

FINAL DEVELOPMENT PLAN FOR MID-CONTINENT PUBLIC LIBRARY COLBERN ROAD BRANCH ENTRANCE IMPROVEMENTS

NE 1/4 OF SECTION 29, TOWNSHIP 48 NORTH, RANGE 31 WEST
LEE'S SUMMIT, JACKSON COUNTY, MISSOURI

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NOT TO SCALE

DEVELOPMENT TEAM CONTACT INFORMATION

OWNER/DEVELOPER	
AARON MASON MID-CONTINENT PUBLIC LIBRARY	ADMINISTRATIVE HEADQUARTERS 15616 E. 24 HWY. INDEPENDENCE, MO 64050 816.836.5200
CIVIL ENGINEER	
TERRY PARSONS OLSSON	7301 W. 133RD STREET SUITE 200 OVERLAND PARK, KS 66213 PH: 913.381.1170 FAX: 913.381.1174 tparsons@olsson.com

UTILITY AND GOVERNING AGENCY CONTACT INFORMATION	
CITY OF LEE'S SUMMIT, MISSOURI	220 SE GREEN LEE'S SUMMIT, MO 64063
CITY HALL	816.969.1000
BUILDING INSPECTIONS	816.969.1200
SPECIAL INSPECTIONS	816.969.1200
BUILDING PERMITS	816.969.1200
LAND DEVELOPMENT DIVISION/INSPECTIONS	816.969.1200
TRAFFIC OPERATIONS	816.969.1800
LEE'S SUMMIT WATER UTILITIES	1200 SE HAMBLEN ROAD LEE'S SUMMIT, MO 64081
LEE'S SUMMIT WATER & SERVICES	816.969.1940
LEE'S SUMMIT PUBLIC WORKS	816.969.1800
SPIRE (MGE)	314.342.0500
KANSAS CITY POWER & LIGHT	8700 EAST FRONT STREET KANSAS CITY, MO 64120 816.471.5275
TELEPHONE SERVICE	
AT&T	816.221.1781
CABLE TELEVISION SERVICE	
SPECTRUM	877.772.2253
GOOGLE FIBER	877.454.6959

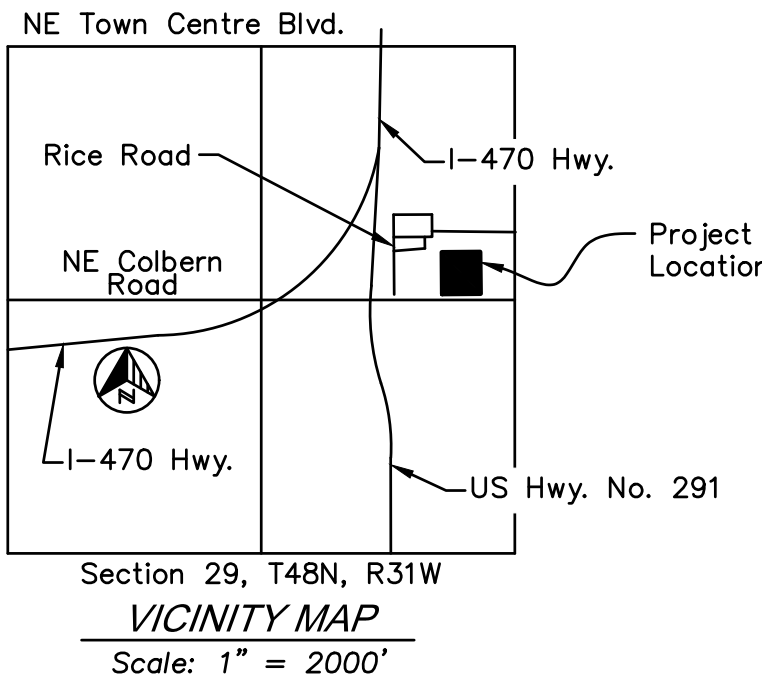
BENCHMARKS:

BENCHMARK #1:
MO BRAUN 3: ALUMINUM NGS DISK SET IN CONCRETE 5"± ABOVE GRADE 57"± WEST OF
I-470 PAVED SHOULDER STAMPED "BRAUN 3, 1979"
PUBLISHED GRID COORDINATES (2003 ADJUSTMENT)
N: 310136.375M, E: 862153.323M
ELEV = 305.7M
GRID FACTOR: 0.9999001
GROUND COORDINATES (US SURVEY FEET):
N: 1017506.29', E: 2828581.25'

LEGAL DESCRIPTION:

ALL OF LOT 1, RICE ACRES, A SUBDIVISION IN THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, TOGETHER WITH ALL THAT PART OF AN UNPLATTED TRACT OF LAND, ALL LYING IN THE NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 48 NORTH, RANGE 3 WEST, DESCRIBED BY TIMOTHY BLAIR ISWELL, MO-PLS 2009000067, OF OLSSON, INC. LC-366, AS FOLLOWS:

COMMENCING AT THE SOUTHEAST CORNER OF THE NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 48 NORTH, RANGE 31 WEST; THENCE NORTH 88 DEGREES 28 MINUTES 52 SECONDS WEST, ON THE SOUTH LINE OF SAID NORTHEAST QUARTER, A DISTANCE OF 755.18 FEET TO A POINT ON THE SOUTHERLY EXTENSION OF THE WEST LINE OF LOT 1, RICE ACRES, A SUBDIVISION IN THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI; THENCE NORTH 01 DEGREE 23 MINUTES 04 SECONDS EAST, DEPARTING SAID SOUTH LINE, ON SAID SOUTHERLY EXTENSION, A DISTANCE OF 55.66 FEET TO THE SOUTHWEST CORNER OF SAID LOT 1, THE POINT OF BEGINNING; THENCE NORTH 01 DEGREE 23 MINUTES 04 SECONDS EAST, ON SAID WEST LINE, A DISTANCE OF 436.21 FEET TO THE NORTHWEST CORNER OF SAID LOT 1; THENCE SOUTH 88 DEGREES 38 MINUTES 41 SECONDS EAST, ON THE NORTH LINE OF SAID LOT 1 AND ITS EASTERLY EXTENSION, A DISTANCE OF 400.00 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 01 DEGREE 23 MINUTES 04 SECONDS WEST, DEPARTING SAID EASTERLY EXTENSION, A DISTANCE OF 436.21 FEET TO A POINT ON THE SOUTHERLY EXTENSION OF THE SOUTH LINE OF SAID LOT 1; THENCE NORTH 88 DEGREES 38 MINUTES 41 SECONDS WEST, ON SAID EASTERLY EXTENSION AND ON SAID SOUTH LINE, A DISTANCE OF 400.00 FEET TO THE POINT OF BEGINNING, CONTAINING 174.485 SQUARE FEET OR 4.0056 ACRES, MORE OR LESS.



olsen

[illegible]

TITLE SHEET

FINAL DEVELOPMENT PLAN

COLBERN ROAD BRANCH ENTRANCE IMPROVEMENTS
1000 NE COLBERN ROAD

LEE'S SUMMIT, MISSOURI

drawn by: _____
checked by: _____
approved by: _____
QA/QC by: _____
project no.: _____
drawing no.: C_TTL01 0180330.dwg
date: _____

SHEET
C1.0

GENERAL NOTES:

1.

THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND MAY NOT INCLUDE ALL LINES PRESENT. THE CONTRACTOR SHALL BE RESPONSIBLE TO CALL "1-800-DIG-RITE", 1(800)344-7483 OR 811 AND COORDINATE FIELD LOCATION OF EXISTING UNDERGROUND UTILITIES PRIOR TO BEGINNING GRADING ACTIVITIES. !STOP!! CALL BEFORE YOU DIG!!
2.

THE CONTRACTOR SHALL NOT CHANGE OR DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE OWNER AND ENGINEER.
3.

ALL WORK AND MATERIALS SHALL BE SUBJECT TO INSPECTION AND APPROVAL BY THE OWNER OR THE OWNER'S REPRESENTATIVE.
4.

ALL ESTIMATES OF QUANTITIES ARE FOR INFORMATION PURPOSES ONLY. CONTRACTOR AND SUBCONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING ALL QUANTITIES AND FOR BRINGING THE PROJECT TO THE LINES AND GRADES SHOWN HEREIN. CONTRACTOR SHALL PROVIDE ALL WORK AND MATERIALS REQUIRED TO FULFILL THE PLANS IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO DETERMINE EARTHWORK QUANTITIES AND TO ACCOUNT FOR HAUL IN OR HAUL OFF OF MATERIAL AS NECESSARY TO MEET THE LINES AND GRADES OF THE PLANS EVEN IF QUANTITY ESTIMATES ARE SHOWN WITHIN THESE DOCUMENTS. NO ADDITIONAL PAYMENTS WILL BE MADE FOR IMPORT OR EXPORT OF MATERIAL OR FOR ADJUSTMENTS TO QUANTITY ESTIMATES.
5.

ALL CONSTRUCTION SHALL CONFORM TO THE LATEST STANDARDS AND SPECIFICATIONS OF THE THE CITY OF LEE'S SUMMIT, MO, EXCEPT WHERE SHOWN OTHERWISE. NOTIFY ENGINEER OF DISCREPANCIES.
6.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS, PAYING ALL FEES AND FOR OTHERWISE COMPLYING WITH ALL APPLICABLE REGULATIONS GOVERNING THE WORK.
7.

THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF MISSOURI STATE LAW WHICH REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT-OF-WAY DO SO ONLY AFTER GIVING NOTICE TO, AND OBTAINING INFORMATION FROM UTILITY COMPANIES.
8.

PRIOR TO COMMENCEMENT OF WORK, THE CONTRACTOR SHALL NOTIFY ALL THOSE COMPANIES WHICH HAVE FACILITIES IN THE NEAR VICINITY OF THE CONSTRUCTION TO BE PERFORMED.
9.

THE CONTRACTOR SHALL LIMIT THE REMOVAL OF TREES TO THE LIMITS OF DEMOLITION SHOWN ON THE DEMOLITION PLAN.
10.

CLEARING AND GRUBBING OPERATIONS AND DISPOSAL OF ALL DEBRIS THEREFROM SHALL BE PERFORMED BY THE CONTRACTOR IN STRICT ACCORDANCE WITH ALL LOCAL CODES AND ORDINANCES.
11.

ALL WASTE MATERIAL RESULTING FROM THE PROJECT SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR.
12.

ALL MANHOLES, CATCH BASINS, UTILITY VALVES AND METER PITS ARE TO BE ADJUSTED OR REBUILT TO GRADE AS REQUIRED.
13.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROL OF SURFACE EROSION DURING CONSTRUCTION AND UNTIL THE OWNER ACCEPTS THE WORK AS COMPLETE. EROSION CONTROL MEASURES INCLUDING, BUT NOT LIMITED TO, THE SILT FENCES AND GRAVEL FILTER BAGS SHOWN ON THE EROSION CONTROL PLAN SHALL BE IN PLACE FOR THE DURATION OF THE SITE IMPROVEMENTS.
14.

ALL HDPE PIPE SHALL BE ADS (N-12) OR APPROVED EQUAL, AND CONFORM TO AASHTO M294 SPECIFICATIONS. ALL PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
15.

IF PRECAST CONCRETE STORM SEWER STRUCTURES ARE TO BE USED ON THIS PROJECT, THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS AND HAVE THEM APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE STRUCTURES. FAILURE TO DO SO SHALL BE CAUSE FOR REJECTION.
16.

EXISTING TOPSOIL SHALL BE STRIPPED TO A POINT WHERE ALL VEGETATION IS REMOVED. SITE PREPARATION, GRADING AND EXCAVATION PROCEDURES SHALL CONFORM TO THE SPECIFICATIONS PROVIDED.
17.

THE CONTRACTOR SHALL, BY HIS OWN INVESTIGATION, AND PRIOR TO COMMENCING WORK, SATISFY HIMSELF AS TO THE SURFACE AND SUBSURFACE CONDITIONS TO BE ENCOUNTERED.
18.

THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL BOUNDARY CORNERS AND SECTION CORNERS. ANY BOUNDARY CORNER AND/OR SECTION CORNER DISTURBED OR DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE RESET BY A LAND SURVEYOR LICENSED IN THE STATE OF MISSOURI, AT THE CONTRACTOR'S EXPENSE.
19.

NO FEDERALLY OWNED MAILBOX MAY BE DISTURBED. THE CONTRACTOR SHALL GIVE AT LEAST TWENTY-FOUR (24) HOURS ADVANCE NOTICE TO THE MANAGER OF DELIVERY AND COLLECTIONS. TAMPERING WITH FEDERAL MAIL FACILITIES MAY SUBJECT THE CONTRACTOR TO PROSECUTION BY THE FEDERAL GOVERNMENT.
20.

THE CONTOUR LINES SHOWN ARE FOR MASS GRADING PURPOSES.
21.

EXISTING CONTOURS REPRESENT MASS GRADING ELEVATIONS.
22.

THE CONTRACTOR SHALL FINISH GRADE SLOPES AS SHOWN NO STEEPER THAN 1 FOOT VERTICAL IN 3 FEET HORIZONTAL UNLESS OTHERWISE SHOWN BY CONTOURS OR SPOT ELEVATIONS.
23.

THE CONTRACTOR SHALL GRADE LANDSCAPED AREAS TO PROVIDE POSITIVE DRAINAGE IN THE BORROW AREA.
24.

THE CONTRACTOR SHALL MAKE HIS OWN ASSUMPTIONS ON THE LOCATION AND CONSISTENCY OF ANY EXISTING ROCK LAYERS UNDERLYING THE PROJECT SITE. ALL ROCK EXCAVATION AND REMOVAL SHALL BE INCLUDED IN THE CONTRACTORS' BID.
25.

CONTRACTOR TO FIELD VERIFY ELEVATIONS AND LOCATIONS OF EXISTING UTILITIES AND INFRASTRUCTURE PRIOR TO CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES BETWEEN PLANS AND FIELD CONDITIONS.
26.

BY ACCEPTING AND UTILIZING ANY ELECTRONIC FILE OF ANY DRAWING, REPORT OR DATA TRANSMITTED BY OLSSON ASSOCIATES (OA), THE RECIPIENT AGREES FOR ITSELF, ITS SUCCESSORS, ASSIGNS, INSURERS AND ALL THOSE CLAIMING UNDER OR THROUGH IT, THAT BY USING ANY OF THE INFORMATION CONTAINED IN THE ELECTRONIC FILE, ALL USERS AGREE TO BE BOUND BY THE FOLLOWING TERMS. ALL OF THE INFORMATION CONTAINED IN THIS ELECTRONIC FILE IS THE WORK PRODUCT AND INSTRUMENT OF SERVICE OF OA, WHO SHALL BE DEEMED THE AUTHOR, AND SHALL RETAIN ALL COMMON LAW, STATUTORY LAW AND OTHER RIGHTS, INCLUDING COPYRIGHTS, UNLESS THE SAME HAVE PREVIOUSLY BEEN TRANSFERRED IN WRITING TO THE RECIPIENT. THE INFORMATION CONTAINED IN THE ELECTRONIC FILE IS PROVIDED FOR THE CONVENIENCE OF THE RECIPIENT AND IS PROVIDED IN "AS IS" CONDITION, THE RECIPIENT IS AWARE THAT DIFFERENCES MAY EXIST BETWEEN THE ELECTRONIC FILES AND THE PRINTED HARD-COPY ORIGINAL SIGNED AND SEALED DRAWINGS OR REPORTS. IN THE EVENT OF A CONFLICT BETWEEN THE SIGNED AND SEALED ORIGINAL DOCUMENTS PREPARED BY OA AND THE ELECTRONIC FILES TRANSFERRED HERewith, THE SIGNED AND SEALED ORIGINAL DOCUMENTS SHALL GOVERN. OA SPECIFICALLY DISCLAIMS ALL WARRANTIES, EXPRESSED OR IMPLIED, INCLUDING WITHOUT LIMITATION, ANY WARRANTY OF MERCHANTABILITY OR FITNESS FOR A PARTICULAR PURPOSE WITH RESPECT TO ELECTRONIC FILES. IT SHALL BE THE RECIPIENT'S RESPONSIBILITY TO CONFIRM THE ACCURACY OF THE INFORMATION CONTAINED IN THE ELECTRONIC FILE AND THAT IF ACCURATELY REFLECTS THE INFORMATION NEEDED BY THE RECIPIENT. THE RECIPIENT SHALL NOT RETRANSMIT THE ELECTRONIC FILE, OR ANY PORTION THEREOF, WITHOUT INCLUDING THIS DISCLAIMER AS PART OF ANY SUCH TRANSMISSION. IN ADDITION, THE RECIPIENT AGREES, TO THE FULLEST EXTENT PERMITTED BY LAW, TO INDEMNIFY AND HOLD HARMLESS OA, ITS OFFICERS, DIRECTORS, EMPLOYEES AND SUBCONSULTANTS AGAINST ANY AND ALL DAMAGES, LIABILITIES, CLAIMS OR COSTS, INCLUDING REASONABLE ATTORNEY'S AND EXPERT WITNESS FEES AND DEFENSE COSTS, ARISING FROM ANY CHANGES MADE BY ANYONE OTHER THAN OA OR FROM ANY REUSE OF THE ELECTRONIC FILES WITHOUT THE PRIOR WRITTEN CONSENT OF OA.
27.

