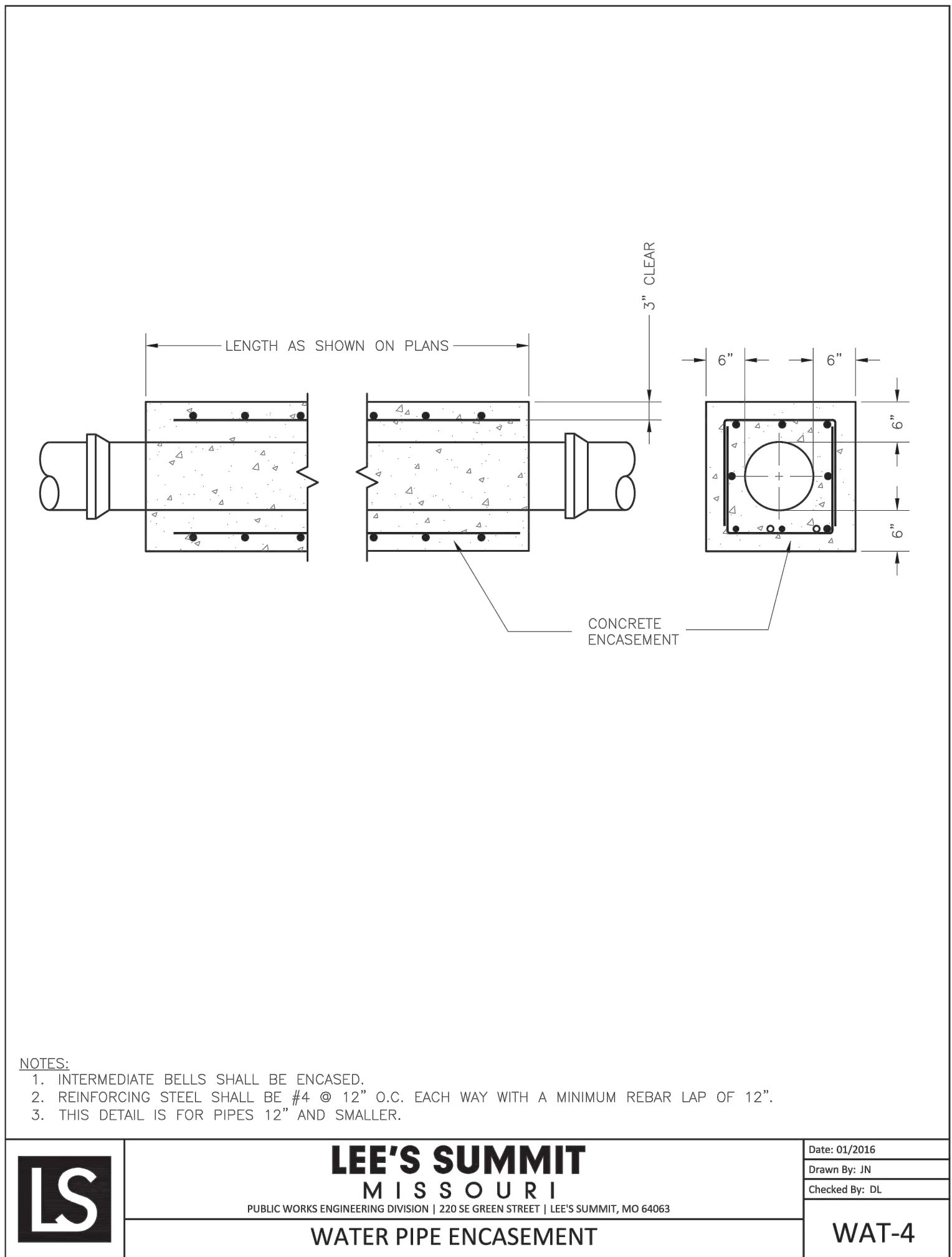
