

LEE'S SUMMIT, JACKSON COUNTY, MO



LOCATION MAP

PROJECT CONTACTS

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VICINITY MAP

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PRELIMINARY PLAT PROPERTY DESCRIPTION (LOT 1 AND LOT 2 PLUS PROPOSED RIGHT-OF-WAY)

A tract of land in the Southwest Quarter of Section 30, Township 48 North, Range 31 West, and the Northwest Quarter of Section 31, Township 48 North, Range 31 West, and the Southeast Quarter of Section 25, Township 48 North, Range 32 West, and the Northeast Quarter of Section 36, Township 48 North, Range 32 West of the 5th Principal Meridian in the City of Lee's Summit, Jackson County, Missouri, including all of Lot 12, Lee's Summit North Industrial Park – Seventh Plat, Lots 9, 11 and 12, a subdivision of land in said Lee's Summit, recorded January 28, 2002 as Document 20020006566, in Book 70, at Page 77, all being bounded and described by or under the direct supervision of Jason S Roudsberg, P.L.S., 2002014092 as follows: Beginning at the Southeast corner of said Section 30, Township 48 North, Range 31 West, said line being the line of the Northeast corner of the Northwest Quarter of said Section 31, then North 87°55'25" West, on said South line, on said Southwest Quarter and on the North line of said Northwest Quarter, 196.78 feet to the Point of Beginning of the tract of land to be herein described; thence continuing North 87°55'25" West, on the South line of said Southwest Quarter and on the North line of said Northwest Quarter, 80.00 feet to the Northeast corner of said Lot 12, Minor Plot of Lee's Summit North Industrial Park – Seventh Plat, Lots 9, 11 and 12; thence South 02°05'12" West, on the East line of said Lot 12, 49.53 feet to the Southeast angle corner of said Lot 12; thence Westerly along the Southerly line of said Lot 12, on a curve to the right having an initial tangent bearing of South 89°54'59" West with a radius of 586.50 feet, a central angle of 02°04'26" and an arc distance of 21.23 feet; thence North 88°00'35" West, on said Southerly lot line, 1,781.36 feet to an angle point on said Southerly lot line; thence North 01°59'25" East, on said Southerly lot line, 25.00 feet; thence Southwesterly on said Southerly lot line, on a curve to the left having an initial tangent bearing of North 88°00'35" West with a radius of 712.27 feet, a central angle of 69°07'41" and an arc distance of 859.36 feet; thence South 22°51'44" West, on said Southerly lot line, 126.11 feet to the most Southerly corner of said Lot 12, also being on the Northeasterly right of way line of NW Main Street (old Lee's Summit Road) as now established; thence North 55°52'47" West, on said Northeasterly right of way line, and on the Southwesterly line of said Lot 12, 361.21 feet to the Northwest corner of said Lot 12; thence North 29°24'24" West, on the Northeasterly right of line of said NW Main Street (old Lee's Summit Road), 212.31 feet to a point on the Southeasterly right of way line of Interstate Highway No. 470 as now established; thence North 60°35'40" East, on said Southeasterly right of way line, 1,230.60 feet; thence Northeasterly on said Southeasterly right of way line of Interstate Highway No. 470, on a curve to the right having an initial tangent bearing of North 60°35'40" East with a radius of 1,230.60 feet, a central angle of 07°45'11" and an arc distance of 758.40 feet; thence North 73°48'53" East, on said Southeasterly right of way line, 191.61 feet; thence North 71°13'09" East, on said Southeasterly right of way line, 178.75 feet; thence North 72°08'07" East, on said Southeasterly right of way line, 277.24 feet; thence South 17°51'53" East, 360.57 feet; thence Southeasterly along a curve to the right having an initial tangent bearing of South 89°35'22" East with a radius of 540.00 feet, a central angle of 91°40'34" and an arc distance of 864.03 feet; thence South 02°05'12" West, 100.90 feet to the Point of Beginning, Containing 1,826,821 square feet or 41.938 acres, more or less.

PRELIMINARY DEVELOPMENT PLAN PROPERTY DESCRIPTION (LOT 1 PLUS PROPOSED RIGHT-OF-WAY)

A tract of land in the Southwest Quarter of Section 30, Township 48 North, Range 31 West, and the Northwest Quarter of Section 31, Township 48 North, Range 31 West, and the Southeast Quarter of Section 25, Township 48 North, Range 32 West, and the Northeast Quarter of Section 36, Township 48 North, Range 32 West of the 5th Principal Meridian in the City of Lee's Summit, Jackson County, Missouri, including all of Lot 12, Lee's Summit North Industrial Park – Seventh Plat, Lots 9, 11 & 12, a subdivision of land in said Lee's Summit, recorded January 28, 2002 as Document 20020006566, in Book 70, at Page 77, all being bounded and described by or under the direct supervision of Jason S Roudeshub, P.L.S. 2002014092 as follows: Commencing at the Southeast corner of the Southwest Quarter of said Section 30, said point also being the Northeast corner of the Northwest Quarter of said Section 31; thence North 87°55'25" West, on the South line of said Southwest Quarter and on the North line of said Northwest Quarter, 196.78 feet to the Point of Beginning of the tract of land to be herein described; thence continuing North 87°55'25" West, on the South line of said Southwest Quarter and on the North line of said Northwest Quarter, 80.00 feet to the Northeast corner of said Lot 12, Minor Plat of Lee's Summit North Industrial Park – Seventh Plat, Lots 9, 11 and 12; thence South 02°05'12" West, on the East line of said Lot 12, 49.53 feet to the Southeast angle corner of said Lot 12; thence Westerly along the Southerly line of said Lot 12, on a curve to the right having an initial tangent bearing of South 89°54'59" West with a radius of 586.50 feet, a central angle of 02°04'26" and an arc distance of 21.23 feet; thence North 88°00'35" West, on said Southerly lot line, 1,781.36 feet to an angle point on said Southerly lot line; thence North 01°59'25" East, on said Southerly lot line, 25.00 feet; thence Southwesterly on said Southerly lot line, on a curve to the left having an initial tangent bearing of North 88°00'35" West with a radius of 712.27 feet, a central angle of 05°41'58" and an arc distance of 70.85 feet; thence North 03°42'33" West, 104.58 feet; thence North 29°24'29" West, 200.72 feet; thence South 60°35'40" West, 544.92 feet; thence South 54°52'55" West, 401.98 feet; thence South 21°06'00" West, 137.26 feet to the Northwest corner of said Lot 12, said point also being on the Northeasterly right of way line of NW Main Street (old Lee's Summit Road) as now established; thence North 29°24'24" West, on said Northeasterly right of line of said NW Main Street (old Lee's Summit Road), 212.31 feet to a point on the Southeasterly right of way line of Interstate Highway No. 470 as now established; thence North 60°35'40" East, on said Southeasterly right of way line, 1,230.60 feet; thence Northeasterly on said Southeasterly right of way line of Interstate Highway No. 470 as established by the Report of Commissioners, Case 741042, recorded as Document No. 19710086010, in Book 1258 at Page 1202, on a curve to the right being tangent to the last described course with a radius of 5,604.58 feet, a central angle of 07°45'11" and an arc distance of 758.40 feet; thence North 73°48'53" East, on said Southeasterly right of way line, 191.61 feet; thence North 71°13'09" East, on said Southeasterly right of way line, 178.75 feet; thence North 72°08'07" East, on said Southeasterly right of way line, 277.24 feet; thence South 17°51'53" East, 360.57 feet; thence Southeasterly along a curve to the right having an initial tangent bearing of South 89°35'22" East with a radius of 540.00 feet, a central angle of 91°40'34" and an arc distance of 864.03 feet; thence South 02°05'12" West, 100.90 feet to the Point of Beginning. Containing 1,549,869 square feet or 35.580 acres, more or less.



THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF THE SENATE BILL NUMBER 583, 78TH GENERAL ASSEMBLY OF THE STATE OF MISSOURI. THE BILL REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT-OF-WAY DO SO ONLY AFTER GIVING NOTICE TO, & OBTAINING INFORMATION FROM, UTILITY COMPANIES. STATE LAW REQUIRES 48 HOURS ADVANCE NOTICE. CALL 1-800-DIG-RITE.

oson

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COVER SHEET

SUMMIT 470 LOGISTICS CENTER
PRELIMINARY DEVELOPMENT PLAN & PRELIMINARY PLAT

LEE'S SUMMIT: MO

2022

REVISIONS

drawn by: _____ CAD
checked by: _____ ENG
approved by: _____ ENG
QA/QC by: _____ ENG
project no.: _____ 022-03974
drawing no.: C TTL01 02203974
date: _____ 02/23/23

SHEET
C1.0

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GENERAL NOTES:

1. ALL PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
2. REFER TO DETAIL SHEET FOR INSTALLATION OF SIGNS.
3. CONTRACTOR SHALL MATCH EXISTING PAVEMENT IN GRADE AND ALIGNMENT TO PROVIDE SMOOTH SURFACE TRANSITIONS BETWEEN NEW ENTRANCE DRIVES AND EXISTING STREETS.
4. CONTRACTOR SHALL MATCH EXISTING CURB & GUTTER IN GRADE, SIZE, TYPE, AND ALIGNMENT AT CONNECTIONS TO EXISTING STREETS.
5. ALL WORK ON THIS PLAN SHALL BE DONE IN STRICT ACCORDANCE WITH THE OWNER'S SITE WORK SPECIFICATIONS.
6. ALL TRAFFIC CONTROL SIGNS SHALL BE FABRICATED AS SHOWN IN THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREET AND HIGHWAYS.
7. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, SLOPED PAVING, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS, SIDEWALK AND SPECIFIC BUILDING AREA TREATMENTS AND IMPROVEMENTS. FOR EXACT BUILDING DIMENSIONS, SEE ARCHITECTURAL PLANS. CONTRACTOR TO STAKE AND CONSTRUCT FOUNDATIONS AND FOOTINGS FROM STRUCTURAL PLAN. BUILDING DIMENSIONS ON THIS PLAN ARE FOR REFERENCE ONLY.
8. ALL DIMENSIONS SHOWN ON BUILDING ARE TO OUTSIDE FACE OF BUILDING.
9. CONTRACTOR SHALL COORDINATE PROTECTION OF BUILDING CORNERS, TRANSFORMERS, AND ALL OTHER APPLICABLE STRUCTURES WITH GUARD POST BOLLARDS WITHIN 5' OF THE BUILDINGS TO BE INSTALLED BY GENERAL CONTRACTOR.
10. PARKING LOT STRIPING SHALL BE INCLUDED IN PAVING CONTRACTOR'S SCOPE OF WORK. ALL STRIPING IS TO BE TWO LAYERS, 4" STROKE, REFLECTIVE PAINT, INCLUDING ADA SYMBOL AND HATCHING. PAINT COLOR TO BE WHITE ON ASPHALT AND YELLOW ON CONCRETE.
11. ALL ACCESSIBLE PARKING SIGNAGE AND STRIPING SHALL BE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS.
12. THE CONTRACTOR SHALL SUPPLY THE OWNER WITH A LIST OF ALL SUBCONTRACTORS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION.
13. ALL ASPHALT PAVING SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF LEES SUMMIT DESIGN AND CONSTRUCTION MANUAL SECTION 2200.
14. THE GENERAL CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR, AND SHALL TAKE ALL PRECAUTIONS NECESSARY TO, AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR IS RESPONSIBLE FOR REPAIRS OF DAMAGE TO ANY EXISTING IMPROVEMENTS DURING CONSTRUCTION, SUCH AS, BUT NOT LIMITED TO: DRAINAGE UTILITIES, PAVEMENT, STRIPING, CURB, ETC. ANY WORK IN CITY R.O.W. REPAIRS SHALL BE EQUAL TO OR BETTER THAN EXISTING CONDITIONS. CONTRACTOR IS RESPONSIBLE FOR PROTECTION OF ALL PROPERTY CORNERS AND SURVEY MONUMENTS AND IS RESPONSIBLE FOR RE-ESTABLISHMENT OF ANY PROPERTY CORNERS OR SURVEY MONUMENTS IF DISTURBED BY CONSTRUCTION ACTIVITIES.
15. SAFETY NOTICE TO CONTRACTOR: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. ANY CONSTRUCTION OBSERVATION BY THE ENGINEER OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.
16. ALL CONSTRUCTION IN STATE HIGHWAY DEPARTMENT RIGHT-OF-WAY SHALL BE COORDINATED WITH THE HIGHWAY DEPARTMENT RESIDENT MAINTENANCE ENGINEER PRIOR TO START OF CONSTRUCTION. LATEST SPECIFICATIONS ADOPTED BY US DEPARTMENT OF TRANSPORTATION AND STATE HIGHWAY DEPARTMENT SHALL GOVERN ON THIS PROJECT.
17. ALL SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE SPECIFICATIONS OF THE RELEVANT UTILITY COMPANY OR REGULATORY AUTHORITY, AND THE SPECIFICATIONS FOR THE CONSTRUCTION OF THE EXISTING IMPROVEMENTS WHICH ARE BEING ALTERED OR REPLACED. CONTRACTOR SHALL CONTACT THE ENGINEER FOR SPECIFICATION SECTIONS FOR ITEMS SUCH AS LANDSCAPING AND IRRIGATION THAT ARE AFFECTED BY THE WORK BUT NOT COMPLETELY DETAILED OR SPECIFIED ON THESE PLANS.
18. ALL CONSTRUCTION WITHIN THE RIGHT-OF-WAY SHALL CONFORM TO THE CITY OF LEE'S SUMMIT, MISSOURI STANDARDS AND SPECIFICATIONS.
19. ALL CURB RETURN RADII ARE 4.0' UNLESS OTHERWISE NOTED.
20. SITE TOPOGRAPHY TAKEN FROM FIELD WORK BY OLSSON ASSOCIATES ON THE SURVEY DATED 10-2016. CONTRACTOR TO VERIFY EXISTING CONDITIONS OF THE SITE THAT MAY NOT BE REPRESENTATIVE OF THE CONSTRUCTION PLANS.

WETLANDS NOTICE:

1. ANY DEVELOPMENT, EXCAVATION, CONSTRUCTION, OR FILLING IN A U.S. CORPS OF ENGINEERS DESIGNATED WETLAND IS SUBJECT TO LOCAL, STATE AND FEDERAL APPROVALS. THE CONTRACTOR SHALL COMPLY WITH ALL PERMIT REQUIREMENTS AND/OR RESTRICTIONS AND ANY VIOLATION WILL BE SUBJECT TO FEDERAL PENALTY. THE CONTRACTOR SHALL HOLD THE OWNER/DEVELOPER, THE ENGINEER AND THE LOCAL GOVERNING AGENCIES HARMLESS AGAINST SUCH VIOLATION.

WARRANTY/DISCLAIMER:

1. THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER THE ENGINEER NOR ITS PERSONNEL CAN OR DO WARRANT THESE DESIGNS OR PLANS AS CONSTRUCTED EXCEPT IN THE SPECIFIC CASES WHERE THE ENGINEER INSPECTS AND CONTROLS THE PHYSICAL CONSTRUCTION ON A TEMPORARY BASIS AT THE SITE.

FLOOD CERTIFICATION:

1. AREA OF 1% CHANCE ANNUAL FLOOD ZONE (AE) AS DEPICTED ON THE FEMA FLOOD INSURANCE RATE MAP (FIRM) COMMUNITY PANEL NO 29095C0409G (JACKSON COUNTY, MO), REVISED JANUARY 20TH, 2017 SHOWN ON PLAN. ALL UNSHADED (ZONE X) OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

OIL/GAS WELLS:

NO OIL OR GAS WELLS LOCATED WITHIN THE PROJECT LIMITS.

INFORMATION OBTAINED FROM THE MISSOURI DEPARTMENT OF NATURAL RESOURCES, GEOLOGICAL SURVEY GEOSCIENCES TECHNICAL RESOURCE ASSESSMENT TOOL (GEOSTRAT).