DESIGN PROFESSIONAL SHALL REVIEW SHOP DRAWINGS OR SAMPLES FOR GENERAL CONFORMANCE WITH THE DESIGN CONCEPTS ON THE PROJECT AND FOR COMPLIANCE WITH THE INFORMATION GIVEN IN THE CONTRACT DOCUMENTS, AND SHALL NOT EXTEND TO MEANS OR METHODS OF CONSTRUCTION. THE DESIGN PROFESSIONAL'S REVIEW SHALL NOT RELIEVE CONTRACTOR FROM RESPONSIBILITY FOR ANY VARIATION FROM THE REQUIREMENTS OF THE CONTRACT DOCUMENTS UNLESS CONTRACTOR HAS IN WRITING CALLED DESIGN PROFESSIONAL'S ATTENTION TO EACH SUCH VARIATION AT THE TIME OF SUBMISSION, AND DESIGN PROFESSIONAL HAS GIVEN WRITTEN APPROVAL OF EACH SUCH VARIATION BY SPECIFIC WRITTEN NOTATION THEREOF INCORPORATED INTO OR ACCOMPANYING THE SHOP DRAWING OR SAMPLE; NOR WILL ANY APPROVAL BY THE DESIGN PROFESSIONAL RELIEVE CONTRACTOR FROM RESPONSIBILITY FOR ERRORS OR OMISSIONS IN SHOP DRAWINGS WITH CONFORMANCE TO CONTRACT DOCUMENTS.

28.

GENERAL CONSTRUCTION NOTE REGARDING SEQUENCING OF EROSION CONTROL – ALL PERIMETER SILT FENCE, EARTH DIKES, SEDIMENT BASINS, AND ROCK CONSTRUCTION ENTRANCES WILL BE INSTALLED BEFORE GRADING OPERATIONS BEGIN, EXCEPT THAT SILT FENCE WHICH IS TO BE PLACED ALONG THE BACK OF CURB FOR PROTECTION OF THE STREET. SILT FENCE AND EARTH DIKES THAT ARE PLACED BEFORE GRADING BEGINS WILL BE MAINTAINED BY THE GRADING CONTRACTOR UNTIL ALL UTILITIES ARE IN PLACE. THE SILT FENCE THAT IS PLACED ALONG THE BACK OF THE CURB OR RIGHT –OF-WAY WILL BE INSTALLED IMMEDIATELY AFTER THE CURB IS CONSTRUCTED. EROSION AND SEDIMENTATION CONTROLS ARE TEMPORARY AND MUST BE REMOVED BY THE CONTRACTOR AFTER CONSTRUCTION IS COMPLETE AND THE DISTURBED AREA IS AT LEAST 70% PERMANENTLY VEGETATED.
29.

HANDICAP PARKING STALLS SHALL BE SIGNED WITH CITY/ADA APPROVED SIGNAGE AND CONSTRUCTED IN STRICT ACCORDANCE WITH THE LATEST STANDARDS AND SPECIFICATIONS OF THE CITY OF LEE'S SUMMIT ADA STANDARDS, AND SHALL NOT EXCEED 2.00 PERCENT IN ANY DIRECTION. ACCESSIBLE SIDEWALKS HAVE A MAXIMUM CROSS SLOPE OF 2 PERCENT AND A MAXIMUM LONGITUDINAL SLOPE OF 5 PERCENT.
30.

ALL WATER LINES SHALL BE INSTALLED PER THE LATEST STANDARDS AND SPECIFICATIONS OF THE CITY OF LEE'S SUMMIT, MO. ALL WATER LINES SHALL BE A MINIMUM OF 48 INCHES BELOW THE FINISHED GRADE ELEVATIONS SHOWN HEREIN.
31.

ALL WATER LINES SHALL BE INSTALLED PER CITY STANDARDS. ALL WATER LINES SHALL BE A MINIMUM OF 48 INCHES BELOW THE FINISHED GRADE ELEVATIONS SHOWN HEREIN.
32.

ALL EXTERIOR CONCRETE SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 4000 PSI, SHALL MEET KOMMB STANDARDS AND SPECIFICATIONS, AND SHALL BE AIR ENTRAINED. FLYASH IS NOT A SUITABLE REPLACEMENT FOR PORTLAND CEMENT.
33.

ALL ON-SITE WIRING AND CABLES SHALL BE PLACED UNDERGROUND.
34.

CONCRETE PAVEMENT JOINTS SHALL BE CONSTRUCTED AS FOLLOWS (REFER TO HARDSCAPE PLANS FOR SPECIFIC TREATMENT OF THESE AREAS):

A.

CONTROL JOINTS SPACED AT INTERVALS NOT GREATER THAN 12 FEET AND TOOLED TO 1/3 THE SLAB THICKNESS.

B.

CONSTRUCTION JOINTS AT THE END OF EACH POUR AND WHEN PAVING OPERATIONS ARE SUSPENDED FOR 30 MINUTES OR MORE.

C.

ISOLATION JOINTS PLACED WHERE THE PAVEMENT ABUTS THE BUILDING, DRAINAGE STRUCTURES AND OTHER FIXED STRUCTURES, CONSTRUCTED WITH A 1/2" NONEXTRUDING FILLER, CLOSED-CELL FOAM RUBBER OR A BITUMEN-TREATED FIBER-BOARD, AND WITH A THICKENED EDGE, INCREASED BY 20 PERCENT, TAPERED TO THE REGULAR THICKNESS IN 5 FEET.

D.

ALL EXPANSION JOINTS SHALL BE FILLED AND SEALED WITH A PLASTIC JOINT SEALANT MATERIAL.
34.

TELEPHONE AND COMMUNICATION SERVICE ROUTING AND CONDUITS NOT SHOWN ON PLANS. CONTRACTOR SHALL INSTALL NECESSARY CONDUIT PRIOR TO PAVEMENT INSTALLATION. CONTRACTOR SHALL COORDINATE ROUTING AND INSTALLATION SCOPE WITH SERVICE PROVIDER.
35.

ANY CONTRACTOR BIDDING ANY PORTION OF THIS WORK SHALL HAVE IN HIS OR HER POSSESSION A COMPLETE SET OF CONSTRUCTION DOCUMENTS AND BE FAMILIAR WITH ALL SCOPES OF WORK AND TRADES TO UNDERSTAND THEIR INTERACTIONS.

GENERAL UTILITY NOTES

1.

THE SIZE AND LOCATION OF SERVICES SHALL BE VERIFIED WITH THE ARCHITECTURAL AND MEP PLANS PRIOR TO CONSTRUCTION. IF DISCREPANCIES EXIST, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY.
2.

IRRIGATION CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING SLEEVING UNDER PAVING AREAS WHERE NECESSARY.
3.

INSTALL ALL PIPE LENGTHS, BENDS AND FITTINGS NECESSARY FOR UTILITY CONNECTIONS.
4.

CONTRACTOR SHALL VERIFY ALL CROSSING ELEVATIONS AND LOCATIONS, SIZES, AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO CONSTRUCTION OF STORM LINES AND ALL UTILITY SERVICE CONNECTIONS. ANY CONFLICTS SHALL BE MADE KNOWN TO THE ENGINEER AND RESOLVED PRIOR TO CONSTRUCTION.
5.

CONTRACTOR TO VERIFY FIRE SERVICE SIZE WITH SPRINKLER DESIGNER/CONTRACTOR PRIOR TO CONSTRUCTION AND INSTALLATION OF METER/BACKFLOW PREVENTER AND SERVICES. NOTIFY ENGINEER OF ALTERATIONS.
6.

CONTRACTOR RESPONSIBLE FOR PAYING ALL TAP AND CONNECTION FEES AND SHALL CONTRACT AND PAY FOR ANY REQUIRED SUB CONTRACTORS BY UTILITY COMPANIES.
7.

REFERENCE MEP PLANS FOR BUILDING CONNECTIONS.
8.

CONTRACTOR TO REPAIR ALL AREA DAMAGED BY CONSTRUCTION TO EXISTING CONDITIONS OR BETTER.
9.

BACK FLOW PREVENTION TO BE PROVIDED INSIDE BUILDING. SEE MEP AND ARCHITECTURAL PLANS FOR DETAILS.
10.

LOCATION FOR POWER SHOWN IS APPROXIMATE AND SUBJECT TO CHANGE. CONTRACTOR TO VERIFY FINAL LOCATION AND DESIGN WITH UTILITY COMPANY PRIOR TO CONSTRUCTION.
11.

CONTRACTOR TO COORDINATE LIGHT POLE LOCATIONS WITH OWNER, STORM SEWER INSTALLATION AND UTILITY COMPANIES PRIOR TO INSTALLATION TO AVOID CONFLICTS. NOTIFY ENGINEER AND ARCHITECT OF ANY CONFLICTS PRIOR TO INSTALLATION.
12.

WATER METER CANNOT BE INSTALLED IN THE BUILDING.
13.

CONTRACTOR SHALL COORDINATE CABLE/FIBER OPTIC CONDUIT AND SERVICE INSTALLATION WITH UTILITY COMPANY.
14.

ALL TAPS AND CONNECTIONS FOR FIRE AND DOMESTIC WATER SERVICES ARE TO BE IN ACCORDANCE WITH THE CITY OF LEES SUMMIT STANDARDS AND SPECIFICATIONS.
15.

CONTRACTOR TO COORDINATE POWER ROUTING TO MONUMENT SIGNS NOT SHOWN ON PLANS.
16.

ALL ROOF DRAIN AND DOWNSPOUT HEADER PIPES SHALL BE 12" HDPE PIPE AND INSTALLED AT 1.00% MINIMUM SLOPE UNLESS OTHERWISE NOTED WITHIN THIS PLAN. ALL BENDS AND FITTINGS NEEDED TO BUILD ROUTING AS SHOWN SHALL BE INCLUDED IN BID.
17.

CONTRACTOR SHALL PROVIDE AND INSTALL ALL NECESSARY FITTINGS TO COMPLETE ROOF DRAIN AND DOWNSPOUT CONNECTIONS TO BUILDINGS. ALL ROOF DRAIN AND DOWNSPOUT CONNECTIONS / FITTINGS, INCLUDING BUT NOT LIMITED TO BENDS AND TEES, SHALL BE MADE OF HDPE PIPE UNLESS OTHERWISE NOTED WITHIN THIS PLAN.



THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF THE SENATE BILL NUMBER 583, 78TH GENERAL ASSEMBLY OF THE STATE OF MISSOURI. THE BILL REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT-OF-WAY DO SO ONLY AFTER GIVING NOTICE TO, & OBTAINING INFORMATION FROM, UTILITY COMPANIES. STATE LAW REQUIRES 48 HOURS ADVANCE NOTICE. CALL 1-800-DIG-RITE.

olsson

7301 West 133rd Street, Suite 200
Overland Park, KS 66213-4750
TEL 913.381.1170 www.olsson.com

STATE OF MISSOURI
TERRY M. PARSONS
NUMBER PE-2018010505
12/14/2024
PROFESSIONAL ENGINEER

BY

REV. NO.

DATE

REVISED PER CITY COMMENTS

RLK

REVISIONS

GENERAL NOTE
FINAL DEVELOPMENT PLAN

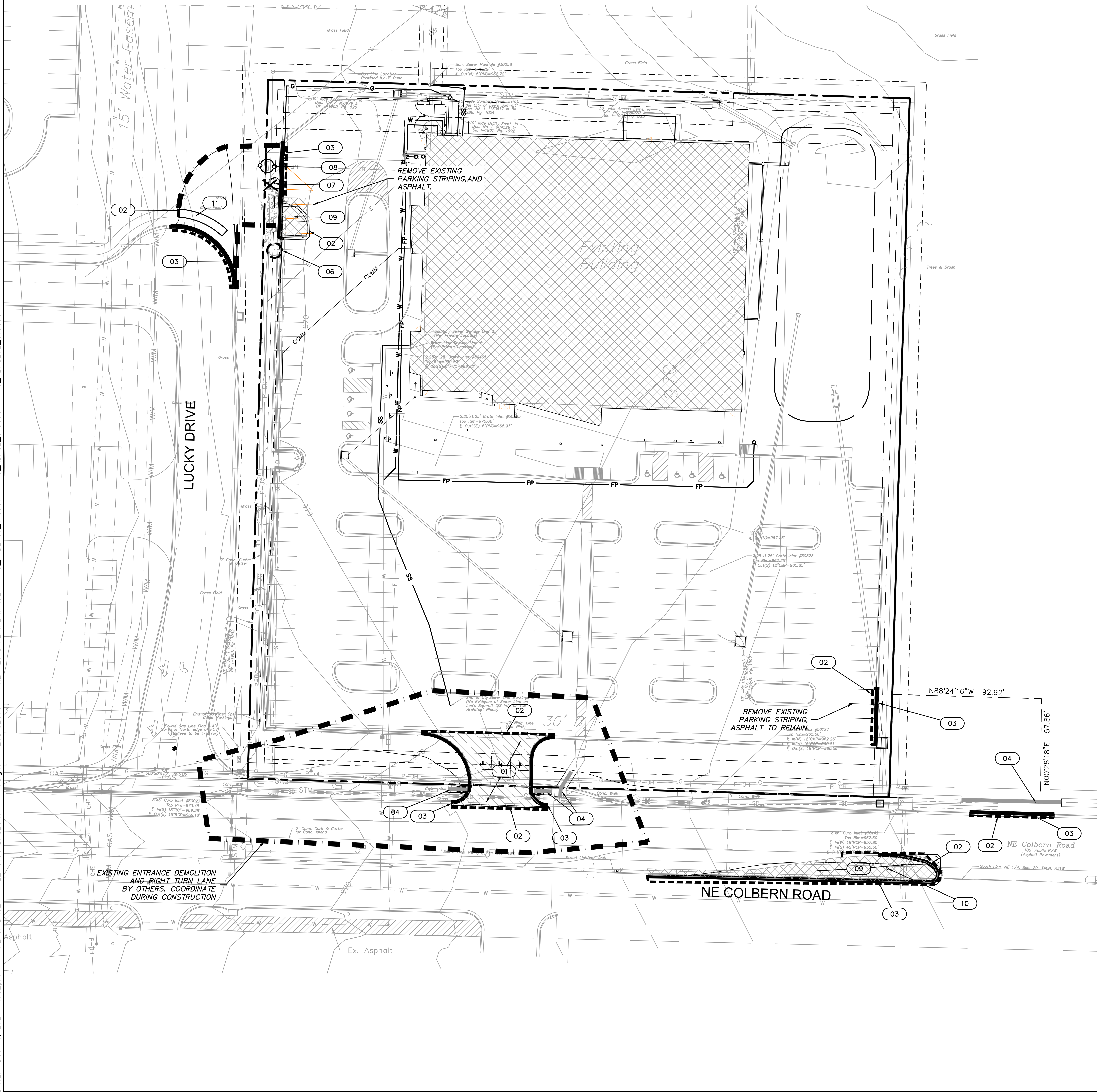
COLBURN ROAD IMPROVEMENTS
MCPL - COLBURN ROAD

2024

LEE'S SUMMIT, MISSOURI

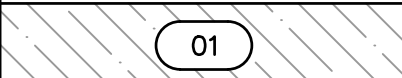
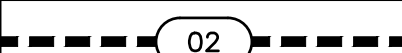
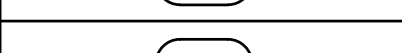
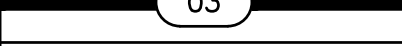
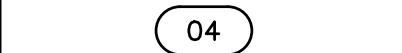
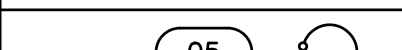
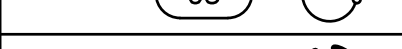
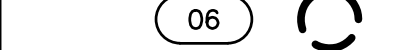
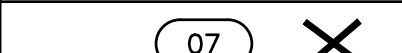
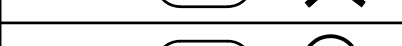
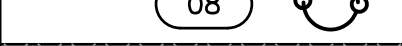
drawn by: JTF
checked by: TP
approved by: TP
QA/QC by: BS
project no.: 018-0330 WP10
drawing no.: TTL01_0180330.dwg
date: 11/01/2024

SHEET
C1.1



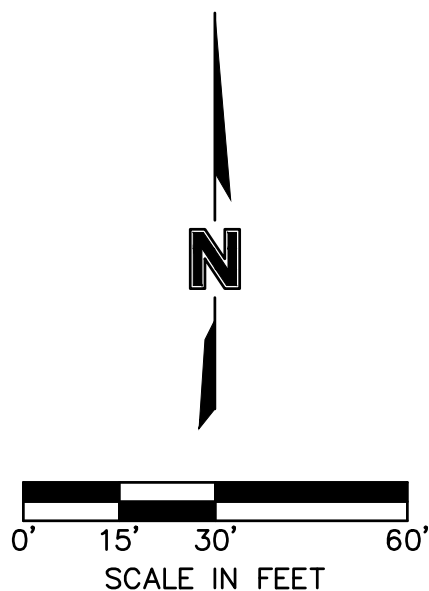
LEGEND

PROPERTY LINE

	01	REMOVE EXISTING CONCRETE ENTRANCE.
	02	SAWCUT EXISTING PAVEMENT
	03	REMOVE EXISTING CURB & GUTTER
	04	NOT USED
	05	PROTECT EXISTING UTILITY EQUIPMENT/ MANHOLE
	06	INSTALL TREE PROTECTION. SEE DETAIL ON C6.5
	07	REMOVE EXISTING TREE
	08	RELOCATE EXISTING ELECTRICAL BOX
	09	REMOVE EXISTING ASPHALT
	10	CONTRACTOR TO RELOCATE EXISTING LIGHT POLE. COORDINATE WITH CITY OF LS.
	11	REMOVE EXISTING CONCRETE SIDEWALK

