

WHATABURGER

204 SW M150 HWY LEE'S SUMMIT, MO 64801 JACKSON COUNTY PROTOTYPE 22-M FINAL DEVELOPMENT PLAN

PRCOM20223975

RELEASED FOR CONSTRUCTION
As Noted on Plan Review
Development Services Department
Lee's Summit, Missouri
05/08/2023

REVISION / DATE / DESCRIPTION	
SIR UPDATES	09/13/21
60% SET	01/24/22
FDP APPLICATION TO CITY	02/24/22
RESPONSE TO CITY COMMENTS I	03/29/22
FINAL DEVELOPMENT SET	06/29/22
RESPONSE TO FDP COMMENTS	08/30/22
RESPONSE TO FDP COMMENTS II	09/29/22
RESPONSE TO FDP COMMENTS III	10/24/22
RESPONSE TO FDP COMMENTS IV	11/03/22
PT22M PROGRESS SET	03/08/23
ISSUE FOR PERMIT	03/15/23

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	CONSTRUCTION LIMITS
---	---	PROPERTY LINE
---	---	EASEMENT
X	X	FENCE
⊠	⊠	LIGHT POLE
---	---	UNDERGROUND ELECTRIC LINE
---	---	ELECTRICAL STRUCTURE
---	---	UNDERGROUND TELECOMMUNICATION LINE
---	---	GAS LINE
⊙	⊙	GAS METER
---	---	WATER LINE
⊙	⊙	WATER STRUCTURE
---	---	FIRE HYDRANT
---	---	SANITARY LINE
⊙	⊙	SANITARY STRUCTURE
---	---	SANITARY GREASE TRAP
---	---	STORM LINE
⊙	⊙	STORM STRUCTURE
---	---	ROOFDRAIN / UNDERDRAIN
950	950	CONTOUR
X 950.00 EX.	X 950.00	TOP OF CURB
X 949.50 EX.	X 949.50	TOP OF PAVEMENT
X 950.00 EX.	X 950.00	FINISHED GRADE SPOT ELEVATION
	1.00%	GRADE SLOPE
	←	MAJOR FLOOD ROUTING
HP	HP	HIGH POINT
		SEEDING/LANDSCAPE AREA
		CONCRETE
		STRUCTURAL CONCRETE
		CONCRETE PAVEMENT
		ROCK AREA
		HEAVY DUTY ASPHALT PAVEMENT



VICINITY MAP
N.T.S.

SHEET INDEX	
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SITE ZONING PLAN	C-3.1
CONCRETE JOINTING PLAN	C-4.0
SITE GRADING PLAN	C-5.0
SITE DRAINAGE PLAN	C-5.1
STORM SEWER PLAN AND PROFILES	C-5.2
SITE UTILITY PLAN	C-6.0
SITE DETAILS	C-7.0
SITE DETAILS	C-7.1
SITE DETAILS	C-7.2
SITE DETAILS	C-7.3
DETENTION SYSTEM DETAILS	C-7.4
SITE DETAILS	C-7.5
SITE DETAILS	C-7.6
FIRE PROTECTION PLAN	C-8.0
SWPPP PHASE - PRE-CONSTRUCTION	C-9.0
SWPPP PHASE - DURING & POST CONSTRUCTION	C-9.1
SWPPP NOTES	C-10.0
SWPPP NOTES	C-10.1
SWPPP DETAILS	C-10.2
SWPPP DETAILS	C-10.3
LANDSCAPE PLAN	L-1.0
IRRIGATION PLAN	I-1.0

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.



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2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

COVER SHEET



DRAWN BY: TDB

CHECKED BY: PGD

PROJECT NO: 40497-21

DRAWING

C-1.0



OWNER

WHATABURGER
300 CONCORD PLAZA DR.
SAN ANTONIO, TX 78216
PHONE: (210) 476-6842
CONTACT: ALYSSIA LESTER
EMAIL: alester@wbhq.com

SURVEYOR

YOUNG - HOBBS AND ASSOCIATES
1202 CROSSLAND AVE.
CLARKSVILLE, TN 37040
PHONE: (931) 645-2524
CONTACT: DAVE R. HOBBS

ENGINEER

ms consultants, inc.
2221 SCHROCK ROAD
COLUMBUS, OHIO 43229
PHONE: (614) 898-7100
CONTACT: IAN AULTMAN
EMAIL: iaaultman@msconsultants.com

GEOTECHNICAL ENGINEER

TERRACON CONSULTANTS, INC.
4765 WEST JUNCTION STREET
SPRINGFIELD, MISSOURI 65802
PHONE: (417) 864-5100
CONTACT: TY G. ALEXANDER, P.E.

BENCHMARK

TBM 1:
1/2 IRON
NORTHING: 2822704.4825
EASTING: 978325.3390
ELEVATION = 1022.68

TBM 2:
1/2 IRON
NORTHING: 2822484.7625
EASTING: 978065.2866
ELEVATION = 1019.71

BASIS OF BEARINGS:
MO (C) STATE PLANE COORDINATE SYSTEM SPC (2402 MO C)

FLOOD INFORMATION

THIS PROPERTY IS LOCATED WITHIN AN AREA HAVING ZONE DESIGNATIONS OF "X" BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY, ON FLOOD INSURANCE RATE MAP NO. 29095C0532G, WITH A MAP EFFECTIVE DATE OF JANUARY 20TH, 2017, IN JACKSON COUNTY, STATE OF MISSOURI, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PROPERTY IS SITUATED.

Development Services Department
Lee's Summit, Missouri

05/18/2023



YOUNG - HOBBS AND ASSOCIATES

1202 CROSSLAND AVE.
CLARKSVILLE, TN 37040
PHONE 931-645-2524
FAX 931-645-2768

PRELIMINARY - NOT FOR RECORDING OR LAND TRANSFER

DAVE R. HOBBS, PLS 2014020711

No.	Date	Revision
1		

CLIENT



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

ALTA/NSPS LAND TITLE SURVEY

OWNER INFORMATION

SHUN PING YEH
TAX ID:
70-400-04-03-02-3-00-000
DOC. # 2021E0043600
DOC. # 2021E0043599

ALSO BEING TRACT "A"
DOC. # 2021E0081920

**CITY OF LEES SUMMIT
COUNTY OF JACKSON
STATE OF MISSOURI**

DRAWN BY: CLH,KAB

APPROVED BY: DRH

DATE: (FIELD) 11/22/2021

DATE: (OFFICE) 12/16/2021

YHA PRO. # 242-21

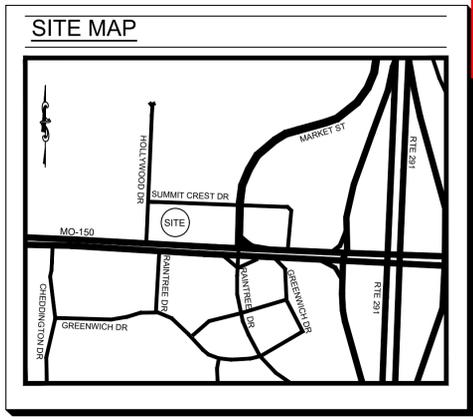
SHEET 1 OF 1

CURVE	ARC LENGTH	RADIUS	CHORD BEARING	CHORD LENGTH
C1 M	31.05'	20.00'	N 47°11'11" E	28.02'
C1 R	31.19'	20.00'	N/A	N/A

SITE ADDRESS:
204 SW MISSOURI 150 HIGHWAY,
LEES SUMMIT, MO 64081

PARKING:

REGULAR: 0
HANDICAPPED: 0
TOTAL: 0



NOTES CORRESPONDING TO SCHEDULE B-11:

CHICAGO TITLE INSURANCE COMPANY
EFFECTIVE DATE: NOVEMBER 17, 2021 AT 8:00 A.M.
FILE NO.: KCC213072

ITEMS 1-7 AND 14-15, ARE STANDARD EXCEPTIONS AND/OR NOT SURVEYING RELATED AND ARE SUBJECT TO THESE EXCEPTIONS.

8. Water Line Easement by the instrument recorded as Document No. 1975211265 in Book 1596 at Page 173. DOES NOT AFFECT.

9. Sewer Line Easement by the instrument recorded as Document No. 19881875029 in Book 11850 at Page 1774. AFFECTS, NOT POTTABLE.

10. Temporary Easement for Highway Purposes by the instrument recorded as Document No. 1988015602 in Book 13155 at Page 270. DOES NOT AFFECT.

11. Right of way granted to the City of Lee's Summit by the instrument recorded as Document No. 2014E0080868. DOES NOT AFFECT.

12. Easements set forth in Warranty Deed recorded as Document No. 2009E0045651. AFFECTS, AS SHOWN HEREON.

13. Matters shown and set forth on Certificate of Survey recorded July 27, 2021 as Document No. 2021E0081920. AFFECTS, NO EASEMENT SHOWN.

TABLE A NOTES:

ITEM 2: ADDRESS SHOWN IS PER TITLE COMMITMENT NO. KCC213072.

ITEM 3: THIS PROPERTY IS LOCATED WITHIN AN AREA HAVING ZONE DESIGNATIONS OF "X" BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY, ON FLOOD INSURANCE RATE MAP NO. 22095025303, WITH A MAP REVISED DATE OF JANUARY 20, 2017, IN JACKSON COUNTY, STATE OF MISSOURI, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PROPERTY IS SITUATED.

ITEM 5: CONTOURS WERE DERIVED FROM RANDOM SHOTS AND CROSS SECTIONS AND ARE SHOWN AT ONE FOOT INTERVALS. ELEVATIONS SHOWN HEREON ARE, BASED ON GPS OBSERVATIONS TOGETHER WITH AN OPUS SOLUTION, DATED 11/22/2021 (NAVDS8.GEIOD18).

ITEM 6A: NO ZONING REPORT PROVIDED TO THIS SURVEYOR.

ITEM 16: THERE WAS NO EVIDENCE OF RECENT EARTH MOVING, BUILDING CONSTRUCTION, OR BUILDING ADDITIONS OBSERVED IN THE PROCESS OF CONDUCTING THE FIELDWORK.

ITEM 17: THERE WAS NO EVIDENCE OF RECENT CHANGES IN STREET RIGHT OF WAY LINES. THERE WAS NO EVIDENCE OF RECENT OR STREET SIDEWALK CONSTRUCTION OR REPAIRS OBSERVED IN THE PROCESS OF CONDUCTING THE FIELDWORK.

LAND DESCRIPTION: AS SURVEYED

PART OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 30, TOWNSHIP 47 NORTH, RANGE 31 WEST, OF THE FIFTH PRINCIPAL MERIDIAN, LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, ALSO DESCRIBED AS FOLLOWS:

BEING ALL OF TRACT "A", ON THE CERTIFICATE OF SURVEY, RECORDED JULY 27, 2021 AS DOCUMENT NO. 2021E0081920 IN THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, ALSO DESCRIBED AS FOLLOWS:

BEGINNING AT A 1/2" IRON PIN FOUND (ID: 2651) AT THE RIGHT OF WAY LINE INTERSECTION OF 150 HIGHWAY AND HOLLYWOOD DRIVE; THENCE WITH THE EAST RIGHT OF WAY LINE OF SAID HOLLYWOOD DRIVE N 02°24'42" E A DISTANCE OF 150.89 FEET TO A 1/2" IRON PIN FOUND (ID: 2651); THENCE WITH THE RIGHT OF WAY LINE INTERSECTION OF SAID HOLLYWOOD DRIVE AND SUMMITCREST DRIVE WITH A CURVE TURNING TO THE RIGHT WITH AN ARC LENGTH OF 31.05 FEET WITH A RADIUS OF 20.00 FEET WITH A CHORD BEARING OF N 47°11'11" E, WITH A CHORD LENGTH OF 28.02 FEET TO A 1/2" IRON PIN FOUND (ID: 2651); THENCE WITH THE SOUTH RIGHT OF WAY LINE OF SAID SUMMITCREST DRIVE S 88°15'48" E A DISTANCE OF 282.03 FEET TO A 1/2" IRON PIN FOUND (ID: 2651); THENCE LEAVING SAID RIGHT OF WAY LINE WITH THE WEST LINE OF HAWKINS COMMERCIAL LOT 1, RECORDED IN DOCUMENT NO. 20000062550 S 02°26'57" W A DISTANCE OF 208.22 FEET TO A 3/8" IRON PIN FOUND IN THE NORTH RIGHT OF WAY LINE OF SAID 150 HIGHWAY; THENCE WITH SAID NORTH RIGHT OF WAY LINE N 88°42'20" W A DISTANCE OF 301.67 FEET TO THE POINT OF BEGINNING, HAVING AN AREA OF 63.083 SQUARE FEET, 1.448 ACRES, MORE OR LESS.

SURVEY NOTES:

INFORMATION REGARDING THE PRESENCE, SIZE AND LOCATION OF UNDERGROUND UTILITIES IS SHOWN HEREON. THIS INFORMATION HAS BEEN SHOWN BASED ON THE LOCATION ABOVE GROUND APPURTENANCES, AVAILABLE DESIGN PLANS, AND FLAGS AND PAINT PLACED BY THE UNDERGROUND PROTECTION SERVICE. NO CERTIFICATION IS MADE AS TO THE ACCURACY OF THOROUGHNESS OF THE INFORMATION CONCERNING UNDERGROUND UTILITIES AND STRUCTURES SHOWN HEREON. (MISSOURI ONE CALL 1-800-DIG-RITE). THIS SURVEY WAS PERFORMED WITHOUT THE BENEFIT OF A PRIVATE UTILITY LOCATE.

CONTACT PROPER AUTHORITIES BEFORE BUILDING NEAR UTILITY LINES. FOR EASEMENT WIDTH AND RESTRICTIONS, UTILITIES ARE APPROXIMATE AND SHOULD BE VERIFIED PRIOR TO ANY CONSTRUCTION.

UNLESS STATED OTHERWISE, ANY MONUMENT REFERRED TO HEREIN AS AN "IRON PIN SET" IS A SET 5/8" DIAMETER REBAR WITH A YELLOW PLASTIC CAP STAMPED "YOUNG-HOBBS"

THIS SURVEY HAS BEEN PREPARED FOR THE EXCLUSIVE USE OF THE PERSON OR ENTITIES NAMED HEREON. NO EXPRESS OR IMPLIED WARRANTIES WITH RESPECT TO THE INFORMATION SHOWN HEREON IS TO BE EXTENDED TO ANY PERSONS OR ENTITIES OTHER THAN THOSE SHOWN HEREON.

THERE WERE NO BUILDINGS ON THE SUBJECT PROPERTY AT THE TIME OF THE SURVEY.

LIST OF ENCROACHMENTS: NONE. THE OWNERSHIP OF CURB, UTILITIES, FENCES, AND/OR PERIMETER WALLS SHOWN HEREON ARE NOT KNOWN AND THIS ARE NOT LISTED AS ENCROACHMENTS. CURB, UTILITIES, FENCES, AND/OR PERIMETER WALLS ARE SHOWN IN THEIR RELATIVE POSITION TO THE BOUNDARY.

I DO HEREBY STATE THAT THIS IS A TRUE, COMPLETE AND CORRECT SURVEY OF THE DESCRIBED REAL PROPERTY SITUATED IN THE COUNTY OF GREENE, MISSOURI AND THAT THIS SURVEY WAS EXECUTED IN ACCORDANCE WITH THE CURRENT MISSOURI MINIMUM STANDARDS FOR PROPERTY SURVEYS (URBAN SURVEY 1.20.000)

SURVEYOR'S CERTIFICATION:

TO: WHATABURGER RESTAURANTS LLC AND CHICAGO TITLE INSURANCE COMPANY

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 6(a), 6(b), 7(a-c), 8, 9, 13, 16, and 17 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON NOVEMBER 22, 2021.

DATE OF PLAT OR MAP: DECEMBER 16, 2021.

DAVE R. HOBBS, PLS 2014020711
dav@younghobbs.com

DATE

LEGEND

- IRON PIN FOUND, AS NOTED
- BENCHMARK, AS NOTED
- SIGN, AS NOTED
- SEWER MANHOLE
- FIRE HYDRANT
- WATER VALVE
- UTILITY POLE
- GUY WIRE
- ELECTRIC METER
- GAS VALVE
- FOC PULL BOX
- TELEPHONE BOX
- GRATE INLET
- STORM MANHOLE
- GATE POST
- PROPERTY LINE
- EASEMENT LINE
- SETBACK LINES
- OVERHEAD WIRE
- UNDERGROUND ELECTRIC
- FENCE LINE
- GAS LINE, AS NOTED
- SANITARY SEWER, AS NOTED
- UNDERGROUND FIBER OPTIC
- ST
- STORM SEWER PIPE, AS NOTED
- WATER LINE
- LIGHT POLE(2-WAY)



SCALE 1"=20'
BASIS OF BEARINGS
MO (W) STATE PLANE COORDINATE SYSTEM
SPC (2403 MO W)



SIR UPD	05/06/2023	09/13/21
60% SET		01/24/22
PDP APPLICATION TO CITY		02/24/22
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fax 614.898.7570

PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

SITE DIMENSION PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

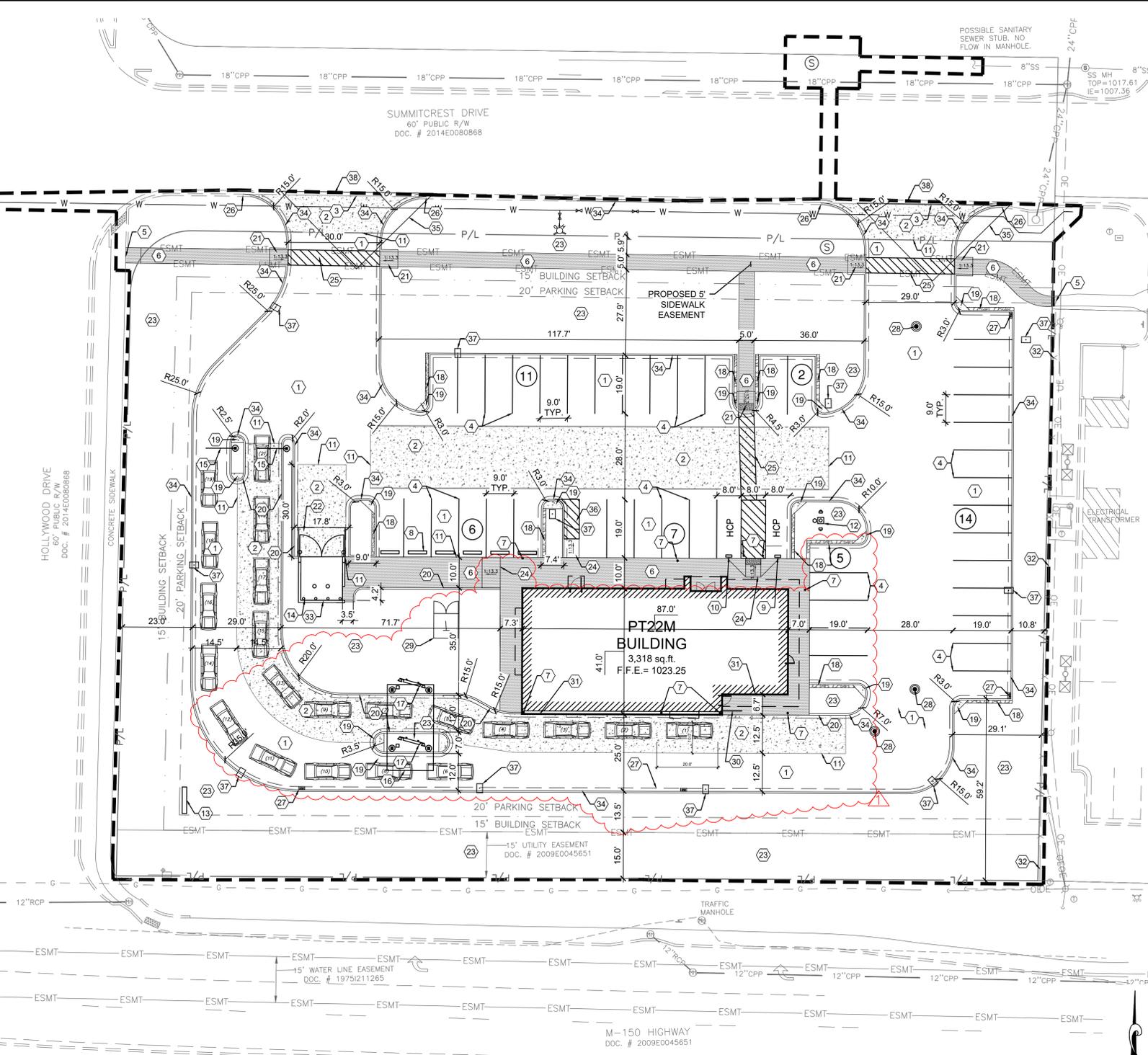
C-3.0

GENERAL NOTES:

- ALL EXISTING CONDITIONS, TOPOGRAPHY, UTILITIES AND PROPERTY INFORMATION ARE TAKEN FROM A SURVEY OF LAND SITUATED IN THE CITY OF LEE'S SUMMIT, COUNTY OF JACKSON AND STATE OF MISSOURI, BY SURVEYOR, YOUNG-HOBBS AND ASSOCIATES, 1202 CROSSLAND AVENUE CLARKSVILLE, TN 37040.
- CONTRACTOR IS RESPONSIBLE FOR LOCATING AND VERIFYING ALL EXISTING UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION, AND IS RESPONSIBLE FOR ANY DAMAGE TO THEM DURING CONSTRUCTION.
- PROVIDE SMOOTH TRANSITION FROM NEWLY PAVED AREAS TO EXISTING PAVED AREAS AS NECESSARY. THE EXISTING EDGE OF PAVEMENT SHALL BE FREE OF ALL LOOSE DEBRIS AT ALL AREAS WHERE PROPOSED PAVEMENT MEETS EXISTING PAVEMENT. THE EDGE OF EXISTING ASPHALT PAVEMENT SHALL BE PROPERLY SEALED WITH A TACK COAT MATERIAL IN ALL AREAS WHERE NEW ASPHALT PAVEMENT IS INDICATED TO JOIN EXISTING.
- ALL DIMENSIONS TO FACE OF CURB AND/OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND TAKE ALL PRECAUTIONS NECESSARY TO AVOID DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- REFER TO ARCHITECTURAL DRAWINGS FOR BUILDING DIMENSIONS AND ADDITIONAL INFORMATION.
- ALL CONSTRUCTION METHODS AND MATERIALS MUST CONFORM TO CURRENT STANDARDS AND SPECIFICATIONS OF THE FEDERAL, STATE, COUNTY, CITY OR LOCAL REQUIREMENTS, WHICHEVER HAS JURISDICTION.
- ALL EXCAVATED AREAS TO BE SEEDED AND/OR SODDED AFTER FINISH GRADING UNLESS OTHERWISE NOTED. ALL NEWLY SEEDDED/SODDED AREAS SHALL HAVE A MINIMUM OF 4" OF TOPSOIL. HOLD SLOPE DOWN 1" FROM PAVEMENT ELEVATION. CONTRACTOR TO SUPPLY STRAW MULCH WHERE GRASS SEED HAS BEEN PLANTED.
- ALL RADII ARE 3.0 FEET UNLESS OTHERWISE SHOWN. ALL RADII INDICATED ON PLANS SHALL BE CONSTRUCTED AS CIRCULAR ARCS.
- THERE ARE NO OIL AND GAS WELLS (ACTIVE/INACTIVE/CAPPED) PER THE MISSOURI DEPARTMENT OF NATURAL RESOURCES MAPPING.
- ALL CONSTRUCTION SHALL FOLLOW THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL AS ADOPTED BY ORDINANCE 5813. WHERE DISCREPANCIES EXISTING BETWEEN THESE PLANS AND THE DESIGN AND CONSTRUCTION MANUAL, THE DESIGN AND CONSTRUCTION MANUAL SHALL PREVAIL.
- THE CONTRACTOR SHALL CONTACT THE CITY'S DEVELOPMENT SERVICES ENGINEERING INSPECTION TO SCHEDULE A PRE CONSTRUCTION MEETING WITH A FIELD ENGINEERING INSPECTOR PRIOR TO ANY LAND DISTURBANCE WORK AT (816) 969-1200.

KEYED NOTES:

- PROPOSED HEAVY DUTY ASPHALT PAVEMENT, SEE DETAIL ON SHEET C-7.0.
- PROPOSED HEAVY DUTY CONCRETE PAVEMENT, SEE DETAIL ON SHEET C-7.0.
- PROPOSED HEAVY DUTY CONCRETE PAVEMENT TO BE FLUSH WITH EXISTING ASPHALT PAVEMENT.
- PROPOSED PAINTED PARKING STRIPING (TYPICAL). ALL PARKING STRIPES ARE TO BE 4" PAINTED WHITE, UNLESS OTHERWISE NOTED ON THE PLANS, DETAILS OR SPECIFICATIONS.
- CONNECT TO EXISTING SIDEWALK. SEE THE GRADING PLAN FOR MORE DETAILS.
- PROPOSED CONCRETE SIDEWALK. SEE DETAIL ON SHEET C-7.0.
- PROPOSED BOLLARD, TYP. OF 9. SEE ARCHITECTURAL AND STRUCTURAL PLANS FOR DETAILS.
- PROPOSED PRE-CAST CONCRETE WHEEL STOP (TYP. OF 6). SEE DETAIL ON SHEET C-7.3.
- GENERAL CONTRACTOR TO PROVIDE AND INSTALL (1) POLE-MOUNTED ADA PARKING SIGNS, SIGNS PROVIDED BY CONTRACTOR TO MEET LOCAL REQUIREMENTS, SEE DETAIL ON SHEET C-7.0.
- GENERAL CONTRACTOR TO PROVIDE AND INSTALL (1) POLE-MOUNTED ADA PARKING SIGN WITH "VAN ACCESSIBLE" SIGN. SIGNS PROVIDED BY CONTRACTOR TO MEET LOCAL REQUIREMENTS, SEE DETAIL ON SHEET C-7.1.
- CONCRETE TO BE FLUSH WITH ADJACENT ASPHALT PAVEMENT. SEE DETAIL ON SHEET C-7.1.
- FLAGPOLE WITH GROUND-MOUNTED LIGHTS, UNITED FLAG AND BANNER, GARRISON TYPE OR OWNER APPROVED EQUAL, 30' HIGH, 5" BUTT ALUMINUM WITH 14 GAUGE ALUMINUM BALL APPROX. 30 FEET IN LENGTH, MEASURED FROM THE ENCLOSURE OPENING. THE PAD AND APPROACH SHALL BE IMPROVED WITH A MINIMUM 6" OF FULL DEPTH UNREINFORCED PORTLAND CEMENT CONCRETE CONSTRUCTED ON A SUB-GRADE OF 4" OF GRANULAR BASE COURSE. SEE ARCHITECTURAL AND STRUCTURAL PLANS FOR DETAILS.
- PROPOSED 35' HT POLE SIGN, CONTRACTOR TO COORDINATE WITH OWNER. SEE ELECTRICAL PLANS AND SIGNAGE PACKAGE FOR DETAILS.
- PROPOSED DUMPSTER ENCLOSURE AND CONCRETE PAD. SEE THE STRUCTURAL AND ARCHITECTURAL SHEETS FOR DETAILS.
- PROPOSED HEADACHE BAR. SEE ARCHITECTURAL PLANS FOR DETAILS.
- PROPOSED MENU BOARD CANOPY. SEE ARCHITECTURAL PLANS FOR DETAILS.
- PROPOSED EXTERIOR MENU BOARD. SEE ARCHITECTURAL PLANS FOR DETAILS.
- PROPOSED 1" WIDE CRUSHED GRANITE STRIP. SEE DETAIL ON SHEET L-1.0.
- PROPOSED CONCRETE ISLAND NOSE. SEE DETAIL ON SHEET C-7.1.
- PROPOSED 6" MONOLITHIC CURB. SEE DETAIL ON SHEET C-7.1.
- PROPOSED ADA SIDEWALK RAMP. SEE DETAIL ON SHEETS C-7.0 & C-7.1.
- CONCRETE DUMPSTER ENCLOSURE APRON, TRASH ENCLOSURE AREA SHALL BE IMPROVED WITH A PORTLAND CEMENT CONCRETE PAD AND PORTLAND CEMENT CONCRETE APPROX. 30 FEET IN LENGTH, MEASURED FROM THE ENCLOSURE OPENING. THE PAD AND APPROACH SHALL BE IMPROVED WITH A MINIMUM 6" OF FULL DEPTH UNREINFORCED PORTLAND CEMENT CONCRETE CONSTRUCTED ON A SUB-GRADE OF 4" OF GRANULAR BASE COURSE. SEE DETAIL ON SHEET C-7.0.
- PROPOSED LANDSCAPE AREA. SEE LANDSCAPE PLAN ON SHEET L-1.
- PROPOSED CURB RAMP. SEE DETAIL ON SHEET C-7.0.
- PROPOSED SIDEWALK CROSSING. SEE DETAIL ON SHEET C-7.2.
- CONNECT PROPOSED CURB TO EXISTING CURB (TRANSITION REQUIRED).
- PROPOSED STORM INLET. SEE SHEETS C-5.0 AND C-7.5 FOR DETAILS.
- PROPOSED STORM MANHOLE/JUNCTION BOX. SEE SHEETS C-5.0, C-7.5, AND C-7.6 FOR DETAILS.
- PROPOSED ELECTRIC TRANSFORMER. COORDINATE DETAILS AND LOCATION WITH UTILITY OWNER AND ELECTRICAL PLANS. SEE SHEET C-6.0.
- PROPOSED HAND RAIL. SEE ARCHITECTURAL SHEETS FOR DETAILS.
- PROPOSED STRUCTURAL FOUNDATION.
- EXISTING WOODEN FENCE TO REMAIN.
- TRASH ENCLOSURE SHALL BE PROTECTED THROUGH INSTALLATION OF 4" BOLLARDS (TYP. OF 4) ALONG THE REAR WALL OF THE TRASH ENCLOSURE. SEE ARCHITECTURAL AND STRUCTURAL PLANS FOR DETAILS.
- TYPE CG-1 CURB AND GUTTER. SEE DETAIL ON SHEET C-7.1.
- SITE TRIANGLE.
- PROPOSED PARKING STRIPING. SEE DETAIL ON SHEET C-7.0.
- PROPOSED LIGHT POLE AND FOUNDATION. SEE STRUCTURAL PLANS FOR DETAILS.
- EXISTING GUTTER LINE TO BE MAINTAINED.
- PROPOSED PRE-CAST CONCRETE WHEEL STOP (TYP. OF 6). SEE DETAIL ON SHEET C-7.3.



**LAND DESCRIPTION:
AS SURVEYED**

PART OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 30, TOWNSHIP 47 NORTH, RANGE 31 WEST, OF THE FIFTH PRINCIPAL MERIDIAN, LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, DESCRIBED AS FOLLOWS:
BEING ALL OF TRACT "A", AS SHOWN ON THE CERTIFICATE OF SURVEY, RECORDED JULY 27, 2021 AS DOCUMENT NO. 2021E0081920 IN THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, ALSO DESCRIBED AS FOLLOWS:
BEGINNING AT A 1/2" IRON PIN FOUND (ID: 2651) AT THE RIGHT OF WAY LINE INTERSECTION OF 150 HIGHWAY AND HOLLYWOOD DRIVE; THENCE WITH THE EAST RIGHT OF WAY LINE OF SAID HOLLYWOOD DRIVE N 02°24'42" E A DISTANCE OF 190.89 FEET TO A 1/2" IRON PIN FOUND (ID: 2651); THENCE WITH THE RIGHT OF WAY LINE INTERSECTION OF SAID HOLLYWOOD DRIVE AND SUMMITCREST DRIVE WITH A CURVE TURNING TO THE RIGHT WITH AN ARC LENGTH OF 31.05 FEET WITH A RADIUS OF 20.00 FEET WITH A CHORD BEARING OF N 47°11'11" E WITH A CHORD LENGTH OF 28.02 FEET TO A 1/2" IRON PIN FOUND (ID: 2651); THENCE WITH THE SOUTH RIGHT OF WAY LINE OF SAID SUMMITCREST DRIVE S 88°15'48" E A DISTANCE OF 282.03 FEET TO A 1/2" IRON PIN FOUND (ID: 2651); THENCE LEAVING SAID RIGHT OF WAY LINE WITH THE WEST LINE OF HAWKINS COMMERCIAL LOT 1, RECORDED IN DOCUMENT NO. 200E06255 S 02°26'57" W A DISTANCE OF 208.22 FEET TO A 3/8" IRON PIN FOUND IN THE NORTH RIGHT OF WAY LINE OF SAID 150 HIGHWAY; THENCE WITH SAID NORTH RIGHT OF WAY LINE N 88°42'20" W A DISTANCE OF 301.67 FEET TO THE POINT OF BEGINNING, HAVING AN AREA OF 63,083 SQUARE FEET, 1.448 ACRES, MORE OR LESS.

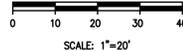
SITE DATA			
	SQ. FT.	ACRES	PERCENT
TOTAL BUILDING FLOOR AREA	3751	0.09	5.95
TOTAL SITE AREA	63083	1.45	100.0
LIMITS OF DISTURBANCE	68942	1.58	109.2
EXISTING PERVIOUS	62787	1.44	99.5
EXISTING IMPERVIOUS	296	0.01	0.5
TOTAL PROPOSED PERVIOUS	24129	0.55	38.2
TOTAL PROPOSED IMPERVIOUS	38954	0.89	61.8
CURRENT ZONING IS CP-2 PLANNED COMMUNITY COMMERCIAL ; APPROVED BY CITY COUNCIL ON 05/31/22			
FLOOR AREA RATION = 0.059 SF			

PHASING PLAN		
PROJECT PHASE	BP DESCRIPTION	DATE
A-PRE-CONSTRUCTION	PREPARE SITE FOR CONSTRUCTION INCLUDING ESTABLISHING E&S PERIMETER CONTROLS	SEPTEMBER-22
B-PHASE I CONSTRUCTION	TOP SOIL STOCK PILE PROTECTION, SITE CLEARING, TEMPORARY SEEDING, GENERAL SITE PREP	SEPTEMBER-22 DECEMBER-22
C-PHASE II CONSTRUCTION	CONSTRUCT BUILDING ASPHALT PAVING, STORM STRUCTURE INSTALLATIONS	DECEMBER-22 JANUARY-23
D-FINAL STABILIZATION	PERMANENT SEEDING	JUNE-23

LEGEND

FEATURE	DESCRIPTION
	CONCRETE SIDEWALK
	HEAVY DUTY ASPHALT PAVEMENT
	HEAVY DUTY CONCRETE PAVEMENT
	CONSTRUCTION LIMITS
	BUILDING SETBACK LINE
	PARKING SETBACK LINE
	GUTTER LINE
	PROPOSED SIDEWALK EASEMENT
	EXISTING EASEMENT
	EXISTING
	PROPOSED
	LIGHT POLE
	FIRE HYDRANT

PARKING DATA		
	REQUIRED	PROVIDED
STANDARD	53	43
HANDICAP	3	2
TOTAL	56	45
*14 PARKING SPACE REQUIRED PER 1000 S.F. OF THE GROSS BUILDING FLOOR AREA (3,751 SF OF DINING AREA)		
**ADA SPACE PER 25 STANDARD SPACES		
***FEWER SPACES ALLOWABLE IF EVIDENCE OF SUCCESS ON SIMILAR PROJECTS CAN BE PROVIDED.		
****PARKING JUSTIFICATION WAS SUBMITTED TO THE CITY ON 03/29/22 AS PART OF THE PDP PROCESS.		