DEMOLITION NOTES:

1. CONTRACTOR SHALL BE RESPONSIBLE FOR RAISING AND REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED.
2. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
3. ALL DEMOLITION WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE OWNER'S SITE WORK SPECIFICATIONS.
4. CONTRACTOR IS RESPONSIBLE FOR REPAIRS OF DAMAGE AND ADJUSTMENTS DUE TO CONFLICTS OR GRADING TO ANY EXISTING STRUCTURES OR UNDERGROUND UTILITIES THAT ARE TO REMAIN IN PLACE.
5. ALL ITEMS DESIGNATED TO BE DEMOLISHED AND REMOVED FROM THE SITE SHALL BE DISPOSED OF IN AN APPROPRIATE LOCATION IN ACCORDANCE WITH STATE OR LOCAL GUIDELINES.
6. PUBLIC STREETS AND SIDEWALKS SHALL BE KEPT CLEAN AND CLEAR OF TRASH AND DEBRIS FROM DEMOLITION OPERATIONS AT ALL TIMES.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST AND EROSION CONTROL DURING DEMOLITION OPERATIONS.
8. THE CONTRACTOR SHALL COORDINATE WITH ALL APPLICABLE UTILITY COMPANIES PRIOR TO REMOVAL OR RELOCATION OF ANY UTILITIES AND TO SAFELY STOP SERVICES AND DISMANTLE SERVICE LINES PRIOR TO BEGINNING DEMOLITION OPERATIONS.
9. CONTRACTOR IS TO REMOVE AND RE-USE SEWER PIPES, POWER POLES AND GUY WIRES, WATER LINES AND METERS, VEGETATION, ASPHALT, AND OTHER UNSUITABLE DEBRIS OR MATERIAL. SHOWN OR NOT SHOWN WITHIN CONSTRUCTION LIMITS AND WHERE NECESSARY TO ALLOW FOR CONSTRUCTION ACTIVITY. ALL MATERIAL TO BE REMOVED AS UNCLASSIFIED EXCAVATION.
10. ALL CAVITIES CREATED BY REMOVAL OF EXISTING FACILITIES IN THE AREA OF PROPOSED CONSTRUCTION SHALL BE FILLED AND COMPACTED IN ACCORDANCE WITH THE SITE WORK SPECIFICATIONS TO SUBGRADE ELEVATION.
11. THE CONTRACTOR SHALL EXERIOSE EXTREME CAUTION WHEN WORKING IN THE VICINITY OF EXISTING OVERHEAD ELECTRICAL POWER LINES.
12. EXISTING UTILITIES ARE SHOWN AS LOCATED AND IDENTIFIED IN THE FIELD BY UTILITY COMPANY REPRESENTATIVE. THE OWNER AND THE ENGINEER MAKE NO ASSURANCE OF THE ACTUAL LOCATION, DEPTH, SIZE OR TYPE OF UTILITY LINES SHOWN. THE OWNER AND THE ENGINEER MAKES NO ASSURANCE THAT ALL OF THE EXISTING UTILITY LINES ON THE SITE ARE SHOWN.

GRADING AND CLEARING NOTES:

1. EXISTING UTILITIES AS SHOWN ARE APPROXIMATE LOCATIONS ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATIONS OF ALL EXISTING UTILITIES PRIOR TO THE START OF ANY CONSTRUCTION WORK. ANY DAMAGE TO EXISTING STRUCTURES, UTILITIES, FENCES AND/OR INCIDENTALS NOT DESIGNATED FOR REMOVAL SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.
2. CONTRACTOR SHALL ADHERE TO THE "DESIGN AND CONSTRUCTION MANUAL" SECTION 2100 AS ADOPTED BY THE CITY OF LEES SUMMIT, MISSOURI (LATEST EDITION), FOR EXCAVATION AND EMBANKMENT WORK WITHIN THE PROPOSED DRIVE LANES.
3. CONTRACTOR SHALL PROVIDE A LEVEL BUILDING PAD BASED UPON PROPOSED FINISHED FLOOR ELEVATION TO ± 0.10' OR AS ESTABLISHED THROUGH ALTERNATIVE BID DOCUMENTS.
4. PRIOR TO FINAL ACCEPTANCE OF THE PROJECT, ALL SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH A MINIMUM OF FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEEDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
5. AREAS OF CONSTRUCTION SHALL BE STRIPPED OF ALL VEGETATION, ORGANIC MATTER AND TOPSOIL TO A DEPTH AS RECOMMENDED BY GEOTECHNICAL ENGINEER AND/ OR TESTING AGENCY. SOILS REMOVED DURING SITE STRIPPING SHOULD BE EVALUATED TO DETERMINE IF PORTIONS OF THE TOPSOIL STRATUM MAY BE UTILIZED AS STRUCTURAL FILL WITHIN PAVEMENT AREAS. ANY MATERIAL NOT DEEMED AS SUITABLE FILL MATERIAL BY THE GEOTECHNICAL ENGINEER AND/ OR TESTING AGENCY SHALL BE REMOVED FROM THE JOB SITE BY THE CONTRACTOR AT HIS EXPENSE.
6. CONTRACTOR SHALL ADHERE TO THE SITE PREPARATION AND STRUCTURAL FILL RECOMMENDATIONS AS CALLED OUT IN THE GEOTECHNICAL REPORT AND ENGINEERING EVALUATION AS PROVIDED BY THE GEOTECHNICAL ENGINEER.
7. ALL EMBANKMENT SHOULD BE PLACED IN CONTROLLED LIFTS HAVING A MAXIMUM LOOSE LIFT THICKNESS OF 9". EMBANKMENT PLACED WITHIN THE PAVEMENT AREAS SHOULD BE COMPACTED TO A MINIMUM OF 95% OF THE MATERIALS MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D-698 (STANDARD PROCTOR COMPACTION). EMBANKMENT PLACED WITHIN THE BUILDING AREAS SHOULD BE COMPACTED TO A MINIMUM OF 95% OF THE MATERIALS MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D-698 (STANDARD PROCTOR COMPACTION). MOISTURE CONTENT OF THE FILL AT THE TIME OF COMPACTION SHALL BE WITHIN A RANGE OF 0 TO 4 PERCENT ABOVE OPTIMUM MOISTURE CONTENT AS DEFINED BY THE STANDARD PROCTOR COMPACTION PROCEDURE. ALL EMBANKMENT PLACED WITHIN 18" OF THE BUILDING SUBGRADE SHOULD HAVE A LIQUID LIMIT LESS THAN 60. THE GEOTECHNICAL REPORT SHALL SUPERSEDE RECOMMENDATIONS AS STATED IN THIS PLAN SET.

UTILITY CONSTRUCTION NOTES:

1. PRIOR TO INSTALLATION OF ANY PROPOSED UTILITY THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL CROSSINGS WITH EXISTING UTILITIES AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS. THE ENGINEER WILL BE HELD HARMLESS IN THE EVENT THE ENGINEER IS NOT NOTIFIED OF CONFLICTS WITH EXISTING UTILITIES.
2. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT IS THE CONTRACTORS RESPONSIBILITY TO RELOCATE AND/OR ADJUST ALL EXISTING UTILITIES THAT CONFLICT WITH PROPOSED SITE IMPROVEMENTS.
3. UNLESS OTHERWISE SHOWN, CALLED OUT OR SPECIFIED HEREON OR WITHIN THE SPECIFICATIONS: ALL STORM DRAIN PIPE BEDDING SHALL BE INSTALLED PER CITY STANDARD DETAILS. ALL STORM DRAIN PIPES ARE MEASURED FROM CENTER OF STRUCTURES AND ENDS OF FLARED END SECTIONS.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTROL DOWNSTREAM EROSION AND SILTATION DURING ALL PHASES OF CONSTRUCTION. EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO CONSTRUCTION.
5. TELEPHONE CONDUIT SHALL HAVE A MINIMUM COVER OF 30". CONDUIT SHALL BE DUAL 4" SCHEDULE 40 PVC. CONTRACTOR SHALL COORDINATE LOCATION WITH THE UTILITY REPRESENTATIVE AND LOCATE PVC CROSSINGS AS NECESSARY. SEE ELEC. PLANS FOR ENTRANCE LOCATIONS.
6. FOR ALL SERVICE LINE ENTRANCE LOCATIONS WITHIN THE BUILDING, INCLUDING ROOF DRAIN CONNECTIONS, SEE ARCHITECTURAL PLANS AND DETAILS.
7. ALL WATER SERVICE LINES SHALL BE A MINIMUM OF 48" BELOW FINISHED GRADE.
8. ALL SANITARY SEWER LINES SHALL BE SDR-26 WITH 42" MIN. COVER.

9. CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS A MINIMUM OF 48 HOURS PRIOR TO DISRUPTION.
10. ALL ELECTRIC AND TELEPHONE, INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTION'S SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
11. PRIOR TO ORDERING PRECAST STRUCTURES, SHOP DRAWINGS SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR APPROVAL.
12. ALL PRIVATE INSTALLATIONS SHALL CONFORM TO THE CURRENT STANDARDS AND SPECIFICATIONS AS ADOPTED BY THE CITY OF LEE'S SUMMITT, MISSOURI.
13. EXTENSION OF BOTH DOMESTIC WATER SERVICE AND FIRE PROTECTION LINE MAY NOT BE PROVIDED UNTIL PUBLIC MAIN HAS BEEN TESTED AND ACCEPTED BY WRITTEN AUTHORIZATION FROM LEE'S SUMMIT WATER DEPARTMENT.
14. CONTRACTOR TO CONTACT LEE'S SUMMIT WATER SERVICES DEPARTMENT FOR MAIN LINE TAP AND METER SET A MINIMUM OF 48 HOURS PRIOR TO CONNECTION.
17. CONSTRUCTION SHALL NOT START ON ANY PUBLIC UTILITY SYSTEM UNTIL THE APPROPRIATE PERMITS HAVE BEEN PULLED FROM THE CITY OF LEE'S SUMMIT AND/OR JACKSON COUNTY AND CONTRACTOR HAS BEEN NOTIFIED BY THE ENGINEER.

18. ALL ELECTRICAL CONDUIT SHALL BE SCHEDULE 40 ELECTRICAL PVC, AS CALLED OUT AND HAVE AN AVERAGE OF 36" TO 42" COVER WITH A MINIMUM OF 30" CONFORMING TO THE CURRENT REGULATIONS SET FORTH BY MISSOURI PUBLIC SERVICE. SEE MECH. PLANS FOR ENTRANCE LOCATIONS.

19. CONTRACTOR SHALL MAKE APPLICATION WITH SPIRE ENERGY FOR PROPOSED METER.

20. CONTRACTOR SHALL MAKE APPLICATION WITH EVERGY FOR PROPOSED ELECTRICAL POWER.

GENERAL		SURVEY MARKERS	
	ACU		BENCHMARK
	AST		CONTROL POINT
	ATL		FOUND MONUMENT
	ATR		ROW MARKER
	BLB		SECTION CORNER
	BOV		SET MONUMENT
	BSH	BOUNDARIES	
	COL		SECTION LINE
	CTR		EXISTING PROPERTY BOUNDARY
	DRN		PROPOSED PROPERTY BOUNDARY
	DTR		EXISTING LOT LINE
	FLP		PROPOSED LOT LINE
	GDP		EXISTING RIGHT-OF-WAY
	GPL		PROPOSED RIGHT-OF-WAY
	GTP	UTILITIES	
	GUY WIRE		CABLE BOX
	HCP		CABLE VAULT
	LST		TELEVISION PEDESTAL
	MLB		TELEVISION RISER
	MP		EXISTING CABLE TV, OVERHEAD
	MWL		EXISTING CABLE TV, UNDERGROUND
	PIV		PROPOSED CABLE TV, OVERHEAD
	PPT		PROPOSED CABLE TV, UNDERGROUND
	RAT		FIBER OPTIC BOX
	SAD		FIBER OPTIC MANHOLE
	SCV		FIBER OPTIC PEDESTAL
	SGN		FIBER OPTIC VAULT
	SLB		EXISTING FIBER OPTIC, OVERHEAD
	SLC		EXISTING FIBER OPTIC, UNDERGROUND
	SPB		PROPOSED FIBER OPTIC, OVERHEAD
	SPH		PROPOSED FIBER OPTIC, UNDERGROUND
	STP		FIRE DEPT. CONNECTION
	SVL		EXISTING FIRE PROTECTION SYSTEM LINE
	TCB		PROPOSED FIRE PROTECTION SYSTEM LINE
	TSA		EXISTING FUEL LINE
	TSC		PROPOSED FUEL LINE
	TSMH		GAS RISER
	TSP		GAS MANHOLE
	EXISTING TREELINE		GAS MARKER
	PROPOSED TREELINE		GAS METER
	EXISTING SIDEWALK		GAS REGULATOR
	PROPOSED SIDEWALK		GAS VALVE
	FUTURE SIDEWALK		EXISTING NATURAL GAS LINE
	EXISTING BUILDINGS		PROPOSED NATURAL GAS LINE
	PROPOSED BUILDINGS		TELEPHONE CABINET
	FUTURE BUILDINGS		TELEPHONE PEDESTAL
	EXISTING EDGE OF PAVEMENT		TELEPHONE RISER
	PROPOSED EDGE OF PAVEMENT		TELEPHONE VAULT
	FUTURE EDGE OF PAVEMENT		TELEPHONE MANHOLE
	EXISTING ROADWAY CENTER LINE		EXISTING TELEPHONE LINE, OVERHEAD
	PROPOSED ROADWAY CENTER LINE		EXISTING TELEPHONE LINE, UNDERGROUND
	FUTURE ROADWAY CENTER LINE		PROPOSED TELEPHONE LINE, OVERHEAD
	EXISTING CURB & GUTTER		PROPOSED TELEPHONE LINE, UNDERGROUND
	PROPOSED CURB & GUTTER		GROUND LIGHT
	FUTURE CURB & GUTTER		LIGHT POLE
	RADIUS		POWER POLE
	ARC DISTANCE		ELECTRIC TRANSFORMER
	DELTA / CENTRAL ANGLE		ELECTRIC BOX
EASEMENTS & SETBACKS			ELECTRIC CABINET
	A.E. ACCESS EASEMENT		ELECTRIC RISER
	B.M.P. BEST MANAGEMENT PRACTICE EASEMENT		ELECTRIC MANHOLE
	B.L. BUILDING SETBACK		ELECTRIC METER
	CABLE TV EASEMENT		ELECTRIC SECTIONALIZER
	CONSERVATION EASEMENT		ELECTRIC VAULT
	CONSTRUCTION GRADING EASEMENT		YARD LIGHT
	FLOOD PLAIN EASEMENT		EXISTING POWER/ELECTRIC LINE, OVERHEAD
	FIBER OPTIC EASEMENT		EXISTING POWER/ELECTRIC LINE, UNDERGROUND
	FIRE PROTECTION SYSTEM EASEMENT		SEWER CLEANOUT
	FUEL LINE EASEMENT		SANITARY MANHOLE
	LANDSCAPE EASEMENT		EXISTING SANITARY SEWER
	NATURAL GAS EASEMENT		PROPOSED SANITARY SEWER
	TELEPHONE EASEMENT		FUTURE SANITARY SEWER
	POWER/ELECTRIC EASEMENT		EXISTING STEAM LINE
	PARKING SETBACK		PROPOSED STEAM LINE
	STREAM BUFFER		STORM SEWER MANHOLE
	SURFACE DRAINAGE EASEMENT		FLARED END SECTION
	SIGHT DISTANCE EASEMENT		ROOF DRAIN
	SANITARY SEWER EASEMENT		EXISTING STORM SEWER
	STEAM LINE EASEMENT		PROPOSED STORM SEWER
	STORM DRAINAGE EASEMENT		
	STORM WATER MANAGEMENT EASEMENT		FIRE HYDRANT
	TEMPORARY CUL-DE-SAC EASEMENT		WATER MANHOLE
	TEMPORARY EASEMENT		WATER MARKER
	TRAIL/PATH EASEMENT		WATER METER
	UTILITY EASEMENT		WATER VALVE
	WATER EASEMENT		EXISTING WATER LINE
	FRONT YARD SETBACK		PROPOSED WATER LINE
	REAR YARD SETBACK		
	SIDE YARD SETBACK		
CONTOURS			
	EXISTING INDEX CONTOURS		
	EXISTING INTERMEDIATE CONTOURS		
	PROPOSED INDEX CONTOURS		
	PROPOSED INTERMEDIATE CONTOURS		

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REVISIONS DESCRIPTION

DATE

REV. NO.