DEMOLITION NOTES

1. CONTRACTOR TO PRESERVE ALL SURVEY CONTROL.
2. CONTRACTOR TO COMPLETE DEMOLITION PER THE INTENT OF THESE PLANS.
3. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE ENGINEER MAKES NO GUARANTEES THAT THE UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE ENGINEER HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. THIS INCLUDES PRIVATE AND PUBLIC UTILITIES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT MISSOURI ONE CALL AT 1-800-344-7483 IN ADVANCE OF ANY EXCAVATION TO COORDINATE UTILITY LOCATIONS.
4. CONTRACTOR IS REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN AND ANY OTHER EXISTING LINES NOT OF RECORD OR SHOWN ON THESE PLANS.
5. CONTRACTOR TO REMOVE ALL TREES AND BRUSH FOR INSTALLATION OF IMPROVEMENTS.
6. REMOVAL AND DISPOSAL OF BUSHES AND TREES SMALLER THAN 12" IN DIAMETER SHALL BE CONSIDERED SUBSIDIARY TO THE PRICE BID FOR CLEARING AND GRUBBING.
7. ALL ITEMS REMOVED SHALL BE LEGALLY DISPOSED OF OFF SITE BY THE CONTRACTOR.
8. DO NOT DISRUPT UTILITY SERVICE TO ADJACENT BUSINESSES OR RESIDENCES WITHOUT PRIOR WRITTEN APPROVAL BY THE ENGINEER.
9. DO NOT DISRUPT TRAFFIC ON ADJACENT PUBLIC STREETS WITHOUT PRIOR WRITTEN APPROVAL BY THE CITY.
10. ALL SIDEWALK AND PAVEMENT TO REMAIN SHALL BE PROTECTED IN PLACE INCLUDING PROTECTION FROM DAMAGE CAUSED BY REMOVAL OF ABUTTING PAVEMENT. CONTRACTOR SHALL SAW CUT WHERE NECESSARY.
11. CONTRACTOR SHALL GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DISCONNECTION, DEMOLITION, AND REMOVAL OF SERVICE LINES. CAP ALL LINES BEFORE PROCEEDING WITH WORK ON THIS CONTRACT.
12. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANIES WORK FORCE AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES.
13. CONTRACTOR SHALL PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC. TO THE BEST PRACTICES AND AS APPROVED BY THE ENGINEER AND THE CITY.
14. DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
15. DEMOLITION OF BUILDINGS SHALL INCLUDE THE BUILDING STRUCTURE, PAD, FOOTINGS, FOUNDATIONS, BASEMENT WALLS, BASEMENT FLOORS, TRUCK DOCKS, STEPS, DECKS, ALL ITEMS REMAINING IN BUILDING, ALL BUILDING UTILITY SERVICES, SIDEWALKS, AND BACKFILLING AND RESTORING REMAINING EXCAVATIONS, BASEMENTS AND TRENCHES PER SPECIFICATIONS. ALL REMOVALS SHALL INCLUDE BUT NOT BE LIMITED TO ITEMS SUCH AS BUILDING FOOTINGS, SUBSURFACE STRUCTURES, SURFACE STRUCTURES, DRAINAGE STRUCTURES, PIPES, PAVEMENT OF ALL TYPES, CURB & GUTTER, SIDEWALK, ROCK, ABANDONED UTILITIES AS DIRECTED BY THE ENGINEERS, TREES, TREE ROOTS, ANY UNKNOWN MATERIALS, AND UNSUITABLE SUBGRADE MATERIALS. THE SITE SHOULD BE CLEARED FOR THE PROPOSED IMPROVEMENTS PER CITY REQUIREMENTS.
16. ALL LIGHT POLE DEMOLITION SHALL INCLUDE FIXTURES, BASES AND WIRING.
17. ALL UTILITY DEMOLITION SHALL INCLUDE METERS, MANHOLES AND OTHER STRUCTURES ASSOCIATED WITH THE UTILITY SERVICE LINE.

[illegible]



ALL OF LOT 1, RICE ACRES, A SUBDIVISION IN THE CITY OF LEE'S SUMMIT, JACKSONCOUNTY,MISSOURI, TOGETHER WITH ALL THAT PART OF AN UNPLATTED TRAC T OF LAND, ALL LYING IN THE NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 48 NORTH, RANGE 31 WEST, DESCRIBED BY TIMOTHY BLAIR WISWELL, MO-PLS 2009000067, AS FOLLOWS:

COMMENCING AT THE SOUTHEAST CORNER OF THE NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 48 NORTH, RANGE 31 WEST; THENCE NORTH 88 DEGREES 28 MINUTES 52 SECONDS WEST, ON THE SOUTH LINE OF SAID NORTHEAST QUARTER, A DISTANCE OF 755.18 FEET TO A POINT ON THE SOUTHERLY EXTENSION OF THE WEST LINE OF LOT 1, RICE ACRES, A SUBDIVISION IN THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI; THENCE NORTH 01 DEGREE 23 MINUTES 04 SECONDS EAST, DEPARTING SAID SOUTH LINE, ON SAID SOUTHERLY EXTENSION, A DISTANCE OF 55.66 FEET TO THE SOUTHWEST CORNER OF SAID LOT 1, THE POINT OF BEGINNING; THENCE NORTH 01 DEGREE 23 MINUTES 04 SECONDS EAST, ON SAID WEST LINE, A DISTANCE OF 436.21 FEET TO THE NORTHWEST CORNER OF SAID LOT 1; THENCE SOUTH 88 DEGREES 38 MINUTES 41 SECONDS EAST, ON THE NORTH LINE OF SAID LOT 1 AND ON SAID SOUTHERLY EXTENSION, A DISTANCE OF 400.00 FEET TO A POINT ON THE SOUTHERLY EXTENSION OF THE WEST LINE OF SECTION 29, DEPARTING SAID EASTERLY EXTENSION, A DISTANCE OF 436.21 FEET TO A POINT ON THE EASTERLY EXTENSION OF THE SOUTH LINE OF SAID LOT 1; THENCE NORTH 88 DEGREES 38 MINUTES 41 SECONDS WEST, ON SAID EASTERLY EXTENSION AND ON SAID SOUTH LINE, A DISTANCE OF 400.00 FEET TO THE POINT OF BEGINNING, CONTAINING 174,485 SQUARE FEET OR 4.0056 ACRES, MORE OR LESS.

ADA	4 (PER CITY TABLE)	8
TOTAL	136	165 (INCLUDING ADA)

1. CONCRETE COMMERCIAL ENTRANCE PER CITY DETAIL
2. CONSTRUCT ADA RAMP
3. CONSTRUCT CONCRETE APRON
4. PROPOSED FULL DEPTH SAW CUT
5. PROPOSED CONCRETE TYPE CG-1 CURB
6. PROPOSED CONCRETE SIDEWALK
7. RELOCATE ELECTRICAL BOX COORDINATE WITH EVERY PRIOR TO PLACEMENT OF PAD FOR FINAL ALIGNMENT.
8. THERMOPLASTIC LEFT TURN ARROW PER CITY STANDARDS (60' SPACING)
9. PROPOSED CONCRETE DRIVE
10. CONTRACTOR TO RELOCATE LIGHT POLE 5.3' TO THE NORTH. COORDINATE WITH CITY OF LS.
11. EXISTING FIRE HYDRANT.
12. RECONSTRUCT ASPHALT PAVEMENT IN EFFECTED AREA. CONTRACTOR TO VERIFY PAVEMENT CONDITION AND REPLACE AS NECESSARY
13. INSTALL DOWELED CURB PER CITY STANDARDS
14. PROPOSED HEAVY DUTY ASPHALT
15. 4" PAINTED WHITE LINE PER CITY STANDARDS
16. CONVERT EXISTING CURB INLET TO CURB GRADE INLET PER DETAIL
17. INSTALL R4-7 24"x30" - KEEP RIGHT SIGN PER CITY STANDARDS

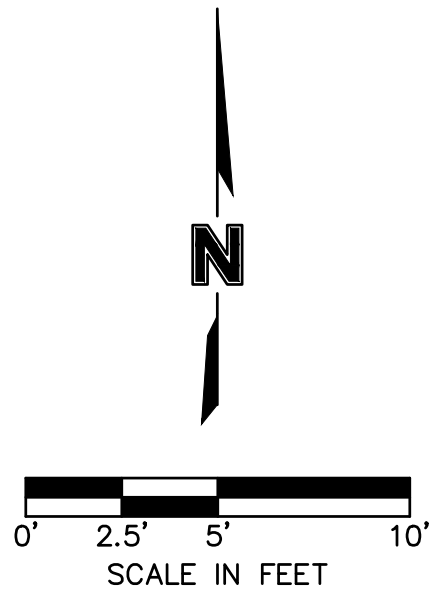
TO DATE, SUBJECT PROPERTY CONTAINS NO VISIBLE EVIDENCE OF ABANDONED OIL OR GAS WELLS LOCATED WITHIN THE PROPERTY BOUNDARY IDENTIFIED IN "ENVIRONMENTAL IMPACT STUDY OF ABANDONED OIL AND GAS WELLS IN LEE'S SUMMIT, MISSOURI," BY EDWARD ALTON MAY, JR., DATED 1995.

THERE ARE NO FEMA DELINATED FLOODPLAINS ON THE PROPERTY.



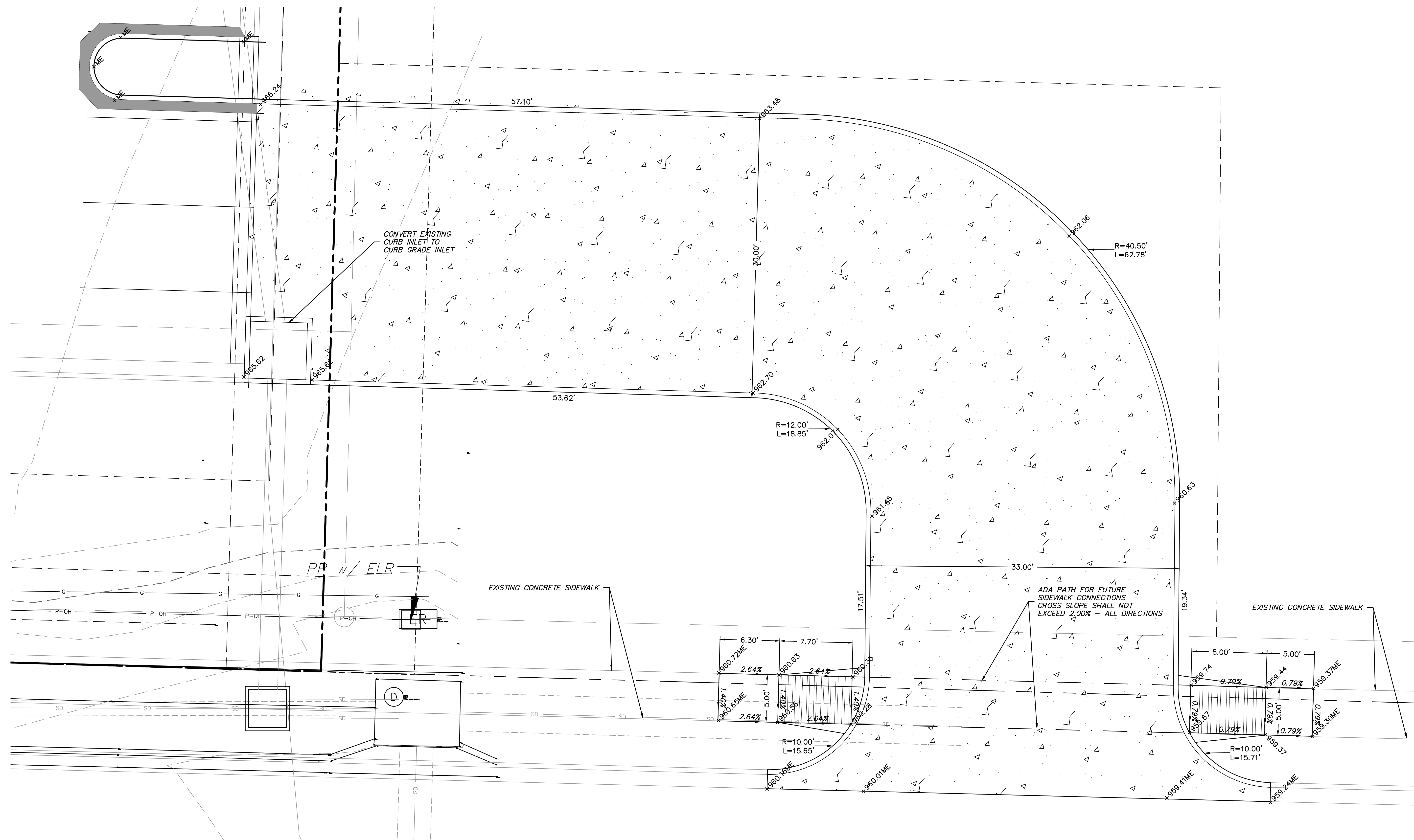


NOTE:
CONTRACTOR TO FIELD VERIFY ELEVATION
PRIOR TO CONSTRUCTION. NOTIFY THE
ENGINEER OF DISCREPANCIES.



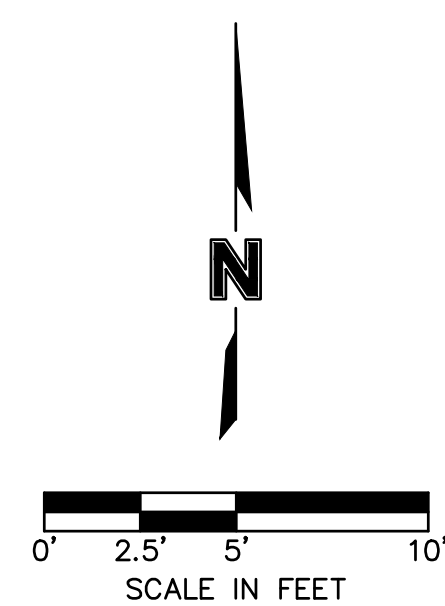
DRAWN BY: JTF		CHECKED BY: TP		APPROVED BY: TP		QA/QC BY: BS		PROJECT NO.: 018-0330 WP10		DRAWING NO. C_GRD_0180330.dwg		DATE: 11/01/2024	
GRADING DETAILS FINAL DEVELOPMENT PLAN												2024	
COLBURN ROAD IMPROVEMENTS MCPL - COLBURN ROAD													
LEE'S SUMMIT, MISSOURI													
olsson												7301 West 133rd Street, Suite 200 Overland Park, KS 66213-4750 TEL 913.381.1170 www.olsson.com	
STATE OF MISSOURI TERRY M. PARSONS NUMBER PE-2018010505 12/14/2024 PROFESSIONAL ENGINEER													
BY													
REV. NO.													
DATE													
REVISIONS DESCRIPTION													
REVISED PER CITY COMMENTS													
RLK													

DWG: F:\2018\0001-0500\018-0330-B\40-Design\AutoCAD\Final Plans\Sheets\GNCV\WORK PACKAGE 10\Revisions\C_GRD_0180330.dwg USER: tparsons
XREFS: C_PTBK001_018330 C_XBASE_018330 BASE 22-0133 BASEGN01 xSignal - 2686132006 NEW 2018 AERIAL IMAGE C_PSURF_018330entrance relocation C_PBASE02_018330
DATE: Dec 10, 2024 9:54pm



NOTE:

CONTRACTOR TO FIELD VERIFY ELEVATION PRIOR TO CONSTRUCTION. NOTIFY THE ENGINEER OF DISCREPANCIES.



