

# CONSTRUCTION PLANS FOR NW PRYOR ROAD SIGNAL IMPROVEMENTS & LANE WIDENING STREETS OF WEST PRYOR LEE'S SUMMIT, MISSOURI

Record Drawing

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Kent.Monter@cityofls.net



CALL BEFORE YOU  
DIG - DRILL - BLAST  
1-800-344-7483  
(TOLL FREE)

MISSOURI ONE CALL SYSTEM, INC.

#### UTILITY STATEMENT:

THE UNDERGROUND UTILITIES SHOWN HEREON ARE FROM FIELD SURVEY INFORMATION OF ONE-CALL LOCATED UTILITIES, FIELD SURVEY INFORMATION OF ABOVE GROUND OBSERVABLE EVIDENCE, AND/OR THE SCALING AND PLOTTING OF EXISTING UTILITY MAPS AND DRAWINGS AVAILABLE TO THE SURVEYOR AT THE TIME OF SURVEY. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SURFH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. FURTHERMORE, THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES BY EXCAVATION UNLESS OTHERWISE NOTED ON THIS SURVEY.

#### CAUTION - NOTICE TO CONTRACTOR

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXPOSE EXISTING UTILITIES AT LOCATIONS OF POSSIBLE CONFLICTS PRIOR TO ANY CONSTRUCTION.

#### SAFETY NOTICE TO CONTRACTOR

IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

#### WARRANTY / DISCLAIMER

THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER KAW VALLEY ENGINEERING, INC NOR ITS PERSONNEL CAN OR DO WARRANTY THESE DESIGNS OR PLANS AS CONSTRUCTED, EXCEPT IN THE SPECIFIC CASES WHERE KAW VALLEY ENGINEERING PERSONNEL INSPECT AND CONTROL THE PHYSICAL CONSTRUCTION ON A CONTEMPORARY BASIS AT THE SITE.

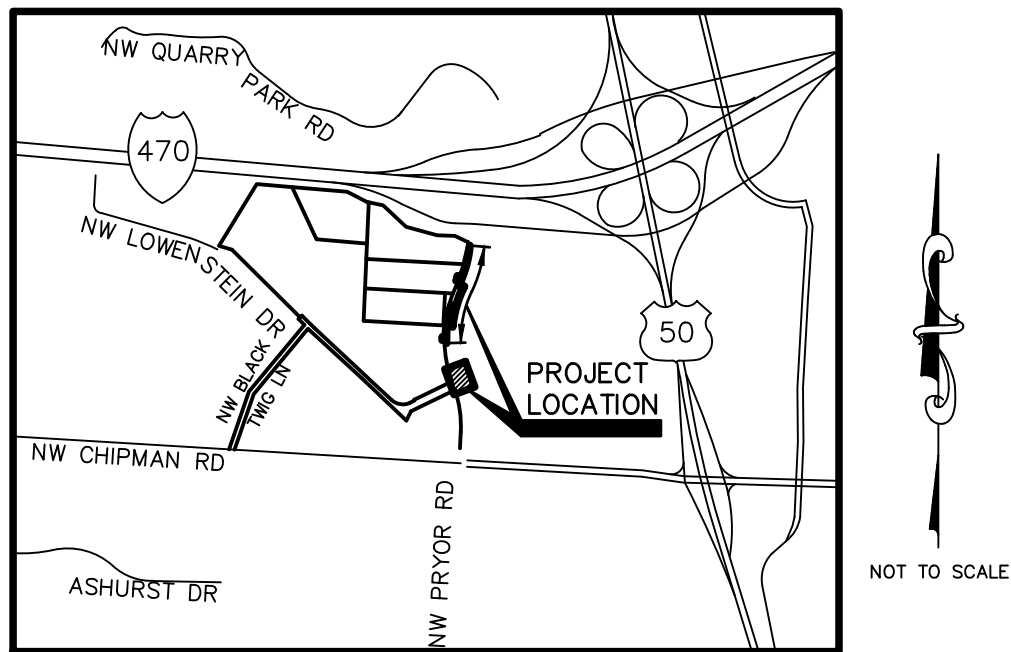
COMMUNICATION SERVICE  
AT&T  
CARRIE CILKE  
(816) 703-4386  
cc3527@att.com