2022

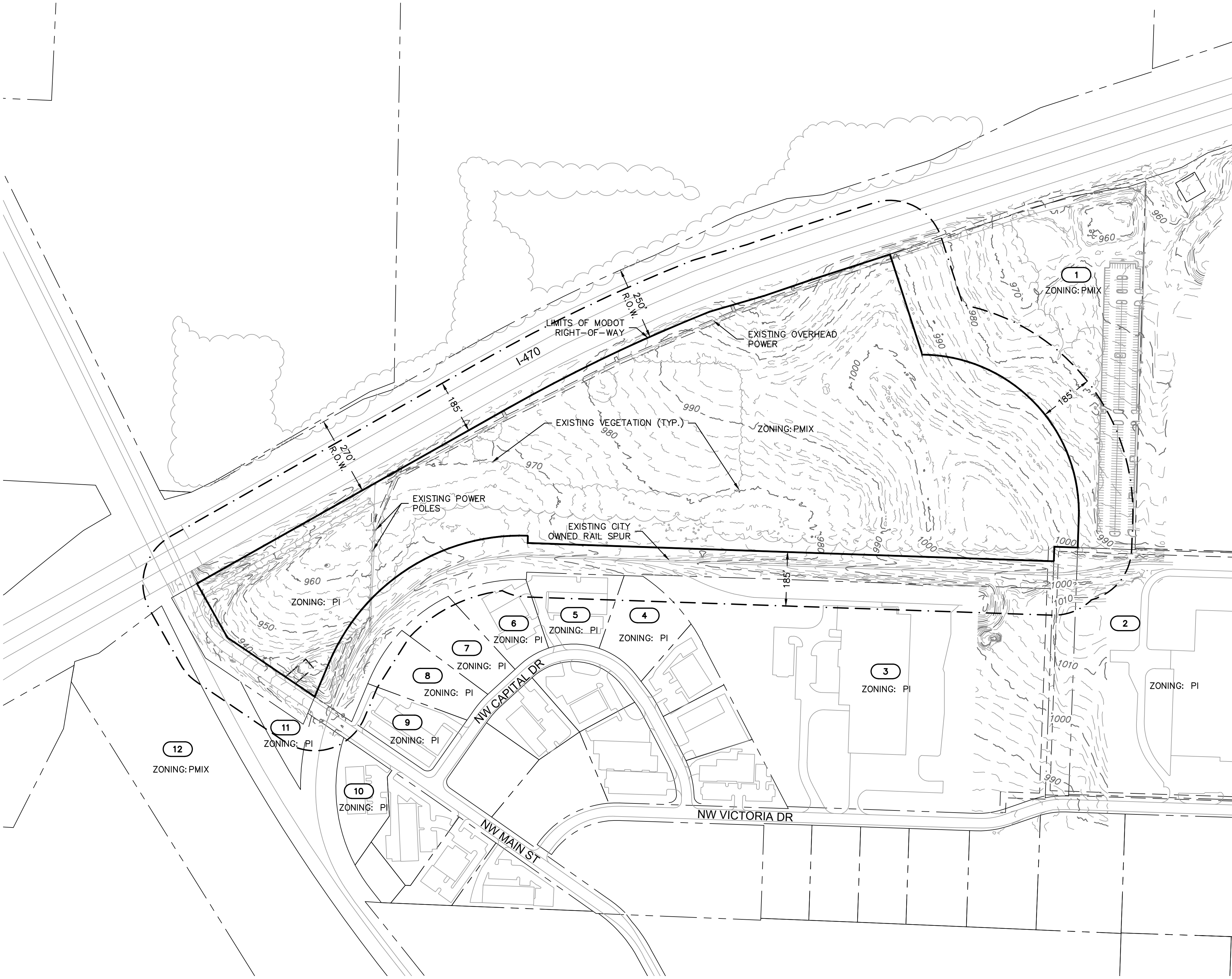
GENERAL NOTES

SUMMIT 470 LOGISTICS CENTER
PRELIMINARY DEVELOPMENT PLAN & PRELIMINARY PLAT

LEE'S SUMMIT, MO

drawn by: CAD
checked by: ENG
approved by: ENG
QA/QC by: ENG
project no.: 022-03974
drawing no.: C_JTL01_02203974
date: 02/23/23

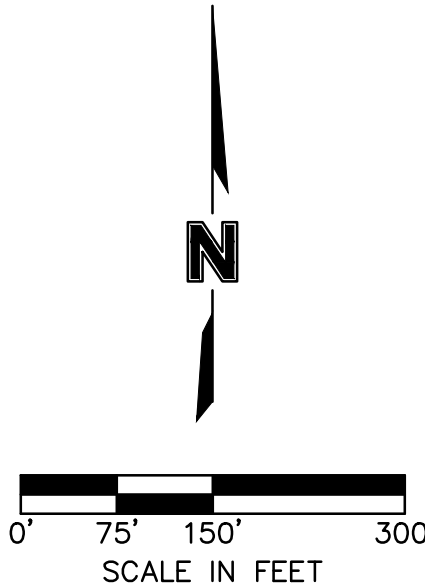
SHEET
C1.1



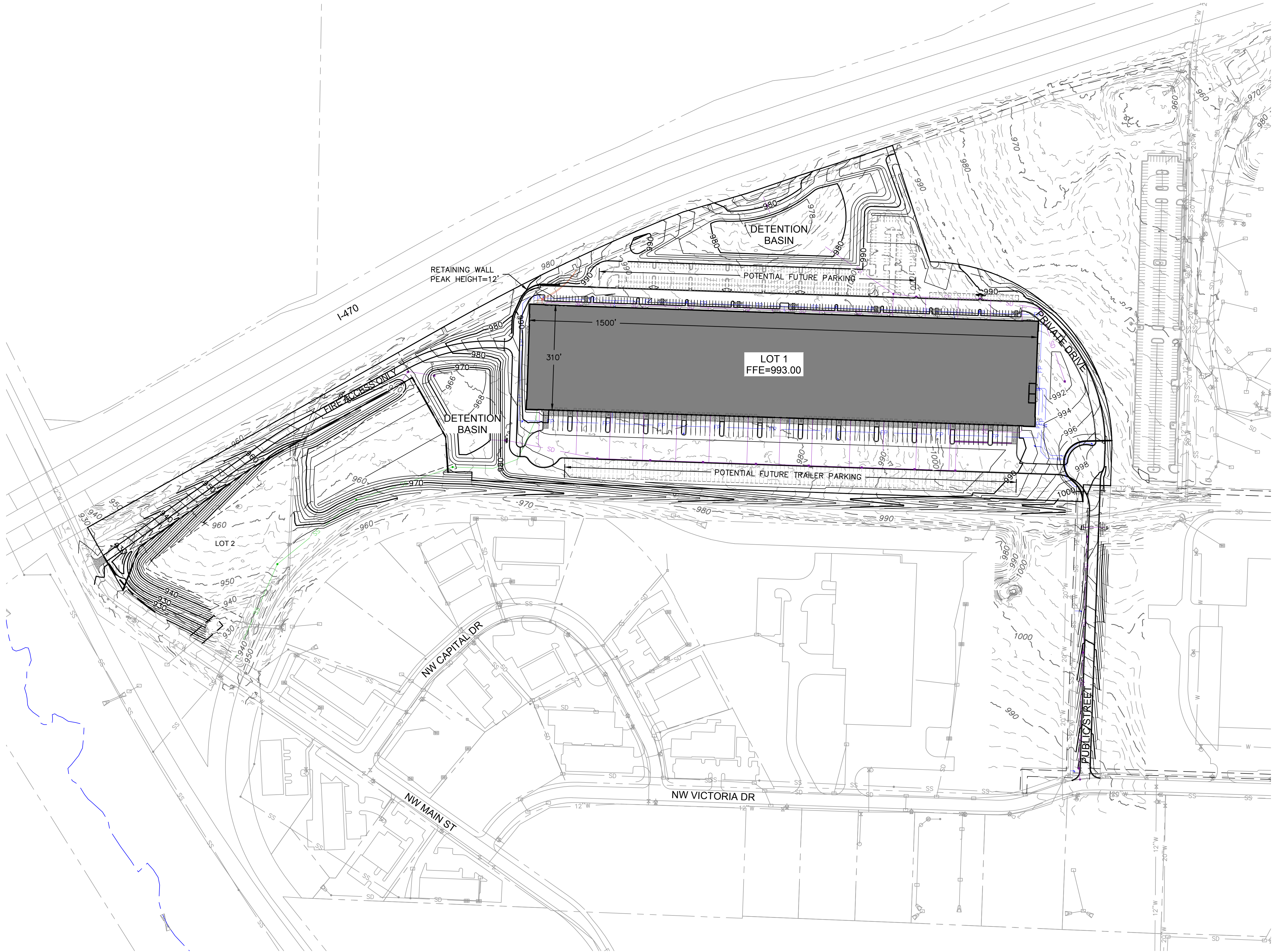
PROPERTY OWNERS WITHIN 185'		
KEY	ADDRESS	OWNER(S) & MAILING ADDRESS
1	** NO ADDRESS **	SAINT LUKES EAST HOSPITAL 901 E 104TH MAILSTOP 900N KANSAS CITY, MO 64131
2	1400 NE DOUGLAS ST LEES SUMMIT, MO 64086	1400 NORTH DOUGLAS CORP 1400 NE DOUGLAS ST LEES SUMMIT, MO 64086-4608
3	200 NW VICTORIA DR LEES SUMMIT, MO 64086	A ZEREGAS SONS INC 760 S 11TH ST PHILADELPHIA, PA 19147
4	330 NW CAPITAL DR LEES SUMMIT, MO 64086	TEMP-STOP ACQUISITIONS LLC 330 NW CAPITAL DR LEES SUMMIT, MO 64086
5	340 NW CAPITAL DR LEES SUMMIT, MO 64086	MEYER & HAYES LLC 340 NW CAPITAL DR LEES SUMMIT, MO 64086
6	400 NW CAPITAL DR LEES SUMMIT, MO 64086	MEYER & HAYES LLC 340 NW CAPITAL DR LEES SUMMIT, MO 64086
7	420 NW CAPITAL DR LEES SUMMIT, MO 64086	TKO PROPERTY GROUP LLC 420 NW CAPITAL DR LEES SUMMIT, MO 64086
8	428 NW CAPITAL DR LEES SUMMIT, MO 64086	VICTORIA PARK LLC PO BOX 213 LEES SUMMIT, MO 64086
9	1331 NW MAIN ST UNIT A LEES SUMMIT, MO 64086	COWLEY PROPERTIES LLC 5832 FOXFIRE LN LOHMAN, MO 65053
10	1324 NW MAIN ST LEES SUMMIT, MO 64086	COKER RANDALL S & KAREN M TRUST 13 Z LAKE SHORE DR LAKE LOTAWANA, MO 64086
11	** NO ADDRESS **	P&M INVESTMENTS LLC 820 NW COMMERCE DR LEES SUMMIT, MO 64086
12	** NO ADDRESS **	TOWNSEND SUMMIT LLC 230 SCHILLING CIR STE 120 HUNT VALLEY, MD 21031

NOTES:

1. EXISTING ZONING: PMIX/PI PROPOSED ZONING: PI
2. EXISTING LAND USE: UNDEVELOPED
3. NO OIL OR GAS WELLS ARE LOCATED ON THE PROPERTY. INFORMATION VERIFIED VIA MISSOURI DNR: <https://dnr.mo.gov/geology/geosrv/oilandgas.htm>

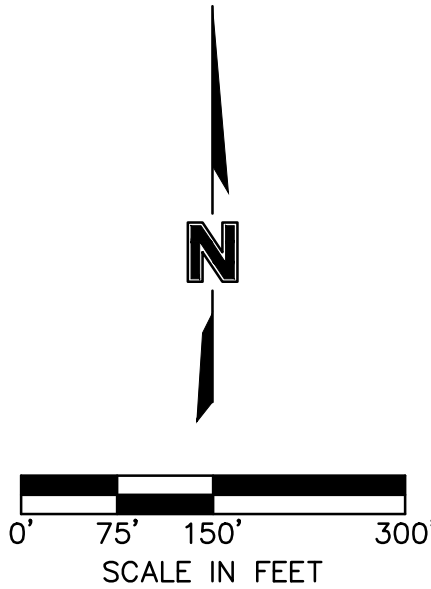
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USER: amuhammad



