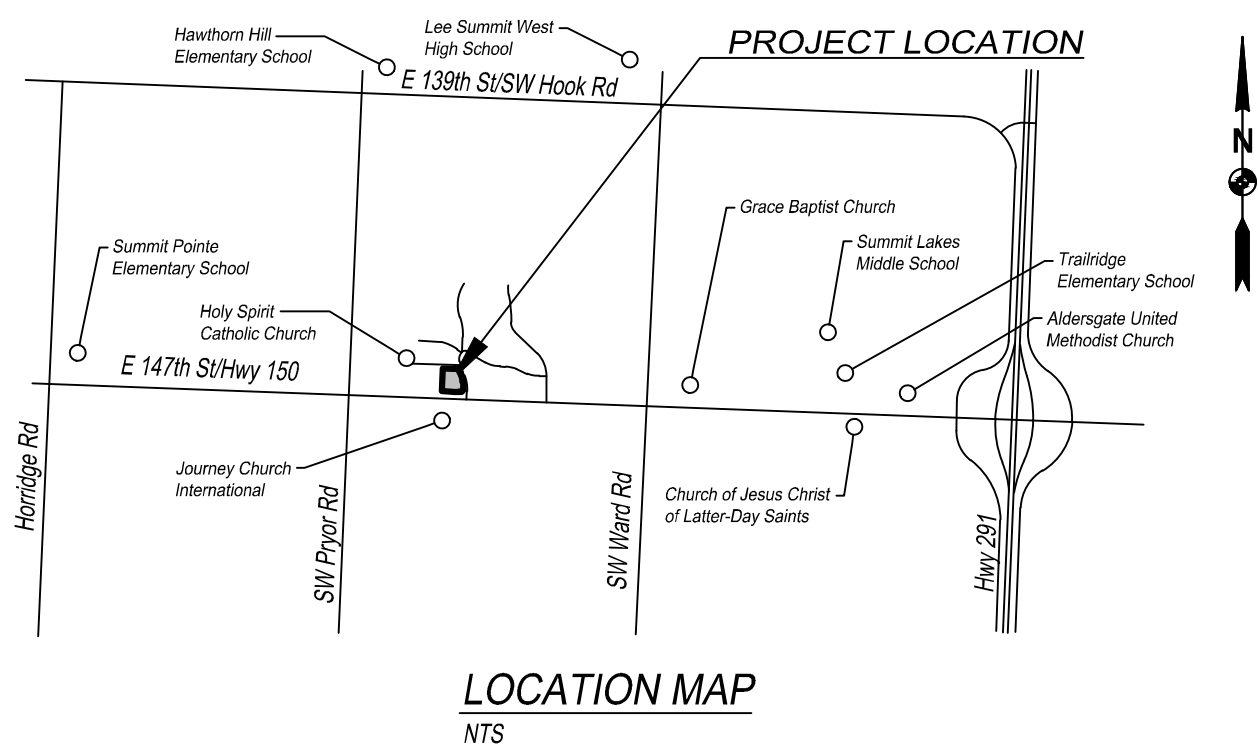


CIVIL/SITE DEVELOPMENT PLANS
FOR
CASEY'S GENERAL STORE #4291
1620 SW M 150 HIGHWAY
LEE SUMMIT, MISSOURI 64082



INDEX OF SHEETS

SHEET	TITLE
C-001	COVER SHEET
C-003 - C-004	ALTA SURVEY
C-100	EXISTING CONDITION PLAN
C-101	OVERALL SITE PLAN
C-102	FIRE TRUCK MOVEMENT PLAN
C-103	SITE PLAN
C-201	GRADING PLAN
C-202 - C-203	GRADING SPOTS
C-301	UTILITY PLAN
C-302	DRAINAGE MAP
C-303	STORM SEWER PLAN & PROFILE
C-401	EROSION CONTROL PLAN
C-402 - C-405	EROSION CONTROL DETAILS
C-601 - C-605	STANDARD DETAILS
C-701	LANDSCAPING PLAN

PROJECT CONTACT INFORMATION:

OWNER: CASEY'S MARKETING COMPANY
ONE SE CONVENIENCE BLVD
ANKENY, IA 50021
ATTN: SHANNON GERARD, email: shannon.gerard@caseys.com
PHONE: 515-381-5732

ENGINEER: SBB ENGINEERING, LLC
101 SOUTH KANSAS AVENUE
TOPEKA, KS 66603
ATTN: JEFF LAUBACH, PE email: jeff.laubach@sbbeng.com
PHONE: 785-215-8630

MISSOURI ONE-CALL:
1-800-DIG-RITE
(1-800-344-7483)



Know what's below.
Call before you dig.

GENERAL NOTES:

- ALL CONSTRUCTION METHODS AND MATERIALS USED IN THE CONSTRUCTION OF THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE IN ACCORDANCE WITH THE STANDARD TECHNICAL SPECIFICATIONS AND CURRENT REVISIONS ON FILE IN THE OFFICE OF THE CITY ENGINEER, CITY OF LEE SUMMIT, MISSOURI.
- UTILITIES WERE FIELD LOCATED THROUGH MISSOURI ONE CALL. THE LOCATION OF ALL OVERHEAD AND UNDERGROUND UTILITIES MAY VARY FROM WHAT IS INDICATED IN THESE PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH THE UTILITY OWNER TO LOCATE AND FLAG ALL UNDERGROUND UTILITIES WHETHER INDICATED OR NOT. NO EXCAVATION WILL BE PERMITTED IN THE AREA OF UNDERGROUND UTILITIES UNTIL ALL SUCH UTILITIES HAVE BEEN LOCATED AND IDENTIFIED TO THE SATISFACTION OF ALL PARTIES AND THEN ONLY WITH EXTREME CARE TO AVOID ANY POSSIBILITY OF DAMAGE TO THE UTILITY.
- ALL PERMITS AND FEES NECESSARY TO CONSTRUCT THE PROJECT IS THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE NOTED.
- UNLESS OTHERWISE SPECIFIED, ALL DISTURBED AREAS THAT ARE NON-HARDSCAPED SHALL BE EITHER LANDSCAPED OR SEEDED AND FERTILIZED.
- ALL PAVEMENT REMOVAL AREAS SHALL BE SAW CUT TO FORM A STRAIGHT AND UNIFORM LINE.
- ALL WORK WITHIN THE MODOT RIGHT-OF-WAY SHALL COMPLY WITH MODOT STANDARDS AND SPECIFICATIONS. ALL WORK WITHIN THE CITY OF LEE'S SUMMIT RIGHT-OF-WAY SHALL COMPLY WITH CITY STANDARDS AND SPECIFICATIONS.

NON-EMERGENCY UTILITY OWNER CONTACTS

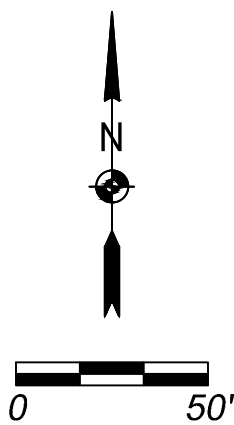
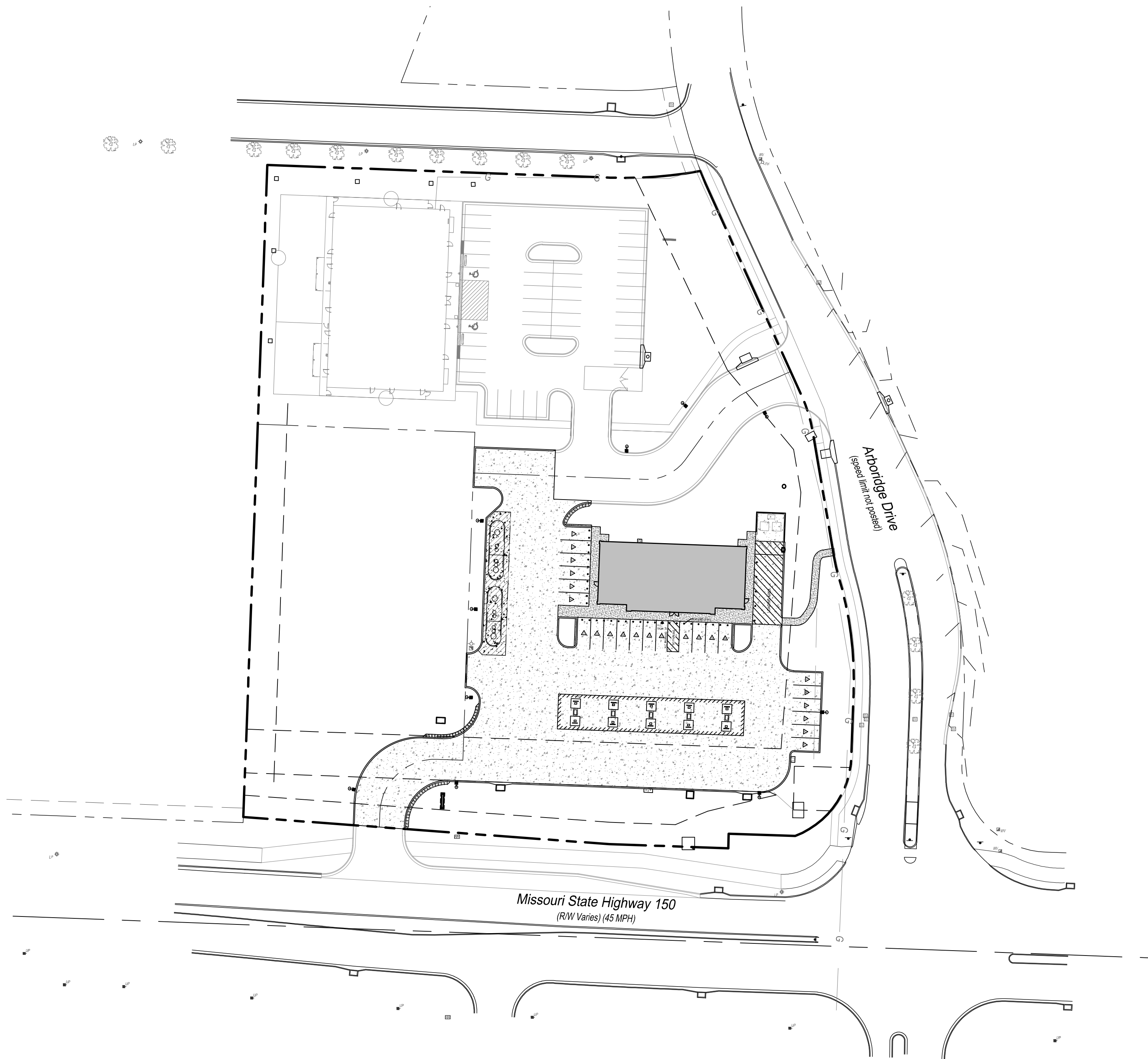
Cable TV
AT&T
Carrie Cilke
(816)703-4386
cc3527@att.com

Gas Service
Spire
Bobbie Saulsberry
(816)591-6909
bobbie.saulsberry@spireenergy.com

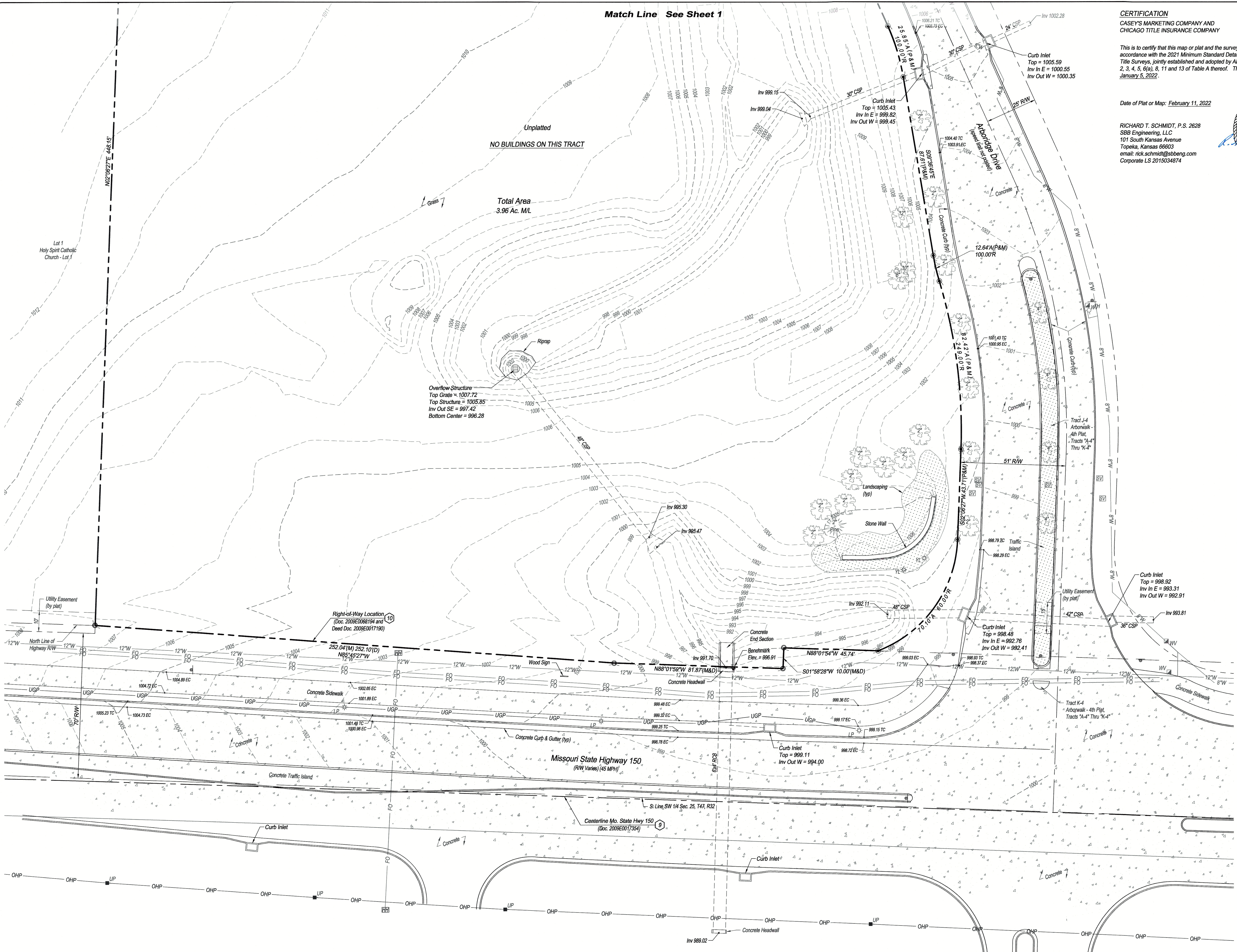
Water / Sanitary Sewer
Water Utilities Department
Jeff Thorn
(816)969-1900
jeff.thorn@cityofs.net

Electric
Evergy
Ron DeJarnette
(816)347-4316
ron.dejarnette@evergy.com

Stormwater
Lee's Summit Public
Works Department
220 SE Green
Lee's Summit, MO 64063
(816)969-1800



CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100			
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVIEWED ON:	Cover Sheet	
DRAWING INFORMATION: CONSTRUCTION DIVISION		C-001	
DRAWN BY:	CHECKED BY:		

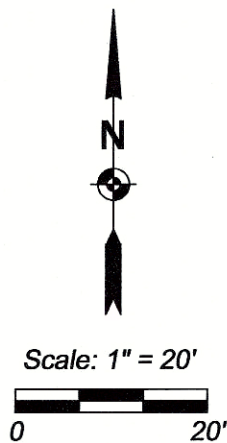


CERTIFICATION
CASEY'S MARKETING COMPANY AND
CHICAGO TITLE INSURANCE COMPANY

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6(a), 8, 11 and 13 of Table A thereof. The field work was completed on January 5, 2022.

Date of Plat or Map: February 11, 2022

RICHARD T. SCHMIDT, P.S. 2628
SBB Engineering, LLC
101 South Kansas Avenue
Topeka, Kansas 66603
email: rick.schmidt@sbbeng.com
Corporate LS 2015034874



Rev.	By	Date	Description

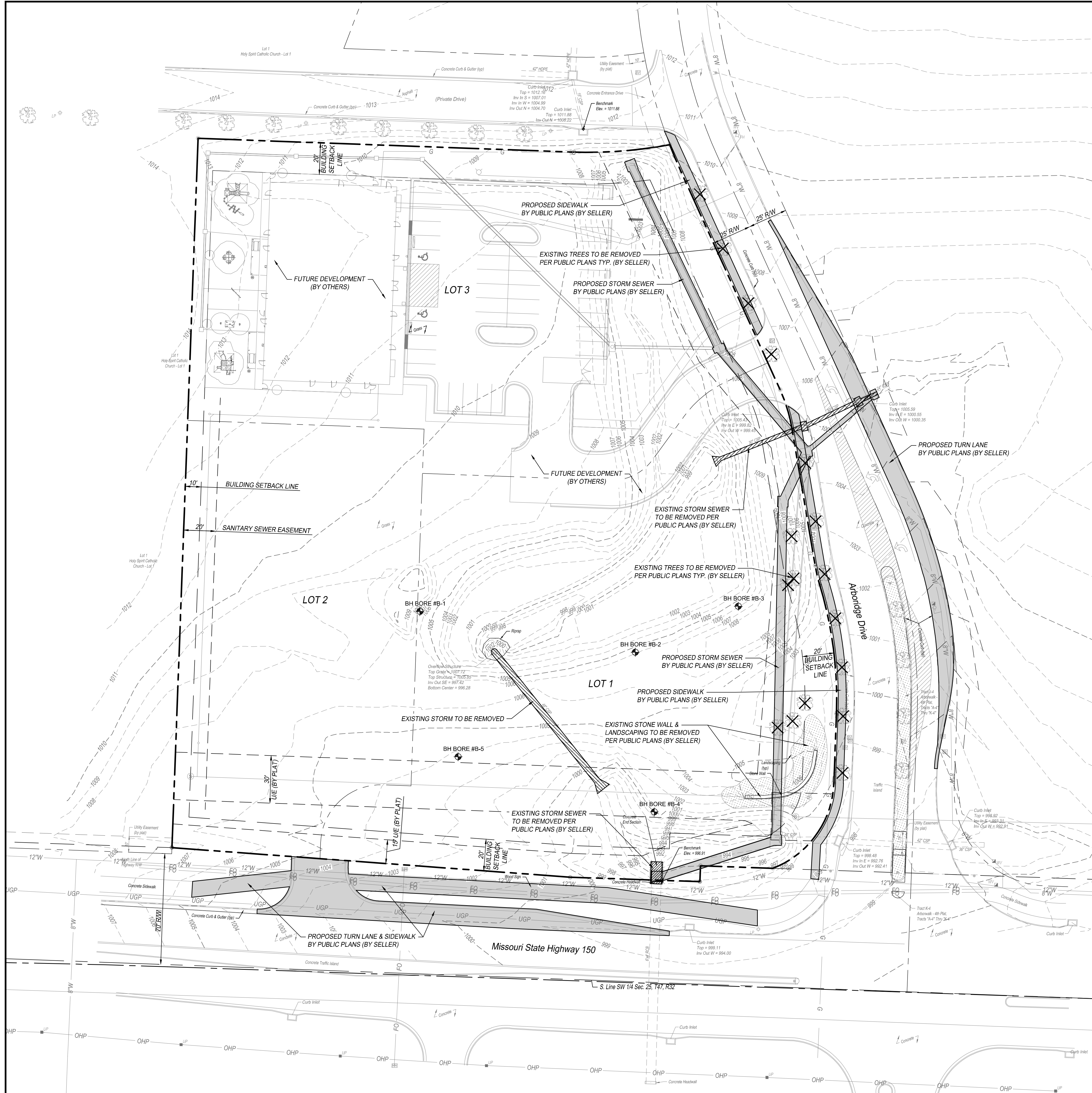
PREPARED FOR:
Casey's Marketing Company
One SE Convenience Boulevard
Ankeny, Iowa 50021

SBB Engineering, LLC
transportation - site development - surveying
101 South Kansas Avenue
Topeka, Kansas 66603
Ph: (785) 215-9630 / www.sbbeng.com

SBB ENGINEERING

ALTA/NSPS LAND TITLE SURVEY
PROJECT LOCATION:
Northwest Corner Of Highway 150
And Abbridge Drive
Lee Summit, Missouri 64081

SHEET TITLE:
SBB Proj. No.: 21-288
Drawn by: JEM
Checked by: RS
Date: 2/11/2022
Drawing No.: C-004
Sh. 2 of 2



GENERAL NOTES:

1. ALL CONSTRUCTION METHODS AND MATERIALS USED IN THE CONSTRUCTION OF THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE IN ACCORDANCE WITH THE STANDARD TECHNICAL SPECIFICATIONS AND CURRENT REVISIONS ON FILE IN THE OFFICE OF THE CITY ENGINEER, CITY OF LEE SUMMIT, MISSOURI.
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6. ALL WORK WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH MODOT STANDARDS AND SPECIFICATIONS.

LEGEND:

- PROPOSED PUBLIC PLANS (BY SELLER)
- EXISTING TO BE REMOVED PER PUBLIC PLANS (BY SELLER)
- EXISTING TO BE REMOVED

BH BORE #X APPROXIMATE BORING LOCATIONS REFER TO GEOTECHNICAL REPORT BY KANSAS CITY TESTING AND ENGINEERING, LLC DATED SEPTEMBER 19, 2022. KOTE JOB #G20-22-107 FOR ADDITIONAL INFORMATION.

NOTES:

1. UTILITIES WERE FIELD LOCATED THROUGH MISSOURI ONE CALL TICKET NO. 213571736, DATED DECEMBER 23, 2021, AND THROUGH OBSERVABLE FIELD EVIDENCE. WATER LINE LOCATION AND SIZE IS SHOWN AS PER GIS INFORMATION PROVIDED BY THE CITY OF LEE SUMMIT.
2. ACCORDING TO THE FLOOD INSURANCE RATE MAP "FIRM" COMMUNITY MAP NUMBER 29095C0531G, EFFECTIVE DATE JANUARY 20, 2017, THIS PROPERTY IS IN FLOOD ZONE "X", AREA DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

BENCHMARKS:

VERTICAL DATUM SHOWN ON THIS SURVEY IS BASED ON NAVD 88 DATUM.




CHISELED SQUARE AT CENTER FRONT FACE OF CURB INLET, LOCATED ON SOUTH SIDE OF PRIVATE DRIVE ALONG THE NORTH LINE OF SUBJECT TRACT. ELEV. = 1011.88

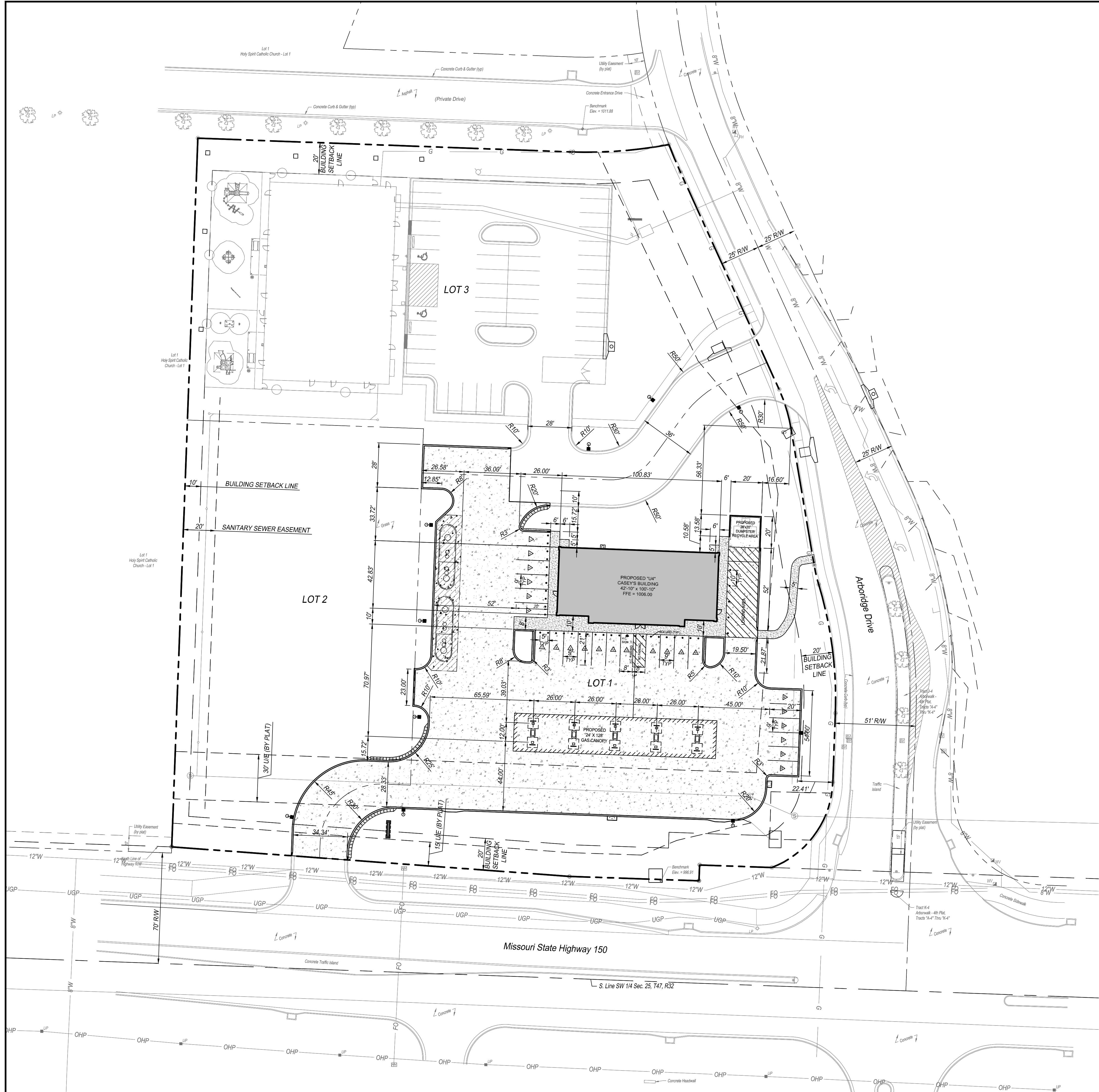
CHISELED SQUARE AT THE NORTHEAST CORNER OF CONCRETE HEADWALL FOR 6'X4' RCB, LOCATED NEAR THE SE CORNER OF SUBJECT TRACT. ELEV. = 996.91