COMMUNICATION SERVICE  
TIME WARNER CABLE  
STEVE BAXTER  
(913) 643-1928  
Steve.Baxter@charter.com

COMMUNICATION SERVICE  
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COMMUNICATION SERVICE  
GOOGLE FIBER  
BECKY DAVIS  
(913) 725-8745  
KC-Google-UC@google.com  
rebeccadavis@google.com

SUMMARY OF QUANTITIES			
Paving			
Item No.	Item	Estimated Quantity	Unit
1	2' Curb & Gutter (CG-1)	1080	L.F.
2	Asphaltic Cement Concrete Surface (2")	1240	S.Y.
3	Asphaltic Cement Concrete Surface (7.5")	1240	S.Y.
4	MoDOT Type 5 Base (Pavement)(12")	1570	S.Y.
5	Terragrid SX2020	1570	S.Y.
6	Concrete Entrance (Commercial)(8")	679	S.Y.
7	Aggregate Base (Commercial)(Driveway)(4")	679	S.Y.
8	Handicap Ramp	4	Ea.
9	Modular Block Retaining Wall	34	L.F.
10	Island Concrete (6")	54	S.F.
11	Concrete Sidewalk (4" RCM/B 4K Concrete)	484	S.Y.
12	Detectable Warning pad (pad only)	2'X3'	Size
Erosion Control			
Item No.	Item	Estimated Quantity	Unit
1	Erosion Control Devices, Sedimentation Fence	1148	L.F.
2	Erosion Control Devices, Curb Inlet Protection	4	Ea.
3	Erosion Control Devices, Area Inlet & Junction Box Protection	4	Ea.
4	Seeding	1	S.F.
Pavment Marking & Signing			
Item No.	Item	Estimated Quantity	Unit
1	Pavement Marking, High-build Paint (Solid White)(4")	574	L.F.
2	Pavement Marking, High-build Paint (Dashed White)(Extension)(6")	82	L.F.
3	Pavement Marking, High-build Paint (Solid White)(Cross-walk)(6")	220	L.F.
4	Pavement Marking, Thermoplastic (Solid White)(Stop Bar)(24")	62	L.F.
5	Pavement Marking, Thermoplastic (White)(Left/Right Arrow)	17	Ea.
6	Permanent Signs	32	S.F.
7	Sign Post (Square Steel Tube)	3	L.F.
Storm Sewer			
Item No.	Item	Estimated Quantity	Unit
1	Setback Curb Inlet (4'x3')	1	Ea.
2	RCP (24")/CLASS 3	42	L.F.
Traffic Signal			
Item No.	Item	Estimated Quantity	Unit
1	Traffic Signal System, NW Pryor & NW Summit Woods	1	L.S.
2	Traffic Signal System, NW Pryor & NW Lowenstein	1	L.S.
3	Interconnect Cable	2370	L.F.
Lighting			
Item	Item	Estimated Quantity	Unit
1	Light Pole Base	2	Ea.



LOCATION MAP  
CITY OF LEE'S SUMMIT, MISSOURI

I certify that I have reviewed the as built information provided on these record drawings and take no exception to the information provided by Emery Sapp and Sons, Inc.

Leon D. Osborn

September 2, 2020

APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, \_\_\_\_\_

OWNER: MATT PENNINGTON

APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, \_\_\_\_\_

CITY APPROVAL

OWNER:  
STREETS OF WEST PRYOR, LLC  
7200 WEST 132ND STREET  
OVERLAND PARK, KS 66213  
CONTACT: MATT PENNINGTON  
email: matt@drakekc.com

PREPARED BY:  
KAW VALLEY ENGINEERING, INC.  
2319 N. JACKSON  
JUNCTION CITY, KS 66441  
785-762-5040  
CONTACT: LEON D OSBOURN  
EMAIL: ldo@kveg.com



DEVELOPER:  
STREETS OF WEST PRYOR, LLC  
7200 WEST 132ND STREET  
OVERLAND PARK, KS 66213  
AGENT: DAVID N. OLSON  
email: daveolson@monarchprojectllc.com

DATUM BENCHMARK:  
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING OPUS PROJECTS ON PROJECT CONTROL.