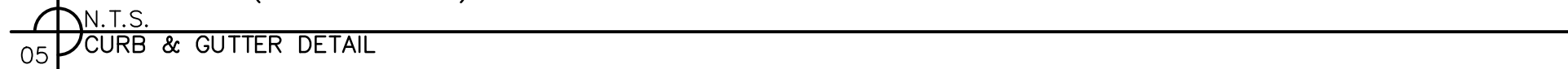
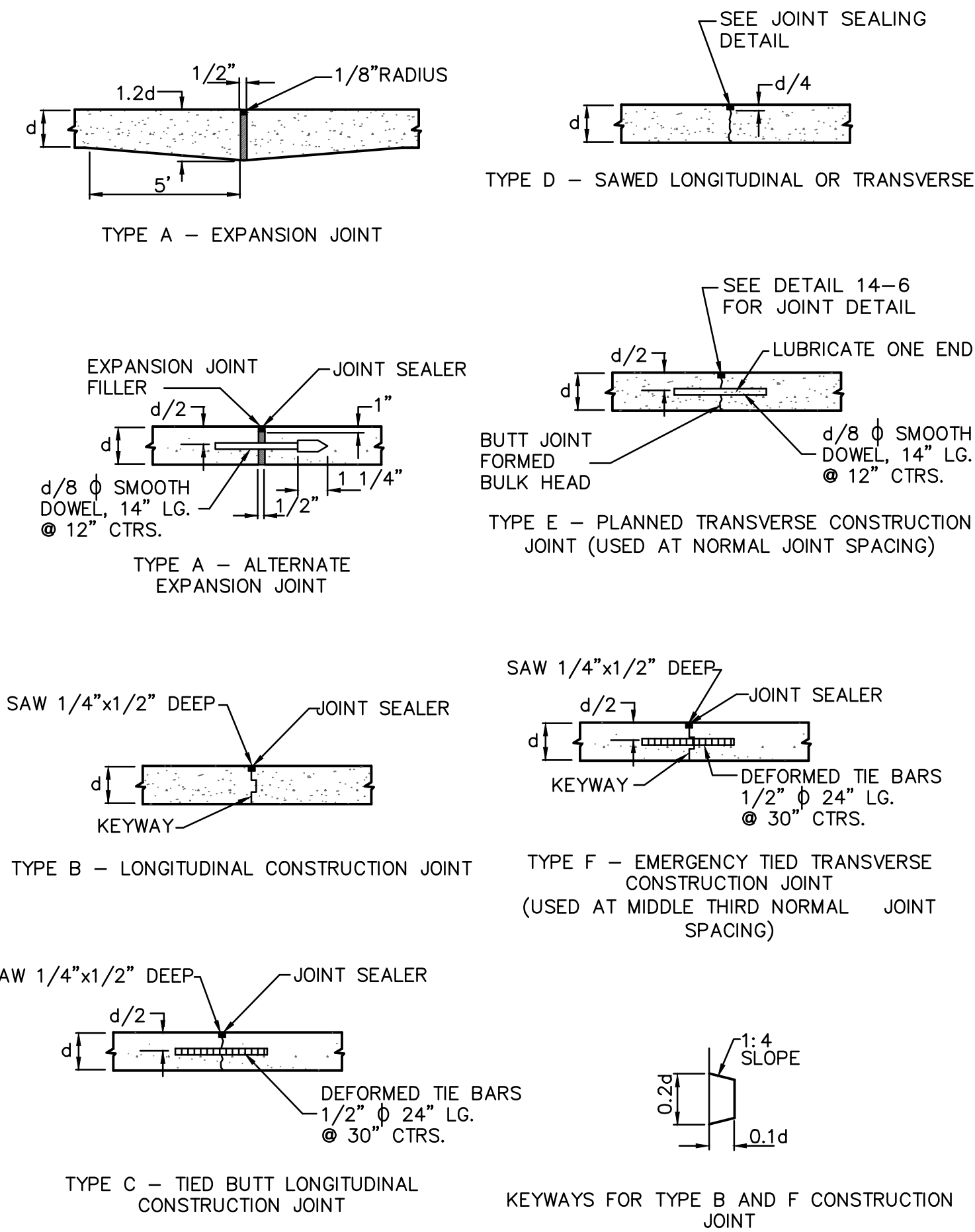
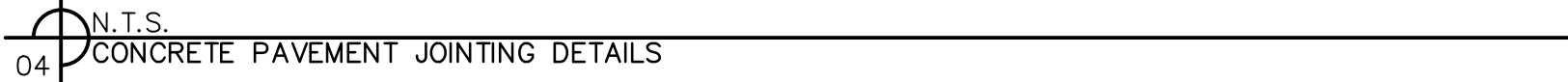
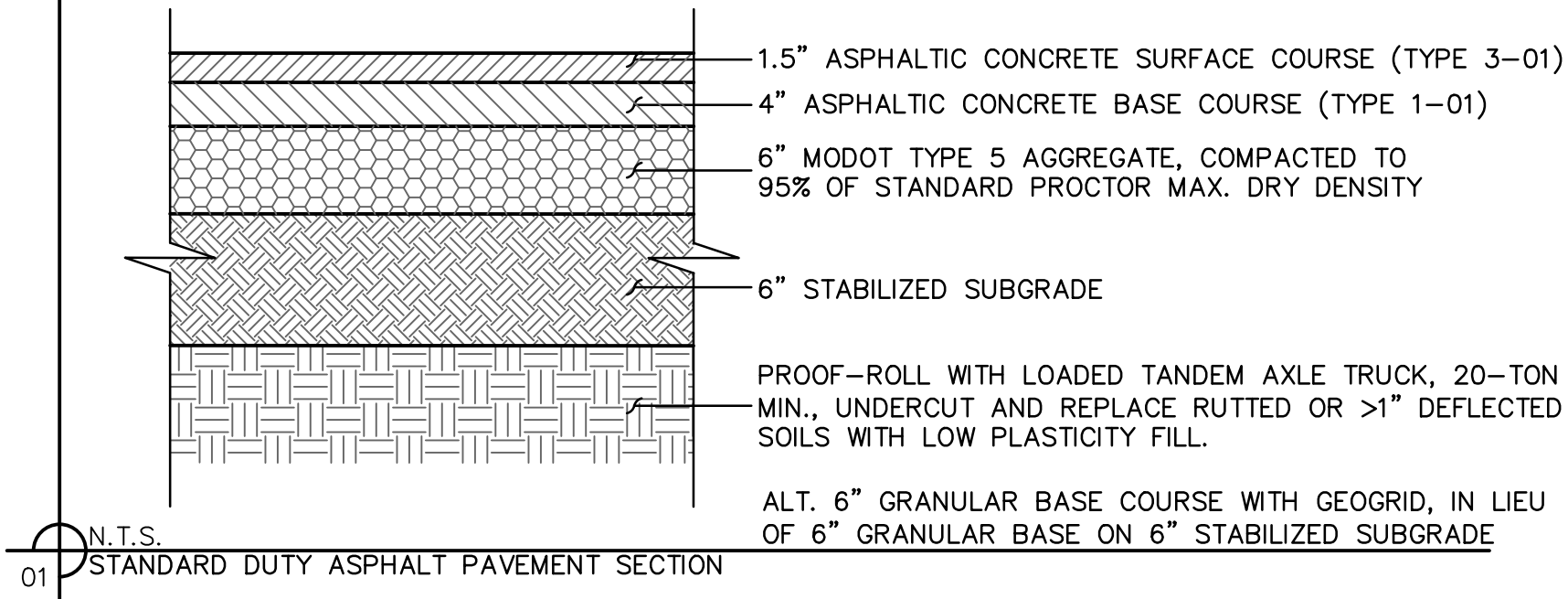
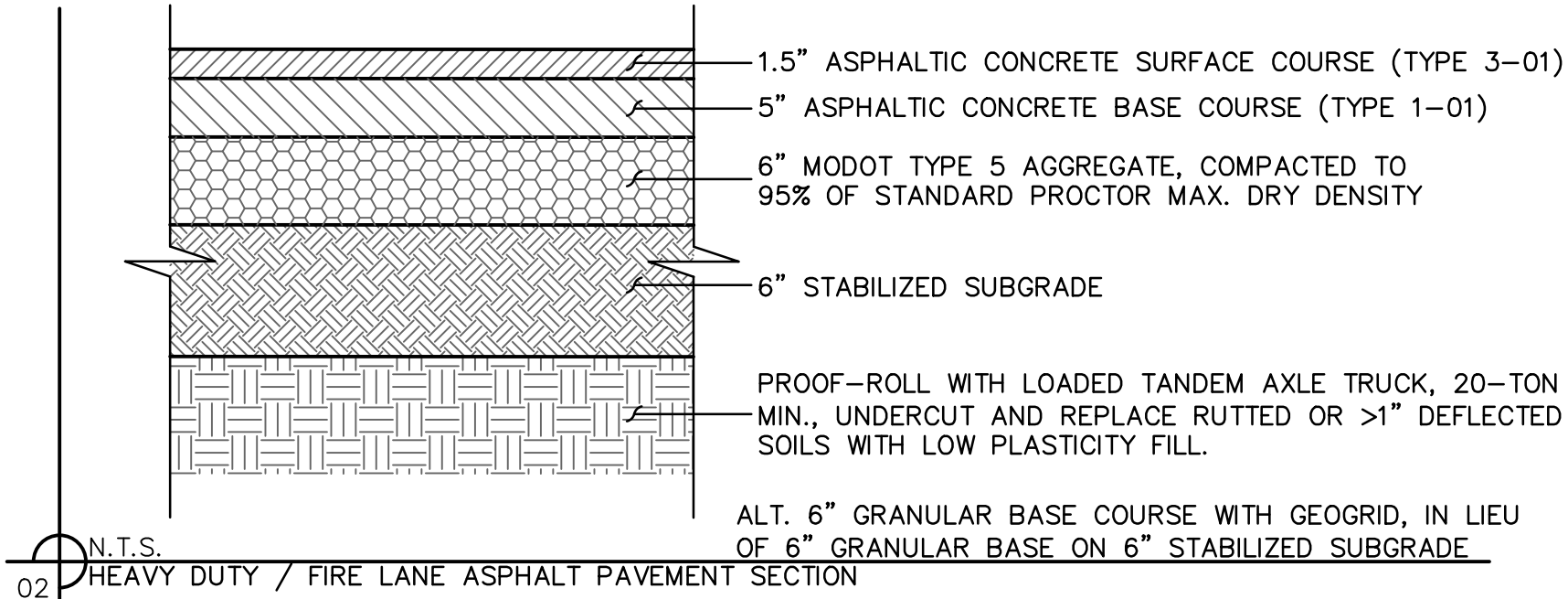
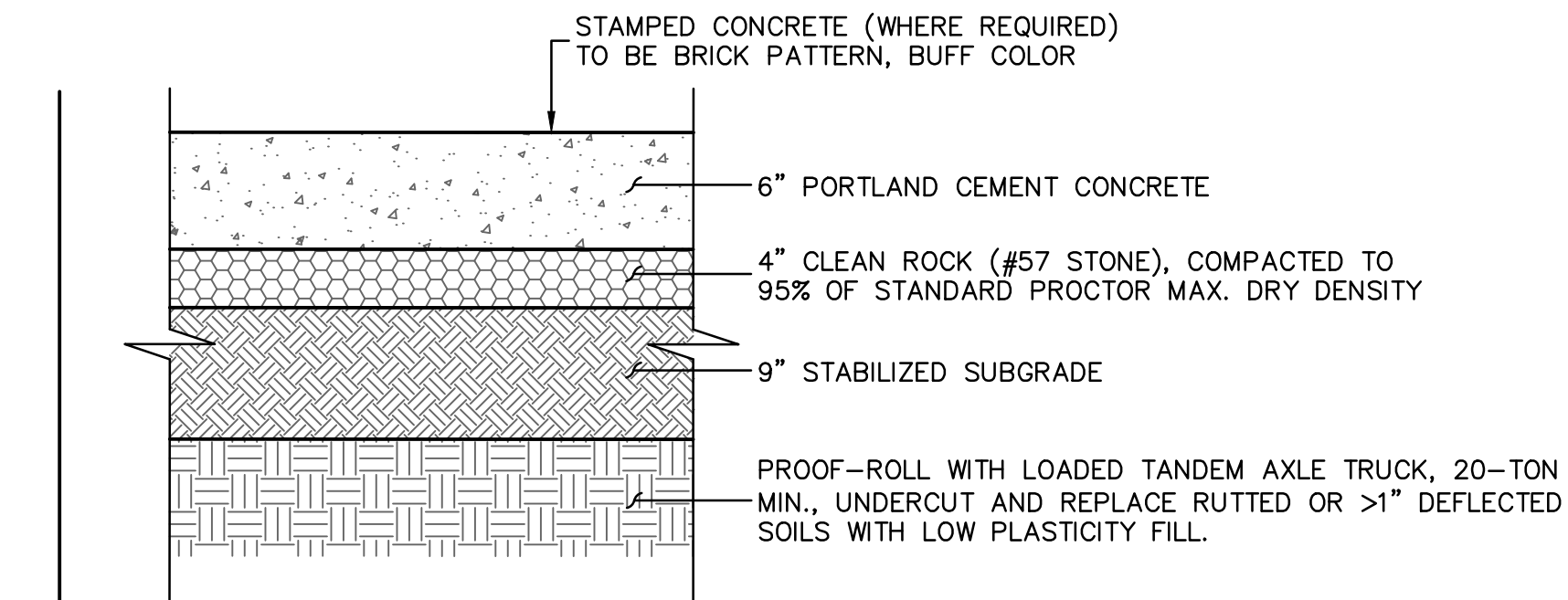
GENERAL LEGEND	
	PROPERTY BOUNDARIES
	EASEMENTS AND SETBACKS
	EXISTING OVERHEAD POWER
	EXISTING WATER MAIN (OTHER SIZES)
	EXISTING SANITARY SEWER
	PROPOSED UNDERGROUND POWER
	PROPOSED COMMUNICATIONS
	PROPOSED STORM SEWER
	PROPOSED 2" WATER MAIN
	PROPOSED 6" WATER MAIN
	PROPOSED 8" PVC SANITARY SEWER

GRADING LEGEND	
	EXISTING INDEX CONTOURS
	EXISTING INTERMEDIATE CONTOURS
	PROPOSED INDEX CONTOURS
	PROPOSED INTERMEDIATE CONTOURS



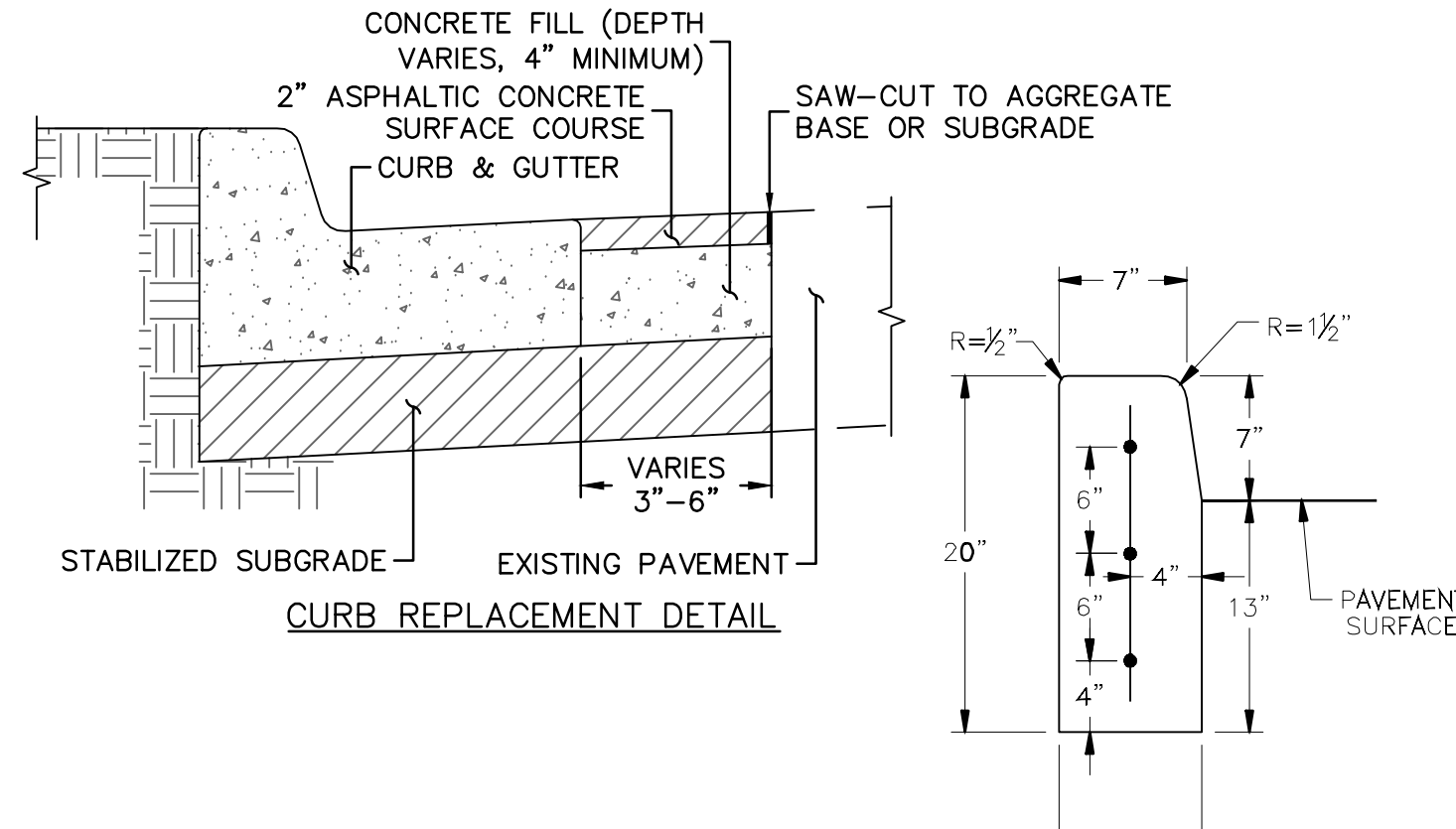
GRADING PLAN		REVISIONS	
SUMMIT 470 LOGISTICS CENTER PRELIMINARY DEVELOPMENT PLAN & PRELIMINARY PLAT	REV. NO.	DATE	REVISIONS DESCRIPTION
LEE'S SUMMIT, MO		2022	
SHEET C5.0			

drawn by: CAD
checked by: ENG
approved by: ENG
QA/QC by: ENG
project no.: 022-03974
drawing no.: C_GRD01_02203974
date: 02/23/23



GENERAL NOTES:

- 3/4" ISOLATION JOINTS WITH 3 (2'-#5 BAR) SMOOTH DOWELS SHALL BE PLACED AT RADIUS POINTS AND AT 150' INTERVALS. THESE DOWEL BARS SHALL BE GREASED AND WRAPPED ON ONE END WITH EXPANSION TUBES.
- 3" DEEP CONTRACTION JOINTS SHALL BE INSTALLED AT APPROXIMATELY 10' INTERVALS. THESE JOINTS SHALL PASS ACROSS THE ENTIRE CURB SECTION.
- CONCRETE FILL SHALL HAVE UNIFORM AND SMOOTH FINISH.
- KOMBB 4K CONCRETE SHALL BE USED FOR ALL CURBS.
- ASPHALTIC CONCRETE SURFACE SHALL CONFORM TO STANDARD SPECIFICATIONS SECTION 2205.2.
- CURBS FOR NEW STREETS SHALL BE BUILT ON ASPHALT OR AGGREGATE BASE AS SHOWN IN TYPICAL SECTION DETAIL. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.



STRAIGHT CURB
(TYPE C-1)

ROLL BACK CURB &
GUTTER
(TYPE CG-2)

DOWELLED CURB
(TYPE DC)

STRAIGHT BACK CURB &
GUTTER
(TYPE CG-1)

STRAIGHT BACK DRY CURB &
GUTTER
(TYPE CG-1 DRY)

ROLL BACK DRY CURB &
GUTTER
(TYPE CG-2 DRY)

CURB & GUTTER DETAIL AT RAMP
(ADA SLOPE REQUIREMENTS)

NOTES

- DETAILS FOR REFERENCE ONLY. NOT TO BE USED FOR CONSTRUCTION.