SBB Proj. No. 21-288

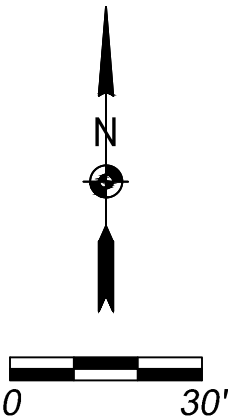


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CASEY'S CONSTRUCTION DIVISION					
One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100					
PROJECT:	LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE		PUBLISHED:	DRAWING INFORMATION:	
			10.13.23		
			REVISED ON:	EXISTING CONDITIONS / DEMOLITION PLAN	
DRAWING INFORMATION:			<div>C-100</div>		
CONSTRUCTION DIVISION					
DRAWN BY:		CHECKED BY:			



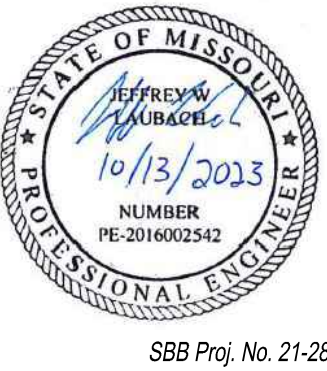
GENERAL NOTES:

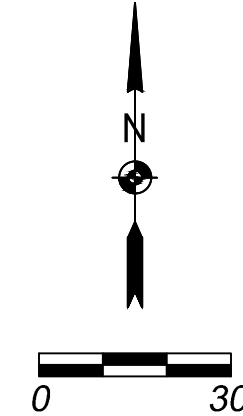
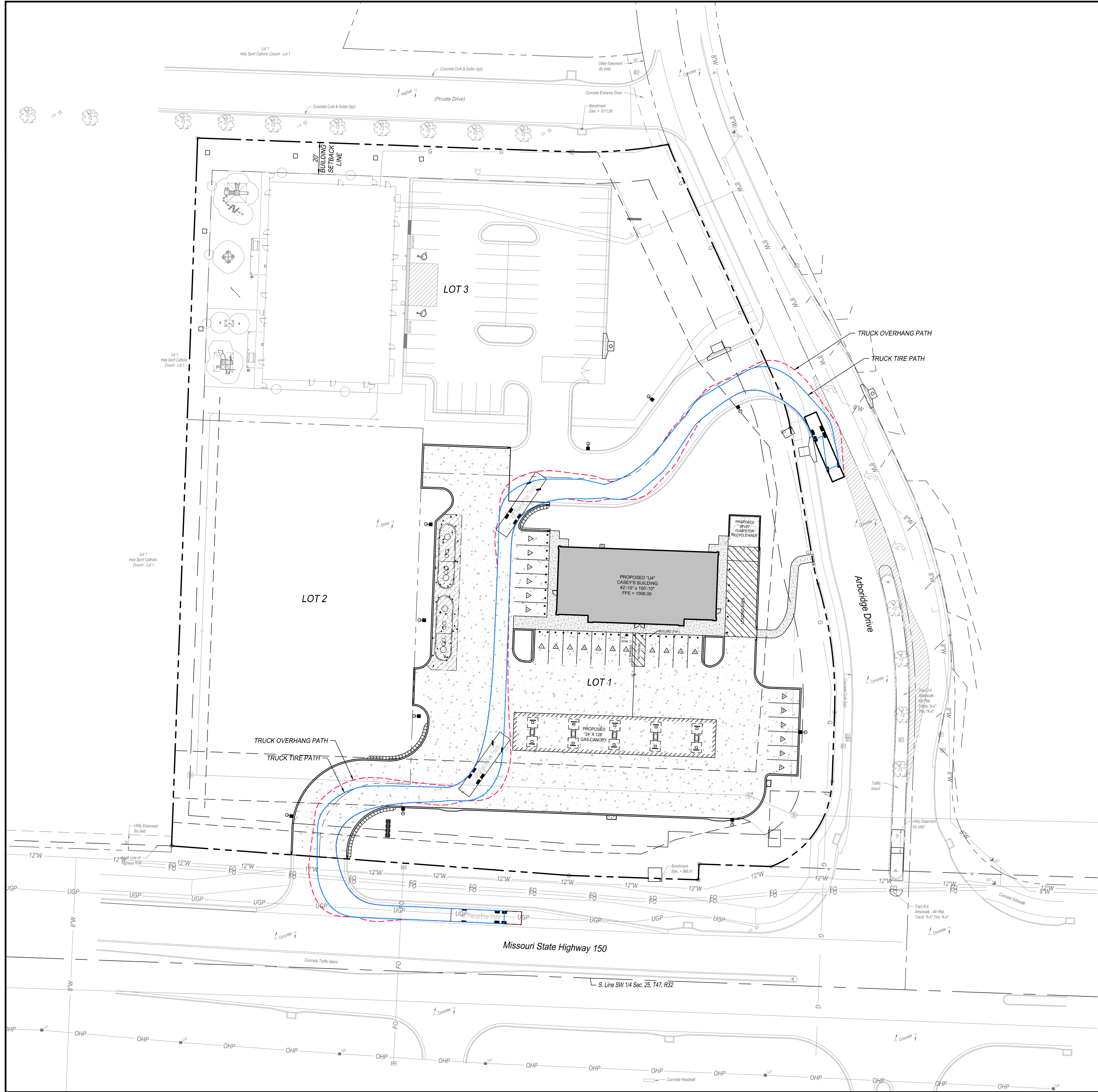
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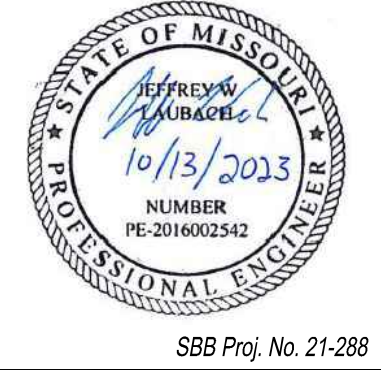
SBB ENGINEERING
transportation • site development • surveying
101 South Kansas Avenue
Topeka, Kansas 66603
Ph: (785) 215-8630 /
www.sbbeng.com

Casey's CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100	
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	DATE: 10.13.23
DRAWING INFORMATION: CONSTRUCTION DIVISION	
DRAWN BY: _____ CHECKED BY: _____	
OVERALL SITE PLAN	
C-101	

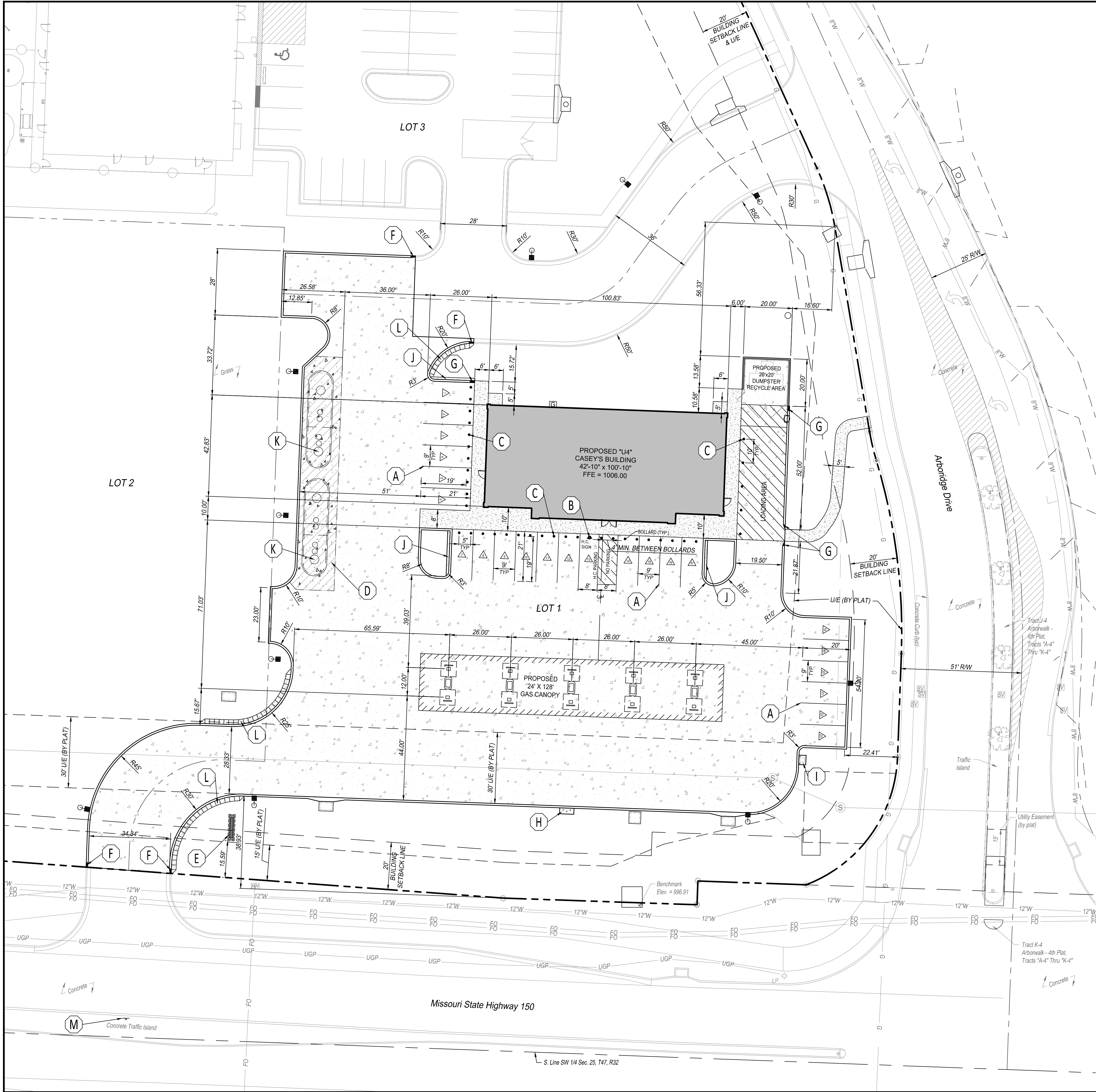




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CASEY'S CONSTRUCTION DIVISION			
One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100			
PROJECT:	LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	DATE:	10.13.23
CONSTRUCTION DIVISION		REVIEWED BY:	
DRAWN BY:		CHECKED BY:	
		FIRE TRUCK MOVEMENT PLAN	
		C-102	



GENERAL NOTES:

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SITE PLAN KEY NOTES:

- | | | | |
|----------|--|----------|---|
| A | PLACE PAVEMENT MARKING, 4" PAINTED, WHITE, 90° TO SIDEWALK (TYP.) OR CURB. | G | CURB TRANSITION. (FULL HEIGHT TO ZERO HEIGHT CURB) |
| B | INSTALL HANDICAP SIGN IN COMPLIANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PLACE PAVEMENT MARKING, ACCESSIBLE PARKING SYMBOL, WHITE. REFER TO DETAILS 11 & 12 ON SHEET C-601. | H | FUEL DISPENSER EMERGENCY SHUT-OFF SWITCH. REFER TO DETAIL 8 ON SHEET C-601. |
| C | INSTALL BOLLARD (TYP.). REFER TO ARCHITECTURAL PLANS AND DETAIL 1 ON SHEET C-601. | I | AIR COMPRESSOR BOX. REFER TO DETAIL 7 ON SHEET C-601. |
| D | PAVEMENT SECTION ADJACENT TO UNDERGROUND TANKS. REFER TO DETAIL 4 ON SHEET C-602. | J | PARKING LANDSCAPE ISLAND. REFER TO DETAIL 7 ON SHEET C-603. |
| E | INSTALL MONUMENT SIGN. REFER TO SIGN PACKAGE. | K | UGST MANHOLE. REFER TO DETAIL 6 ON SHEET C-602. |
| F | 2' CURB TRANSITION. (FULL HEIGHT TO CURB (BY OTHERS)) | L | LANDSCAPE PROTECTOR. REFER TO DETAIL 4 ON SHEET C-601. |
| | | M | INSTALL "ONE WAY" SIGN IN THE HIGHWAY MEDIAN. |

LEGEND:

- | | |
|--|--|
| | 6" CONCRETE PAVEMENT. REFER TO DETAIL 2 ON SHEET C-601 FOR CONCRETE PAVING. |
| | 8" REINFORCED CONCRETE PAVEMENT. REFER TO DETAIL 4 ON SHEET C-602 FOR PAVEMENT SECTION ADJACENT TO UNDERGROUND TANKS |
| | 6" CONCRETE SIDEWALK. REFER TO DETAIL 1 ON SHEET C-601 FOR TYPICAL BUILDING SIDEWALK AND BOLLARD. |
| | CURB (MONOLITHIC). REFER TO DETAIL 2 ON SHEET C-601 FOR CURB. |
| | PROPOSED LIGHT POLE. REFER TO LIGHTING PLAN. |

LEGAL DESCRIPTION:

LOT 1, ARBORWALK WEST ADDITION, A SUBDIVISION IN LEE'S SUMMIT, JACKSON COUNTY, MISSOURI.

TANK BALLAST NOTE:

CITY DOES NOT HAVE ANY REQUIREMENTS FOR DISCHARGING WATER FROM UNDERGROUND STORAGE TANKS.

NOTES:

- UTILITIES WERE FIELD LOCATED THROUGH MISSOURI ONE CALL TICKET NO. 213571736, DATED DECEMBER 23, 2021, AND THROUGH OBSERVABLE FIELD EVIDENCE. WATER LINE LOCATION AND SIZE IS SHOWN AS PER GIS INFORMATION PROVIDED BY THE CITY OF LEE SUMMIT.
- ACCORDING TO THE FLOOD INSURANCE RATE MAP "FIRM" COMMUNITY MAP NUMBER 29095C0513G, EFFECTIVE DATE JANUARY 20, 2017, THIS PROPERTY IS IN FLOOD ZONE "X", AREA DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.
- THERE ARE NO GAS/OIL WELLS ONSITE PER MDNR DATA BASE OF OIL AND GAS PERMITS.
- THE CONTRACTOR SHALL CONTACT THE CITY'S DEVELOPMENT SERVICES ENGINEERING INSPECTION TO SCHEDULE AN INSPECTION COORDINATION MEETING WITH A FIELD ENGINEERING INSPECTOR PRIOR TO ANY LAND DISTURBANCE WORK AT (816) 969-1200.

BENCHMARKS:

VERTICAL DATUM SHOWN ON THIS SURVEY IS BASED ON NAVD 88 DATUM.

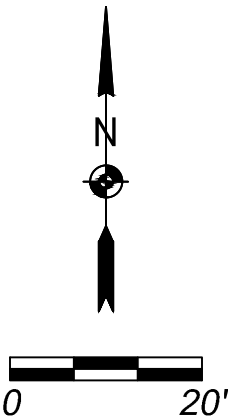
CHISELED SQUARE AT CENTER FRONT FACE OF CURB INLET, LOCATED ON SOUTH SIDE OF PRIVATE DRIVE ALONG THE NORTH LINE OF SUBJECT TRACT. ELEV. = 1011.88

CHISELED SQUARE AT THE NORTHEAST CORNER OF CONCRETE HEADWALL FOR 6'X4' RCB, LOCATED NEAR THE SE CORNER OF SUBJECT TRACT. ELEV. = 996.91

SITE DATA

LOT	EXISTING / PROPOSED ZONING	LOT AREA (SF)	LOT AREA (ACRES)	BUILDING AREA SF (ENVELOPE)	FAR	NUMBER OF FLOORS	PARKING REQUIRED	PARKING PROVIDED
LOT 1	M-150 CORRIDOR DEVELOPMENT / PLANNED MIXED USE	71,267	1.64	4,562 SF	0.064	1	23 SPACES	23 SPACES

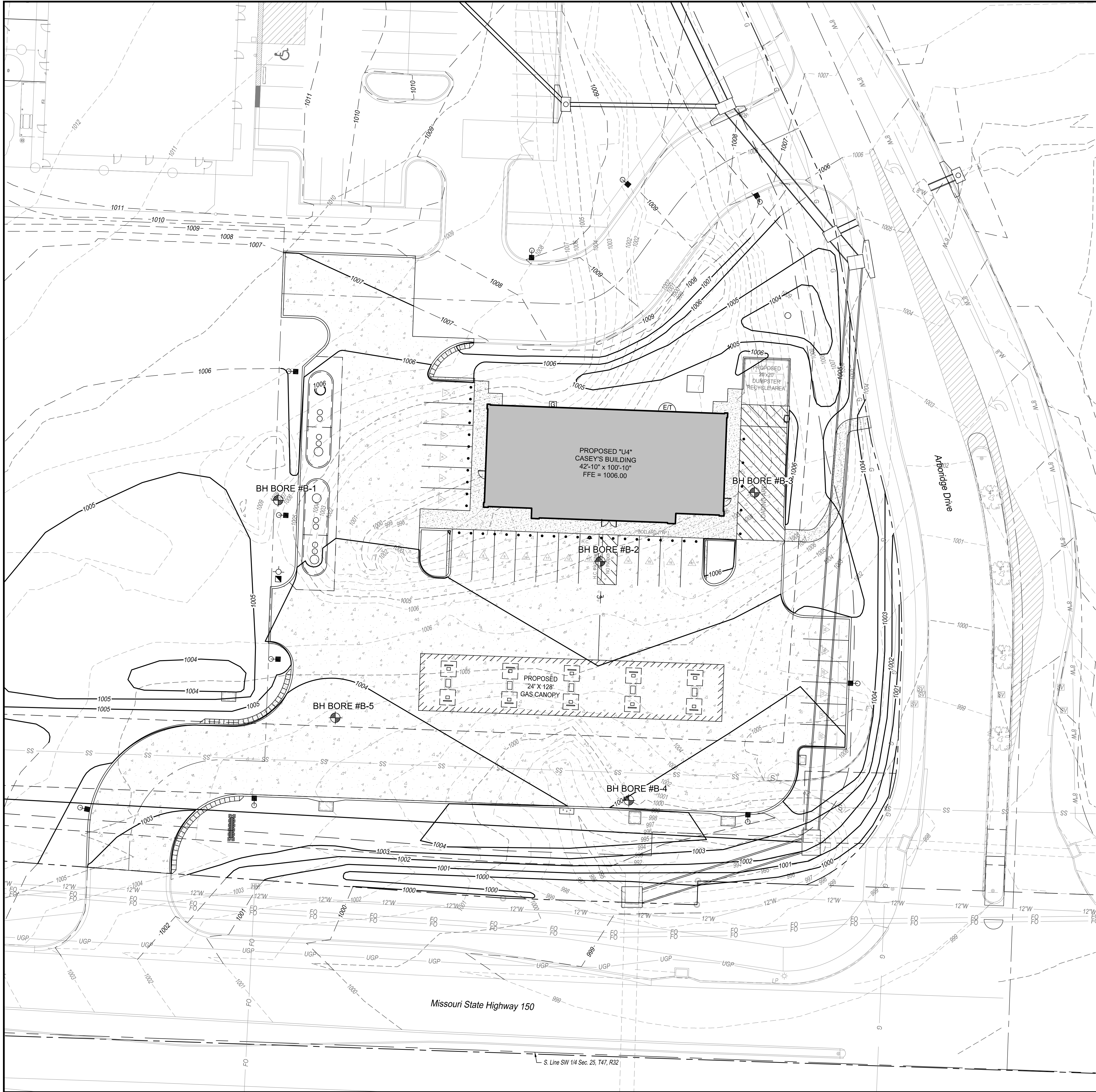
ADA SPACES REQUIRED (VAN)	ADA SPACES PROVIDED (VAN)	PARKING LOT COVERAGE	TOTAL IMPERVIOUS COVERAGE	OPEN SPACE
1 SPACE	1 SPACE	36,054 SF	45,474 SF	25,790 SF



Casey's		Casey's	
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-8100			
PROJECT:	LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE 'U4' - STYLE STORE	DATE:	10/13/23
DRAWING INFORMATION:	CONSTRUCTION DIVISION	REVISIONS:	
DRAWN BY:		CHECKED BY:	
		SITE PLAN	
		C-103	



SBB Proj. No. 21-288



GENERAL NOTES:

- ALL CONSTRUCTION METHODS AND MATERIALS USED IN THE CONSTRUCTION OF THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE IN ACCORDANCE WITH THE STANDARD TECHNICAL SPECIFICATIONS AND CURRENT REVISIONS ON FILE IN THE OFFICE OF THE CITY ENGINEER, CITY OF LEE SUMMIT, MISSOURI.
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LEGEND:

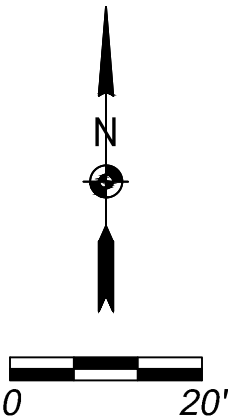
- 1005 PROPOSED CONTOURS
- 1005 PROPOSED CONTOURS BY OTHERS
- 1004 EXISTING CONTOURS
- <-X.X% SLOPE FLOW DIRECTION

LIST OF ABBREVIATIONS:

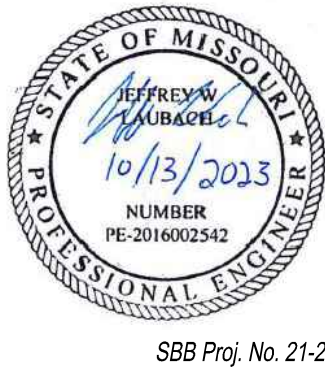
- | | | |
|-----|---|--------------------------|
| EX. | - | EXISTING |
| FFE | - | FINISHED FLOOR ELEVATION |
| TC | - | TOP OF CURB |
| TG | - | TOP OF GROUND |
| TI | - | TOP OF INLET |
| TP | - | TOP OF PAVEMENT |
| TPE | - | TOP OF EXISTING PAVEMENT |
| TS | - | TOP OF SIDEWALK |
| BW | - | BOTTOM OF WALL |
| TW | - | TOP OF WALL |
| TM | - | TOP OF MANHOLE |
| TF | - | TOP OF FOOTING |

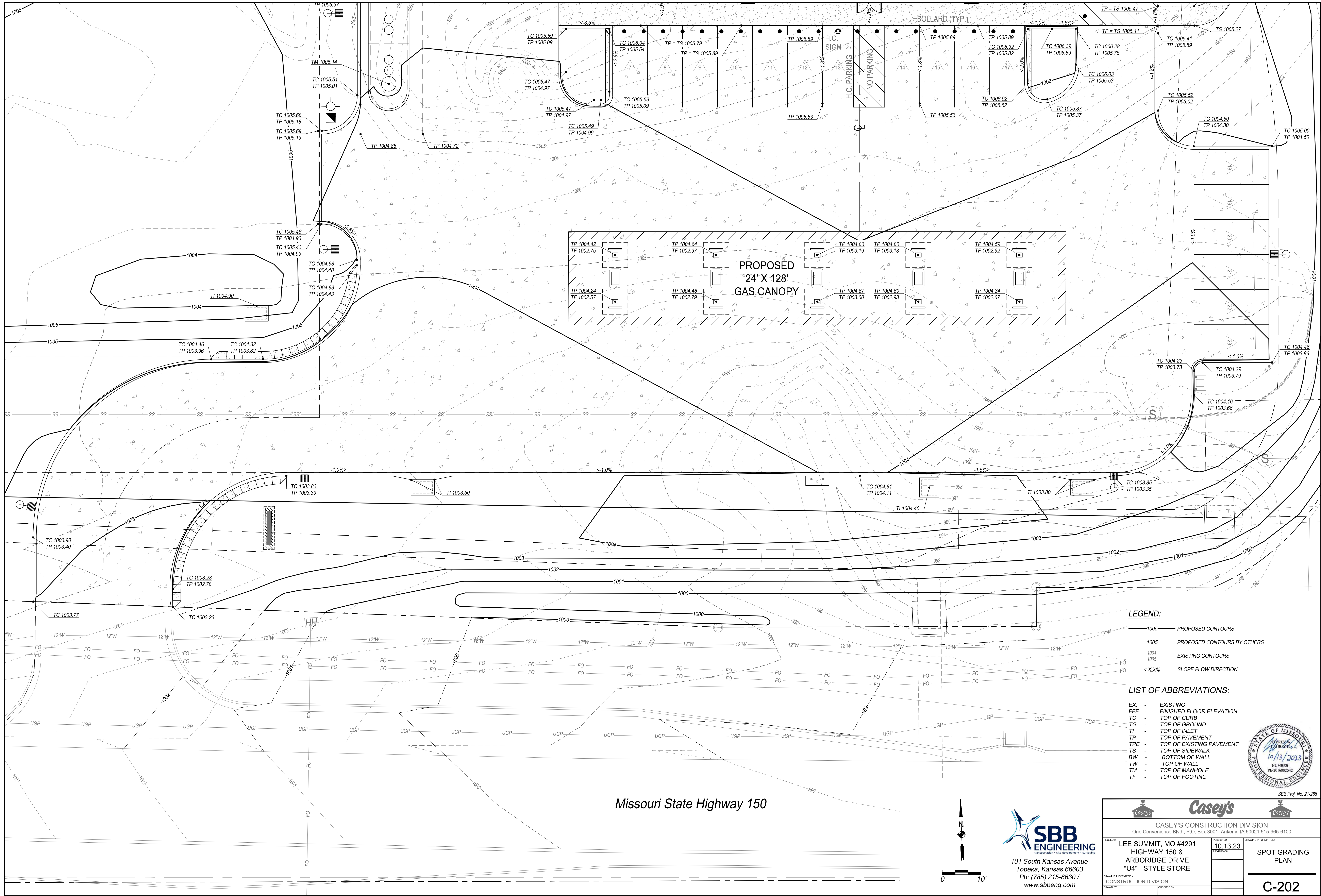
BH BORE # APPROXIMATE BORING LOCATIONS. REFER TO GEOTECHNICAL REPORT BY KANSAS CITY TESTING AND ENGINEERING, LLC DATED SEPTEMBER 19, 2022. KCTE JOB #G20-22-107 FOR ADDITIONAL INFORMATION.