N:\03\62\40497\21-Lee's Summit, MO Market\Docs\CAD\CIVIL\DWG-set\C-3.0 Site Dimension Plan.dwg, 3/15/2023 2:33 PM, boley, jason

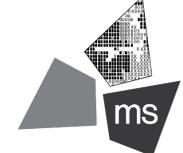
REVISION	DATE	DESCRIPTION
05/08/2023	09/13/21	Lee's Summit, Missouri
60% SET	01/24/22	
PDP APPLICATION	02/24/22	TO CITY
RESPONSE TO CITY COMMENTS I	03/29/22	
FINAL DEVELOPMENT SET	06/29/22	
RESPONSE TO FDP COMMENTS	08/30/22	
RESPONSE TO FDP COMMENTS II	09/29/22	
RESPONSE TO FDP COMMENTS III	10/24/22	
RESPONSE TO FDP COMMENTS IV	11/03/22	
PT22M PROGRESS SET	03/08/23	
ISSUE FOR PERMIT	03/15/23	

**LAND DESCRIPTION:
AS SURVEYED**

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NOTICE

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2221 Schrock Road
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phone 614.898.7100
fax 614.898.7570

PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE ZONING PLAN



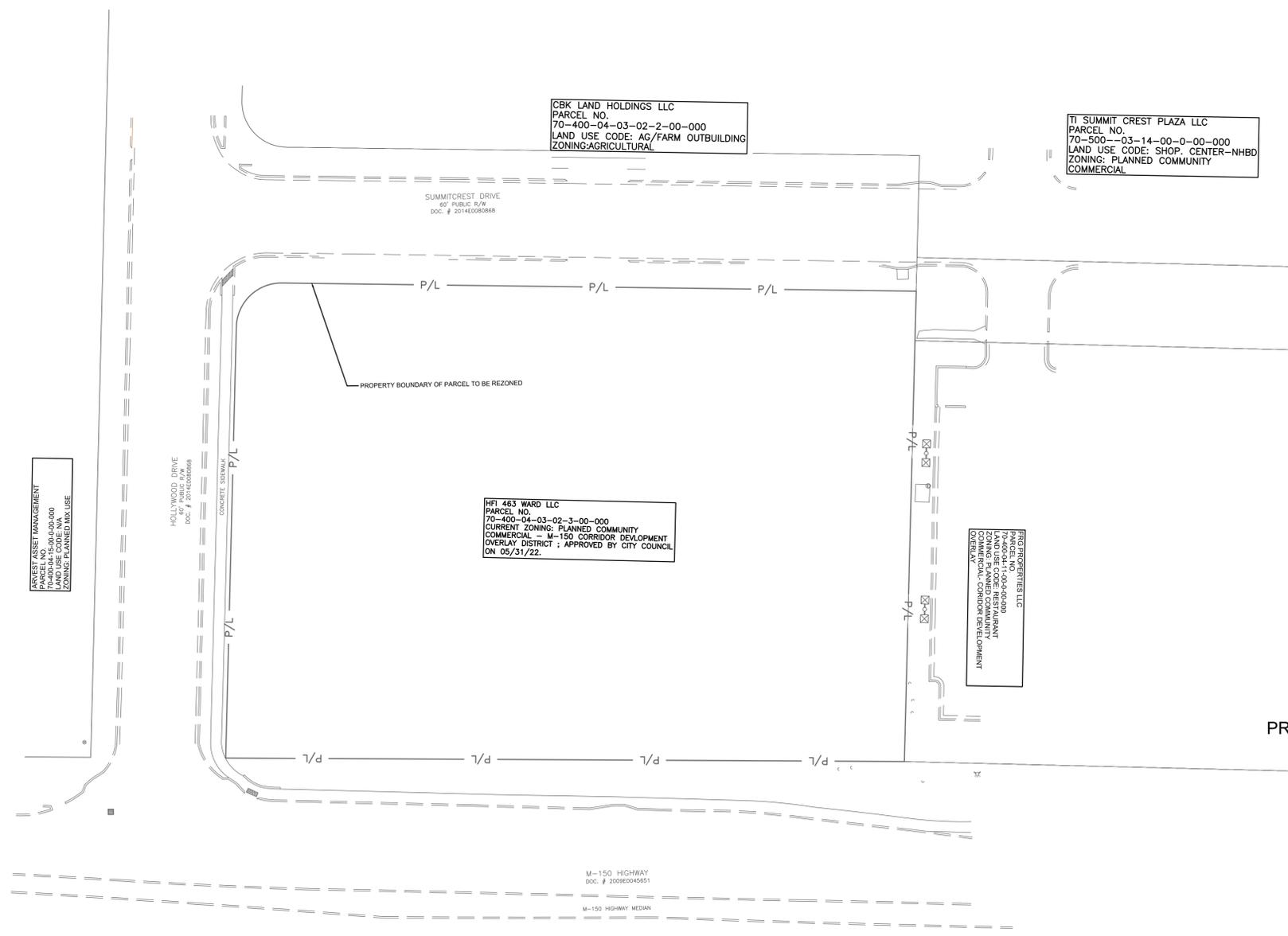
DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21

DRAWING
C-3.1



ZONING MAP
NTS

PROPOSED SITE LOCATION



CBK LAND HOLDINGS LLC
PARCEL NO.
70-400-04-03-02-2-00-000
LAND USE CODE: AG/FARM OUTBUILDING
ZONING: AGRICULTURAL

TI SUMMIT CREST PLAZA LLC
PARCEL NO.
70-500-03-14-00-0-00-000
LAND USE CODE: SHOP. CENTER-NHBD
ZONING: PLANNED COMMUNITY COMMERCIAL

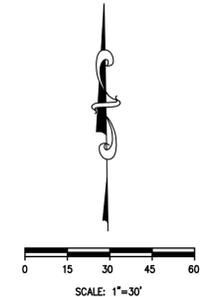
HFI 463 WARD LLC
PARCEL NO.
70-400-04-03-02-3-00-000
CURRENT ZONING: PLANNED COMMUNITY COMMERCIAL - M-150 CORRIDOR DEVELOPMENT OVERLAY DISTRICT ; APPROVED BY CITY COUNCIL ON 05/31/22.

FRUITBERRY BERRIES LLC
Parcel No. 70-400-04-03-02-3-00-000
CURRENT ZONING: PLANNED COMMUNITY COMMERCIAL - M-150 CORRIDOR DEVELOPMENT OVERLAY DISTRICT

ADJUST ASSET MANAGEMENT
PARCEL NO. 70-910-20-01-00-0-00-000
LAND USE CODE: BANK
ZONING: PLANNED COMMERCIAL - M-150 CORRIDOR DEVELOPMENT OVERLAY DISTRICT

BANK OF LEE'S SUMMIT
PARCEL NO.
70-910-20-01-00-0-00-000
LAND USE CODE: BANK
ZONING: PLANNED COMMUNITY COMMERCIAL - M-150 CORRIDOR DEVELOPMENT OVERLAY DISTRICT

TALLGRASS HOLDINGS LLC
PARCEL NO.
70-910-19-03-00-0-00-000
LAND USE CODE: RESTAURANT
ZONING: PLANNED COMMUNITY COMMERCIAL - M-150 CORRIDOR DEVELOPMENT OVERLAY DISTRICT



SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
FINAL DEVELOPMENT SET		06/28/22
RESPONSE TO FDP COMMENTS		08/30/22
RESPONSE TO FDP COMMENTS II		09/29/22
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RESPONSE TO FDP COMMENTS IV		11/03/22
PT22M PROGRESS SET		03/08/23
ISSUE FOR PERMIT		03/15/23

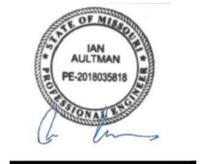
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PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
CONCRETE JOINTING PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

C-4.0

GENERAL NOTES:

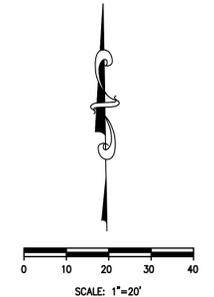
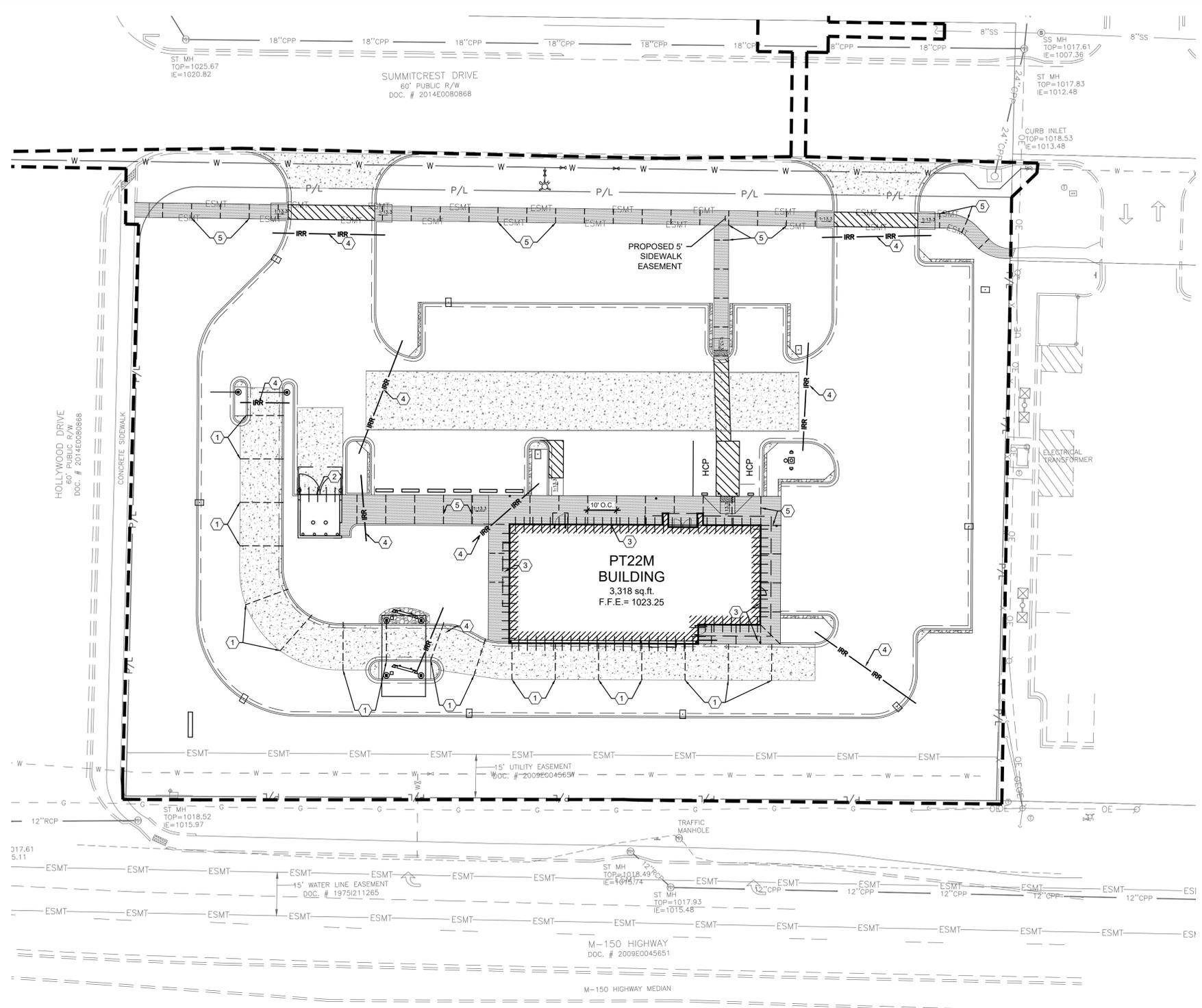
- A. PAVEMENT SPECIFICATION AND RECOMMENDATIONS TO BE TAKEN FROM GEOTECHNICAL REPORT.
- B. PORTLAND CEMENT CONCRETE SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 4,000 PSI.
- C. MAXIMUM CONTROL JOINT SPACING SHALL NOT EXCEED 15 FEET.
- D. EXPANSION JOINTS SHALL BE USED WHEREVER THE PAVEMENT WILL ABUT A STRUCTURAL ELEMENT SUBJECT TO DIFFERENT MAGNITUDE OF MOVEMENT (E.G., LIGHT POLES, RETAINING WALLS, EXISTING PAVEMENT, STAIRWAYS, ENTRYWAY PIERS, BUILDING WALLS, MANHOLES, ETC.)
- E. EXPANSION JOINTS SHALL BE SEALED PER DETAILS ON SHEET C-7.0 TO MINIMIZE MOISTURE INFILTRATION INTO SUBGRADE SOILS AND RESULTANT CONCRETE DETERIORATION AT THE JOINTS.
- F. SLEEVES SHOWN ARE FOR IRRIGATION ONLY. ADDITIONAL SLEEVES MAY BE REQUIRED FOR OTHER FRANCHISE UTILITIES. CONTRACTOR SHALL COORDINATE LOCATION AND SUPPLY ADDITIONAL SLEEVES REQUIRED FOR ELECTRICAL AND TELECOMMUNICATION SERVICES.
- G. ALL CONCRETE JOINTS SHALL RUN CONTINUOUSLY THROUGH CURBS.

KEYED NOTES:

- ① SAWED CONSTRUCTION JOINT REQUIRED, TYPICAL. SEE DETAIL ON SHEET C-7.0.
- ② DOWELED EXPANSION JOINT REQUIRED, TYPICAL. SEE DETAIL ON SHEET C-7.0.
- ③ EXPANSION JOINT REQUIRED WHERE CONCRETE OR CURB ABUTS BUILDING FOUNDATION, STORM STRUCTURE, FLUME, OR SIDEWALK OPENING. SEE DETAIL ON SHEET C-7.0.
- ④ SCHEDULE 40 PVC IRRIGATION SLEEVE. SEE SHEET I-1.0 FOR MORE INFORMATION.
- ⑤ INSTALL CONTRACTION JOINTS 10' ON CENTER AS SHOWN ON DETAIL C ON SHEET C-7.0 (TYP).

LEGEND

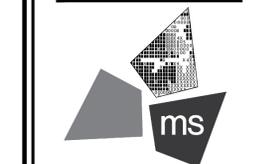
PROPOSED	DESCRIPTION
-----	CONTRACTION JOINT
	EXPANSION JOINT
---IRR---	4" SCHEDULE 40 PVC SLEEVE
[Hatched Box]	CONCRETE SIDEWALK
[White Box]	HEAVY DUTY ASPHALT PAVEMENT
[Dotted Box]	HEAVY DUTY CONCRETE PAVEMENT



N:\03162-40497\21-Lee's Summit, MO MarketDocs\CAD\CIVIL\DWG-set\C-4.0 Concrete Jointing Plan.dwg, 3/15/2023 2:34 PM, boley, jason

SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
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RESPONSE TO FDP COMMENTS IV		11/03/22
PT22M PROGRESS SET		03/08/23
ISSUE FOR PERMIT		03/15/23

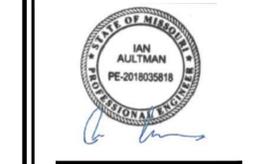
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2221 Schrock Road
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phone 614.898.7100
fax 614.898.7570

PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE GRADING PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21

DRAWING
C-5.0

GENERAL NOTES:

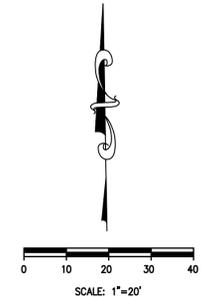
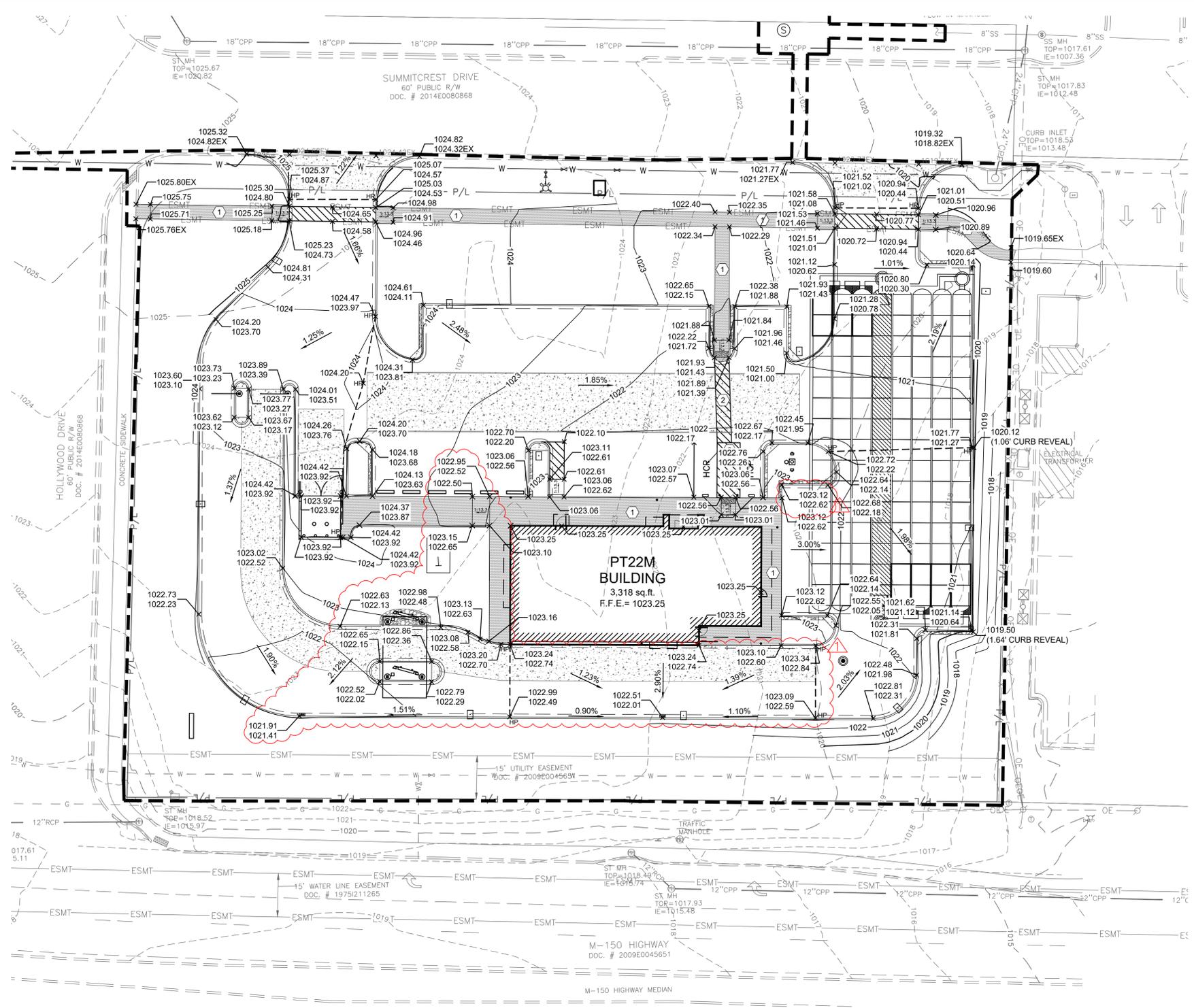
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- ALL CONSTRUCTION METHODS AND MATERIAL MUST CONFORM TO CURRENT STANDARDS AND SPECIFICATIONS OF THE FEDERAL, STATE, COUNTY, CITY OR LOCAL REQUIREMENTS, WHICHEVER HAS JURISDICTION.
- ALL PROPOSED SPOT ELEVATIONS SHOWN ARE TOP OF CURB AND FINAL GRADE ELEVATIONS UNLESS OTHERWISE NOTED.
- CONTRACTOR IS RESPONSIBLE FOR LOCATING AND VERIFYING ALL EXISTING UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO VERIFY ALL EXISTING GRADES AND CONTACT ENGINEER PRIOR TO BEGINNING WORK IF DISCREPANCY IS FOUND. CONTRACTOR TO VERIFY ASSUMED FINISHED FLOOR ELEVATION PRIOR TO BEGINNING WORK.
- THE EXCAVATING CONTRACTOR MUST TAKE PARTICULAR CARE WHEN EXCAVATING IN AND AROUND EXISTING UTILITY LINES AND EQUIPMENT. VERIFY COVER REQUIREMENTS BY UTILITY CONTRACTORS AND/OR UTILITY COMPANIES SO AS TO NOT CAUSE DAMAGE.
- THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES 48 HOURS BEFORE CONSTRUCTION IS TO START, TO VERIFY IF ANY UTILITIES ARE PRESENT ON SITE. ALL VERIFICATIONS (LOCATION, SIZE AND DEPTH) SHALL BE MADE BY THE APPROPRIATE UTILITY COMPANIES. WHEN EXCAVATION IS AROUND OR OVER EXISTING UTILITIES, THE CONTRACTOR MUST NOTIFY THE UTILITY SO A REPRESENTATIVE OF THAT UTILITY COMPANY CAN BE PRESENT TO INSTRUCT AND OBSERVE DURING CONSTRUCTION.
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- ALL STORM CONDUITS ARE ADS N-12 SMOOTH INTERIOR HDPE PIPE OR APPROVED EQUAL, UNLESS OTHERWISE NOTED.
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- THE CONTRACTOR SHALL CONTACT THE CITY'S DEVELOPMENT SERVICES ENGINEERING INSPECTION TO SCHEDULE A PRE-CONSTRUCTION MEETING WITH A FIELD ENGINEERING INSPECTOR PRIOR TO ANY LAND DISTURBANCE WORK AT (816) 969-1200.

KEYED NOTES:

- CONTRACTOR TO MAINTAIN 2.00% MAX CROSS SLOPE ON SIDEWALK.
- CONTRACTOR TO MAINTAIN MAX 2.00% SLOPE IN ALL DIRECTIONS IN HANDICAP ACCESSIBLE AREA.

LEGEND

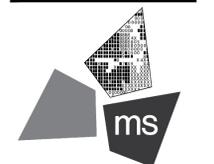
EXISTING	PROPOSED	DESCRIPTION
X 950.00 EX. X 949.50 EX.	X 950.00 X 949.50	TOP OF CURB TOP OF PAVEMENT
X 950.00 EX.	X 950.00	FINISHED GRADE SPOT ELEVATION
	1.00%	GRADE SLOPE
HP	HP	HIGH POINT
ST	ST	STORM MAIN
RD	RD	ROOF DRAIN / UNDERDRAIN
950	950	CONSTRUCTION LIMITS
950	950	MAJOR CONTOUR LINES
950	950	MINOR CONTOUR LINES
		INLET
		CATCH BASIN
		STORM MANHOLE
		MAJOR FLOOD ROUTING



N:\03162-40497\21-Lee's Summit, MO MarketDocs\CAD\CIVIL\DWG-set\C-5.0 Site Grading and Drainage Plan.dwg, 3/15/2023 2:35 PM, boley, jason

SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
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ISSUE FOR PERMIT		03/15/23

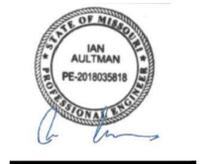
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fax 614.898.7570

PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE DRAINAGE PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

GENERAL NOTES:

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KEYED NOTES:

- PROPOSED CURB INLET AND FINGER DRAIN. SEE DETAILS ON SHEETS C-7.1 AND C-7.5.
- PROPOSED STORM MANHOLE/JUNCTION BOX. SEE DETAILS ON SHEET C-7.5 AND C-7.6.
- PROPOSED UNDERGROUND DETENTION SYSTEM. ADS STORMTECH MC-3500 CHAMBER SYSTEM, 19665 CF. SEE DETAILS ON SHEET C-7.5.
- PROPOSED DOWNSPOUT COLLECTOR SYSTEM.
- 6" STORM LINE FROM ROOF DRAIN TO CONNECT TO PROPOSED STORM SYSTEM. CONTRACTOR TO MAINTAIN A MINIMUM SLOPE OF 1.00% ON ALL ROOF DRAIN PIPES.
- PROPOSED STORM CLEANOUT. SEE DETAIL ON SHEET C-7.1 FOR DETAILS.
- PROPOSED OUTLET CONTROL STRUCTURE. SEE DETAIL ON SHEET C-7.6 FOR DETAILS.

LEGEND

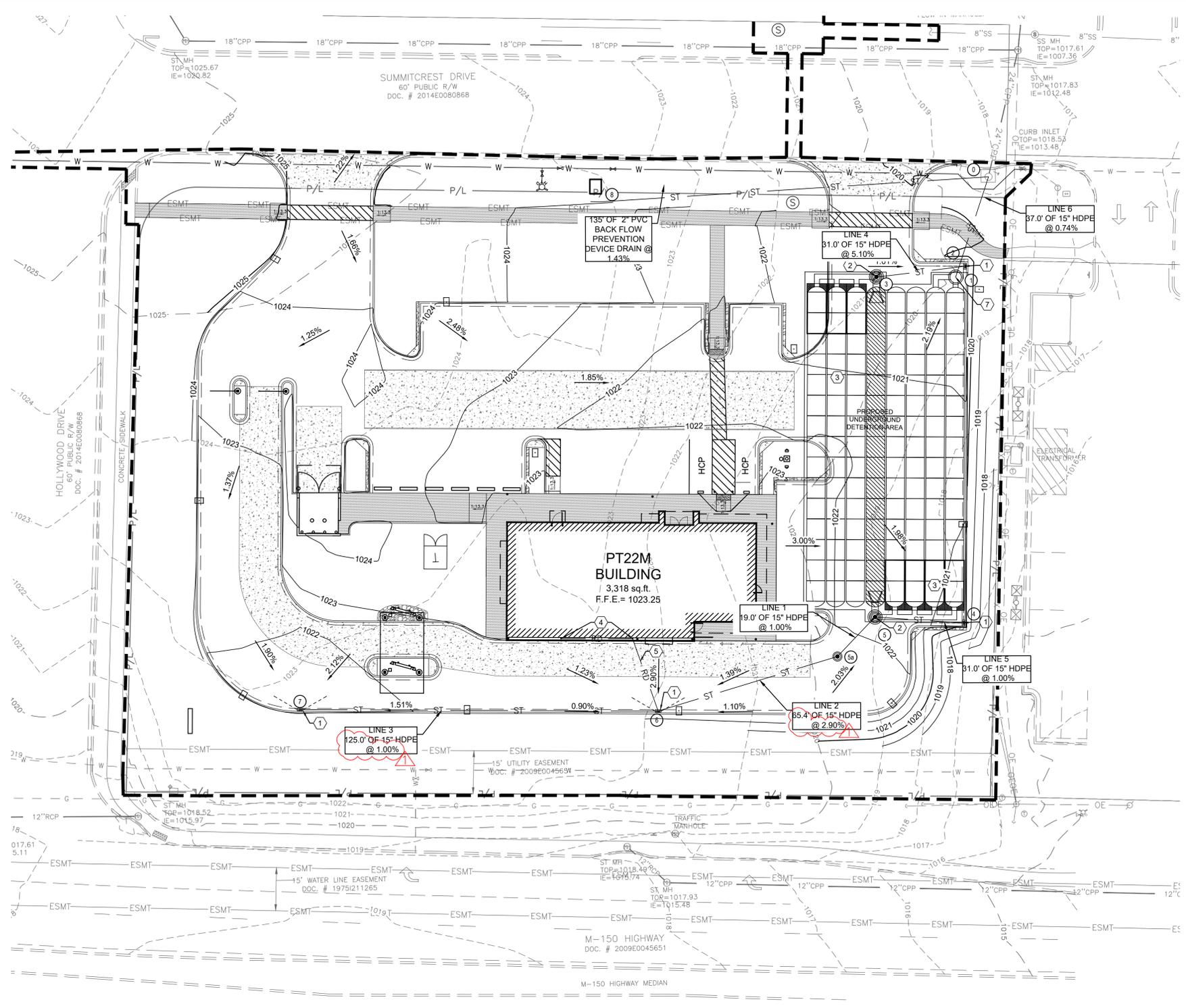
EXISTING	PROPOSED	DESCRIPTION
X 950.00 EX. X 949.50 EX.	X 950.00 X 950.00	TOP OF CURB TOP OF PAVEMENT
X 950.00 EX.	X 950.00	FINISHED GRADE SPOT ELEVATION
	1.00%	GRADE SLOPE
	HP	HIGH POINT
	ST	STORM MAIN
	RD	ROOF DRAIN / UNDERDRAIN
	---	CONSTRUCTION LIMITS
950	950	MAJOR CONTOUR LINES
	950	MINOR CONTOUR LINES
	INLET	INLET
	CATCH BASIN	CATCH BASIN
	STORM MANHOLE	STORM MANHOLE
	MAJOR FLOOD ROUTING	MAJOR FLOOD ROUTING

DETENTION SUMMARY

DESIGNED DETENTION VOLUME	AS-BUILT DETENTION VOLUME	REQUIRED DETENTION VOLUME
20,540 CU. FT.		19,000 CU. FT.

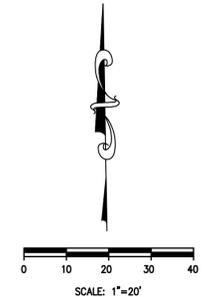
STORM STRUCTURE DATA

- EXISTING STORM INLET
RIM = 1018.53
EX: 24" INV. (NE) = 1013.48
PR: 15" INV (SW) = 1013.48
PR: 2" INV (W) = 1014.56
- PROPOSED OUTLET CONTROL STRUCTURE (48" DIA. CONCRETE MANHOLE)
RIM = 1020.30
PR: 15" INV = 1013.75
- PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1020.16
PR: 15" INV OUT (W) = 1016.06
- PROPOSED STORM MANHOLE (48" DIA. CONCRETE MANHOLE)
RIM = 1020.54
PR: 15" INV IN (E) = 1015.75
PR: BOTTOM INV = 1013.75
- PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1020.67
PR: 15" INV. OUT (W) = 1014.06
- PROPOSED STORM MANHOLE (48" DIA. CONCRETE MANHOLE)
RIM = 1021.58
PR: 15" INV IN (SW,E) = 1013.75
PR: BOTTOM INV = 1013.75
- PROPOSED STORM MANHOLE (48" DIA. CONCRETE MANHOLE)
RIM = 1022.25
PR: 15" INV OUT (NE) = 1013.94
PR: 15" INV IN (SW) = 1014.14
- PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1022.01
PR: 6" INV IN (NW) = 1016.80
PR: 15" INV OUT (NE) = 1016.05
PR: 15" INV IN (W) = 1016.25
- PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1021.41
PR: 15" INV OUT (E) = 1017.50
- PROPOSED 2" BACKFLOW VAULT DRAIN
RIM = 1021.75
PR: 2" INV (S) = 1015.89



Storm Sewer Summary Report

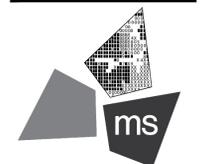
Line No.	Line ID	Flow rate (cfs)	Line Size (in)	Line shape	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line Slope (%)	HGL Down (ft)	HGL Up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns Line No.	Junction Type
1	Pipe - 13	2.73	15	Cir	18.974	1013.75	1013.94	1.001	1014.41	1014.60	0.14	1014.60	End	Manhole
2	Pipe - 10	2.79	15	Cir	65.400	1014.14	1016.05	2.920	1014.60	1016.72	n/a	1016.72	1	Curb-
3	Pipe - 9	1.96	15	Cir	125.000	1016.25	1017.50	1.000	1016.72	1018.06	n/a	1018.06	2	Curb-
4	Pipe - 12	2.71	15	Cir	31.102	1013.75	1015.34	5.112	1014.41	1016.00	0.26	1016.00	End	Curb-
5	Pipe - 14	1.02	15	Cir	30.930	1013.75	1014.06	1.002	1014.15	1014.46	n/a	1014.46	End	Curb-
6	Pipe - 11	0.19	15	Cir	36.725	1013.48	1013.75	0.735	1013.65	1013.92	0.06	1013.92	End	None



N:\03162-40497\21-Lee's Summit, MO MarketDocs\CAD\CIVIL\DWG-set\C-5.0 Site Grading and Drainage Plan.dwg, 3/15/2023 2:35 PM, boley, jason

NOTICE

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phone 614.898.7100
fax 614.898.7570

PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

STORM SEWER PLAN AND PROFILE



DRAWN BY: TDB

CHECKED BY: PGD

PROJECT NO: 40497-21

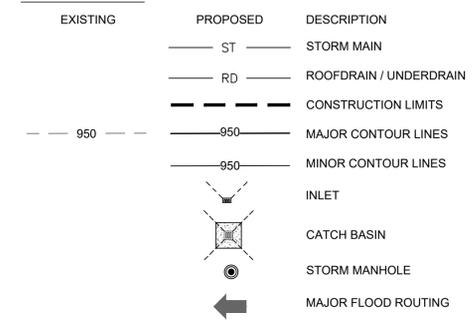
DRAWING

C-5.2

STORM STRUCTURE DATA

- ① EXISTING STORM INLET
RIM = 1018.53
EX: 24" INV. (NE) = 1013.48
PR: 15" INV. (SW) = 1013.48
PR: 2" INV. (W) = 1014.56
- ② PROPOSED OUTLET CONTROL STRUCTURE (48" DIA. CONCRETE MANHOLE)
RIM = 1020.30
PR: 15" INV = 1013.75
- ③ PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1020.16
PR: 15" INV. OUT (W) = 1016.06
- ④ PROPOSED STORM MANHOLE (48" DIA. CONCRETE MANHOLE)
RIM = 1020.54
PR: 15" INV IN (E) = 1015.75
PR: BOTTOM INV = 1013.75
- ⑤ PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1020.67
PR: 15" INV. OUT (W) = 1014.06
- ⑥ PROPOSED STORM MANHOLE (48" DIA. CONCRETE MANHOLE)
RIM = 1021.58
PR: 15" INV IN (SW,E) = 1013.75
PR: BOTTOM INV = 1013.75
- ⑦ PROPOSED STORM MANHOLE (48" DIA. CONCRETE MANHOLE)
RIM = 1022.25
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PR: 15" INV IN (SW) = 1014.14
- ⑧ PROPOSED 2'X3' CONCRETE CURB INLET
RIM = 1022.01
PR: 6" INV IN (NW) = 1016.80
PR: 15" INV. OUT (NE) = 1016.05
PR: 15" INV IN (W) = 1016.25
PR: 2'X3' CONCRETE CURB INLET
RIM = 1021.41
PR: 15" INV. OUT (E) = 1017.50
- ⑨ PROPOSED 2" BACKFLOW VAULT DRAIN
RIM = 1021.75
PR: 2" INV. (S) = 1015.89

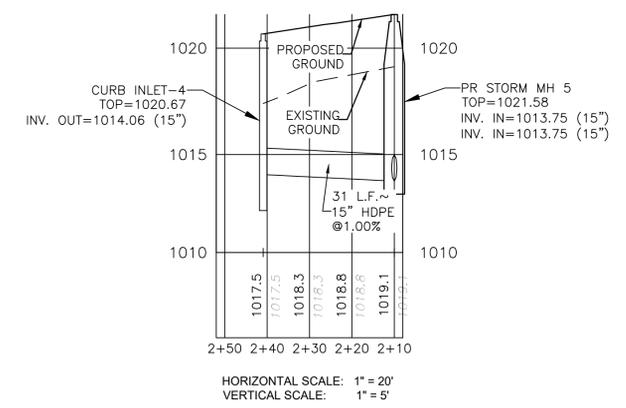
LEGEND



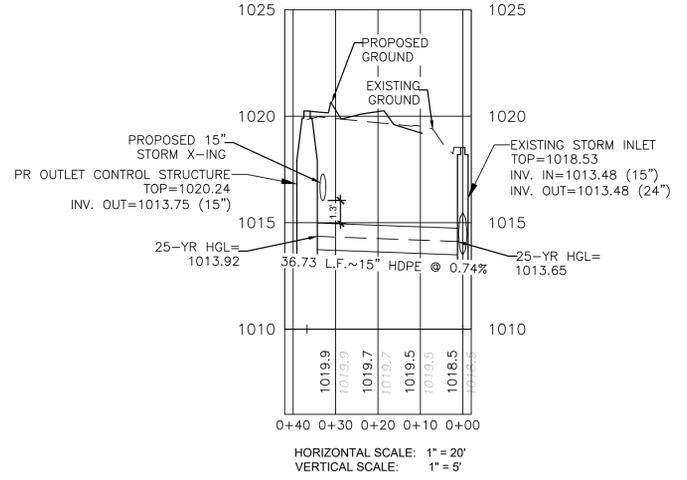
KEYED NOTES:

- ① PROPOSED CURB INLET AND FINGER DRAIN. SEE DETAILS ON SHEETS C-7.1 AND C-7.5.
- ② PROPOSED STORM MANHOLE/JUNCTION BOX. SEE DETAILS ON SHEET C-7.5 AND C-7.6.
- ③ PROPOSED UNDERGROUND DETENTION SYSTEM, ADS STORMTECH MC-3500 CHAMBER SYSTEM, 19665 CF. SEE DETAILS ON SHEET C-7.5.
- ④ PROPOSED DOWNSPOUT COLLECTOR SYSTEM.
- ⑤ 6" STORM LINE FROM ROOF DRAIN TO CONNECT TO PROPOSED STORM SYSTEM. CONTRACTOR TO MAINTAIN A MINIMUM SLOPE OF 1.00% ON ALL ROOF DRAIN PIPES.
- ⑥ PROPOSED STORM CLEANOUT. SEE DETAIL ON SHEET C-7.1 FOR DETAILS.
- ⑦ PROPOSED OUTLET CONTROL STRUCTURE. SEE DETAIL ON SHEET C-7.6 FOR DETAILS.