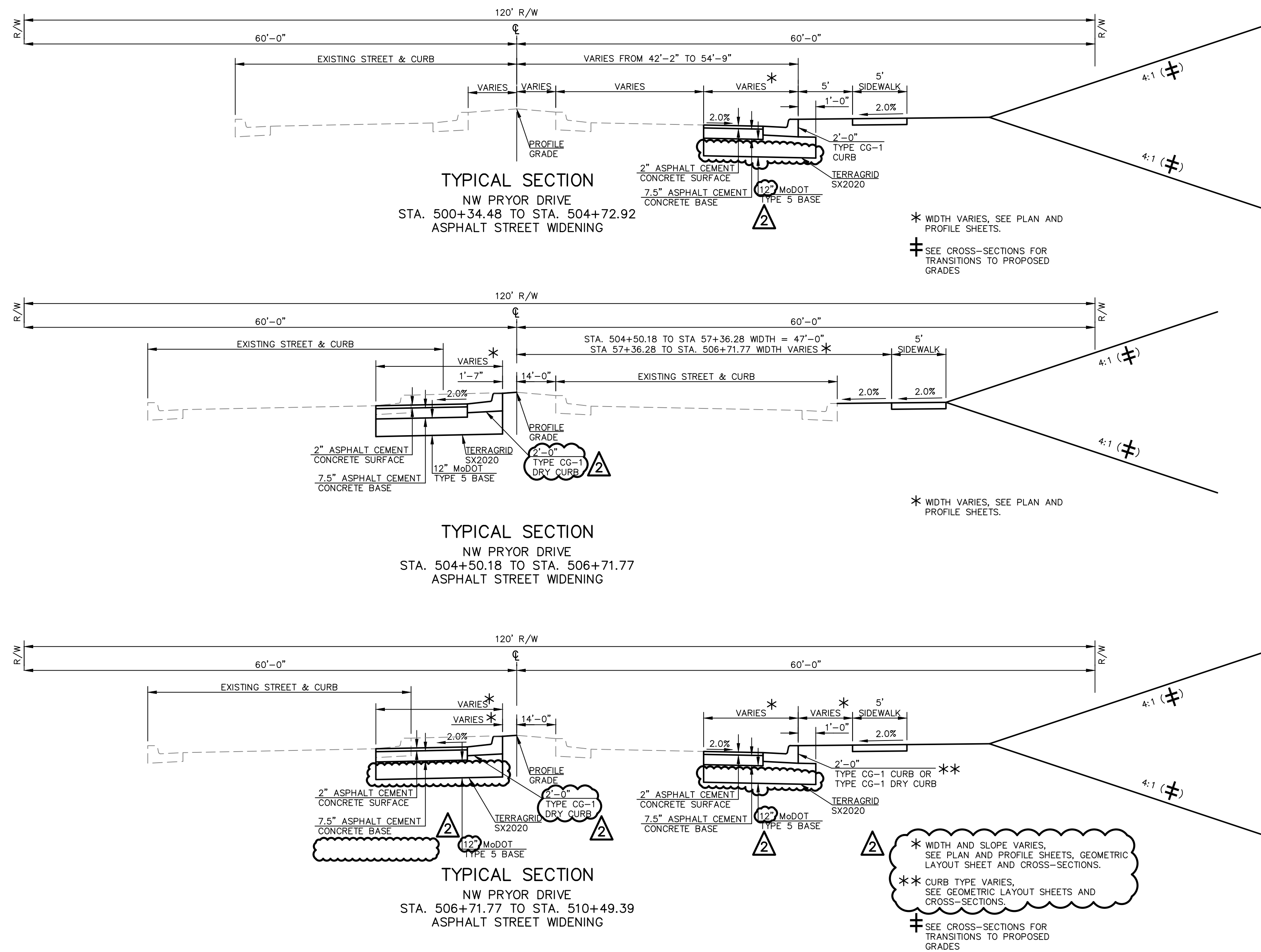
BENCHMARKS:  
BM #1: CHISELED "SQUARE" ON TOP OF CURB POINT OF INTERSECTION OF WEST PARK PARKING LOT AT EAST DRIVE ENTRANCE. ELEV=984.97  
BM #2: CHISELED "SQUARE" ON NORTHWEST CORNER AREA INLET, 25'± EAST OF CURB LINE AND ON-LINE WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90° BEND IN ROAD. ELEV=970.98























