SITE GRADING
ANTICIPATE OVER EXCAVATION IN SOME AREAS OF 2-3 FEET DUE TO POOR SOIL CONDITION.
UNDERGROUND STORAGE TANKS
HARD ROCK EXCAVATION MAY BE PRESENT IN THE BOTTOM OF THE PIT; PERCHED WATER MAY ALSO BE ENCOUNTERED ABOVE ROCK STRATA.

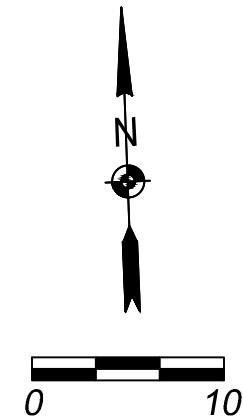


CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100			
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVISED ON:	DRAWING INFORMATION GRADING PLAN	
DRAWING INFORMATION CONSTRUCTION DIVISION		C-201	





Missouri State Highway 150



SBB
ENGINEERING
transportation • site development • surveying
101 South Kansas Avenue
Topeka, Kansas 66603
Ph: (785) 215-8630 /
www.sbbeng.com

LEGEND:

- 1005— PROPOSED CONTOURS
- 1005— PROPOSED CONTOURS BY OTHERS
- - -1004- - - EXISTING CONTOURS
- <-X.X%> SLOPE FLOW DIRECTION

LIST OF ABBREVIATIONS:

EX	-	EXISTING
FPE	-	FINISHED FLOOR ELEVATION
TC	-	TOP OF CURB
TG	-	TOP OF GROUND
TI	-	TOP OF INLET
TP	-	TOP OF PAVEMENT
TPE	-	TOP OF EXISTING PAVEMENT
TS	-	TOP OF SIDEWALK
BW	-	BOTTOM OF WALL
TW	-	TOP OF WALL
TM	-	TOP OF MANHOLE
TF	-	TOP OF FOOTING

Casey's
CASEY'S CONSTRUCTION DIVISION
One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100

PROJECT: LEE SUMMIT, MO #4291
HIGHWAY 150 &
ARBORIDGE DRIVE
"U4" - STYLE STORE

DATE: 10.13.23
REVISION: 10.13.23

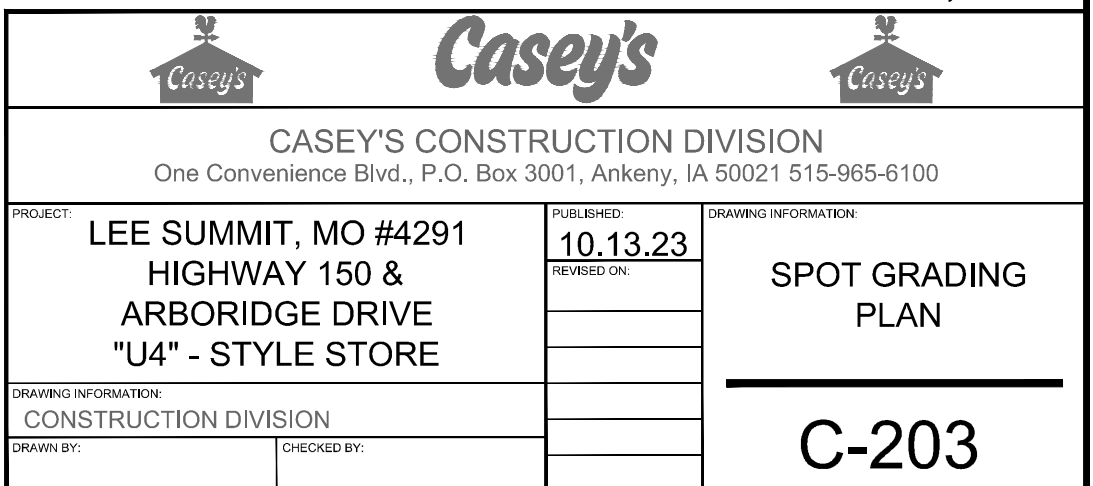
CONSTRUCTION DIVISION

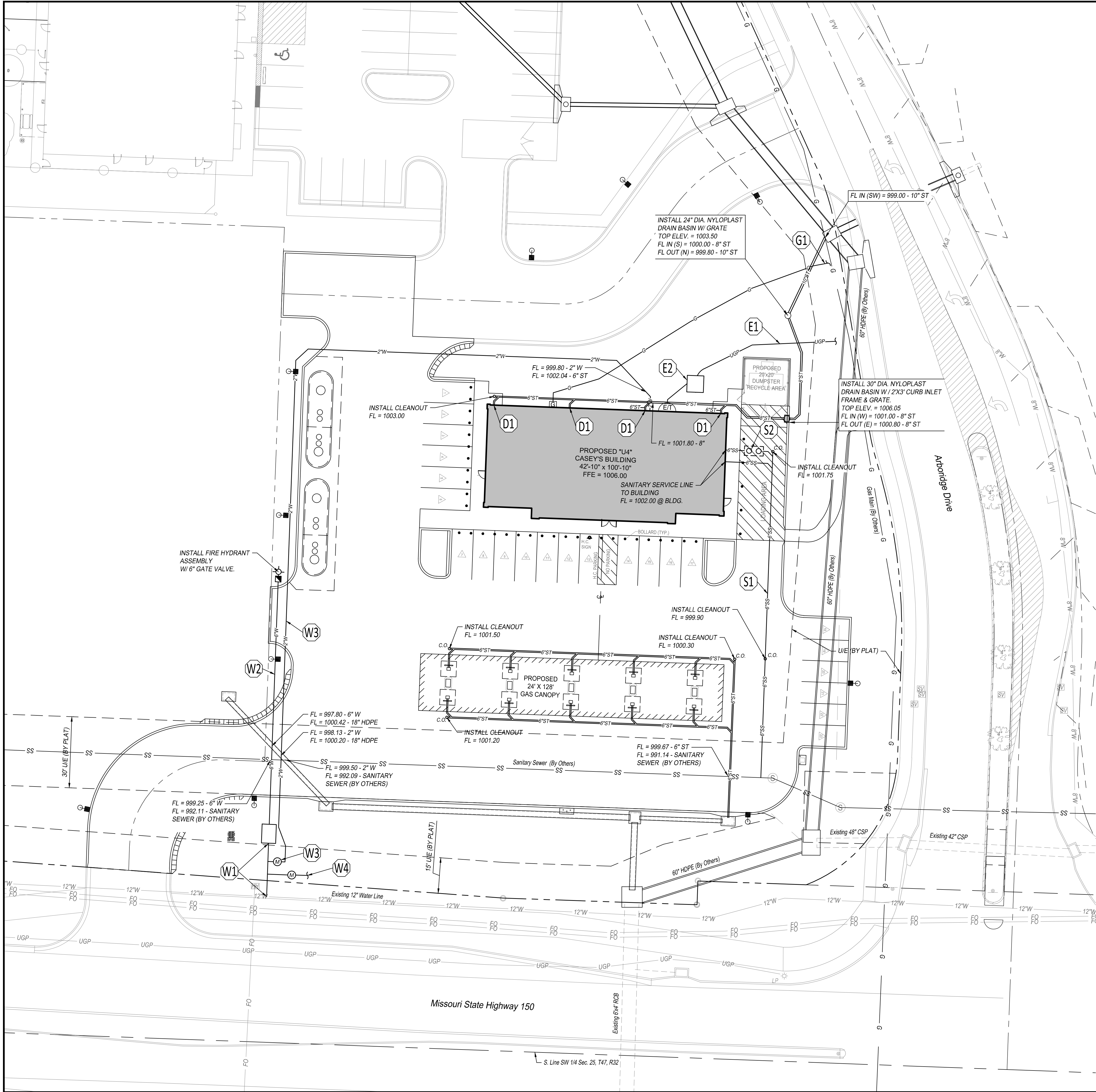
SPOT GRADING PLAN

C-202



SBB Proj. No. 21-288





GENERAL NOTES:

- ALL CONSTRUCTION METHODS AND MATERIALS USED IN THE CONSTRUCTION OF THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE IN ACCORDANCE WITH THE STANDARD TECHNICAL SPECIFICATIONS AND CURRENT REVISIONS ON FILE IN THE OFFICE OF THE CITY ENGINEER, CITY OF LEE SUMMIT, MISSOURI.
- UTILITIES WERE FIELD LOCATED THROUGH MISSOURI ONE CALL. THE LOCATION OF ALL OVERHEAD AND UNDERGROUND UTILITIES MAY VARY FROM WHAT IS INDICATED IN THESE PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH THE UTILITY OWNER TO LOCATE AND FLAG ALL UNDERGROUND UTILITIES WHETHER INDICATED OR NOT. NO EXCAVATION WILL BE PERMITTED IN THE AREA OF UNDERGROUND UTILITIES UNTIL ALL SUCH UTILITIES HAVE BEEN LOCATED AND IDENTIFIED TO THE SATISFACTION OF ALL PARTIES AND THEN ONLY WITH EXTREME CARE TO AVOID ANY POSSIBILITY OF DAMAGE TO THE UTILITY.
- ALL PERMITS AND FEES NECESSARY TO CONSTRUCT THE PROJECT IS THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE NOTED.
- UNLESS OTHERWISE SPECIFIED, ALL DISTURBED AREAS THAT ARE NON-HARDSCAPED SHALL BE EITHER LANDSCAPED OR SEEDED AND FERTILIZED.
- ALL PAVEMENT REMOVAL AREAS SHALL BE SAW CUT TO FORM A STRAIGHT AND UNIFORM LINE.
- ALL WORK WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH MODOT STANDARDS AND SPECIFICATIONS.

SITE PLAN KEY NOTES:

- E1** PLACE ELECTRICAL AND TELEPHONE ENTRY INTO BUILDING.
(RE: BUILDING ELECTRIC PLAN FOR SECONDARY CONDUIT ROUTING INSIDE BUILDING TO ELECTRIC METERS).
ELECTRIC: 3 PHASE, 800 AMP, 120/208 VOLTS, 4 WIRE.
TELEPHONE: 20 PAIR, 8 LINES.
- E2** PROPOSED LOCATION OF TRANSFORMER PAD. CONTRACTOR TO VERIFY EXACT LOCATION AND SIZE WITH POWER COMPANY PRIOR TO INSTALLATION. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF CONCRETE PAD, CONDUIT AND PIPE BOLLARDS AS REQUIRED BY THE ELECTRIC COMPANY. CONTRACTOR SHALL COORDINATE SAID WORK WITH THE ELECTRIC COMPANY. FOLLOW ALL ELECTRIC COMPANY'S SERVICE STANDARDS.
- G1** INSTALL GAS LINE TO BUILDING ENTRY WITH 1 GAS METER. SIZE OF GAS MAIN SHALL BE AS DETERMINED BY UTILITY OR AS SHOWN ON BUILDING PLANS. CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH GAS COMPANY REGARDING THE SIZE AND INSTALLATION OF GAS SERVICE LINE AND METER.
- W1** CONNECT TO EXISTING 12" WATERLINE. INSTALL 6" WATER LINE (C900) AND INSTALL VAULT FOR DOUBLE DETECTOR CHECK. REFER TO DETAIL WAT-12 ON SHEET C-604. INSTALL SUMP PUMP TO DRAIN VAULT.
- W2** INSTALL 6" FIRE LINE (C900) TO FIRE HYDRANT ASSEMBLY.
- W3** CONNECT TO 6" FIRE LINE. INSTALL 1 1/2" METER SETTING AND METER PIT. INSTALL 2" WATER LINE BETWEEN TAP AND METER. INSTALL 2" DOMESTIC WATER LINE (HDPE SDR 9) TO BUILDING.
- W4** CONNECT TO 6" FIRE LINE. INSTALL 1 1/2" METER SETTING AND METER PIT. INSTALL 2" WATER LINE BETWEEN TAP AND METER. FOR IRRIGATION SERVICE.
- D1** TYPICAL STORM WATER DRAIN LINE FROM ROOF OR LANDSCAPE DRAIN. SEE BUILDING PLANS CONNECTION LOCATIONS. DRAIN LINES SHALL BE PVC SDR 35 OR HDPE N-12. MINIMUM SLOPE SHALL BE 1.0%
- S1** 6" SANITARY SEWER SERVICE LINE PIPE TO BE SCHEDULE 40 PVC AND MAINTAIN A MINIMUM SLOPE OF 2.0%.
- S2** GREASE TRAP TO BE PROCURED AND INSTALLED BY CONTRACTOR. GREASE TRAP TO BE PROCURED FROM SCHIER PRODUCTS, UTILIZE THE FOLLOWING URL: [HTTPS://SCHIERPRODUCTS.COM/STOCKING-DEALERS](https://schierproducts.com/stocking-dealers). CONTACT SCHIER PRODUCTS WITH ANY QUESTIONS OR CASEY'S TEAM MEMBER IF THERE IS AN ISSUE.

LEGEND:

8"W	EXISTING WATERLINE
12"W	EXISTING UNDERGROUND POWER
UGP	EXISTING FIBER OPTIC LINE
FO	EXISTING STORM SEWER
Existing Storm Sewer	
G	PROPOSED GAS LINE (BY OTHERS)
SS	PROPOSED SANITARY SEWER (BY OTHERS)
2"W	PROPOSED WATERLINE
6"W	PROPOSED GASLINE
G	PROPOSED SANITARY SEWER
6"SS	PROPOSED UNDERGROUND POWER
UGP	PROPOSED STORM SEWER
6"ST	

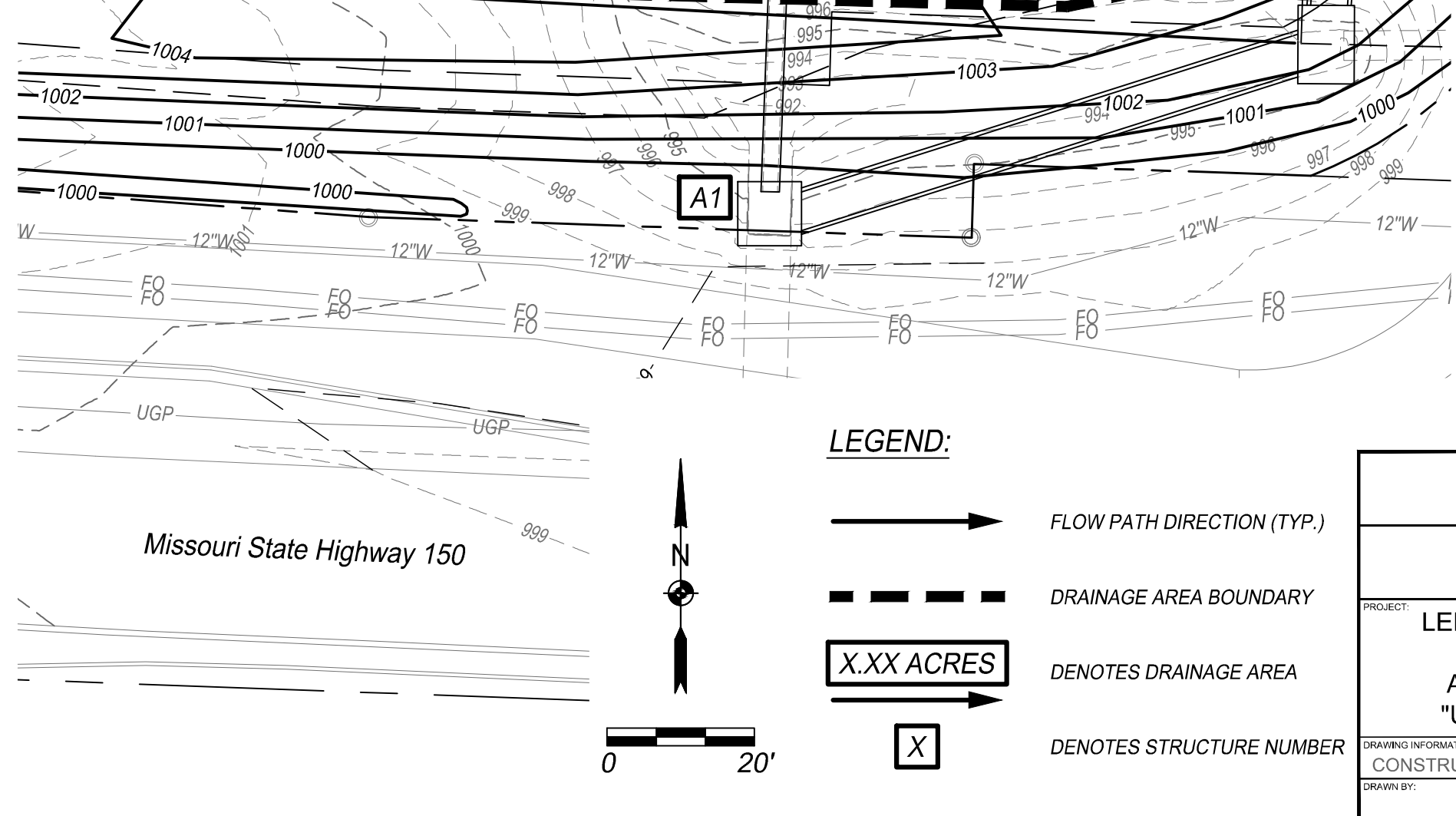


S88 Proj. No. 21-288

Casey's		Casey's	
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100			
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	DATE: 10.13.23 REVISED ON:	UTILITY PLAN	
DRAWING INFORMATION CONSTRUCTION DIVISION		C-301	
DRAWN BY:	CHECKED BY:		



PIPE FLOW DATA										PIPE DESIGN (n = 0.012 HDPE, n = 0.022 CMP*)									
STRUC ID	RUNOFF			SYSTEM TIME OF CONCENTRATION "Tc" AT STRUCTURE (MIN)			UPSTREAM STRUCTURE NUMBER	DOWNSTREAM STRUCTURE NUMBER	DIAMETER "D" (IN)	LENGTH "L" (FT)	UPSTREAM INVERT ELEVATION	DOWNSTREAM INVERT ELEVATION	100-YR HGL UPSTREAM	100-YR HGL DOWNSTREAM	SLOPE "S" (FT/FT)	TRAVEL TIME IN PIPE "TT" (MIN)	VELOCITY FULL Vp (FPS)	RUNOFF "Q2/Q10/Q100" (CFS)	FULL FLOW Q (CFS)
	INCREMENTAL RUNOFF COEFFICIENT "C"	AREA "A" (AC.)	C X A	AREA "A" (AC.)	C X A														
A4	0.81	1.33	1.08	1.33	1.08	5.0	A4	A3	18	61.89	1000.75	1000.00	1002.03	1001.30	0.0121	0.1	7	5.8	12.5
A3	0.81	0.74	0.60	2.07	1.68	5.0	A3	A2	24	130.31	999.80	998.50	1001.30	999.81	0.0100	0.3	8	12.3	24.5
A2	0.81	0.00	0.00	2.40	1.94	5.0	A2	A1	24	33.46	995.90	994.40	997.50	995.30	0.0448	0.0	17	10.5	51.9
B1	0.81	0.33	0.27	0.33	0.27	5.0	B1	A2	15	39.36	999.35	998.50	1000.02	998.96	0.0216	0.1	8	2.0	10.3
C0	0.81	0.05	0.04	0.05	0.04	5.0	C0	C1	6	65.00	1003.00	1002.00	1003.33	1002.28	0.0154	0.3	4	0.3	0.8
C1	0.81	0.05	0.04	0.10	0.08	5.0	C1	C2	8	70.00	1001.80	1001.00	1002.23	1001.39	0.0114	0.3	4	0.6	1.4
C2	0.81	0.03	0.02	0.13	0.11	5.0	C2	C3	8	50.00	1000.80	1000.00	1001.29	1000.79	0.0160	0.2	5	0.8	1.7
C3	0.81	0.17	0.14	0.30	0.24	5.0	C3	C4	10	38.00	999.80	999.00	1000.49	999.55	0.0211	0.1	6	1.3	3.4



STATE OF MISSOURI

Professional Engineer

10/13/2023

PE-2016002542

SBB Proj. No. 21-268

Casey's

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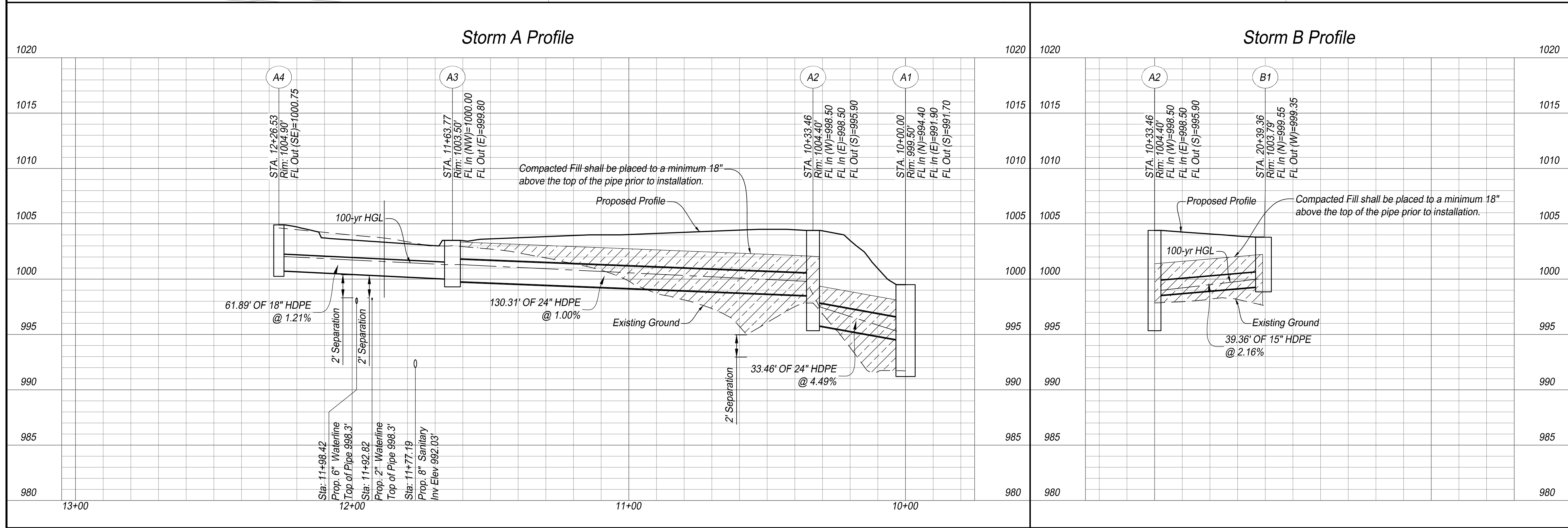
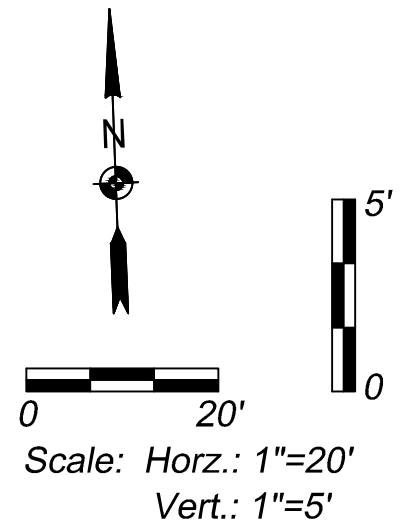
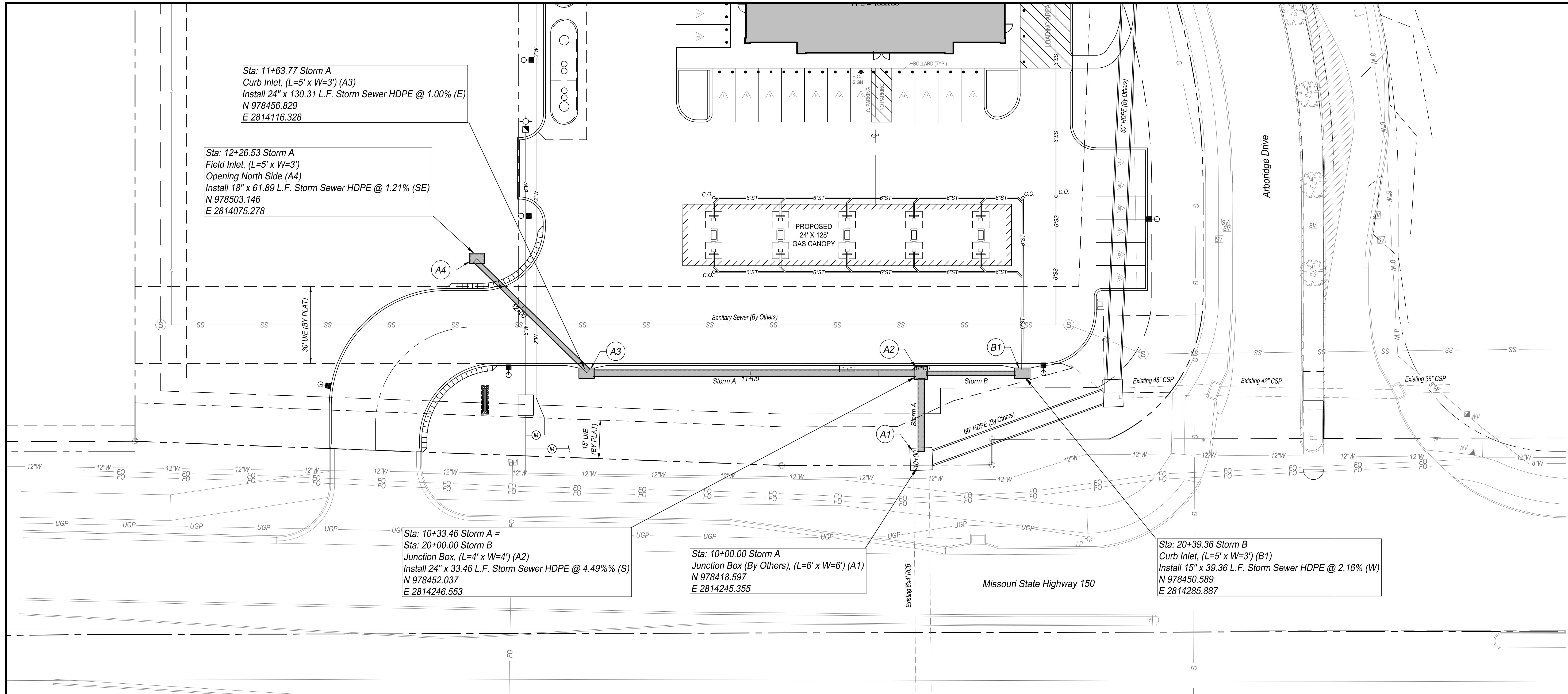
PROJECT: LEE SUMMIT, MO #4291