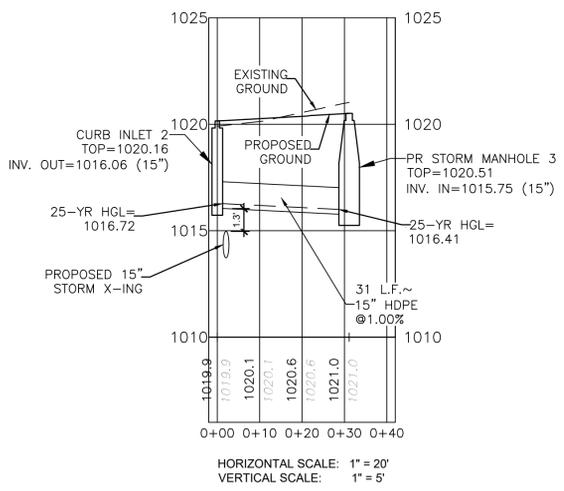
CURB INLET-4 TO STORM MH-5



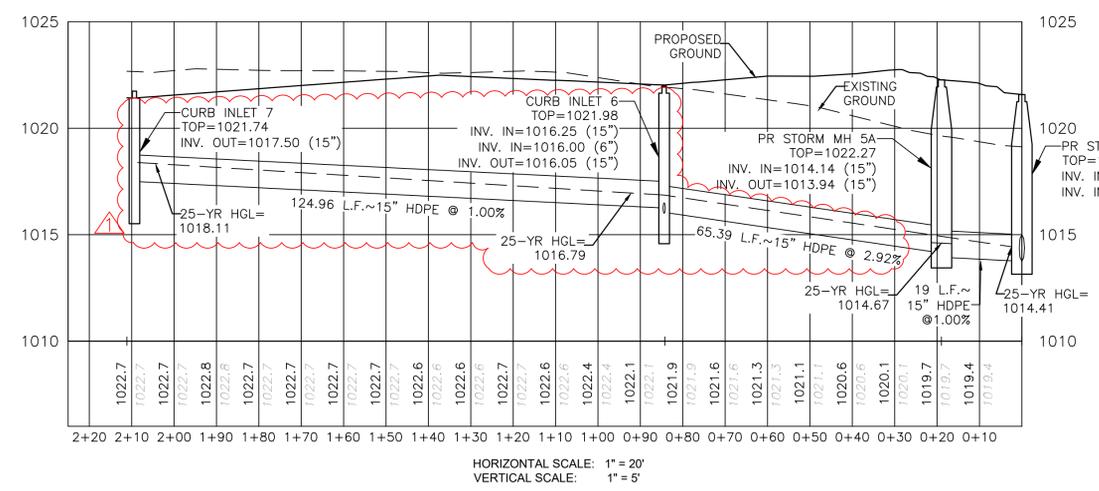
OUTLET CONTROL STRUCTURE TO EXISTING STORM INLET



CURB INLET 2 TO STORM MH 3



CURB INLET 7 TO STORM MH 5

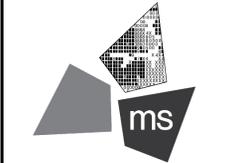


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SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
FINAL DEVELOPMENT SET		06/29/22
RESPONSE TO FDP COMMENTS I		08/30/22
RESPONSE TO FDP COMMENTS II		09/29/22
RESPONSE TO FDP COMMENTS III		10/24/22
RESPONSE TO FDP COMMENTS IV		11/03/22
PT22M PROGRESS SET		03/08/23
ISSUE FOR PERMIT		03/15/23

NOTICE

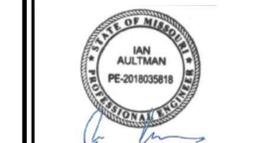
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fax 614.898.7570

PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE UTILITY PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

C-6.0

GENERAL NOTES:

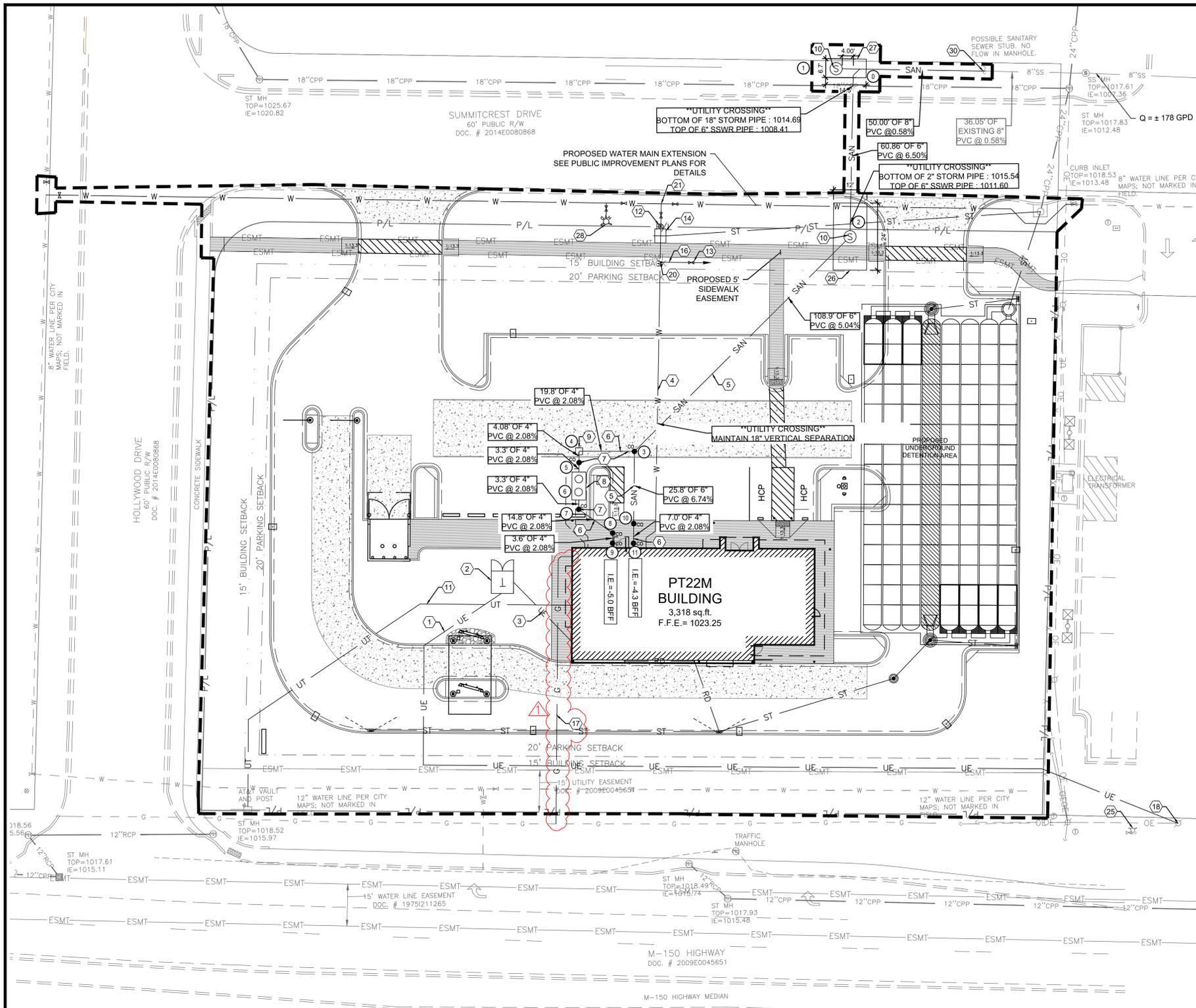
- ALL EXISTING CONDITIONS, TOPOGRAPHY, UTILITIES AND PROPERTY INFORMATION ARE TAKEN FROM A SURVEY OF LAND SITUATED IN THE CITY OF LEE'S SUMMIT, COUNTY OF JACKSON AND STATE OF MISSOURI, BY SURVEYOR: YOUNG-HOBBS AND ASSOCIATES, 1202 CROSSLAND AVENUE CLARKSVILLE, TN 37040.
- ALL EXISTING UTILITIES ARE TAKEN FROM SURVEY AND DO NOT NECESSARILY REPRESENT ALL UNDERGROUND UTILITIES ADJACENT TO OR UPON PREMISES SHOWN ON PLAN.
- CONTRACTOR RESPONSIBLE FOR MAINTAINING A MIN. COVER OF 42" OVER PROPOSED WATER SERVICE.
- CONTRACTOR IS RESPONSIBLE FOR LOCATING AND VERIFYING ALL EXISTING UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION, AND IS RESPONSIBLE FOR ANY DAMAGE TO THEM DURING CONSTRUCTION.
- CLEANOUT LOCATIONS ARE NUMBERED ON PLAN. ALL CLEANOUTS IN PAVEMENT AREAS ARE TO BE H-20 RATED. CLEANOUTS SHALL BE INSTALLED PER DETAIL ON SHEET C-7.1.
- SEE PLUMBING PLANS FOR CONTINUATION OF UTILITY LINES INTO BUILDING.
- CONTRACTOR SHALL INSTALL AND BACKFILL ALL TRENCHES AND STRUCTURES PER DETAIL ON SHEET C-7.1.
- STORM SEWER SHOWN HERE FOR REFERENCE ONLY. SEE GRADING PLAN FOR DESIGN DATA.
- THERE SHALL BE A MINIMUM 10 FOOT HORIZONTAL SEPARATION BETWEEN WATER TAPS, WATER SERVICES, PRIVATE WATER SYSTEMS, AND ANY SANITARY AND/OR STORM SEWER SYSTEMS. WHERE 10 FEET HORIZONTAL SEPARATION CANNOT BE OBTAINED, THE BOTTOM OF THE WATER LINE SHALL BE AT LEAST 18 INCHES ABOVE THE TOP OF THE SEWER.
- THE EXCAVATING CONTRACTOR MUST TAKE PARTICULAR CARE WHEN EXCAVATING IN AND AROUND EXISTING UTILITY LINES AND EQUIPMENT. VERIFY COVER REQUIREMENTS BY UTILITY CONTRACTORS AND/OR UTILITY COMPANIES SO AS TO NOT CAUSE DAMAGE.
- CAUTION: OVERHEAD LINES ARE PRESENT ON SITE. CONTRACTOR TO TAKE SPECIAL CARE TO PREVENT DAMAGE TO THE LINES AND COORDINATE WITH UTILITY OWNER.
- A SANITARY SEWER IMPACT STATEMENT THAT WILL ADDRESS THE PROPOSED DISCHARGE INTO THE EXISTING SANITARY SEWER RECEIVING SYSTEM, IF REQUIRED BY THE CITY ENGINEER.
- APPROPRIATE WATER SERVICE DEMAND DATA (INCLUDING, BUT NOT LIMITED TO, PLANNED LAND USAGE, DENSITIES OF PROPOSED DEVELOPMENT, PIPE SIZES, CONTOURS AND FIRE HYDRANT LAYOUT) TO ALLOW FOR THE PRELIMINARY ANALYSIS OF THE DEMAND FOR WATER SERVICE IF REQUIRED BY THE CITY ENGINEER.
- ALL CONSTRUCTION SHALL FOLLOW THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL AS ADOPTED BY ORDINANCE 5813. WHERE DISCREPANCIES EXIST BETWEEN THESE PLANS AND THE DESIGN AND CONSTRUCTION MANUAL, THE DESIGN AND CONSTRUCTION MANUAL SHALL PREVAIL.
- THE CONTRACTOR SHALL CONTACT THE CITY'S DEVELOPMENT SERVICES ENGINEERING INSPECTION TO SCHEDULE A PRE-CONSTRUCTION MEETING WITH A FIELD ENGINEERING INSPECTOR PRIOR TO ANY LAND DISTURBANCE WORK AT (816) 989-1200.

KEYED NOTES:

- PROPOSED PRIMARY ELECTRICAL SERVICE. CONTRACTOR TO COORDINATE CONDUIT SIZE, NUMBER OF CONDUITS, CONNECTIONS, AND BEND RADII WITH UTILITY OWNER AND MEP PLANS. CONTRACTOR TO COORDINATE CONNECTION WITH UTILITY OWNER.
- PROPOSED ELECTRIC TRANSFORMER. COORDINATE DETAILS AND LOCATION WITH UTILITY OWNER AND ELECTRICAL PLANS.
- PROPOSED SECONDARY ELECTRICAL SERVICE. CONTRACTOR TO COORDINATE CONDUIT SIZE, NUMBER OF CONDUITS, CONNECTIONS, AND BEND RADII WITH UTILITY OWNER AND MEP PLANS. CONTRACTOR TO COORDINATE CONNECTION WITH UTILITY OWNER.
- PROPOSED SOFT TYPE K COPPER 1.5" DOMESTIC WATER SERVICE. INCLUDE IN BASE BID ALL VALVES, PIPING, STRUCTURES, ETC. THAT WILL BE REQUIRED. SEE MEP PLANS FOR CONTINUATION INTO BUILDING. SEE DETAIL ON SHEET C-7.2.
- PROPOSED 6" SANITARY SEWER. ASTM D3034, SDR-26. SEWER TO HAVE MINIMUM SLOPE OF 1.00%. CONTRACTOR TO MAINTAIN A MINIMUM OF 36" OF COVER OF SEWER LINES.
- PROPOSED 4" SANITARY SEWER. ASTM D3034, SDR-26. SEWER TO HAVE MINIMUM SLOPE OF 2.08%. CONTRACTOR TO MAINTAIN A MINIMUM OF 36" OF COVER OF SEWER LINES.
- PROPOSED SANITARY CLEANOUT (TYP.). SEE DETAIL ON SHEET C-7.1.
- GREASE TRAP REQUIRED. SEE PLUMBING SHEETS FOR DETAILS.
- MONITORING WELL. SEE PLUMBING PLANS FOR DETAILS.
- PROPOSED SANITARY SEWER SERVICE MANHOLE. SEE DETAIL ON SHEET C-7.2.
- 2" PVC CONDUIT FOR UNDERGROUND TELEPHONE SERVICE. CONTRACTOR TO COORDINATE CONNECTION WITH UTILITY OWNER.
- PROPOSED 2" DOMESTIC WATER METER PER CITY OF LEE'S SUMMIT STANDARD DRAWING WAT-11. SEE SHEET C-7.2.
- PROPOSED WATER VALVE PER LOCAL REGULATIONS AND DETAILS.
- PROPOSED BACKFLOW PREVENTER VAULT PER CITY OF LEE'S SUMMIT STANDARD DRAWING WAT-12. SEE SHEET C-7.2. A DEPARTMENT OF NATURAL RESOURCES APPROVED DOUBLE CHECK BACKFLOW MUST BE USED.
- PROPOSED PVC C900 6" FIRE SERVICE LINE. INCLUDE IN BASE BID ALL VALVES, PIPING, STRUCTURES, ETC. THAT WILL BE REQUIRED. SEE MEP PLANS FOR CONTINUATION INTO BUILDING.
- PROPOSED 1" PVC IRRIGATION LINE. SEE IRRIGATION PLAN FOR MORE INFORMATION.
- PROPOSED 2" GAS SERVICE LINE.
- PROPOSED ELECTRICAL CONNECTION. CONTRACTOR TO COORDINATE WITH UTILITY OWNER.
- PROPOSED SANITARY SEWER WYE CONNECTION TO EXISTING SANITARY SEWER STUB. SEE DETAIL ON SHEET C-7.2.
- PROPOSED 1" IRRIGATION TAP.
- PROPOSED 2" DOMESTIC WATER TAP WITH SADDLE, CORPORATION STOP, AND 1" METER PER CITY OF LEE'S SUMMIT STANDARD DRAWING WAT-11. SEE SHEET C-7.2.
- PROPOSED 6" FIRE CONNECTION TO WATER MAIN BY TEE THAT WILL BE INSTALLED WITH PUBLIC WATER MAIN EXTENSION.
- PROPOSED 6"x6" 45" ELBOW.
- PROPOSED 6"x6"x6" WYE FITTING.
- EXISTING FIRE HYDRANT
- PROPOSED DIRECTIONAL BORE PIT LOCATION.
- PROPOSED RECEPTION PIT.
- PROPOSED FIRE HYDRANT. REFER TO THE PUBLIC IMPROVEMENT PLANS FOR MORE INFORMATION.
- PROPOSED 8"x8"x6" WYE FITTING.
- PROPOSED 8" MAXADAPTOR COUPLING FOR EXISTING/PROPOSED 8" SANITARY SEWER CONNECTION.

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	CONSTRUCTION LIMITS
---	ST	STORM LINE
---	RD	ROOFDRAIN/UNDERDRAIN
---	SAN	SANITARY LINE
⊙	⊙	SANITARY CLEANOUT
⊙	⊙	SANITARY MANHOLE
⊙	⊙	SANITARY GREASE TRAP
---	W	WATER LINE
⊙	⊙	FIRE HYDRANT
⊙	⊙	WATER METER
---	UE	UNDERGROUND ELECTRIC LINE
---	UT	ELECTRIC TRANSFORMER
---	UT	UNDERGROUND TELEPHONE LINE
⊙	⊙	LIGHT POLE



SANITARY STRUCTURE DATA

⊙ PROPOSED WYE CONNECTION TC: 1017.61 PR. 8" INV (E) = 1007.83 PR. 8" INV (W) = 1007.83 PR. 6" INV (S) = 1007.66	⊙ PROPOSED CLEANOUT TC: 1021.99 PR. 6" INV (S, NE) = 1016.89 PR. 4" INV (W) = 1017.06	⊙ PROPOSED GREASE TRAP TC: 1022.27 PR. 4" INV OUT (N) = 1017.63 PR. 4" INV IN (S) = 1017.8	⊙ PROPOSED CLEANOUT TC: 1023.21 PR. 4" INV (N, S) = 1018.25
⊙ PROPOSED MANHOLE TC: 1020.19 PR. 8" INV (E) = 1007.86	⊙ PROPOSED SAMPLING WELL TC: 1022.42 PR. 4" INV (S) = 1017.47	⊙ PROPOSED CLEANOUT TC: 1022.48 PR. 4" INV (N, SE) = 1017.87	⊙ PROPOSED CLEANOUT TC: 1023.09 PR. 6" INV (N) = 1018.63 PR. 4" INV (S) = 1018.80
⊙ PROPOSED MANHOLE TC: 1021.80 PR. 6" INV (SW) = 1011.40 PR. 6" INV (N) = 1011.20	⊙ PROPOSED CLEANOUT TC: 1022.28 PR. 4" INV (N, S) = 1017.56	⊙ PROPOSED CLEANOUT TC: 1023.12 PR. 4" INV (NW, S) = 1018.17	⊙ PROPOSED CLEANOUT TC: 1023.21 PR. 4" INV (N, S) = 1018.95

NOTE: CONTRACTOR TO VERIFY INVERT OF EXISTING SANITARY SEWER MAIN PRIOR TO CONSTRUCTING PROPOSED SANITARY SEWER SERVICE LINE.



Revision Date: 05/08/2023
Lee's Summit, Missouri

SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
FINAL DEVELOPMENT SET		06/29/22
RESPONSE TO FDP COMMENTS		08/30/22
RESPONSE TO FDP COMMENTS II		09/29/22
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RESPONSE TO FDP COMMENTS IV		11/03/22
PT22M PROGRESS SET		03/08/23
ISSUE FOR PERMIT		03/15/23

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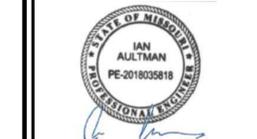
ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT

PROPOSED PT22M BUILDING

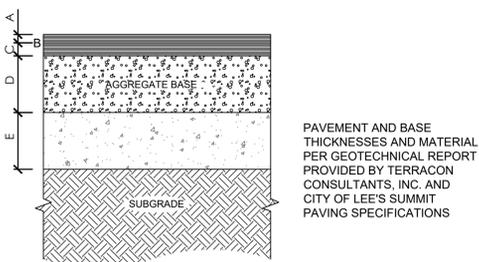
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE DETAILS



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

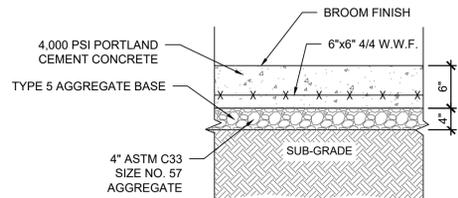
C-7.0



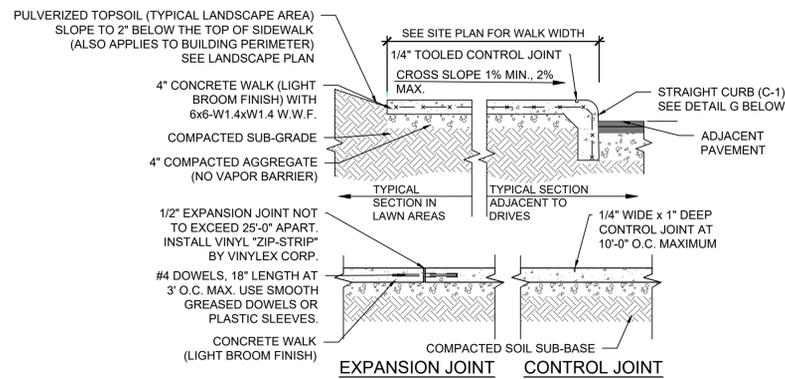
HEAVY DUTY PAVING

A = 1.5" ASPHALT CONCRETE (MIX TYPE 5, SURFACE COURSE)
B = TACK COAT (0.05 GAL/S.Y.)
C = 4" ASPHALT CONCRETE (MIX TYPE 5, BASE COURSE)
D = 6" AGGREGATE BASE (TYPE 5)
E = 6" CHEMICAL STABILIZATION WITH FLY ASH, PORTLAND CEMENT, HYDRATED LIME, QUICKLIME, OR LIME KILN DUST

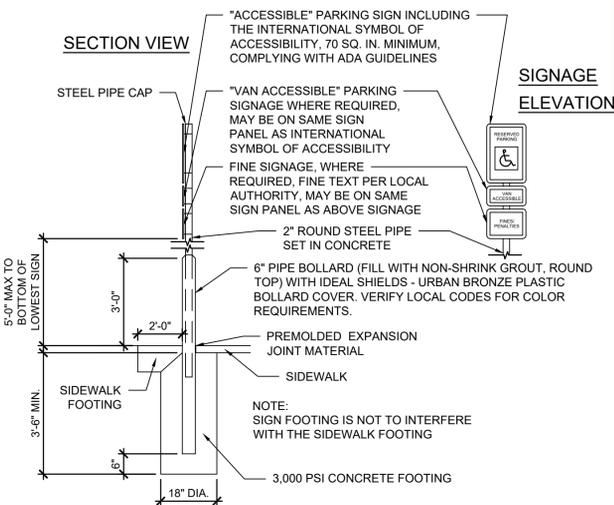
PAVEMENT AND BASE THICKNESSES AND MATERIAL PER GEOTECHNICAL REPORT PROVIDED BY TERRACON CONSULTANTS, INC. AND CITY OF LEE'S SUMMIT PAVING SPECIFICATIONS



PAVEMENT AND BASE THICKNESSES AND MATERIAL PER GEOTECHNICAL REPORT PROVIDED BY TERRACON CONSULTANTS, INC. AND CITY OF LEE'S SUMMIT PAVING SPECIFICATIONS

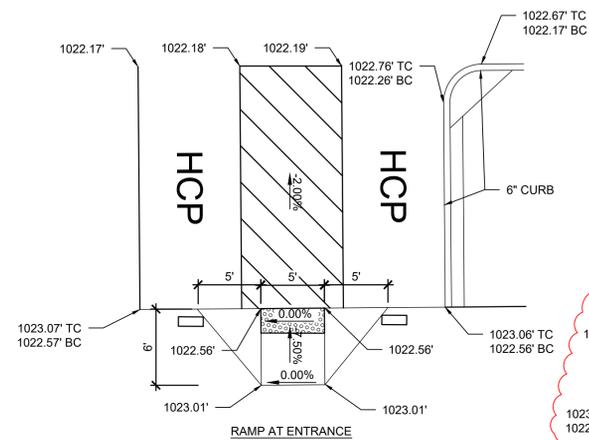


NOTES:
A. LOCATE CONTROL JOINTS AS SHOWN ON PLAN OR 10' O.C. MAXIMUM. VERIFY WITH SITE REPRESENTATIVE.
B. CONCRETE WALKS AGAINST THE BUILDING OR CONCRETE PAVEMENT SHALL HAVE 1/2" PREFORMED EXPANSION JOINT BETWEEN WALK AND BUILDING OR WALK AND CONCRETE PAVEMENT.
C. CONCRETE PADS OVER 4" THICK REQUIRE CONTROL JOINTS TO BE 12' O.C. MAXIMUM.

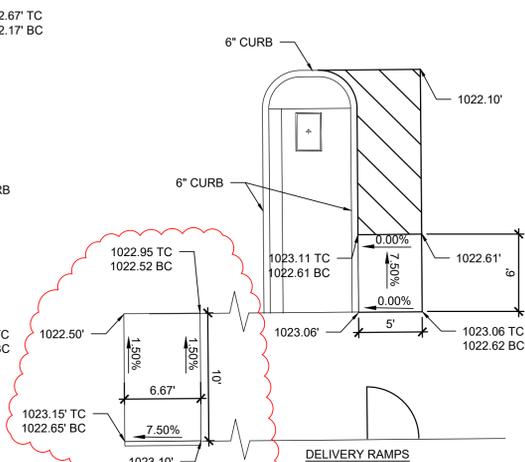


D POLE-MOUNTED HANDICAP PARKING SIGN
C7.0 N.T.S.

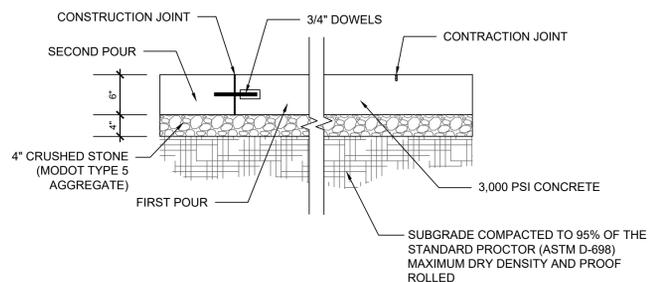
A ASPHALT PAVING SECTION
C9 N.T.S.



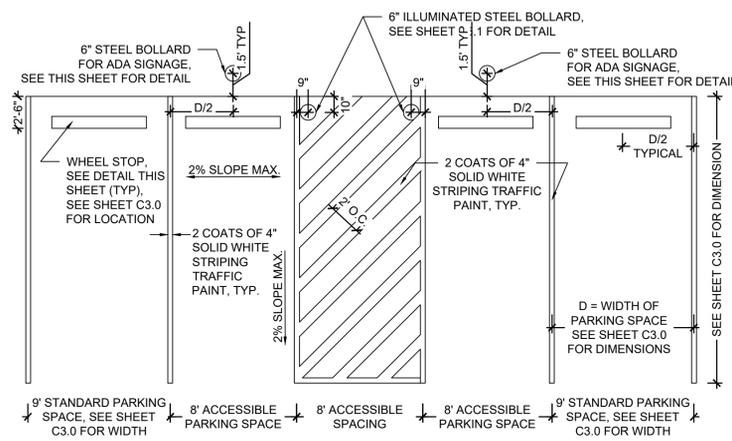
B TRASH ENCLOSURE APRON DETAIL
C9 N.T.S.



C CONCRETE WALK
C9 N.T.S.

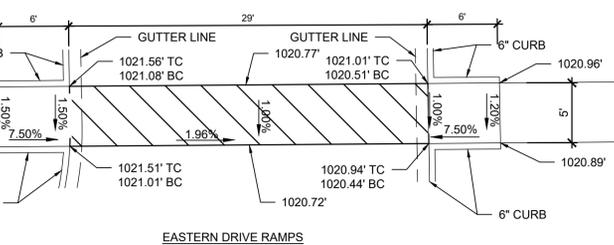


NOTES:
A. SUBGRADE COMPACTION PER SOILS REPORT.
B. ALL SUB-GRADE AND PAVEMENT OPERATIONS AND MATERIALS SHALL MEET THE MINIMUM REQUIREMENTS OF THE CURRENT MODOT SPECIFICATIONS.

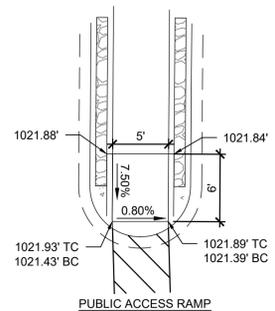


NOTE:
SEE DIMENSIONAL CONTROL PLAN FOR PARKING DIMENSIONS. DIMENSIONS SHOWN ARE MINIMUM REQUIRED.

SLOPE NOTE:
IF CIVIL GRADING PLAN INDICATES A CROSS SLOPE GREATER THAN 2% AT ADA SPACES, CONTRACTOR SHALL IMMEDIATELY NOTIFY ENGINEER FOR CLARIFICATION AND PLAN REVISION.

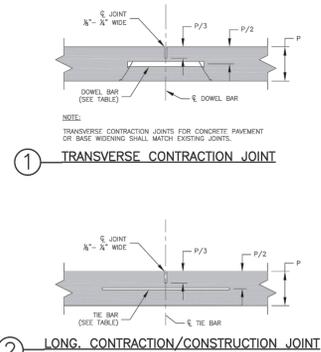


EASTERN DRIVE RAMPS



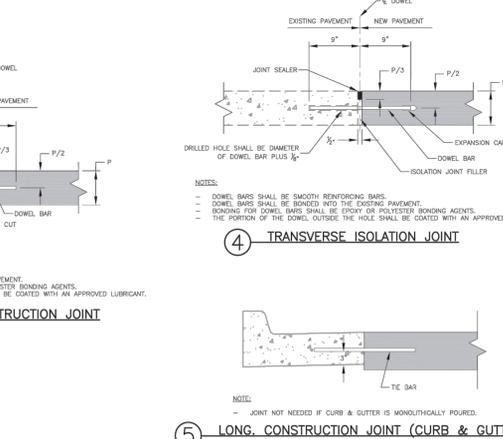
PUBLIC ACCESS RAMP

E HEAVY DUTY CONCRETE PAVEMENT
C7.0 N.T.S.



I PAVEMENT JOINT DETAILS - CITY OF LS STANDARD DETAIL GEN-9
C7.0 N.T.S.

G PARKING STRIPING
C7.0 N.T.S.



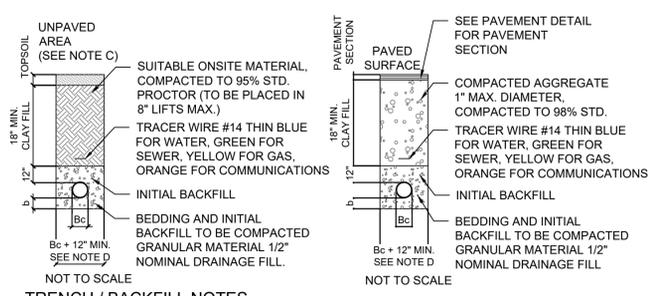
NOTE:
- JOINT NOT NEEDED IF CURB & GUTTER IS MONOLITHICALLY POURED.



N:\031621-040497\21-Lee's Summit, MO Market\Docs\CAD\CIVIL\DWG-set\C-7.0 Site Details.dwg, 3/15/2023 2:38 PM, boley, jason

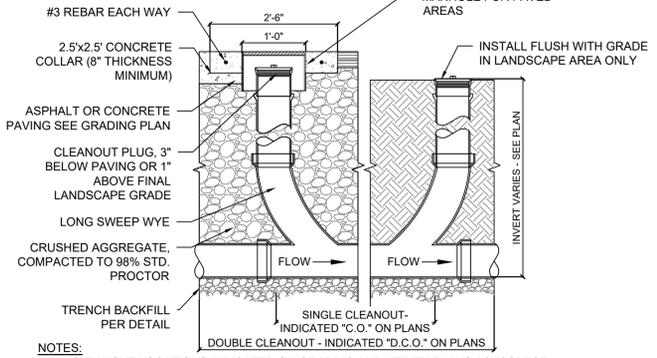
SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
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RESPONSE TO FDP COMMENTS III		10/24/22
RESPONSE TO FDP COMMENTS IV		11/03/22
PT22M PROGRESS SET		03/08/23
ISSUE FOR PERMIT		03/15/23

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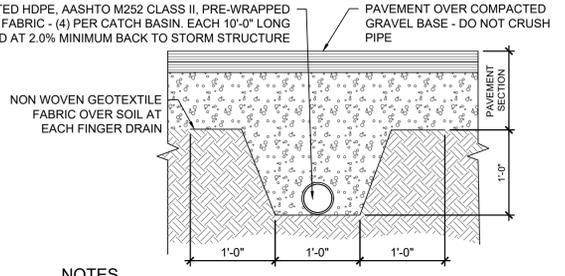
TRENCH / BACKFILL NOTES
A. BEDDING THICKNESS UNDER PIPE BARREL b, SHALL BE 1/8 OF Bc; Bc 6" MIN. Bc IS OUTSIDE DIAMETER OF PIPE AT BELL.
B. THE HAUNCH AREA OF THE PIPE MUST BE FULLY SUPPORTED; THEREFORE THE BEDDING MATERIAL SHALL BE HAND PLACED AND COMPACTED UNDER THE PIPE HAUNCH.
C. IF UNPAVED AREA IS WITHIN 10' OF PAVEMENT OR STRUCTURE THEN FOLLOW TRENCH GUIDELINES FOR PAVED AREA.
D. PIPE DIAMETER OF 4" OR SMALLER SHALL HAVE A MAXIMUM TRENCH WIDTH OF 12".
E. BEDDING AND INITIAL BACKFILL SHALL BE SAND FOR ALL UTILITY CONDUIT CARRYING WATER, ELECTRIC, GAS, AND TELEPHONE.

A TRENCH BACKFILL DETAIL
C7.1 N.T.S.



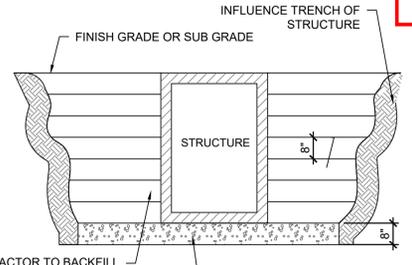
NOTES:
A. CLEANOUT LOCATIONS INDICATED ON GRADING AND UTILITY PLANS AS "CO" FOR SINGLE CLEANOUT AND "DCO" FOR DOUBLE CLEANOUT.
B. PROVIDE CLEANOUTS AS SPECIFIED:
1. ZURN Z-1400 CLEANOUTS IN NON-TRAFFIC AREAS AND SIDEWALKS
2. ZURN-1449 CLEANOUTS IN LANDSCAPED AREAS
3. ZURN Z-1400 HD CLEANOUTS IN TRAFFIC AREAS WITH A "SERVICE STATION" TYPE MANHOLE. OPW #104 A12 - DOVER CORP./OPW DIV.

B PIPE CLEANOUT DETAIL
C7.1 N.T.S.



NOTES
A. THE INTENTION OF THE FINGER DRAIN SYSTEM IS TO PREVENT EXCESS WATER ACCUMULATION AT THE LOW POINTS IN THE GRAVEL BASE AT DRAINAGE STRUCTURES. SYSTEM TO BE INSTALLED TO ASSURE ADEQUATE DRAINAGE OF PAVEMENT BASE.

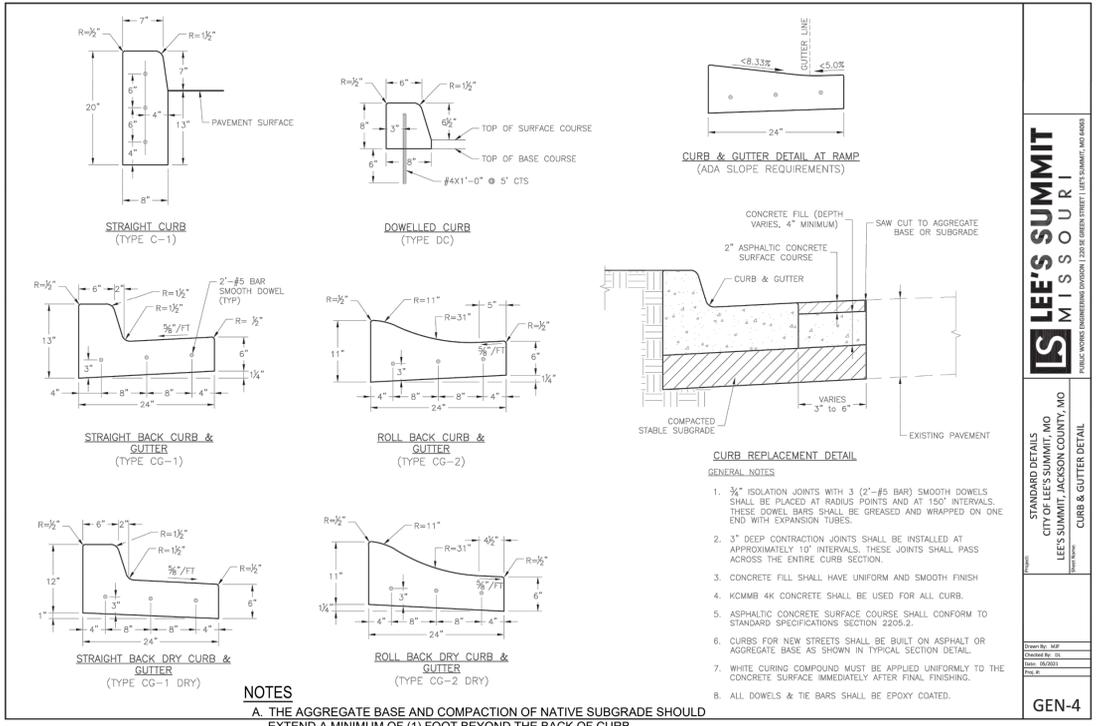
C FINGER DRAIN
C7.1 N.T.S.



CONTRACTOR TO UNDERCUT STRUCTURE BASE BY 8". REPLACE WITH COMPACTED GRANULAR BACKFILL, 95% STANDARD PROCTOR.

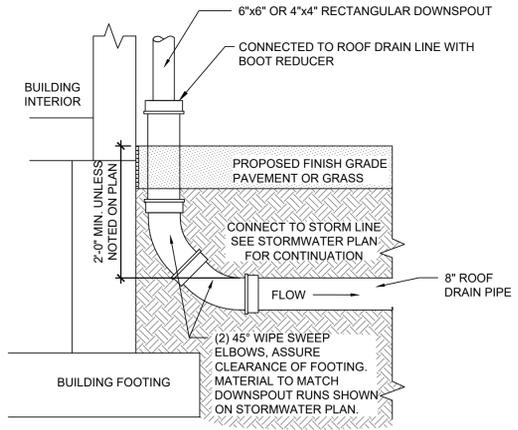
STRUCTURE BACKFILL NOTES
A. BACKFILL TO BE PLACED IN 8" LIFTS
B. NO ON SITE FILL WILL BE ALLOWED FOR UTILITY STRUCTURES.

D STRUCTURE BACKFILL
C7.1 N.T.S.