GRADING DETAILS
FINAL DEVELOPMENT PLAN

LEE'S SUMMIT, MISSOURI

SHEET
C3.1

drawn by: JTF
checked by: TP
approved by: TP
QA/QC by: BS
project no.: 018-0330 WP10
drawing no. C GRD 0180330.dwg
date: 11/01/2024

[illegible]

olsson

7301 West 133rd Street, Suite 200
Overland Park, KS 66213-4750
TEL 913.381.1170 www.olsson.com



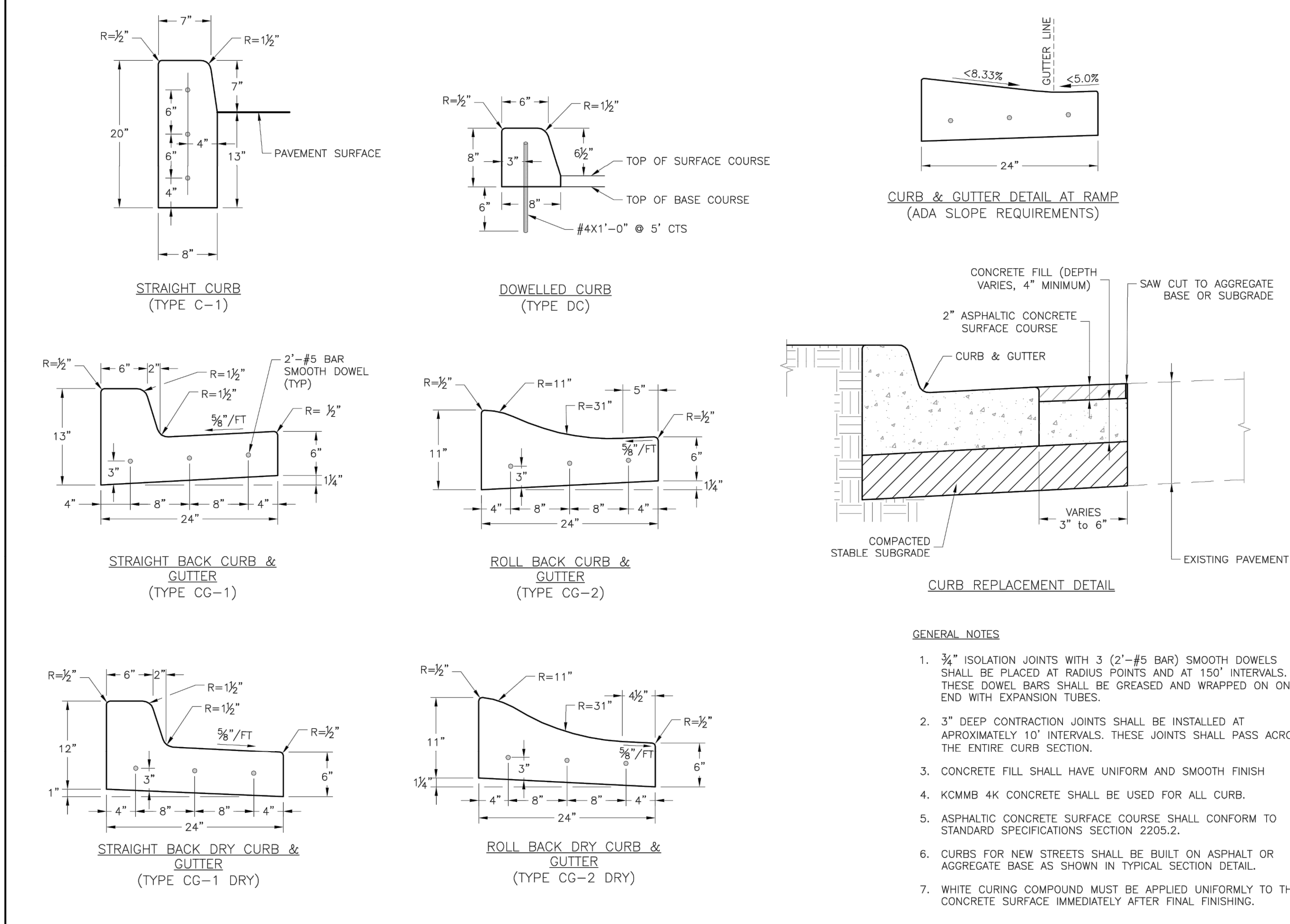
1. SUBGRADE MUST BE OF STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
2. 1.5% CROSS SLOPE MUST BE MAINTAINED THROUGH DRIVEWAYS.
3. KOMMB 4K CONCRETE MIX SHALL BE REQUIRED FOR ALL SIDEWALKS OR AS APPROVED BY THE CITY INSPECTOR.
4. ALL SIDEWALKS SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG)
5. AN EXPANSION JOINT SHALL BE PLACED AT A MAXIMUM OF 150 FT. CONSTRUCTION JOINTS SHALL BE PLACED THE SAME WITHIN SIDEWALKS OF NO GREATER THAN 10 FT.
6. SIDEWALK FINISHING (NO PICTURE FRAMING) AS DIRECTED BY CITY INSPECTOR.
7. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.



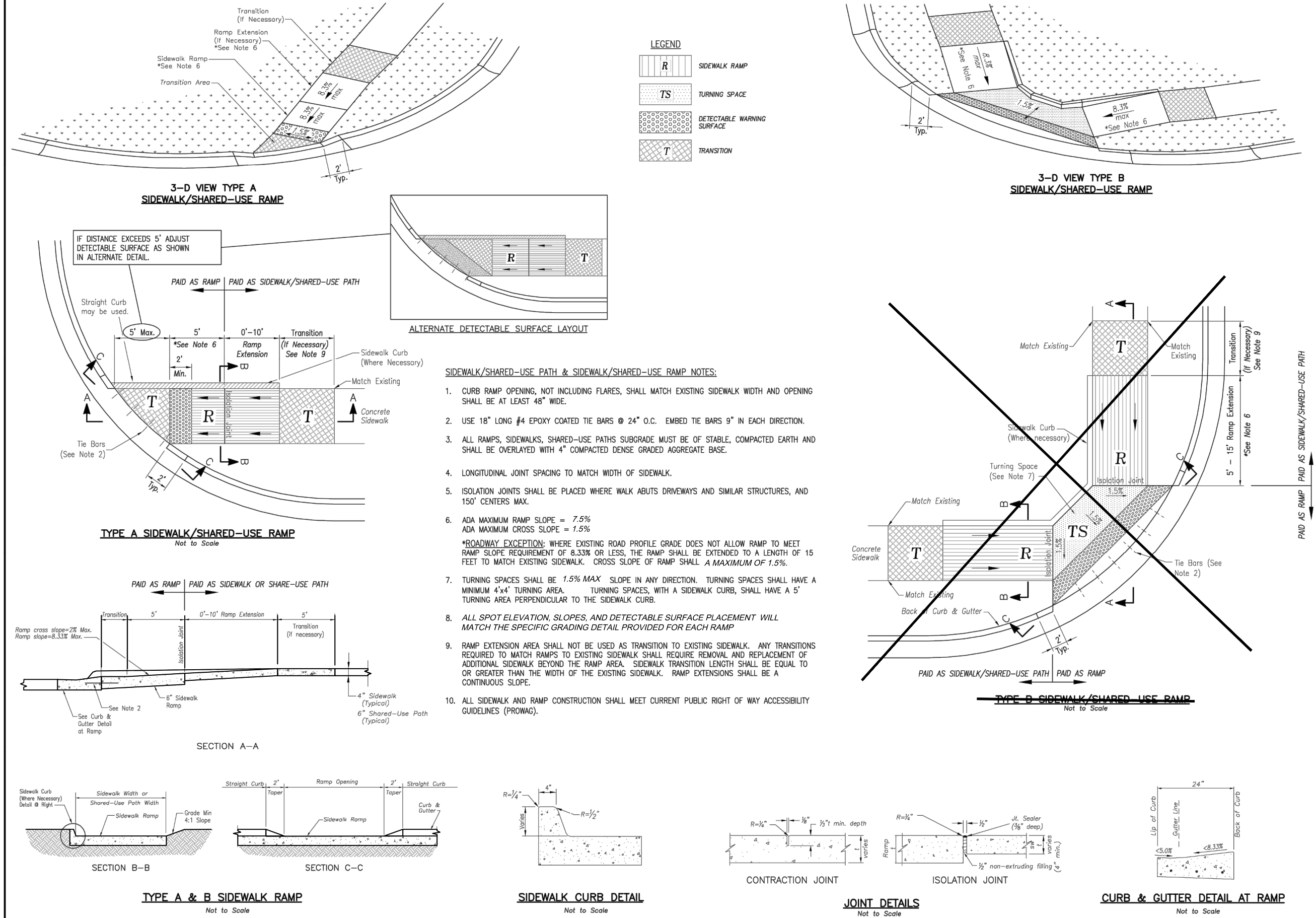
- NOTES:**
1. REINFORCING STEEL SHALL BE NEW BILLET, MINIMUM GRADE 60 AS PER ASTM A615M, AND SHALL BE BENT COLD.
 2. ALL DIMENSIONS RELATIVE TO REINFORCING STEEL ARE TO CENTERLINE OF BARS. 2" CLEARANCE SHALL BE PROVIDED THROUGHOUT UNLESS NOTED OTHERWISE. TOLERANCE OF $\pm 1/8$ " SHALL BE PERMITTED.
 3. ALL LAP SPICES NOT SHOWN SHALL BE A MINIMUM OF 40 BAR DIAMETERS IN LENGTH.
 4. ALL REINFORCING STEEL SHALL BE SUPPORTED ON FABRICATED STEEL BAR SUPPORTS @ 3'-0" MAXIMUM SPACING.
 5. FLOOR OF INLET SHALL BE SHAPED WITH INVERT TO PROVIDE SMOOTH FLOW.
 6. BEVEL ALL EXPOSED EDGES WITH 3/4" CHAMFER OR 1/2" TOOLED EDGE.

[illegible]

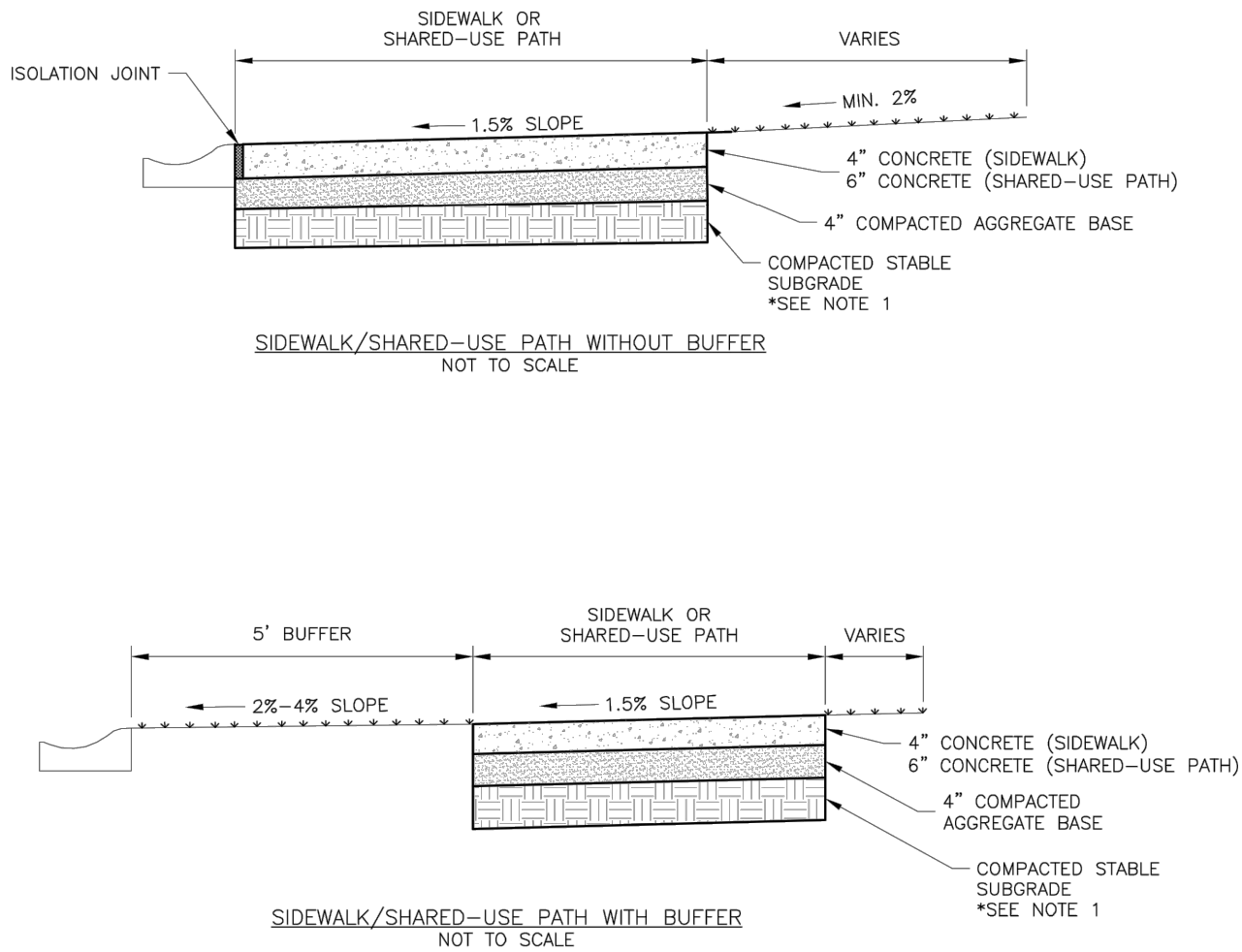
STANDARD DETAILS FINAL DEVELOPMENT PLAN		COLBERN ROAD IMPROVEMENTS MCPL -COLBERN ROAD		2024
drawn by: _____ JTF checked by: _____ TP approved by: _____ TP O&M by: _____ BS project no.: 018-0330 WP10 drawing no.: C DTL01 80330.dwg date: 11/01/2024		LEE'S SUMMIT, MISSOURI		
SHEET C4.0				



STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
CURB & GUTTER DETAIL
GEN-4