DRAWING INFORMATION: 10/13/23

CONSTRUCTION DIVISION

DRAINAGE MAP

C-302



SBB ENGINEERING
101 South Kansas Avenue
Topeka, Kansas 66603
Ph: (785) 215-8630 /
www.sbbeng.com

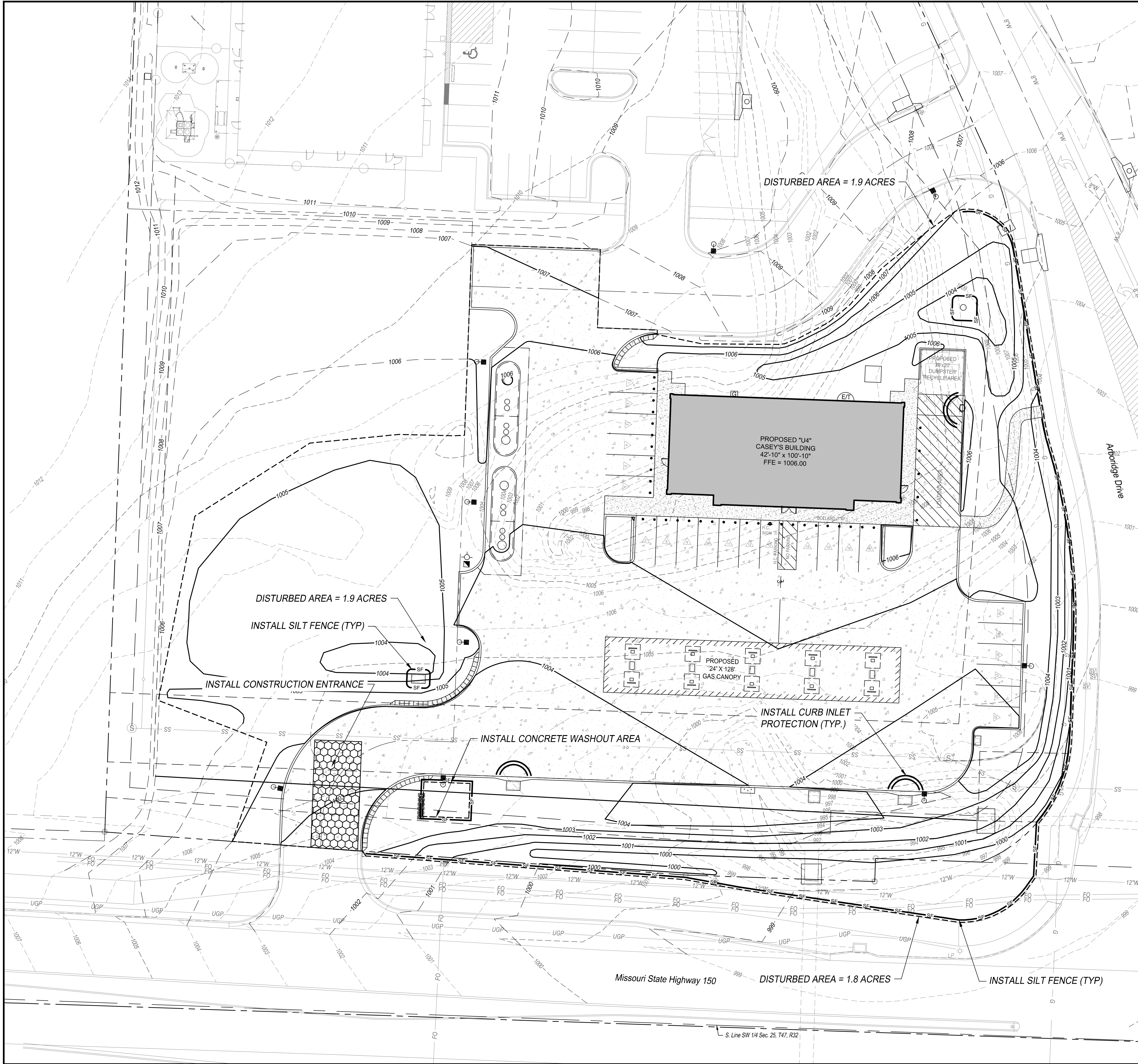
Casey's CASEY'S CONSTRUCTION DIVISION
One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100

PROJECT: LEE SUMMIT, MO #4291
HIGHWAY 150 & ARBORIDGE DRIVE
"U4" - STYLE STORE

PUBLISHED: 10.13.23
REVISIONS:

DRAWING INFORMATION: STORM SEWER LINES A & B PLAN & PROFILES

C-303



GENERAL NOTES:

1. THE LOCATION OF ALL OVERHEAD AND UNDERGROUND UTILITIES MAY VARY FROM WHAT IS INDICATED IN THESE PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH THE UTILITY OWNER TO LOCATE AND FLAG ALL UNDERGROUND UTILITIES WHETHER INDICATED OR NOT. NO EXCAVATION WILL BE PERMITTED IN THE AREA OF UNDERGROUND UTILITIES UNTIL ALL SUCH UTILITIES HAVE BEEN LOCATED AND IDENTIFIED TO THE SATISFACTION OF ALL PARTIES AND THEN ONLY WITH EXTREME CARE TO AVOID ANY POSSIBILITY OF DAMAGE TO THE UTILITY.
2. ALL DISTURBED AREAS THAT ARE NOT PAVED OR LANDSCAPED SHALL BE SEED, FERTILIZED AND MULCHED IN ACCORDANCE WITH THE SPECIFICATIONS. SOIL STABILIZATION SHALL BE COMPLETED WITHIN FOURTEEN (14) DAYS OF CLEARING OR INACTIVITY IN CONSTRUCTION.

EROSION CONTROL GENERAL NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING, MONITORING AND MAINTAINING BEST MANAGEMENT PRACTICES (BMP'S) AT ALL TIMES DURING THE CONSTRUCTION ON THIS PROPERTY.
2. PRIOR TO COMMENCEMENT OF EARTHWORK OPERATIONS, CONTRACTOR SHALL INSTALL PERIMETER WATTLES AND CONSTRUCTION ENTRANCE AS SHOWN ON THE PLAN. THE INLET PROTECTION AND ADDITIONAL WATTLES WITHIN WORK AREA SHALL BE INSTALLED IMMEDIATELY AFTER INSTALLATION OF THE INLETS AND COMPLETION OF ROUGH GRADING.
3. EROSION CONTROL MEASURES SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR NOT LESS THAN WEEKLY OR WITHIN 24 HOURS AFTER A RAINFALL EVENT OF 0.5 INCHES OR MORE. MAINTENANCE SHALL INCLUDE BUT NOT BE LIMITED TO SEDIMENT REMOVAL AND SILT FENCE REPAIR AND/OR REPLACEMENT. FIELD ADJUSTMENTS MAY BE MADE AS NECESSARY TO ENSURE OPTIMAL PERFORMANCE.
4. CONTRACTOR SHALL CLEAR ALL SURROUNDING PARKING LOTS AND STREETS OF ANY TRACKED DEBRIS BY SWEEPING OR SCRAPING THE EXISTING PAVEMENT BY THE END OF EACH WORKDAY AND THE CONTRACTOR SHALL CLEAN UP SOIL WASHED OFF THE CONSTRUCTION SITE AFTER A STORM BY THE END OF THE NEXT WORKDAY.
5. DURING ALL SOIL DISTURBING ACTIVITIES, THE CONTRACTOR WILL TAKE APPROPRIATE STEPS USING ACCEPTED CONSTRUCTION METHODS TO MINIMIZE THE TIME OF EXPOSURE OF UNPROTECTED SOIL AND OTHER CONSTRUCTION MATERIALS TO RAINFALL.
6. CONTRACTOR SHALL KEEP A WRITTEN LOG OF WHEN CONSTRUCTION ACTIVITIES BEGIN, EROSION AND SEDIMENT CONTROLS ARE INSTALLED, INSPECTED AND REPAIRED.
7. EROSION AND SEDIMENT CONTROL MEASURES SHALL NOT BE REMOVED UNTIL GRASS COVER HAS BEEN ESTABLISHED OR AS DIRECTED BY ENGINEER.
8. ALL AREAS SHALL BE GRADED TO FINISHED GRADE PRIOR TO SEEDING AND MULCHING. ALL AREAS NOT PART OF THE HARDSCAPE OR OTHER NOTED LANDSCAPING SHALL BE PERMANENTLY SEED, FERTILIZED AND MULCHED.
9. ALL WATTLES SHALL BE STRAW WATTLE WS-12 AS MANUFACTURED BY NORTH AMERICAN GREEN, OR APPROVED EQUAL. THE WATTLES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR MAY USE WATTLES AND SILT FENCE INTERCHANGEABLY IN AREAS IDENTIFIED ON THE DRAWINGS.
10. THE CONTRACTOR HAS THE OPTION TO MODIFY THE LOCATION OF THE CONSTRUCTION ENTRANCE TO FIT THE PREFERRED WORK PATTERN.

WASTE DISPOSAL:

1. WASTE MATERIALS: ANY WASTE AND OTHER UNUSABLE MATERIALS WILL BE REMOVED FROM THE SITE ON A REGULAR BASIS AND PROPERLY DISPOSED OF IN AN APPROVED SITE.
2. CHEMICAL WASTE: ALL CHEMICAL WASTE MATERIALS WILL BE COLLECTED AND STORED IN A TIGHTLY SEALED METAL OR OTHER CHEMICAL RESISTANT CONTAINER. THE CONTAINER WILL MEET ALL LOCAL AND ANY STATE SOLID WASTE MANAGEMENT REGULATIONS. THE WASTE MATERIALS AND ALL DISPOSABLE MATERIALS WILL BE TRANSPORTED TO A COMMERCIAL CHEMICAL DISPOSAL FACILITY CAPABLE OF EITHER RECYCLING OR PROPERLY DISPOSING OF THE POLLUTANTS IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS. THE STATE WASTE MANAGEMENT SECTION CONTACT NUMBER IS (573) 751-3176.
3. HAZARDOUS WASTE: ALL HAZARDOUS WASTE MATERIALS, SUCH AS OIL FILTERS, PETROLEUM PRODUCTS, EQUIPMENT MAINTENANCE FLUIDS AND PAINTS, SHALL NOT BE STORED ON SITE AND WILL BE DISPOSED OF IN THE MANNER SPECIFIED BY LOCAL, STATE AND/OR FEDERAL REGULATIONS. IF THERE ARE QUESTIONS REGARDING THE PROPER HANDLING OF HAZARDOUS WASTES THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE MISSOURI DEPARTMENT OF NATURAL RESOURCES HAZARDOUS WASTE PROGRAM AT (573) 751-3176.
4. SANITARY WASTE: PORTABLE TOILET FACILITIES WILL BE PROVIDED ON THE PROJECT SITE AND SERVICED BY THE PROVIDER ON A REGULAR BASIS.

MATERIAL MANAGEMENT PRACTICES:

GOOD HOUSEKEEPING: THE FOLLOWING GOOD HOUSEKEEPING PRACTICES WILL BE FOLLOWED ON-SITE DURING CONSTRUCTION.

1. ALL MATERIALS STORED ON-SITE WILL BE STORED IN A NEAT, ORDERLY MANNER IN ORIGINAL CONTAINERS IF APPROPRIATE.
2. PRODUCTS WILL BE KEPT IN THEIR ORIGINAL CONTAINERS WITH THE ORIGINAL MANUFACTURER'S LABELS.
3. MANUFACTURERS' RECOMMENDATIONS FOR PROPER USE AND DISPOSAL WILL BE FOLLOWED.

SPILL PREVENTION:

1. PETROLEUM PRODUCTS: ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTATIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE. PETROLEUM PRODUCTS WILL BE STORED IN TIGHTLY SEALED CONTAINERS WHICH ARE CLEARLY LABELED.
2. FERTILIZERS: FERTILIZERS USED WILL BE APPLIED ONLY IN THE MINIMUM AMOUNTS RECOMMENDED BY THE MANUFACTURER. ONCE APPLIED, FERTILIZER WILL BE WORKED INTO THE SOIL TO LIMIT EXPOSURE TO STORM WATER. FERTILIZER SHALL NOT BE STORED ON-SITE.
3. CONCRETE TRUCKS: CONCRETE TRUCKS ARE LIMITED TO A DESIGNATED AREA TO WASH OUT OR DISCHARGE SURPLUS CONCRETE OR DRUM WASH WATER ON THE SITE. PROPER SIGNAGE SHALL BE INSTALLED AND MAINTAINED ON SITE DEFINING DIRECTIONS TO AND LOCATIONS OF THE SPECIFIED WASH OUT AREA. IF A WASH OUT AREA CANNOT BE DESIGNATED OR MAINTAINED ON SITE, CONCRETE WASH OUT AND DISPOSAL MAY BE PROHIBITED AT THE OWNERS DISCRETION.

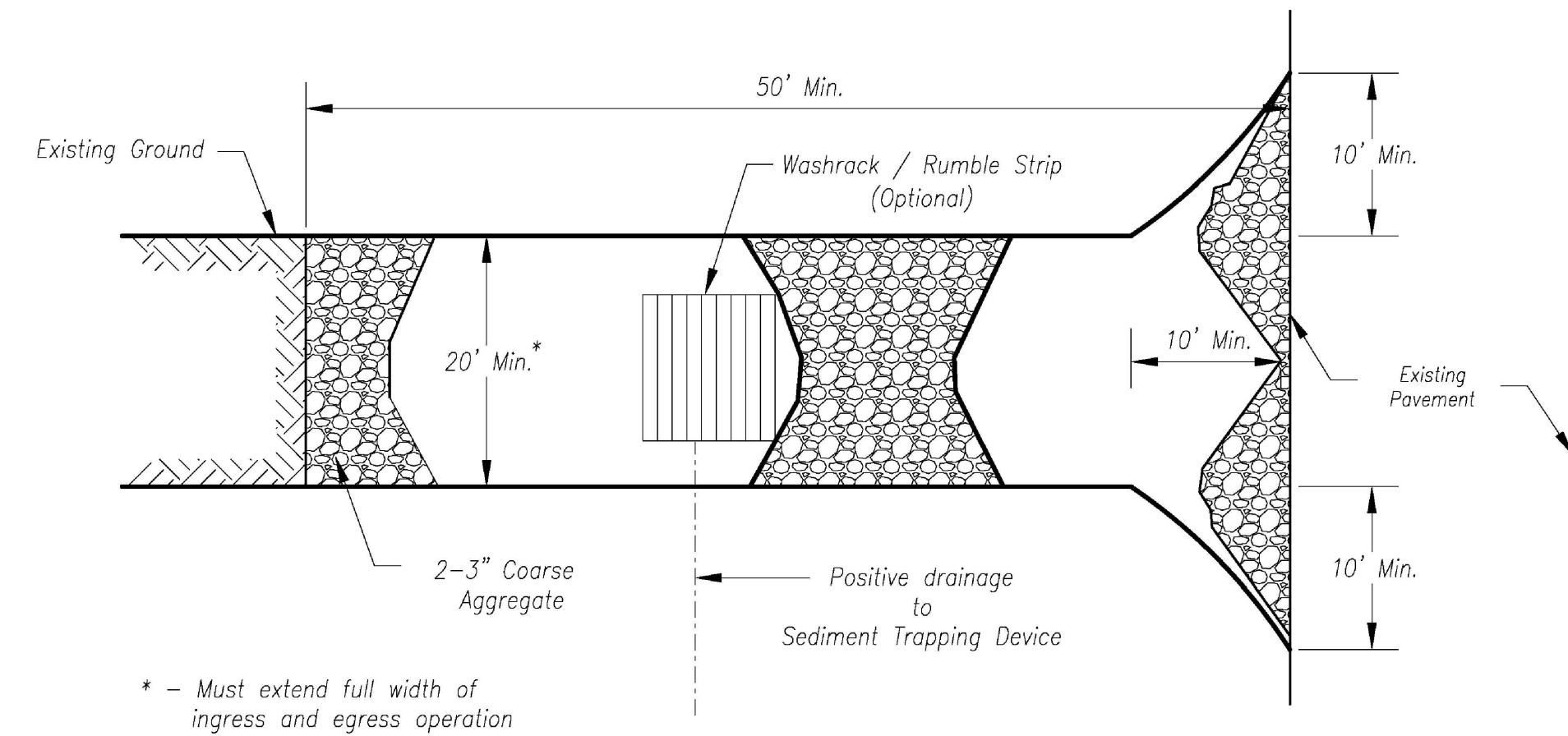
LEGEND:

- 1005— PROPOSED CONTOURS
- - -1005- - - PROPOSED CONTOURS BY OTHERS
- - -1004- - - EXISTING CONTOURS
- - -1005- - - EXISTING CONTOURS
- - -SF- - - SILT FENCE
- - - CURB INLET PROTECTION
- - - DISTURBED AREA = 1.9 ACRES
- - - CONSTRUCTION ENTRANCE
- - - CONCRETE WASHOUT AREA

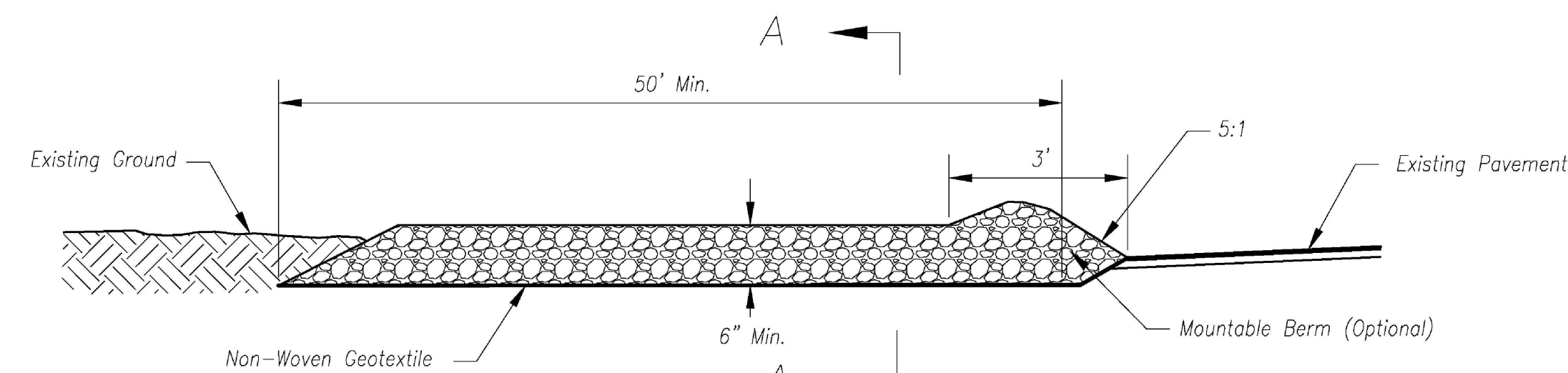


S88 Proj. No. 21-288

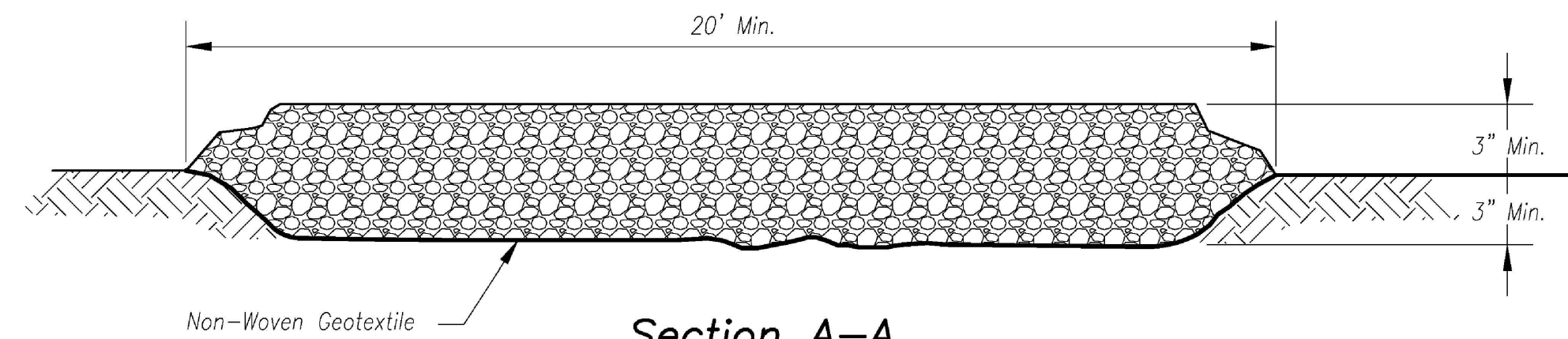
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100			
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	DATE: 10.13.23	REVIEWED BY:	EROSION CONTROL PLAN
DRAWING INFORMATION: CONSTRUCTION DIVISION		CHECKED BY:	C-401



Plan View
Not to Scale



Side Elevation
Not to Scale



Section A-A
Not to Scale

Notes for Construction Entrance:

1. Avoid locating on steep slopes, at curves on public roads, or downhill of disturbed area.
2. Remove all vegetation and other unsuitable material from the foundation area, grade, and crown for positive drainage.
3. If slope towards the public road exceeds 2%, construct a 6- to 8-inch high ridge with 3H:1V side slopes across the foundation approximately 15 feet from the edge of the public road to divert runoff from it.
4. Install pipe under the entrance if needed to maintain drainage ditches along public roads.
5. Place stone to dimensions and grade as shown on plans. Leave surface sloped for drainage.
6. Divert all surface runoff and drainage from the entrance to a sediment control device.
7. If conditions warrant, place geotextile fabric on the graded foundation to improve stability.

Maintenance for Construction Entrance:

1. Reshape entrance as needed to maintain function and integrity of installation. Top dress with clean aggregate as needed.

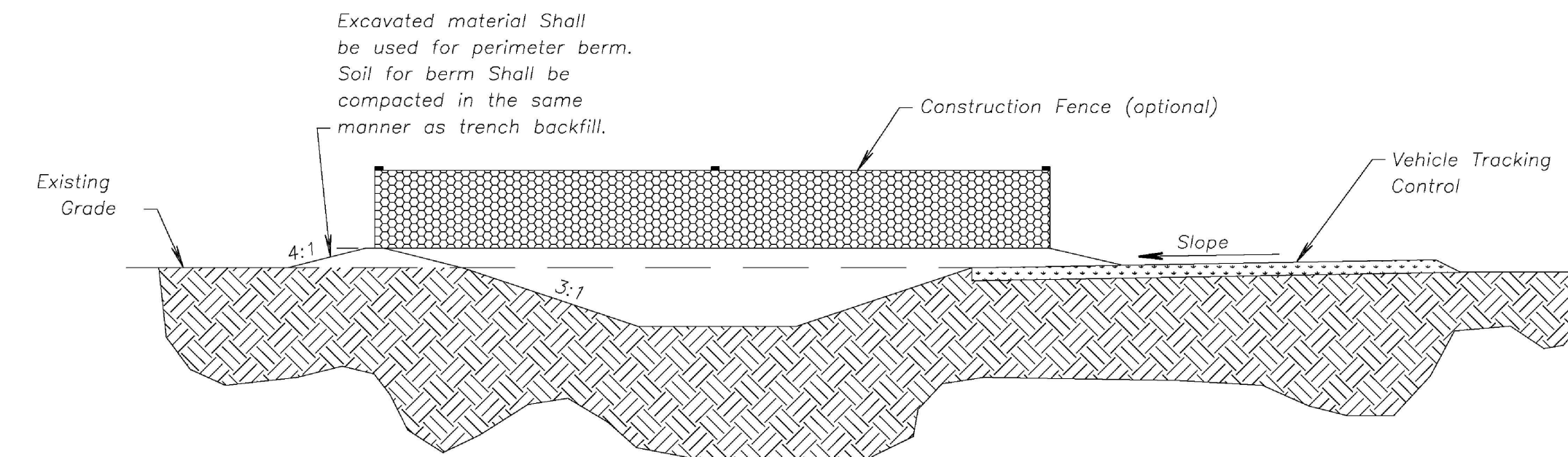
CONSTRUCTION ENTRANCE

Notes for Concrete Washout:


1. Concrete washout areas shall be installed prior to any concrete placement on site.
2. Concrete washout area shall include a flat subsurface pit sized relative to the amount of concrete to be placed on site. The slopes leading out of the subsurface pit shall be 3:1. The vehicle tracking pad shall be sloped towards the concrete washout area.
3. Vehicle tracking control is required at the access point to all concrete washout areas.
4. Signs shall be placed at the construction site entrance, washout area and elsewhere as necessary to clearly indicate the location(s) of the concrete washout area(s) to operators of concrete truck and pump rigs.
5. A one-piece impervious liner may be required along the bottom and sides of the subsurface pit in sandy or gravelly soils.

Maintenance for Concrete Washout:



1. Concrete washout materials shall be removed once the materials have filled the washout to approximately 75% full.
2. Concrete washout areas shall be enlarged as necessary to maintain capacity for wasted concrete.
3. Concrete washout water, wasted pieces of concrete and all other debris in the subsurface pit shall be transported from the job site in a water-tight container and disposed of properly.
4. Concrete washout areas shall remain in place until all concrete for the project is placed.
5. When concrete washout areas are removed, excavations shall be filled with suitable compacted backfill and topsoil, any disturbed areas associated with the installation, maintenance, and/or removal of the concrete washout areas shall be stabilized.