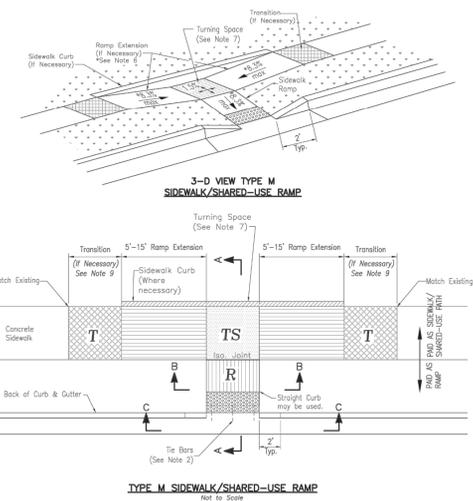


NOTES
A. THE AGGREGATE BASE AND COMPACTION OF NATIVE SUBGRADE SHOULD EXTEND A MINIMUM OF (1) FOOT BEYOND THE BACK OF CURB

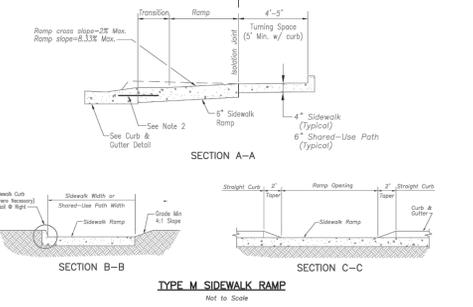
E CONCRETE CURB AND GUTTER DETAIL - CITY OF LS STANDARD DETAIL GEN-4
C7.1 N.T.S.



F EXTERIOR DOWNSPOUT BOOT
C7.1 N.T.S.



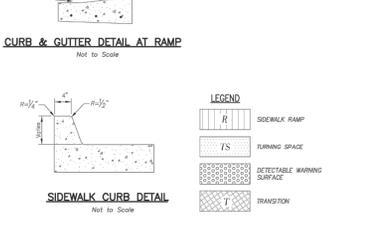
TYPE M SIDEWALK / SHARED-USE RAMP
Not to Scale



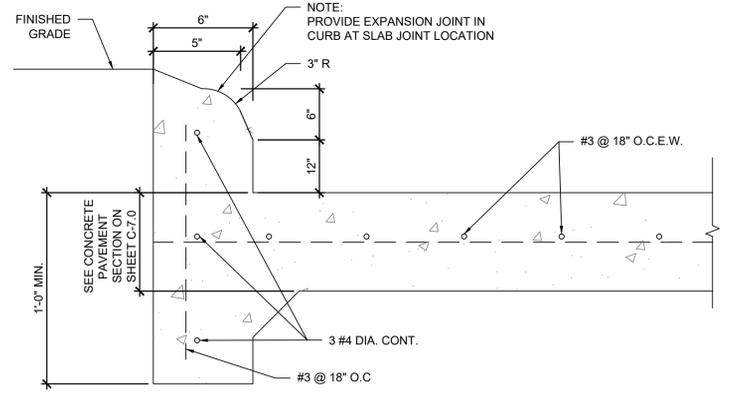
TYPE M SIDEWALK RAMP
Not to Scale

SIDEWALK / SHARED-USE PATH & SIDEWALK / SHARED-USE RAMP NOTES:
1. CURB RAMP OPENING, NOT INCLUDING FLARES, SHALL MATCH EXISTING SIDEWALK WIDTH AND OPENING SHALL BE AT LEAST 48" WIDE.
2. USE 18" LONG #4 EPOXY COATED BARS @ 24" O.C. EMBED THE BARS 9" IN EACH DIRECTION.
3. ALL RAMP, SIDEWALKS, SHARED-USE PATHS SUBGRADE MUST BE OF STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
4. LONGITUDINAL JOINT SPACING TO MATCH WIDTH OF SIDEWALK.
5. ISOLATION JOINTS SHALL BE PLACED WHERE WALK ABUTS DRIVEWAYS AND SIMILAR STRUCTURES, AND 100' CENTERS MAX.
6. ADA MAXIMUM RAMP SLOPE = 8.33%
ADA MAXIMUM CROSS SLOPE = 2.0%
*ROADWAY EXCEPTION: WHERE EXISTING ROAD PROFILE GRADE DOES NOT ALLOW RAMP TO MEET RAMP SLOPE REQUIREMENT OF 8.33% OR LESS, THE RAMP SHALL BE EXTENDED TO A LENGTH OF 15 FEET TO MATCH EXISTING SIDEWALK. CROSS SLOPE OF RAMP SHALL BE 1.5%, 8.05%.
7. TURNING SPACES SHALL BE 1.5%, 8.05%, SLOPE IN ANY DIRECTION. TURNING SPACES SHALL HAVE A MINIMUM 4'4" TURNING AREA. TURNING SPACES WITH A SIDEWALK CURB, SHALL HAVE A 5' TURNING AREA PERPENDICULAR TO THE SIDEWALK CURB.
8. FOR RETROFIT WORK, SLOPES TO BE DETERMINED IN FIELD BY CONTRACTOR AND APPROVED BY CITY INSPECTOR.
9. RAMP EXTENSION AREA SHALL NOT BE USED AS TRANSITION TO EXISTING SIDEWALK. ANY TRANSITIONS REQUIRED TO MATCH RAMP TO EXISTING SIDEWALK SHALL REQUIRE REMOVAL AND REPLACEMENT OF ADDITIONAL SIDEWALK BEYOND THE RAMP AREA. SIDEWALK TRANSITION LENGTH SHALL BE EQUAL TO OR GREATER THAN THE WIDTH OF THE EXISTING SIDEWALK. RAMP EXTENSIONS SHALL BE A CONTINUOUS SLOPE.
10. ALL SIDEWALK AND RAMP CONSTRUCTION SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROVING).

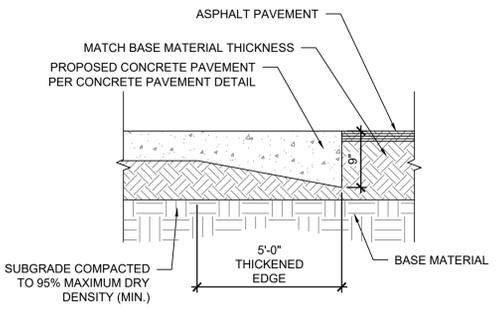
CURB & GUTTER DETAIL AT RAMP
Not to Scale



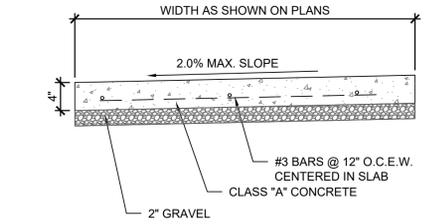
MONOLITHIC CURB DETAIL
C7.1 N.T.S.



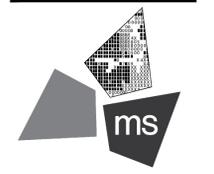
J ADA RAMP RETROFIT DETAIL - CITY OF LS STANDARD DETAIL GEN-3A
C7.1 N.T.S.



K PAVEMENT TRANSITION
C7.1 N.T.S.



L CONCRETE CURB NOSE DETAIL
C7.1 N.T.S.



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PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE DETAILS



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING



N:\03162-40497\21-Lee's Summit, MO Market\Docs\CAD\CIVIL\DWG-set\C-7.1 Site Details.dwg, 3/15/2023 2:39 PM, boleyn, jason

SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
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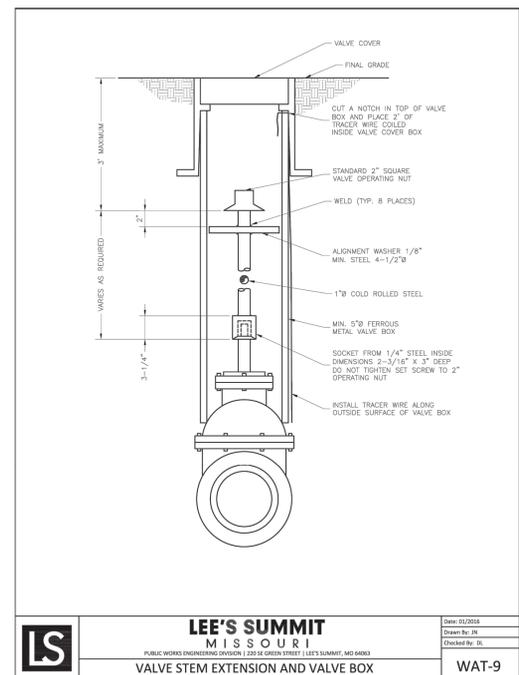
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PROJECT
PROPOSED PT22M BUILDING
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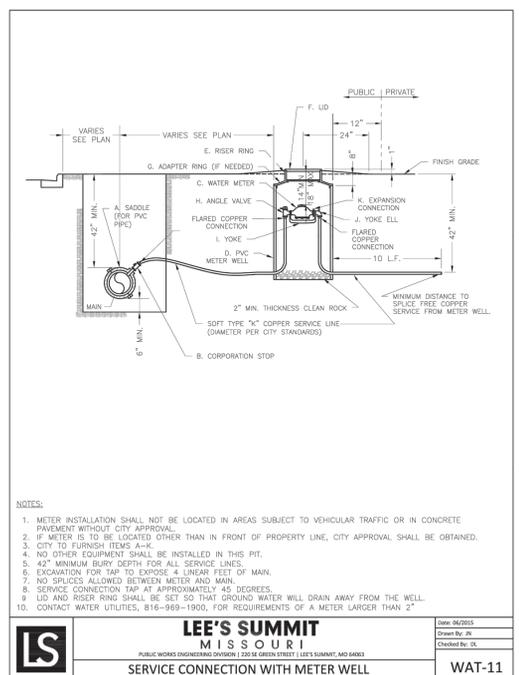
SHEET TITLE
SITE DETAILS



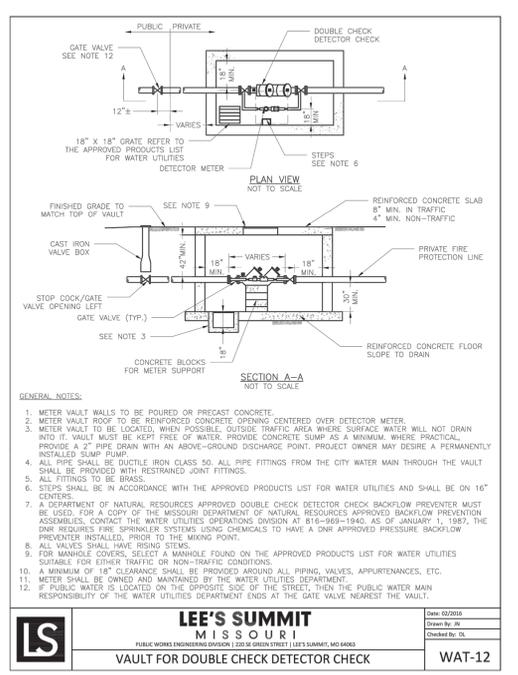
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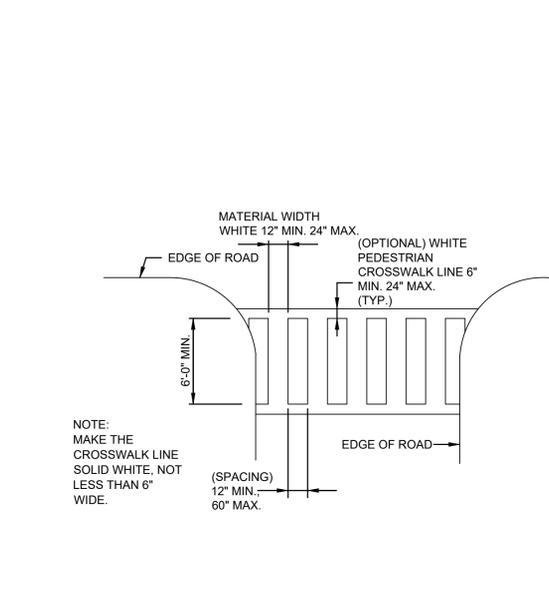
A VALVE STEM EXTENSION AND VALVE BOX
C7.2 N.T.S.



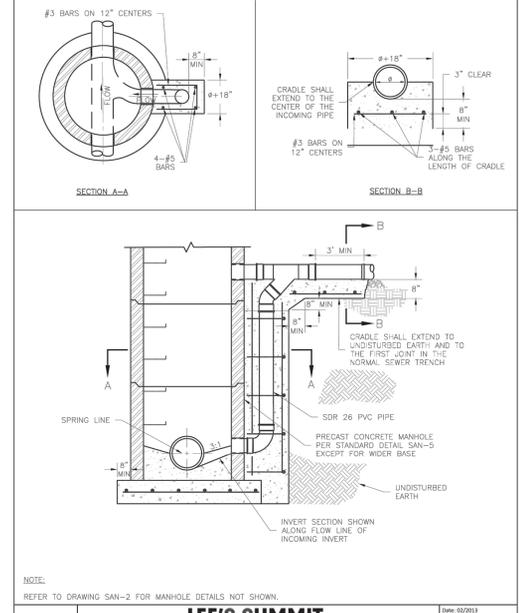
B SERVICE CONNECTION/METER WELL
C7.2 N.T.S.



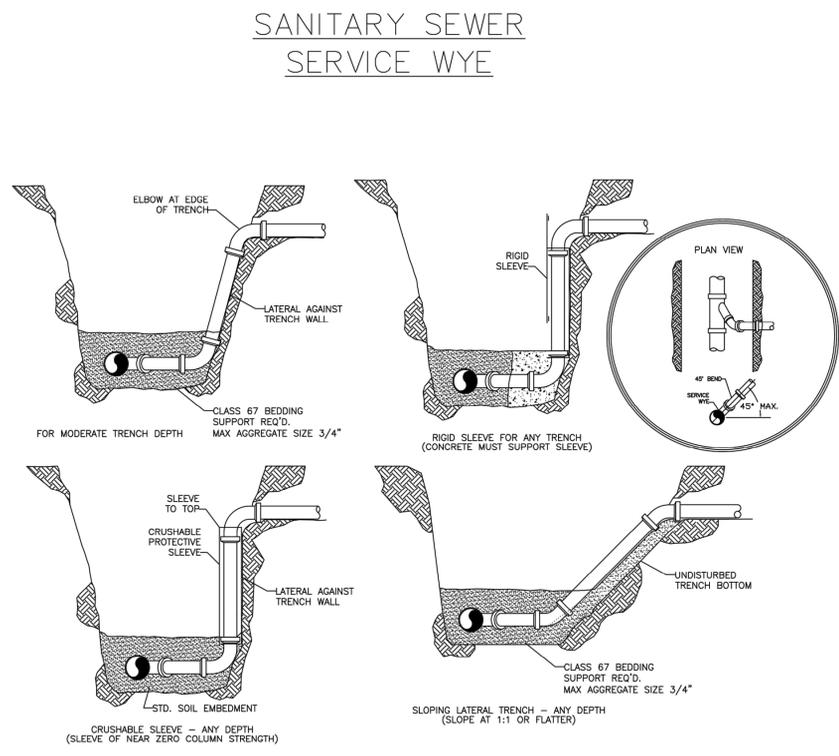
C VAULT FOR DOUBLE CHECK DETECTOR CHECK
C7.2 N.T.S.



D SIDEWALK CROSSING DETAIL
C7.2 N.T.S.



E STANDARD DROP MANHOLE DETAIL
C7.2 N.T.S.



F SANITARY SERVICE WYE DETAIL
C7.2 N.T.S.



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60% SET		01/24/22
PDP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
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PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

SITE DETAILS



DRAWN BY: TDB

CHECKED BY: PGD

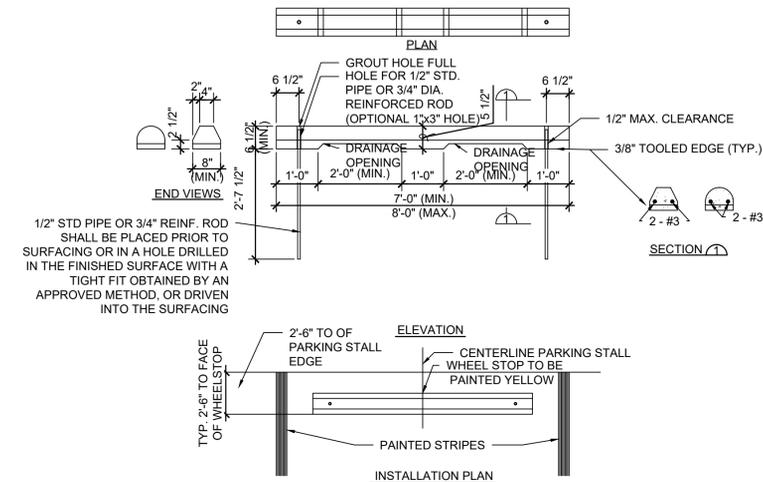
PROJECT NO: 40497-21

DRAWING

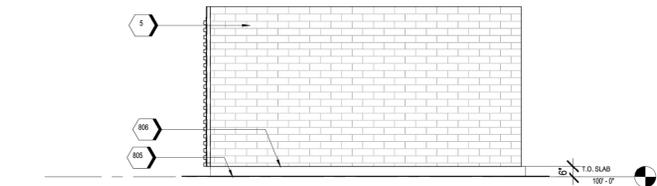
C-7.3

KEYNOTES

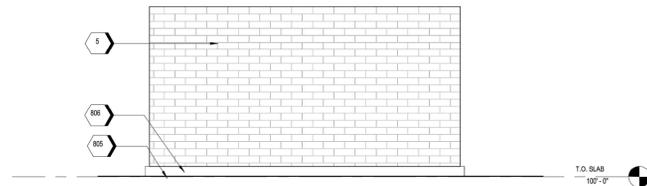
5	PAINTEd CMU FOR WALLS OF ENCLOSURE.
801	PRE-FINISHED CORRUGATED METAL PANEL (MP-01)
802	STRUCTURAL STEEL, PAINT WELDED UNITS PT-5, RE: STRUCTURAL
804	PIPE BOLLARDS: IDEAL SHIELD: URBAN BRONZE, PANTONE 2336 XGC. RE: D3/S5.1
805	FINISHED PAVEMENT, RE: CIVIL
806	CONCRETE CURB, RE: STRUCTURAL
807	WELD ANGLE STOPS TO CORNER POST, PAINT PT-5



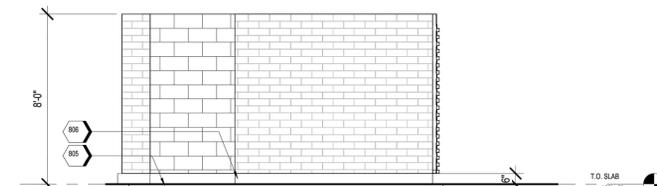
B PRE-CAST CONCRETE WHEEL STOP
C7.3 / N.T.S.



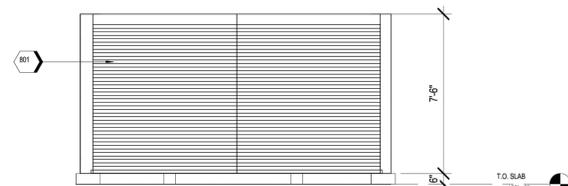
C2 DUMPSTER - ELEVATION @ EAST
SCALE 1/4" = 1'-0"



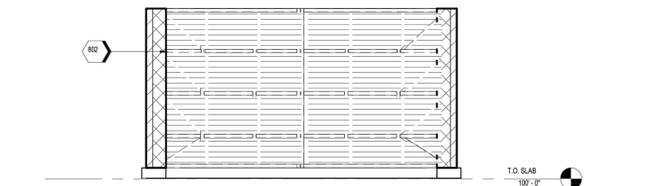
C1 DUMPSTER - ELEVATION @ REAR
SCALE 1/4" = 1'-0"



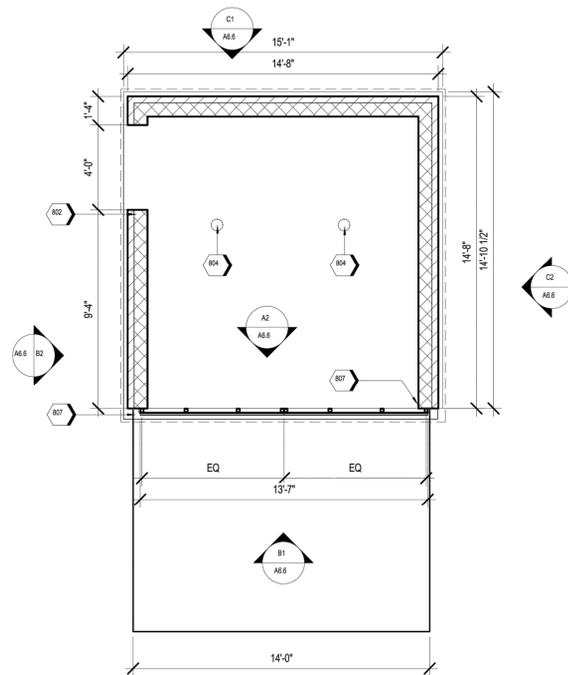
B2 DUMPSTER - ELEVATION @ SIDE ENTRY
SCALE 1/4" = 1'-0"



B1 DUMPSTER - ELEVATION @ GATE
SCALE 1/4" = 1'-0"



A2 DUMPSTER - GATE INSIDE FACE
SCALE 1/4" = 1'-0"



A1 DUMPSTER - ENLARGED
SCALE 1/4" = 1'-0"



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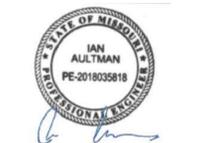
PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

DETENTION BASIN DETAILS



DRAWN BY: TDB

CHECKED BY: PGD

PROJECT NO: 40497-21

DRAWING

C-7.4



ACCEPTABLE FILL MATERIALS: STORMTECH MC-3500 CHAMBER SYSTEMS

MATERIAL LOCATION	DESCRIPTION	ASHTO MATERIAL CLASSIFICATION	COMPACTION / DENSITY REQUIREMENT
D	FINAL FILL MATERIAL FOR LAYER 'C' STARTS FROM THE TOP OF THE SUBGRADE STONE (LAYER 'C' IS 12" ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBGRADE MAY BE PART OF THE LAYER.	ANY SOILS/ROCKS, NATIVE SOILS, OR PER ENGINEER'S PLAN. CHECK PLANS FOR PAVEMENT SUBGRADE REQUIREMENTS.	PREPARE PER SITE DESIGN ENGINEER'S PLANS. PAVED INSTALLATION MAY HAVE STORMTECH MATERIAL AND PREPARATION REQUIREMENTS.
C	FINAL FILL MATERIAL FOR LAYER 'C' STARTS FROM THE TOP OF THE SUBGRADE STONE (LAYER 'C' IS 12" ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBGRADE MAY BE PART OF THE LAYER.	GRANULAR WELL-GRADED SOILS/AGGREGATE MATERIALS - 100% FRIED OR PROCESSED AGGREGATE. OR MOST PAVEMENT SUBGRADE MATERIALS CAN BE USED IN LAYER 'C' OF THIS LAYER.	ASHTO M40 A-1, A-1.5, A-2 OR ASHTO M47 3, 3.5, 4, 4.5, 5, 5.5, 6, 6.5, 7, 7.5, 8, 8.5, 9, 10
B	UNDERBASE STONE: FILL SUBGRADING THE CHAMBERS FROM THE FOUNDATION STONE (LAYER 'B' IS 12" ABOVE THE TOP OF THE CHAMBER).	CLEAN, CRUSHED, ANGULAR STONE	NO COMPACTION REQUIRED.
A	FOUNDATION STONE: FILL SUBGRADING FROM THE SUBGRADE UP TO THE FOOT (BOTTOM) OF THE CHAMBER.	CLEAN, CRUSHED, ANGULAR STONE	ASHTO M47 3, 4 PLATE COMPACT OR ROLL TO ACHIEVE A FLAT SURFACE. ¹⁾

PLEASE NOTE:
1. THE LISTED ASHTO DESIGNATIONS ARE FOR GUIDANCE ONLY. THE STONE MUST ALSO BE CLEAN, CRUSHED, ANGULAR. FOR EXAMPLE, A SPECIFICATION FOR #4 STONE WOULD STATE "CLEAN, CRUSHED, ANGULAR NO. 4 (ASHTO M47) STONE."
2. STORMTECH CONNECTION REQUIREMENTS ARE MET FOR ALL CONNECTION MATERIALS WHEN PLACED AND COMPACTED BY (200 WH) MAXI LIFTS USING TWO FULL COMPACTORS WITH A VIBRATORY COMPACTOR.
3. THERE ARE NO FRICTION SURFACES TO BE COMPACTED BY CONSTRUCTION. FOR STORMTECH DESIGN LOAD CONDITIONS, AT 4" SURFACE MAY BE ACHIEVED BY MARKS OF GRADING WITHOUT CONSTRUCTION EQUIPMENT. FOR SPECIAL LOAD DESIGN, CONTACT STORMTECH FOR CONNECTION REQUIREMENTS.
4. ONCE LAYER 'C' IS PLACED, ANY SOIL/MATERIAL CAN BE PLACED IN LAYER 'D' UP TO THE FINISHED GRADE. MOST PAVEMENT SUBGRADE SOILS CAN BE USED TO REPLACE THE MATERIAL REQUIREMENTS OF LAYER 'C' OR 'D' AT THE SITE DESIGN ENGINEER'S DISCRETION.

NOTES:
1. CHAMBERS SHALL MEET THE REQUIREMENTS OF ASTM F2344, "STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORRUGATED WALL STORMWATER COLLECTION CHAMBERS" CHAMBER CLASSIFICATION 45/10 DESIGNATION IS.
2. MC-3500 CHAMBERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM F2378 "STANDARD PRACTICE OF THERMOPLASTIC COLLECTOR WALL STORMWATER COLLECTION CHAMBERS".
3. THE SITE DESIGN ENGINEER IS RESPONSIBLE FOR ADDRESSING THE BEARING CAPACITY (ALLOWABLE BEARING CAPACITY) OF THE SUBGRADE SOILS AND THE DEPTH OF FOUNDATION STONE WITH CONSIDERATION FOR THE RANGE OF EXPECTED SOIL MOISTURE CONDITIONS.
4. PERMITTER STONE MUST BE EXTENDED HORIZONTALLY TO THE EXCAVATION WALL FOR BOTH VERTICAL AND SLOPED EXCAVATION WALLS.
5. REQUIREMENTS FOR HANDLING AND RETENTION:
• TO MAINTAIN THE WIDTH OF CHAMBERS DURING SHIPPING AND HANDLING, CHAMBERS SHALL HAVE INTERNAL, INTERLOCKING STAKING LUGS.
• TO ENSURE A SECURE, CONTAINMENT INTO LATERATION, THE HEIGHT OF THE CHAMBER JOINT SHALL NOT BE LESS THAN 1".
• TO ENSURE THE INTEGRITY OF THE ARCH SHAPE DURING INSTALLATION, AT THE ARCH STIFFNESS CONSTANT SHALL BE GREATER THAN OR EQUAL TO 405 LB/FT². THE ARCH IS DEFINED IN SECTION 6.2 OF ASTM F2344 AND IS TO BE MAINTAINED DURING INSTALLATION AT ELEVATED TEMPERATURES ABOVE 77°F (23°C). CHAMBERS SHALL BE PRODUCED FROM REFLECTIVE GOLD OR YELLOW COLORS.

PROPOSED LAYOUT

PROPOSED ELEVATIONS

NO.	DESCRIPTION	MINIMUM FINISH ELEVATION (FOOTING)	MAXIMUM FINISH ELEVATION (FOOTING)	PART TYPE	ITEM NO.	INVERT	MAX. FLOW
1	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	A	2.00'	1.77'
2	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	B	2.00'	1.77'
3	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	C	2.00'	1.77'
4	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	D	2.00'	1.77'
5	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	E	2.00'	1.77'
6	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	F	2.00'	1.77'
7	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	G	2.00'	1.77'
8	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	H	2.00'	1.77'
9	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	I	2.00'	1.77'
10	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	J	2.00'	1.77'
11	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	K	2.00'	1.77'
12	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	L	2.00'	1.77'
13	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	M	2.00'	1.77'
14	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	N	2.00'	1.77'
15	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	O	2.00'	1.77'
16	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	P	2.00'	1.77'
17	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	Q	2.00'	1.77'
18	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	R	2.00'	1.77'
19	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	S	2.00'	1.77'
20	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	T	2.00'	1.77'
21	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	U	2.00'	1.77'
22	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	V	2.00'	1.77'
23	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	W	2.00'	1.77'
24	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	X	2.00'	1.77'
25	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	Y	2.00'	1.77'
26	STORMTECH MC-3500 CHAMBER	111.20'	111.20'	MANUFACTURED END CAP	Z	2.00'	1.77'

IMPORTANT - NOTES FOR THE BIDDING AND INSTALLATION OF MC-3500 CHAMBER SYSTEM

- STORMTECH MC-3500 CHAMBERS SHALL NOT BE INSTALLED UNTIL THE MANUFACTURER'S REPRESENTATIVE HAS COMPLETED A PRE-CONSTRUCTION MEETING WITH THE INSTALLERS.
- STORMTECH MC-3500 CHAMBERS SHALL BE INSTALLED IN ACCORDANCE WITH THE "STORMTECH MC-3500AC-4500 CONSTRUCTION GUIDE".
- CHAMBERS ARE NOT TO BE BACKFILLED WITH A DOZER OR AN EXCAVATOR SITUATED OVER THE CHAMBERS. STORMTECH RECOMMENDS BACKFILL METHOD:
 - STORMTECH LOCATED OFF THE CHAMBER BED.
 - BACKFILL FROM OUTSIDE THE EXCAVATION USING A LONG BOOM DOZER OR EXCAVATOR.
- THE FOUNDATION STONE SHALL BE LEVELLED AND COMPACTED PRIOR TO PLACING CHAMBERS.
- JOINTS BETWEEN CHAMBERS SHALL BE PROPERLY SEATED PRIOR TO PLACING STONE.
- MAINTAIN MINIMUM - 4" (100 mm) SPACING BETWEEN THE CHAMBER ROWS.
- INLET AND OUTLET MANHOLES MUST BE INSERTED A MINIMUM OF 12" (300 mm) INTO CHAMBER END CAPS.
- THE CONTRACTOR MUST REPORT ANY DISCREPANCIES WITH CHAMBER FOUNDATION MATERIALS BEARING CAPACITIES TO THE SITE DESIGN ENGINEER.
- STONE MUST BE PLACED ON THE TOP CENTER OF THE CHAMBER TO ANCHOR THE CHAMBERS IN PLACE AND PRESERVE ROW SPACING.
- THE CONTRACTOR MUST REPORT ANY DISCREPANCIES WITH CHAMBER FOUNDATION MATERIALS BEARING CAPACITIES TO THE SITE DESIGN ENGINEER.
- ADS RECOMMENDS THE USE OF "TELEVISION CATCH" METHODS DURING CONSTRUCTION FOR ALL INLETS TO PROTECT THE SUBSURFACE STORMWATER MANAGEMENT SYSTEM FROM CONSTRUCTION SITE RUNOFF.

NOTES FOR CONSTRUCTION EQUIPMENT

- STORMTECH MC-3500 CHAMBERS SHALL BE INSTALLED IN ACCORDANCE WITH THE "STORMTECH MC-3500AC-4500 CONSTRUCTION GUIDE".
- THE USE OF EQUIPMENT OVER MC-3500 CHAMBERS IS LIMITED:
 - NO EQUIPMENT IS ALLOWED ON BASE CHAMBERS.
 - NO EQUIPMENT IS ALLOWED ON CHAMBERS IF THE EQUALIZER IS REQUESTED PROFESSIONAL ENGINEER.
 - THE STRUCTURAL EVALUATION SHALL DOCUMENT THAT THE SAFETY FACTORS ARE GREATER THAN OR EQUAL TO 1.56 FOR EACH LOAD CASE. THE MINIMUM REQUIRED BY ASTM F2378 AND BY SECTIONS 3 AND 12 OF THE ASHTO LTPD BRIDGE DESIGN SPECIFICATION FOR TRUCKS AND TRAILERS.
 - THE TEST DEVICE DEPTH WOULD BE AS SPECIFIED IN ASTM F2378 SHALL BE USED FOR PERMANENT DEAD LOAD DESIGN EXCEPT THAT IT SHALL BE THE TO-TANK HEIGHTS USED FOR DESIGN.
- FULL 18" (450 mm) OF ENRICHED COVER MATERIALS OVER THE CHAMBERS IS REQUIRED FOR CLAMP TRUCK TRAVEL OR DUMPING.
- USE OF A DOZER TO PILE ENRICHMENT STONE BETWEEN THE ROWS OF CHAMBERS MAY CAUSE DAMAGE TO CHAMBERS AND IS NOT AN ACCEPTABLE BACKFILL METHOD. ANY CHAMBERS DAMAGED BY USING THE "DOZER AND PILE" METHOD ARE NOT COVERED UNDER THE STORMTECH STANDARD WARRANTY.

NOTES:
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100. THE MINIMUM SIZE TO BE DETERMINED BY THE SITE DESIGN ENGINEER. SEE NOTE 12 FOR MANHOLE SETTING GUIDANCE.