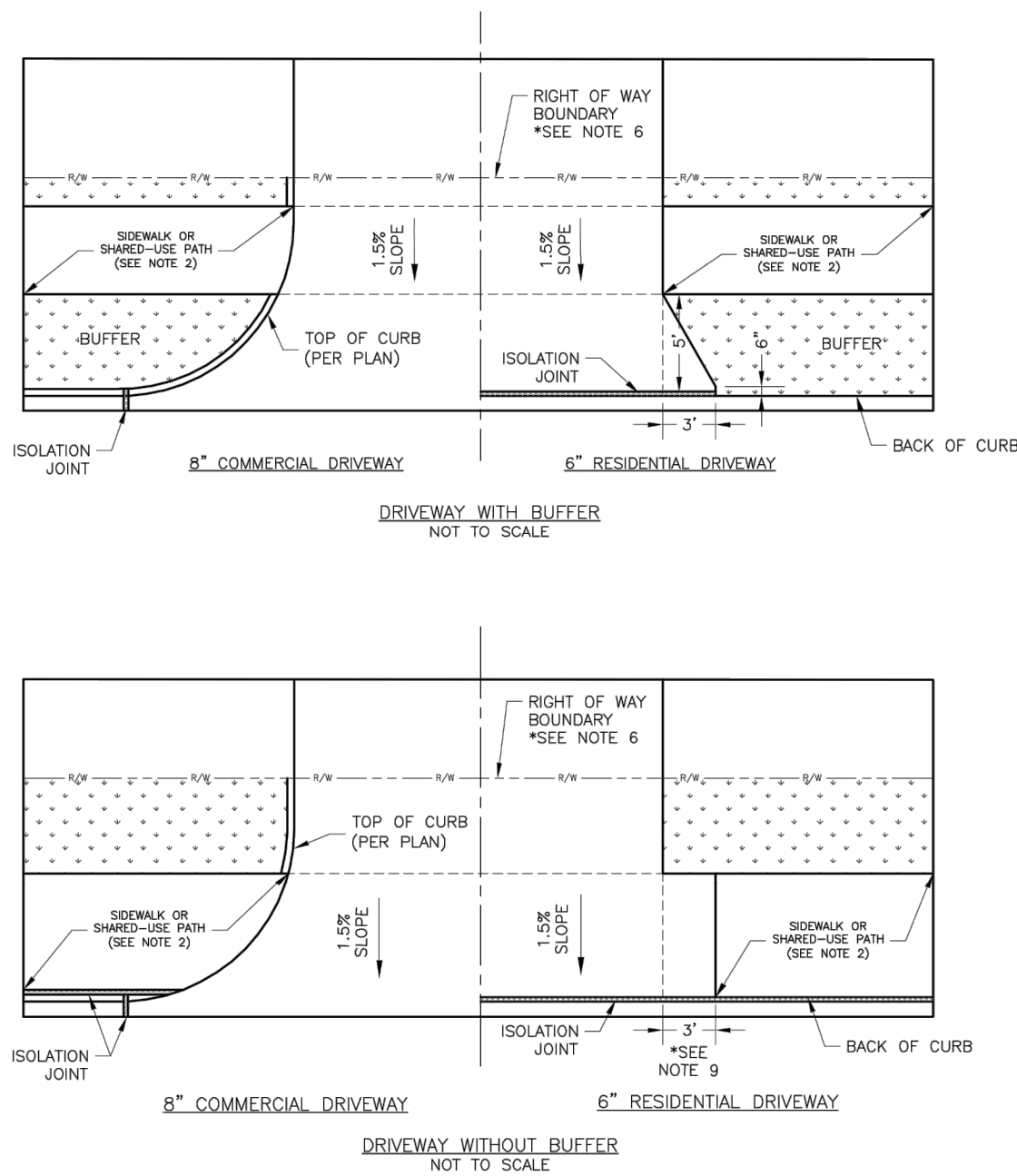


STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
ADA RAMP DETAIL
GEN-3A

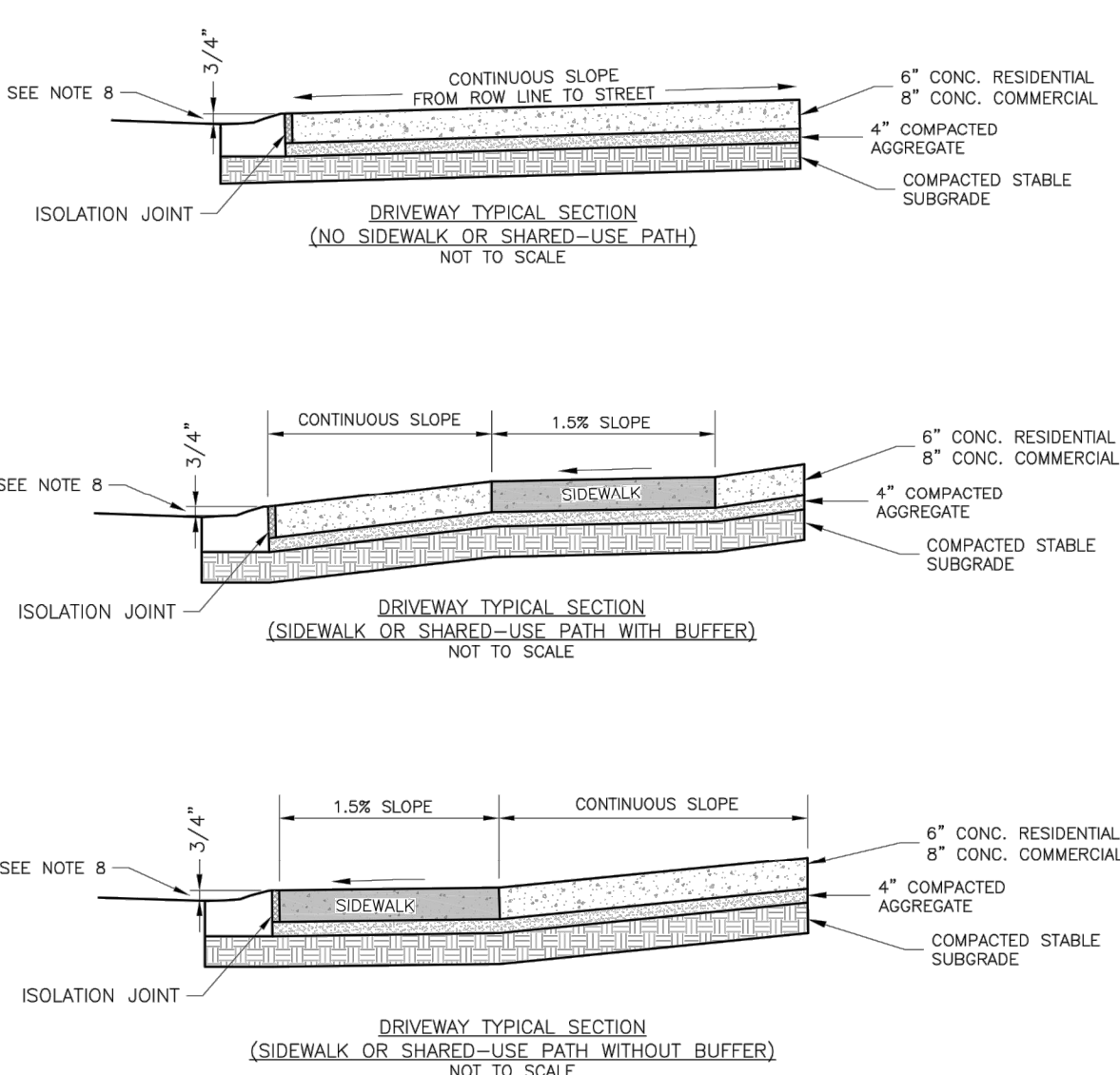


**LEE'S SUMMIT
MISSOURI**
PUBLIC WORKS ENGINEERING DIVISION | 1200 SE GREEN STREET | LEE'S SUMMIT, MO 64063

DATE: 04/17
DRAWN BY: MJP
CHECKED BY: DE
GEN-2



- GENERAL NOTES**
- SUBGRADE SHALL BE STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
 - ALL DRIVE APPROACHES SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG) FOR SLOPE REQUIREMENTS WHEN SIDEWALK IS REQUIRED (SEE ADA RAMP RETROFIT DETAIL).
 - JOINT AT BACK OF CURB LINE SHALL BE AN ISOLATION JOINT FOR RESIDENTIAL DRIVEWAYS.
 - KCMBB 4K CONCRETE MIX IS REQUIRED FOR ALL CURBS.
 - COMMERCIAL DRIVEWAYS AND DRIVEWAY APPROACHES, IN THE PUBLIC RIGHT OF WAY, SHALL BE KCMBB 4K CONCRETE MIX.
 - A JOINT MUST BE INSTALLED AT THE RIGHT OF WAY BOUNDARY FOR PROPERTY DELINEATION.
 - WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.
 - 3/4" FROM TOP OF CURB TO FLOWLINE AT DRIVEWAY (TYPE CG-1 CURB ONLY). MUST MAINTAIN ORIGINAL FLOWLINE OF CURB.
 - SIDEWALK ADJOINING CURB SHALL BE 4" THICK, EXTENDING 3' FROM THE DRIVEWAY.
 - THE MAXIMUM WIDTH OF A RESIDENTIAL DRIVEWAY IS 36 FEET WITHIN THE RIGHT OF WAY.



STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
DRIVEWAY DETAIL
GEN-1

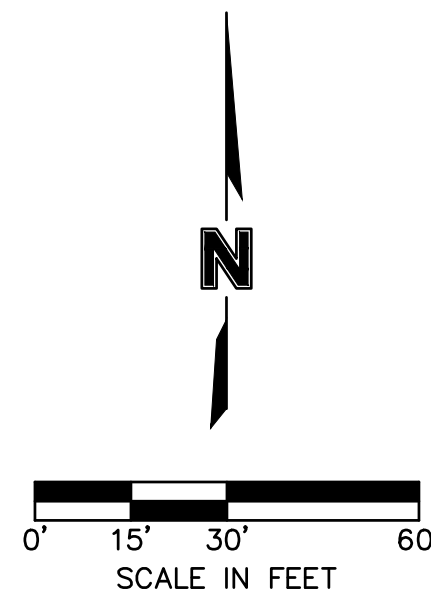
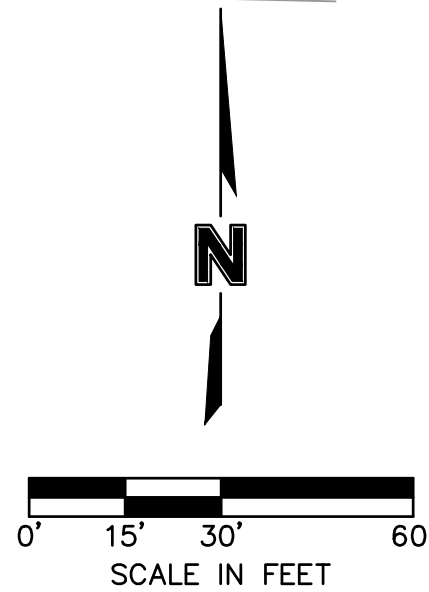
STANDARD DETAILS
FINAL DEVELOPMENT PLAN
COLBORN ROAD IMPROVEMENTS
MCPL - COLBORN ROAD
LEE'S SUMMIT, MISSOURI
SHEET
C4.1

REV. NO.	DATE	REVISIONS DESCRIPTION
1	11.14.2024	REVISED PER CITY COMMENTS



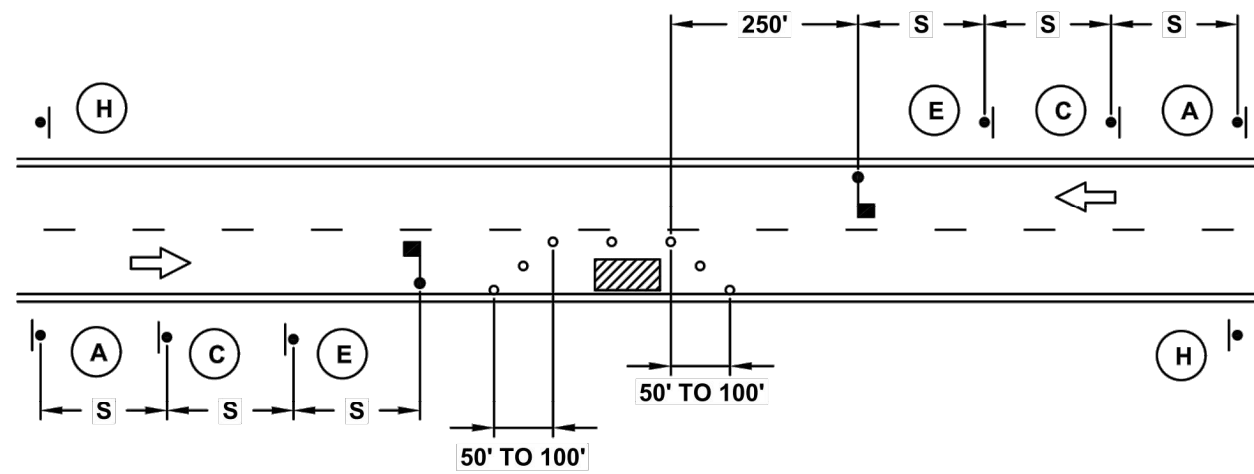
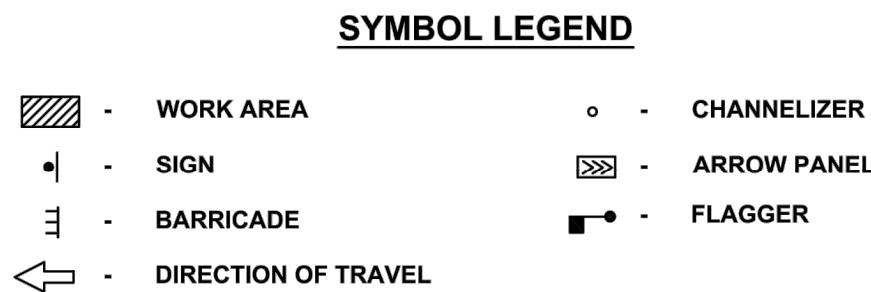
BY
RLK

olsson
7301 West 133rd Street, Suite 200
Overland Park, KS 66213-4750
TEL 913.381.1170
www.olson.com

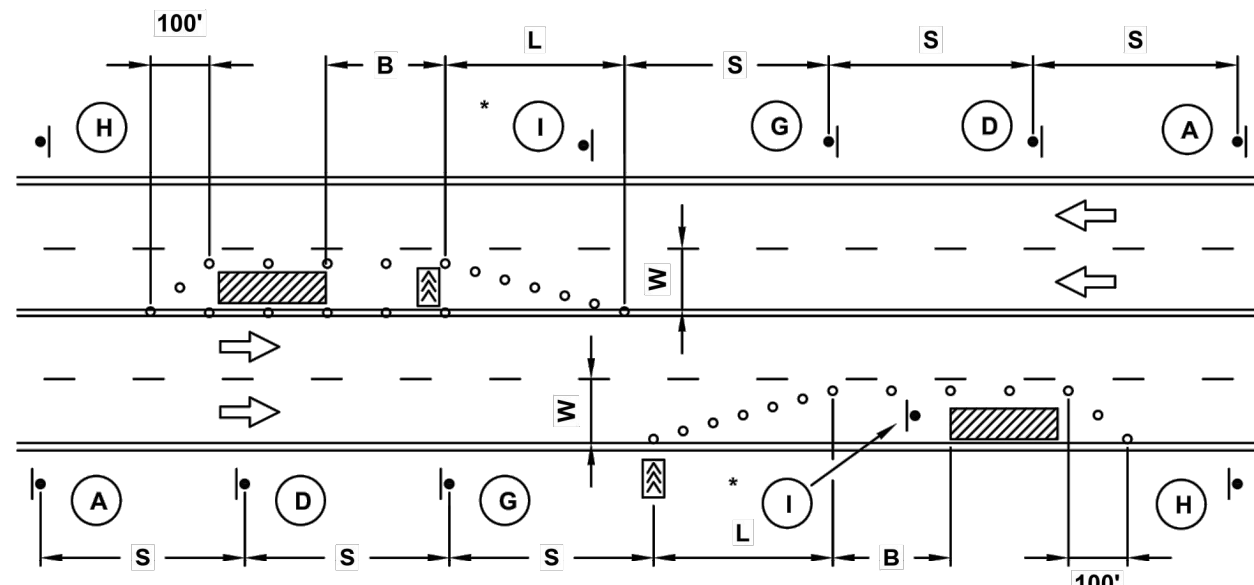


GENERAL NOTES:

1. All signs, barricades, drums, markings and other traffic control devices shall conform to the Manual on Uniform Traffic Control Devices (M.U.T.C.D.), most current edition.
2. All traffic control devices shall be standard in size, shape, color and message, in good condition, and reflectorized. All signs and barricades in place at night shall be reflectorized with high intensity sheeting. All signs shall be securely mounted with height and lateral location as described in the M.U.T.C.D.
3. Warning lights should be used on all channelization devices and warning signs per M.U.T.C.D. Type A Low-Intensity Flashing Warning lights should be mounted on warning signs as well as barricades where used singly to warn users of potentially hazardous areas during nighttime hours. Type B High-Intensity Flashing warning lights should be used during day and nighttime operations to increase awareness of potentially hazardous areas. Type C Steady-Burn lights should be mounted on barricades where used in series to delineate the edge of the travel way.
4. Advance Warning Arrow Panels shall be used for all lane closures on multi-lane streets but should not be used in lieu of proper traffic control signs, barricades and channelization devices.
5. Flaggers shall be used where indicated on the plans, where construction vehicles interact with normal traffic or where construction activities are conducted in the street. The Engineer in charge of construction, Where flaggers are used, advance signing shall be erected as shown above or as specified in the M.U.T.C.D. Flaggers shall meet the requirements in the M.U.T.C.D. in regard to character, training, attire and behavior.
6. Channelization devices used in the street shall be either drums, cones or Triflume channelizers.
7. Traffic control devices not in use shall be either covered or removed from the work area.
8. The Contractor shall place as many barricades as needed to effectively shield pedestrians and traffic from exposed objects, excavations and construction activities. ADA Pedestrian Barricades shall be installed around all excavations left open during non-construction times or as directed by the Engineer in charge of construction.
9. Access shall be maintained to all driveways and side streets unless noted otherwise on the plans.
10. Construction materials shall be kept off of sidewalks, consolidated in one location within City Right-Of-Way, and removed daily unless otherwise approved by the Engineer in charge of construction.
11. Dirt, mud and other construction debris on streets and sidewalks shall be removed immediately.
12. The Contractor shall not perform any work that will restrict traffic in any way between the hours of 7:00 a.m. and 8:30 a.m. or 4:30 p.m. and 6:00 p.m. unless prior approval has been given.
13. The Contractor shall be responsible for maintaining all traffic control devices on an around-the-clock basis, whether or not work is actively being pursued and any deficiencies noted shall be corrected immediately.
14. The traffic control requirements shown on these plans are minimum requirements only and do not attempt to address in depth the variety of situations that may occur once construction has started. In no way do the requirements shown on these plans relieve the contractor of his responsibility for proper traffic control devices and implementation procedures that will assure the safety of motorists, pedestrians, and workers at all times. Any additional quantities of traffic control devices necessary to complete the contract or as ordered by the Engineer shall be considered subsidiary to the contract bid price.
15. Construction vehicles parked along streets and construction signs shall not restrict sight distance for vehicles exiting at streets or any drives.
16. Should the contractor fail to enforce the traffic control plan or fail to clean, replace or otherwise maintain the traffic control devices when directed to do so by the Engineer or representative, the City/Corporation Jurisdiction may take one or more of the following actions:
 - A) Employ another agency to correct deficiencies in signing or warning devices and deduct the cost from the contractor's pay estimate
 - B) Suspend all pay Estimates until deficiencies are corrected.
 - C) Stop the work until deficiencies are corrected.
 - D) Place the contractor in default.
17. The contractor shall keep roadway closures due to construction activities to a minimum and provide advanced notice to affected users.
18. The Contractor shall implement and emergency call system before the start of construction to request their cooperation and to provide coordination of services when emergencies arise during the construction at the project site. When the contractor completes this notification with enforcement and emergency agencies, a report shall be furnished to the engineer on the status of incident management.
19. The Contractor may, if need arises or at the request of the Engineer, place reduced speed limit signing and work zone speed limit signing in advance of all active work zone areas. Work zone speed limit reduction shall not be greater than 10 mph throughout construction areas and advance signing in place. Contractor shall cover or replace all speed limit signs in conflict with the work zone speed limit throughout the construction area.