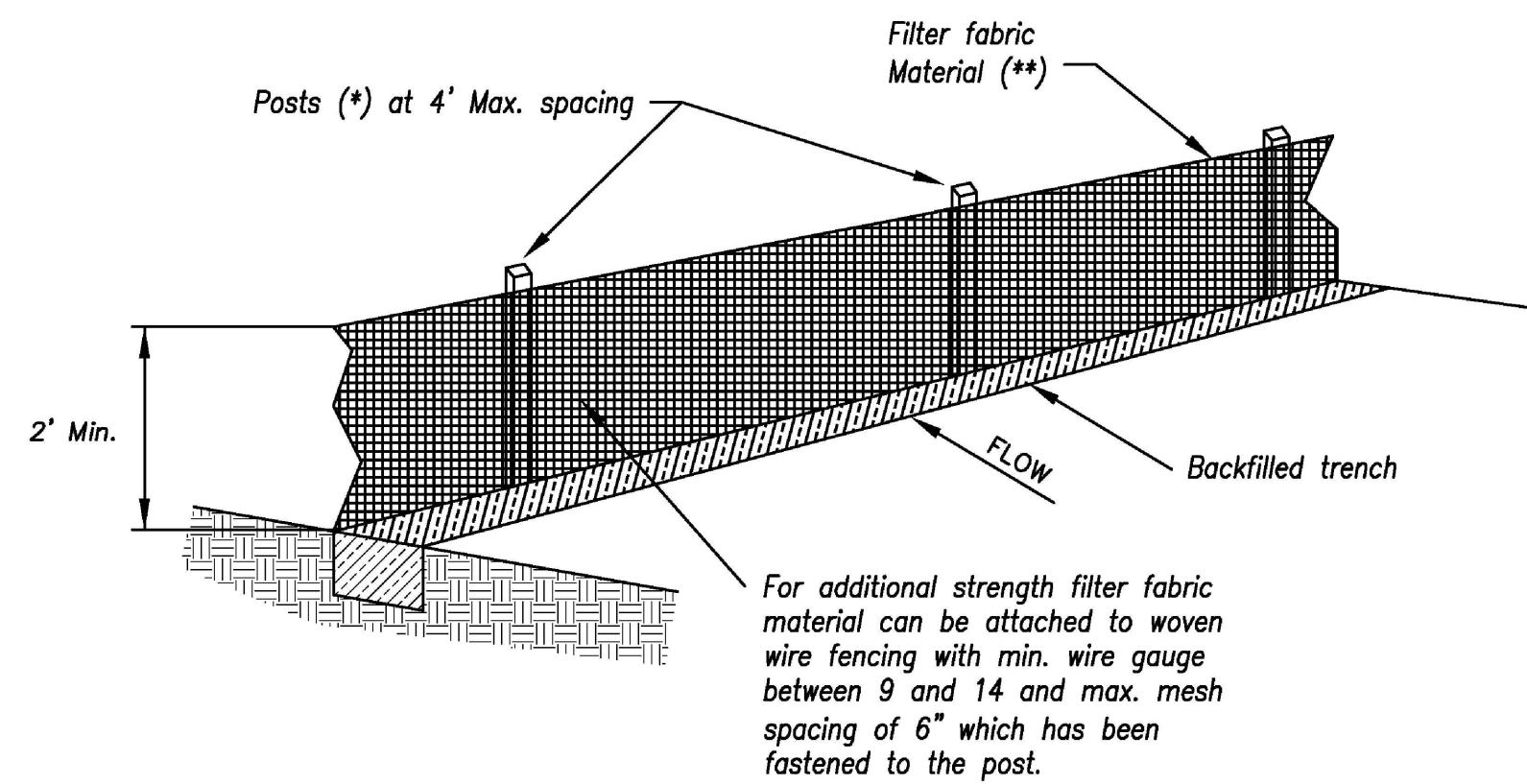
CONCRETE WASHOUT

AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT	STANDARD DRAWING NUMBER ESC-01
	ADOPTED: 10/24/2016

Construction Entrance modified from 2015 Overland Park Standard Details for Erosion and Sediment Control; Concrete Washout modified from 2009 City of Great Bend Standard Drawings.

 Casey's 	
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100	
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVISIONS: _____ _____ _____ _____ _____
DRAWING INFORMATION: CONSTRUCTION DIVISION DRAWN BY: _____ CHECKED BY: _____	EROSION CONTROL DETAILS C-402

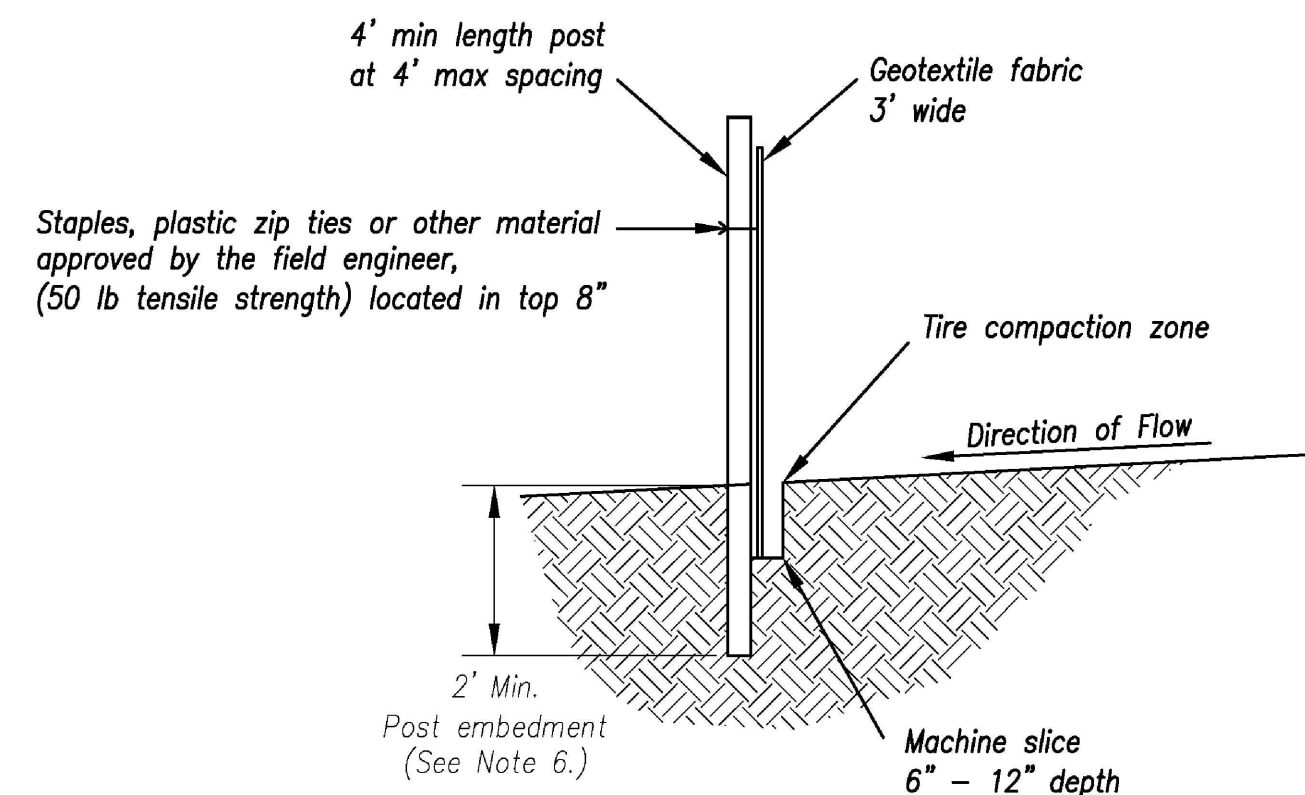
SBB Proj. No. 21-288



(*) POSTS

- MIN. LENGTH 4'
- HARDWOOD 1 3/8" x 1 3/8"
- NO.2 SOUTHERN PINE 2 5/8" x 2 5/8"
- STEEL 1.33 LB/FT

(**) - Geotextile Fabric shall meet the requirements of AASHTO M288



SILT FENCE DETAILS
Not to Scale

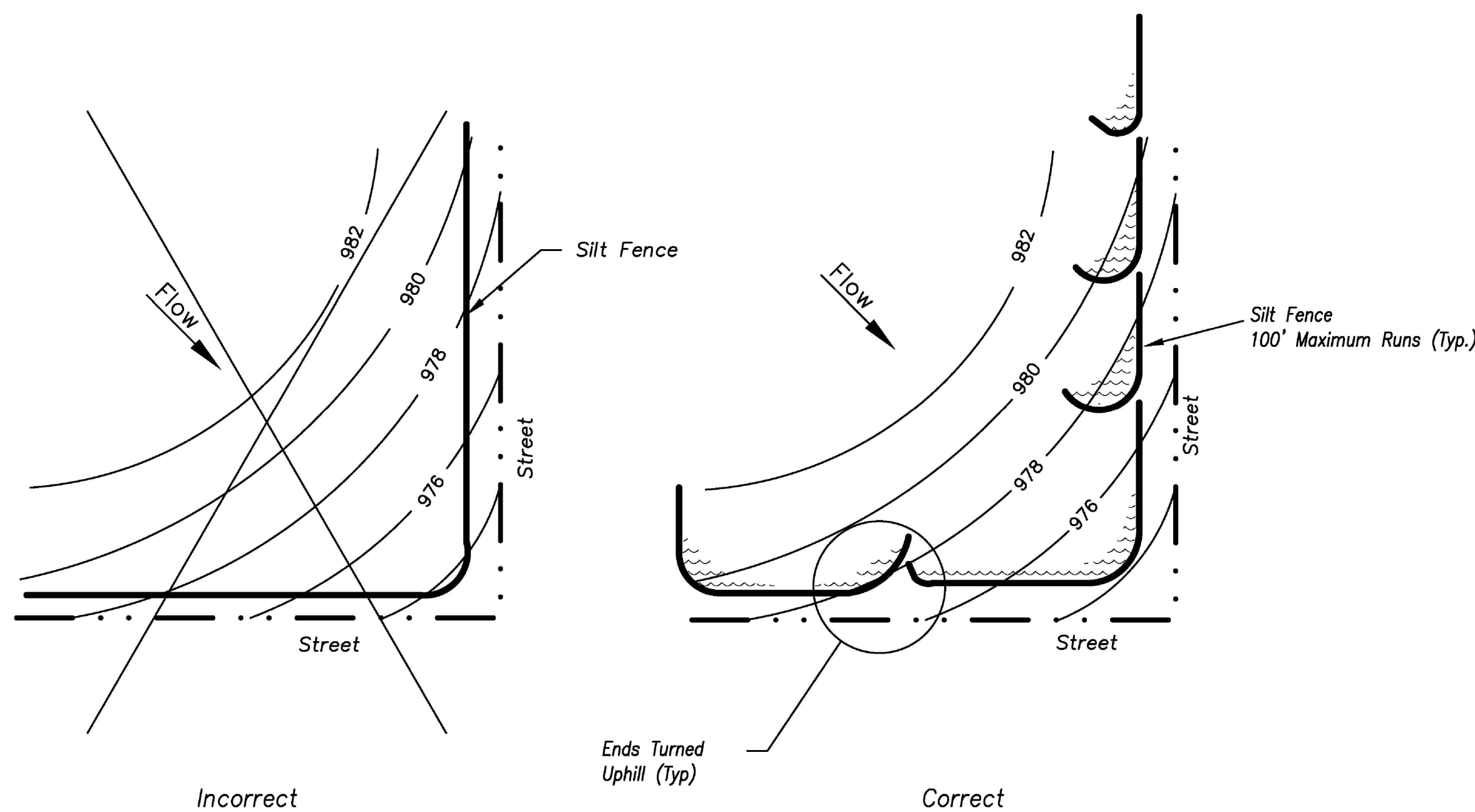
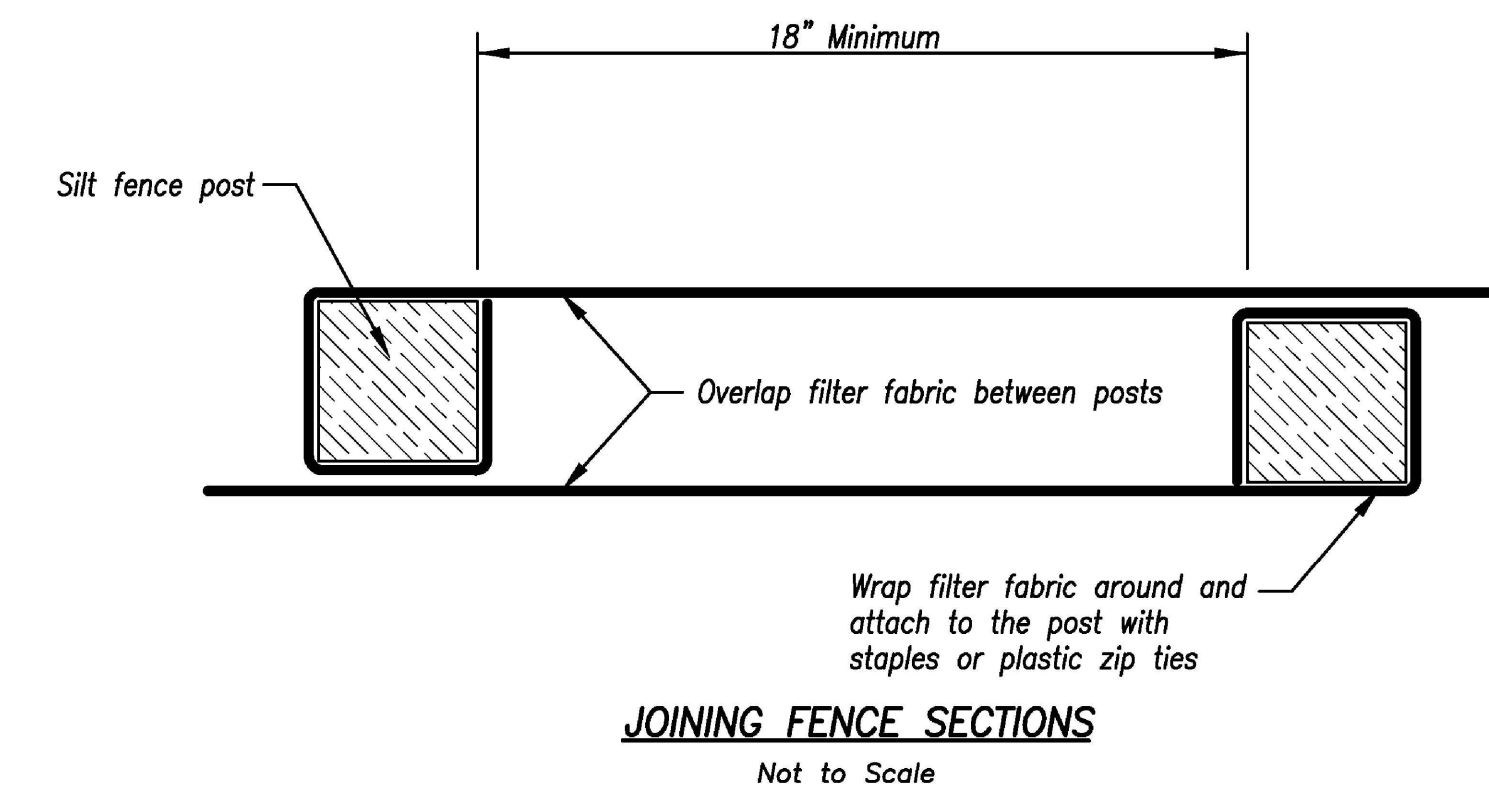
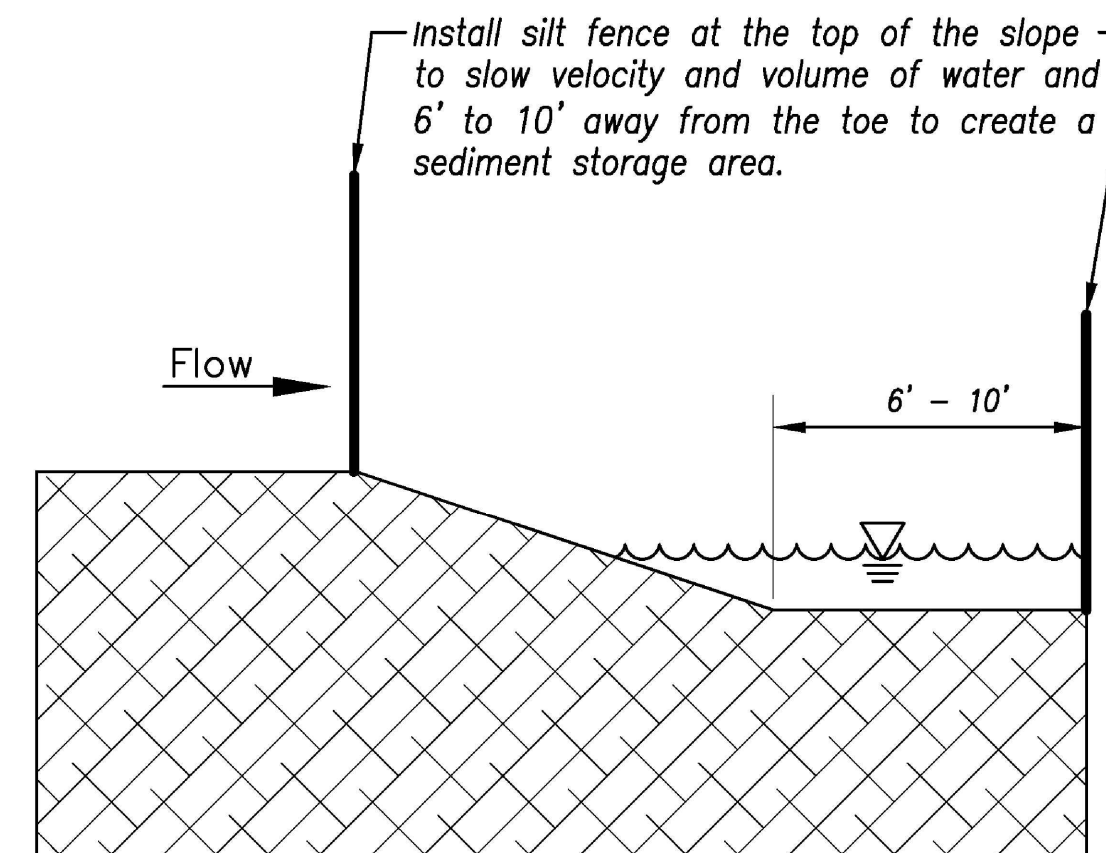


Figure A

SILT FENCE LAYOUT
Not to Scale



Notes:

1. In order to contain water, the ends of the silt fence must be turned uphill (Figure A).
2. Long perimeter runs of silt fence must be limited to 100'. Runs should be broken up into several smaller segments to minimize water concentrations (Figure A).
3. Long slopes should be broken up with intermediate rows of silt fence to slow runoff velocities.
4. Attach fabric to upstream side of post.
5. Install posts a minimum of 2' into the ground.
6. Trenching will only be allowed for small or difficult installation, where slicing machine cannot be reasonably used.

Maintenance:

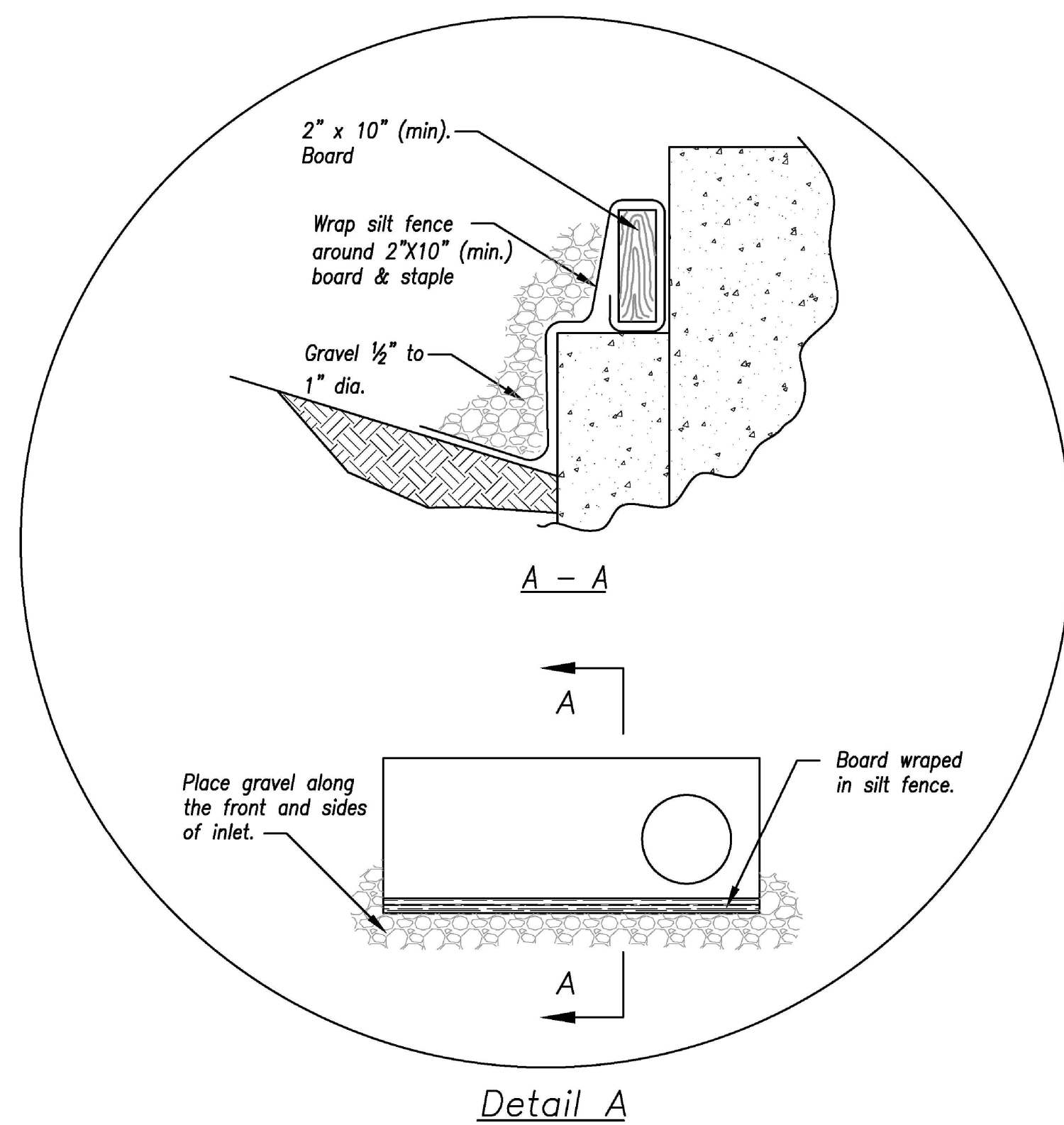
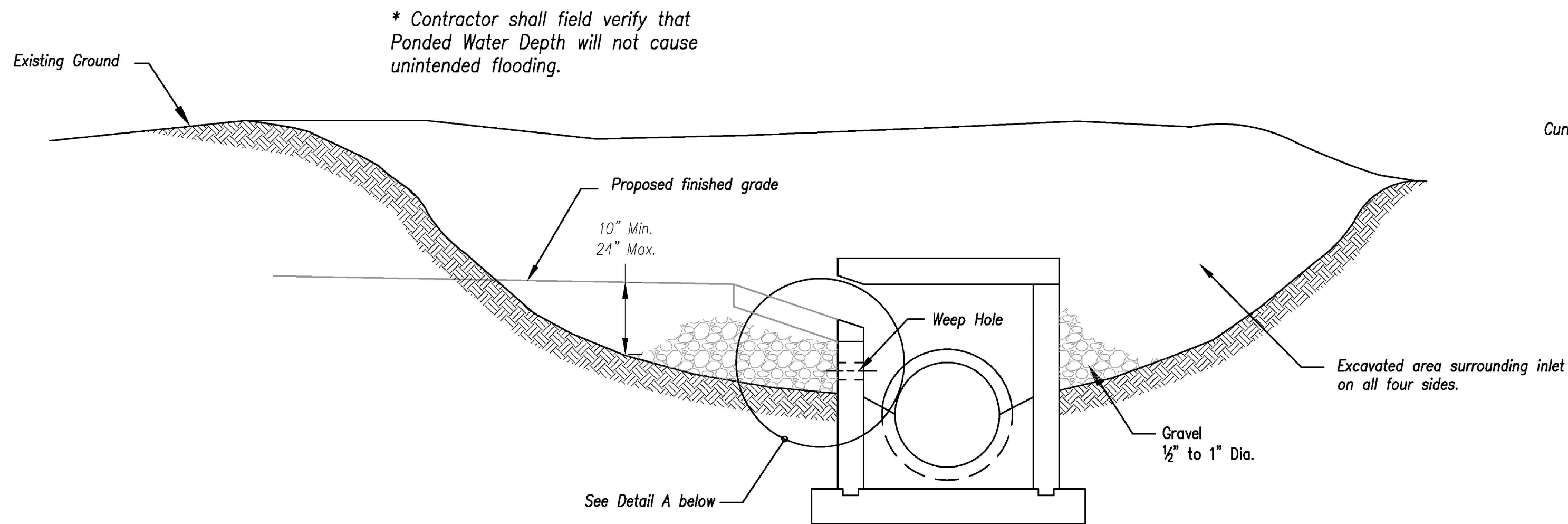
1. Remove and dispose of sediment deposits when the deposit approaches 1/2 the height of silt fence.
2. Repair as necessary to maintain function and structure.