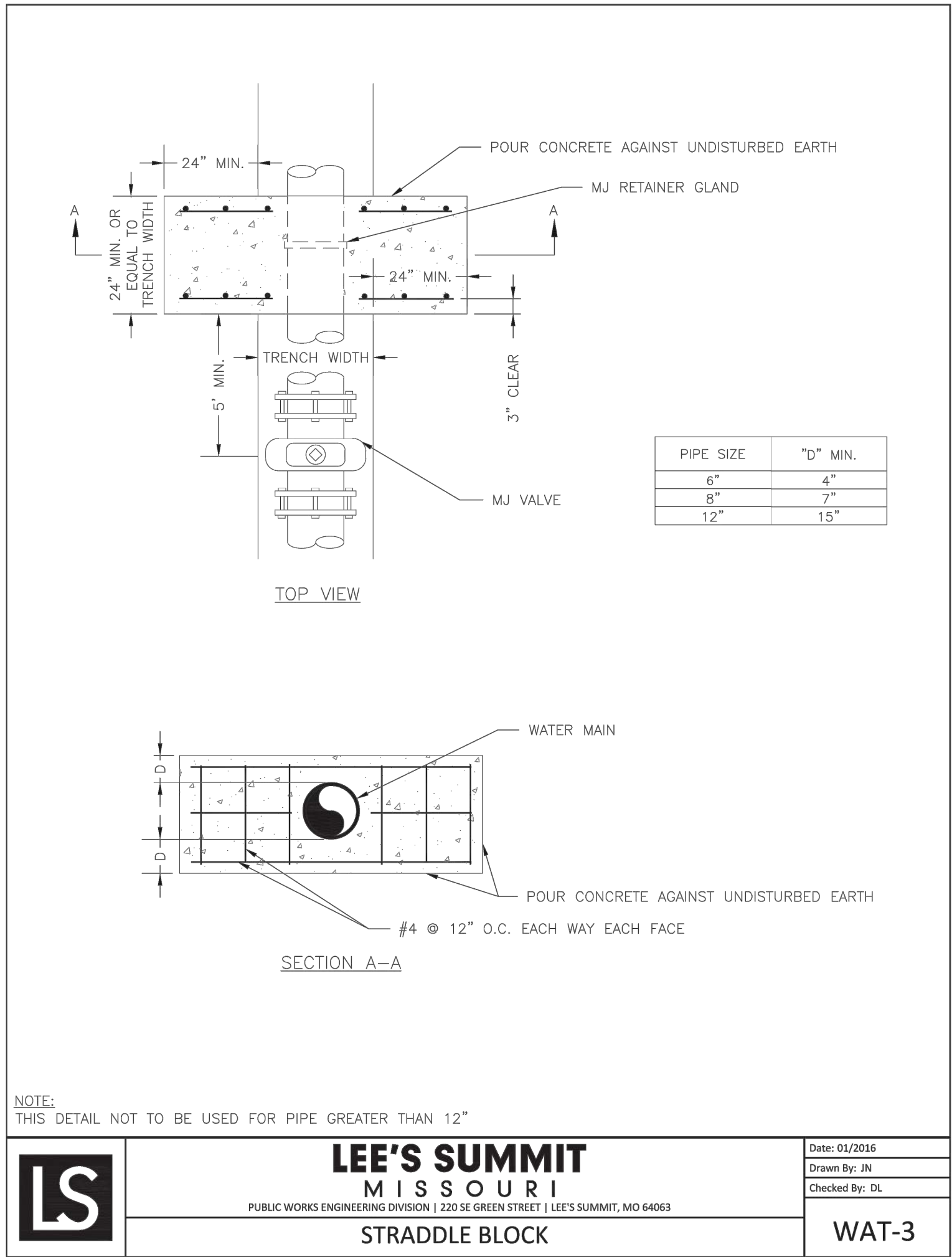
