



LEE'S SUMMIT
MISSOURI
Development Services Department

Development Services Staff Report

File Number	PL2022-394
File Name	REZONING from PMIX and PI to PI and PRELIMINARY DEVELOPMENT PLAN – Summit 470 Logistic Center
Applicant	Ryan Companies US, Inc.
Property Address	1451 NW Main St
Planning Commission Date	February 9, 2023
Heard by	Planning Commission and City Council
Analyst	Hector Soto, Jr., AICP, Senior Planner
Checked By	Sue Pyles, PE, Development Engineering Manager

Public Notification

Pre-application held: July 12, 2022
Neighborhood meeting conducted: December 20, 2022
Newspaper notification published on: December 24, 2022
Radius notices mailed to properties within 300 feet on: December 21, 2022
Site posted notice on: December 21, 2022

Table of Contents

1. Project Data and Facts	2
2. Land Use	3
3. Project Proposal	4
4. Unified Development Ordinance (UDO)	5
5. Comprehensive Plan	5
6. Analysis	6
7. Recommended Conditions of Approval	10

Attachments

Rezoning Exhibit, dated October 18, 2022

Transportation Impact Analysis prepared by Brad Cooley, P.E., dated January 31, 2023
– 3 pages

Trip Generation Memorandum prepared by Olsson, dated November 8, 2022 – 4 pages

Preliminary Development Plan, revision date December 13, 2022 – 17 pages

Preliminary Storm Drainage Study prepared by Olsson, sealed November 9, 2022 – 17
pages

Building Elevations, dated November 4, 2022 – 4 pages

Neighborhood Meeting Minutes, dated December 20, 2022

City of Lee's Summit Unimproved Road Policy

Location Map

1. Project Data and Facts

Project Data	
Applicant/Status	Ryan Companies US, Inc./Developer
Applicant's Representative	Andy Crimmins
Location of Property	1451 NW Main St
Size of Property	±35.58 Acres (1,549,869 sq. ft.) – Lot 1 ±6.34 Acres (276,952 sq. ft.) – Lot 2 ±41.94 Acres (1,826,821 sq. ft.) - total
Number of Lots	2
Building Area	465,000 sq. ft. – Lot 1 Future TBD – Lot 2
FAR	0.30 – Lot 1
Zoning (Existing)	PMIX (Planned Mixed Use) and PI (Planned Industrial)
Zoning (Proposed)	PMIX
Comprehensive Plan Designation	Mixed Use and Industrial
Procedure	<p>The Planning Commission makes a recommendation to the City Council on the proposed rezoning and preliminary development plan. The City Council takes final action on the rezoning and preliminary development plan in the form of an ordinance.</p> <p>Duration of Validity: Rezoning approval by the City Council shall be valid upon approval and has no expiration.</p> <p>Preliminary development plan approval by the City Council shall not be valid for a period longer than twenty-four (24) months from the date of such approval, unless within such period a final development plan application is submitted. The City Council may grant one extension not exceeding twelve (12) months upon written request.</p>

Current Land Use

The subject 41.94-acre site is an undeveloped property. The site makes up the area south of I-470 bounded by NW Main St to the west, the Saint Luke's East Hospital campus to the east and Union Pacific rail spur serving the NW Victoria Dr area to the south.

Description of Applicant's Request

The applicant proposes a rezoning and preliminary development plan for a two-lot industrial development. Lot 1 will be the site of a 465,000 sq. ft. logistics center building. Lot 2 has no building proposed at this time. Under the proposed rezoning to PI (Planned Industrial), the future development of Lot 2 will not require going through a separate public hearing process in front of the Planning Commission and City Council. The UDO allows for the development of PI-zoned property to be approved at the staff level as long as the proposed development complies with all UDO standards. An application for the future development of Lot 2 would only be presented to the Planning Commission and City Council if the applicant were to request a modification seeking relief from any UDO requirement.

Architecturally, the proposed Lot 1 building will be constructed of pre-cast concrete panels. Color banding will provide horizontal relief. Additional painted concrete panels will be spaced out on all four building sides to provide vertical relief. These additional panels will have 12" of overlap in order to provide some projection to break up the large expanses of wall.