AMERICAN PUBLIC WORKS ASSOCIATION Kansas City Metro Chapter 		KANSAS CITY METRO CHAPTER
SILT FENCE		STANDARD DRAWING NUMBER ESC-03 ADOPTED: 10/24/2016

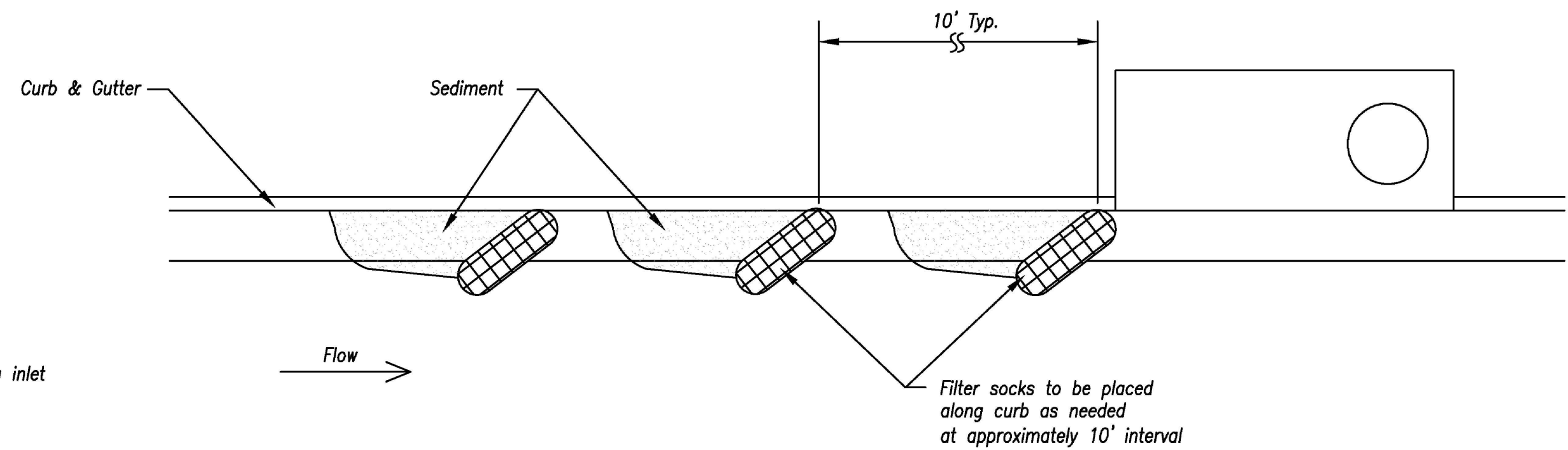
Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.

CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100		
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVISIONS: _____ _____ _____ _____ _____	DRAWING INFORMATION: CONSTRUCTION DIVISION DRAWN BY: _____ CHECKED BY: _____
		EROSION CONTROL DETAILS C-403

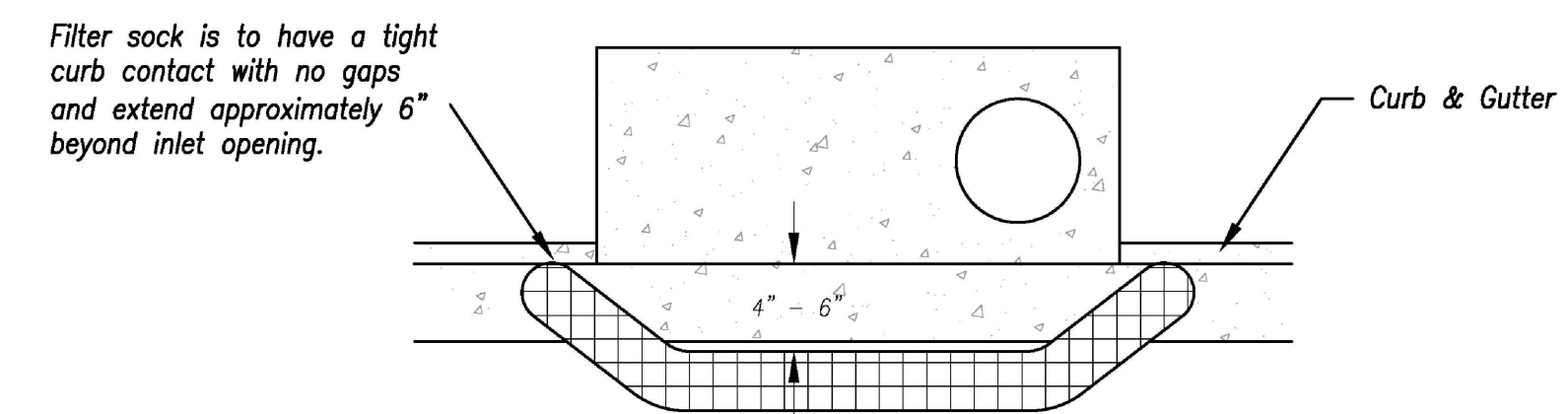
SBB Proj. No. 21-288



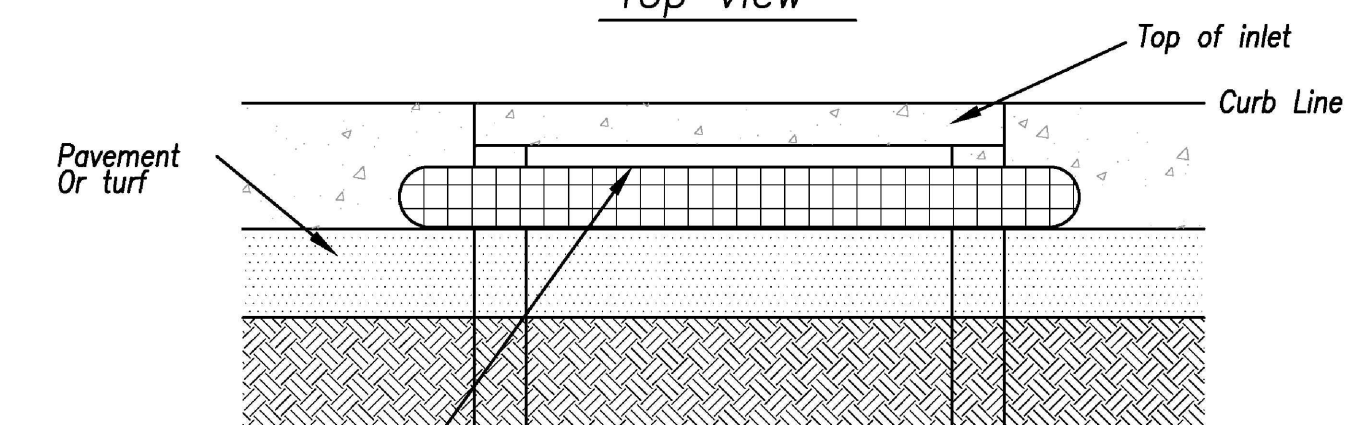
EARLY STAGE CURB INLET
(Open Box and Prior to Pouring Curb and Inlet Throat)



On Grade Curb Inlet Protection



Top View



Front View

Sump Inlet Sediment Filter

LATE STAGE CURB INLET
(After Pouring Curb and Inlet Throat)


Notes:



1. Immediately following inlet construction and prior to construction of curb and inlet throat, protect inlet opening by installing 2" X 10" (min.) board wrapped in silt fence. Structures shall have excavated storage area on all four sides to allow settling of sediment (Early Stage Curb Inlet).
2. When inlet is completed and curb poured, filter socks or approved equal should be used (Late Stage Curb Inlet). Straw wattles are not approved for curb inlet use.
3. Contractor to field verify ponding water shall not create a traffic hazard.

Maintenance:

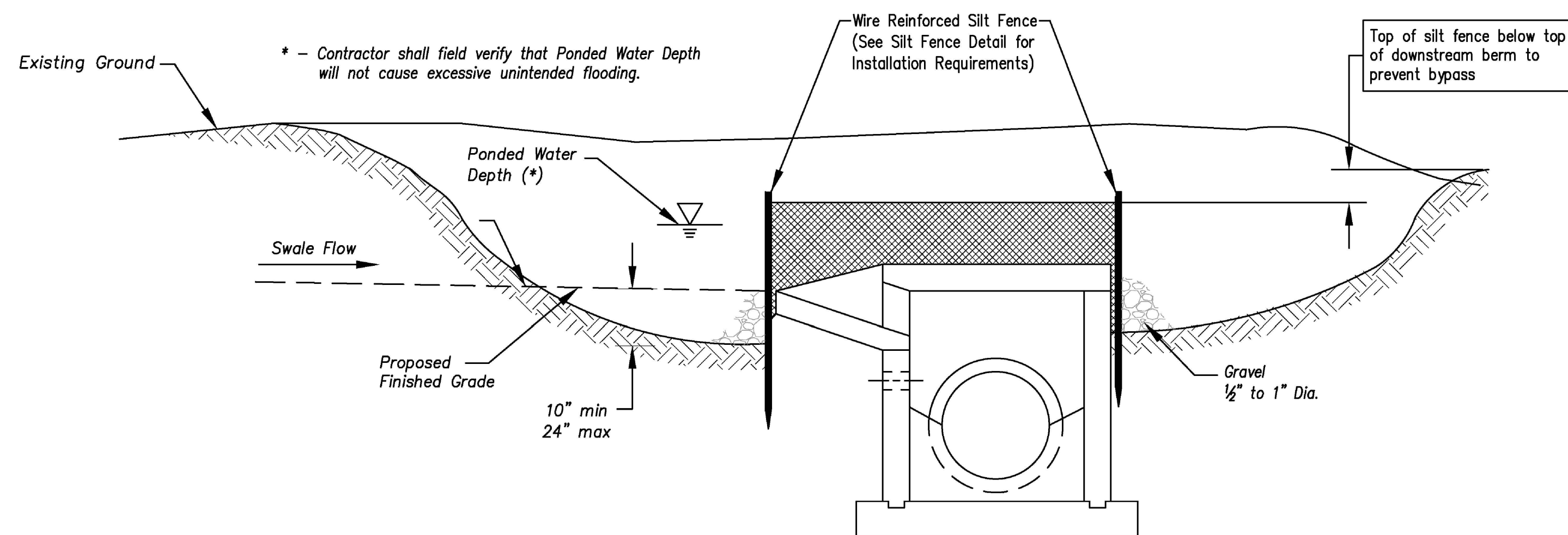
1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
3. Repair or replace as necessary to maintain function and integrity of installation.

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.

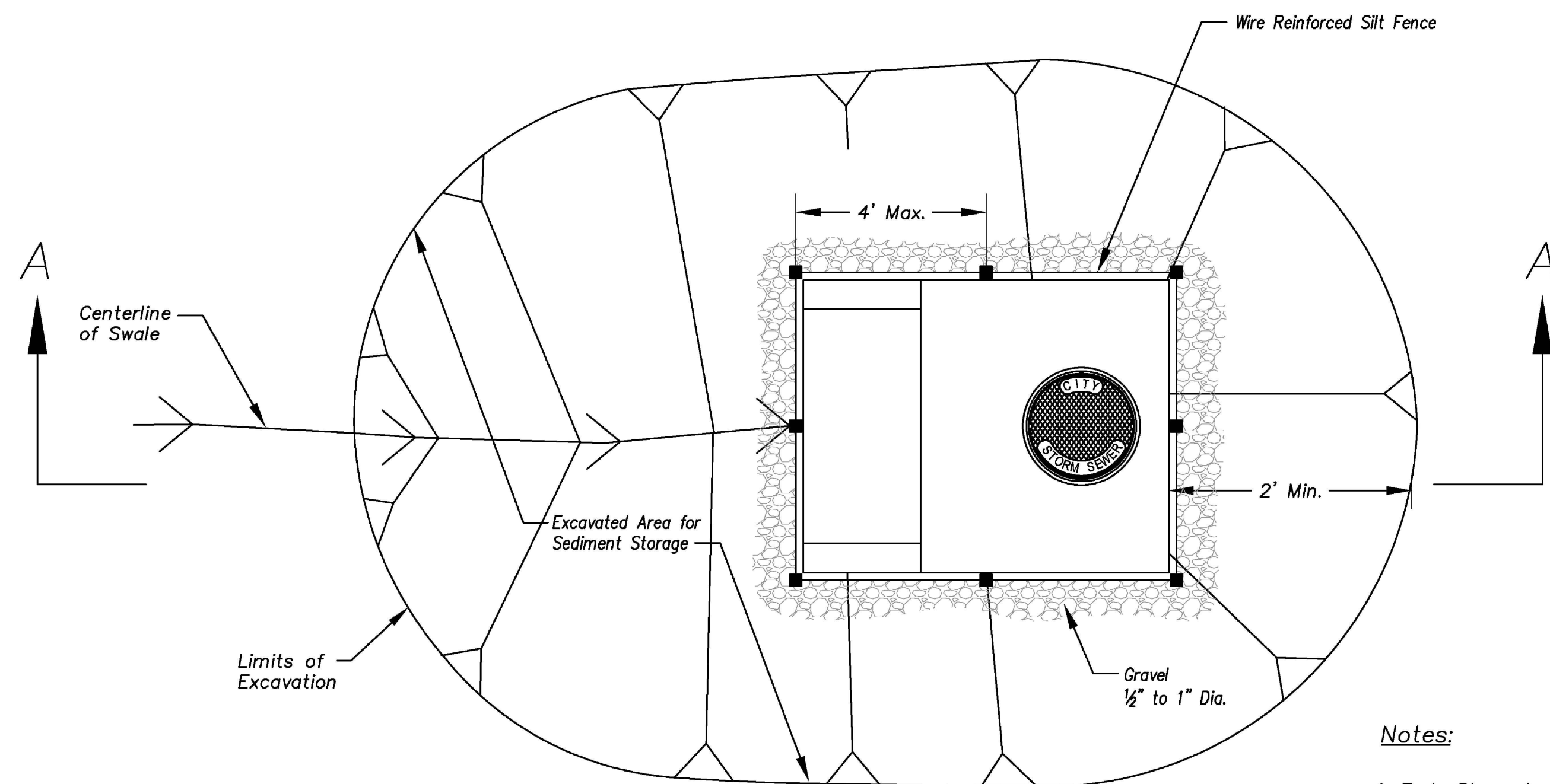
AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
CURB INLET PROTECTION	STANDARD DRAWING NUMBER ESC-06 ADOPTED: 10/24/2016

 	
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100	
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVIEWED ON:
DRAWING INFORMATION: CONSTRUCTION DIVISION	EROSION CONTROL DETAILS
DRAWN BY:	CHECKED BY:
C-404	

SBB Proj. No. 21-288



Section A-A
Not to Scale

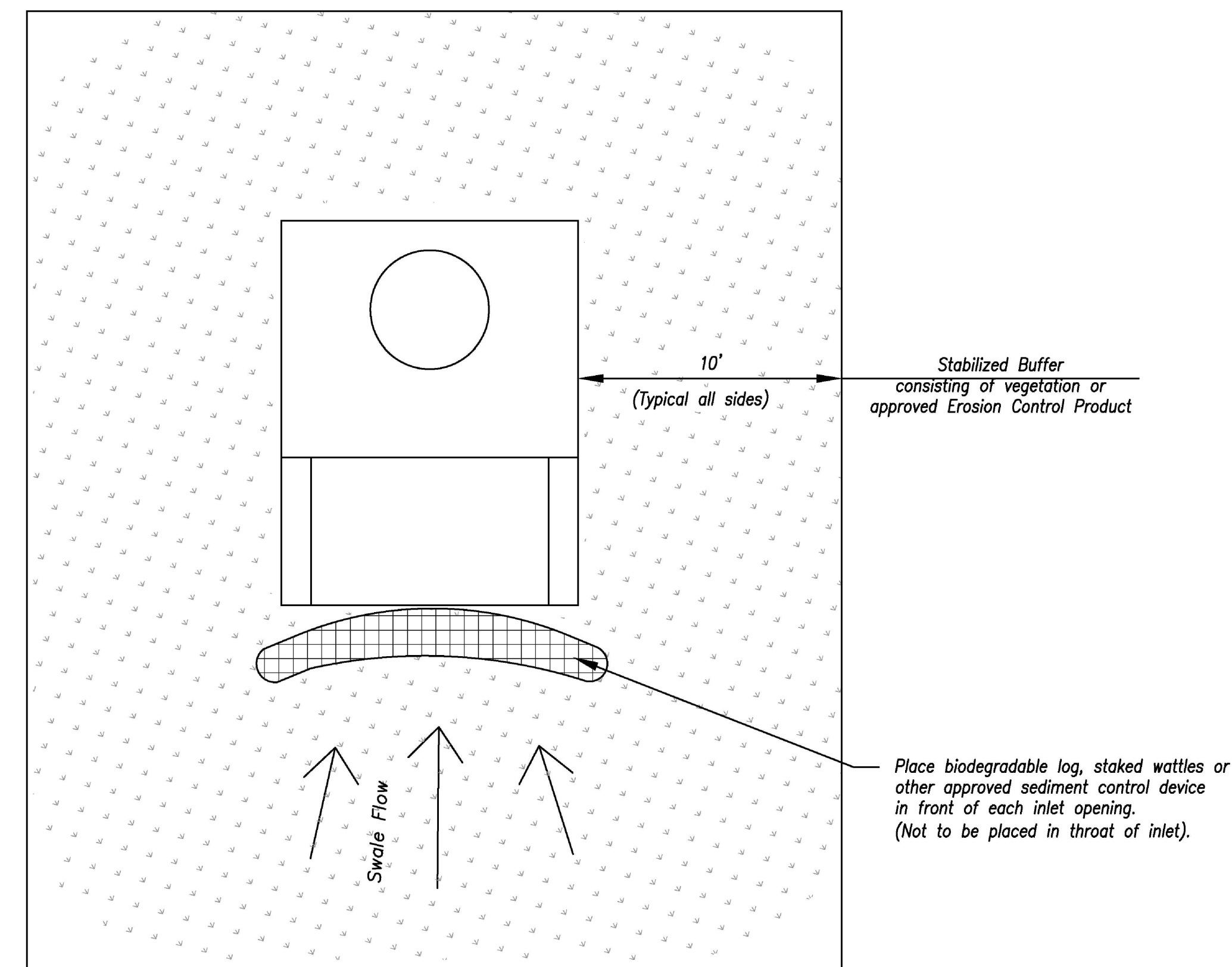


Plan
Not to Scale

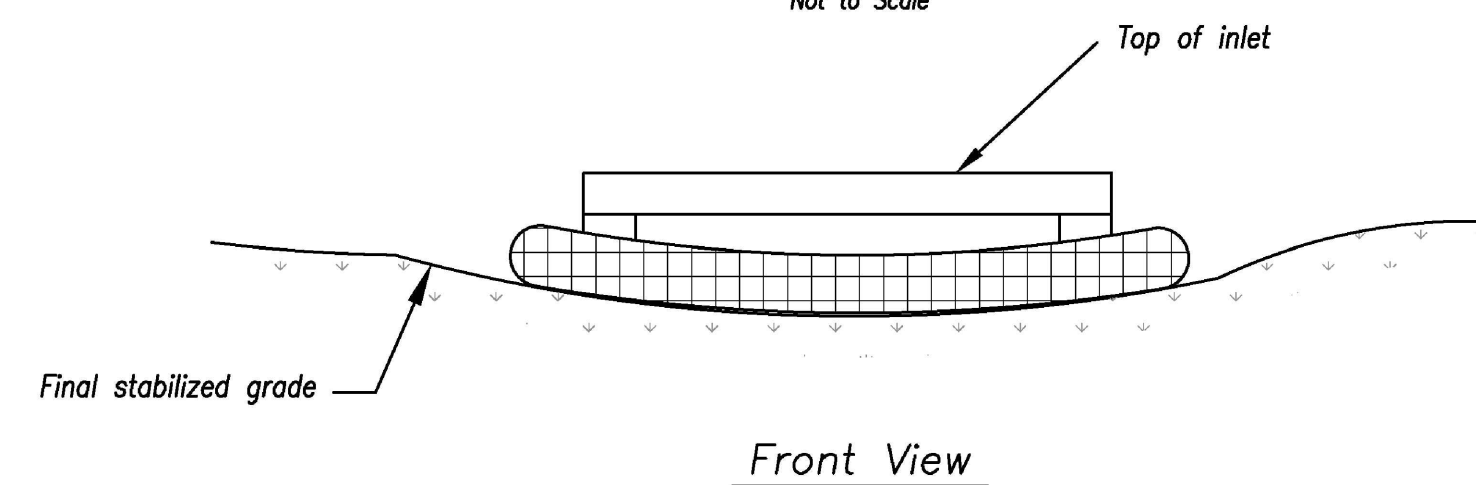
EARLY STAGE AREA INLET
(All open boxes and inlets not at final grade)

Notes:

1. Early Stage Area Inlet Sediment Barrier to be installed immediately after inlet or junction box is constructed.
2. Silt fence shall remain in place until excavated area is removed and Late Stage Area Inlet is being installed.
3. Backfill excavated area ONLY after final grading of the site. Stabilization of the site is to immediately follow.
4. Wire reinforced silt fence may be used in place of silt fence attached to wood frame.



Plan
Not to Scale

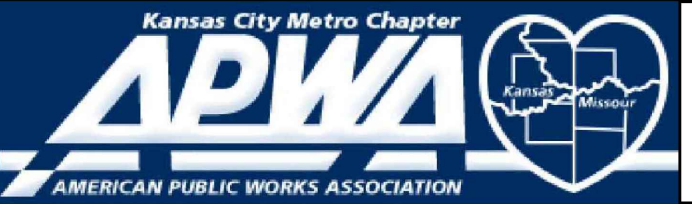


Front View



LATE STAGE AREA INLET
(Area inlets at final grade and existing inlets)

Maintenance:

1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
3. Repair or replace as necessary to maintain function and integrity of installation.

AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
AREA INLET AND JUNCTION BOX PROTECTION	STANDARD DRAWING NUMBER ESC-07 ADOPTED: 10/24/2016

Modified from 2015 Overland Park Standard Details
for Erosion and Sediment Control.

 									
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100									
PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVISIONS: <table border="1"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>								
DRAWING INFORMATION: CONSTRUCTION DIVISION DRAWN BY: _____ CHECKED BY: _____	EROSION CONTROL DETAILS <div style="border: 1px solid black; padding: 5px; text-align: center; font-weight: bold;">C-405</div>								

SBB Proj. No. 21-288


$$3/4" = 1'-0"$$

***SUBGRADE AGGREGATE SECTION TO BE DESIGNED
BASED ON SITE SOILS AND USE***



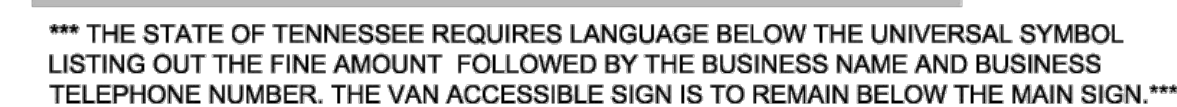
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Not to Scale


$$1/2'' = 1'-0''$$

$$1/2'' = 1'-0''$$

$$3/16'' = 1'-0''$$

$$1/4'' = 1'-0''$$

Concrete:

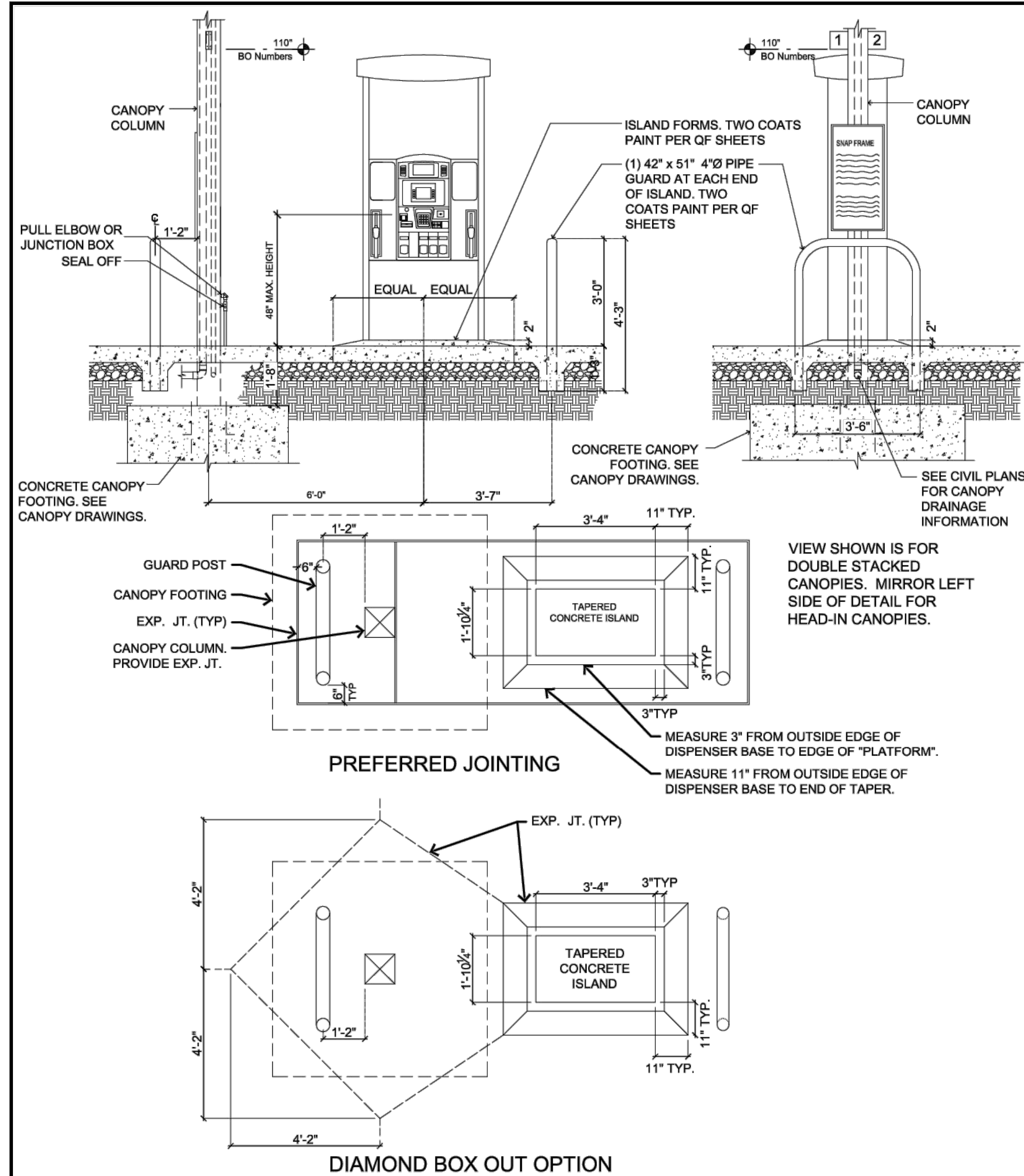
1. All concrete and reinforcing work shall conform to the latest edition of the American Concrete Institute's "Standard Building Code Requirements for Reinforced Concrete," (ACI 318) and "Specifications for Structural Concrete for Buildings," (ACI 301).
2. Concrete shall use type II cement. Concrete mix designs shall meet the following requirements:

If Contractor desires to increase slump above allowable limits to facilitate placement or pumping, this shall be done utilizing a superplasticizer approved by the Ready Mix Designer at a dosage rate provided by Ready Mix Designer.