DETAILS

SUMMIT 470 LOGISTICS CENTER
PRELIMINARY DEVELOPMENT PLAN & PRELIMINARY PLAT

LEE'S SUMMIT, MO

2022

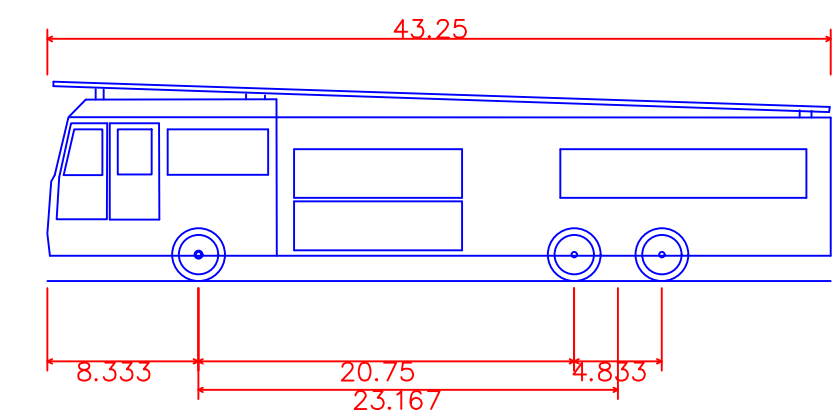
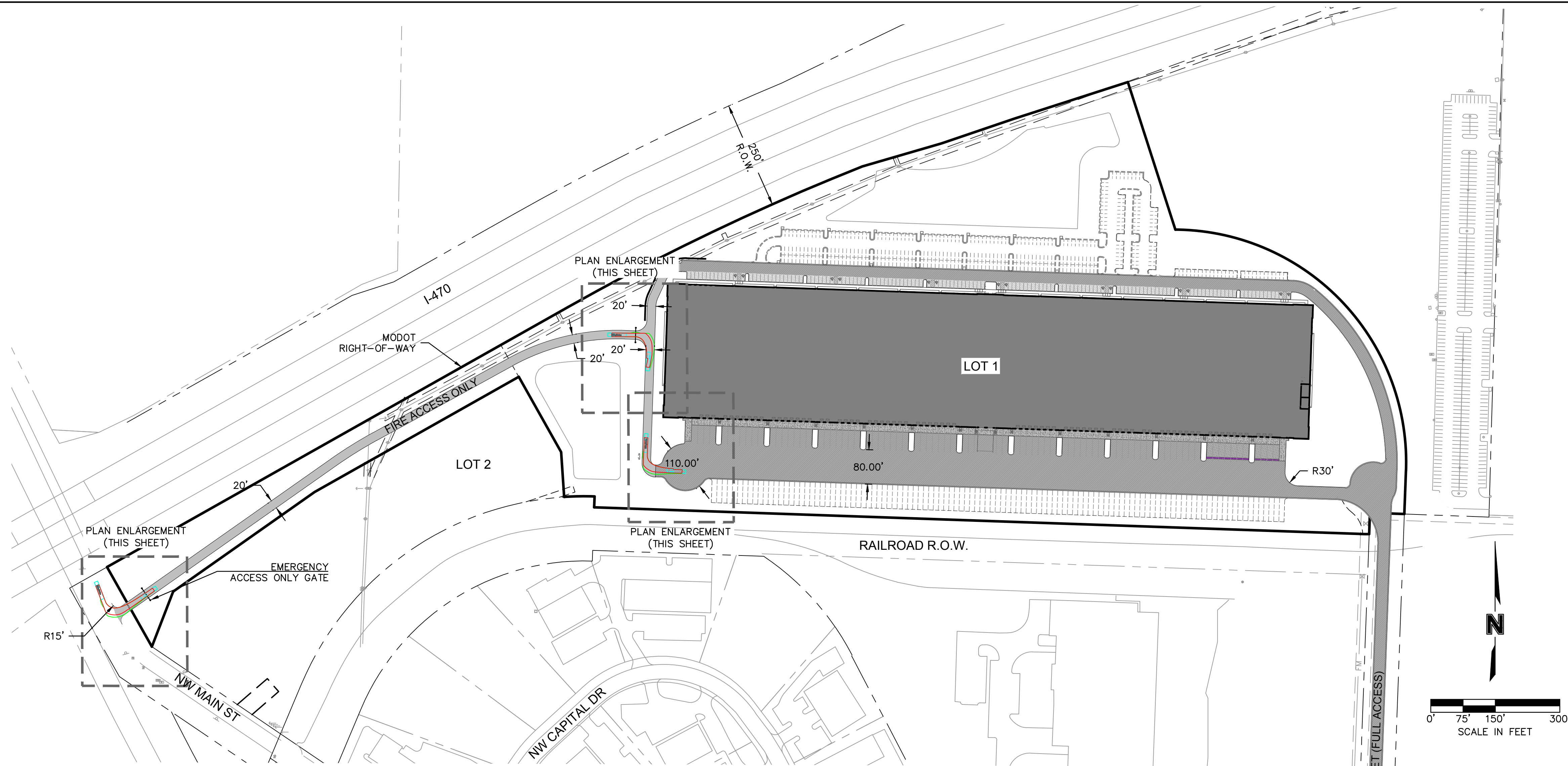
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REVISIONS

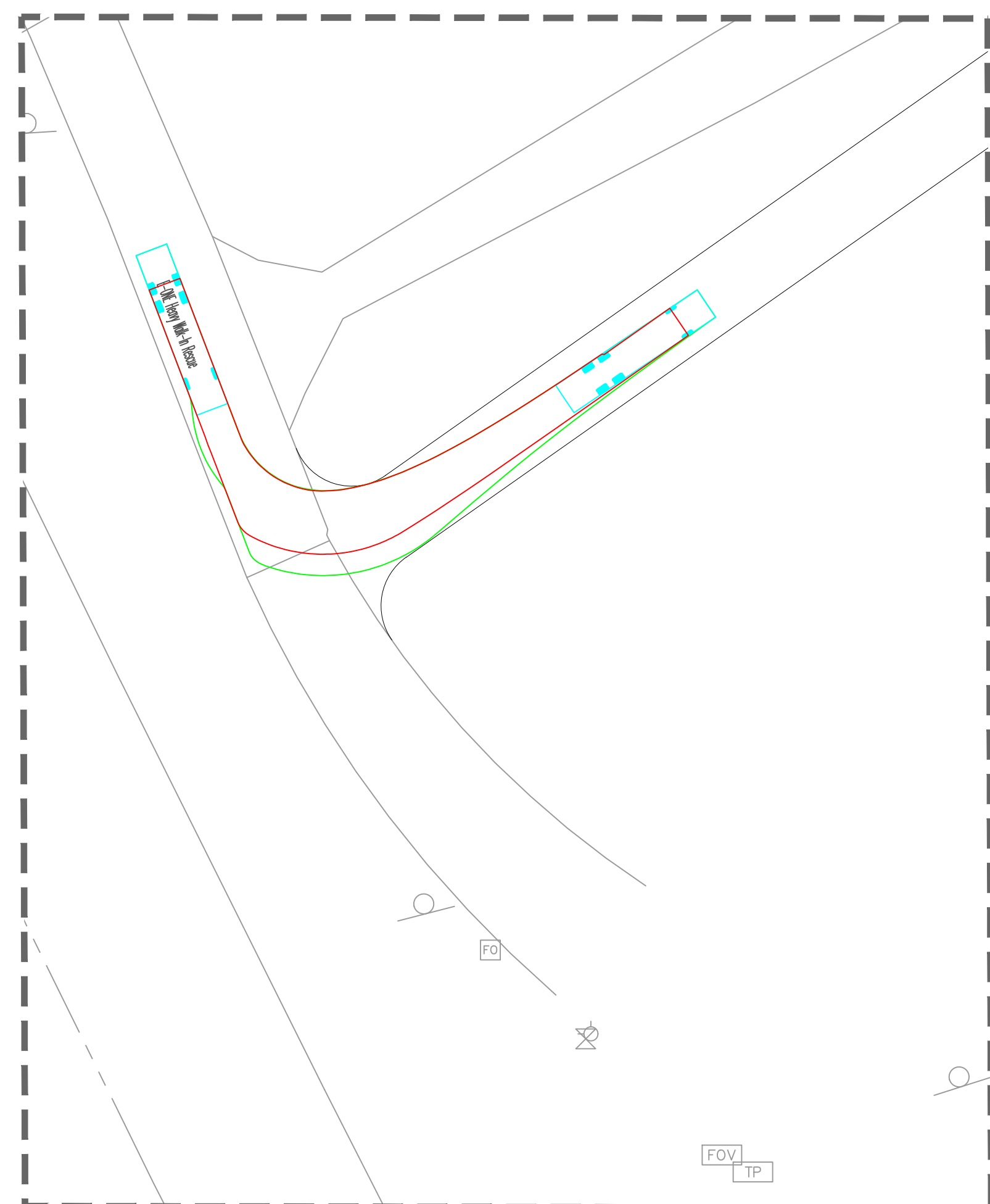
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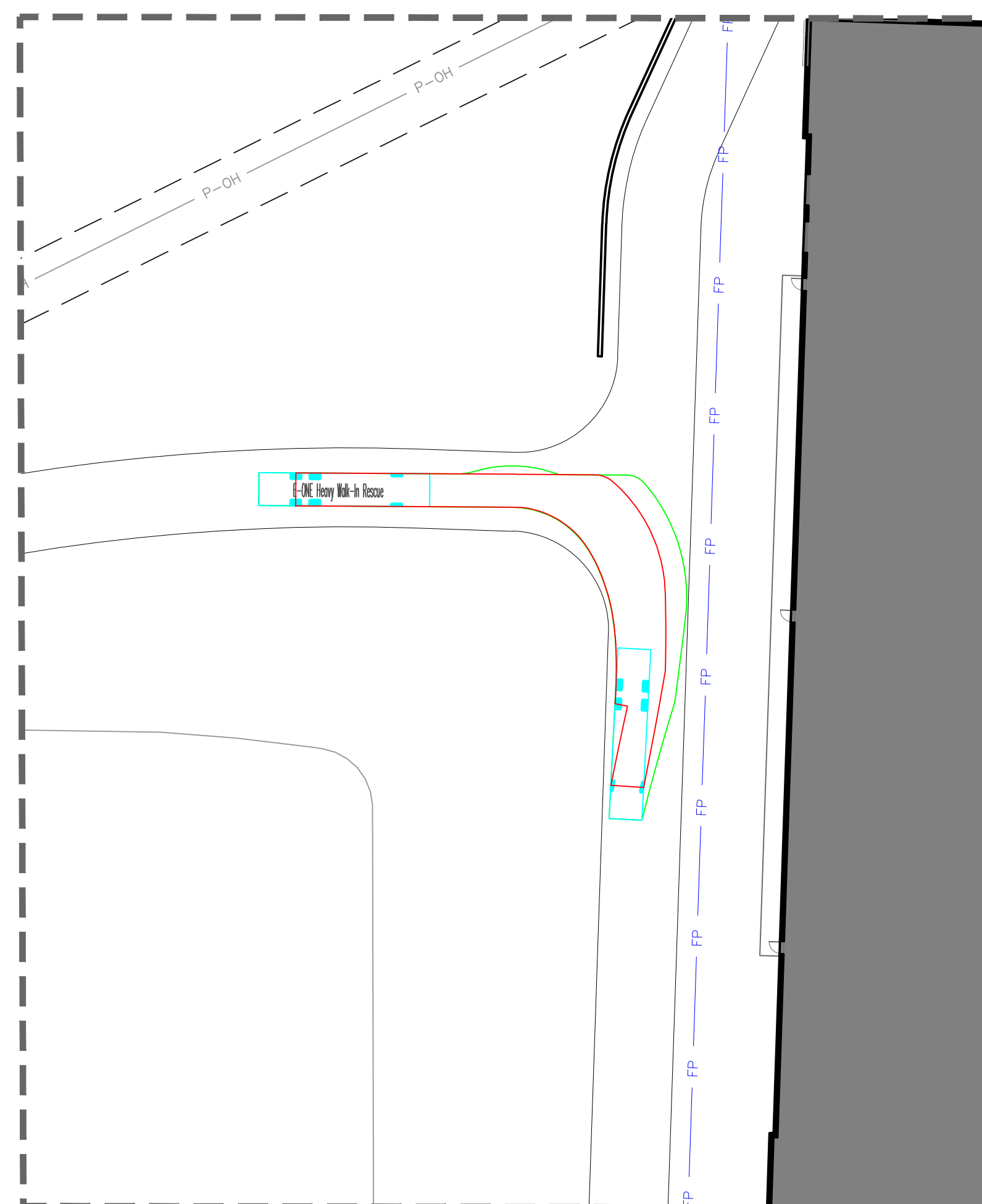
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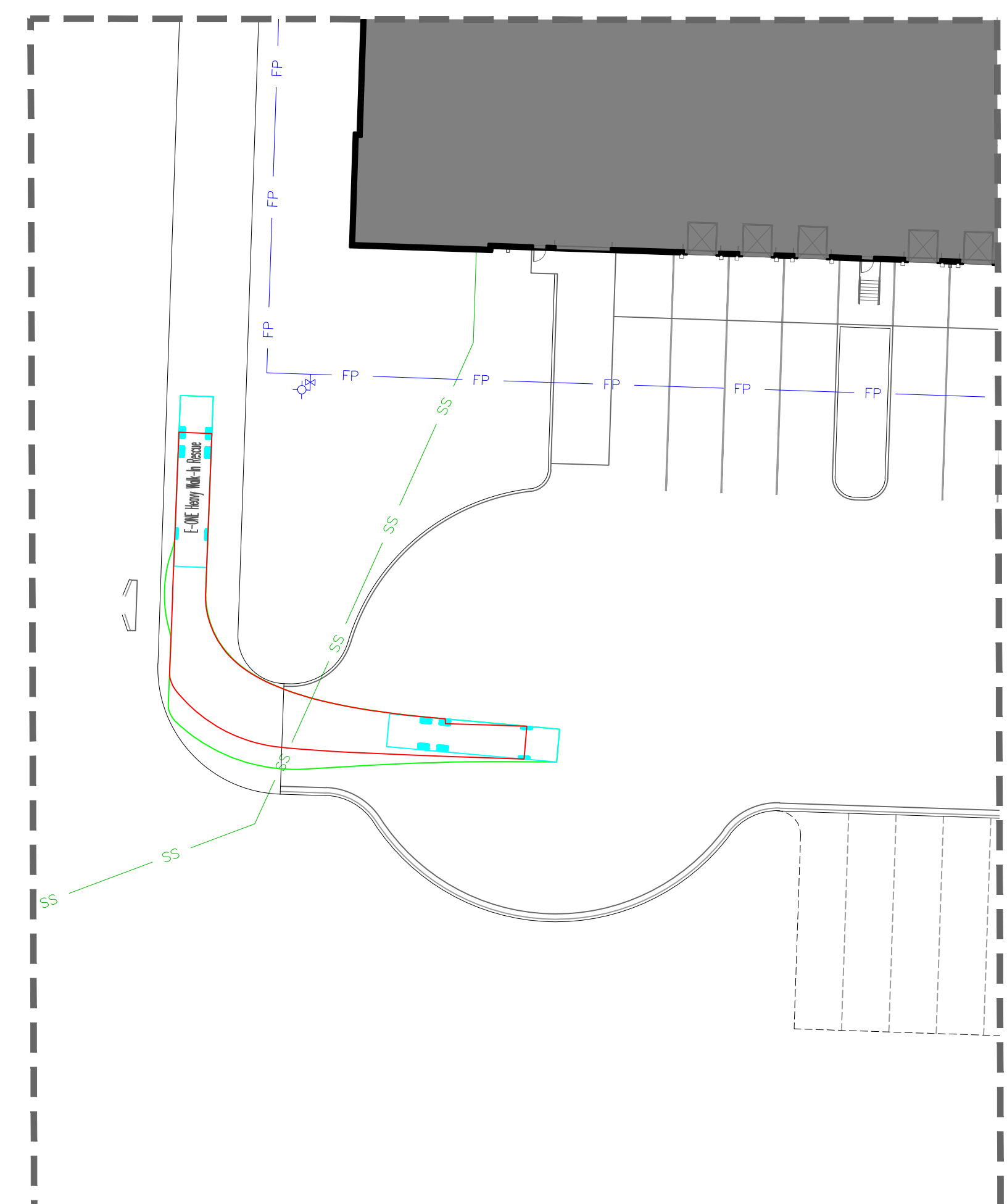
E-ONE Heavy Walk-In Rescue
Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width
Lock-to-lock time
Max Wheel Angle



FIRE TRUCK TURNING FROM MAIN ST Scale: 1' = 30'

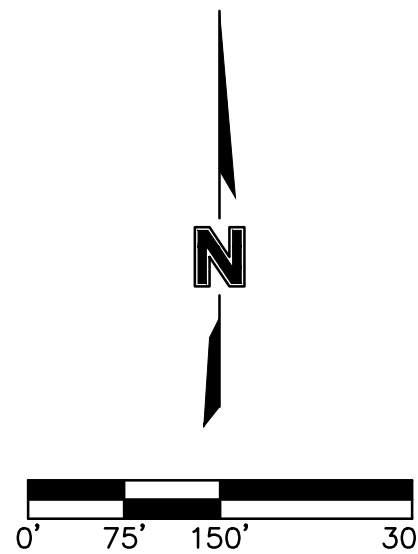
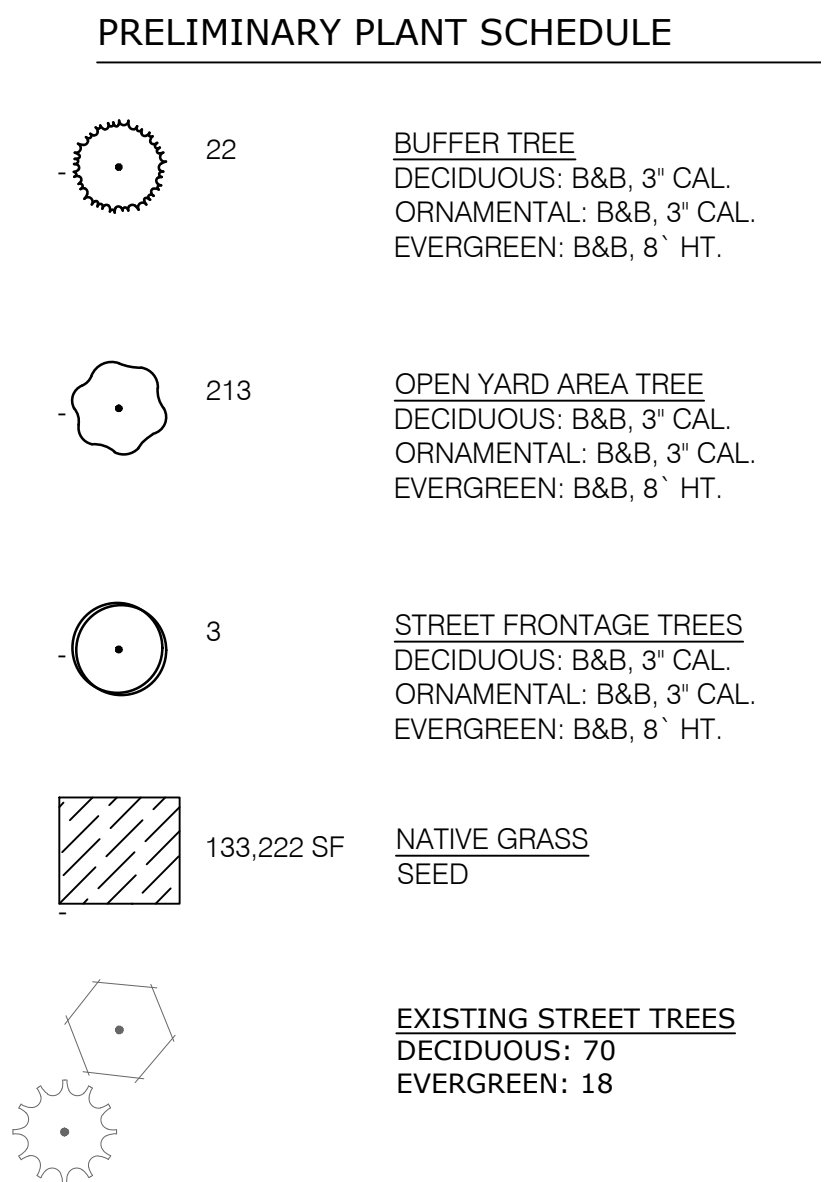