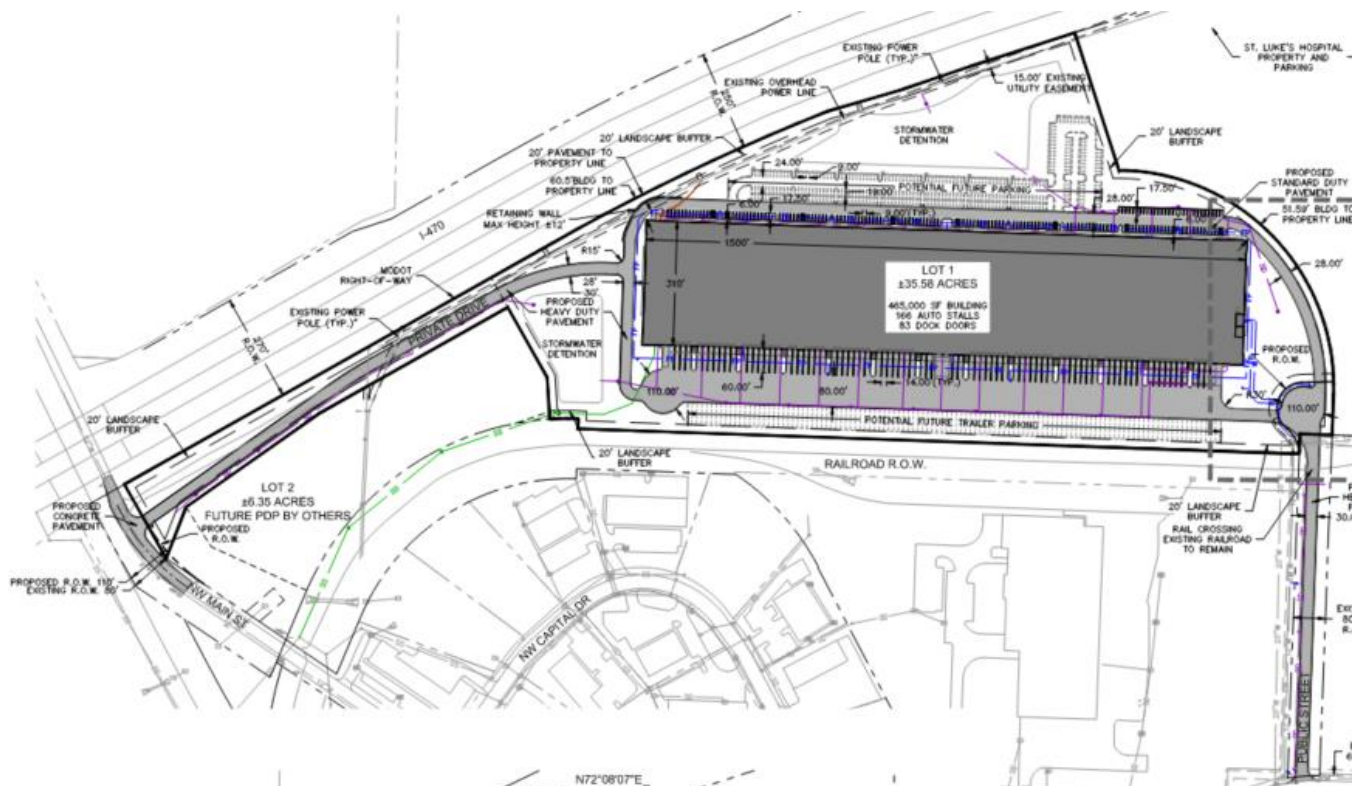


Figure 1 - Site plan

2. Land Use

Description and Character of Surrounding Area

The site makes up the area south of I-470 bounded by NW Main St to the west, the Saint Luke's East Hospital campus to the east and Union Pacific rail spur serving the NW Victoria Dr area to the south. NW Main St north of NW Tudor Rd has existing industrial development and recently had a logistics center approved. The adjacent NW/NE Victoria Dr is an industrial area that is nearly built out.