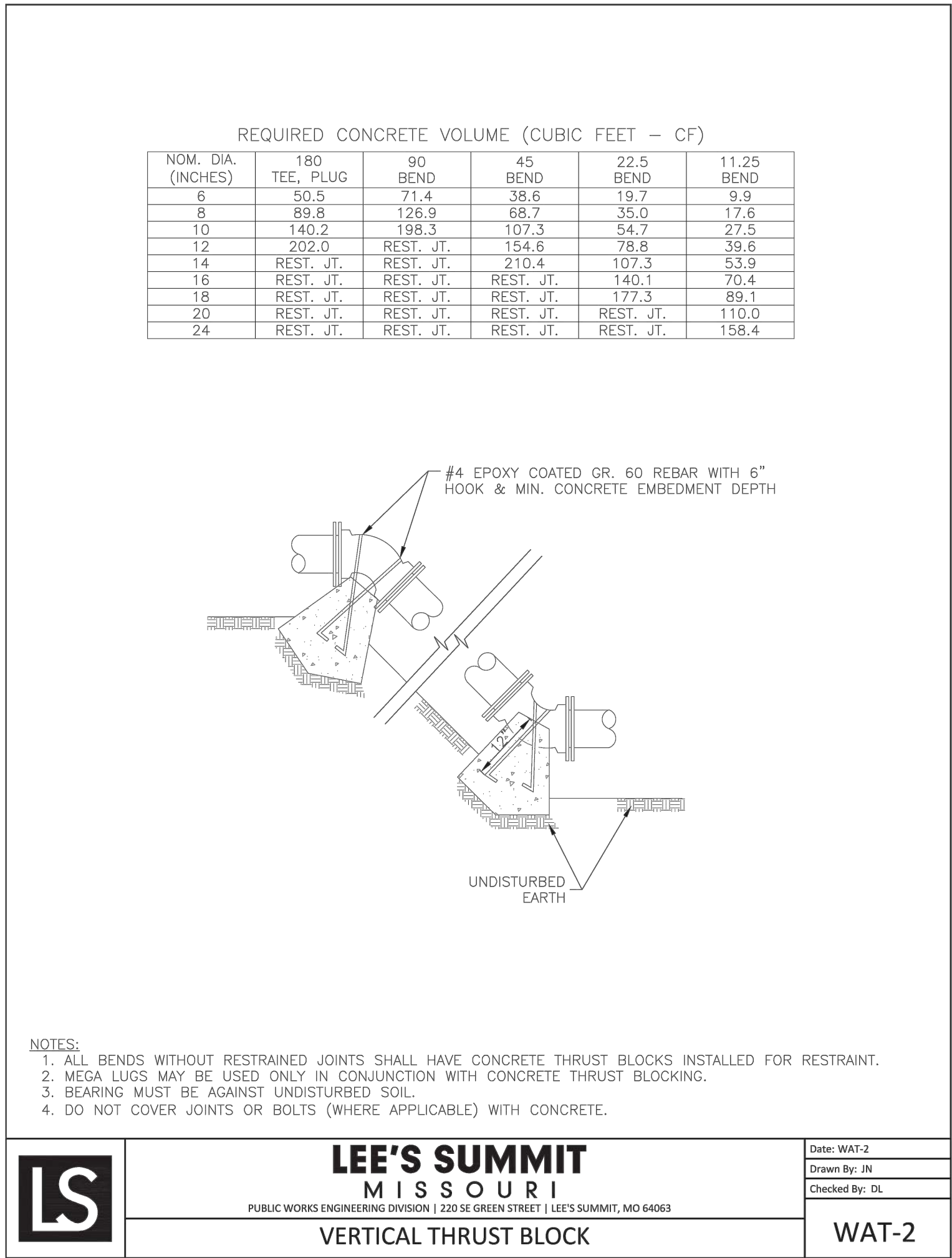