FIRE TRUCK TURNING SOUTH Scale: 1' = 30'

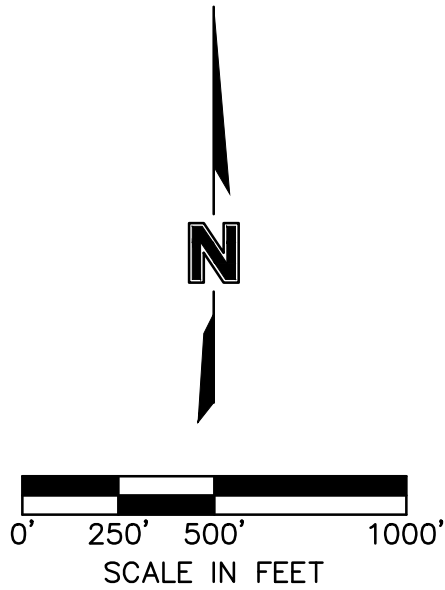
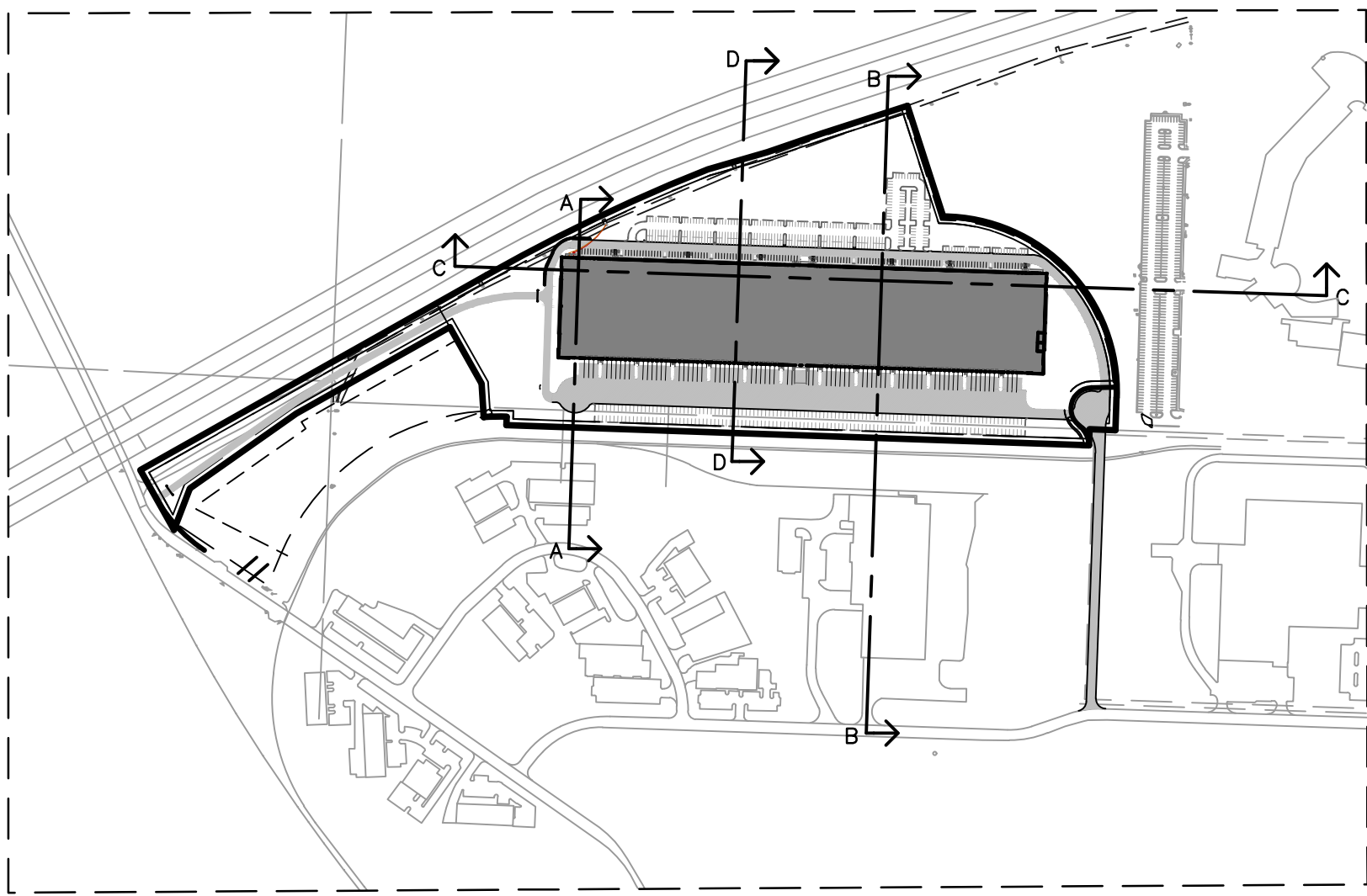


FIRE TRUCK ACCESSING DOCK AREA Scale: 1' = 30'

0' 75' 150'
SCALE IN FEET

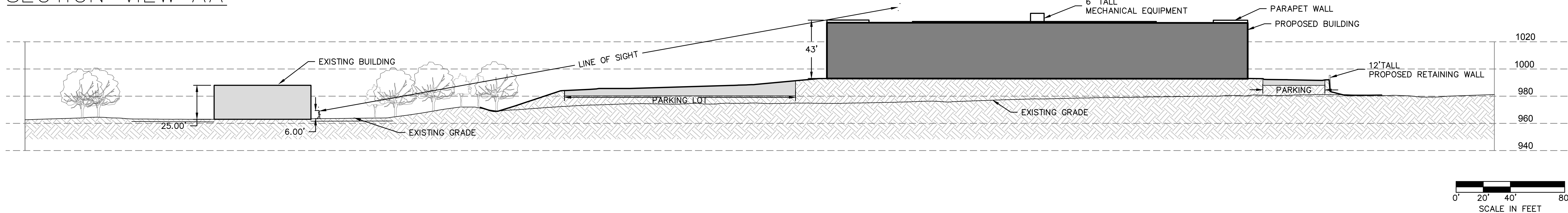


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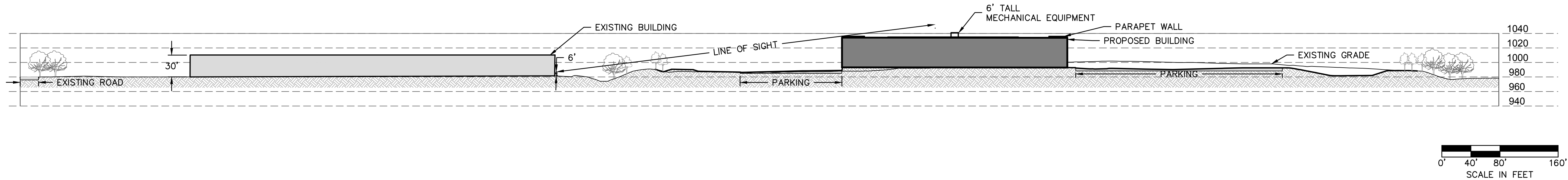


PLAN

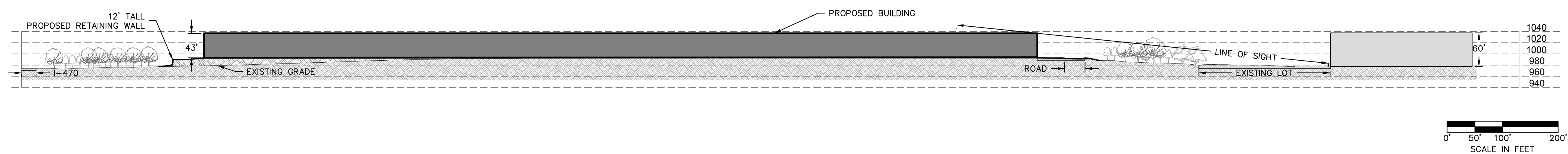
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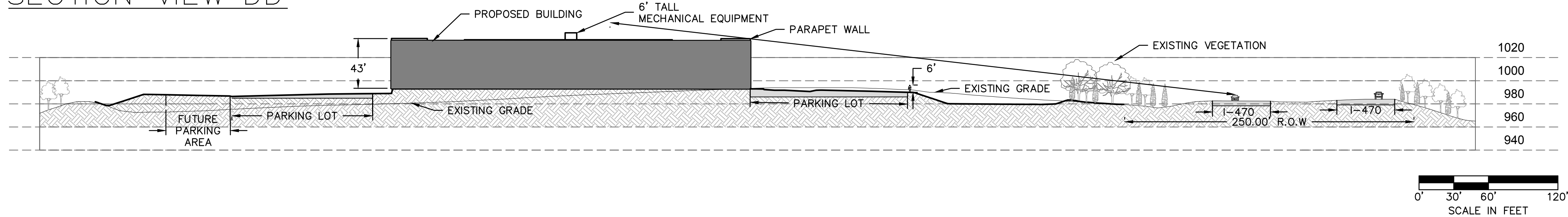
SECTION VIEW BB



SECTION VIEW CC



SECTION VIEW DD



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REV. NO.

REVISIONS

LINE OF SIGHT EXHIBIT

SUMMIT 470 LOGISTICS CENTER

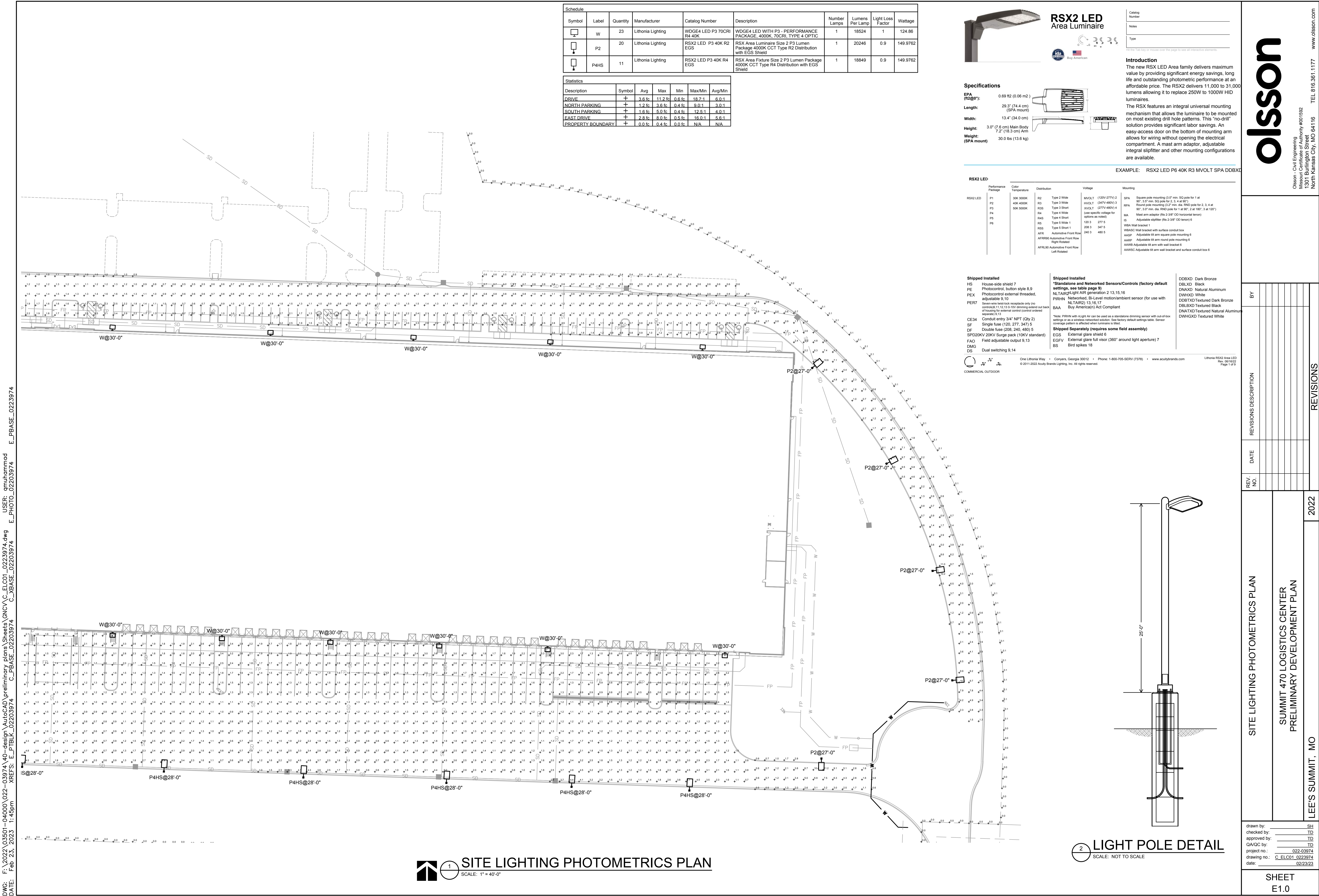
PRELIMINARY DEVELOPMENT PLAN & PRELIMINARY PLAT

2022

LEE'S SUMMIT, MO

drawn by: CAD
checked by: ENG
approved by: ENG
QA/QC by: ENG
project no.: 022-03974
drawing title: LINE-SIGHT-LINES_02203974
date: 02/23/23

SHEET
L2.0



Schedule							
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Number Lamps	Lumens Per Lamp
	W	23	Lithonia Lighting	WDGE4 LED P3 70CRI R4 40K	WDGE4 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 70CRI, TYPE 4 OPTIC	1	18524
	P2	20	Lithonia Lighting	RSX2 LED P3 40K R2 EGS	RSX Area Luminaire Size 2 P3 Lumen Package 4000K CCT Type R2 Distribution with EGS Shield	1	20246
	P4HS	11	Lithonia Lighting	RSX2 LED P3 40K R4 EGS	RSX Area Fixture Size 2 P3 Lumen Package 4000K CCT Type R4 Distribution with EGS Shield	1	18849
Statistics							
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min	
DRIVE	+	3.6 fc	11.2 fc	0.6 fc	19.7:1	6.0:1	
NORTH PARKING	+	1.2 fc	3.6 fc	0.4 fc	9.0:1	3.0:1	
SOUTH PARKING	+	1.6 fc	5.0 fc	0.4 fc	12.5:1	4.0:1	
EAST DRIVE	+	2.8 fc	8.0 fc	0.5 fc	16.0:1	5.6:1	
PROPERTY BOUNDARY	+	0.0 fc	0.4 fc	0.0 fc	N/A	N/A	

RSX2 LED Area Luminaire

Buy American

Specifications

EPA (ft2@9'): 0.69 ft2 (0.06 m2)

Length: 29.3" (74.4 cm) (SPA mount)

Width: 13.4" (34.0 cm)

Height: 3.0" (7.6 cm) Main Body
7.2" (18.3 cm) Arm

Weight: (SPA mount) 30.0 lbs (13.6 kg)

Introduction

The new RSX LED Area family delivers maximum value by providing significant energy savings, long life and outstanding photometric performance at an affordable price. The RSX2 delivers 11,000 to 31,000 lumens allowing it to replace 250W to 1000W HID luminaires.

The RSX features an integral universal mounting mechanism that allows the luminaire to be mounted on most existing drill hole patterns. This 'no-drill' solution provides significant labor savings. An easy-access door on the bottom of mounting arm allows for wiring without opening the electrical compartment. A mast arm adaptor, adjustable integral slipfitter and other mounting configurations are available.