Adjacent Land Uses and Zoning

North:	I-470
South:	Industrial / PI
East:	Saint Luke's East Hospital campus / CP-2
West (across NW Main St):	Union Pacific railroad line

Site Characteristics

The 41-acre site is heavily wooded, except for a 7-acre clearing near the middle of the site. The site generally slopes from east to west. The site has street access from NW Main St along the west and has access to NW Victoria Dr via existing public right-of-way that connects to the southeast corner of the subject site.

Special Considerations

The area that makes up Lot 2 is located in a specified metal building area, which is more permissive in the type of metal panels that may be used for a building exterior. Lot 1 is not located within the boundaries of a specified metal building area.

3. Project Proposal

Site Design

Land Use	
Impervious Coverage:	49.9%
Pervious:	50.1%
TOTAL	100%

Parking (Lot 1)

Proposed		Required	
Total parking spaces proposed:	465 ¹	Total parking spaces required:	465
Accessible spaces proposed:	14	Accessible spaces required:	9
Parking Reduction requested?	No	Off-site Parking requested?	No

¹ – 166 parking spaces will initially be constructed for Lot 1. 299 parking spaces are land banked and will be constructed as needed based on actual tenant needs. The building has an additional 83 dock door spaces plus a land banked area for 111 future trailer parking spaces that are not included in the listed parking count.

Setbacks (Perimeter)

Yard	Building / Parking Required	Building / Parking Proposed
Front	20' (Building) / 20' (Parking)	248' (Building) / 128' (Parking)
Side	10' (Building) / 20' (Parking)	61' (Building) / 20' (Parking) – to the north; and 216' (Building) / 78' (Parking) – to the south
Rear	20' (Building) / 6' (Parking)	52' (Building) / 20' (Parking)

Structure(s) Design

Number and Proposed Use of Buildings (Lot 1)
1 building – logistics/distribution center
Building Height
44'-4"
Number of Stories
1 story

4. Unified Development Ordinance (UDO)

Section	Description
2.240,2.250,2.260	Rezoning
2.040,2.260,2.300,2.320	Preliminary Development Plans
4.080	Zoning Districts (PI)
8.790	Landscaping (Minimum Requirements)
2.320	Modifications

Unified Development Ordinance

The proposed logistics/distribution center is a use permitted by right under the proposed PI zoning district. The PI District is intended to provide areas for light manufacturing uses that primarily involve finishing or assembly of previously manufactured goods. The district is also intended to provide for the location of wholesaling, distribution or warehousing uses.

The proposed use is consistent and compatible with institutional, commercial and industrial uses in the surrounding CP-2 and PI-zoned properties along NE Main St, NE Victoria Dr and hospital campus.

5. Comprehensive Plan

Focus Areas	Goals, Objectives & Policies
Resilient Economy	Objective: Diversify Lee's Summit economy. Objective: Increase business retention and grow business activity. Objective: Maintain a diverse and valuable tax base.

Comprehensive Plan

The proposed logistics/distribution center is consistent with the surrounding commercial, industrial and mixed-use designations recommended by the Comprehensive Plan for the surrounding area.

6. Analysis

Background and History

- January 27, 2002 – The minor plat (Appl. #2001-112) of *Lee's Summit North Industrial Park, 7th Plat, Lots 9, 11 & 12* was recorded by the Jackson County Recorder of Deeds office by Instrument #2002-I-0006566. A 7-acre portion of the proposed development site is platted as Lot 12.
- December 15, 2011 – The City Council approved an annexation of 124.67 acres from Unity Village by Ordinance No. 7130. Approximately 34 acres of the subject 42-acre site were a part of the annexed property.

Compatibility

The proposed development abuts two industrial subdivisions (Lee's Summit North Industrial Park and Victoria Park) that sit along the NW Victoria Dr and NW Main St corridors. Existing area uses include office-warehouse, manufacturing, distribution, service providers, food products and general offices.

The proposed tilt-up concrete construction and building architecture are compatible with existing area industrial development and similar to recently approved logistics/distribution developments along NE Independence Ave, NW Main St (just north of NW Tudor Rd) and the M-291 Hwy/SE Bailey Rd area.