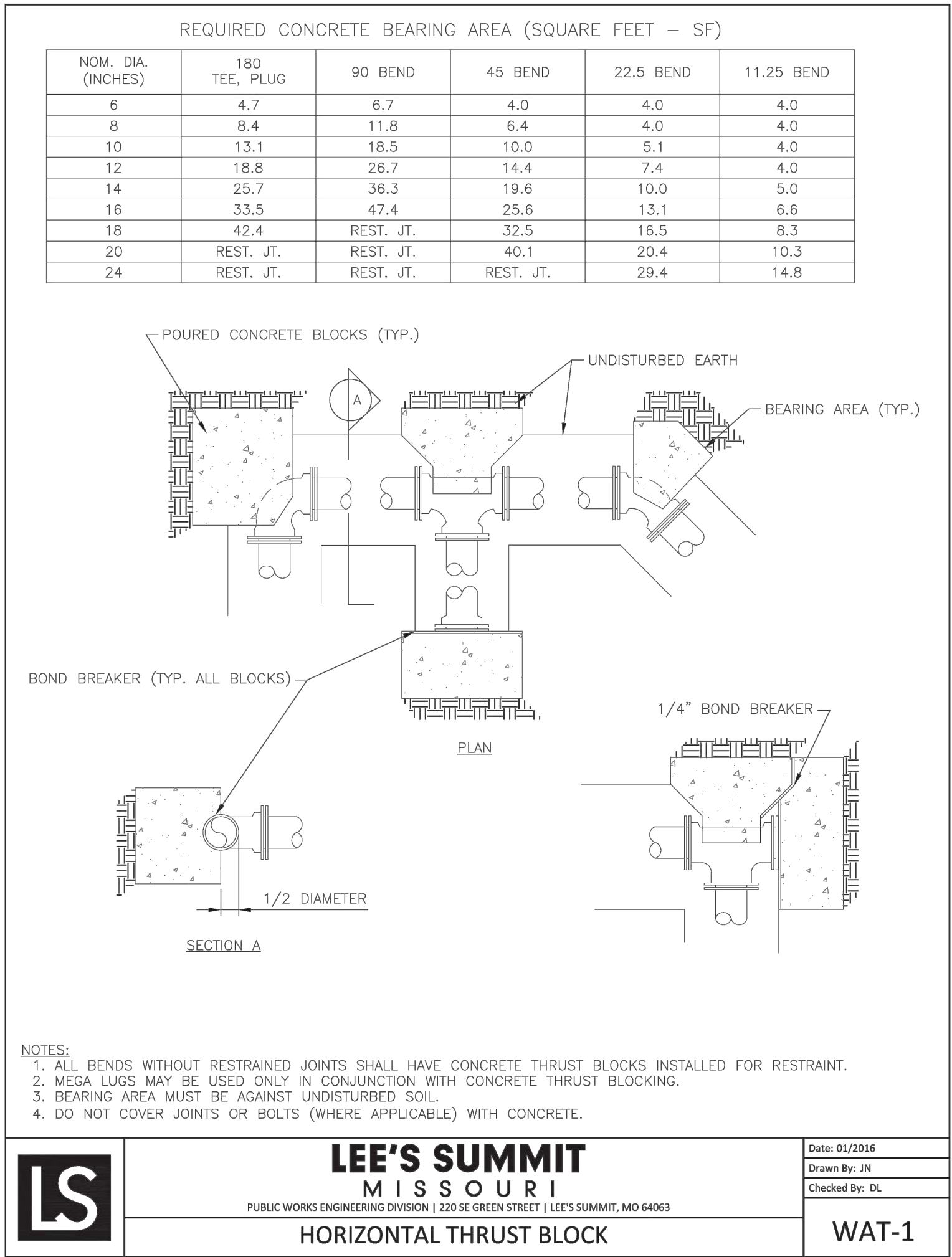
ISSUED FOR
PERMITTING