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- STREET LIGHT STANDARD CITY DETAILS
- EROSION CONTROL DETAIL SHEET
- SHEET NOT USED
- NW PRYOR ROAD - CROSS-SECTIONS
- NW PRYOR ROAD - CROSS-SECTIONS

STREETS OF WEST PRYOR NW& NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		2319 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX (785) 762-7744 jdo@kveg.com   www.kveg.com		 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19				LEON D. OSBOURN ENGINEER MO # 021726											
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING TITLE SHEET		PROJ. NO. <b>A14-7067-1</b>		DESIGNER <b>LDO</b>		DRAWN BY <b>JT/BKR</b>		5		10-18-19		REVISED PER CITY COMMENTS		LDO		JT		LDO	
SHEET		7067-1SIG_TS		CFN		1		REV		5		10-3-19		REVISED PER CITY COMMENTS		LDO		JT	
5												7-24-19		REVISED PER CITY COMMENTS		LDO		JT	
												7-2-19		REVISED PER CITY COMMENTS		LDO		JT	
												0		INITIAL ISSUE		LDO		JT	
												1		REVISED PER CITY COMMENTS		LDO		JT	
												0		5-23-19		LDO		JT	
												REV		DATE		DSN		DWN	



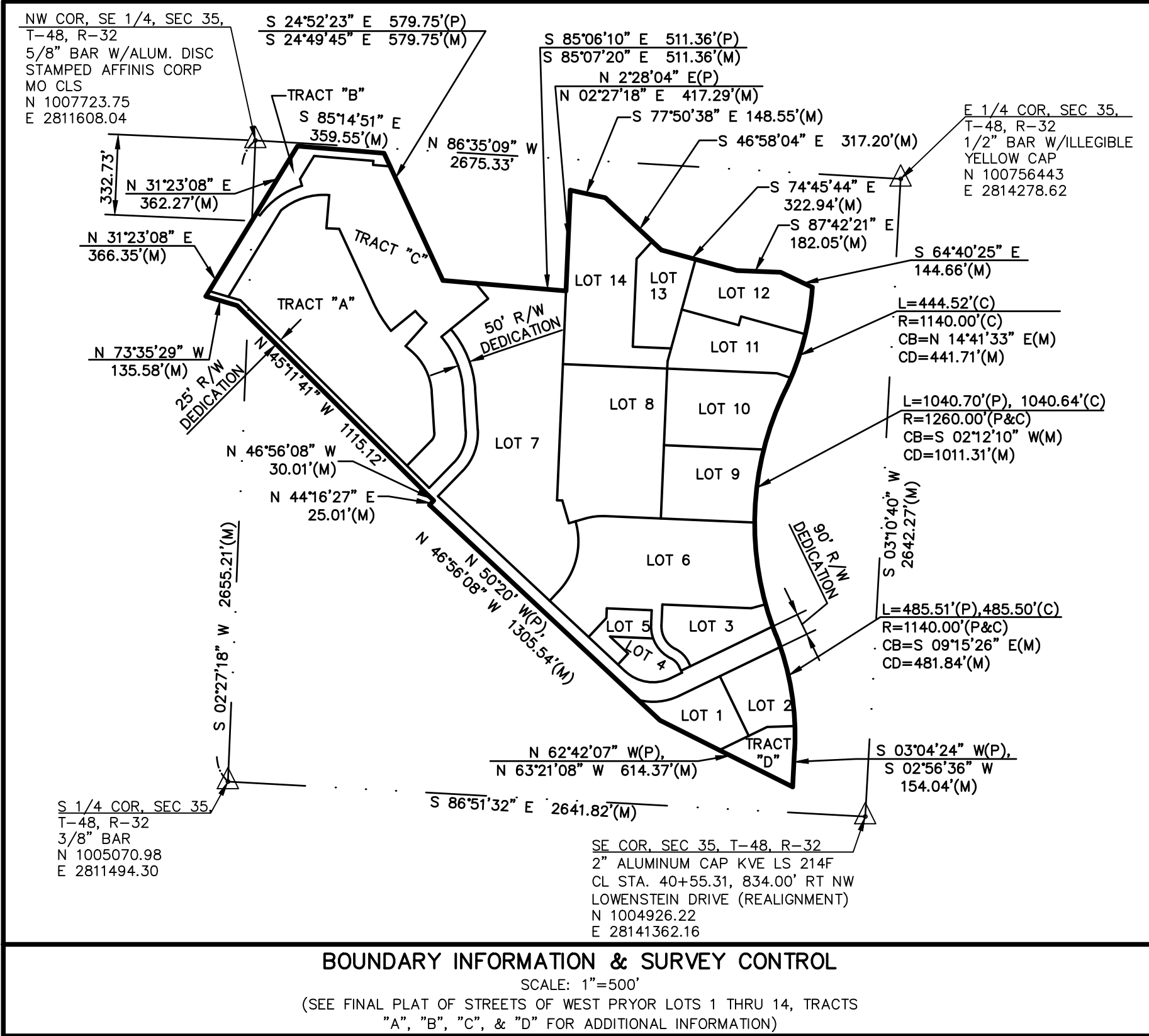
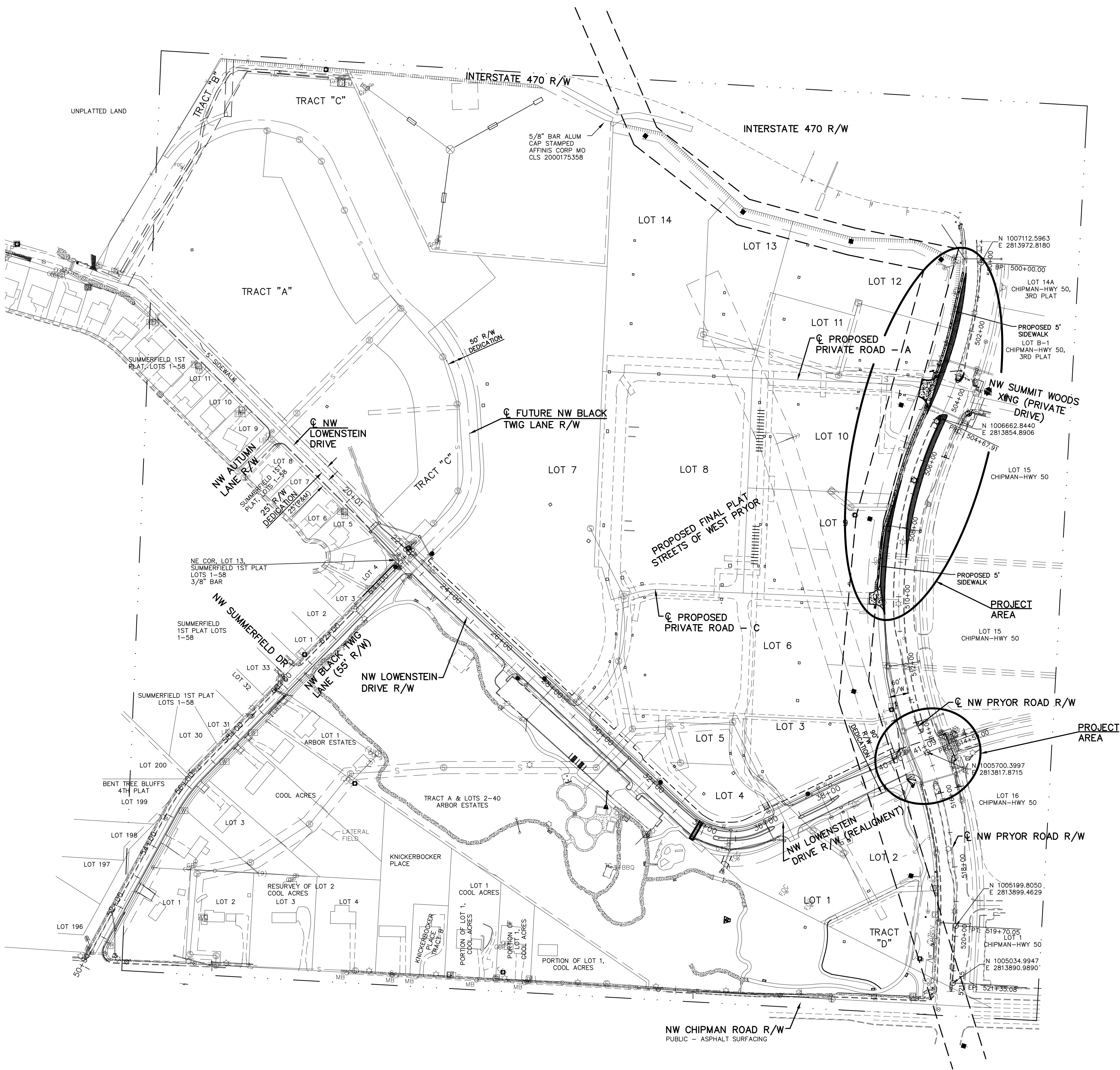