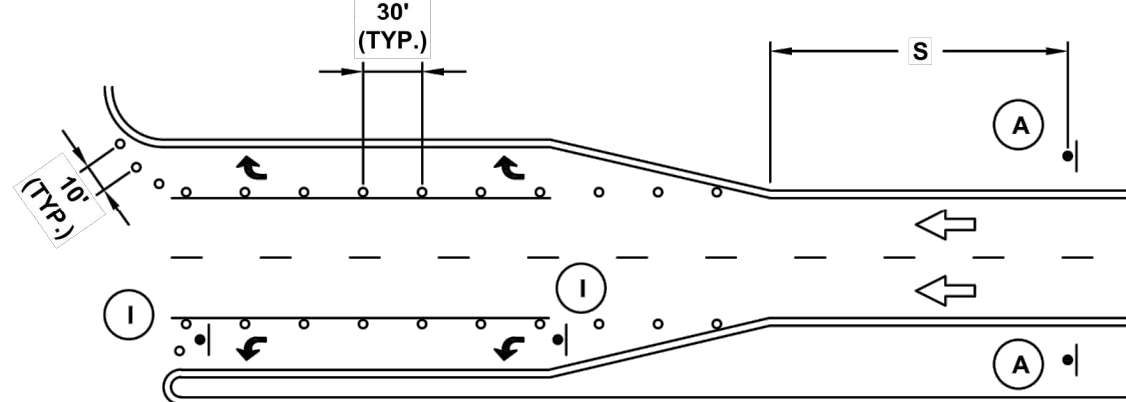


LANE CLOSURE - TWO LANE STREET

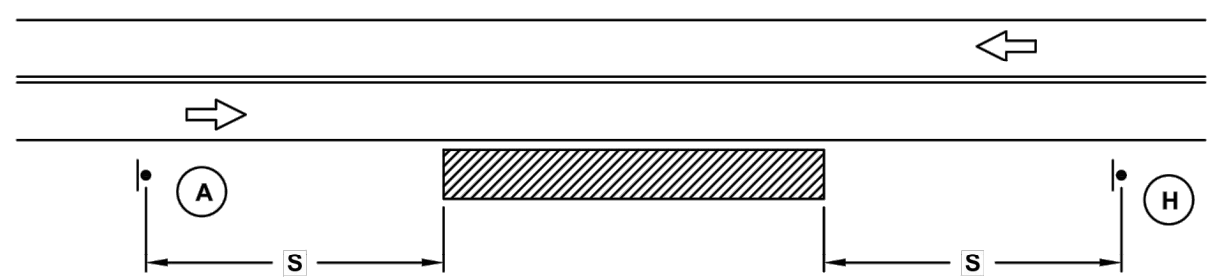


LANE CLOSURE - FOUR LANE STREET

* INSTALL SIGNS EVERY 200 FEET THROUGHOUT THE CLOSED LANE OR AS NEEDED



TURN LANE CLOSURE

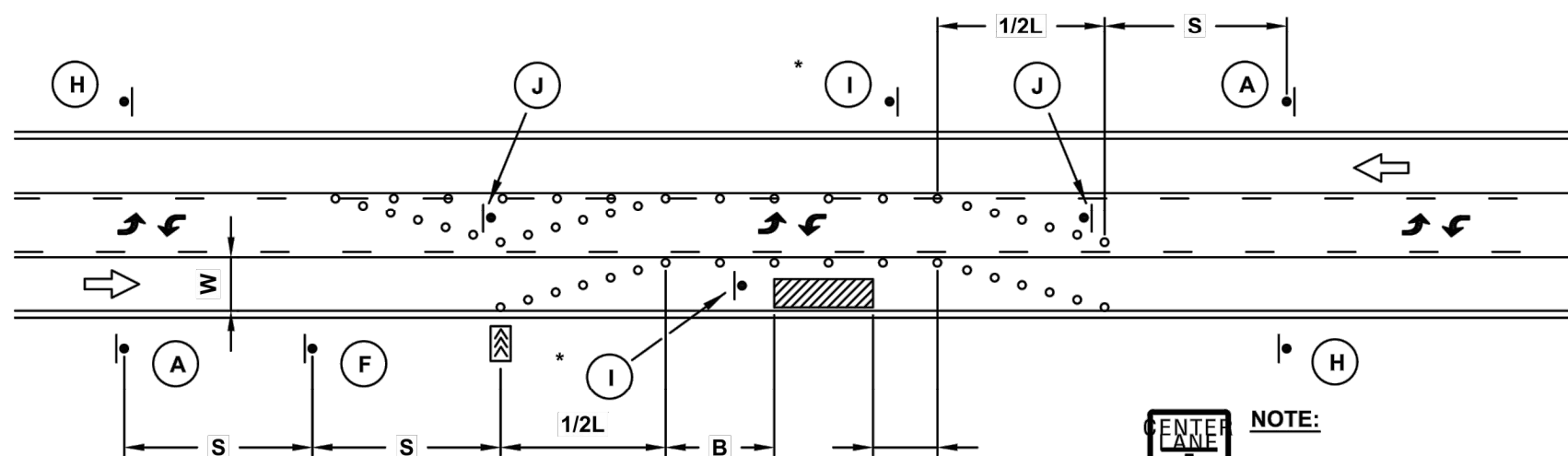
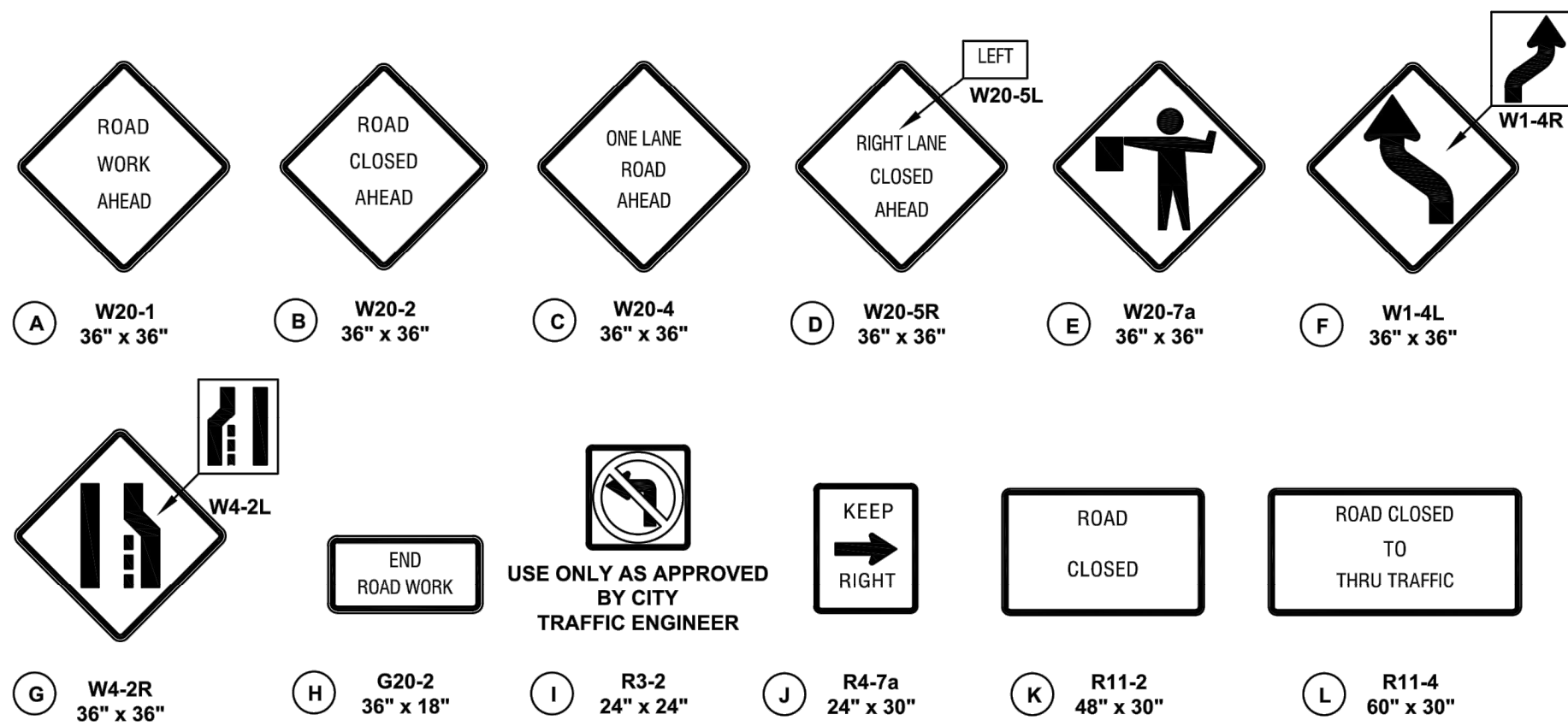


TYPICAL SIGNING FOR WORK ADJACENT TO THE STREET

GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE "B"		MAXIMUM CHANNELIZER SPACING		
SPEED LIMIT (MPH)	LENGTH (FEET)	SPEED LIMIT (MPH)	WITHIN TAPER (FEET)	OUTSIDE TAPER (FEET)
25	35	25	25	50
30	55	30	30	60
35	85	35	35	70
40	120	40	40	80
45	170	45	45	90

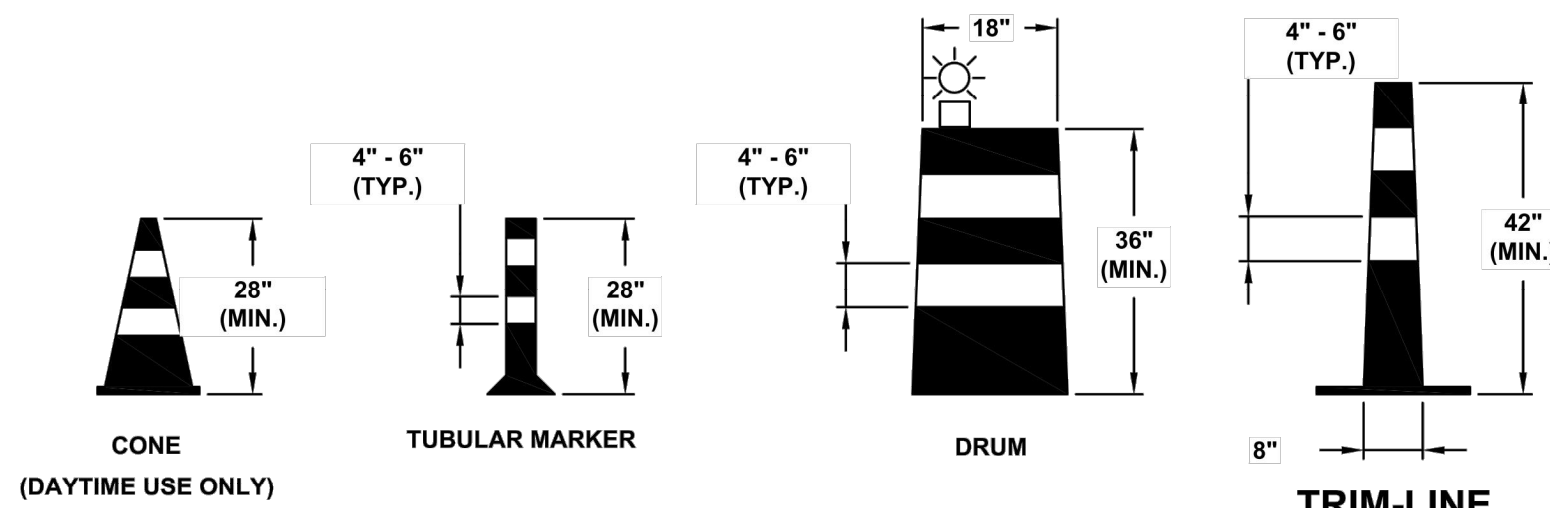
TAPER DIMENSIONS (FEET)					SIGN SPACING "S"	
SPEED LIMIT (MPH)	MINIMUM TAPER LENGTH "L", PER LANE WIDTH "W"			MINIMUM NUMBER OF CHANNELIZERS	SPEED LIMIT (MPH)	SPACING (FEET)
	10	11	12			
25	105	115	125	6	25	100
30	150	165	180	7	30-35	250
35	205	225	245	8	≥ 40	350
40	270	295	320	9		
45	450	495	540	13		

SIGN LEGEND



LANE CLOSURE - THREE LANE STREET

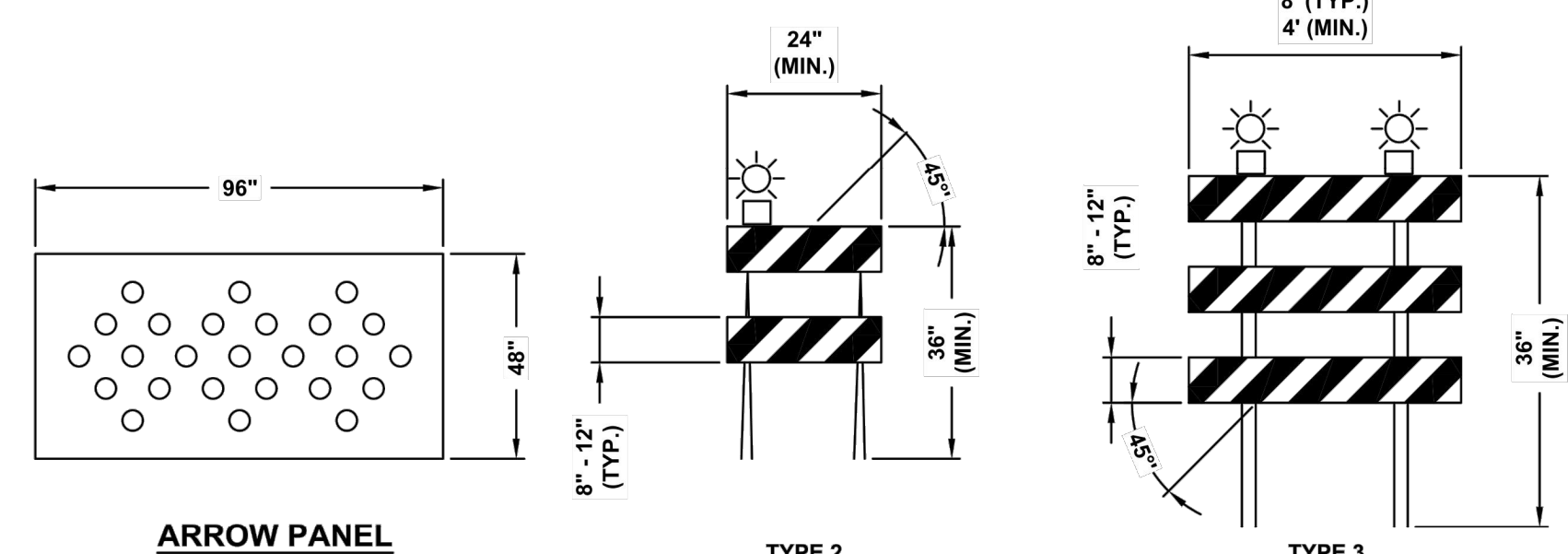
* INSTALL SIGNS EVERY 200 FEET THROUGHOUT THE CLOSED LANE OR AS NEEDED



CHANNELIZERS

TRIM-LINE

NOTE: WHITE BANDS ON BARRICADES AND CHANNELIZERS SHALL BE MADE FROM HIGH INTENSITY SHEETING MATERIAL.



ARROW PANEL

TYPE 2

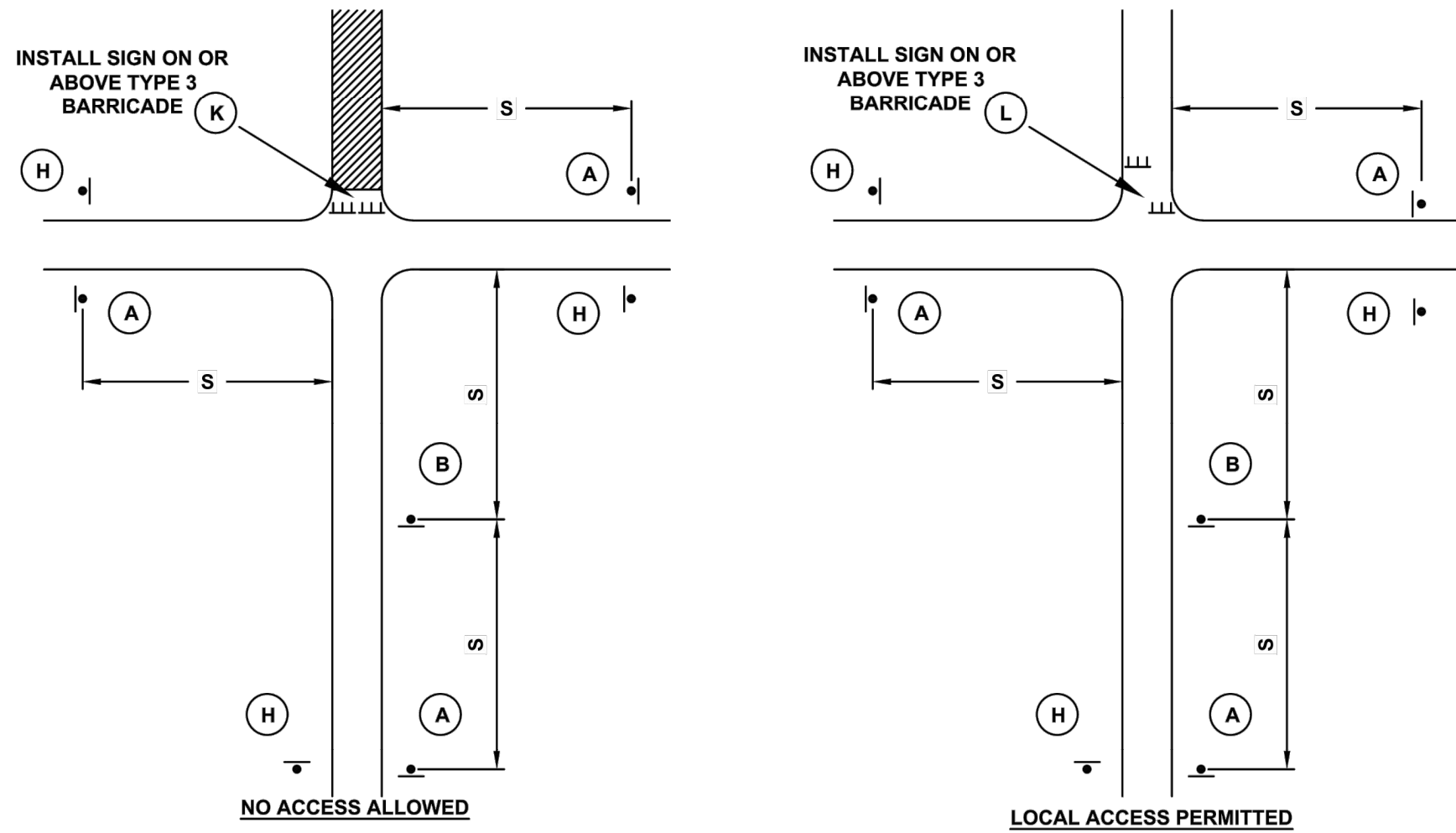
TYPE 3

BARRICADES

(OPTIONAL ON TWO LANE AND THREE LANE STREETS WITH SPEEDS LESS THAN 35 MPH)