PROJECT NO.
22-218

FILE
22-218 C-4 Details

SHEET
C-4.8

UTILITY DETAILS



UTILITY DETAILS

REVISION	BY

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com

DATE: JULY 30, 2024

SIGNATURE:

STAMP:

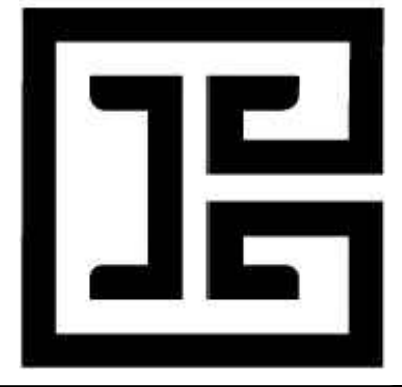
PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI

FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN: KRG
CHECKED: RCG
ISSUED DATE: 07/30/2024
ISSUED FOR PERMITTING
PROJECT NO.: 22-218
FILE: 22-218 C-4 Details
SHEET: C-4.9

REVISION	BY
REVISION #1	

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com



SIGNATURE
DATE
STAMP
STATE OF MISSOURI
RYAN
McKNIGHT
NUMBER
PLA-2021099815
PROFESSIONAL LANDSCAPE ARCHITECT

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN
ML
CHECKED
BG/EM
ISSUED DATE
9/08/2023
ISSUED FOR
PRELIMINARY DEVELOPMENT
PROJECT NO.
FILE
L1.0 LANDSCAPE ORDINANCE PLAN
SHEET
L1.0

LANDSCAPE MATERIALS AND PLANTS LIST	
QUANTITY	PLANT / MATERIAL NAME AND DESCRIPTION
832	Bed Preparation (square feet) 6" planting soil over 8" loosened topsoil. Mix top 4" of topsoil with first 3" of imported planting soil.
AS NEEDED	Hardwood Mulch, shredded (square feet) 3" thick layer in all planting areas and 2x root ball diameter ring around all trees
272	Metal Edging (linear feet) 1/2" x 4" aluminum edging color black; install between planting and gravel border
63	Gravel Border (square feet) 1/2" - 1" river rock. 3" deep layer over non-woven filter fabric
880	Lawn - Turf Type Fescue (square yards) certified weed and pest free
14	Drift Rose 'Popcorn' (Rosa 'Navarospop') 3 gallon container, 15"-18" spread, dense and compact growth habit
660	Liriope (Liriope muscari) 4" Container, 4"-6" height and spread, full container 4 per square foot
6	New Horizon Elm (Ulmus davidiana var. japonicus x Ulmus pumila) 45 gallon container, 12'-14' height, 5'-7' spread, 2 1/2"-3" caliper, straight trunk, heavily branched, heavy canopy
4	Sweetbay Magnolia 'Green Shadow'; (Magnolia virginiana 'Green Shadow') 7'-8' height, 4'-5' spread, standard, 1" caliper, heavily branched
6	Viburnum (Viburnum rhytidophyloides 'Allegheny') 5 gallon container, 3' height, dense foliage, full container
46	Japanese Yew (Podocarpus macrophyllus) 5 gallon container, 18" height, 18" spread, dense and compact growth habit, strong central leader
64	'Hayden's Sedge' Carex haydenii 3 gallon container, 7"- 8" height, 12" - 18" spread at base, full container, heavy and active foliage, planted 24" o.c.

NOTE:
OWNER'S REPRESENTATIVE SHALL REVIEW LANDSCAPING FOLLOWING INSTALLATION TO CERTIFY COMPLIANCE WITH APPROVED PLAN.