RSX2 LED			
Performance Package	Color Temperature	Distribution	Voltage
RSX2 LED	P1 30K 3000K	R2 Type 2 Wide	120V/277V/2
P2	40K 4000K	R3 Type 3 Wide	120V/277V/2
P3	50K 5000K	R4 Type 4 Wide	120V/277V/2
P4		R5 Type 5 Short	120V/277V/2
P5		R6 Type 6 Short	120V/277V/2
P6		R7 Type 7 Short	120V/277V/2

Shipped Installed

HS House-side shield 7

PE Photocell, button style 8.9

PEX Photocell external threaded, adjustable 9.10

PER7 Seven-wire twist lock receptacle only (no cordsets). 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000

Shipped Separately (requires some field assembly)

EGS External glare shield 6

EGSV External glare full visor (360° around light aperture) 7

BS Bird spikes 18

DBX Dark Bronze

DBLXD Black

DNAXD Natural Aluminum

DWAXD White

DBTXD Textured Dark Bronze

DBLXD Textured Black

DNAXD Textured Natural Aluminum

DWAXD Textured White

2 LIGHT POLE DETAIL
SCALE: NOT TO SCALE

1 SITE LIGHTING PHOTOMETRICS PLAN
SCALE: 1" = 40'-0"

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Missouri Certificate of Authority #001592
1301 Burlington Street
North Kansas City, MO 64116
TEL 816.361.1177
www.olsson.com

BY	
REVISIONS DESCRIPTION	
DATE	
REV. NO.	

SITE LIGHTING PHOTOMETRICS PLAN	
SUMMIT 470 LOGISTICS CENTER PRELIMINARY DEVELOPMENT PLAN	
LEE'S SUMMIT, MO	2022

drawn by: SH

checked by: TB

approved by: TB

QA/QC by: TB

project no.: 022-03974

drawing no.: C_ELCO1_0223974

date: 02/23/23

SHEET E1.0

DWG: F:\2022\03501-04000\022-03974\40-design\AutoCAD\preliminary plans\Sheets\CONVC_ELC01_0223974.dwg USER: gpkubarnad E_PHOTO_02203974 E_PBASE_02203974
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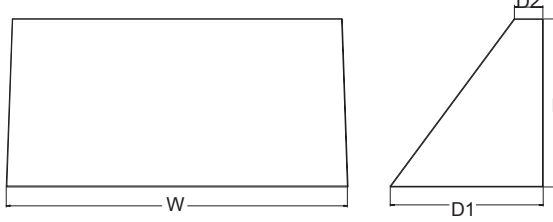


WEDGE3 LED Architectural Wall Sconce



Specifications

Depth (D1): 8"
Depth (D2): 1.5"
Height: 9"
Width: 18"
Weight: 19.5 lbs
(without options)



Catalog Number
Notes
Type

Use the Tab key to mouse over the page to see all interactive elements.

Introduction
The WEDGE LED family is designed to meet specifier's every wall-mounted lighting need in a widely accepted shape that blends with any architecture. The clean rectilinear design comes in four sizes with lumen packages ranging from 1,200 to 25,000 lumens, providing a true site-wide solution. Embedded with nLight® AIR wireless controls, the WEDGE family provides additional energy savings and code compliance.

WEDGE3 has been designed to deliver up to 12,000 lumens through a precision refractive lens with wide distribution, perfect for augmenting the lighting from pole mounted luminaires.

WEDGE1 LED	4W	--	--	1,200	2,000	--	--	--	--
WEDGE2 LED	10W	18W	Standalone / nLight	1,200	2,000	3,000	4,500	6,000	--
WEDGE3 LED	15W	18W	Standalone / nLight	7,500	8,500	10,000	12,000	--	--
WEDGE4 LED	--	--	Standalone / nLight	12,000	16,000	18,000	20,000	22,000	25,000

EXAMPLE: WEDGE3 LED P3 40K 70CRI R3 MVOLT SRM DDBXD

WEDGE3 LED	P1	30K	3000K	70CRI	R2	Type 2	MVOLT	Shipped included	Shipped separately
	P2	40K	4000K	80CRI	R3	Type 3	347 1	SRM Surface mounting bracket	AWSS 3/8" Architectural wall spacer
	P3	50K	5000K		R4	Type 4	480 1	ICW Indirect Canopy/Ceiling	PBBW Surface-mounted back box (top, left, right, conduit entry). Use when there is no junction box available.
	P4				RFT	Forward Throw		Washer bracket (city/damp locations only)	

E15WH	Emergency battery backup. Certified in Title 20 MAEDBS (15W, 3°C min)	Standalone Sensors/Controls	PIR	Bi-level (100/35%) motion sensor for 8-15' mounting heights. Intended for use on switched circuits with external dusk to dawn switching.	DDBXD	Dark bronze
E20WC	Emergency battery backup. Certified in Title 20 MAEDBS (18W, -20°C min)		PIRH	Bi-level (100/35%) motion sensor for 15-30' mounting heights. Intended for use on switched circuits with external dusk to dawn switching.	DBLXD	Black
PE 2	PhotoCell, Button Type		PIR1FC3V	Bi-level (100/35%) motion sensor for 8-15' mounting heights with photocell pre-programmed dusk to dawn operation.	DNWGD	Natural aluminum
DMG 3	0-10V dimming wires pulled outside. Intended for use with an external control, ordered separately.		PIR1HFC3V	Bi-level (100/35%) motion sensor for 15-30' mounting heights with photocell pre-programmed for dusk to dawn operation.	DNWGD	White
BCE	Bottom conduit entry for back box (PBBW). Total of 4 entry points.	Networked Sensors/Controls	NLTAIR2	nLight® AIR Wireless enabled bi-level motion/ambient sensor for 8-15' mounting heights.	DSBXD	Sandstone
SPD10KV	10kV Surge pack		NLTAIR2	nLight® AIR Wireless enabled bi-level motion/ambient sensor for 15-30' mounting heights.	DBDBXD	Textured dark bronze
BAA	Buy American(s) Act Compliant			See page 4 for out of box functionality	DBLXD	Textured black

Accessories

WEDGEAWSS DDBXD	WEDGE 3/8" Architectural Wall Spacer (specify finish)
WEDGE3PBBW DDBXD	U WEDGE3 surface-mounted back box (specify finish)

NOTES
1 347V and 480V not available with E15WH and E20WC.
2 PE not available in 480V and with sensors/controls.
3 DMG option not available with sensors/controls.
4 Not qualified for D.C. Not available with emergency battery backup or sensors/controls.



SITE LIGHTIN PHOTOMETRICS PLAN

SCALE: 1" = 40'-0"

SITE LIGHTIN PHOTOMETRICS PLAN

SUMMIT 470 LOGISTICS CENTER PRELIMINARY DEVELOPMENT PLAN

LEE'S SUMMIT, MO

drawn by: SH
checked by: TD
approved by: TD
QA/QC by: TD
project no.: 022-03974
drawing no.: C_ELC01_0223974
date: 02/23/23

SHEET
E1.1

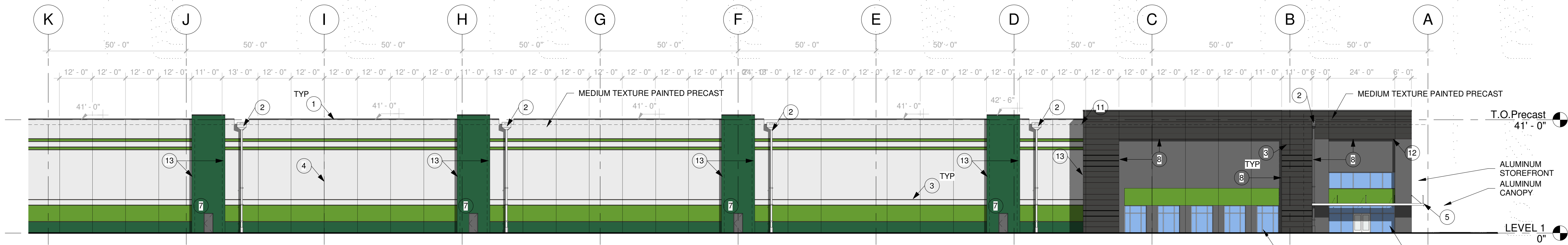
REVISIONS

REV. NO.	DATE	REVISIONS DESCRIPTION	BY

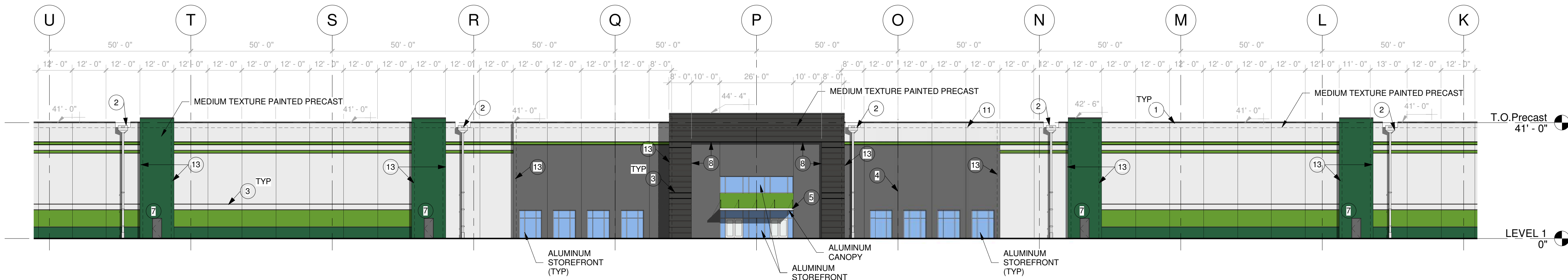
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North Kansas City, MO 64116
TEL 816.361.1177
www.olson.com

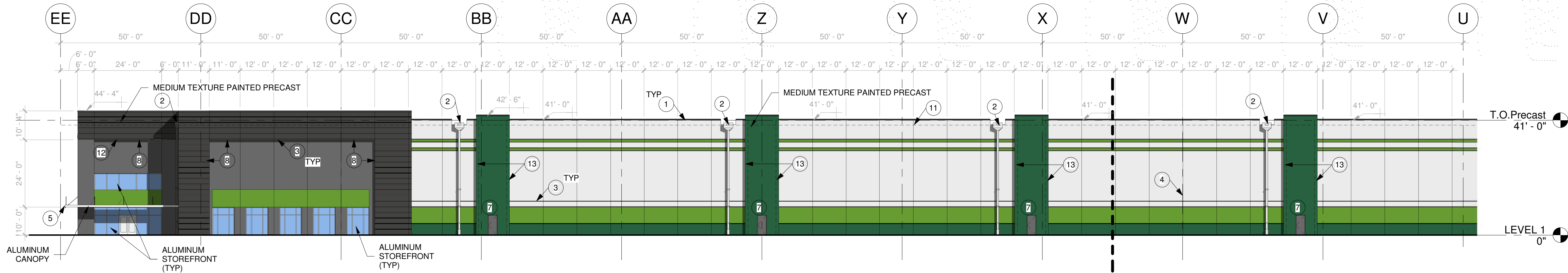
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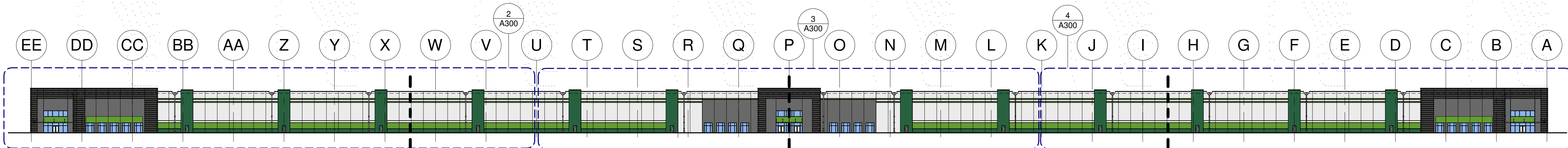
4 NORTH ELEVATION - PARTIAL 3
A300 3/64" = 1'-0"



3 NORTH ELEVATION - PARTIAL 2
A300 3/64" = 1'-0"



2 NORTH ELEVATION - PARTIAL 1
A300 3/64" = 1'-0"



1 NORTH ELEVATION - OVERALL
A300 1" = 60'-0"

EXTERIOR ELEVATION KEYNOTES

- 1 PREFINISHED METAL COPING WITH CONT. KEEPER
- 2 SCUPPER, COLLECTOR HEAD AND DOWNSPOUT. COLOR TO MATCH ADJACENT PRECAST
- 3 HORIZONTAL REVEAL
- 4 EXPANSION JOINT
- 5 PREFABRICATED ALUMINUM CANOPY WITH TIE BACK RODS. PROVIDE DRAINAGE SCUPPER AT EACH END
- 6 KNOCK OUT PANEL FOR FUTURE LOADING DOCK OPENING
- 7 EXTERIOR HOLLOW METAL DOOR AND FRAME. PAINT TO MATCH ADJACENT PRECAST
- 8 RECESS IN PRECAST PANEL. PANEL JOINT HERE TO HAVE PROFILE TO MATCH TYPICAL HORIZONTAL REVEAL
- 9 12" TALL ALUMINUM BUILDING SIGNAGE. BLACK FINISH.
- 10 ROOF LINE BEYOND
- 11 PANEL JOINT HERE TO MATCH HORIZONTAL REVEAL PROFILE
- 12 12" PANEL OVERLAP CONDITION

EXTERIOR ELEVATION FINISH KEY

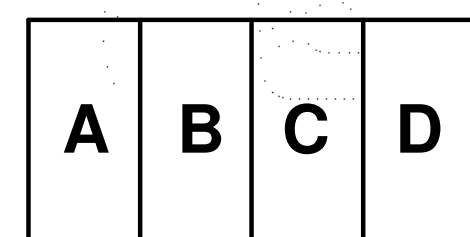
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CUSTOM COLOR MATCH
RGB: 40 97 63
- LIGHT GREEN
CUSTOM COLOR MATCH
RGB: 102 156 50
- SW 6253 OLYMPUS WHITE
- SW 3063 CHARCOAL
- SW 7069 IRON ORE



RYAN A+E, INC.
533 South Third Street
Suite 100
Minneapolis, MN 55415
612-492-4000 tel
612-492-3000 fax

WWW.RYANCOMPANIES.COM

CONSULTANTS



KEY PLAN

PROJECT INFORMATION

**SUMMIT 470
LOGISTICS**

LEES SUMMIT, MO

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Registered Architect under the laws of the State of Minnesota

Name _____

REGISTRATION NO. _____ DATE _____
Number _____ Date _____

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DRAWN BY _____ CHECKED BY _____
Author _____ Checker _____
JOB NO. _____ DATE _____
0000 _____ Issue Date _____

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION
A	11/4/20	SCHEMATIC DESIGN
22		

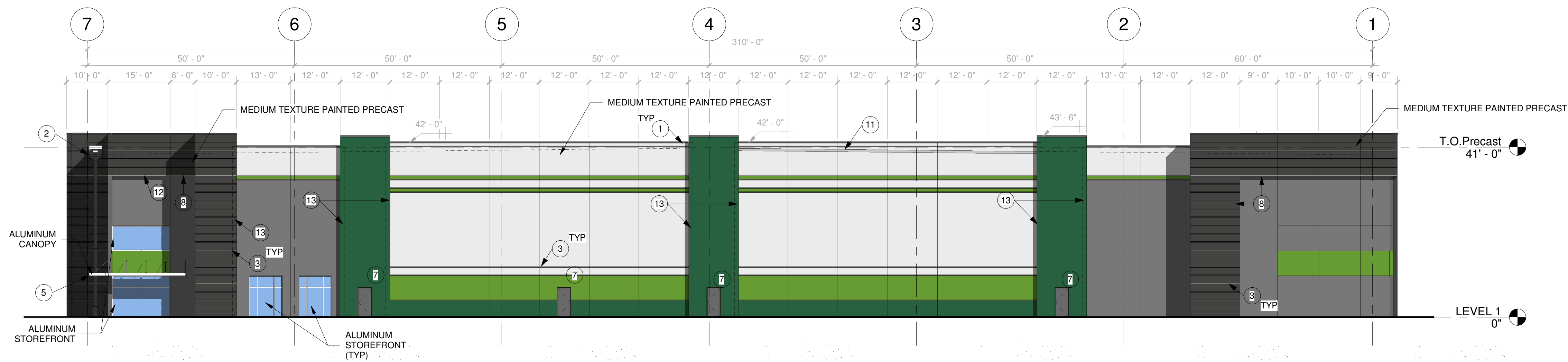
SD

11.04.2022

EXTERIOR
ELEVATIONS

A300

NOT FOR CONSTRUCTION



Architectural elevation drawing of a building facade, showing seven bays (1 through 7) separated by vertical columns. The drawing includes dimensions for each bay and overall sections, as well as material and finish callouts.