3. The Contractor shall reject any concrete that exceeds the slump limits noted above (prior to adding superplasticizer) or concrete that can not be placed within ninety (90) minutes of batch time.
4. No aluminum shall be placed in concrete.
5. All concrete is reinforced unless specifically noted as Unreinforced. Reinforce all concrete not otherwise shown with the same reinforcing as in similar sections or areas.
6. During hot weather (80 degrees F and above), the Contractor shall comply with the recommendations ACI-305 "Hot Weather Concrete". During cold weather (40 degrees and below), the Contractor shall comply with the recommendations of ACI-306 "Cold Weather Concreting".
7. The concrete mix designs are to be submitted as a formal submittal to the Owner for acceptance PRIOR to concrete being delivered to the site.
8. Verify with local authorities the required thickness of poured concrete for approaches and parking lot.

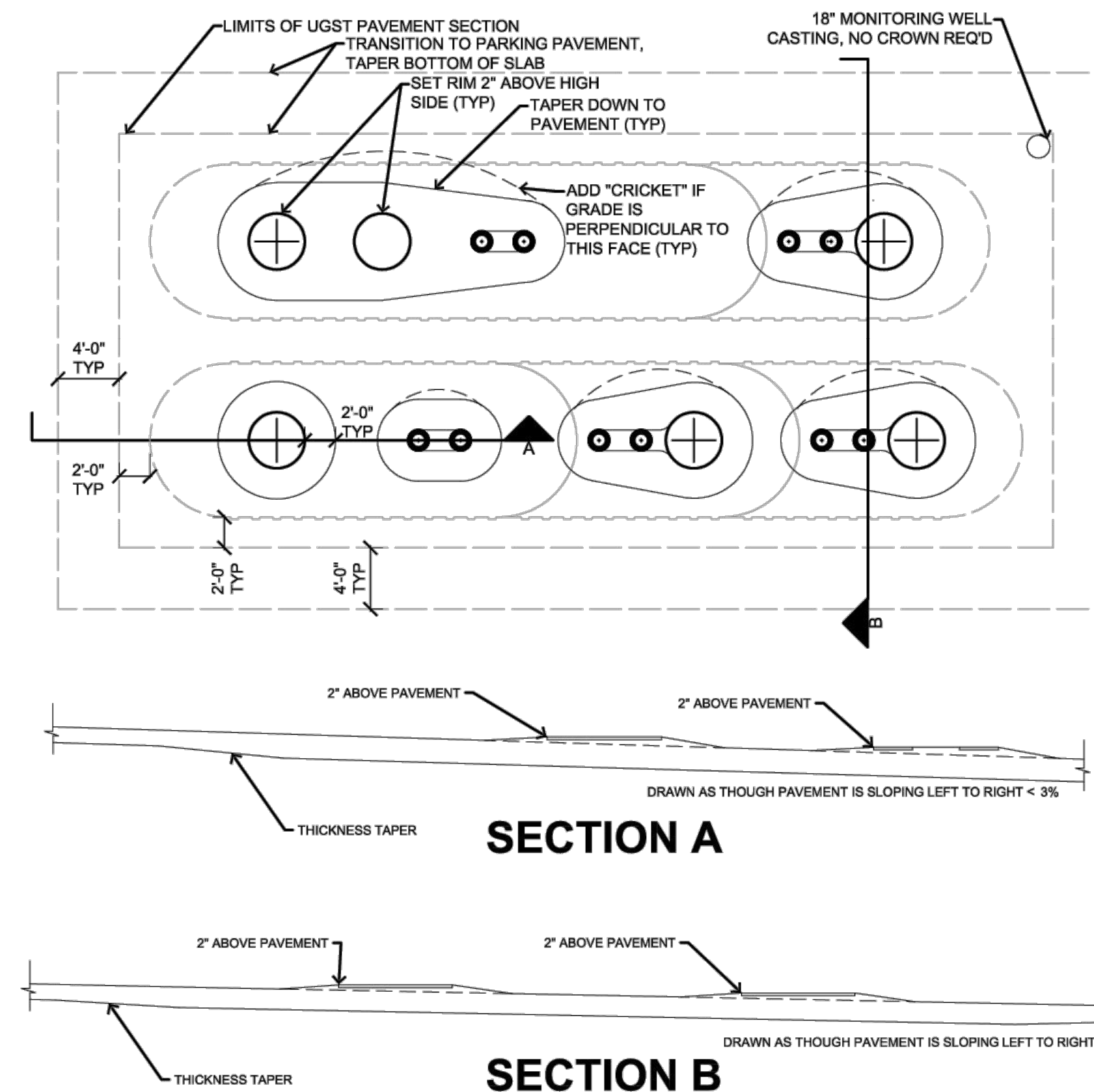
Sub-Base And Aggregate:

1. Sub-base Course Under Exterior Concrete Paved Surfaces: Spread and compact sub-base in 6"-9" lifts compacted to 98% Standard Proctor.
2. Aggregate Backfill as follows or approved equal by Casey's:
 - 2.1. Building Slab - 3/4" to 1-1/2" max. clean stone
 - 2.2. Building Sidewalk - 3/4" to 1-1/2" max. clean stone
 - 2.3. Pavement - 1" to 1-1/2" max. stone with fines
 - 2.4. Sidewalk Not Adjacent to Building - Same as pavement
 - 2.5. Tank pit - See QF sheets



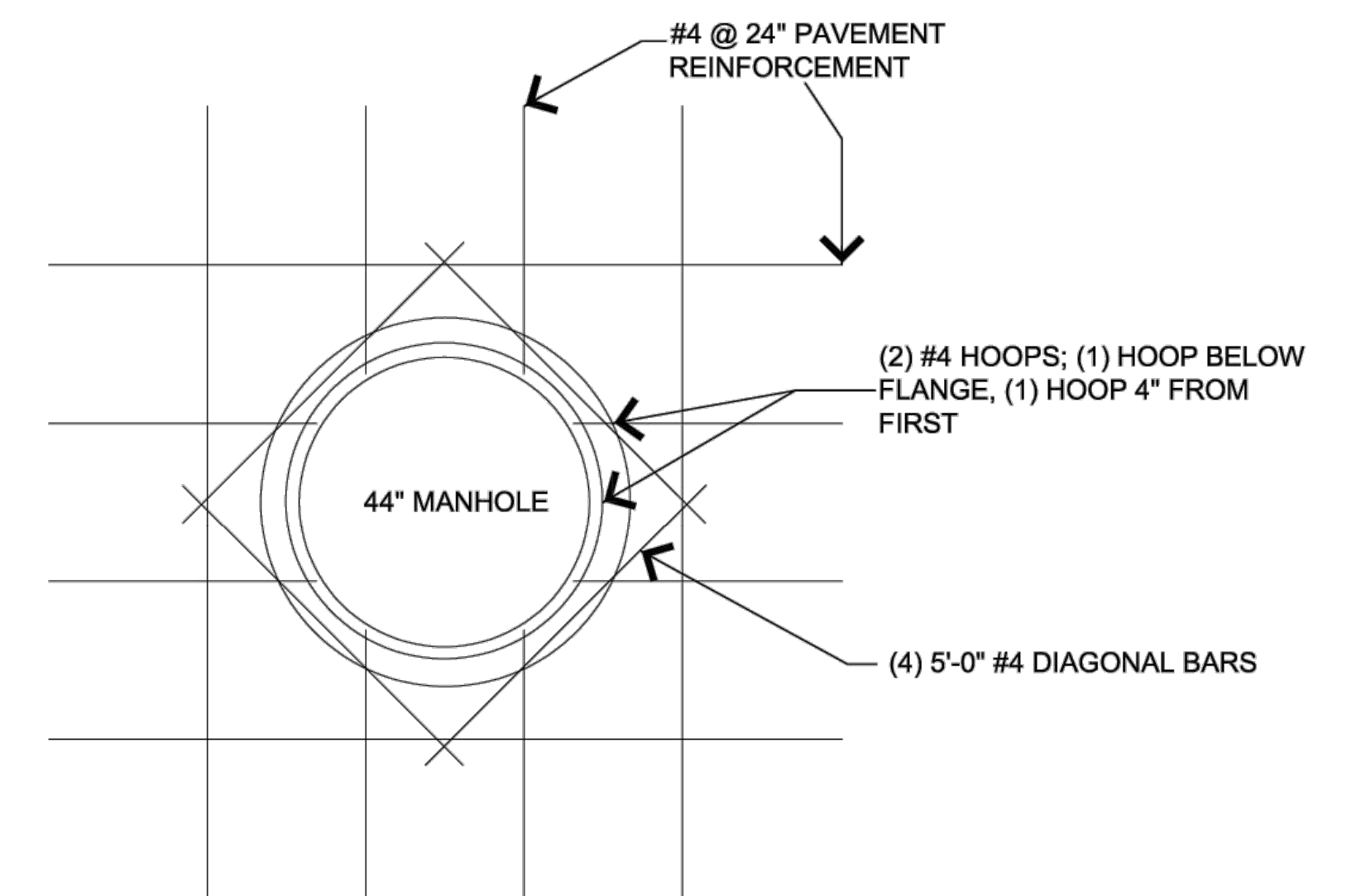
1 GAS TAPERED FUEL ISLAND (PREFERRED OPTION)

Not to Scale



4 UGST PAVING FOR PAVEMENT SLOPE <3%

Not to Scale

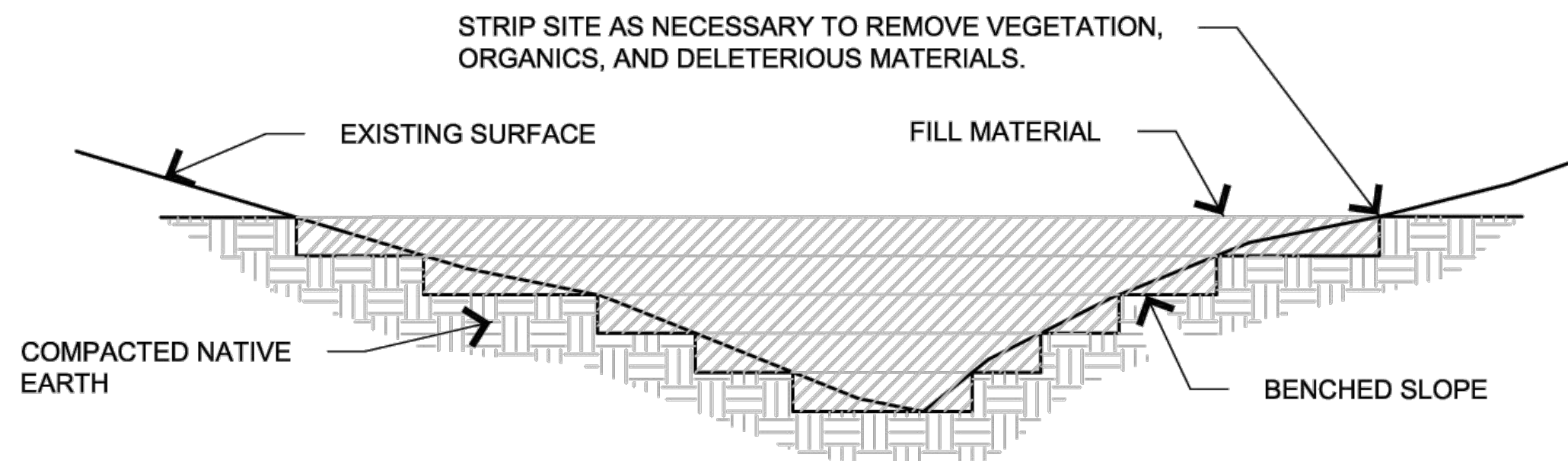


General Notes:

1 ALL REINFORCING IS CENTER OF SLAB.

6 UGST Manhole Detail

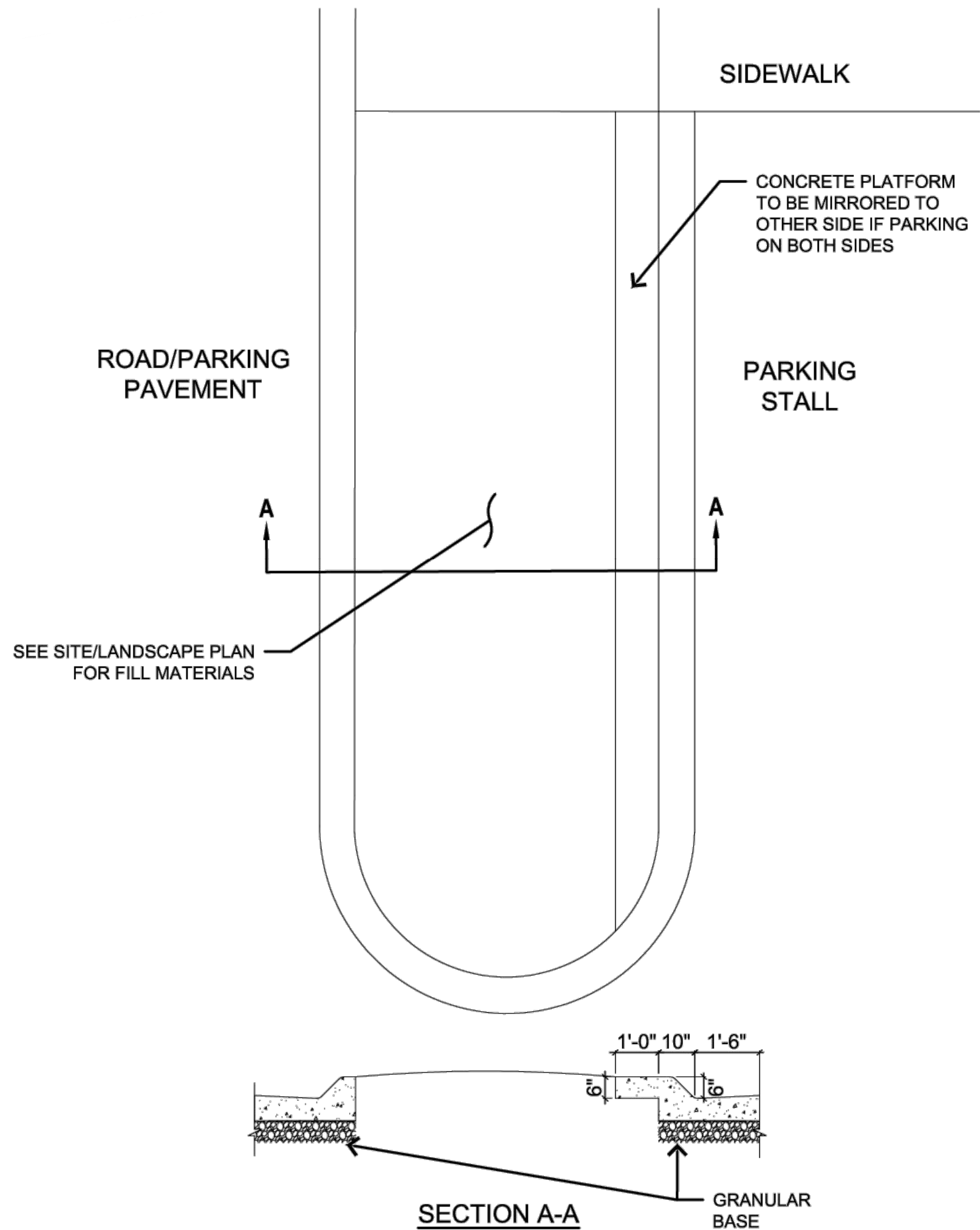
1/2" = 1'-0"



- NOTES:
1. BENCH SLOPES TO MAINTAIN VERTICAL SIDE SLOPES AT FILL LIMITS +/- 3' VERTICAL TO +/- 8' HORIZONTAL.
 2. COMPACTION SHALL BE IN 8" LIFTS TO 95% STANDARD PROCTOR OR OTHERWISE SPECIFIED IN GEOTECHNICAL REPORT.
 3. SCARIFY ORIGINAL GROUND AND BLEND WITH FIRST LIFT ON EACH BENCH.
 4. ALL FILL MATERIALS, INCLUDING FILL GENERATED ON-SITE, SHALL BE OF SUITABLE MATERIAL. UNSUITABLE FILL MATERIAL INCLUDES, BUT IS NOT LIMITED TO, SOIL THAT CONTAINS LARGE ROCKS, TREE AND PLANT ROOTS, CONCRETE, TRASH, ETC.

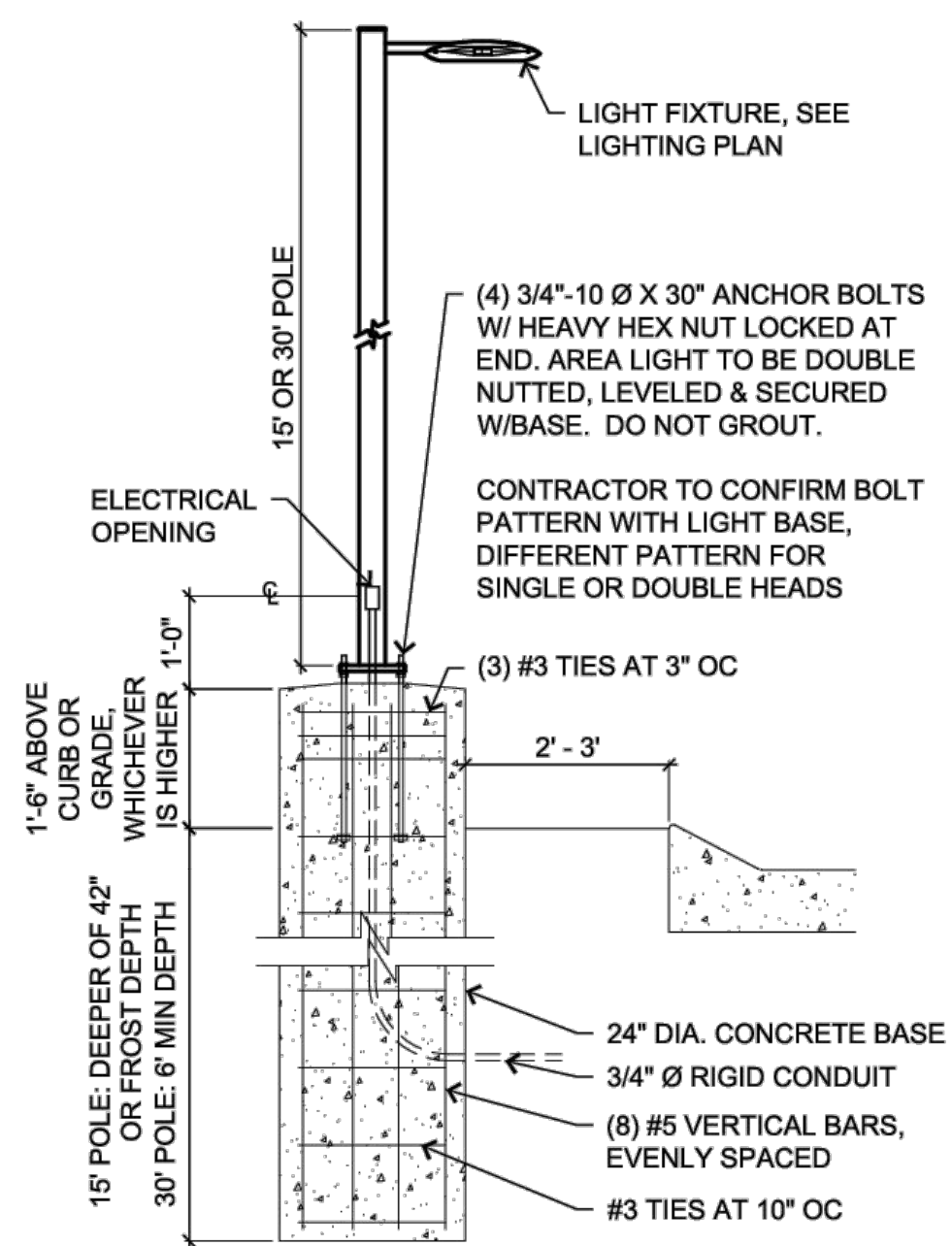
2 TYPICAL FILL SECTION

Not to Scale



7 PARKING LANDSCAPE ISLAND DETAIL

3/8" = 1'-0"