LANDSCAPE ORDINANCE STANDARDS

- THE CONTRACTOR SHALL VERIFY AND COORDINATE ALL FINAL GRADE WITH THE LANDSCAPE ARCHITECT AND OR DESIGN TEAM PRIOR TO COMPLETION.
- LOCATION AND PLACEMENT OF ALL PLANT MATERIAL SHALL BE COORDINATED WITH THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- THE LOCATION OF ALL UTILITIES ARE APPROXIMATE, THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION OPERATIONS.
- REFER TO CIVIL DRAWINGS FOR ALL GRADING AND BERMING, EROSION CONTROL, STORM DRAINAGE, UTILITIES AND SITE LAYOUT.
- PLANT QUANTITIES ARE FOR INFORMATION ONLY DRAWING SHALL PREVAIL IF CONFLICT OCCURS. CONTRACTOR IS RESPONSIBLE FOR CALCULATING OWN QUANTITIES AND BID ACCORDINGLY.
- THE CONTRACTOR IS TO NOTIFY LANDSCAPE ARCHITECT AFTER STAKING IS COMPLETE AND BEFORE PLANT PITS ARE EXCAVATED. PROVIDE PHOTOGRAPHS.
- TREE LOCATIONS IN AREAS ADJACENT TO DRIVES, WALKS, WALLS AND LIGHT FIXTURES MAY BE FIELD ADJUSTED AS APPROVED BY LANDSCAPE ARCHITECT.
- THE CONTRACTOR SHALL REPORT SUBSURFACE SOIL OR DRAINAGE PROBLEMS TO THE LANDSCAPE ARCHITECT.
- THE PLAN IS SUBJECT TO CHANGES BASED ON PLANT SIZE AND MATERIAL AVAILABILITY. ALL CHANGES OR SUBSTITUTIONS MUST BE APPROVED BY THE CITY OF LEE'S SUMMIT, MISSOURI AND THE LANDSCAPE ARCHITECT.
- ALUMINUM LANDSCAPE EDGING TO BE USED ON ALL LANDSCAPE BEDS ABUTTING TURF AREAS AS NOTED ON LANDSCAPE PLANS/LEGEND.
- LANDSCAPE CONTRACTOR IS TO BE RESPONSIBLE FOR WATERING ALL PLANT MATERIAL UNTIL THE TIME THAT A PERMANENT WATER SOURCE IS READY.
- THE CONTRACTOR SHALL SHOW PROOF OF PROCUREMENT, SOURCES, QUANTITIES AND VARIETIES FOR ALL SHRUBS, PERENNIALS ORNAMENTAL GRASSES AND ANNUALS WITHIN 21 DAYS FOLLOWING THE AWARD OF THE CONTRACT.
- CONTRACTOR SHALL PROVIDE FULL MAINTENANCE FOR NEWLY LANDSCAPED AREAS FOR A PERIOD OF 30 DAYS AFTER THE DATE OF FINAL ACCEPTANCE. AT THE END OF THE MAINTENANCE PERIOD, A HEALTHY, WELL-ROOTED, EVEN-COLORED, VIABLE TURF AND LANDSCAPED AREA MUST BE ESTABLISHED. THE LANDSCAPED AREAS SHALL BE FREE OF WEEDS, OPEN JOINTS, BARE AREAS AND SURFACE IRREGULARITIES.
- LANDSCAPE CONTRACTOR SHALL PROVIDE HARDWOOD MULCH SAMPLE TO OWNER FOR APPROVAL.

LANDSCAPE ORDINANCE CALCULATIONS

LEE'S SUMMIT MO DEVELOPMENT CODE, DIVISION III, SECTION 8.710-900,
ZONED: CP2

STREET FRONTAGE (NON RESIDENTIAL ZONE)

- A. 1 TREE PER 30 L.F. OF STREET FRONTAGE
REQUIRED: 5 TREES
PROVIDED: 5 TREES
- B. 20' WIDE LANDSCAPE STRIP TO SEPARATE PARKING AREA FROM THE STREET.
PROVIDED: 20' LANDSCAPE STRIP
- C. 1 SHRUB FOR EACH 20' OF STREET FRONTAGE
REQUIRED: 7 SHRUBS
PROVIDED: 14 SHRUBS (NEAR SIGNAGE)

OPEN YARD AREA

LOT CONTAINS A LARGE CONSERVATION AREA THAT WILL REMAIN. CALCULATIONS BELOW ARE BASED ON CLEARED PORTION OF THE LOT; 23,670 SQUARE FEET.