MC-3500 TECHNICAL SPECIFICATION

MC-SERIES END CAP INSERTION DETAIL

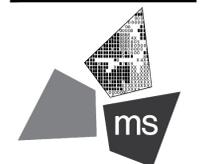
PART #	STUB	INVERT	MAX. FLOW
MC3500EP001	4" (100 mm)	32.27' (984 mm)	0.88' (27 mm)
MC3500EP002	6" (150 mm)	31.10' (953 mm)	0.81' (25 mm)
MC3500EP003	8" (200 mm)	29.94' (913 mm)	0.74' (23 mm)
MC3500EP004	10" (250 mm)	28.77' (874 mm)	0.67' (21 mm)
MC3500EP005	12" (300 mm)	27.60' (835 mm)	0.60' (19 mm)
MC3500EP006	14" (350 mm)	26.44' (795 mm)	0.53' (17 mm)
MC3500EP007	16" (400 mm)	25.27' (756 mm)	0.46' (15 mm)
MC3500EP008	18" (450 mm)	24.10' (716 mm)	0.39' (13 mm)
MC3500EP009	20" (500 mm)	22.94' (676 mm)	0.32' (10 mm)
MC3500EP010	22" (550 mm)	21.77' (637 mm)	0.25' (8 mm)
MC3500EP011	24" (600 mm)	20.60' (597 mm)	0.18' (6 mm)
MC3500EP012	26" (650 mm)	19.44' (557 mm)	0.11' (4 mm)
MC3500EP013	28" (700 mm)	18.27' (518 mm)	0.04' (1 mm)

NOTES:
1. ALL ENGINEERING ARE DIMENSIONS.
2. THE MINIMUM SIZE TO BE DETERMINED BY THE SITE DESIGN ENGINEER. SEE NOTE 12 FOR MANHOLE SETTING GUIDANCE.
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31. THE MINIMUM SIZE TO BE DETERMINED BY THE SITE DESIGN ENGINEER. SEE NOTE 12 FOR MANHOLE SETTING GUIDANCE.
32. THE MINIMUM SIZE TO BE DETERMINED BY THE SITE DESIGN ENGINEER. SEE NOTE 12 FOR MANHOLE SETTING GUIDANCE.
33. THE MINIMUM SIZE TO BE DETER

Revision Date: 05/08/2023
Lee's Summit, Missouri

SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
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RESPONSE TO FDP COMMENTS IV		11/03/22
PT22M PROGRESS SET		03/08/23
ISSUE FOR PERMIT		03/15/23

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PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

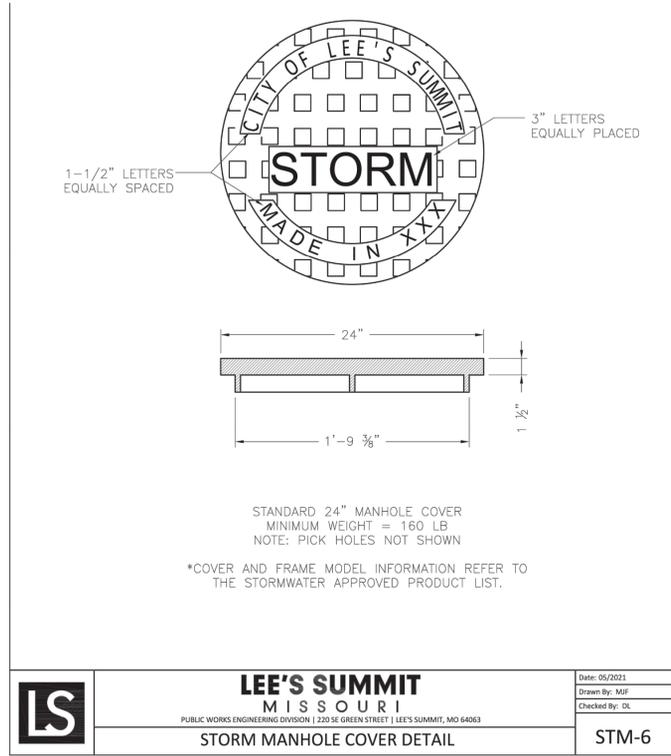
SHEET TITLE
SITE DETAILS



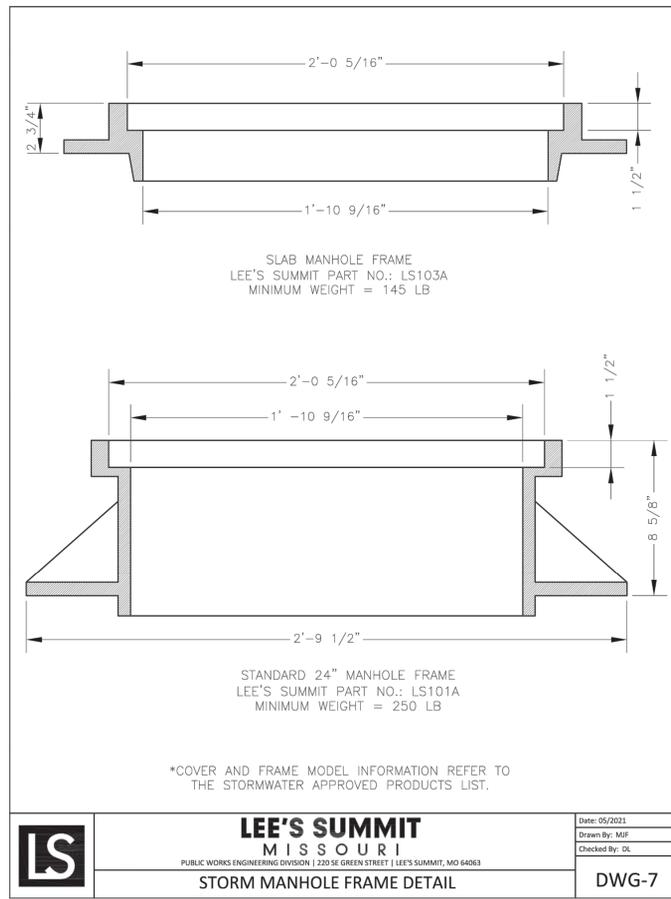
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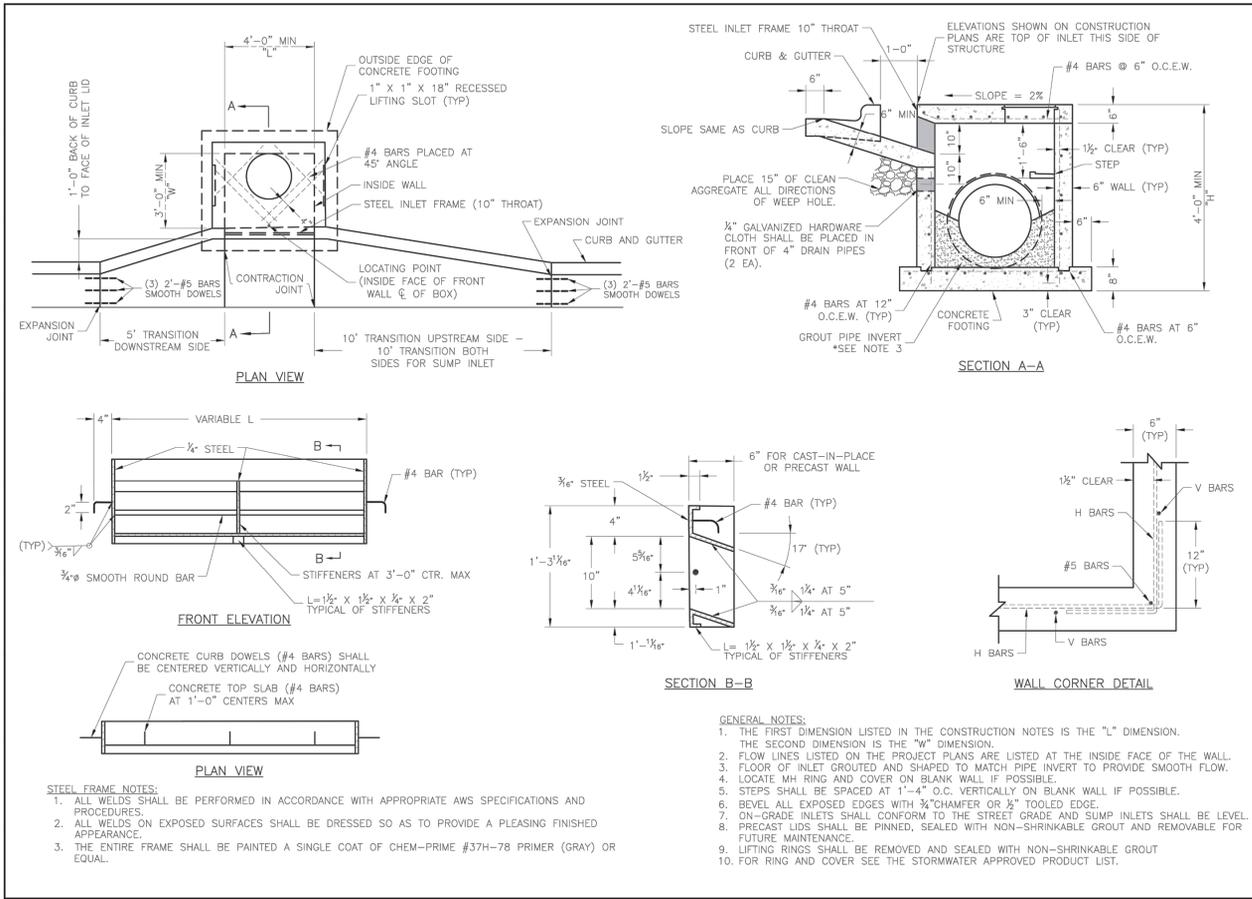
C-7.5



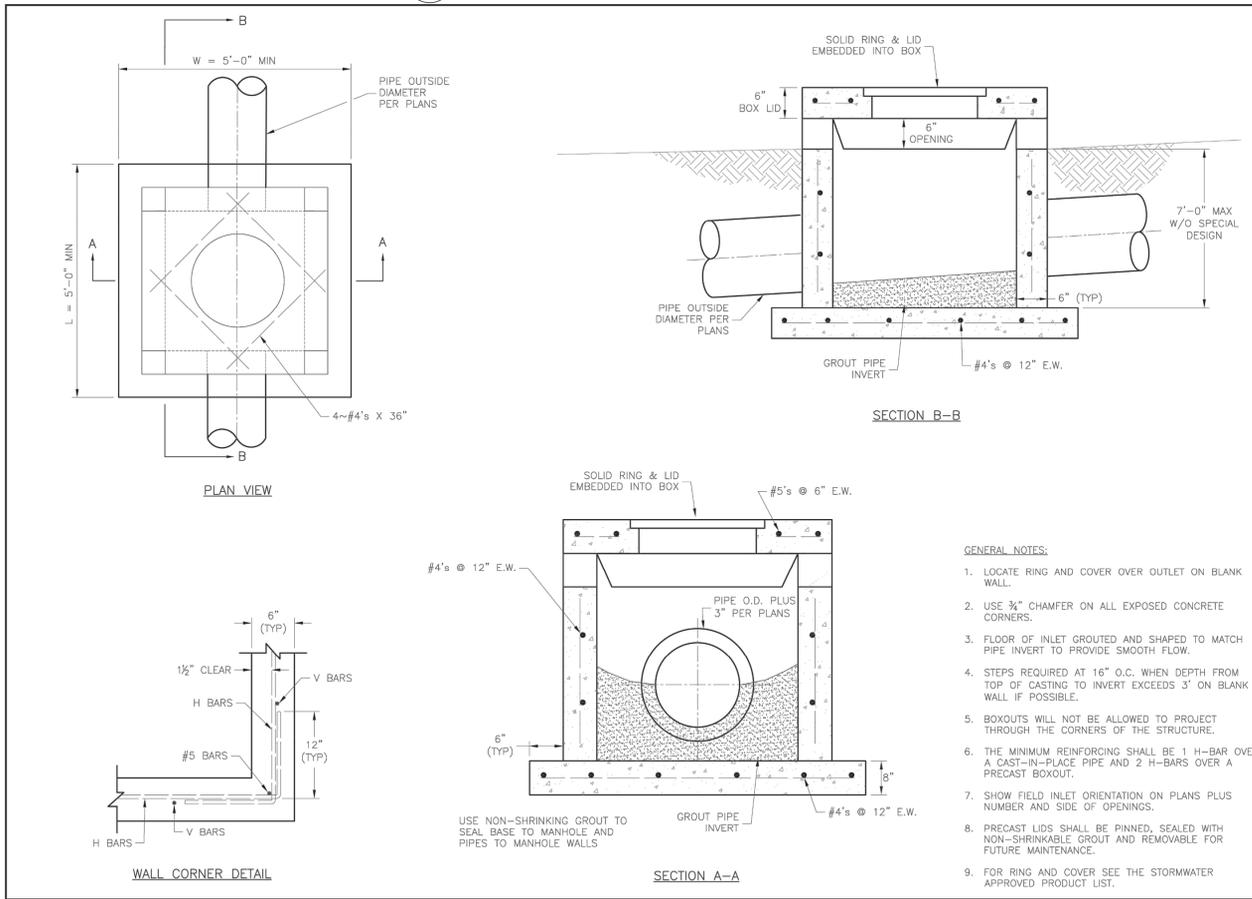
C STORM MANHOLE COVER DETAIL
C7.5 N.T.S.



D STORM MANHOLE FRAME DETAIL
C7.5 N.T.S.



A CURB INLET DETAIL
C7.5 N.T.S.



B FIELD INLET DETAIL
C7.5 N.T.S.

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REVISION DATE OF SUBMIT
Lee's Summit, Missouri

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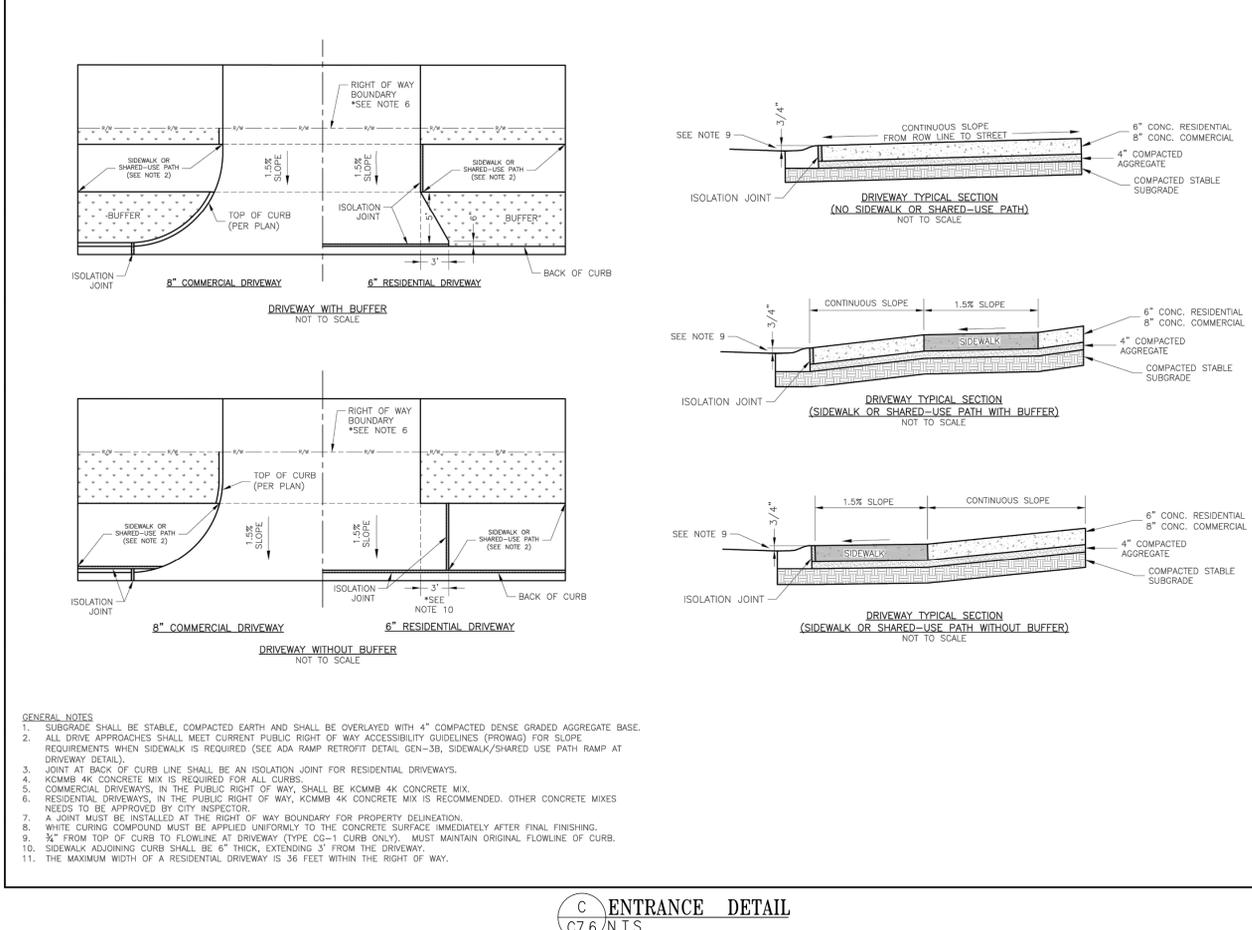
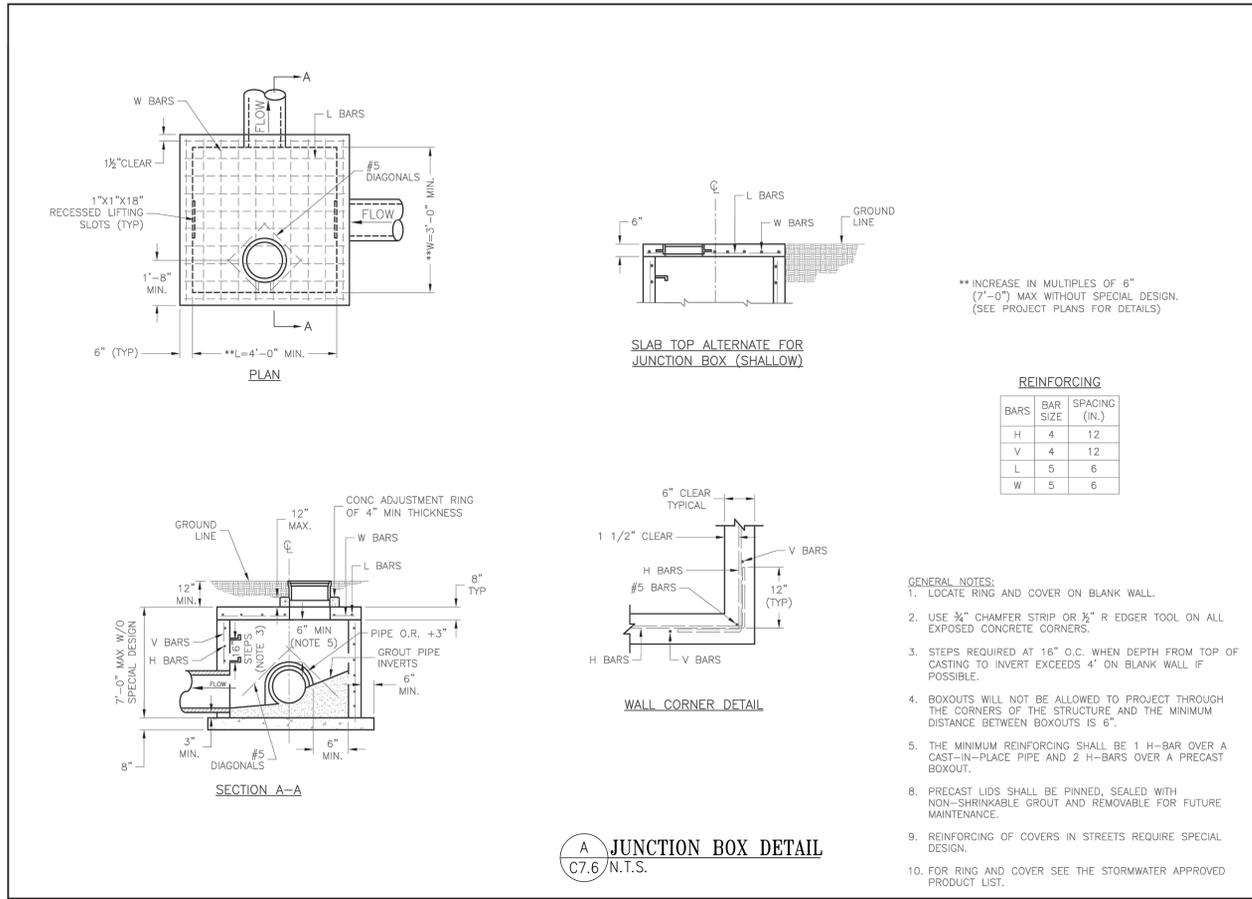
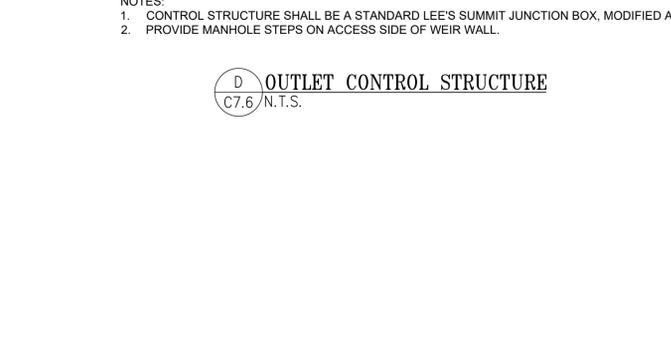
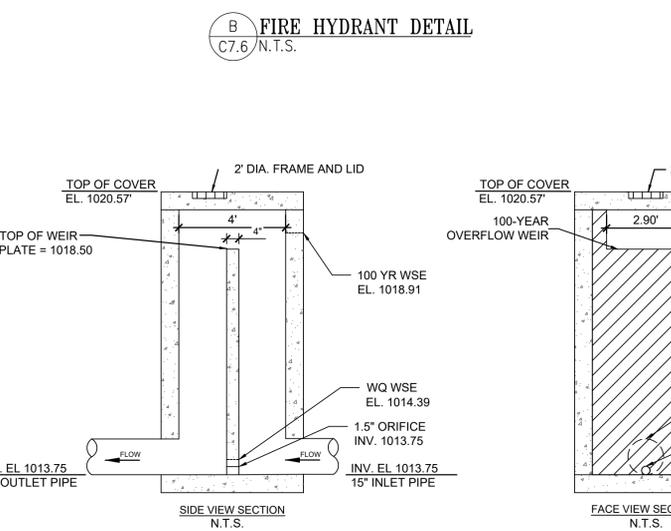
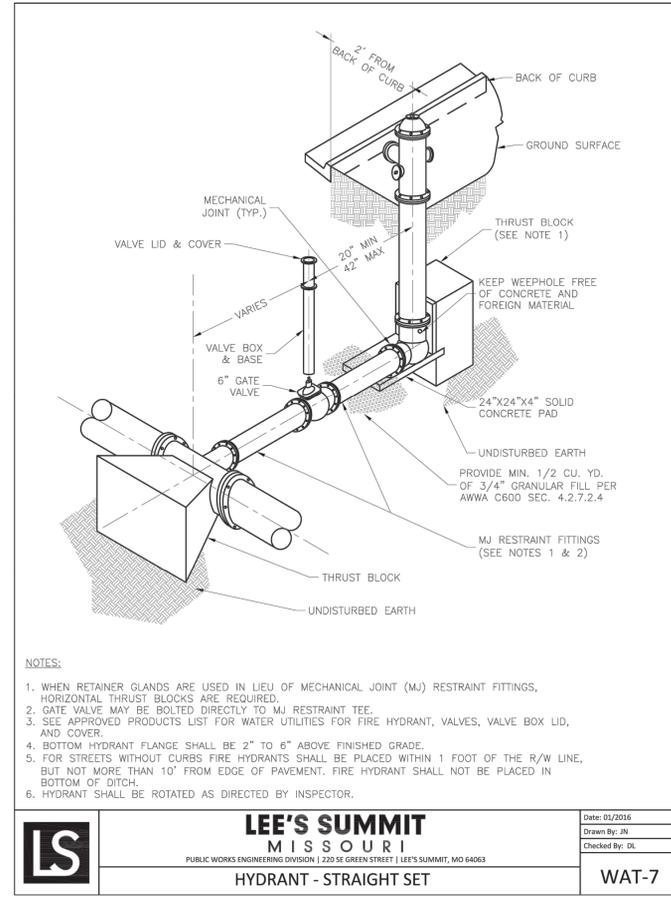
PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SITE DETAILS



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

C-7.6



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Revision Date: 05/08/2023
Project: Lee's Summit, Missouri

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KEYED NOTES:

- ① CONSTRUCTION ENTRANCE. SEE DETAIL ON SHEET C-10.2.
- ② TEMPORARY SILT FENCE. SEE DETAIL ON SHEET C-10.2.

CONSTRUCTION SEQUENCE
THE ORDER OF MAJOR ACTIVITIES WILL BE AS FOLLOWS:

1. PRE-CONSTRUCTION MEETING
2. BEFORE ANY SITE GRADING ACTIVITIES BEGIN
 - a. INSTALL PERIMETER SILT FENCES
 - b. INSTALL INLET PROTECTING ON EXISTING INLETS
 - c. CONSTRUCT TEMPORARY CONSTRUCTION ENTRANCES

*ALL CONSTRUCTION SHALL FOLLOW THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL AS ADOPTED BY ORDINANCE 5813. WHERE DISCREPANCIES EXIST BETWEEN THESE PLANS AND THE DESIGN AND CONSTRUCTION MANUAL, THE DESIGN AND CONSTRUCTION MANUAL SHALL PREVAIL.

*THE CONTRACTOR SHALL CONTACT THE CITY'S DEVELOPMENT SERVICES ENGINEERING INSPECTION TO SCHEDULE A PRE-CONSTRUCTION MEETING WITH A FIELD ENGINEERING INSPECTOR PRIOR TO ANY LAND DISTURBANCE WORK AT (816) 969-1200.

NOTICE

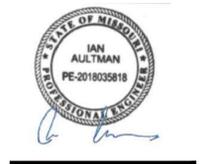
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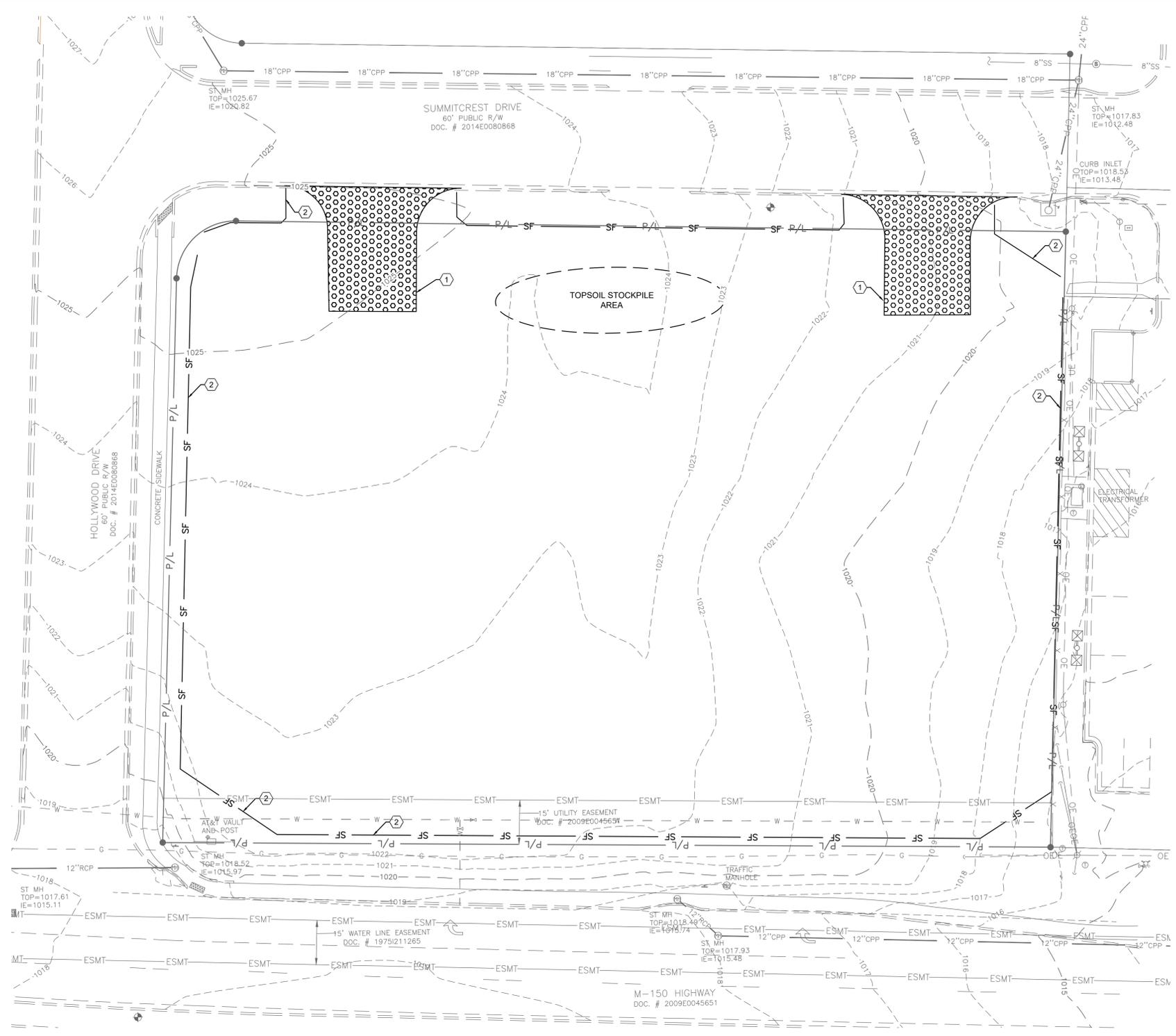
PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
STORMWATER POLLUTION PREVENTION PLAN PRE CONSTRUCTION



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING

C-9.0



LEGEND

EXISTING	PROPOSED	DESCRIPTION
950	1015	CONTOUR
	SF	SILT FENCE
	[Symbol]	INLET PROTECTION
	[Symbol]	CONCRETE WASHOUT
	[Symbol]	CONSTRUCTION ENTRANCE

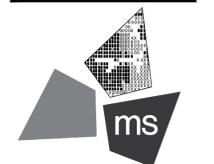


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Development Date: 05/18/2023
Lee's Summit, Missouri

SIR UPD	05/18/2023	09/13/21
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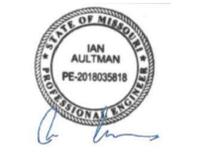
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PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
STORMWATER POLLUTION PREVENTION PLAN DURING AND POST CONSTRUCTION



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21

DRAWING
C-9.1

KEYED NOTES:

- ① CONCRETE WASHOUT, SEE DETAIL ON SHEET C-10.2.
- ② INLET PROTECTION. SEE DETAILS ON SHEET C-10.3.
- ③ TEMPORARY SILT FENCE. SEE DETAIL ON SHEET C-10.2.
- ④ CONSTRUCTION ENTRANCE. SEE DETAIL ON SHEET C-10.2.

CONSTRUCTION SEQUENCE

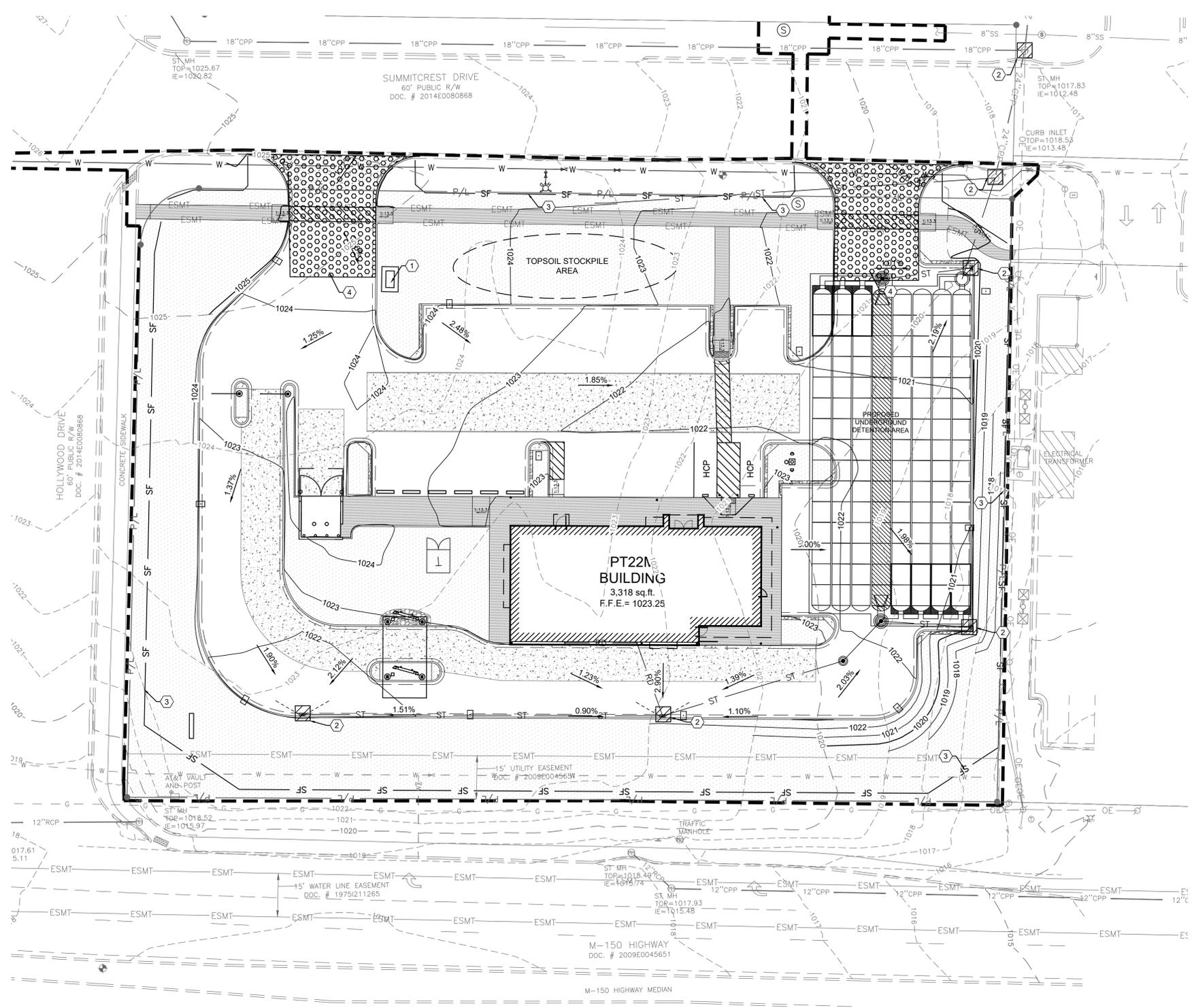
- THE ORDER OF MAJOR ACTIVITIES WILL BE AS FOLLOWS:
- BEGIN SITE GRADING AND TOPSOIL STRIPPING
 - ESTABLISH TOPSOIL STOCK PILE WITHIN SILT FENCE PERIMETER
 - STABILIZE DENUDED AREAS AND STOCKPILES WITHIN 14 DAYS OF LAST CONSTRUCTION ACTIVITY IN THAT AREA
 - INSTALL EROSION CONTROL MATTING AT LOCATIONS INDICATED ON PLAN
 - INSTALL UTILITIES, SANITARY SEWERS, WATER SERVICES, AND STORM SEWERS
 - BEGIN CONSTRUCTION OF BUILDING FOUNDATION AND STRUCTURE
 - INSTALL CURBS, PREPARE PAVEMENT SUBGRADE AND PROVIDE GOOD AGGREGATE BASE TO AREAS TO BE PAVED
 - PAVE AREAS AND EXTERIOR BUILDING CONSTRUCTION
 - FINAL GRADING AND PERMANENT SEEDING OF NON-PAVED AREAS OF THE SITE WITHIN 7 DAYS OF FINISHING FINAL GRADE
 - ONCE 70% VEGETATIVE COVERAGE IS ACHIEVED, REMOVE EROSION PROTECTION

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LEGEND

EXISTING	PROPOSED	DESCRIPTION
950	— 1015 —	CONTOUR
	— SF —	SILT FENCE
	▧	INLET PROTECTION
	□	CONCRETE WASHOUT
	⊞	CONSTRUCTION ENTRANCE
	▨	PERMANENT STABILIZATION AREA



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PROJECT NAME AND LOCATION
WHATABURGER
NWQ HWY 150 & HOLLYWOOD ST
LEE'S SUMMIT, MO 64802

OWNER NAME AND ADDRESS
WHATABURGER
300 CONCORD PLAZA DR.
SAN ANTONIO, TX 78216
PHONE: (210) 476-6842
CONTACT: CINDY ESPINOZA
EMAIL: cespinoza@wbhq.com

SITE CONTACT
WHATABURGER
300 CONCORD PLAZA DR.
SAN ANTONIO, TX 78216
PHONE: (210) 476-6842
CONTACT: CINDY ESPINOZA
EMAIL: cespinoza@wbhq.com

GENERAL SCOPE OF PROJECT
THIS PROJECT WILL CONSIST OF A RESTAURANT AND THE CONSTRUCTION OF ASSOCIATED DRAINAGE FACILITIES AND OTHER MISCELLANEOUS SITE WORK.

NATURE OF CONSTRUCTION ACTIVITY (CHECK ALL THAT APPLY)
SUBDIVISION _____
COMMERCIAL X
INDUSTRIAL _____
P.U.D. _____
OTHER _____

SOIL TYPES

CONSTRUCTION SITE ESTIMATES

TOTAL SITE AREA:	1.45 AC.
CONSTRUCTION SITE AREA TO BE DISTURBED:	1.53 AC.
PERCENTAGE IMPERVIOUS AREA BEFORE CONSTRUCTION:	0.5%
RUNOFF COEFFICIENT BEFORE CONSTRUCTION:	
PERCENTAGE IMPERVIOUS AREA AFTER CONSTRUCTION:	61.8%
RUNOFF COEFFICIENT AFTER CONSTRUCTION:	

RECEIVING WATERS
LAKE WINNEBAGO

CONSTRUCTION SEQUENCE
THE ORDER OF MAJOR ACTIVITIES WILL BE AS FOLLOWS:

- PRE-CONSTRUCTION MEETING
- BEFORE AND SITE GRADING ACTIVITIES BEGIN
 - INSTALL PERIMETER SILT FENCES
 - INSTALL INLET PROTECTION ON EXISTING INLETS
 - CONSTRUCT TEMPORARY CONSTRUCTION ENTRANCE
- BEGIN SITE GRADING AND TOPSOIL STRIPPING
 - ESTABLISH TOPSOIL STOCKPILE WITHIN SILT FENCE PERIMETER
 - STABILIZE DENUDE AREAS AND STOCKPILES WITHIN 14 DAYS OF LAST CONSTRUCTION ACTIVITY IN THAT AREA
 - INSTALL EROSION CONTROL MATTING AT LOCATIONS INDICATED ON PLAN
- INSTALL UTILITIES, SANITARY SEWERS, WATER SERVICES AND STORM SEWERS
- BEGIN CONSTRUCTION OF BUILDING FOUNDATION AND STRUCTURE
- INSTALL CURBS, PREPARE PAVEMENT SUBGRADE AND PROVIDE GOOD AGGREGATE BASE TO AREAS TO BE PAVED.
- PAVE AREAS AND EXTERIOR BUILDING CONSTRUCTION.
- FINAL GRADING AND PERMANENT SEEDING OF THE NON-PAVED AREAS OF THE SITE WITHIN 7 DAYS OF FINISHING FINAL GRADE
- ONCE 70% VEGETATIVE COVERAGE IS ACHIEVED, REMOVE EROSION PROTECTION.

POTENTIAL SOURCES OF POLLUTION

CONCRETE
DETERGENTS
WOOD
FERTILIZERS
PAINTS (ENAMEL AND LATEX)
CLEANING SOLVENTS
PETROLEUM BASED PRODUCTS

EROSION AND SEDIMENT CONTROLS

BMP DESCRIPTION: CLEARING AND GRUBBING
MAINTENANCE AND INSPECTION: AS NEEDED
REFERENCE: TECHNICAL SPECIFICATION

BMP DESCRIPTION: DUST CONTROL
MAINTENANCE AND INSPECTION: AS NEEDED
REFERENCE: E&S DETAILS

BMP DESCRIPTION: TEMPORARY SEEDING AND MULCHING
MAINTENANCE AND INSPECTION: WEEKLY AND AFTER HEAVY RAIN
REFERENCE: E&S DETAILS

BMP DESCRIPTION: PERMANENT SEEDING AND MULCHING
MAINTENANCE AND INSPECTION: WEEKLY AND AFTER HEAVY RAIN
REFERENCE: E&S DETAILS

BMP DESCRIPTION: CONSTRUCTION ENTRANCE
MAINTENANCE AND INSPECTION: AS NEEDED
REFERENCE: E&S DETAILS

BMP DESCRIPTION: ADS - ISOLATOR ROW
MAINTENANCE AND INSPECTION: AS NEEDED
REFERENCE: O&M MANUAL

BMP DESCRIPTION: TOPSOIL STOCKPILE
MAINTENANCE AND INSPECTION: AS NEEDED
REFERENCE: O&M MANUAL

POST CONSTRUCTION BMP'S

- GREEN SPACE

OTHER SEDIMENT AND EROSION CONTROL NOTES

- TEMPORARY EROSION CONTROLS WILL BE APPLIED PRIOR TO ONSET OF WINTER WEATHER FOR DISTURBED AREAS THAT WILL BE LEFT IDLE OVER WINTER.
- PERMANENT EROSION CONTROLS WILL BE APPLIED WITHIN 7 DAYS FOR DISTURBED AREAS REMAINING DORMANT FOR OVER 1 YEAR OR AT FINAL GRADE.
- SEDIMENT CONTROL DEVICES WILL BE IMPLEMENTED FOR ALL AREAS REMAINING DISTURBED OVER 7 DAYS.