Dimensions (from left to right):

- Bay 1: 9'-0"
- Bay 2: 10'-0"
- Bay 3: 10'-0"
- Bay 4: 21'-0"
- Bay 5: 12'-0"
- Bay 6: 13'-0"
- Bay 7: 11'-0"
- Overall Section 1 (Bays 1-2): 60'-0"
- Overall Section 2 (Bays 3-4): 50'-0"
- Overall Section 3 (Bays 5-6): 50'-0"
- Overall Section 4 (Bays 7-8): 50'-0"
- Overall Section 5 (Bays 9-10): 50'-0"
- Overall Section 6 (Bays 11-12): 50'-0"
- Overall Section 7 (Bays 13-14): 50'-0"
- Overall Section 8 (Bays 15-16): 50'-0"
- Overall Section 9 (Bays 17-18): 50'-0"
- Overall Section 10 (Bays 19-20): 50'-0"
- Overall Section 11 (Bays 21-22): 50'-0"
- Overall Section 12 (Bays 23-24): 50'-0"
- Overall Section 13 (Bays 25-26): 50'-0"
- Overall Section 14 (Bays 27-28): 50'-0"
- Overall Section 15 (Bays 29-30): 50'-0"
- Overall Section 16 (Bays 31-32): 50'-0"
- Overall Section 17 (Bays 33-34): 50'-0"
- Overall Section 18 (Bays 35-36): 50'-0"
- Overall Section 19 (Bays 37-38): 50'-0"
- Overall Section 20 (Bays 39-40): 50'-0"
- Overall Section 21 (Bays 41-42): 50'-0"
- Overall Section 22 (Bays 43-44): 50'-0"
- Overall Section 23 (Bays 45-46): 50'-0"
- Overall Section 24 (Bays 47-48): 50'-0"
- Overall Section 25 (Bays 49-50): 50'-0"
- Overall Section 26 (Bays 51-52): 50'-0"
- Overall Section 27 (Bays 53-54): 50'-0"
- Overall Section 28 (Bays 55-56): 50'-0"
- Overall Section 29 (Bays 57-58): 50'-0"
- Overall Section 30 (Bays 59-60): 50'-0"
- Overall Section 31 (Bays 61-62): 50'-0"
- Overall Section 32 (Bays 63-64): 50'-0"
- Overall Section 33 (Bays 65-66): 50'-0"
- Overall Section 34 (Bays 67-68): 50'-0"
- Overall Section 35 (Bays 69-70): 50'-0"
- Overall Section 36 (Bays 71-72): 50'-0"
- Overall Section 37 (Bays 73-74): 50'-0"
- Overall Section 38 (Bays 75-76): 50'-0"
- Overall Section 39 (Bays 77-78): 50'-0"
- Overall Section 40 (Bays 79-80): 50'-0"
- Overall Section 41 (Bays 81-82): 50'-0"
- Overall Section 42 (Bays 83-84): 50'-0"
- Overall Section 43 (Bays 85-86): 50'-0"
- Overall Section 44 (Bays 87-88): 50'-0"
- Overall Section 45 (Bays 89-90): 50'-0"
- Overall Section 46 (Bays 91-92): 50'-0"
- Overall Section 47 (Bays 93-94): 50'-0"
- Overall Section 48 (Bays 95-96): 50'-0"
- Overall Section 49 (Bays 97-98): 50'-0"
- Overall Section 50 (Bays 99-100): 50'-0"

Material and Finish Callouts:

- MEDIUM TEXTURE PAINTED PRECAST
- T.O. Precast 41'-0"
- ALUMINUM CANOPY
- ALUMINUM STOREFRONT (TYP)
- LEVEL 1 0"

Numbered Callouts (1 through 13):

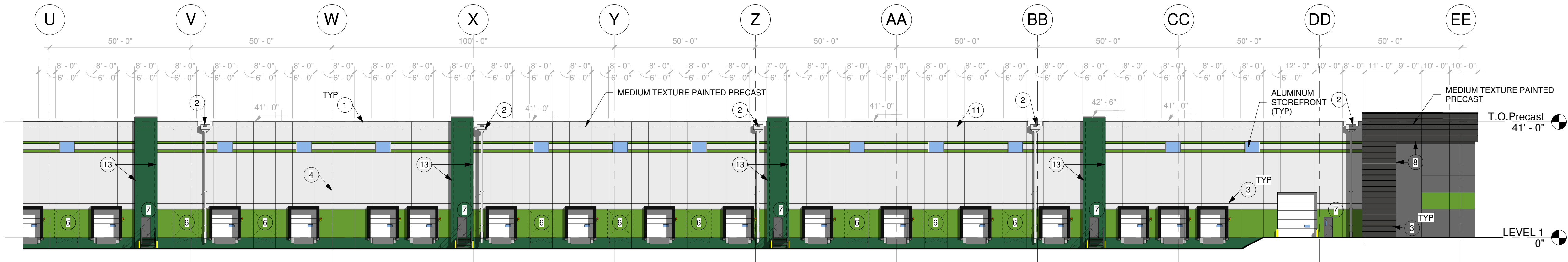
- 1: Top of precast section
- 2: Top of precast section
- 3: Top of precast section
- 4: Top of precast section
- 5: Top of precast section
- 6: Top of precast section
- 7: Top of precast section
- 8: Top of precast section
- 9: Top of precast section
- 10: Top of precast section
- 11: Top of precast section
- 12: Top of precast section
- 13: Top of precast section

EXTERIOR ELEVATION KEYNOTES

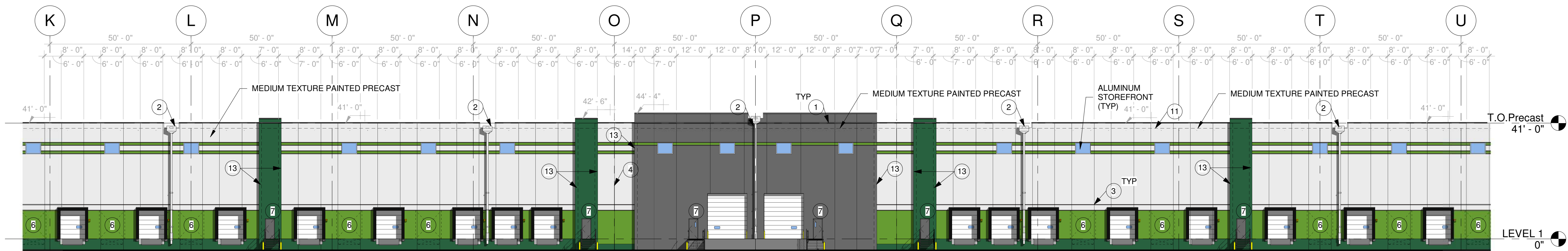
1	PREFINISHED METAL COPING WITH CORN. KEEPER
2	SCUPPER, COLLECTOR HEAD AND DOWNSPOUT. COLOR TO MATCH ADJACENT PRECAST
3	HORIZONTAL REVEAL
4	EXPANSION JOINT
5	PREFABRICATED ALUMINUM CANOPY WITH THE BACK RODS. PROVIDE DRAINAGE SCUPPER AT EACH END
6	KNOCK OUT PANEL FOR FUTURE LOADING DOCK OPENING
7	EXTERIOR HOLLOW METAL DOOR AND FRAME. PAINT TO MATCH ADJACENT PRECAST
8	RECESS IN PRECAST PANEL
9	PANEL JOINT HERE TO HAVE PROFILE TO MATCH TYPICAL HORIZONTAL REVEAL
10	12" TALL ALUMINUM BUILDING SIGNAGE. BLACK FINISH.
11	ROOF LINE BEYOND
12	PANEL JOINT HERE TO MATCH HORIZONTAL REVEAL PROFILE
13	12" PANEL OVERLAP CONDITION

NOT FOR CONSTRUCTION

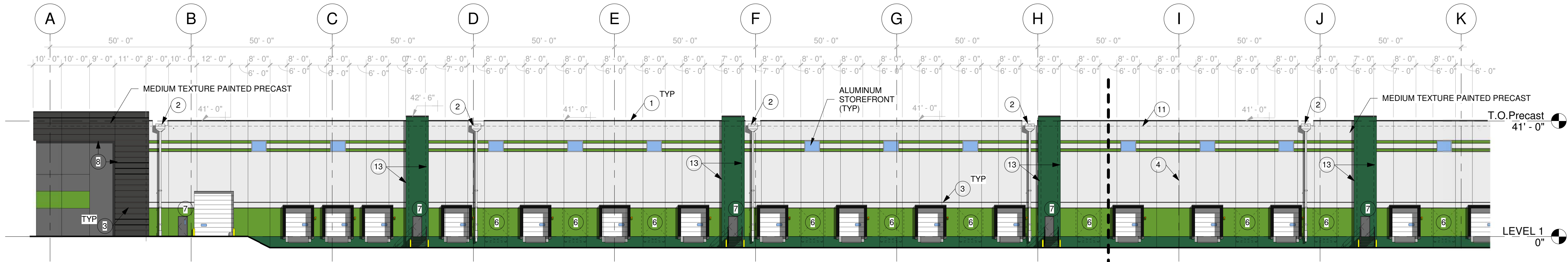
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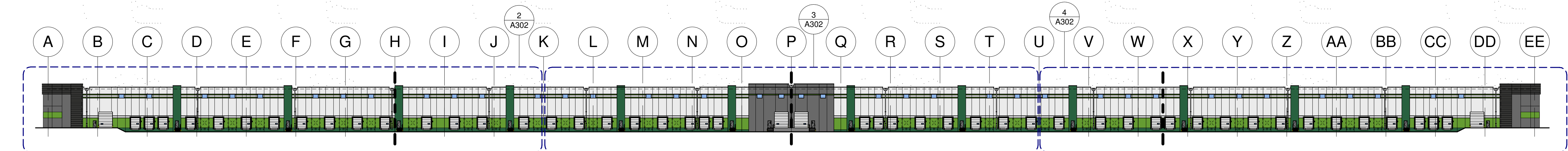
4 SOUTH ELEVATION - PARTIAL 3
A302 3/64" = 1'-0"



3 SOUTH ELEVATION - PARTIAL 2
A302 3/64" = 1'-0"



2 SOUTH ELEVATION - PARTIAL 1
A302 3/64" = 1'-0"



1 SOUTH ELEVATION - OVERALL
A302 1" = 60'-0"

EXTERIOR ELEVATION KEYNOTES

- 1 PREFINISHED METAL COPING WITH CONT. KEEPER
- 2 SCUPPER, COLLECTOR HEAD AND DOWNSPOUT. COLOR TO MATCH ADJACENT PRECAST
- 3 HORIZONTAL REVEAL
- 4 EXPANSION JOINT
- 5 PREFABRICATED ALUMINUM CANOPY WITH TIE BACK RODS. PROVIDE DRAINAGE SCUPPER AT EACH END
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- 13 12" PANEL OVERLAP CONDITION

EXTERIOR ELEVATION FINISH KEY

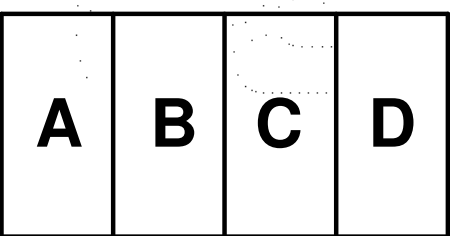
- DARK GREEN
CUSTOM COLOR MATCH
RGB: 40 97 63
- LIGHT GREEN
CUSTOM COLOR MATCH
RGB: 102 156 50
- SW 6253 OLYMPUS WHITE
- SW 3063 CHARCOAL
- SW 7069 IRON ORE



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Suite 100
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612-492-3000 fax

WWW.RYANCOMPANIES.COM

CONSULTANTS



PROJECT INFORMATION

**SUMMIT 470
LOGISTICS**

LEES SUMMIT, MO

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Registered Architect under the laws of the State of Minnesota

Name _____

REGISTRATION NO. _____ DATE _____
Number _____ Date _____

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DRAWN BY _____ CHECKED BY _____
Author _____ Checker _____
JOB NO. _____ DATE _____
0000 _____ Issue Date _____

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION
A	11/4/2022	SCHEMATIC DESIGN

SD

11.04.2022

**EXTERIOR
ELEVATIONS**

A302

PRELIMINARY
NOT FOR CONSTRUCTION

NOT FOR CONSTRUCTION



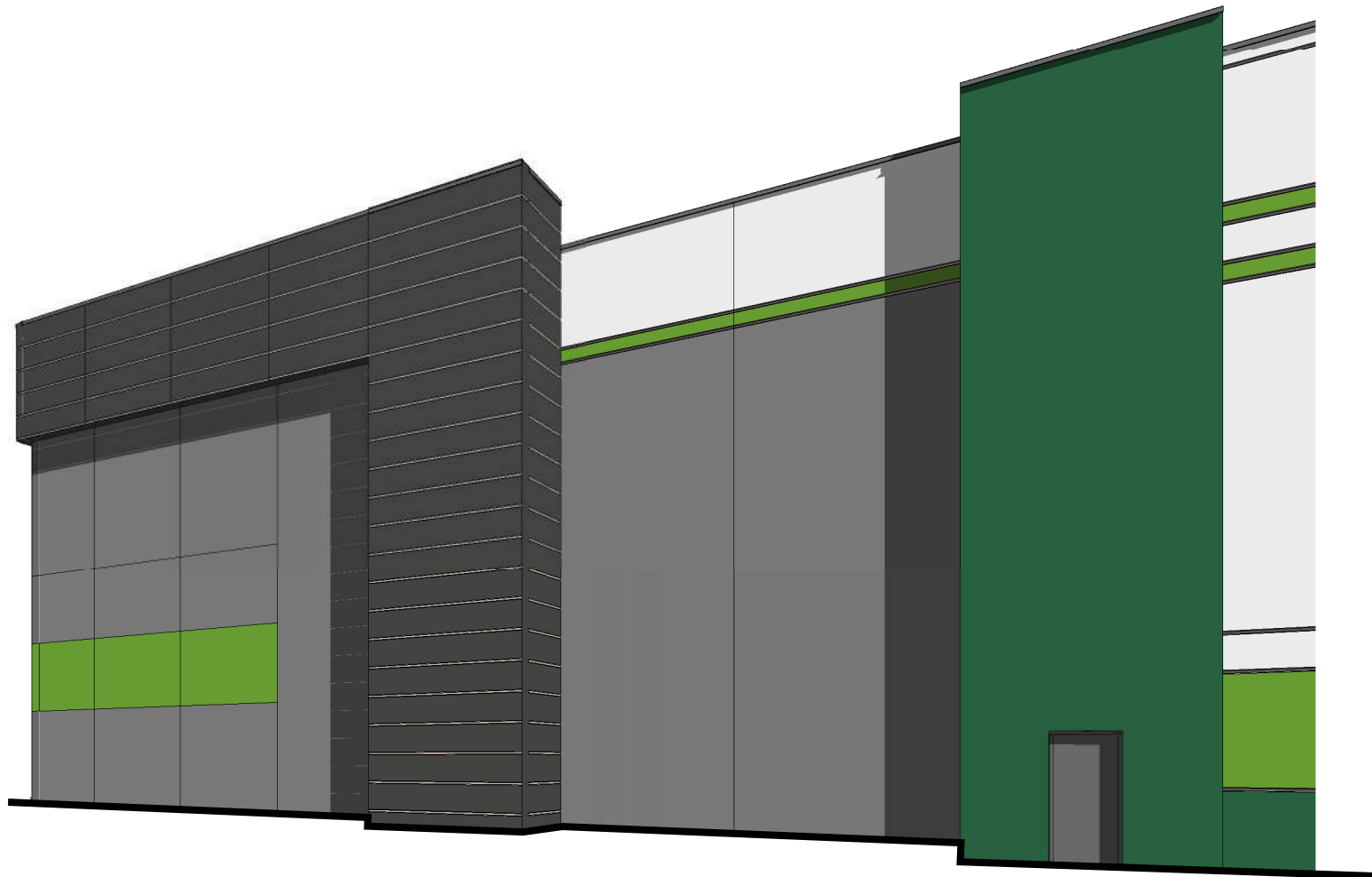
1
A303 NE CORNER



2
A303 NW CORNER



4
A303 N ENTRY



3
A303 SE CORNER



5
A303 SW CORNER

A	B	C	D
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KEY PLAN

**SUMMIT 470
LOGISTICS**

LEES SUMMIT, MO

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Registered Architect under the laws of the State of Minnesota

Name _____

REGISTRATION NO. _____ DATE _____
Number _____ Date _____

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DRAWN BY	CHECKED BY
Author	Checker
JOB NO.	DATE
0000	Issue Date

ISSUE RECORD		
ISSUE #	DATE	DESCRIPTION

SD

11.04.2022

**PERSPECTIVE
VIEWS**

A303

PRELIMINARY
NOT FOR CONSTRUCTION