- A. 2 SHRUBS PER 5,000 SQUARE FEET
REQUIRED: 10 SHRUBS
PROVIDED: 20 SHRUBS (PROVIDED TO HIDE TRASH ENCLOSURE AND DRIVEWAY)
- B. 1 TREE PER 5,000 SQUARE FEET
REQUIRED: 5 TREES
PROVIDED: 5 TREES

PARKING LOT SCREENING

- A. 2 1/2' HEIGHT OF SCREENING REQUIRED ALONG THE EDGE OF PARKING LOT CLOSEST TO THE STREET
- B. A HEDGE CONSISTING OF AT LEAST 12 SHRUBS PER 40'. HEDGE MUST BE AT LEAST 18" TALL AT TIME OF PLANTING.
REQUIRED: 36 SHRUBS (CONTINUOUS HEDGE)
PROVIDED: 44 SHRUBS (PROVIDED TO HAVE CONTINUOUS HEDGE AROUND PARKING LOT PER CODE)

SOUTH & EAST BUFFER (CP2 ADJACENT TO CP2)

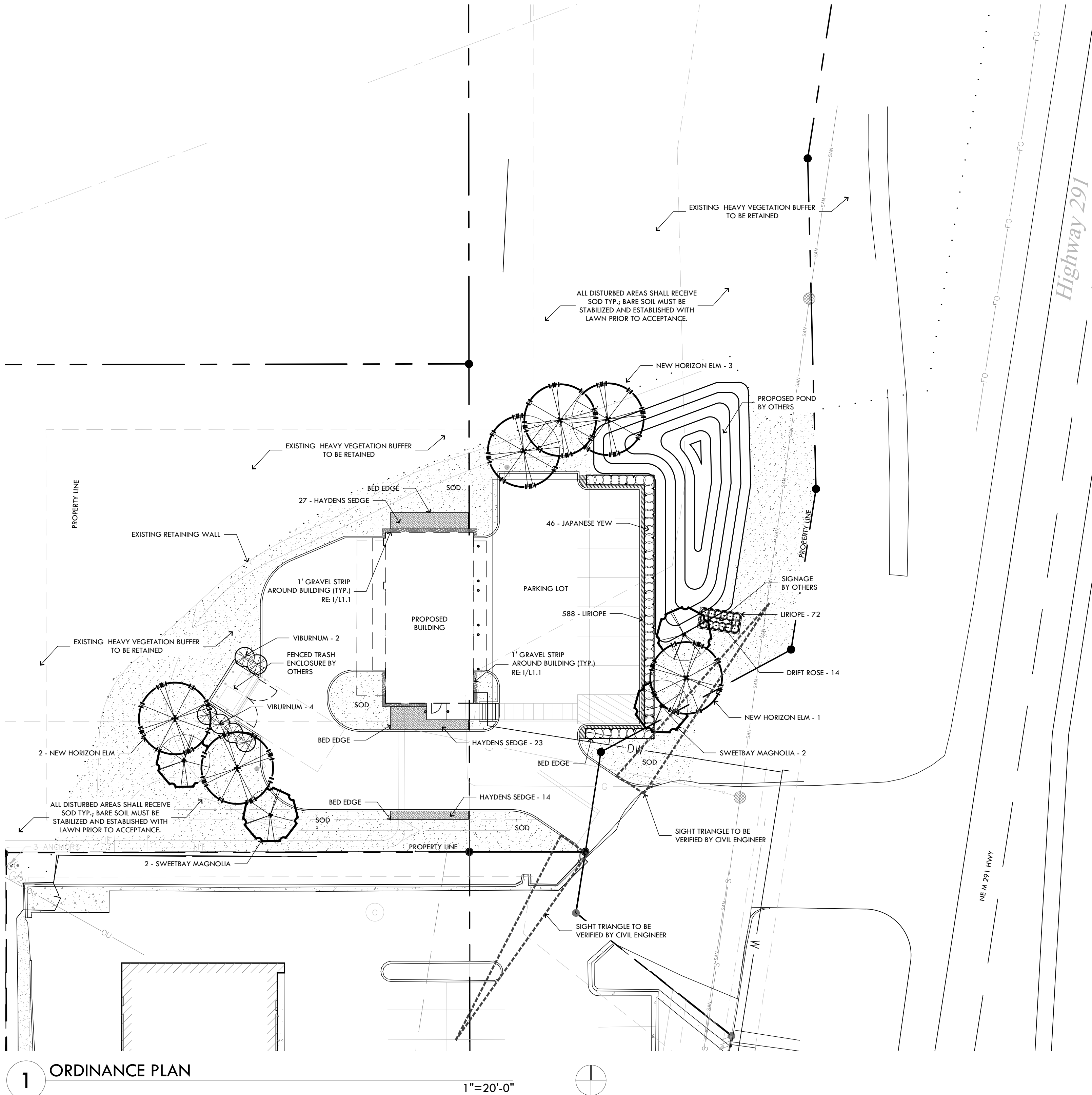
NO REQUIRED BUFFER

NORTH & WEST BUFFER (CP2 ADJACENT TO RP-2 & RP-4)

REQUIRED: 20' BUFFER YARD (HEAVY)
PROVIDED: EXISTING 30'-100' VEGETATED BUFFER & EXISTING RETAINING WALL

GENERAL PLANTING NOTES

- LOCATE ALL UTILITIES ON SITE PRIOR TO COMMENCING WORK. ANY DAMAGE DONE TO EXISTING OR NEW UTILITIES SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO OWNER.
- PLANTS SHALL BE WELL FORMED, NO. 1 GRADE OR BETTER NURSERY STOCK AND SHALL MEET THE APPLICABLE STANDARDS NOTED HEREIN AND SHALL BE SUBJECT TO REJECTION BY THE LANDSCAPE ARCHITECT.