GENERAL NOTES:

- ALL SIGNS, BARRICADES, CHANNELIZERS, MARKINGS AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE **MUTCD**.
- ALL TRAFFIC CONTROL DEVICES SHALL BE STANDARD IN SIZE, SHAPE, COLOR, AND MESSAGE, IN GOOD CONDITION, AND RETRO-REFLECTORIZED. ALL SIGNS SHALL BE SECURELY MOUNTED WITH HEIGHT AND LATERAL LOCATION AS DESCRIBED IN THE **MUTCD**.
- WARNING LIGHTS SHALL BE USED ON BARRICADES IN PLACE AT NIGHT AND ON WARNING SIGNS WHICH ALERT DRIVERS ABOUT A CHANGE IN ALIGNMENT, TRAFFIC CONTROL, LANE CLOSURE, OR ROAD CLOSURE.
- FLAGGERS SHALL BE USED WHERE INDICATED ON THE PLANS, WHERE CONSTRUCTION VEHICLES INTERACT WITH NORMAL TRAFFIC, OR WHERE CONSTRUCTION ACTIVITIES IMPOSE A RESTRICTION ON TRAFFIC, AS DIRECTED BY THE CITY TRAFFIC ENGINEER. WHERE FLAGGERS ARE USED, ADVANCE SIGNING SHALL BE ERECTED AS SHOWN IN THE DETAILS OR AS SPECIFIED IN THE **MUTCD**. FLAGGERS SHALL MEET THE REQUIREMENTS IN THE **MUTCD** IN REGARD TO CHARACTER, TRAINING, ATTIRE, AND BEHAVIOR.
- TRIM-LINES ARE THE CITY'S PREFERRED CHANNELIZING DEVICE. CONES MAY NOT BE USED AT NIGHTTIME.
- TRAFFIC CONTROL DEVICES NOT IN USE OR NOT APPLICABLE SHALL BE EITHER COVERED OR REMOVED FROM THE WORK AREA.
- THE CONTRACTOR SHALL USE BARRICADES, STREET PLATES, OR FENCING AS NEEDED TO EFFECTIVELY SHIELD PEDESTRIAN AND VEHICULAR TRAFFIC FROM EXPOSED OBJECTS, EXCAVATIONS, AND CONSTRUCTION ACTIVITIES.
- ACCESS SHALL BE MAINTAINED TO ALL DRIVEWAYS AND SIDE STREETS UNLESS NOTED OTHERWISE ON THE PLANS.
- NO STREET SHALL BE CLOSED WITHOUT THE APPROVAL OF THE CITY TRAFFIC ENGINEER. THE CONTRACTOR SHALL NOTIFY THE CITY TRAFFIC ENGINEER AT LEAST 7 DAYS IN ADVANCE OF ANY STREET CLOSURE. IF A DETOUR ROUTE AROUND THE CLOSURE IS TO BE PROVIDED, ALL DETOUR SIGNING SHALL BE AS SHOWN ON A PLAN APPROVED BY THE CITY TRAFFIC ENGINEER.
- CONSTRUCTION VEHICLES PARKED ALONG STREETS SHALL BE LOCATED WITHIN THE WORK AREA (TRAFFIC CONTROL) OR WHERE OTHERWISE NORMALLY PERMITTED. CONSTRUCTION MATERIALS, INCLUDING TRAFFIC CONTROL AND VEHICLES SHALL NOT RESTRICT SIGHT DISTANCE FOR VEHICLES EXITING AT STREETS OR DRIVES.
- CONSTRUCTION MATERIALS SHALL BE KEPT OFF OF SIDEWALKS, CONSOLIDATED IN ONE LOCATION WITHIN CITY RIGHT-OF-WAY, AND REMOVED DAILY UNLESS OTHERWISE APPROVED BY THE INSPECTOR. DIRT, MUD, AND OTHER CONSTRUCTION DEBRIS ON STREETS AND SIDEWALKS SHALL BE REMOVED IMMEDIATELY.
- THE CONTRACTOR SHALL NOT PERFORM ANY WORK THAT WILL RESTRICT VEHICULAR TRAFFIC IN ANY WAY BETWEEN THE HOURS OF 7:00 A.M. AND 9:00 A.M. OR 4:00 P.M. AND 6:00 P.M. MONDAY THROUGH FRIDAY UNLESS OTHERWISE INDICATED IN THE SPECIFICATIONS.
- ALL TRAVEL LANES SHOULD BE AT LEAST 11 FEET WIDE UNLESS OTHERWISE AUTHORIZED BY THE CITY TRAFFIC ENGINEER. A "NARROW LANES" SIGN SHALL BE INSTALLED IN ADVANCE OF A LANE WIDTH REDUCTION TO LESS THAN 11 FEET.
- ALL EDGE DROP-OFFS OF MORE THAN 2 INCHES AND LESS THAN 4 INCHES SHOULD BE PROTECTED BY A WEDGE OR BARRIER AND ALL EDGE DROP-OFFS GREATER THAN 4 INCHES SHALL HAVE EDGE PROTECTION (SEE TRAFFIC CONTROL SPECIFICATIONS FOR EDGE TREATMENT REQUIREMENTS).
- THE "WORKERS" SYMBOLIC SIGN (**MUTCD** NO. W21-1A) MAY BE USED INSTEAD OF THE "ROAD WORK AHEAD" SIGN FOR WORK WITH A DURATION OF 12 HOURS OR LESS. THE "END ROAD WORK" SIGN IS NOT REQUIRED TO BE INSTALLED AFTER THE "WORKERS" SIGN.
- NO TRAFFIC SIGNAL SHALL BE ALTERED OR MODIFIED IN ANY WAY WITHOUT A PLAN APPROVED BY THE CITY TRAFFIC ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL TRAFFIC CONTROL DEVICES ON AN AROUND-THE-CLOCK BASIS, WHETHER OR NOT WORK IS ACTIVELY BEING PURSUED AND ANY DEFICIENCIES NOTED SHALL BE CORRECTED IMMEDIATELY.
- THE TRAFFIC CONTROL REQUIREMENTS SHOWN ON THESE PLANS ARE MINIMUM REQUIREMENTS ONLY AND DO NOT ATTEMPT TO ADDRESS IN DEPTH THE VARIETY OF SITUATIONS THAT MAY OCCUR ONCE CONSTRUCTION HAS STARTED. IN NO WAY DO THE REQUIREMENTS SHOWN ON THESE PLANS RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITY FOR SELECTING THE PROPER TRAFFIC CONTROL DEVICES AND IMPLEMENTATION PROCEDURES THAT WILL ASSURE THE SAFETY OF DRIVERS, PEDESTRIANS, AND WORKERS AT ALL TIMES.
- SHOULD THE CONTRACTOR FAIL TO ENFORCE THE TRAFFIC CONTROL PLAN OR FAIL TO CLEAN, REPLACE OR OTHERWISE MAINTAIN THE TRAFFIC CONTROL DEVICES WHEN DIRECTED TO DO SO BY THE CITY TRAFFIC ENGINEER OR REPRESENTATIVE, THE CITY MAY TAKE ONE OR MORE OF THE FOLLOWING ACTIONS:
 - EMPLOY ANOTHER AGENCY TO CORRECT DEFICIENCIES IN TRAFFIC CONTROL DEVICES AND DEDUCT THE COST FROM THE CONTRACTOR'S PAY ESTIMATE.
 - STOP THE WORK UNTIL DEFICIENCIES ARE CORRECTED.
 - SUSPEND ALL PAY ESTIMATES UNTIL DEFICIENCIES ARE CORRECTED, OR
 - PLACE THE CONTRACTOR IN DEFAULT.



TYPICAL STREET CLOSURE

REV.	NO.	DATE	DESCRIPTION
1		11/14/2024	

PROJECT: STANDARD DETAILS
PUBLIC WORKS ENGINEERING
LEE'S SUMMIT, JACKSON COUNTY, MISSOURI

SHEET NAME: TRAFFIC CONTROL DETAILS

DRAWN BY: CNS
CHECKED BY: MJF
DATE: 05/13/2024
PROJECT #: ---

TRAFFIC CONTROL DETAILS
FINAL DEVELOPMENT PLAN
COLBURN ROAD IMPROVEMENTS
MCPL - COLBURN ROAD

LEE'S SUMMIT, MISSOURI

drawn by: JTF
checked by: TP
approved by: TP
QA/QC by: BS
project no.: 018-0330 WP10
drawing no.: C_DTL01_80330.dwg
date: 11/01/2024

SHEET
C5.1



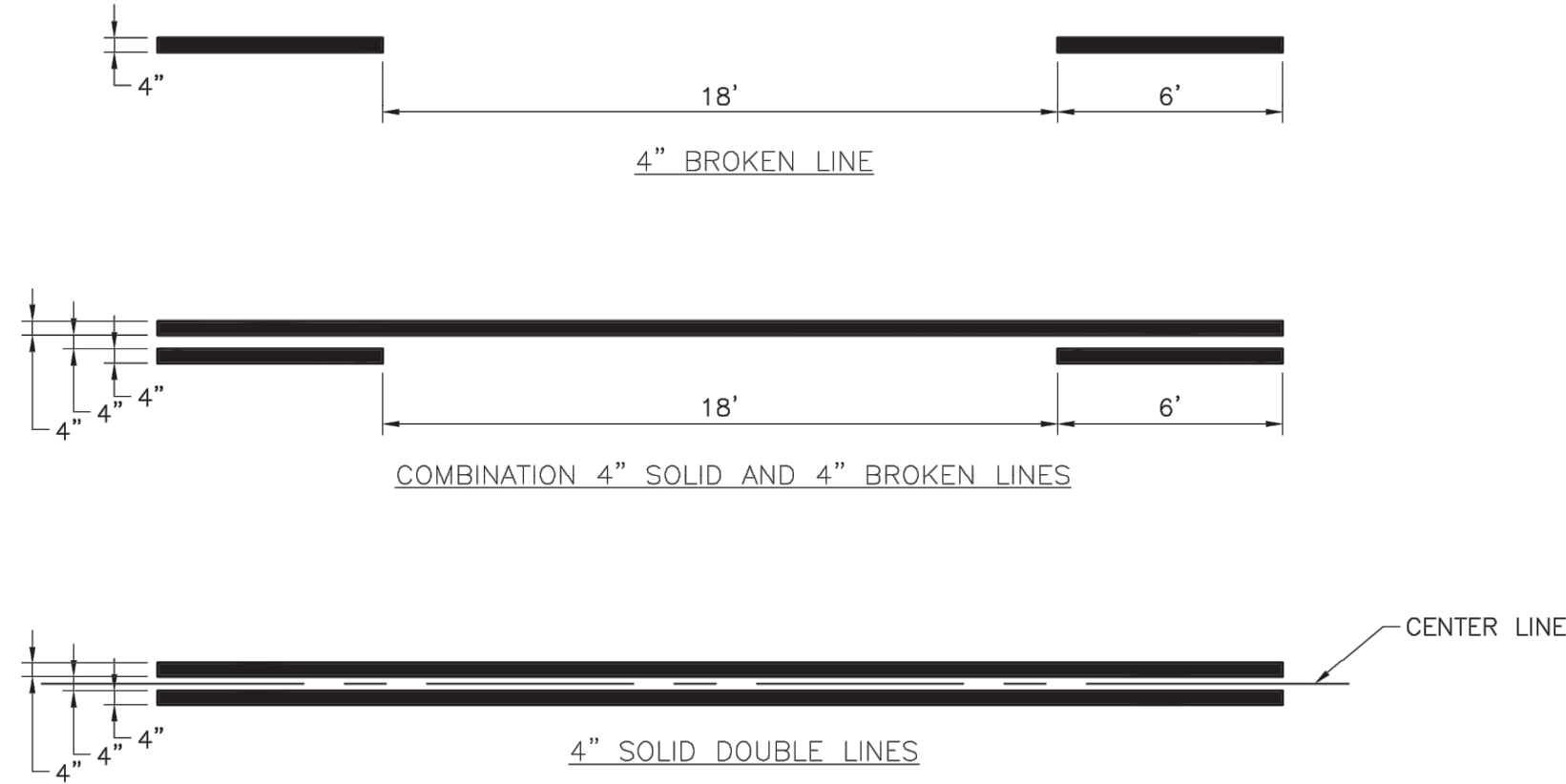
REVISIONS

2024

BY: _____
DATE: _____
REVISED PER CITY COMMENTS

LEE'S SUMMIT, MISSOURI

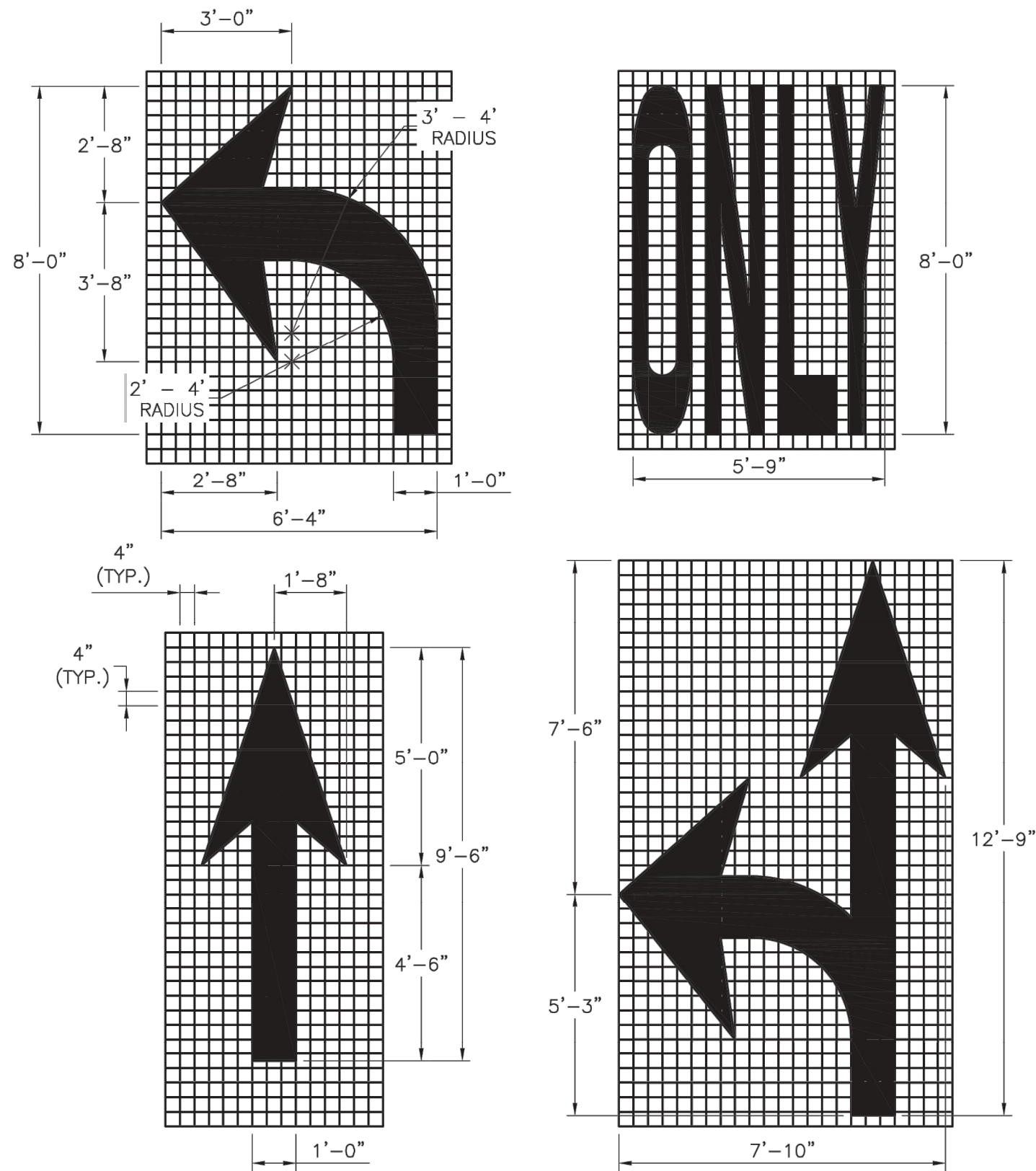
SHEET
C5.1



TYPICAL LINE DETAILS

NOTES:

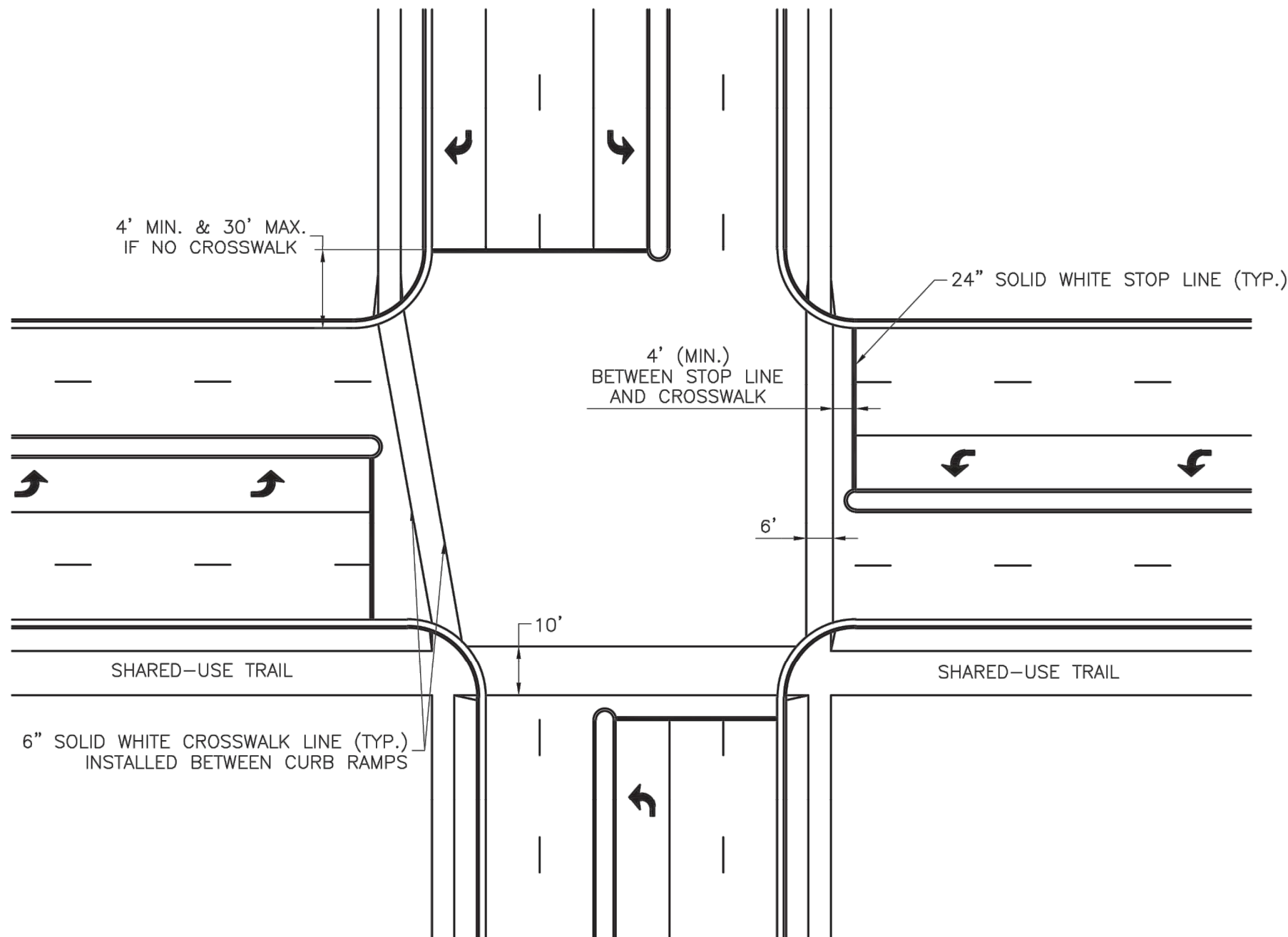
- ALL EDGE LINE, CENTER LINE, AND LANE LINE PAVEMENT MARKINGS SHALL BE 4" WIDE UNLESS OTHERWISE NOTED.
- EDGE LINES SHALL BE CONTINUOUS SOLID WHITE OR YELLOW LINES. RIGHT SIDE EDGE LINES SHALL BE SOLID WHITE. MEDIAN OR LEFT SIDE EDGE LINES ON DIVIDED ROADWAYS ARE TO BE SOLID YELLOW. EDGE LINES AND CENTER LINES SHALL BE CONTINUOUS ACROSS DRIVEWAYS.



ARROW AND SYMBOL DETAILS

NOTES:

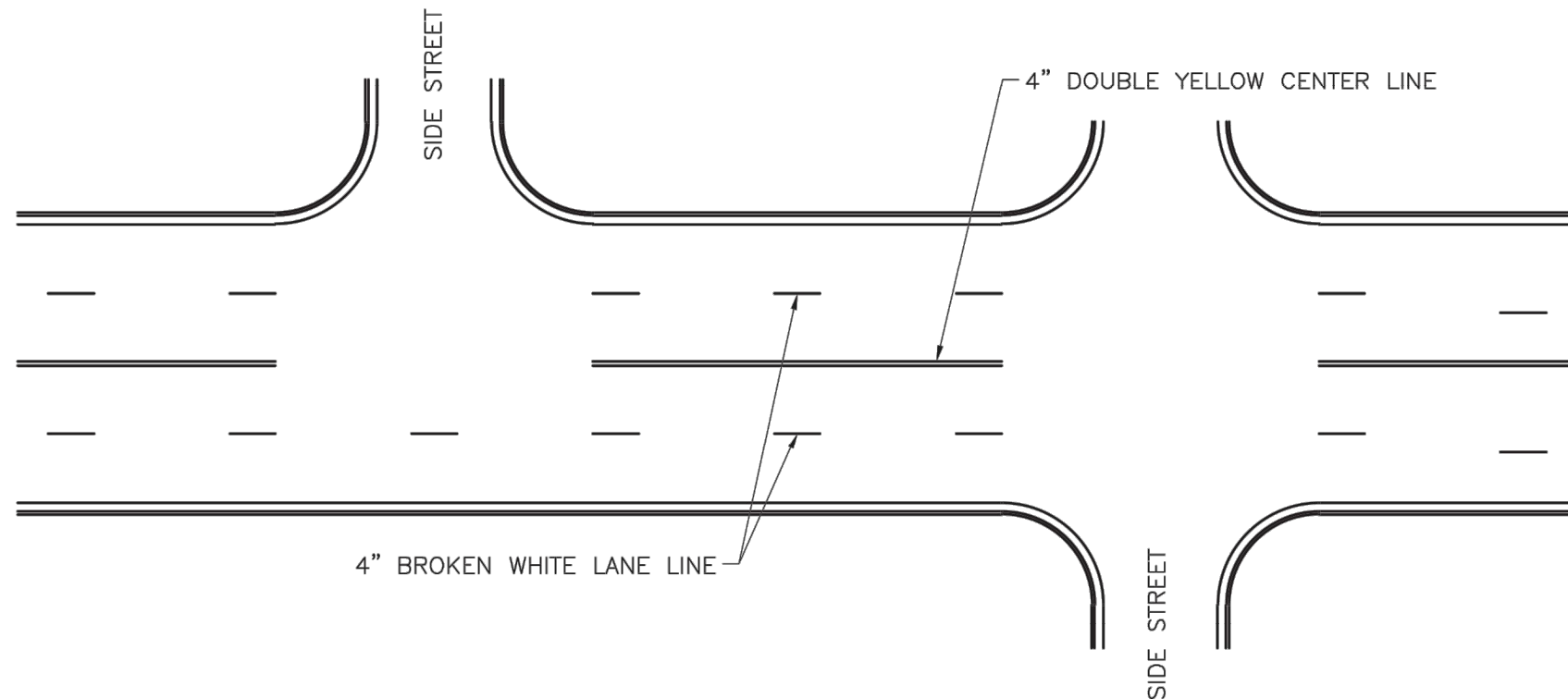
- ALL ARROW AND SYMBOL MARKINGS SHALL BE WHITE, AND SHALL BE CENTERED IN THEIR RESPECTIVE TRAFFIC LANES.
- RIGHT-TURN AND COMBINATION RIGHT-TURN/STRAIGHT ARROWS ARE REVERSE OF ARROWS SHOWN.



TYPICAL INTERSECTION MARKINGS

NOTES:

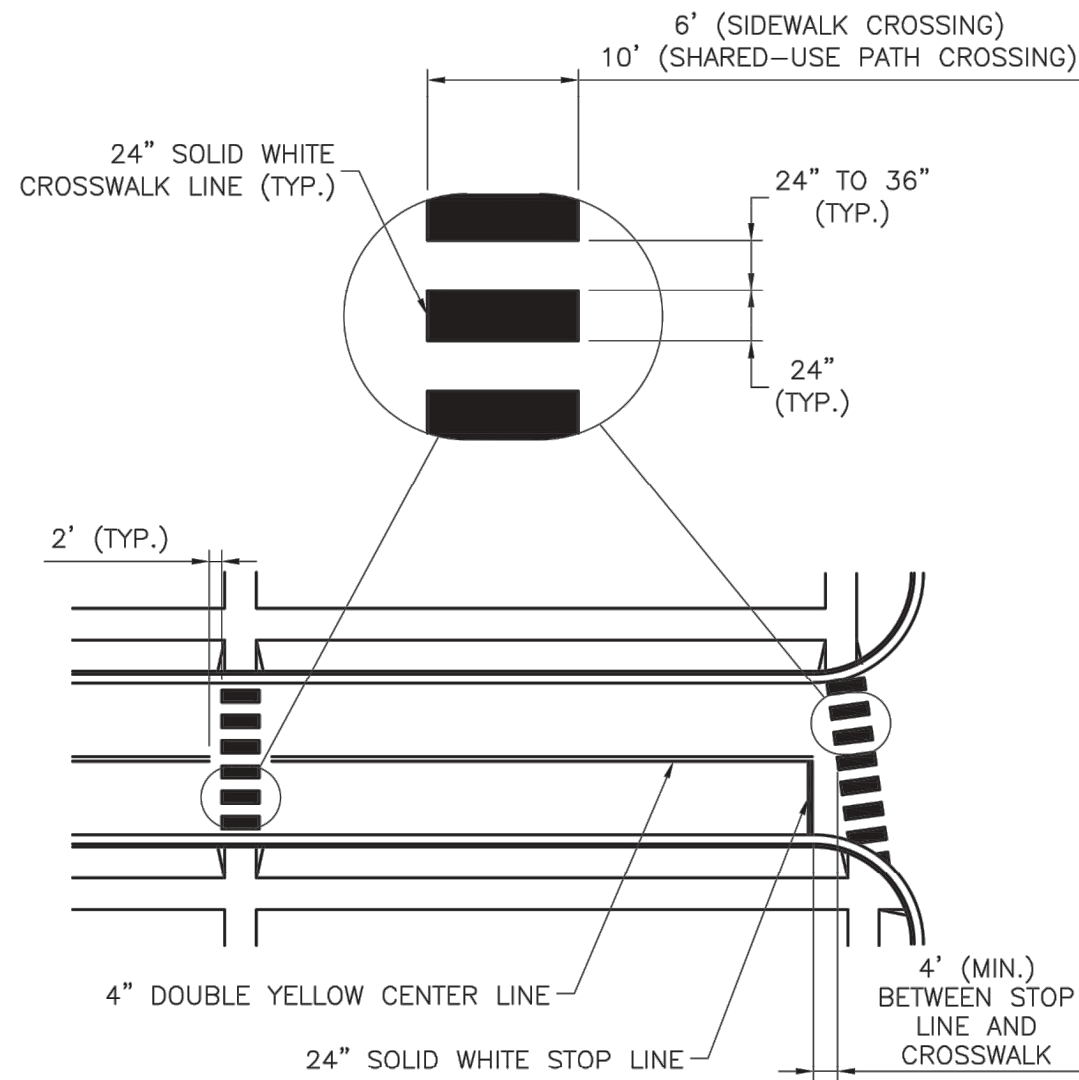
- TRANSVERSE CROSSWALK LINES SHALL BE INSTALLED SUCH THAT THE DISTANCE BETWEEN LINES IS AT LEAST 6 FEET OR 10 FEET.
- STOP LINES ARE REQUIRED AT SIGNALIZED INTERSECTIONS, ON MULTI-LANE STOP CONTROLLED APPROACHES, OR ADVANCE OF CROSSWALKS AT CONTROLLED INTERSECTIONS.



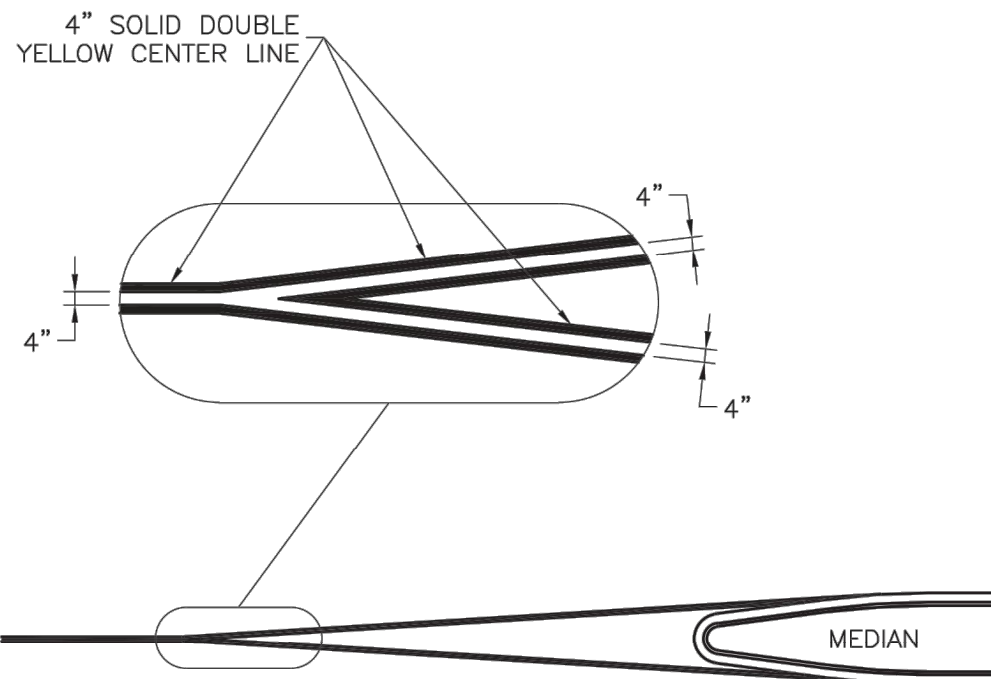
TYPICAL MARKINGS FOR FOUR-LANE UNDIVIDED ROADWAY

PAVEMENT MARKING GENERAL NOTES:

- ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- ALL WORDS AND SYMBOLS SHALL CONFORM TO THE LATEST EDITION OF STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS PRINTED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.
- PAVEMENT MARKINGS, EITHER TEMPORARY OR PERMANENT ARE REQUIRED AT ALL TIMES IF THE ROADWAY IS OPEN TO TRAFFIC.
- ALL PAVEMENT MARKINGS THAT CONFLICT WITH THE DESIRED MARKINGS SHALL BE COMPLETELY REMOVED. REMOVALS SHALL NOT LEAVE THE ROAD SURFACE SCARRED WITH AN IMAGE THAT MISLEADS TRAFFIC. ANY EXCESS DAMAGE OR SCARRING OF PAVEMENT SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE PROPOSED PERMANENT MARKINGS SHALL BE LAID OUT BY THE CONTRACTOR IN ADVANCE OF THE MARKING INSTALLATION. MARKINGS SHALL NOT BE APPLIED UNTIL THE LAYOUT HAS BEEN APPROVED BY THE CITY TRAFFIC ENGINEER.
- CENTER LINES SHALL BE MARKED ON ALL UNDIVIDED ARTERIAL STREETS, AND ANY OTHER UNDIVIDED STREET WITH MORE THAN TWO LANES AND/OR A SPEED LIMIT OF 30 MPH OR MORE.
- EDGE LINES SHALL BE MARKED ON ALL NON-CURBED STREETS.



TYPICAL MID-BLOCK OR SCHOOL CROSS WALK



TYPICAL MEDIAN NOSE CENTER LINE DETAIL

LEE'S SUMMIT
MISSOURI

PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

ROADWAY MARKING DETAILS

Drawn By: BWC
Checked By: MP
Date: 01/2020
Proj. #:

TRAFFIC CONTROL DETAILS
FINAL DEVELOPMENT PLAN

COLBURN ROAD IMPROVEMENTS
MCPL - COLBURN ROAD

LEE'S SUMMIT, MISSOURI

drawn by: JTF
checked by: TP
approved by: TP
QA/QC by: BS
project no.: 018-0330 WP10
drawing no.: C_DTL01_80330.dwg
date: 11/01/2024

SHEET
C5.2



REV.	NO.	DATE	REVISIONS DESCRIPTION	BY
1	1	11/14/2024	REVISED PER CITY COMMENTS	RLK

REVISIONS

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