ADDITIONAL BMP'S

OPEN BURNING: NO MATERIALS MAY BE BURNED WHICH CONTAIN RUBBER, GREASE, ASPHALT, OR PETROLEUM PRODUCTS SUCH AS TIRES, CARS, AUTO PARTS, PLASTICS OR PLASTIC COATED WIRE. OPEN BURNING IS NOT ALLOWED IN RESTRICTED AREAS. RESTRICTED AREAS ARE DEFINED AS:

- WITHIN CORPORATION LIMITS
- WITHIN 1,000 FEET OF A MUNICIPAL CORPORATION
- WITHIN A ONE MILE ZONE OUTSIDE OF A CORPORATION OF 10,000 OR MORE

OUTSIDE THE RESTRICTED AREA, NO OPEN BURNING CAN TAKE PLACE WITHIN 1,000 FEET OF AN INHABITED BUILDING LOCATED OFF THE PROPERTY WHERE THE FIRE IS SET. OPEN BURNING IS PERMISSIBLE IN A RESTRICTED AREA FOR THE FOLLOWING ACTIVITIES: HEATING TAR, WELDING AND ACETYLENE TORCHES, SMUDGE POTS AND SIMILAR OCCUPATIONAL NEEDS, AND HEATING OR WARMTH FOR OUTDOOR BARBEQUES. OUTSIDE OF RESTRICTED AREAS, OPEN BURNING IS PERMISSIBLE FOR LANDSCAPE WASTES (PLANT MATERIAL), LAND-CLEARING WASTES (PLANT MATERIAL, WITH PRIOR WRITTEN PERMISSION FROM EPA), AND AGRICULTURAL WASTES (MATERIAL GENERATED BY CROP, HORTICULTURAL, OR LIVESTOCK PRODUCTION PRACTICES).

DUST CONTROL/SUPPRESSANTS: DUST CONTROL IS REQUIRED TO PREVENT NUISANCE CONDITIONS. DUST CONTROLS MUST BE USED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATION AND NOT BE APPLIED IN A MANNER, WHICH WOULD RESULT IN A DISCHARGE TO WATERS OF THE STATE. ISOLATION DISTANCES FROM BRIDGES, CATCH BASINS, AND OTHER DRAINAGE WAYS MUST BE OBSERVED. APPLICATION (EXCLUDING WATER) MAY NOT OCCUR WHEN PRECIPITATION IS IMMINENT AS NOTED IN THE SHORT TERM FORECAST. USED OIL MAY NOT BE APPLIED FOR DUST CONTROL.

AIR PERMITTING REQUIREMENTS: ALL CONTRACTORS AND SUB CONTRACTORS MUST BE MADE AWARE THAT CERTAIN ACTIVITIES ASSOCIATED WITH CONSTRUCTION WILL REQUIRE AIR PERMITS. ACTIVITIES INCLUDING BUT NOT LIMITED TO MOBILE CONCRETE BATCH PLANTS, MOBILE ASPHALT PLANTS, CONCRETE CRUSHERS, LARGE GENERATORS, ETC., WILL REQUIRE SPECIFIC MISSOURI EPA AIR PERMITS FOR INSTALLATION AND OPERATION. THESE ACTIVITIES MUST SEE AUTHORIZATION FROM THE CORRESPONDING OF MISSOURI EPA. NOTIFICATION FOR RESTORATION AND DEMOLITION MUST BE SUBMITTED TO MISSOURI EPA FOR ALL COMMERCIAL SITES TO DETERMINE IF ASBESTOS CORRECTIVE ACTIONS ARE REQUIRED.

WASTE DISPOSAL: THE CONTRACTOR SHALL PROVIDE LITTER CONTROL AND COLLECTION OF MATERIALS WITHIN THE PROJECT BOUNDARIES DURING CONSTRUCTION. ALL FERTILIZER, HYDROCARBON, OR OTHER CHEMICAL CONTAINERS SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH THE EPA'S STANDARD PRACTICES. NO SOLID MATERIAL INCLUDING BUILDING AND CONSTRUCTION MATERIAL SHALL BE DISPOSED OF, DISCHARGED OR BURIED ONSITE.

OFFSITE VEHICLE TRACKING: LOADED HAUL TRUCKS SHALL BE COVERED WITH A TARPULIN. EXCESS DIRT MATERIAL ON THE ROADS SHALL BE REMOVED IMMEDIATELY. HAULING ON UNPAVED SURFACES SHALL BE MONITORED TO MINIMIZE DUST AND CONTROL EROSION. HAUL ROADS SHALL BE WATERED OR OTHER CONTROLS PROVIDED AS NECESSARY TO REDUCE DUST AND CONTROL SEDIMENTS.

SANITARY WASTE: THE CONTRACTOR SHALL PROVIDE PORTABLE SANITARY WASTE FACILITIES. THESE FACILITIES SHALL BE COLLECTED OR EMPTIED BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR AS REQUIRED BY STATE REGULATIONS.

FERTILIZERS AND PESTICIDES: FERTILIZER SHALL BE APPLIED AT A RATE SPECIFIED BY THE SPECIFICATIONS OR THE MANUFACTURER. THE APPLICATION OF FERTILIZERS SHALL BE ACCOMPLISHED IN A MANNER AS DESCRIBED BY THE SPECIFICATION OR MANUFACTURER TO ENSURE THE PROPER INSTALLATION AND TO AVOID OVER FERTILIZING. PESTICIDES ARE NOT ANTICIPATED FOR THIS PROJECT.

MAINTENANCE

THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE AND REPAIRS OF EROSION AND SEDIMENT CONTROL DEVICES AND THE REMOVAL OF THE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE NOTICE OF TERMINATION IS EXECUTED.

THE CONTRACTOR SHALL REVIEW THE PROJECT AND ALL EROSION AND SEDIMENT CONTROLS ON A DAILY BASIS AND DURING AND FOLLOWING RAINFALL EVENTS. AN INSPECTION FORM HAS BEEN PROVIDED IN THE SPECIFICATIONS. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A LOG OF ALL THE DAILY INSPECTION REPORTS, GRADING AND STABILIZATION ACTIVITIES, AND SWPPP AMENDMENTS AT THE SITE. THE FOLLOWING PRACTICES WILL BE IMPLEMENTED TO MAINTAIN AND MONITOR EROSION AND SEDIMENT CONTROLS.

- PROJECT REVIEW ON A DAILY BASIS.
- PROVIDE AND MAINTAIN RAIN GAUGES ONSITE (IF NOT AVAILABLE IN THE AREA) TO RECORD RAINFALL DATA DAILY.
- REVIEW STABILIZATION PRACTICES AND CONTROLS ON A DAILY BASIS AND MAINTAIN AND REPAIR THESE MEASURES AND CONTROLS AS NECESSARY. TEMPORARY AND/OR PERMANENT SEEDING, MULCHING AND SODDING SHALL BE REPAIRED IN BARE SPOTS AND WASHOUTS, AND HEALTHY GROWTH ESTABLISHED.
- ONCE HEALTHY GROWTH OF TURF IS ESTABLISHED, THE CONTRACTOR SHALL MAINTAIN THESE AREAS TO INSURE THE HEIGHT OF THE GRASS DOES NOT REACH MORE THAN 6 INCHES ABOVE THE ESTABLISHED GRADE.
- REVIEW STRUCTURAL PRACTICES ON A DAILY BASIS AND MAINTAIN AND REPAIR THESE MEASURES AND CONTROLS AS NECESSARY. BUILT UP SEDIMENTS SHALL BE REMOVED FROM SILT FENCES AND FILTER CLOTH SHALL BE REPLACED AS NECESSARY AND WHEN THEY HAVE SERVED THEIR USEFULNESS.
- AN INSPECTION AND MAINTENANCE REPORT SHALL BE COMPLETED WEEKLY AND WITHIN 24 HOURS OF A RAINFALL EVENT OF 0.5 INCHES OR MORE. THE CONTRACTOR SHALL CREATE AN INSPECTION AND MAINTENANCE REPORT LOG AND NOTE ANY AMENDMENTS TO THE SWPPP THAT OCCUR DURING CONSTRUCTION.
- IF THE CONTRACTOR ELECTS TO APPLY FOR PERMITS FOR DISCHARGE OF STORMWATER FROM THE SITE DURING CONSTRUCTION, ALL POINTS OF DISCHARGE OF STORMWATER RUNOFF FROM THE SITE SHALL BE INSPECTED ON A DAILY BASIS AND CONTROLS AND MEASURES REPAIRED AS NECESSARY TO MAINTAIN ACCEPTABLE WATER QUALITY AND DISCHARGE VOLUMES IN ACCORDANCE WITH THE PERMIT.

INSPECTIONS

QUALIFIED PERSONNEL SHALL INSPECT ALL POINTS OF DISCHARGE, AS APPLICABLE, FROM THE PROJECT SITE AND ALL DISTURBED AREAS OF THE CONSTRUCTION SITE THAT HAVE NOT BEEN STABILIZED. DISTURBED AREAS AND AREAS USED FOR STORAGE OF MATERIALS EXPOSED TO PRECIPITATION SHALL BE INSPECTED FOR EVIDENCE OF, OR POTENTIAL FOR POLLUTANTS ENTERING THE STORMWATER MANAGEMENT SYSTEM. THE STORMWATER MANAGEMENT SYSTEM AND EROSION AND SEDIMENT CONTROL MEASURES SHALL BE OBSERVED TO ENSURE THAT THEY ARE OPERATING CORRECTLY. INSPECTION AND MAINTENANCE REPORTS SHALL BE COMPLETED AT LEAST EVERY WEEK AND FOLLOWING A RAINFALL EVENT OF 0.5 INCHES OF WATER OR GREATER (SEE ATTACHED FORM). THESE FORMS SHALL BE RETAINED FOR A PERIOD OF AT LEAST 3 YEARS FOLLOWING THE DATE THE SITE IS FINALLY STABILIZED.

ALLOWABLE NON-STORMWATER DISCHARGE MANAGEMENT

ALLOWABLE NON-STORMWATER DISCHARGES AND THE MEASURES USED TO ELIMINATE OR REDUCE THEM AND TO PREVENT THEM FROM BECOMING CONTAMINATED MAY INCLUDE DEPENDING ON THE PERMIT:

- WATERS USED TO WASH VEHICLES WHERE DETERGENTS ARE NOT USED
- WATER USED TO CONTROL DUST
- POTABLE WATER INCLUDING UNCONTAMINATED WATER LINE FLUSHINGS
- ROUTINE EXTERNAL BUILDING WASH DOWN THAT DOES NOT USE DETERGENTS
- PAVEMENT WASH WATER WHERE SPILLS OR LEAKS OF TOXIC OR HAZARDOUS MATERIALS HAVE NOT OCCURRED (UNLESS ALL SPILLED MATERIAL HAS BEEN REMOVED) AND WHERE DETERGENTS ARE NOT USED
- UNCONTAMINATED AIR CONDITIONING OR COMPRESSOR CONDENSATE
- UNCONTAMINATED GROUND WATER OR SPRING WATER
- FOUNDATION OR FOOTING DRAINS WHERE FLOWS ARE NOT CONTAMINATED WITH PROCESS MATERIALS SUCH AS SOLVENTS
- UNCONTAMINATED EXCAVATION DEWATERING
- LANDSCAPE IRRIGATION

ESTABLISH PROPER EQUIPMENT/VEHICLE FUELING AND MAINTENANCE PRACTICES

EQUIPMENT FUELING AND MAINTENANCE, OIL CHANGING, ETC., SHALL BE PERFORMED AWAY FROM WATERCOURSES, DITCHES, OR STORM DRAINS, IN AN AREA DESIGNATED FOR THAT PURPOSE. THE DESIGNATED AREA SHALL BE EQUIPPED FOR RECYCLING OIL AND CATCHING SPILLS. SECONDARY CONTAINMENT SHALL BE PROVIDED FOR ALL FUEL OIL STORAGE TANKS. THESE AREAS MUST BE INSPECTED EVERY SEVEN DAYS AND WITHIN 24 HOURS OF A 0.5 INCH OR GREATER RAIN EVENT TO ENSURE THERE ARE NO EXPOSED MATERIALS WHICH WOULD CONTAMINATE STORM WATER.

SPILL PREVENTION CONTROL PLAN

SITE OPERATORS MUST BE AWARE THAT SPILL PREVENTION CONTROL AND COUNTERMEASURES (SPCC) REQUIREMENTS APPLY. AN SPCC PLAN IS REQUIRED FOR SITES WITH ONE SINGLE ABOVEGROUND STORAGE OF 1,320 GALLONS OR MORE, OR 42,000 GALLONS OF UNDERGROUND STORAGE. SOILS THAT HAVE BEEN CONTAMINATED MUST BE DISPOSED OF IN ACCORDANCE WITH SECTION "CONTAMINATED SOILS" FOUND BELOW.

SPILLS ON PAVEMENT SHALL BE ABSORBED WITH SAWDUST, CAT LITTER OR OTHER ABSORBENT MATERIAL AND DISPOSED OF WITH THE TRASH AT A LICENSED SANITARY LANDFILL. HAZARDOUS OR INDUSTRIAL WASTES SUCH AS MOST SOLVENTS, GASOLINE, OIL-BASED PAINTS, AND CEMENT CURING COMPOUNDS REQUIRE SPECIAL HANDLING. SPILLS SHALL BE REPORTED TO THE EPA (1-913-281-0991). SPILLS OF 25 GALLONS OR MORE OF PETROLEUM PRODUCTS SHALL BE REPORTED TO EPA (1-913-281-0991), THE LOCAL FIRE DEPARTMENT, AND THE LOCAL EMERGENCY PLANNING COMMITTEE WITHIN 30 MINUTES OF THE DISCOVERY OF THE RELEASE. ALL SPILLS, WHICH RESULT IN CONTACT WITH WATER OF THE STATE, MUST BE REPORTED TO THE EPA'S HOTLINE.

CONTAMINATED SOILS

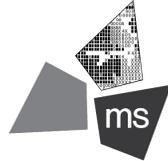
IF SUBSTANCES SUCH AS OIL, DIESEL FUEL, HYDRAULIC FLUID, ANTIFREEZE, ETC., ARE SPILLED, LEAKED, OR RELEASED ONTO THE SOIL, THE SOIL SHOULD BE DUG UP AND DISPOSED OF AT A LICENSED SANITARY LANDFILL OR OTHER APPROVED PETROLEUM CONTAMINATED SOIL REMEDIATION FACILITY (NOT A CONSTRUCTION/DEMOLITION DEBRIS LANDFILL). PLEASE BE AWARE THAT STORM WATER RUN OFF ASSOCIATED WITH CONTAMINATED SOILS ARE NOT BEING AUTHORIZED UNDER THE EPA'S GENERAL STORMWATER PERMIT ASSOCIATED WITH CONSTRUCTION ACTIVITIES. IN THE EVENT THERE ARE LARGE EXTENSIVE AREAS OF CONTAMINATED SOILS ADDITIONAL MEASURES ABOVE AND BEYOND THE CONDITIONS OF THE EPA'S GENERAL CONSTRUCTION STORMWATER PERMIT WILL BE REQUIRED. DEPENDING ON THE EXTENT OF CONTAMINATION, ADDITIONAL TREATMENT AND/OR COLLECTION AND DISPOSAL MAY BE REQUIRED. ALL STORMWATER DISCHARGES ASSOCIATED WITH CONTAMINATED SOILS MUST BE AUTHORIZED UNDER AN ALTERNATE NPDES PERMIT.

Revision Date of Permit
Lee's Summit, Missouri

SIR UPD	05/08/2023	09/13/21
60% SET		01/24/22
POP APPLICATION TO CITY		02/24/22
RESPONSE TO CITY COMMENTS I		03/29/22
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PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

SWPPP NOTES



DRAWN BY: TDB

CHECKED BY: PGD

PROJECT NO: 40497-21

DRAWING

C-10.0



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TEMPORARY SEEDING

DESCRIPTION

TEMPORARY SEEDINGS ESTABLISH TEMPORARY COVER ON DISTURBED AREAS BY PLANTING APPROPRIATE RAPIDLY GROWING ANNUAL GRASSES OR SMALL GRAINS. TEMPORARY SEEDING PROVIDES EROSION CONTROL ON AREAS IN BETWEEN CONSTRUCTION OPERATIONS. GRASSES, WHICH ARE QUICK GROWING, ARE SEEDED AND USUALLY MULCHED TO PROVIDE PROMPT, TEMPORARY SOIL STABILIZATION. IT EFFECTIVELY MINIMIZES THE AREA OF A CONSTRUCTION SITE PRONE TO EROSION AND SHOULD BE USED EVERYWHERE THE SEQUENCE OF CONSTRUCTION OPERATIONS ALLOWS VEGETATION TO BE ESTABLISHED.

SPECIFICATIONS FOR TEMPORARY SEEDING

TEMPORARY SEEDING SPECIES SELECTION			
SEEDING DATES	SPECIES	LB/1000 SF	LB/ACREA
MAR 1 TO AUG 15	OATS	3	128-4 BUSHEL
	TALL FESCUE	1	40
	ANNUAL RYEGRASS	1	40
	PERENNIAL RYEGRASS	1	40
	TALL FESCUE	1	40
	ANNUAL RYEGRASS	1	40
	ANNUAL RYEGRASS	1.25	55
	PERENNIAL RYEGRASS	3.25	142
	CREEPING RED FESCUE	0.40	17
	KENTUCKY BLUEGRASS	0.40	17
	OATS	3	128-3 BUSHEL
	TALL FESCUE	1	40
AUG 16 TO NOV	RYE	3	112-3 BUSHEL
	TALL FESCUE	1	40
	ANNUAL RYEGRASS	1	40
	WHEAT	3	120-2 BUSHEL
	TALL FESCUE	1	40
	ANNUAL RYEGRASS	1	40
	PERENNIAL RYE	1	40
	TALL FESCUE	1	40
	ANNUAL RYEGRASS	1	40
	ANNUAL RYEGRASS	1.25	40
	PERENNIAL RYEGRASS	3.25	40
	CREEPING RED FESCUE	0.40	40
KENTUCKY BLUEGRASS	0.40	40	
NOV 1 TO FEB 29	USE MULCH ONLY OR DORMANT SEEDING		

- STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SUCH AS DIVERSIONS AND SEDIMENT TRAPS SHALL BE INSTALLED AND STABILIZED WITH TEMPORARY SEEDING PRIOR TO GRADING THE REST OF THE CONSTRUCTION SITE.
- TEMPORARY SEED SHALL BE APPLIED BETWEEN CONSTRUCTION OPERATIONS ON SOIL THAT WILL NOT BE GRADED OR REWORKED FOR 14 DAYS OR GREATER. THESE IDLE AREAS SHALL BE SEEDED WITHIN 7 DAYS AFTER GRADING.
- THE SEEDBED SHOULD BE PULVERIZED AND LOOSE TO ENSURE THE SUCCESS OF ESTABLISHING VEGETATION. TEMPORARY SEEDING SHOULD NOT BE POSTPONED IF IDEAL SEEDBED PREPARATION IS NOT POSSIBLE.
- SOIL AMENDMENTS—TEMPORARY VEGETATION SEEDING RATES SHALL ESTABLISH ADEQUATE STANDS OF VEGETATION, WHICH MAY REQUIRE THE USE OF SOIL AMENDMENTS. BASE RATES FOR LIME AND FERTILIZER SHALL BE USED.
- SEEDING METHOD—SEED SHALL BE APPLIED UNIFORMLY WITH A CYCLONE SPREADER, DRILL, CULTIPACKER SEEDER, OR HYDROSEEDER. WHEN FEASIBLE, SEED THAT HAS BEEN BROADCAST SHALL BE COVERED BY RAKING OR DRAGGING AND THEN LIGHTLY TAMPED INTO PLACE USING A ROLLER OR CULTIPACKER. IF HYDROSEEDING IS USED, THE SEED AND FERTILIZER WILL BE MIXED ON-SITE AND THE SEEDING SHALL BE DONE IMMEDIATELY AND WITHOUT INTERRUPTION.

MULCHING TEMPORARY SEEDING:

- APPLICATIONS OF TEMPORARY SEEDING SHALL INCLUDE MULCH, WHICH SHALL BE APPLIED DURING OR IMMEDIATELY AFTER SEEDING. SEEDINGS MADE DURING OPTIMUM SEEDING DATES ON FAVORABLE, VERY FLAT SOIL CONDITIONS MAY NOT NEED MULCH TO ACHIEVE ADEQUATE STABILIZATION.
- MATERIALS:
 - STRAW—IF STRAW IS USED, IT SHALL BE UNROTTED SMALL-GRAIN STRAW APPLIED AT A RATE OF 2 TONS PER ACRE OR 90 LBS/ 1,000 SQ. FT. (2-3 BALES)
 - HYDROSEEDERS—IF WOOD CELLULOSE FIBER IS USED, IT SHALL BE USED AT 2000 LBS / AC. OR 46 LB. / 1,000-SQ.-FT.
 - OTHER—OTHER ACCEPTABLE MULCHES INCLUDE MULCH MATTINGS APPLIED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS OR WOOD CHIPS APPLIED AT 6 TON/AC.
- STRAW MULCH SHALL BE ANCHORED IMMEDIATELY TO MINIMIZE LOSS BY WIND OR WATER. ANCHORING METHODS:
 - MECHANICAL—A DISK, CRIMPER, OR SIMILAR TYPE TOOL SHALL BE SET STRAIGHT TO PUNCH OR ANCHOR THE MULCH MATERIAL INTO THE SOIL. STRAW MECHANICALLY ANCHORED SHALL NOT BE FINELY CHOPPED BUT LEFT TO A LENGTH OF APPROXIMATELY 6 INCHES.
 - MULCH NETTING—NETTING SHALL BE USED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS. NETTING MAY BE NECESSARY TO HOLD MULCH IN PLACE IN AREAS OF CONCENTRATED RUNOFF AND ON CRITICAL SLOPES.
 - SYNTHETIC BINDERS—SYNTHETIC BINDERS SUCH AS ACRYLIC DLR (AGRI-TAC), DCA-70, PETROSET, TERRA TRACK OR EQUIVALENT MAY BE USED AT RATES RECOMMENDED BY THE MANUFACTURER.
 - WOOD-CELLULOSE FIBER—WOOD-CELLULOSE FIBER BINDER SHALL BE APPLIED AT A NET DRY WT. OF 750 LB./AC. THE WOOD-CELLULOSE FIBER SHALL BE MIXED WITH WATER AND THE MIXTURE SHALL CONTAIN A MAXIMUM OF 50 LB. / 100 GAL.

DUST CONTROL

DESCRIPTION

DUST CONTROL INVOLVES PREVENTING OR REDUCING DUST FROM EXPOSED SOILS OR OTHER SOURCES DURING LAND DISTURBING, DEMOLITION AND CONSTRUCTION ACTIVITIES TO REDUCE THE PRESENCE OF AIRBORNE SUBSTANCES WHICH MAY PRESENT HEALTH HAZARDS, TRAFFIC SAFETY PROBLEMS OR HARM ANIMAL OR PLANT LIFE.

SPECIFICATIONS FOR DUST CONTROL

- VEGETATIVE COVER AND/MULCH – APPLY TEMPORARY OR PERMANENT SEEDING AND MULCH TO AREAS THAT WILL REMAIN IDLE FOR OVER 21 DAYS. SAVING EXISTING TREES AND LARGE SHRUBS WILL ALSO REDUCE SOIL AND AIR MOVEMENT ACROSS DISTURBED AREAS. SEE TEMPORARY SEEDING; PERMANENT SEEDING; MULCHING PRACTICES; AND TREE AND NATURAL AREA PROTECTION PRACTICES.
- WATERING – SPRAY SITE WITH WATER UNTIL THE SURFACE IS WET BEFORE AND DURING GRADING AND REPEAT AS NEEDED, ESPECIALLY ON HAUL ROADS AND OTHER HEAVY TRAFFIC ROUTES. WATERING SHALL BE DONE AT A RATE THAT PREVENTS DUST BUT DOES NOT CAUSE SOIL EROSION. WETTING AGENTS SHALL BE UTILIZED ACCORDING TO MANUFACTURER'S INSTRUCTIONS.
- SPRAY-ON ADHESIVES – APPLY ADHESIVE ACCORDING TO THE FOLLOWING TABLE OR MANUFACTURER'S INSTRUCTIONS.
- STONE – GRADED ROADWAYS AND OTHER SUITABLE AREAS WILL BE STABILIZED USING CRUSHED STONE OR COARSE GRAVEL AS SOON AS PRACTICABLE AFTER REACHING AN INTERIM OR FINAL GRADE. CRUSHED STONE OR COARSE GRAVEL CAN BE USED AS A PERMANENT COVER TO PROVIDE CONTROL OF SOIL EMISSIONS.
- BARRIERS – EXISTING WINDBREAK VEGETATION SHALL BE MARKED AND PRESERVED. SNOW FENCING OR OTHER SUITABLE BARRIER MAY BE PLACED PERPENDICULAR TO PREVAILING AIR CURRENTS AT INTERVALS OF ABOUT 15 TIMES THE BARRIER HEIGHT TO CONTROL AIR CURRENTS AND BLOWING SOIL.
- OPERATION AND MAINTENANCE – WHEN TEMPORARY DUST CONTROL MEASURES ARE USED, REPETITIVE TREATMENT SHOULD BE APPLIED AS NEEDED TO ACCOMPLISH CONTROL. STREET CLEANING - PAVED AREAS THAT HAVE ACCUMULATED SEDIMENT FROM CONSTRUCTION SHOULD BE CLEANED DAILY, OR AS NEEDED, UTILIZING A STREET SWEEPER OR BUCKET-TYPE END LOADER OR SCRAPER.

PERMANENT SEEDING

DESCRIPTION

PERENNIAL VEGETATION IS ESTABLISHED ON AREAS THAT WILL NOT BE RE-DISTURBED FOR PERIODS LONGER THAN 12 MONTHS. PERMANENT SEEDING INCLUDES SITE PREPARATION, SEEDBED PREPARATION, PLANTING SEED, MULCHING, IRRIGATION AND MAINTENANCE.

PERMANENT VEGETATION IS USED TO STABILIZE SOIL, REDUCE EROSION, PREVENT SEDIMENT POLLUTION, REDUCE RUNOFF BY PROMOTING INFILTRATION, AND PROVIDE STORMWATER QUALITY BENEFITS OFFERED BY DENSE GRASS COVER.

SPECIFICATION FOR PERMANENT SEEDING

SITE PREPARATION:

- SUBSOILER, PLOW, OR OTHER IMPLEMENT SHALL BE USED TO REDUCE SOIL COMPACTION AND ALLOW MAXIMUM INFILTRATION. (MAXIMIZING INFILTRATION WILL HELP CONTROL BOTH RUNOFF RATE AND WATER QUALITY.) SUBSOILING SHOULD BE DONE WHEN THE SOIL MOISTURE IS LOW ENOUGH TO ALLOW THE SOIL TO CRACK OR FRACTURE. SUBSOILING SHALL NOT BE DONE ON SLIP-PRONE AREAS WHERE SOIL PREPARATION SHOULD BE LIMITED TO WHAT IS NECESSARY FOR ESTABLISHING VEGETATION.
- THE SITE SHALL BE GRADED AS NEEDED TO PERMIT THE USE OF CONVENTIONAL EQUIPMENT FOR SEEDBED PREPARATION AND SEEDING.
- TOPSOIL SHALL BE APPLIED WHERE NEEDED TO ESTABLISH VEGETATION.

SEEDBED PREPARATION:

- TEST THE SOIL CONDITIONS FOR FEEDING BEFORE STARTING SEEDING AND MULCHING.
- LIME—AGRICULTURAL GROUND LIMESTONE SHALL BE APPLIED TO ACID SOIL AS RECOMMENDED BY A SOIL TEST. IN LIEU OF A SOIL TEST, LIME SHALL BE APPLIED AT THE RATE OF 100 POUNDS PER 1,000-SQ. FT. OR 2 TONS PER ACRE.
- FERTILIZER—FERTILIZER SHALL BE APPLIED AS RECOMMENDED BY A SOIL TEST. CONTRACTOR SHALL PERFORM LAB TESTING ON SOIL AND PROVIDE A CERTIFIED FERTILIZER RATIO FOR THE SITE SOILS AND SPECIFIED SEED MIX.
- THE LIME AND FERTILIZER SHALL BE WORKED INTO THE SOIL WITH A DISK HARROW, SPRING-TOOTH HARROW, OR OTHER SUITABLE FIELD IMPLEMENT TO A DEPTH OF 3 INCHES. ON SLOPING LAND, THE SOIL SHALL BE WORKED ON THE CONTOUR.

SEEDING DATES AND SOIL CONDITIONS:

SEEDING SHOULD BE DONE MARCH 1 TO MAY 31 OR AUGUST 1 TO SEPTEMBER 30. IF SEEDING OCCURS OUTSIDE OF THE ABOVE SPECIFIED DATES, ADDITIONAL MULCH AND IRRIGATION MAY BE REQUIRED TO ENSURE A MINIMUM OF 80% GERMINATION. TILLAGE FOR SEEDBED PREPARATION SHOULD BE DONE WHEN THE SOIL IS DRY ENOUGH TO CRUMBLE AND NOT FORM RIBBONS WHEN COMPRESSED BY HAND. FOR WINTER SEEDING, SEE THE FOLLOWING SECTION ON DORMANT SEEDING.

DORMANT SEEDINGS:

- SEEDINGS SHOULD NOT BE MADE FROM OCTOBER 1 THROUGH NOVEMBER 20. DURING THIS PERIOD, THE SEEDS ARE LIKELY TO GERMINATE BUT PROBABLY WILL NOT BE ABLE TO SURVIVE THE WINTER.
- THE FOLLOWING METHODS MAY BE USED FOR 'DORMANT SEEDING':
 - FROM OCTOBER 1 THROUGH NOVEMBER 20, PREPARE THE SEEDBED, ADD THE REQUIRED AMOUNTS OF LIME AND FERTILIZER, THEN MULCH AND ANCHOR. AFTER NOVEMBER 20, AND BEFORE MARCH 15, BROADCAST THE SELECTED SEED MIXTURE. INCREASE THE SEEDING RATES BY 50% FOR THIS TYPE OF SEEDING.
 - FROM NOVEMBER 20 THROUGH MARCH 15, WHEN SOIL CONDITIONS PERMIT, PREPARE THE SEEDBED, LIME AND FERTILIZE, APPLY THE SELECTED SEED MIXTURE, MULCH AND ANCHOR. INCREASE THE SEEDING RATES BY 50% FOR THIS TYPE OF SEEDING.
 - APPLY SEED UNIFORMLY WITH A CYCLONE SEEDER, DRILL, CULTIPACKER SEEDER, OR HYDRO-SEEDER (SLURRY MAY INCLUDE SEED AND FERTILIZER) ON A FIRM, MOIST SEEDBED.
 - WHERE FEASIBLE, EXCEPT WHEN A CULTIPACKER TYPE SEEDER IS USED, THE SEEDBED SHOULD BE FIRMED FOLLOWING SEEDING OPERATIONS WITH A CULTIPACKER, ROLLER, OR LIGHT DRAG. ON SLOPING LAND, SEEDING OPERATIONS SHOULD BE ON THE CONTOUR WHERE FEASIBLE.

MULCHING:

- MULCH MATERIAL SHALL BE APPLIED IMMEDIATELY AFTER SEEDING. DORMANT SEEDING SHALL BE MULCHED. 100% OF THE GROUND SURFACE SHALL BE COVERED WITH AN APPROVED MATERIAL.
- MATERIALS:
 - STRAW—IF STRAW IS USED IT SHALL BE UNROTTED SMALL-GRAIN STRAW APPLIED AT THE RATE OF 2 TONS PER ACRE OR 90 POUNDS (TWO TO THREE BALES) PER 1,000-SQ. FT. THE MULCH SHALL BE SPREAD UNIFORMLY BY HAND OR MECHANICALLY APPLIED SO THE SOIL SURFACE IS COVERED. FOR UNIFORM DISTRIBUTION OF HAND-SPREAD MULCH, DIVIDE AREA INTO APPROXIMATELY 1,000-SQ.-FT. SECTIONS AND SPREAD TWO 45-LB. BALES OF STRAW IN EACH SECTION.
 - HYDROSEEDERS—IF WOOD CELLULOSE FIBER IS USED, IT SHALL BE APPLIED AT 2,000 LB./AC. OR 46 LB./1,000 SQ. FT.
 - OTHER—OTHER ACCEPTABLE MULCHES INCLUDE ROLLED EROSION CONTROL MATTINGS OR BLANKETS APPLIED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS OR WOOD CHIPS APPLIED AT 6 TONS PER ACRE.
- STRAW AND MULCH ANCHORING METHODS—STRAW MULCH SHALL BE ANCHORED IMMEDIATELY TO MINIMIZE LOSS BY WIND OR WATER:
 - MECHANICAL—A DISK, CRIMPER, OR SIMILAR TYPE TOOL SHALL BE SET STRAIGHT TO PUNCH OR ANCHOR THE MULCH MATERIAL INTO THE SOIL. STRAW MECHANICALLY ANCHORED SHALL NOT BE FINELY CHOPPED BUT, GENERALLY, BE LEFT LONGER THAN 6 INCHES.
 - MULCH NETTING—NETTING SHALL BE USED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS. NETTING MAY BE NECESSARY TO HOLD MULCH IN PLACE IN AREAS OF CONCENTRATED RUNOFF AND ON CRITICAL SLOPES.
 - ASPHALT EMULSION—ASPHALT SHALL BE APPLIED AS RECOMMENDED BY THE MANUFACTURE OR AT THE RATE OF 160 GALLONS PER ACRE.
 - SYNTHETIC BINDERS—SYNTHETIC BINDERS SUCH AS ACRYLIC DLR (AGRI-TAC), DCA-70, PETROSET, TERRA TACK OR EQUIVALENT MAY BE USED AT RATES SPECIFIED BY THE MANUFACTURER.
 - WOOD CELLULOSE FIBER—WOOD CELLULOSE FIBER SHALL BE APPLIED AT A NET DRY WEIGHT OF 750 POUNDS PER ACRE. THE WOOD CELLULOSE FIBER SHALL BE MIXED WITH WATER WITH THE MIXTURE CONTAINING A MAXIMUM OF 50 POUNDS CELLULOSE PER 100 GALLONS OF WATER.

IRRIGATION:

PERMANENT SEEDING SHALL INCLUDE IRRIGATION TO ESTABLISH VEGETATION DURING DRY WEATHER OR ON ADVERSE SITE CONDITIONS, WHICH REQUIRE ADEQUATE MOISTURE FOR SEED GERMINATION AND PLANT GROWTH. IRRIGATION RATES SHALL BE MONITORED TO PREVENT EROSION AND DAMAGE TO SEEDED AREAS FROM EXCESSIVE RUNOFF. CONTRACTOR SHALL MAINTAIN PERMANENT SEEDING FOR UP TO ONE YEAR FROM SUBSTANTIAL COMPLETION TO FIX, REPAIR, WATER, REFERTILIZE AND/OR RESEED GRASSED AREAS.

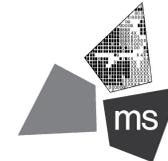
SEED MIX	SEEDING RATE		NOTES
	LBS/ACRE	LBS/1,000 SF	
GENERAL USE			
CREEPING RED FESCUE	20-40	½-1	FOR CLOSE MOWING AND FOR WATERWAYS WITH <2.0 FT/SEC VELOCITY
DOMESTIC RYEGRASS	10-20	½-½	
KENTUCKY BLUEGRASS	20-40	½-1	
TALL FESCUE	40-50	1-1½	
TURF-TYPE (DWARF) FESCUE	90	2½	
STEEP BANKS OR CUT SLOPES			
TALL FESCUE	40-50	1-1½	
CROWN VETCH	10-20	½-½	DO NOT SEED LATER THAN AUGUST
TALL FESCUE	20-30	½-¾	
FLAT PEA	20-25	½-¾	DO NOT SEED LATER THAN AUGUST
TALL FESCUE	20-30	½-¾	
ROAD DITCHES AND SWALES			
TALL FESCUE	40-50	1-1½	
TURF-TYPE (DWARF) FESCUE	90	2½	
KENTUCKY BLUE GRASS	5	½	
LAWNS			
KENTUCKY BLUEGRASS	100-120	2	
PERENNIAL RYEGRASS		2	
KENTUCKY BLUEGRASS	100-120	2	FOR SHADED AREAS
CREEPING RED FESCUE		1-½	

PERMANENT STABILIZATION	
AREA REQUIRING PERMANENT STABILIZATION	TIME FRAME TO APPLY EROSION CONTROLS
ANY AREA THAT WILL LIE DORMANT FOR ONE YEAR OR MORE.	WITHIN 7 DAYS OF THE MOST RECENT DISTURBANCE.
ANY AREA WITHIN 50 FEET OF A STREAM OR A RIPARIAN SETBACK AREA AND AT FINAL GRADE.	WITHIN 2 DAYS OF REACHING FINAL GRADE.
ANY AREA AT FINAL GRADE.	WITHIN 7 DAYS OF REACHING FINAL GRADE WITHIN THAT AREA.

TEMPORARY STABILIZATION	
AREA REQUIRING TEMPORARY STABILIZATION	TIME FRAME TO APPLY EROSION CONTROLS
ANY DISTURBED AREA WITHIN 50 FEET OF A STREAM OR A RIPARIAN SETBACK AREA AND NOT AT FINAL GRADE.	WITHIN 2 DAYS OF THE MOST RECENT DISTURBANCE IF THAT AREA WILL REMAIN IDLE FOR MORE THAN 14 DAYS.
FOR ALL CONSTRUCTION ACTIVITIES, ANY DISTURBED AREA, INCLUDING SOIL STOCKPILES THAT WILL BE DORMANT FOR MORE THAN 14 DAYS BUT LESS THAN ONE YEAR.	WITHIN 7 DAYS OF THE MOST RECENT DISTURBANCE WITHIN THE AREA.
DISTURBED AREAS THAT WILL BE IDLE OVER WINTER.	PRIOR TO NOVEMBER 1.
NOTE: WHERE VEGETATIVE STABILIZATION TECHNIQUES MAY CAUSE STRUCTURAL INSTABILITY OR ARE OTHERWISE UNOBTAINABLE, ALTERNATIVE STABILIZATION TECHNIQUES MUST BE EMPLOYED. THESE TECHNIQUES MAY INCLUDE MULCHING OR EROSION MATTING.	