- STAKE OUT ALL TREE LOCATIONS FOR APPROVAL BY THE OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION. LOCATE ALL TREES AS SHOWN ON PLAN.
- COORDINATE WORK WITH THE WORK OF OTHER TRADES ON THE SITE.
- ENTIRE SITE SHALL BE GRADED TO FINISH GRADE PRIOR TO SCHEDULING PLANTING INSTALLATION.
- PLANTS SHALL BE SPECIMEN QUALITY, FULL POT AND HEAD, SYMMETRICAL FOLIAGE AND BRANCHING STRUCTURE. SHRUBS SHALL BE FULL TO GROUND.
- PLANT MATERIAL OF THE SAME SPECIES SHALL BE MATCHING IN CHARACTER AND SIZE, OBTAINED FROM THE SAME SOURCE.
- ANY CHANGES IN PLANT MATERIAL SIZE, QUANTITY, SPECIES OR VARIETY MUST BE APPROVED BY THE OWNER AND/OR LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- INSTALLATION MUST BE COMPLIANT WITH LANDSCAPE ORDINANCE STANDARDS.
- SOD ALL AREAS DISTURBED BY CONSTRUCTION.

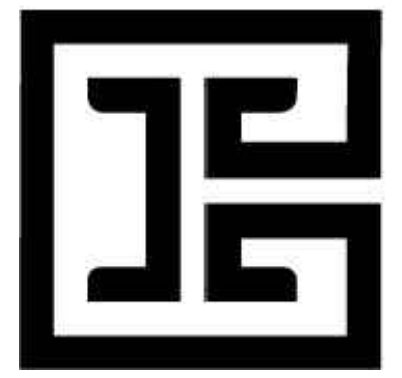


1 ORDINANCE PLAN

1"=20'-0"

REVISION	BY

HIGH TIDE
CONSULTANTS LLC
434 N. COLUMBIA ST, SUITE 200A
COVINGTON, LA 70433
www.hightidela.com



SIGNATURE	DATE
STAMP	

PROPOSED TAKE 5
LEE'S SUMMIT, MISSOURI
FOR DRIVEN ASSETS, LLC
2101 PEARL STREET
BOULDER, CO 80302

DRAWN ML
CHECKED BG/AM
ISSUED DATE 9/08/2023
ISSUED FOR PRELIMINARY DEVELOPMENT
PROJECT NO.
FILE L1.2 PLANTING DETAILS
SHEET L1.1