8 AREA LIGHTING

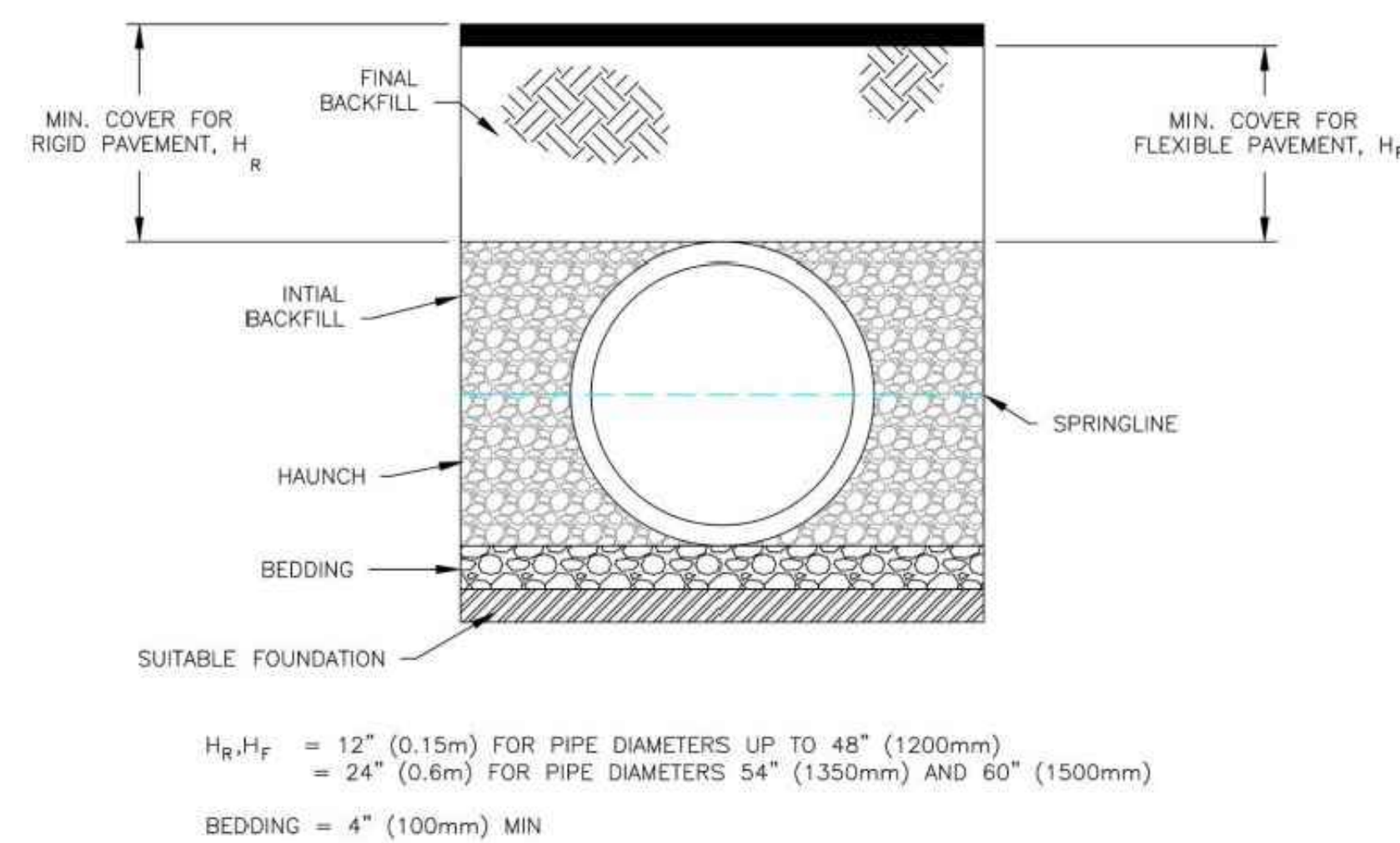
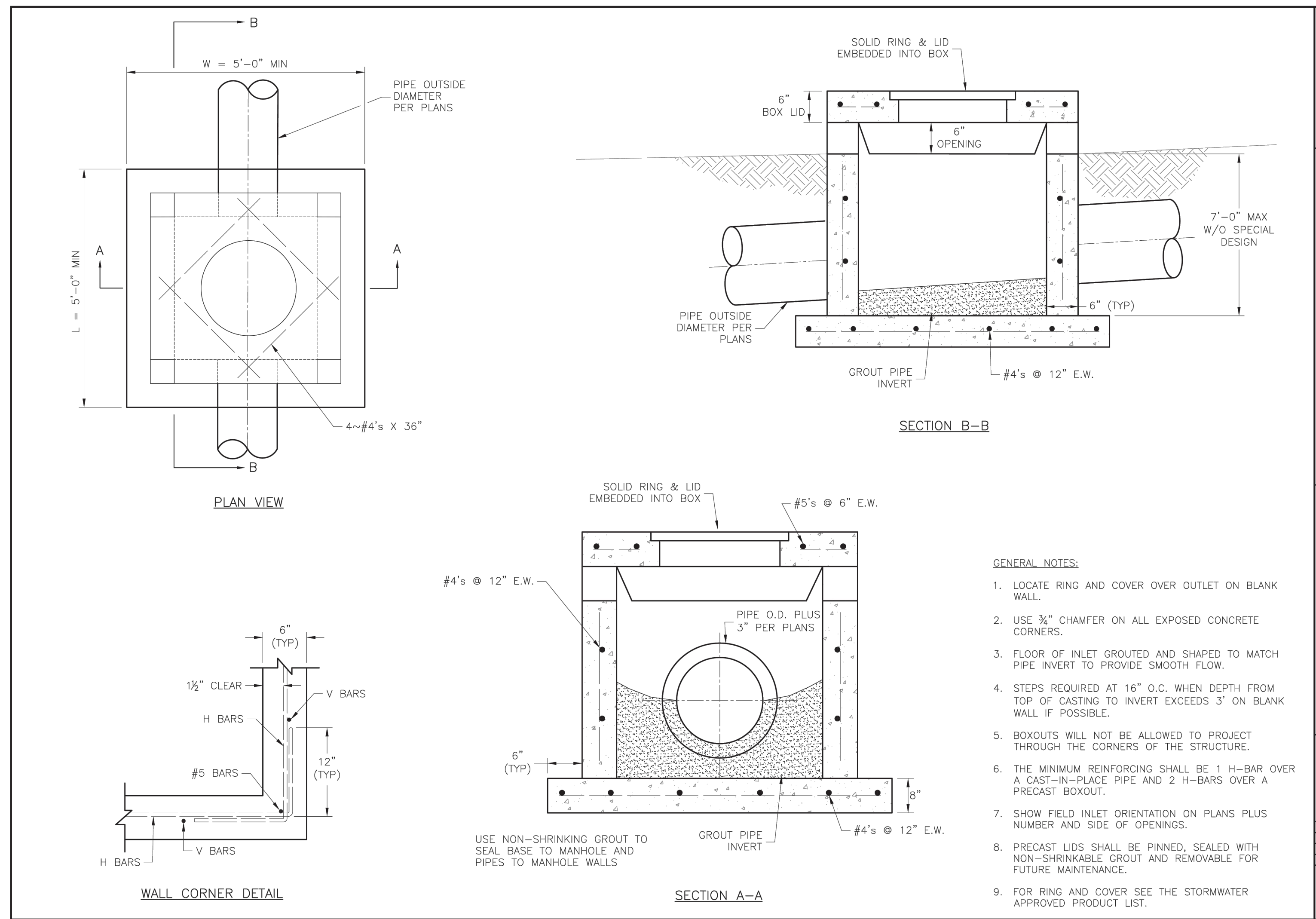
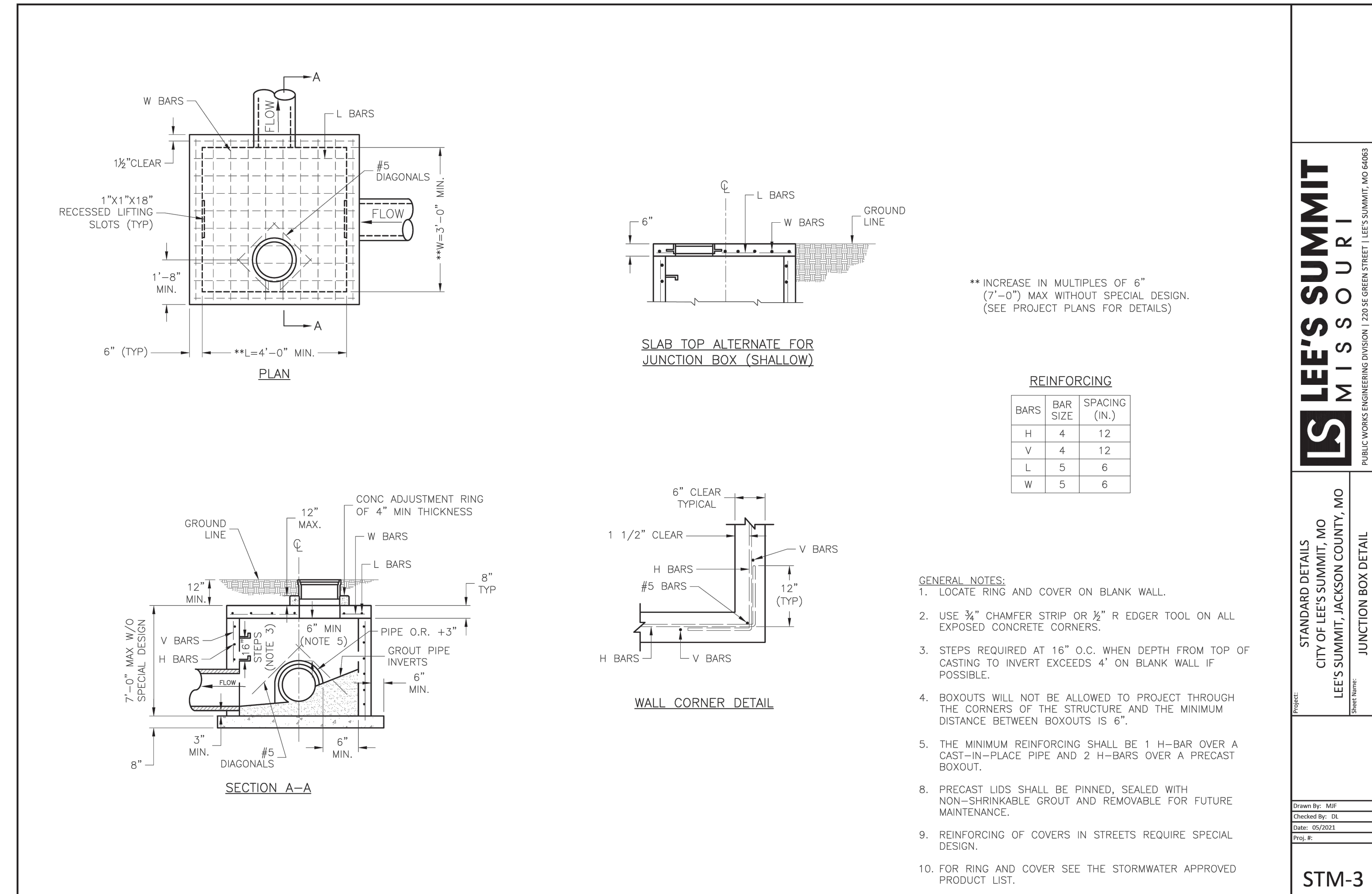
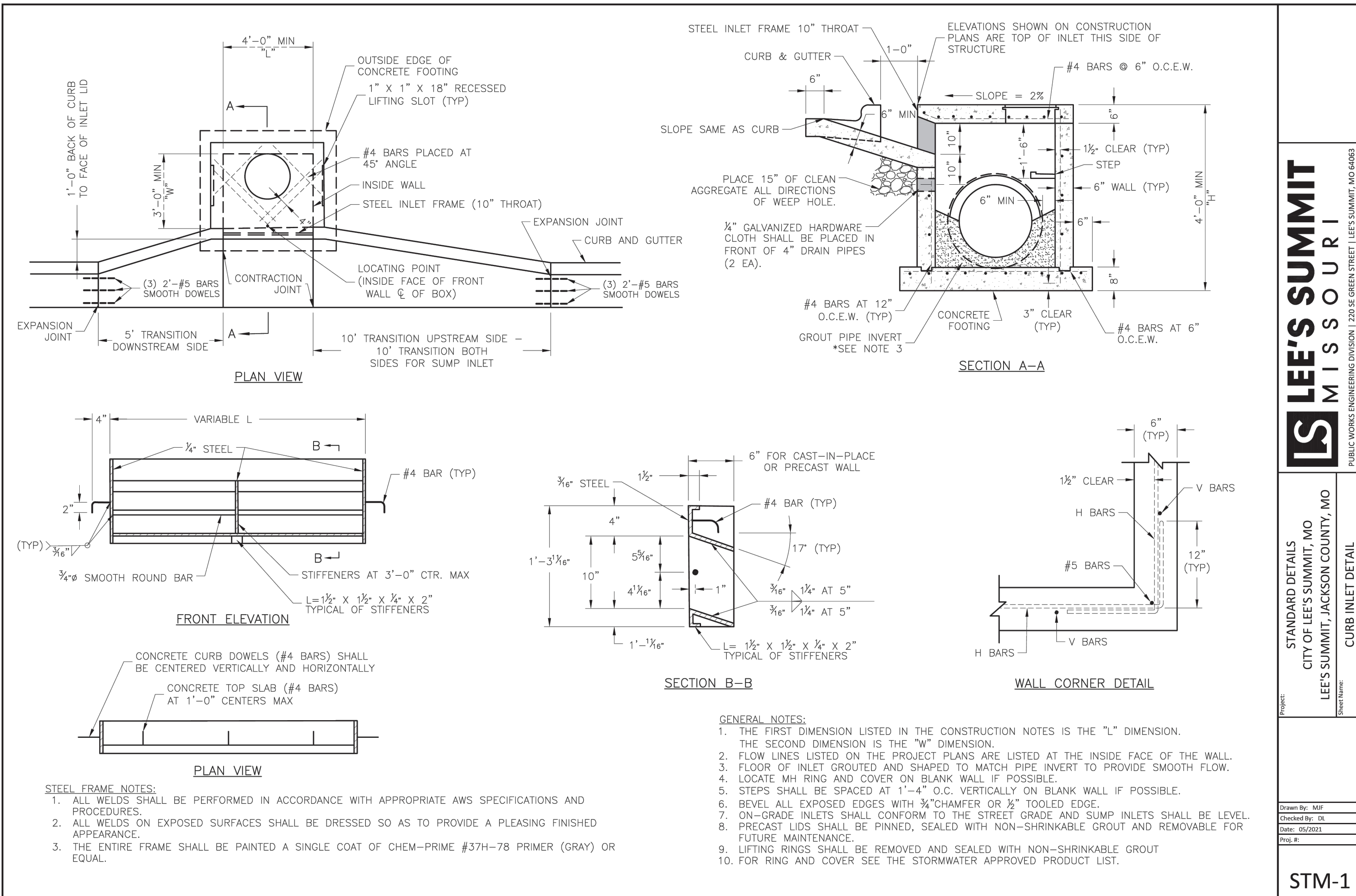
1/2" = 1'-0"

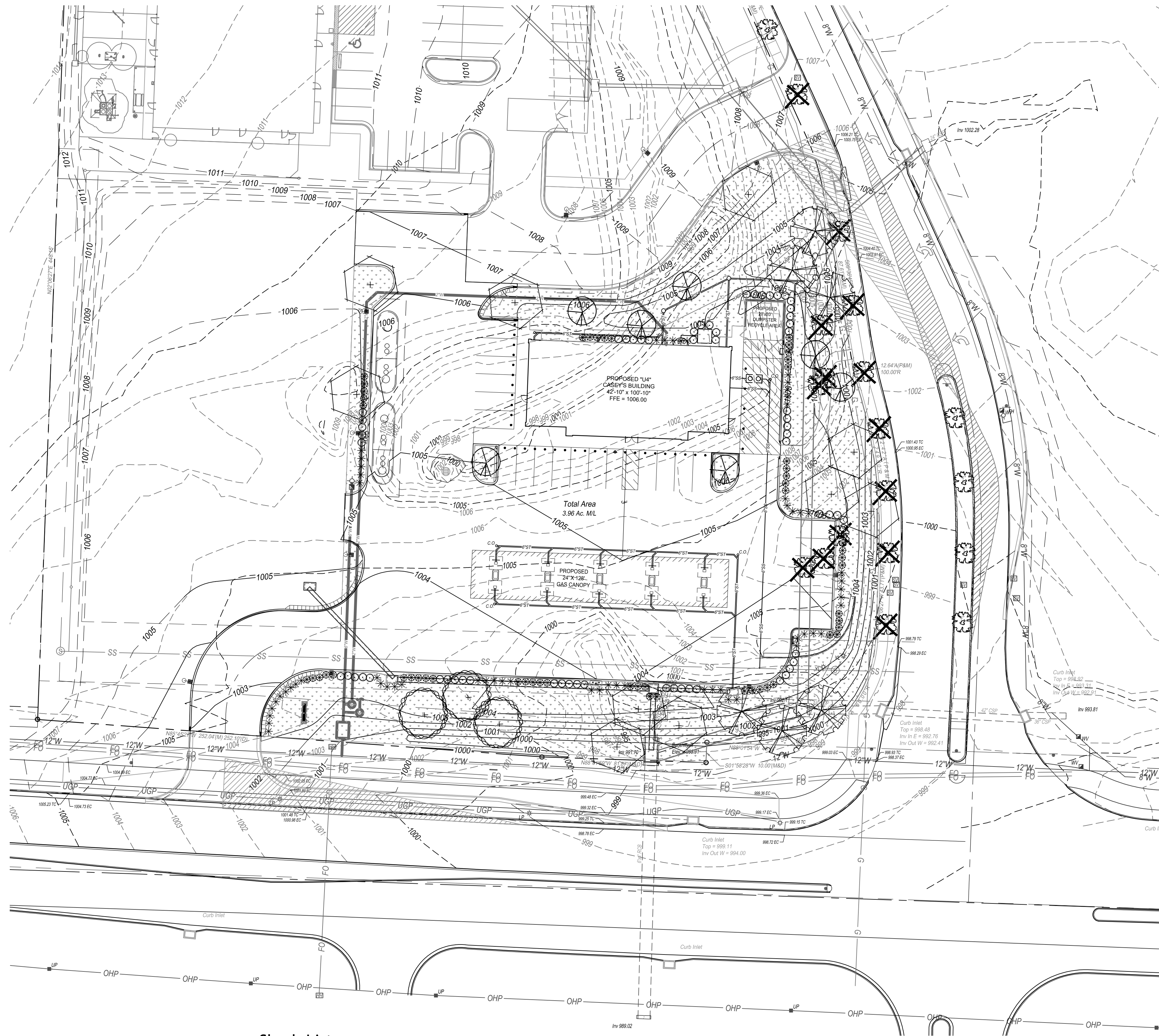


101 South Kansas Avenue
Topeka, Kansas 66603
Ph: (785) 215-8630 /
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SBB Proj. No. 21-288

Casey's			
CASEY'S CONSTRUCTION DIVISION			
One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100			
2023 CIVIL DETAILS	PROJECT: LEE SUMMIT, MO #4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 10.13.23 REVISED ON:	DRAWING INFORMATION STANDARD DETAILS
DRAWING INFORMATION CONSTRUCTION DIVISION		C-603	
DRAWN BY:		CHECKED BY:	





LOT 1 SITE DATA:

HWY 150 264'
REQUIRED:
STREET TREES 1/30' = 9
PARKING LOT SHRUBS 12/40' = 79

PROVIDED:
SHADE TREES = 9
SHRUBS = 80

ARBOR RIDGE DRIVE 188'
REQUIRED:
STREET TREES 1/30' = 6
PARKING LOT SHRUBS 12/40' = 56

PROVIDED:
SHADE TREES = 6
SHRUBS = 56

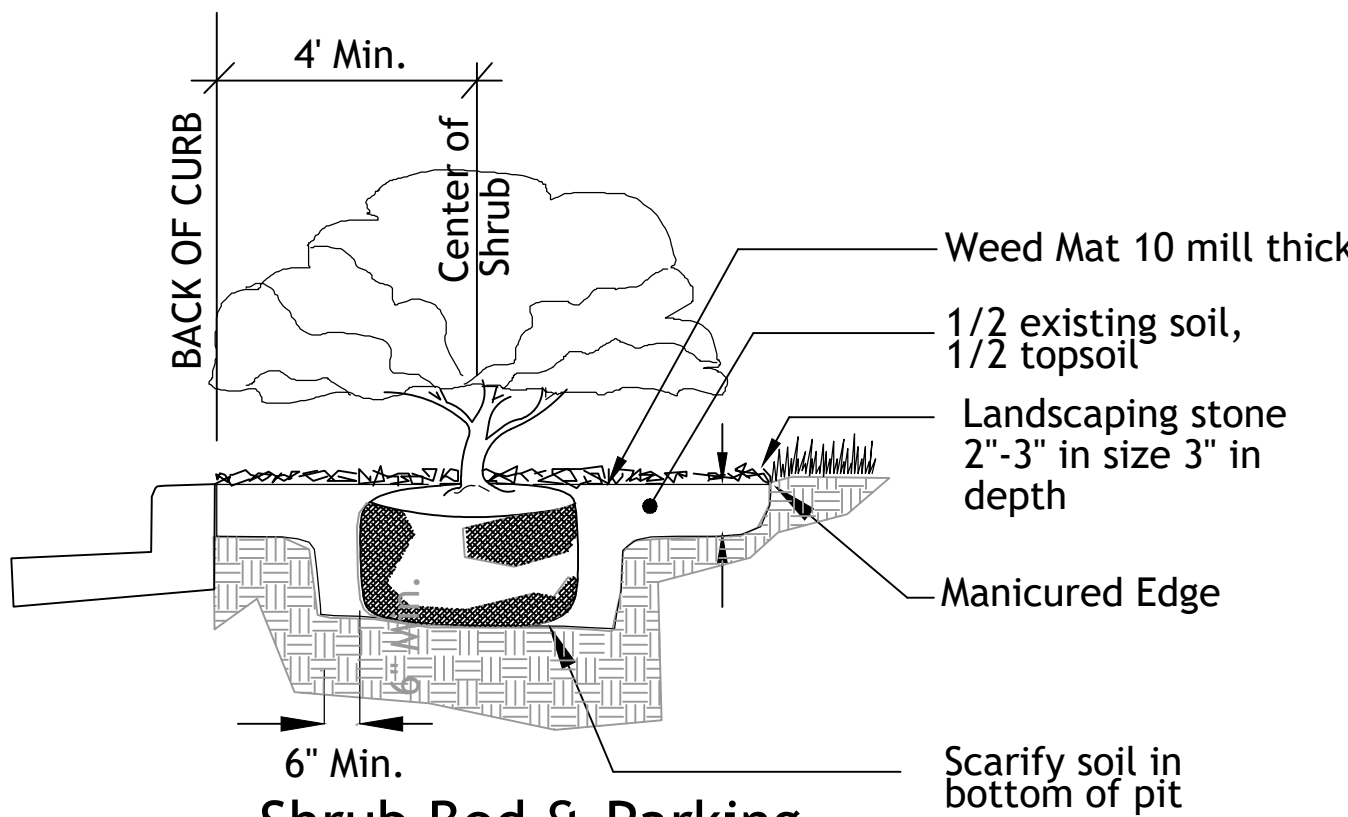
INTERIOR PARKING
TOTAL PARKING SURFACE = 13,869sf
REQUIRED
5% LANDSCAPE AREA = 693sf
PROVIDED = 992sf

OPEN SPACE TREES
TOTAL SITE 1.51ac (65,775sf)
BUILDING AREA 4,569sf
OPEN SPACE 61,206sf

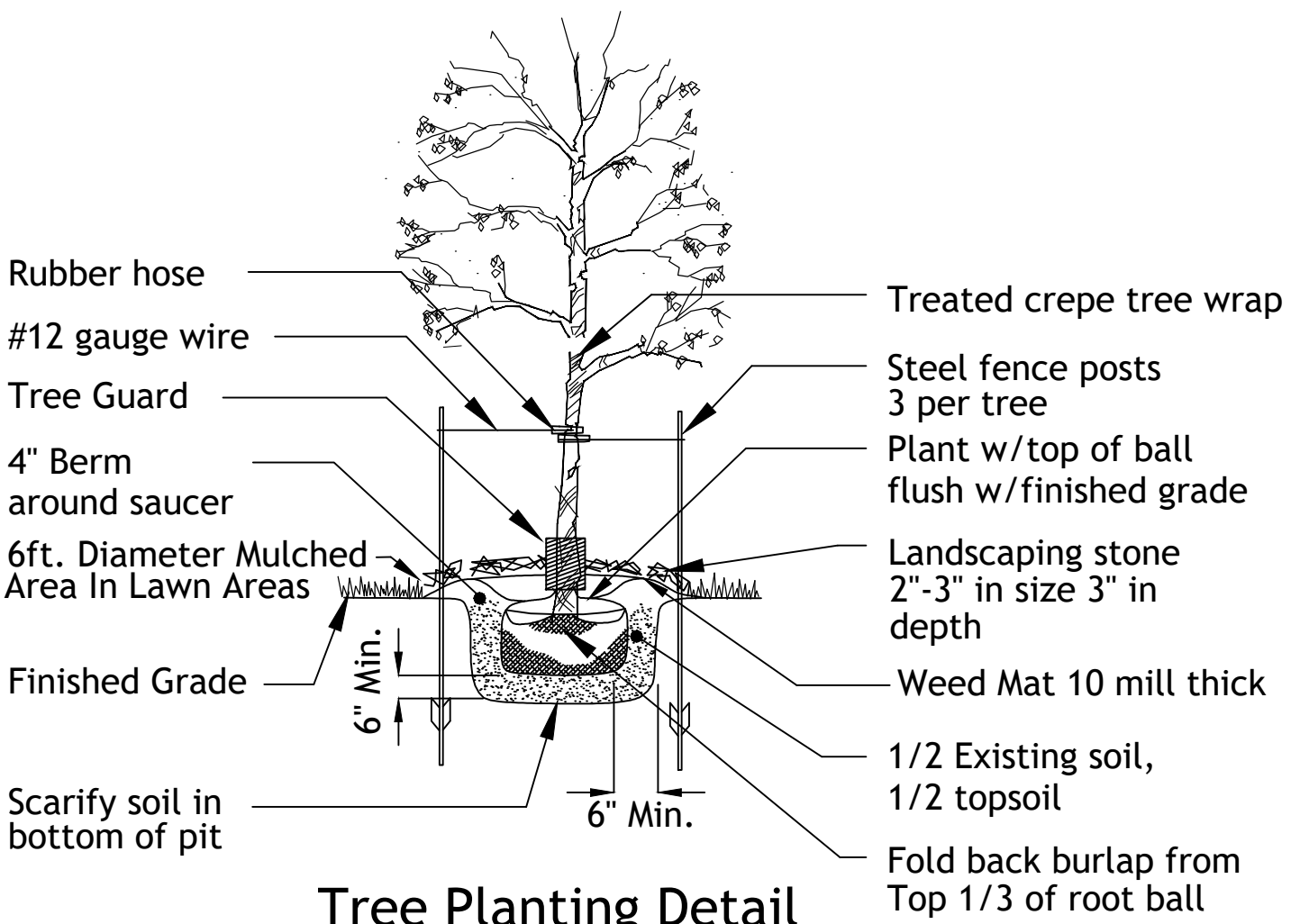
REQUIRED TREES
1 / 5,000sf = 12

PROVIDED
SHADE TREES = 5
ORNAMENTALS = 7

OPEN SPACE SHRUBS
REQUIRED 2 / 5,000sf = 24
PROVIDED = 41



Shrub Bed & Parking Setback Detail
No Scale



Tree Planting Detail
No Scale

LANDSCAPE NOTES

CONTRACTOR REQUIRED TO LOCATE ALL UTILITIES BEFORE INSTALLATION TO BEGIN.

Contractor shall verify all landscape material quantities and shall report any discrepancies to the Landscape Architect prior to installation.

No plant material substitutions are allowed without Landscape Architect or Owners approval.

Contractor shall guarantee all landscape work and plant material for a period of one year from date of acceptance of the work by the Owner. Any plant material which dies during the one year guarantee period shall be replaced by the contractor during normal planting seasons.

Contractor shall be responsible for maintenance of the plants until completion of the job and acceptance by the Owner.

Successful landscape contractor shall be responsible for design that complies with minimum irrigation requirements, and installation of an irrigation system. Irrigation system to be approved by the owner before starting any installation.

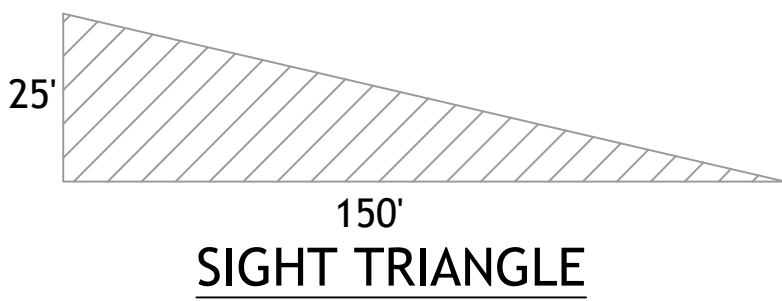
All plant material shall be specimen quality stock as determined in the "American Standards For Nursery Stock" published by The American Association of Nurseryman, free of plant diseases and pest, of typical growth of the species and having a healthy, normal root system.

Sizes indicated on the plant list are the minimum, acceptable size. In no case will sizes less than specified be accepted.

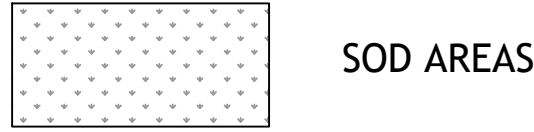
All shrub beds within lawn areas to receive a manicured edge.

All shrub beds shall be mulched with landscaping stone 2'-3' in size and 3' in depth. Color to be selected by owner.

All areas within the property line to be fertilized, sodded, and irrigated. Sod shall be Turf-Type-Tall Fescue and seed blend



LEGEND



Typical Utility Box Screening Details

No Scale



UTILITY BOXES SHALL BE CLUSTERED AS MUCH AS POSSIBLE

Shrub List

Symbol	Quantity	Common Name	Botanical Name	Size	Condition	Spacing
	61	Seagreen Juniper	Juniperus Chinensis 'Seagreen'	18"-24"sp.	Cont.	4'o.c.
	55	Dwarf Winged Euonymus	Euonymus Alatus 'Compactus'	18"-24"sp.	Cont.	4'o.c.
	60	Morning Light Maiden Grass	Miscanthos Sinensis 'Morning Light'	18"-24"sp.	Cont.	4'o.c.

Tree List

Symbol	Quantity	Common Name	Botanical Name	Size	Condition	Spacing
	3	October Glory Maple	Acer Rubrum 'October Glory'	3" cal	BB	As Shown
	10	Skyline Honeylocust	Gleditsia Triacanthos 'Skyline'	3" cal	BB	As Shown
	6	Swamp White Oak	Quercus Bicolor	3"cal	BB	As Shown
	7	Golden Raintree	Koelreuteria Paniculata	3"cal	BB	As Shown



Garrett Ochs, Landscape Architect
No Licence 2008000587



SBB Proj. No. 21-288	
Casey's	
CASEY'S CONSTRUCTION DIVISION One Convenience Blvd., P.O. Box 3001, Ankeny, IA 50021 515-965-6100	
PROJECT: LEE SUMMIT, MO # 4291 HIGHWAY 150 & ARBORIDGE DRIVE "U4" - STYLE STORE	PUBLISHED: 8-02-23 REVISED ON:
DRAWING INFORMATION: CONSTRUCTION DIVISION	LANDSCAPE PLAN
DRAWN BY:	CHECKED BY:
C-701	