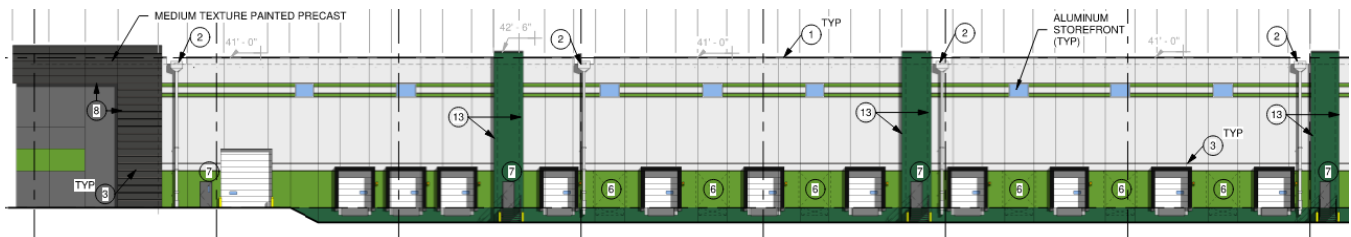


1 NE CORNER



2 NW CORNER

Figure 2 - Renderings of NE and NW perspectives



2 SOUTH ELEVATION - PARTIAL 1

Figure 3 – South Elevation (partial)

Adverse Impacts

The proposed development is not expected to detrimentally impact the surrounding area. The proposed project develops a vacant property along the I-470 corridor that is bounded by the NW Main St and NW Victoria Dr industrial corridors to the west and east.

The stormwater impacts will be mitigated through on-site detention. Two detention basins will serve Lot 1. One basin will be located west of the building; a second basin will be located north of the building. Stormwater mitigation for Lot 2 shall be determined at the time a final development plan is submitted for the lot's development, but shall comply with the City's Design and Construction Manual.

Public and Private Infrastructure Services

The subject property is a vacant site. The proposed development will not impede the normal and orderly development and improvement of the surrounding property. The property to the east is developed as a hospital campus. The property to the south is developed with two industrial subdivisions. The subject 42-acre site is the remaining undeveloped acreage south of I-470, east of NW Main St and north of NW Victoria Dr.

Sanitary sewer service for the site is provided by an existing 16" main located along NW Main St. Access to water will come from an existing 12" main located off the SE corner of the site.

Stormwater from the proposed development will be managed by two on-site detention basins. The north detention basin will discharge onto MoDOT's I-470 right-of-way. The west detention basin will discharge on-site near the driveway connection to NW Main St where it will drain to an existing box culvert under NW Main St that ultimately drains to Little Cedar Creek.

Unimproved Road Policy (URP) – NW Main St

Existing Conditions. NW Main St is currently a two-lane undivided commercial collector with a 35-mph speed limit. Following the construction of the previously approved Lee's Summit Logistics development near the intersection of NW Main St and NW Tudor Rd, NW Main St will be improved to include a center two-way-left-turn-lane (TWLTL), sidewalk, and curb and gutter south of the railroad spur that borders the subject Summit 470 Logistic Center application. NW Main St is considered an unimproved road from the railroad spur north to NW Colbern Rd, which includes street frontage for the subject development.

NW/NE Victoria Dr is a two-lane, undivided commercial collector with a 25-mph speed limit. The cross-section of NW/NE Victoria Dr is currently constructed with two 15' lanes with curb and gutter from NW Main St to NE Douglas St with intermittent sidewalk.

The intersection of NE Victoria Dr and NE Douglas St is signal-controlled, while the intersection of NW Victoria Dr and NW Main St is stop-controlled along NW Victoria Dr. The intersection of NW Main St and NW Tudor Rd is also stop-controlled and will remain that way following the completion of the previously approved Lee's Summit Logistics development. The intersection of NW Main St and NW Colbern Rd is currently stop-controlled.

Policy. The URP is intended to provide direction to staff when considering proposed development with the intent to provide a safe travel way to and from development(s). The policy establishes criteria and

design standards intended to guide development activity impacting roadways constructed to unimproved and/or interim standards. The policy does allow for deviations by the City Engineer at their discretion with each development judged on its own merits. According to the policy, no residential subdivision, industrial or commercial developments shall be permitted on unimproved roads.