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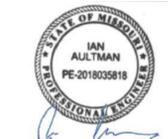
PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

SWPPP NOTES



DRAWN BY: TDB

CHECKED BY: PGD

PROJECT NO: 40497-21

DRAWING

C-10.1



Division of Environment
Lee's Summit, Missouri

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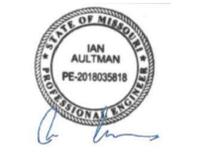
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SHEET TITLE
SWPPP DETAILS



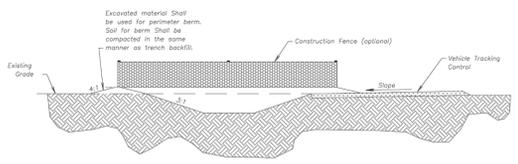
DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21

DRAWING
C-10.2



- Notes for Concrete Washout:**
1. Concrete washout areas shall be installed prior to any concrete placement on site.
 2. Concrete washout area shall include a flat subsurface pit sized relative to the amount of concrete to be placed on site. The slopes leading out of the subsurface pit shall be 3:1. The vehicle tracking post shall be sloped towards the concrete washout area.
 3. Vehicle tracking control is required of the access point to all concrete washout areas.
 4. Signs shall be placed at the construction site entrance, washout area, and elsewhere as necessary to clearly indicate the location(s) of the concrete washout area(s) to operators of concrete truck and pump rigs.
 5. A one-piece impervious liner may be required along the bottom and sides of the subsurface pit in sand or gravelly soils.

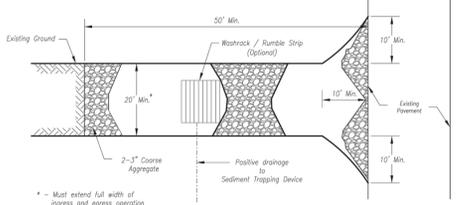
- Maintenance for Concrete Washout:**
1. Concrete washout materials shall be removed once the materials have filled the washout to approximately 75% full.
 2. Concrete washout areas shall be enlarged as necessary to maintain capacity for washed concrete.
 3. Concrete washout water, washed pieces of concrete and all other debris in the subsurface pit shall be transported from the job site in a water-tight container and disposed of properly.
 4. Concrete washout areas shall remain in place until all concrete for the project is placed.
 5. When concrete washout areas are removed, excavations shall be filled with suitable compacted backfill and topped, any disturbed areas associated with the installation, maintenance, and/or removal of the concrete washout areas shall be stabilized.



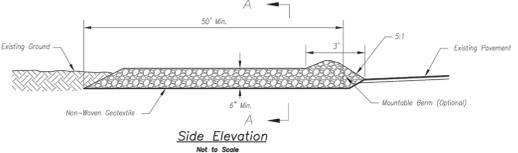
CONCRETE WASHOUT

AMERICAN PUBLIC WORKS ASSOCIATION
Kansas City Metro Chapter
CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT
STANDARD DRAWING NUMBER ESC-01
ADOPTED: 10/24/2016

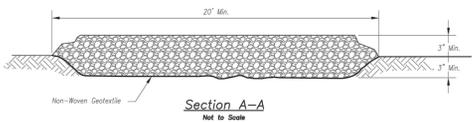
Construction Entrance modified from 2015 Overland Park Standard Details for Erosion and Sediment Control; Concrete Washout modified from 2009 City of Great Bend Standard Drawings.



Plan View
Not to Scale



Side Elevation
Not to Scale

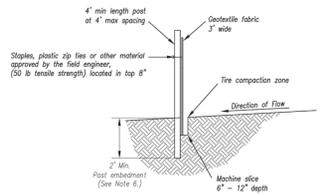
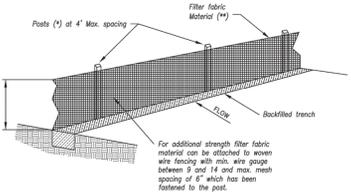


Section A-A
Not to Scale

- Notes for Construction Entrance:**
1. Avoid locating on steep slopes, at curves on public roads, or downhill of disturbed area.
 2. Remove all vegetation and other unsuitable material from the foundation area, grade, and crown for positive drainage.
 3. If slope towards the public road exceeds 2%, construct a 6- to 8-inch high ridge with 3:1 side slopes across the foundation approximately 15 feet from the edge of the public road to divert runoff from it.
 4. Install pipe under the entrance if needed to maintain drainage ditches along public roads.
 5. Place stone to dimensions and grade as shown on plans. Leave surface sloped for drainage.
 6. Divert all surface runoff and drainage from the entrance to a sediment control device.
 7. If conditions warrant, place geotextile fabric on the graded foundation to improve stability.

- Maintenance for Construction Entrance:**
1. Reshape entrance as needed to maintain function and integrity of installation. Top dress with clean aggregate as needed.

CONSTRUCTION ENTRANCE



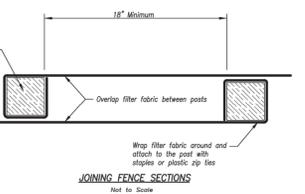
- (*) EOSTS**
- MIN. LENGTH 4'
 - HARDWOOD 1 3/4" x 1 3/4"
 - NO. 3 SOUTHWEST PNE 2 1/4" x 2 1/4"
 - STEEL 1.33 LB/FT

(**) - Geotextile Fabric shall meet the requirements of ASTM M200

SILT FENCE DETAILS
Not to Scale

- Notes:**
1. In order to contain water, the ends of the silt fence must be turned uphill (Figure A).
 2. Long perimeter runs of silt fence must be limited to 100'. Runs should be broken up into several smaller segments to minimize water concentrations (Figure A).
 3. Long slopes should be broken up with intermediate rows of silt fence to slow runoff velocities.
 4. Attach fabric to upstream side of post.
 5. Install posts a minimum of 2' into the ground.
 6. Trenching will only be allowed for small or difficult installation, where slicing machine cannot be reasonably used.

- Maintenance:**
1. Remove and dispose of sediment deposits when the deposit approaches 1/2 the height of silt fence.
 2. Repair as necessary to maintain function and structure.



JOINING FENCE SECTIONS
Not to Scale

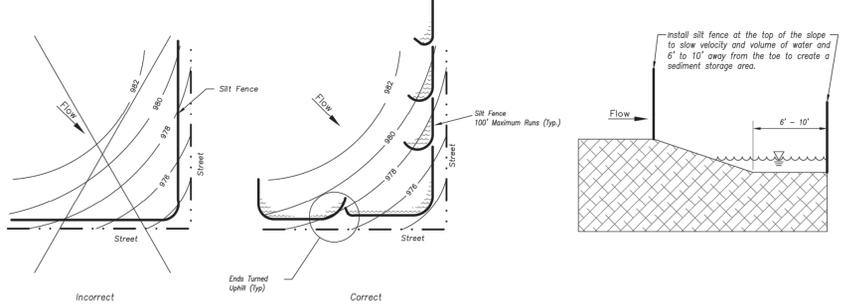


Figure A

SILT FENCE LAYOUT
Not to Scale

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.

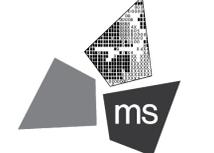
AMERICAN PUBLIC WORKS ASSOCIATION
Kansas City Metro Chapter
SILT FENCE
STANDARD DRAWING NUMBER ESC-03
ADOPTED: 10/24/2016

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Revision Date: 05/08/2023
Location: Lee's Summit, Missouri

SIR UPD	09/13/21
60% SET	01/24/22
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ISSUE FOR PERMIT	03/15/23

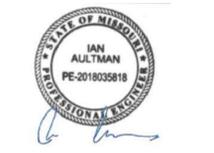
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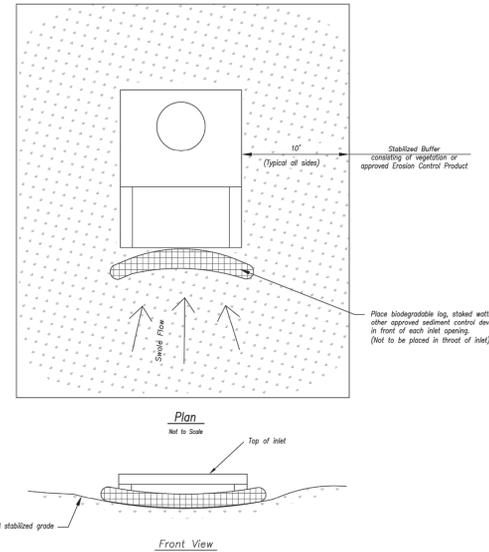
PROJECT
PROPOSED PT22M BUILDING
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE
SWPPP DETAILS



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21

DRAWING
C-10.3

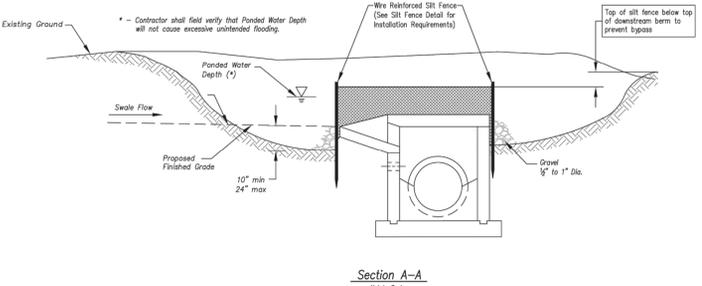


LATE STAGE AREA INLET
(Area inlets at final grade and existing inlets)

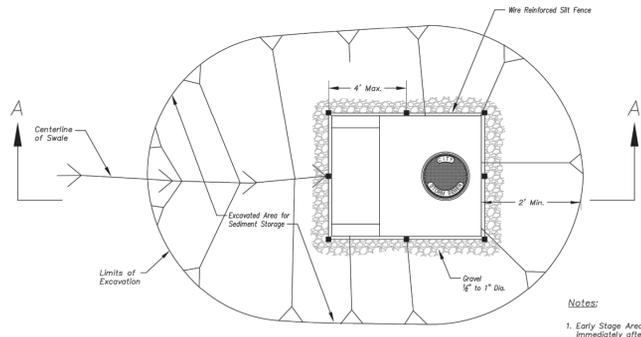
- Maintenance:**
1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
 2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
 3. Repair or replace as necessary to maintain function and integrity of installation.

AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
AREA INLET AND JUNCTION BOX PROTECTION	STANDARD DRAWING NUMBER ESC-07 ADOPTED 10/24/2016

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.



Section A-A
Not to Scale

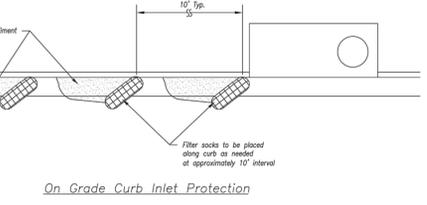


Plan
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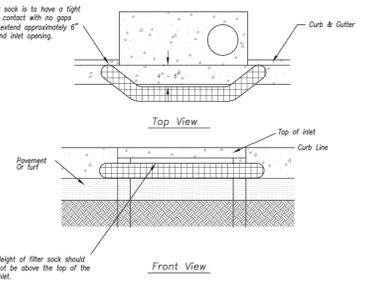
EARLY STAGE AREA INLET
(All open boxes and inlets not at final grade)

- Notes:**
1. Early Stage Area Inlet Sediment Barrier to be installed immediately after inlet or junction box is constructed.
 2. Silt fence shall remain in place until excavated area is removed and Late Stage Area Inlet is being installed.
 3. Backfill excavated area ONLY after final grading of the site. Stabilization of the site is to immediately follow.
 4. Wire reinforced silt fence may be used in place of silt fence attached to wood frame.

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.



On Grade Curb Inlet Protection

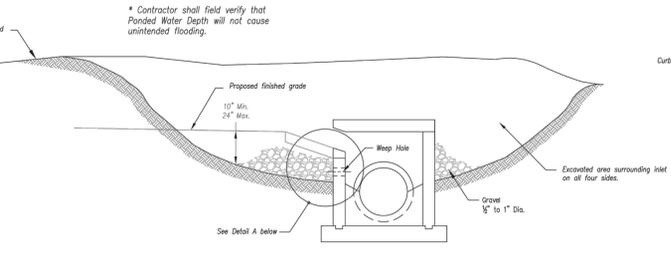


Sump Inlet Sediment Filter

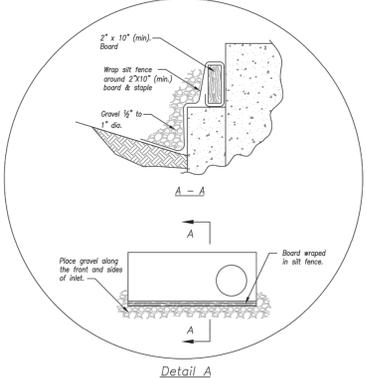
LATE STAGE CURB INLET
(After Pouring Curb and Inlet Throat)

AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
CURB INLET PROTECTION	STANDARD DRAWING NUMBER ESC-06 ADOPTED 10/24/2016

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.



EARLY STAGE CURB INLET
(Open Box and Prior to Pouring Curb and Inlet Throat)



Detail A

- Notes:**
1. Immediately following inlet construction and prior to construction of curb and inlet throat, protect inlet opening by installing 2' x 10' (min.) board wrapped in silt fence. Structure shall have excavated storage area on all four sides to allow settling of sediment (Early Stage Curb Inlet).
 2. When inlet is completed and curb poured, filter socks or approved sump should be used (Late Stage Curb Inlet). Show wetlines are not approved for curb inlet use.
 3. Contractor to field verify ponding water shall not create a traffic hazard.

Maintenance:

1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
3. Repair or replace as necessary to maintain function and integrity of installation.

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SIR UPD	05/08/2023	09/13/21
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PROJECT

PROPOSED PT22M BUILDING

204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

LANDSCAPE PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21

DRAWING

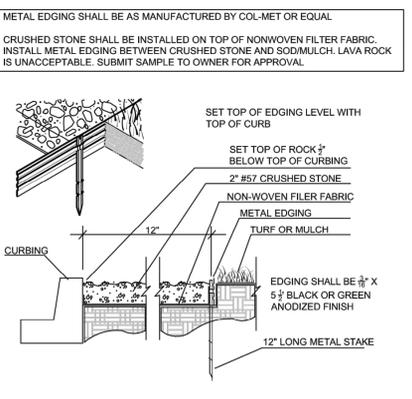
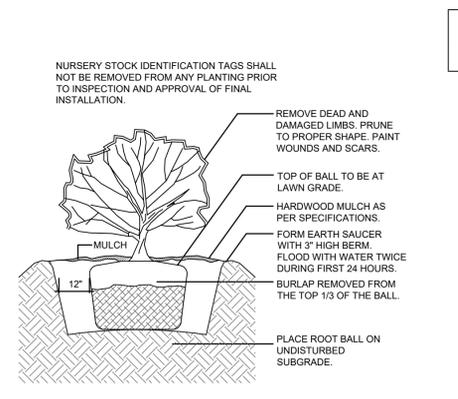
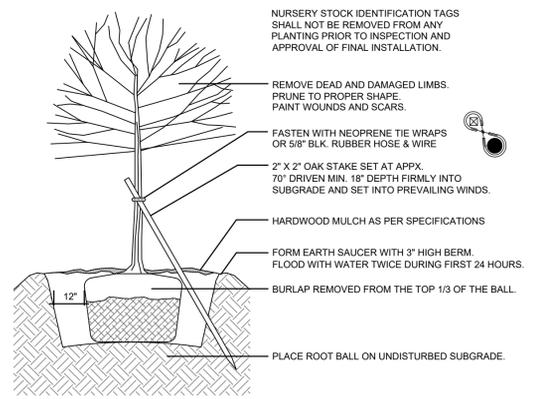
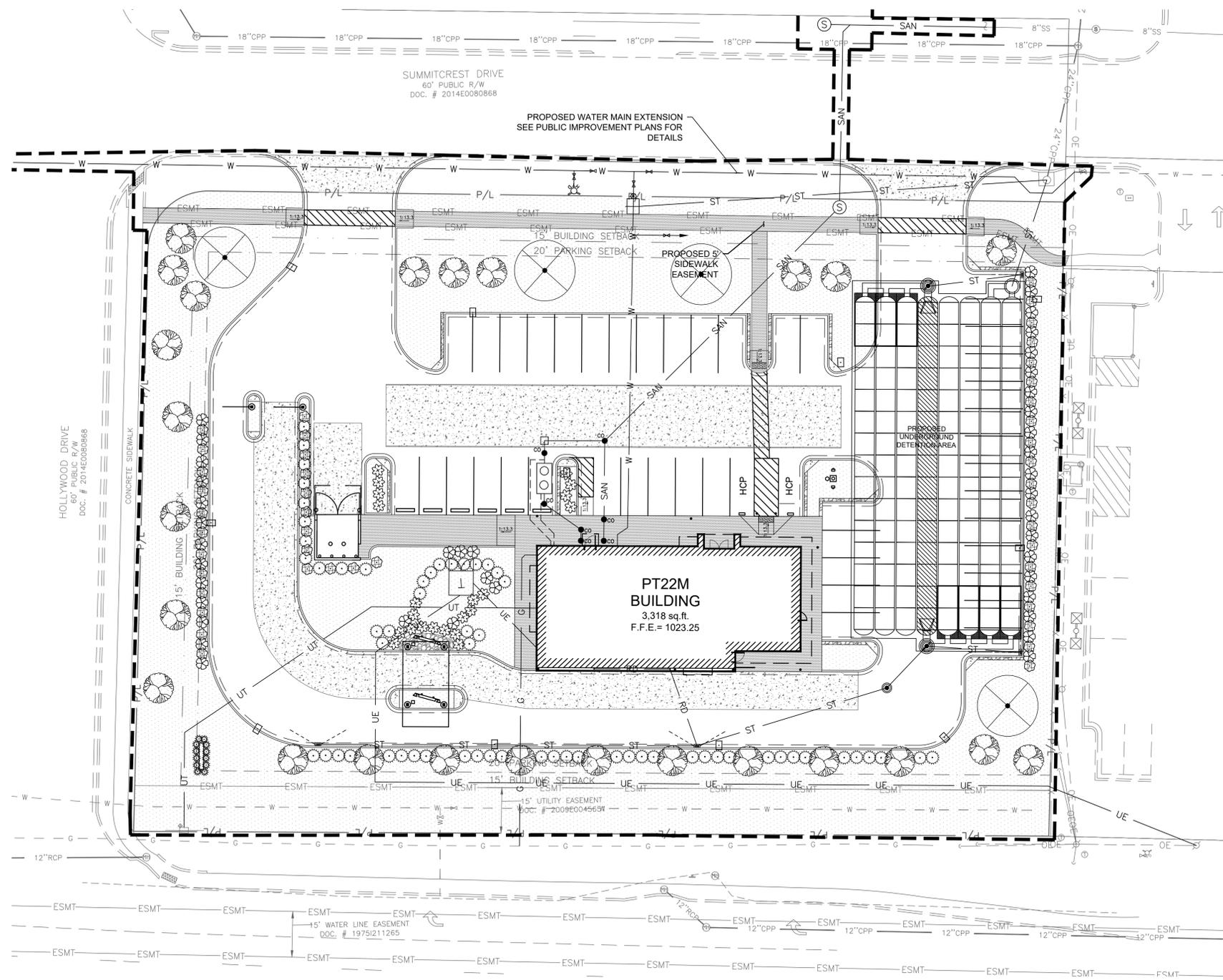
L-1.0

GENERAL NOTES:

- ALL PLANT MATERIALS TO COMPLY WITH THE LATEST EDITION OF A.N.A. STANDARDS FOR NURSERY STOCK AND BE GUARANTEED UNTIL THE CERTIFICATE OF OCCUPANCY IS OBTAINED OR FOR A PERIOD OF ONE (1) YEAR FROM DATE OF FINAL ACCEPTANCE, WHICHEVER IS GREATER. ANY PLANTINGS NEEDING REPLACEMENT WILL BE GUARANTEED FROM THE TIME OF REPLACEMENT IF AFTER FINAL ACCEPTANCE.
- LANDSCAPE CONTRACTOR IS TO VERIFY LOCATION OF ALL UNDERGROUND UTILITIES AND RECEIVE APPROVAL FROM GENERAL CONTRACTOR OR SITE SUPERVISOR, IF NECESSARY, TO MAKE CHANGES IN PLANT LOCATIONS.
- LANDSCAPE CONTRACTOR MUST COORDINATE WITH GENERAL CONTRACTOR AND OTHER SITE OPERATIONS.
- MINOR ADJUSTMENTS TO THE PLANT LOCATIONS ARE TO BE MADE IN THE CASE OF ANY CONFLICTS WITH PROPOSED UTILITIES.
- ALL PLANTING BEDS AND FREE STANDING TREES TO BE MULCHED WITH 4" OF SHREDDED HARDWOOD MULCH. BEDS ARE TO BE GRADED SMOOTH AND FREE OF SOIL CLODS AND STONES. ALL TREES TO BE STAKED AND WRAPPED WITH KRAFT TREE WRAP.
- ALL PLANTS ARE TO BE REMOVED FROM CONTAINERS, CAGES AND NON-BIODEGRADABLE MATERIALS.
- GENERAL CONTRACTOR IS RESPONSIBLE FOR FINISHED GRADES; LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR FINE GRADING AND TO PROVIDE 4" OF AMENDED TOPSOIL FOR PLANTING BEDS.
- ALL ORGANIC MATTER AND DEBRIS ARE TO BE REMOVED FROM THE SITE BY THE LANDSCAPE CONTRACTOR. LAWN AREAS AND BEDS SHOULD BE FREE OF STONES GREATER THAN 2".
- PLANT QUANTITIES HAVE BEEN PROVIDED FOR CONVENIENCE ONLY; THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR HIS OWN "TAKE OFFS". DRAWING PREVAILS OVER WRITTEN QUANTITIES.
- THE LANDSCAPE CONTRACTOR SHALL SUBMIT A ONE (1) YEAR MAINTENANCE CONTRACT FOR CONSIDERATION BY THE OWNER. CONTRACT SHALL BE SEPARATE FROM INSTALLATION CONTRACT.
- PLANTING BEDS SHALL BE TREATED WITH A PRE-EMERGENT HERBICIDE APPLIED AT PRODUCT SPECIFIED RATE UNLESS OTHERWISE NOTED.
- PLANTING SHALL BE FERTILIZED UPON INSTALLATION. RECOMMENDED FERTILIZER SHALL BE MIXED WITH BACKFILL AT PRODUCT SPECIFIED RATE.
- BED EDGE SHALL BE SMOOTH, CONSISTENT 4 1/2" DEEP AND HAND CUT, EDGES TO BE LOCATED BETWEEN ALL BEDS (INCLUDING TREES) AND LAWN AREAS.
- CONTRACTOR TO SEED ALL DISTURBED AREAS WITH A LOCALLY ADAPTIVE SEED MIX UNLESS OTHERWISE DIRECTED BY THE GENERAL CONTRACTOR.
- TOPSOIL SHALL BE BACK FILLED TO PROVIDE POSITIVE DRAINAGE OF ALL LANDSCAPE AREAS. SEE GRADING AND DRAINAGE PLAN SHEET C-5.0.
- LANDSCAPING MATERIALS ARE REQUIRED TO PROVIDE FULL SCREENING AT THE TIME OF PLANTING.
- ALL OPEN AREAS NOT COVERED WITH OTHER MATERIALS SHALL BE COVERED WITH SOD.

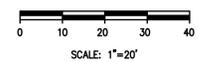
PROPOSED PLANT SCHEDULE

TREES	BOTANICAL NAME	COMMON NAME	TYPE	QTY	HEIGHT AT TIME OF PLANTING	APPROXIMATE MATURE HEIGHT (H X W)	CONT
AR	ACER RUBRUM	RED MAPLE	DECIDUOUS	4	3" CAL MEASURED AT A POINT 6" ABOVE THE GROUND OR TOP OF ROOT BALL, AT PLANTING.	(40' X 40')	B+B
AC	AMELANCHIER CANADENSIS	SERVICEBERRY	DECIDUOUS	25	3" CAL MEASURED AT A POINT 6" ABOVE THE GROUND OR TOP OF ROOT BALL, AT PLANTING.	(15' X 25')	B+B
SHRUBS	BOTANICAL NAME	COMMON NAME	TYPE	QTY			
WF	WEIGELA FLORIDA 'ALEXANDRA'	WINE AND ROSES WEIGELA	DECIDUOUS	38	2 GAL	(4' X 4')	
RI	RHAPHIOLEPIS INDICA 'MAJESTIC BEAUTY'	MAJESTIC BEAUTY INDIAN HAWTHORN	EVERGREEN	44	2 GAL	(4' X 4')	
HP	HYDRANGEA PANICULATA 'LITTLE LIME'	LITTLE LIME HYDRANGEA	DECIDUOUS	22	2 GAL	(3' X 3')	
IV	ILEX VOMITORIA 'NANA'	DWARF YAUPON	EVERGREEN	60	2 GAL	(4' X 5')	



LEGEND

PROPOSED	DESCRIPTION
[Pattern]	GRASS/LANDSCAPED AREA TO BE IRRIGATED
[Pattern]	CONCRETE SIDEWALK
[Pattern]	HEAVY DUTY CONCRETE PAVEMENT
[Pattern]	ROCK AREA
[Pattern]	HEAVY DUTY ASPHALT PAVEMENT

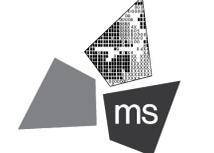


Revision Date: 05/08/2023
Lee's Summit, Missouri

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PROJECT

PROPOSED PT22M BUILDING

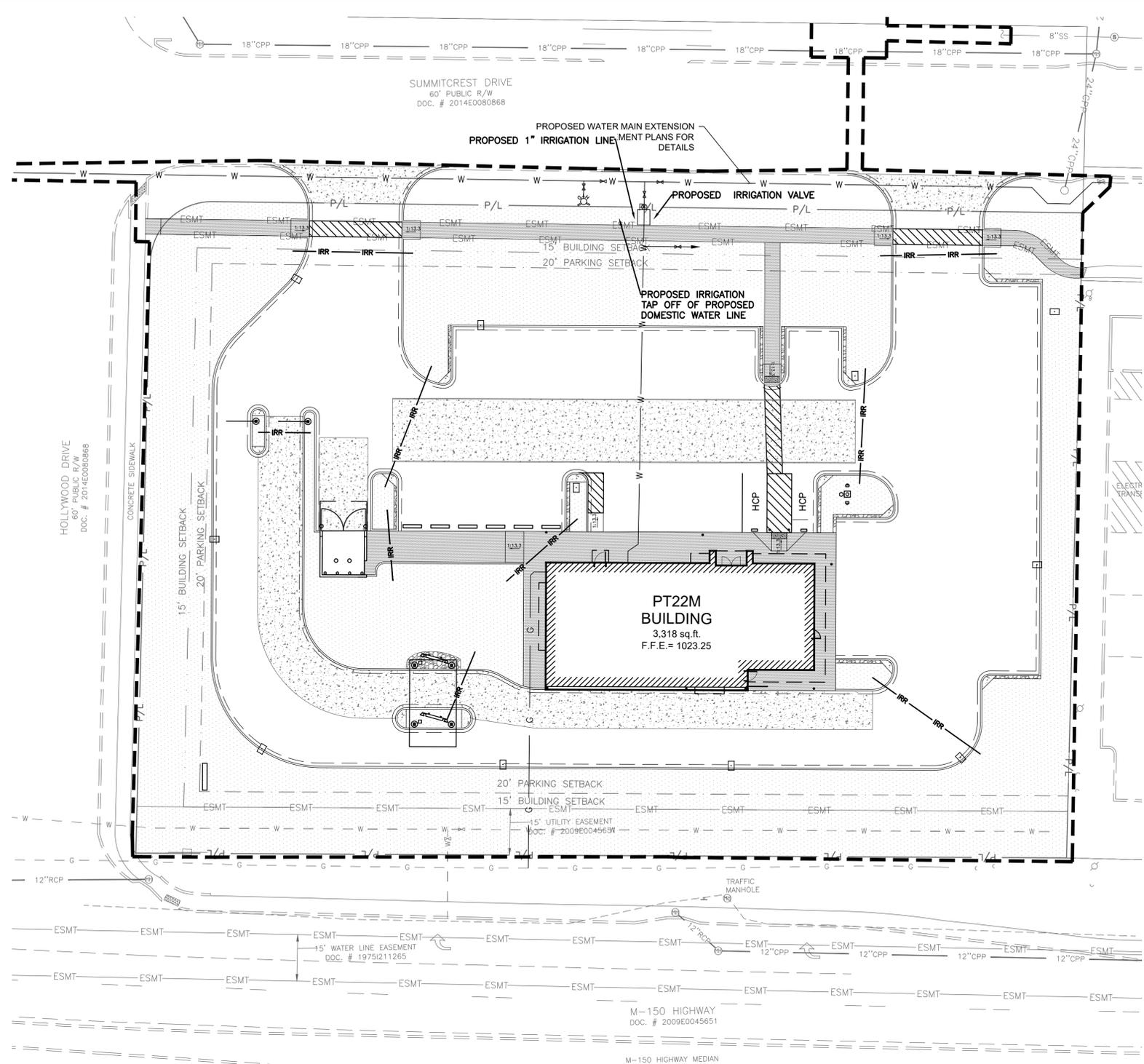
204 SW M150 HWY
LEE'S SUMMIT, MO 64081

SHEET TITLE

IRRIGATION PLAN



DRAWN BY: TDB
CHECKED BY: PGD
PROJECT NO: 40497-21
DRAWING



LEGEND

PROPOSED	DESCRIPTION	PROPOSED	DESCRIPTION
	GRASS/LANDSCAPED AREA TO BE IRRIGATED		ROCK AREA
	CONCRETE SIDEWALK		HEAVY DUTY ASPHALT PAVEMENT
	HEAVY DUTY CONCRETE PAVEMENT		4\"/>

- A. THE LANDSCAPE IRRIGATION SYSTEM SHALL IRRIGATE ALL PROPOSED LANDSCAPE AND GRASS AREAS ON THE PROPERTY. THE DESIGN, PERMITTING, AND INSTALLATION OF THE SYSTEM SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE/IRRIGATION CONTRACTOR (CONTRACTOR).
- B. THE CONTRACTOR IS TO INSTALL EQUIPMENT NECESSARY TO PROVIDE A COMPLETE, FUNCTIONAL SYSTEM THAT IS IN COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS.
- C. THE IRRIGATION CONTRACTOR SHALL SUBMIT SHOP DRAWINGS TO THE OWNER'S REPRESENTATIVE FOR APPROVAL, PRIOR TO CONSTRUCTION, WHICH WILL ILLUSTRATE TYPE OF HEADS, VALVES, CONTROLLER, PIPING AND ACCESSORIES. IRRIGATION HEADS, VALVES AND CONTROLLER ARE TO BE FROM A SINGLE MANUFACTURER. ALL EQUIPMENT MUST HAVE A MANUFACTURERS FIVE YEAR WARRANTY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND FIELD ADJUSTMENT OF THE ABOVE ITEMS.
- D. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH THE OWNER THE FINAL LOCATION OF THE CONTROL PANEL(S). NO ADDITIONAL COSTS SHALL BE ALLOWED FOR ANY ADJUSTMENTS MADE TO THE FINAL LOCATION OF ALL EQUIPMENT.
- E. THE IRRIGATION CONTRACTOR SHALL SUBMIT A WARRANTY POLICY TO THE OWNER WHICH SHALL COVER THE FUNCTION OF THE ENTIRE SYSTEM FOR A PERIOD OF ONE YEAR AFTER THE ACCEPTANCE OF THE SYSTEM BY THE OWNER.
- F. CONTRACTOR WILL VERIFY STATIC PRESSURE AND VOLUME OF SITE WATER SUPPLY AND ADJUST ENTIRE IRRIGATION SYSTEM ACCORDINGLY. EACH ZONE OF IRRIGATION SYSTEM IS TO BE DESIGNED WITH A MINIMUM OPERATING PRESSURE OF 45 PSI. IF THE PRESSURE IS BELOW 45 PSI, A PROPERLY SIZED BOOSTER PUMP WILL BE REQUIRED. AS PART OF THE SHOP DRAWINGS, THE IRRIGATION CONTRACTOR WILL PROVIDE CALCULATIONS SHOWING PRESSURE LOSS FROM THE POINT OF CONNECTION TO THE FURTHEST HEAD (AND FOR THE FURTHEST HEAD ON THE LARGEST ZONE). ADJUST DESIGN TO MEET AVAILABLE PRESSURES AND VOLUMES. A CURRENT STATIC PRESSURE READING AT THE POINT OF CONNECTION WAS NOT AVAILABLE PRIOR TO DESIGN.
- G. THE CONTRACTOR IS TO INSTALL ALL EQUIPMENT SUCH THAT THE BUILDING, PARKING AREAS, AND SIDEWALKS ARE NOT SPRAYED OR SUBJECT TO EXCESSIVE RUNOFF. FIELD ADJUSTMENTS MAY BE NECESSARY TO AVOID UNFORESEEN OBSTACLES AND SIMPLIFY INSTALLATION. IRRIGATION SYSTEM ACCESSORIES SUCH AS QUICK COUPLER VALVES, ISOLATION VALVES, AND MANUAL DRAIN VALVES ARE TO BE LOCATED AS NECESSARY TO COMPLETE THE SYSTEM.
- H. THE IRRIGATION CONTROLLER IS TO BE A HYBRID SOLID STATE TYPE WITH PLASTIC LOCKABLE CABINET. CONTROLLER MUST HAVE DUAL PROGRAMMING FOR TURF SPRAY ZONES AND SHRUB SPRAY ZONES AND BE CAPABLE OF OPERATING MULTIPLE VALVES PER STATION.
- I. PROVIDE DESIGNATED PVC SLEEVES FOR IRRIGATION PIPES AND WIRING THAT CROSSES UNDER WALKS, STREETS AND CONCRETE PADS. COMBINE PIPING WHENEVER POSSIBLE TO REDUCE QUANTITY OF SLEEVING MATERIALS. WHEN INSTALLING IRRIGATION PIPE ALONG CURBS OR IN ISLANDS, PLACE PIPE AS CLOSE TO CURB AS POSSIBLE TO ALLOW FOR PLANTING OF FUTURE TREES AND SHRUBS.

PART 1 GENERAL

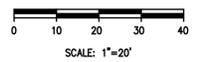
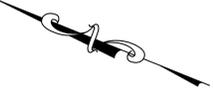
- 1.1 REFERENCES
 - A. ASTM INTERNATIONAL:
 1. ASTM B32 - STANDARD SPECIFICATION FOR SOLDER METAL.
 2. ASTM B42 - STANDARD SPECIFICATION FOR SEAMLESS COPPER PIPE, STANDARD SIZES.
 3. ASTM B88 - STANDARD SPECIFICATION FOR SEAMLESS COPPER WATER TUBE.
 4. ASTM D2235 - STANDARD SPECIFICATION FOR SOLVENT CEMENT FOR ACRYLONITRILE-BUTADIENE-STYRENE (ABS) PLASTIC PIPE AND FITTINGS.
 5. ASTM D2241 - STANDARD SPECIFICATION FOR POLYETHYLENE (PE) PLASTIC PIPE (SIDR-PR) BASED ON CONTROLLED INSIDE DIAMETER.
 6. ASTM D2564 - STANDARD SPECIFICATION FOR SOLVENT CEMENTS FOR POLY (VINYL CHLORIDE) (PVC) PLASTIC PIPING SYSTEMS.
 - B. NATIONAL ELECTRICAL MANUFACTURERS ASSOCIATION:
 1. NEMA 250 - ENCLOSURES FOR ELECTRICAL EQUIPMENT (1000 VOLTS MAXIMUM).
- 1.2 SYSTEM DESCRIPTION
 - C. HYBRID SOLID STATE CONTROLLED UNDERGROUND IRRIGATION SYSTEM, WITH PRESSURE FLOW-OUT DRAIN.
 - D. SOURCE POWER: 120 VOLT.
- 1.3 SUBMITTALS
 - A. SHOP DRAWINGS: INDICATE PIPING LAYOUT TO WATER SOURCE, LOCATION OF SLEEVES UNDER PAVEMENT, LOCATION AND COVERAGE OF SPRINKLER HEADS, COMPONENTS, PLANT AND LANDSCAPING FEATURES, SITE STRUCTURES, SCHEDULE OF OUTLETS AND FITTINGS TO BE USED.
 - B. PRODUCT DATA: SUBMIT COMPONENT AND CONTROL SYSTEM AND WIRING DIAGRAMS.
- 1.4 CLOSEOUT SUBMITTALS
 - A. PROJECT RECORD DOCUMENTS: RECORD ACTUAL LOCATIONS OF CONCEALED COMPONENTS BY NORthing AND EASTING.
 - B. OPERATION AND MAINTENANCE DATA TO OWNER:
 1. SUBMIT INSTRUCTIONS FOR OPERATION AND MAINTENANCE OF SYSTEM AND CONTROLS, SEASONAL ACTIVATION AND SHUTDOWN, AND MANUFACTURER'S PARTS CATALOG.
 2. SUBMIT SCHEDULE INDICATING LENGTH OF TIME EACH VALVE IS REQUIRED TO BE OPEN TO DELIVER DETERMINED AMOUNT OF WATER.
- 1.5 QUALITY ASSURANCE
 - A. PERFORM WORK IN ACCORDANCE WITH MANUFACTURER'S STANDARDS.
- 1.6 COORDINATION
 - A. COORDINATE THE WORK WITH SITE BACKFILLING, PAVING, LANDSCAPE GRADING AND DELIVERY OF PLANT LIFE.

PART 2 PRODUCTS

- 2.1 PIPE MATERIALS
 - A. PVC PIPE: ASTM D2241, SDR 26; 160 PSI SOLVENT WELDED SOCKETS.
 - B. HDPE PIPE: ASTM D-2239, SDR-15, 100 PSI.
 - C. COPPER TUBING: ASTM B88 TYPE K.
 - D. FITTINGS: TYPE AND STYLE OF CONNECTION TO MATCH PIPE.
 - E. SOLVENT CEMENT: [ASTM D2564 FOR PVC PIPE AND FITTINGS] [ASTM D2235 FOR ABS PIPE AND FITTINGS].
 - F. SLEEVE MATERIAL: PVC SCH 40.
- 2.2 OUTLETS
 - A. OUTLETS: BRASS CONSTRUCTION.
 - B. ROTARY TYPE SPRINKLER HEAD: POP-UP TYPE WITH SCREENS; FULLY ADJUSTABLE FOR FLOW AND PRESSURE; WITH LETTER OR SYMBOL DESIGNATING DEGREE OF ARC AND ARROW INDICATING CENTER OF SPRAY PATTERN.
 - C. SPRAY TYPE SPRINKLER HEAD: POP-UP HEAD WITH FULL CIRCLE PATTERN.
 - D. QUICK COUPLER: GALVANIZED.
- 2.3 MANUAL VALVES
 - A. VALVES: HIGHLY CORROSION RESISTANT CONSTRUCTION (BRASS, STAINLESS STEEL, ETC.). ALL VALVES SHALL BE ACCESSIBLE FROM ABOVE THROUGH A VALVE BOX.
 - B. BACKFLOW PREVENTERS: BRONZE BODY CONSTRUCTION, REDUCED PRESSURE TYPE OR AS DESIGNATED BY LOCAL PLUMBING CODE REQUIREMENTS.
 - C. VALVE BOX AND COVER: HDPE RESIN THAT IS RESISTANT TO UV LIGHT, CORROSION, MOISTURE, AND CHEMICALS.
- 2.4 CONTROLS AND CONTROL VALVES
 - A. CONTROLLER: MUST WORK WITH MANUFACTURER FLOW SENSOR, RAIN SENSOR, AND ***** [OR] *****
 - B. CONTROLLER: AUTOMATIC CONTROLLER, MICROPROCESSOR SOLID STATE CONTROL WITH VISIBLE READOUT DISPLAY, TEMPORARY OVERRIDE FEATURE TO BYPASS CYCLE FOR INCLEMENT WEATHER, PROGRAMMABLE FOR 7 DAYS IN QUARTER HOUR INCREMENTS, WITH AUTOMATIC START AND SHUTDOWN.
 - C. CONTROLLER HOUSING: NEMA 250 TYPE 3R; WEATHERPROOF, WATERTIGHT, WITH LOCKABLE ACCESS DOOR.
 - D. VALVES: HYDRAULIC; NORMALLY CLOSED, INCLUDING REQUIRED FITTINGS AND ACCESSORIES.
 - E. WIRE CONDUCTORS: COPPER CONDUCTOR, DIRECT BURIAL TYPE.
 - F. RAIN SENSORS: PER SELECTED MANUFACTURER.
- 2.5 ELECTRICAL CHARACTERISTICS AND COMPONENTS
 - A. ELECTRICAL CHARACTERISTICS:
 1. 120 VOLTS, SINGLE PHASE, 60 HZ.
 - B. DISCONNECT SWITCH: FACTORY MOUNT DISCONNECT SWITCH IN CONTROL PANEL.

PART 3 EXECUTION

- 3.1 EXAMINATION
 - A. VERIFY LOCATION OF EXISTING UTILITIES.
 - B. VERIFY REQUIRED UTILITIES ARE AVAILABLE, IN PROPER LOCATION, AND READY FOR USE.
- 3.2 PREPARATION
 - A. ROUTE PIPING TO AVOID PLANTS, GROUND COVER, AND STRUCTURES.
 - B. LAYOUT AND STAKE LOCATIONS OF SYSTEM COMPONENTS.
 - C. REVIEW LAYOUT REQUIREMENTS WITH OTHER AFFECTED WORK, COORDINATE LOCATIONS OF SLEEVES UNDER PAVING TO ACCOMMODATE SYSTEM.
- 3.3 TRENCHING
 - A. TRENCH SIZE:
 1. MINIMUM COVER OVER INSTALLED SUPPLY PIPING: 18 INCHES.
 2. MINIMUM COVER OVER INSTALLED BRANCH PIPING: 15 INCHES.
 - B. TRENCH TO ACCOMMODATE GRADE CHANGES AND SLOPE TO DRAIN(S).
 - C. MAINTAIN TRENCHES FREE OF DEBRIS, MATERIAL, OR OBSTRUCTIONS DAMAGING TO PIPE.
- 3.4 INSTALLATION
 - A. CONNECT TO UTILITIES.
 - B. SET OUTLETS AND BOX COVERS AT FINISH GRADE ELEVATIONS.
 - C. PROVIDE FOR THERMAL MOVEMENT OF COMPONENTS IN SYSTEM.
 - D. SLOPE PIPING FOR SELF DRAINAGE TO DAYLIGHT.
 - E. USE THREADED NIPPLES FOR RISERS TO EACH OUTLET.
 - F. INSTALL CONTROL WIRING IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDED INSTALLATION PRACTICES. PROVIDE 10 INCH EXPANSION COIL AT EACH CONTROL VALVE, AND AT 100 FT INTERVALS. BURY WIRE BESIDE PIPE. MARK VALVES WITH NEOPRENE VALVE MARKERS CONTAINING LOCKING DEVICE. SET VALVE MARKERS IN VALVE BOXES SET TO FINISH GRADE.
 - G. AFTER PIPING IS INSTALLED, BUT BEFORE OUTLETS ARE INSTALLED AND BACKFILLING COMMENCES, OPEN VALVES AND FLUSH SYSTEM WITH FULL HEAD OF WATER.
- 3.5 BACKFILLING
 - A. BACKFILL WITH COMPACTED BACKFILL IN ACCORDANCE WITH DETAIL A ON SHEET C-7.1.
 - B. INSTALL 3 INCH SAND BEDDING BELOW AND COVER OVER PIPING.
 - C. PROTECT PIPING FROM DISPLACEMENT.
- 3.6 FIELD QUALITY CONTROL
 - A. PRIOR TO BACKFILLING, TEST SYSTEM FOR LEAKAGE FOR WHOLE SYSTEM TO MAINTAIN 100 PSI PRESSURE FOR ONE HOUR.
 - B. SYSTEM IS ACCEPTABLE WHEN NO LEAKAGE OR LOSS OF PRESSURE OCCURS DURING TEST PERIOD.
 - C. PROVIDE ONE COMPLETE SPRING SEASON START-UP AND FALL SEASON SHUTDOWN.
- 3.7 ADJUSTING
 - A. ADJUST CONTROL SYSTEM TO ACHIEVE TIME CYCLES REQUIRED
 - B. ADJUST HEAD TYPES FOR FULL WATER COVERAGE AS DIRECTED BY OWNER'S REPRESENTATIVE.
- 3.8 DEMONSTRATION AND TRAINING
 - A. INSTRUCT OWNER'S PERSONNEL IN OPERATION AND MAINTENANCE OF SYSTEM, INCLUDING ADJUSTING OF SPRINKLER HEADS. USE OPERATION AND MAINTENANCE MATERIAL AS BASIS FOR DEMONSTRATION.

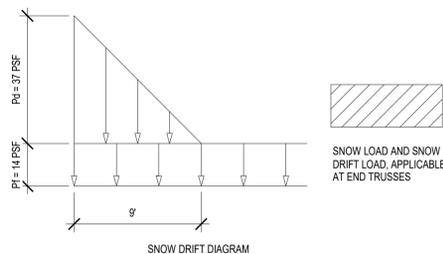


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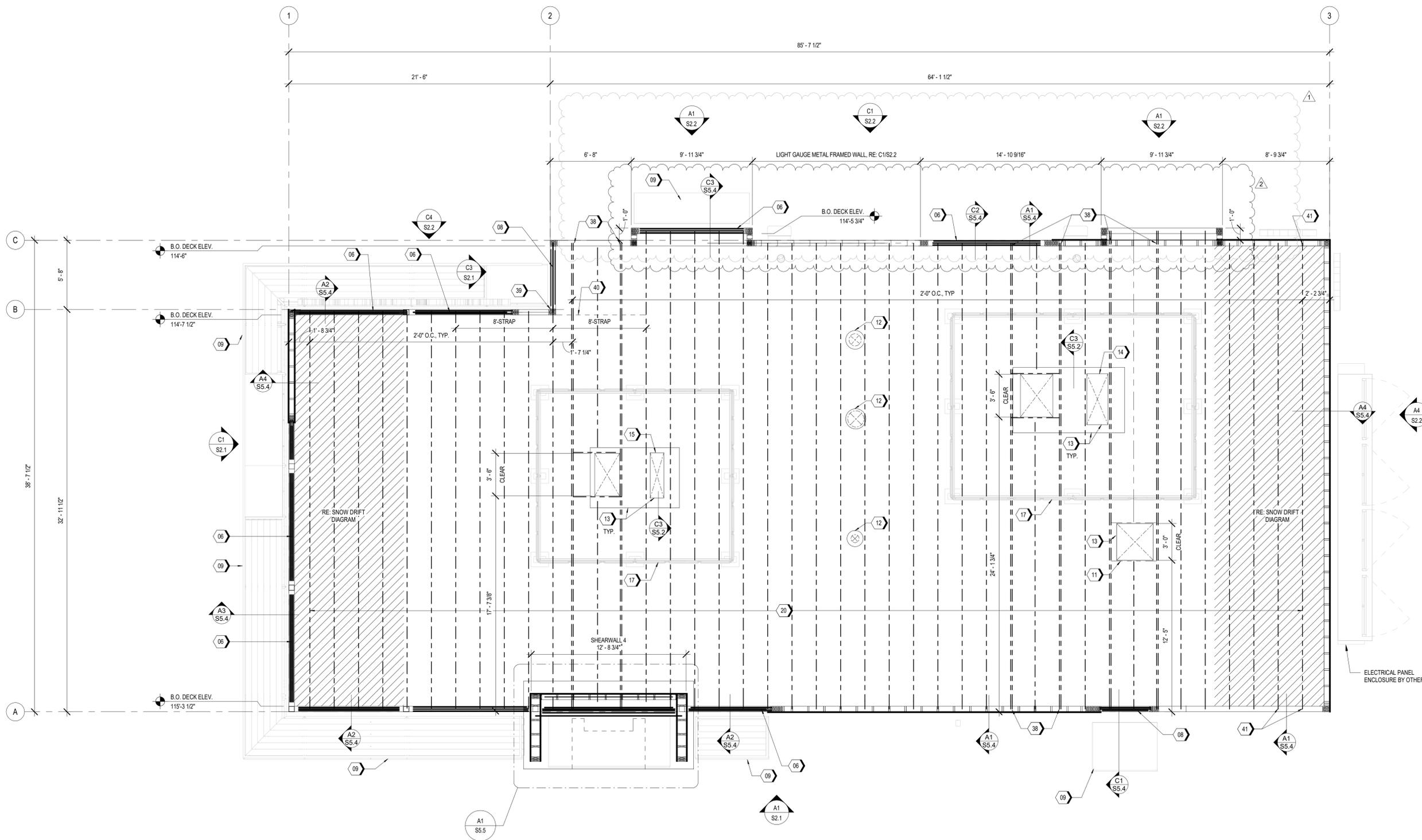
PLAN NOTES

1. REFER TO SHEET S0.2 FOR GENERAL NOTES.
2. COORDINATE LOCATIONS OF ALL WALLS AND WALL OPENINGS WITH ARCHITECTURAL DRAWINGS.
3. REFER TO MECHANICAL DRAWINGS FOR RTU DETAILS. RTU WEIGHTS IN KEY NOTES 14 AND 15.
4. PROVIDE STUD PACK AT ALL BUILDING CORNERS. RE: A2/S4.1 AND A4/S5.2 FOR DETAILS.
5. ALL NAILING SHALL CONFORM TO IBC TABLE 2304.10.1, U.N.O.
6. RE: C1/S5.2 FOR TYPICAL TOP PLATE SPICE DETAIL AT ALL EXTERIOR WALLS.
7. RE: B4 & B5/S5.2 FOR TYPICAL CUTTING, NOTCHING, AND BORING OF WOOD STUDS.
8. PROVIDE 2x SOLID BLOCKING IN WALLS AS REQUIRED FOR REINFORCEMENT OF ALL GRAB BARS, RESTROOM FIXTURES, PLUMBING LINES, WALL BUMPERS, ETC. SEE ARCHITECTURAL BUILDING AND INTERIOR ELEVATIONS FOR EQUIPMENT HEIGHTS AND LOCATIONS. SEE ARCHITECTURAL BUILDING AND WALL SECTIONS FOR LOCATIONS FOR ADDITIONAL BLOCKING REQUIREMENTS.
9. PROVIDE 2x6 SOLID BLOCKING BETWEEN WALL STUDS AT SHEATHING JOINTS.
10. PRE-MANUFACTURED ROOF WOOD TRUSSES TO BE SPACED AT 2'-0" ON CENTER, U.N.O. RE: S5.6 FOR TRUSS DIAGRAMS AND LOADING CRITERIA. TRUSS SUPPLIER SHALL PROVIDE DOUBLE TRUSSES WHERE REQUIRED TO SUPPORT THE PROVIDED LOADING.
11. REFER TO GENERAL NOTES FOR ROOF DECKING AND NAILING PATTERN.



KEYNOTES

- 06 HDR1, RE: C2/S5.2
- 08 HDR2, RE: C2/S5.2
- 09 EXTERIOR CANOPY AND ATTACHMENT TO BUILDING BY OTHERS
- 11 ROOF HATCH, RE: ARCH
- 12 EXHAUST FAN OPENING IN ROOF DECK, RE: MECH. FOR SIZE. SHIFT LOCATION ACCORDINGLY TO AVOID ROOF FRAMING.
- 13 PROVIDE DOUBLE 2x6 BLOCKING BETWEEN TRUSSES AT CURB BEARING LOCATIONS AND AROUND OPENINGS.
- 14 RTU 1, RE: MECH. MAX WEIGHT = 3,000 LBS
- 15 RTU 2, RE: MECH. MAX WEIGHT = 3,000 LBS
- 17 ROOF TOP SCREENWALL AND ATTACHMENT TO CURB BY OTHERS.
- 20 30" DEEP PRE-MANUFACTURED WOOD ROOF TRUSS SPACED AT 24" O.C. U.N.O. RE: TRUSS DIAGRAM ON S5.6 FOR DETAILS.
- 38 A LOAD BEARING DOUBLE STUD SHALL BE PROVIDED AT DOUBLE TRUSS LOCATIONS.
- 39 ATTACH TRUSS TO WALL WITH SIMPSON DSCSL-SDS3 DRAG STRUT CONNECTOR.
- 40 PROVIDE A SIMPSON CS14 CONTINUOUS TIE STRAP x 16'-0" CENTERED ON RE-ENTRANT CORNER. ATTACH WITH (24) 10d NAILS AT END TERMINATION AND INTERMEDIATE 10d NAILS AT EVERY 6" OVER ROOF SHEATHING AND ATTACH THROUGH TO THE 2x BLOCKING. INSTALL PER MANUF. RECOMMENDATIONS. INSTALL BLOCKING BETWEEN TOP CHORD OF TRUSSES FOR LENGTH SHOWN. ATTACH TO ROOF SHEATHING PER B1/S5.2
- 41 A LOAD BEARING STUD SHALL BE PROVIDED AT (2) END TRUSS LOCATIONS. INSTALL INTERMEDIATE STUDS BETWEEN LOAD BEARING STUDS.



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05.02.2023	ISSUE FOR PERMIT REV 2

Seal / Signature

Project Name PT22M
 240 SW M150 HWY, LEE'S SUMMIT, MO 64081

Project Number
 122.0088.470

Description
 ROOF FRAMING PLAN

Scale
 1/4" = 1'-0"

S1.2

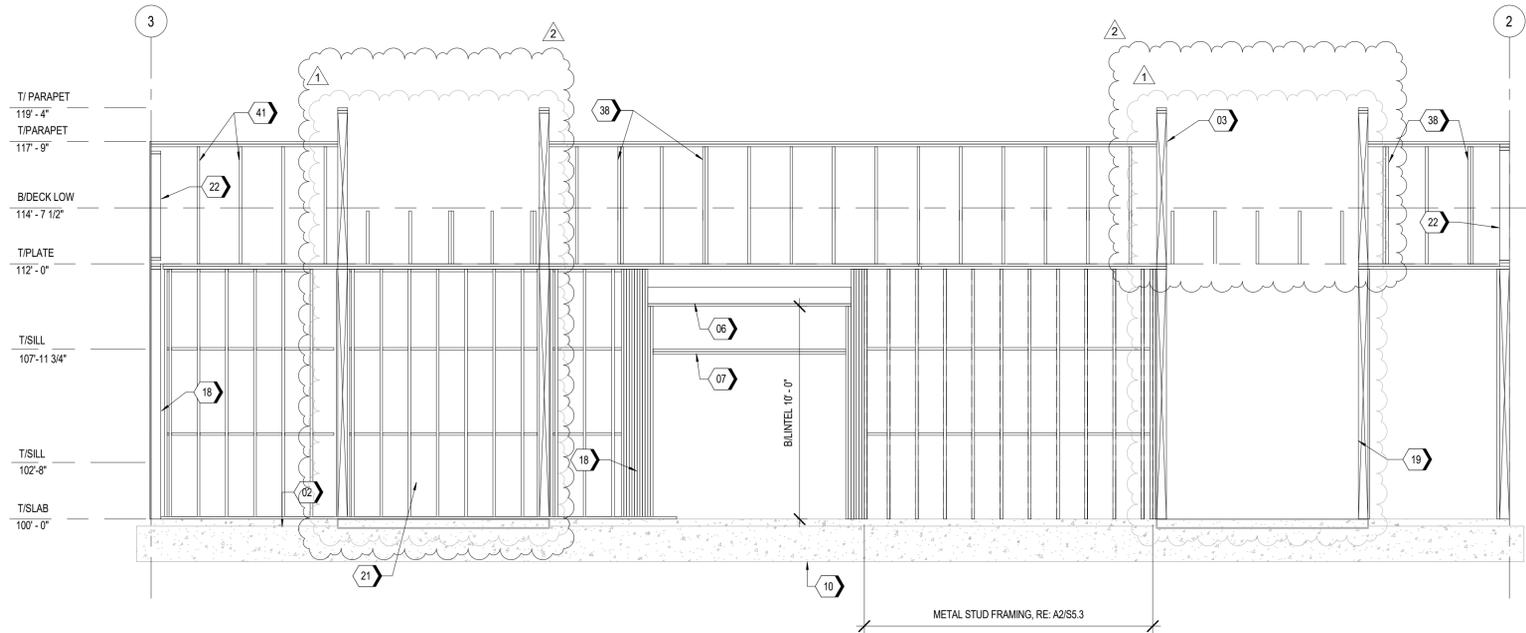
A1 ROOF FRAMING PLAN
 1/4" = 1'-0"



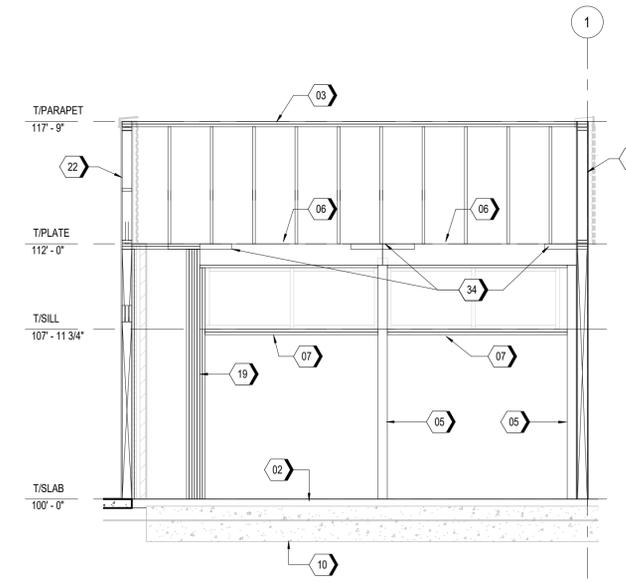
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KEYNOTES

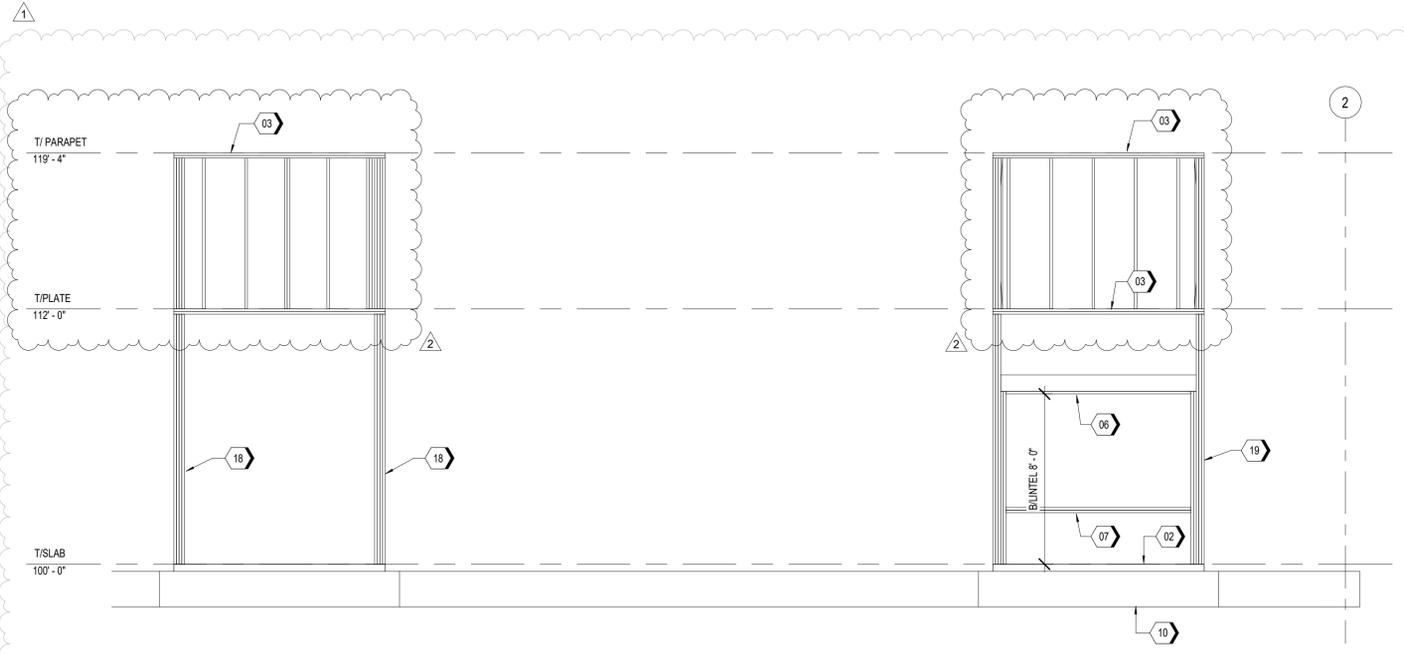
- 02 8" WIDE x4" TALL BRICK LEDGE, TYPICAL AROUND PERIMETER. OMIT AT DOORS.
- 03 DOUBLE 2x6 TOP PLATE.
- 05 PSL 5 1/4x5 1/4 POST
- 06 HDR1, RE: C2/S5.2
- 07 (2) 2x6 SILL, RE: B1/S5.3, ATTACHED TO JAMB EACH SIDE WITH SIMPSON A34 CLIP ANGLE EACH END.
- 10 REFER FOUNDATION PLAN FOR DETAILS
- 18 SHEAR WALL END/CORNER STUD PACK, RE: A2/S4.1 AND A4/S5.2
- 19 HEADER SUPPORT STUDS, RE: C2/S5.2 FOR FRAMING DETAILS.
- 21 WOOD SHEAR WALL, RE: A1/S4.1 SIMPSON HOLDOWN AT EACH END OF SHEAR WALL, RE: A2/S4.1
- 22 2x6 END WALL TRUSS, SHALL BE DESIGNED FOR AN ALLOWABLE SHEAR LOAD OF 400 PLF.
- 23 2x6 BLOCKING INTEGRAL WITH END WALL TRUSS PROVIDED BY TRUSS MANUFACTURER, RE: A3/S5.4 AND C2/S5.6
- 34 SIMPSON STRAP AT DISCONTINUOUS TOP PLATE, RE: D1/S5.3 AND D2/S5.3
- 38 A LOAD BEARING DOUBLE STUD SHALL BE PROVIDED AT DOUBLE TRUSS LOCATIONS.
- 41 A LOAD BEARING STUD SHALL BE PROVIDED AT (2) END TRUSS LOCATIONS. INSTALL INTERMEDIATE STUDS BETWEEN LOAD BEARING STUDS.



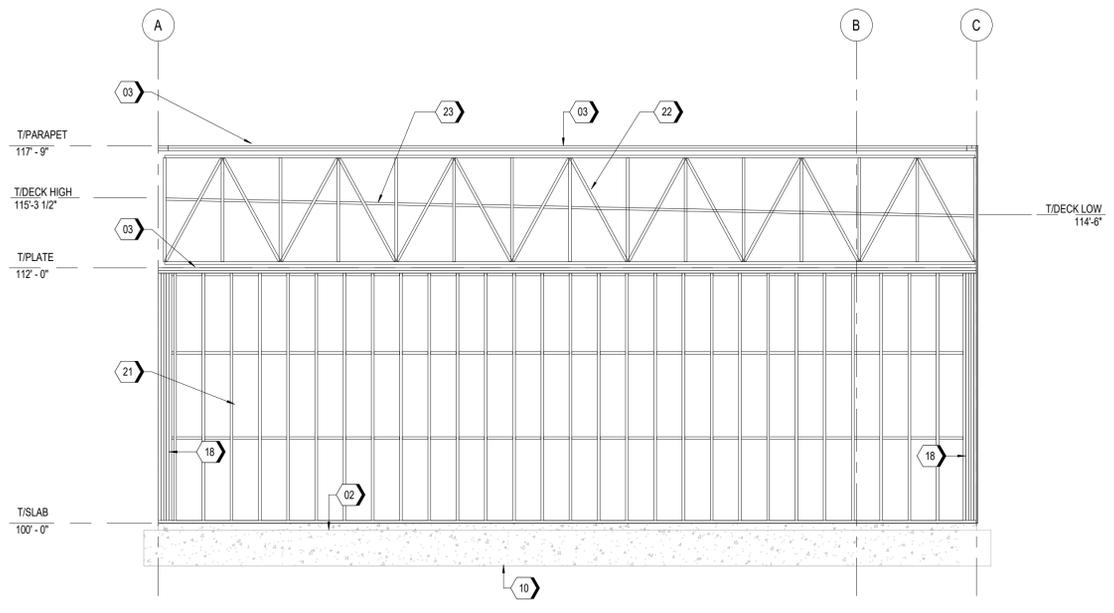
C1 PLAN NORTH FRAMING ELEVATION - DRIVE THRU WALL
1/4" = 1'-0"



C4 PLAN NORTH FRAMING ELEVATION
1/4" = 1'-0"



A1 PLAN NORTH FRAMING ELEVATION - DRIVE THRU BUMP OUTS
1/4" = 1'-0"



A4 PLAN EAST FRAMING ELEVATION
1/4" = 1'-0"

Date	Description
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Project Name PT22M
240 SW M150 HWY, LEE'S SUMMIT, MO 64081
Project Number
122.0088.470
Description
FRAMING ELEVATIONS

Scale
1/4" = 1'-0"

S2.2

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Project Number
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Description
EXTERIOR RENDERINGS

Scale

A2.4





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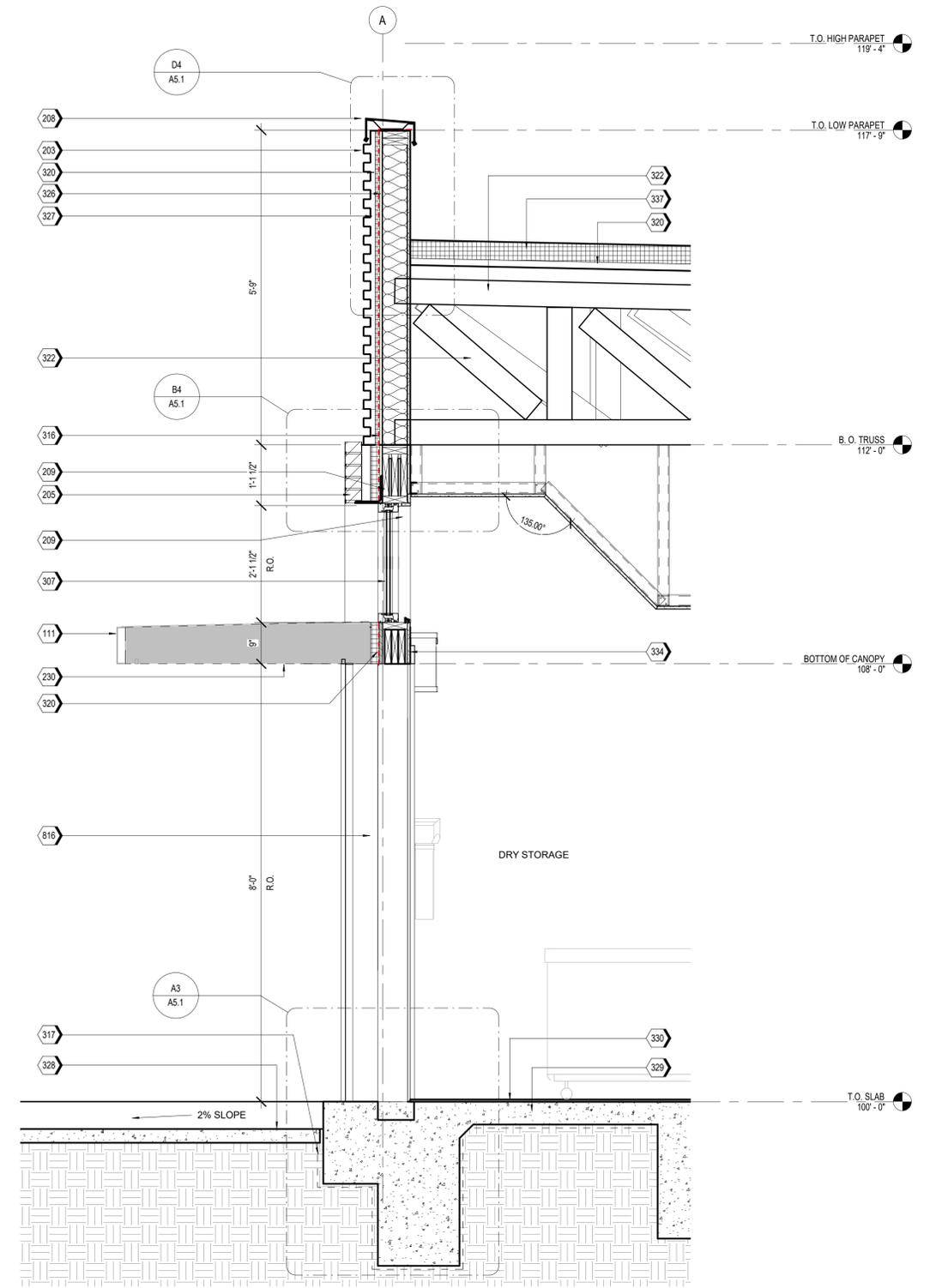
Project Name PT22M
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Project Number
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Description
WALL SECTIONS

Scale
3/4" = 1'-0"

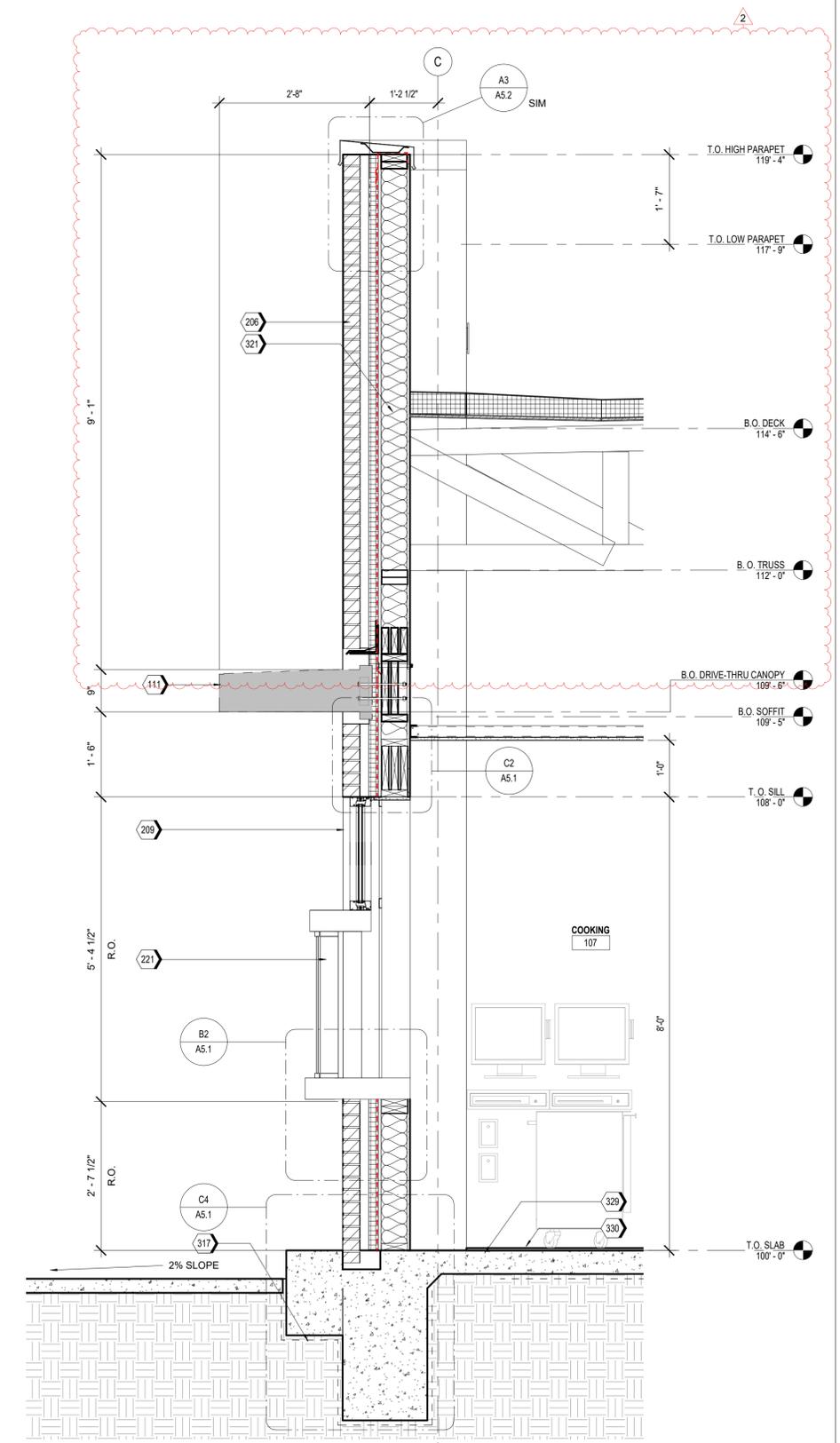
A4.3



A2 WALL SECTION AT DRY STORAGE
SCALE 3/4" = 1'-0"

KEYNOTES	
111	ENTERA PRE-MANUFACTURED CANOPY, BY OTHERS
203	MP-01, PRE-FINISHED CORRUGATED METAL PANEL
205	BR-01, BRICK VENEER
206	BR-02, BRICK VENEER
208	MC-01, PRE-FINISHED METAL COPING
209	SS-01, ALUMINUM STOREFRONT SYSTEM
221	DRIVE-THRU WINDOW
230	LED FLEXIBLE LINEAR LIGHT STRIP
307	WINDOW AS SCHEDULED
316	AIR AND WATER BARRIER
317	VAPOR RETARDER

KEYNOTES	
320	RIGID INSULATION
321	UNFACED BATT INSULATION
322	WOOD TRUSSES, RE: STRUCTURAL
326	PLYWOOD SHEATHING, RE: STRUCTURAL
327	AIR SPACE
328	PAVING, RE: CIVIL
329	RE-INFORCED CONCRETE FOUNDATION, RE: STRUCTURAL
330	FLOORING AS SCHEDULED, RE: 10.1
334	PAINTED GYP. BD., RE: FINISH SCHEDULE 10.1
337	TAPERED RIGID INSULATION
816	DOOR ASSEMBLY AT SECTION CUT



A1 WALL SECTION AT DRIVE-THRU
SCALE 3/4" = 1'-0"