As previously stated under the “Existing Conditions” section above, the segment of NW Main St between NW Colbern Rd and the railroad spur that borders the subject development to the south is considered an unimproved road. The proposed development includes a driveway connection to the unimproved section of NW Main St, thus making the development subject to the policy and its guidance that NW Main St be improved to an urban standard in order to accommodate the proposed development. The City’s urban standard includes a minimum of two 12-foot travel lanes, curb, sidewalk, share-use path (when appropriate) and street lighting.

Transportation Impact Analysis (TIA). Given the existing unimproved road conditions of NW Main St north of the railroad spur to NE Colbern Rd plus the fact that the proposed development includes a full access driveway connection to NW Main St, the TIA prepared by staff recommends that the aforementioned unimproved section of NW Main St be improved to an urban standard per the URP.

Alternatively, staff would support the improvement to an urban standard of only the section of NW Main St from the railroad spur to a point parallel to the proposed development’s northern boundary, with the condition that the driveway connection to NW Main St be restricted to provide emergency access only via gated access or similar alternative. The driveway onto NW Main St could be made a full-access connection in the future when NW Main St is improved to an urban standard up to NW Colbern Rd.

Modifications

Street frontage landscaping along I-470. Modification requested. Staff supports the modification request.

- Required – 1 street tree shall be provided for every 30’ of street (right-of-way) frontage; and 1 street shrub shall be provided for every 20’ of street (right-of-way) frontage.
- Proposed – Existing vegetation within MoDOT right-of-way along the development’s I-470 frontage shall serve as the required street frontage landscaping.
- Recommended – The existing vegetation along the I-470 frontage of the subject property is heavy and mature. The vegetation includes a continuous tree line and a continuous and substantial understory of shrubs and small trees. MoDOT has provided correspondence stating that there is no intention to remove the existing vegetation along the right-of-way.

The purpose of the City’s landscaping requirements is to improve the aesthetic qualities of the city and to protect and preserve the appearance, character and value of its neighborhoods and businesses. Planting street frontage trees and shrubs on the subject property as required along I-470 would be ineffective because they would sit behind the existing I-470 vegetation and thus not be visible from the interstate. Staff supports the modification request to not require any additional street frontage landscaping along I-470 given the existing presence of heavy and mature vegetation within the I-470 right-of-way.

Recommendation

With the conditions of approval below, the application meets the requirements of the UDO and Design & Construction Manual.

7. Recommended Conditions of Approval

Site Specific

1. A modification shall be granted to the street tree and street shrub landscaping requirement, to allow that no further landscaping be required to be planted along the subject property's I-470 frontage.
2. Development shall be in accordance with the preliminary development plan dated December 13, 2022, revised preliminary development plan Site Plan (Sheet C4.0) dated February 1, 2023, and building renderings and elevations dated November 4, 2022.
3. Development shall include the construction of both the driveway connection to NW Main St and the driveway connection to NW Victoria Dr as depicted on the preliminary development plan dated December 13, 2022, in order to maintain adequate fire access.
4. In accordance with the City's Unimproved Road Policy, construction of a full-access driveway connection to NW Main St shall require the developer to improve NW Main St to an urban standard from a point approximately 150' south of the subject development (where improvements to NW Main St currently under way will terminate) north to NW Colbern Rd.
5. In lieu of condition #4 above, only the portion of NW Main St from a point approximately 150' south of the subject development to a point parallel to the development's northern boundary shall be required to be improved by the developer to an urban standard under the condition that the driveway connection to NW Main St is restricted to only emergency access via gated access or similar alternative acceptable to the City. Access restriction for the driveway connection onto NW Main St shall be removed at the time NW Main St is improved to an urban standard to NW Colbern Rd.
6. The Developer shall execute a mutually satisfactory development agreement with the City, which addresses, at a minimum, the off-site road improvement of NW Main St to NW Colbern Rd required under condition #4 above and addressed in the City Traffic Engineer's TIA dated January 31, 2023. No building permits shall be issued for any structure in the development until written proof is provided to the City that the development agreement has been recorded in the Jackson County Records' Office.
7. A cross-access easement through Lot 1 shall be provided for Lot 2 as Lot 2 cannot have direct access onto NW Main St and comply with the minimum curb cut separation requirements established under the City's Access Management Code.

Standard Conditions of Approval

8. All required engineering plans and studies, including water lines, sanitary sewers, storm drainage, streets and erosion and sediment control shall be submitted along with the final development plan. All public infrastructure must be substantially complete, prior to the issuance of any certificates of occupancy.
9. All Engineering Plan Review and Inspection Fees shall be paid prior to approval of the associated engineering plans and prior to the issuance of any infrastructure permits or the start of construction (excluding land disturbance permit).

10. All subdivision-related public improvements must have a Certificate of Final Acceptance prior to approval of the final plat, unless security is provided in the manner set forth in the City's Unified Development Ordinance (UDO) Section 7.340. If security is provided, building permits may be issued upon issuance of a Certificate of Substantial Completion of the public infrastructure as outlined in Article 3, Division V, Sections 3.540 and 3.550 and Article 3, Division IV, Section 3.475 of the UDO, respectively.
11. A Land Disturbance Permit shall be obtained from the City if ground breaking will take place prior to the issuance of an infrastructure permit, building permit, or prior to the approval of the Final Development Plan / Engineering Plans.
12. All permanent off-site easements, in a form acceptable to the City, shall be executed and recorded with the Jackson County Recorder of Deeds prior to the issuance of a Certificate of Substantial Completion or approval of the final plat. A certified copy shall be submitted to the City for verification.
13. Certain aspects of the development plan will be further reviewed during the Final Development Plan phase of the project. This includes detailed aspects of the design to help ensure that the plan meets the design criteria and specifications contained in the Design and Construction Manual.
14. Private parking lots shall follow Article 8 of the Unified Development Ordinance for pavement thickness and base requirements.
15. Any cut and / or fill operations, which cause public infrastructure to exceed the maximum / minimum depths of cover shall be mitigated by relocating the infrastructure vertically and / or horizontally to meet the specifications contained within the City's Design and Construction Manual.
16. All ADA sidewalk ramps shall be constructed by the developer at the time the street is constructed.
17. All sidewalks adjacent to a common area tract, unplatted land or any land where no structure is intended to be built, and is required, shall be constructed by the developer at the time the street is constructed.
18. All issues pertaining to life safety and property protection from the hazards of fire, explosion or dangerous conditions in new and existing buildings, structures and premises, and to the safety to fire fighters and emergency responders during emergency operations, shall be in accordance with the 2018 International Fire Code.
19. Provide a public/private hydrant within 100 feet of the FDC.
20. IFC 507.1 - An approved water supply capable of supplying the required fire flow for fire protection shall be provided to premises upon which facilities, buildings or portions of buildings are hereafter constructed or moved into or within the jurisdiction. Confirm water fire flow capacity at the site.
21. All exterior mechanical equipment, whether roof mounted or ground mounted, shall be entirely screened from view. Roof mounted equipment shall be screened by the parapet equal to the height of the mechanical equipment. Ground mounted equipment shall be screened by masonry wall or landscaping equal to the height of the units.
22. All wall-mounted exterior lighting and parking lot lighting shall comply with the requirements of UDO Sections 8.220, 8.250 and 8.260.
23. Accessible parking spaces shall be provided in conformance with the International Building Code at the time of final development plan.
24. Accessible parking signs shall meet the requirements set forth in the Manual on Uniform Traffic Devices

(R7-8). Each accessible parking space shall be identified by a sign, mounted on a pole or other structure, located 60 inches (5 feet) above the ground measured from the bottom of the sign, at the head of the parking space.

25. Sign permits shall be obtained prior to installation of any signs through the Development Services Department. All signs proposed must comply with the sign requirements as outlined in the sign section of the Unified Development Ordinance.
26. A final or minor plat shall be approved and recorded prior to any building permits being issued.