LEGEND			
	SECTION CORNER, ORIGIN UNKNOWN UNLESS OTHERWISE NOTED		STORM SEWER MANHOLE
	MONUMENT FOUND, ORIGIN UNCERTAIN UNLESS OTHERWISE NOTED		TELEPHONE SIGN
<b>(D)</b>	DESCRIBED		TELEPHONE MANHOLE
<b>(M)</b>	MEASURED		TELEPHONE PEDESTAL
<b>(C)</b>	CALCULATED		UNDERGROUND TELEPHONE LINE
<b>(P)</b>	PLATTED		SPLICE BOX
	STREET SIGN		FIBER OPTIC CABLE SIGN
	UTILITY POLE		UNDERGROUND FIBER OPTIC CABLE
	UTILITY POLE W/ LIGHT		TRAFFIC CONTROL POLE
	UTILITY POLE W/TRANSFORMER		PULL BOX
	LIGHT POLE		FLAG POLE
	DEADMAN ANCHOR		MAILBOX
<b>(5)</b> 	OVERHEAD UTILITY - # LINES		HANDICAP SIGN
	AIR CONDITIONING UNIT		HANDICAP PAINTED SYMBOL
	ELECTRIC PEDESTAL		LEFT TURN ARROW
	ELECTRIC METER		STRAIGHT ARROW
	UNDERGROUND ELECTRIC LINE		RIGHT TURN ARROW
	UTILITY MANHOLE		GATE POST
	CABLE TV SIGN		FENCE POST
	CABLE TV PEDESTAL		WOOD FENCE
	GAS SIGN		CHAIN LINK FENCE
	GAS METER		BARBED WIRE FENCE
	UNDERGROUND GAS LINE		DECIDUOUS TREE W/SIZE & DRIP LINES
	GAS CATHODIC PROTECTION STATION		EVERGREEN TREE W/SIZE & DRIP LINES
	WATER LINE		SAPPLING TREE
	WATER LINE GATE VALVE		SHRUB
	WATER SPIGOT		STUMP
	WATER METER		TREE LINE
	WELL		SHRUB LINE
	FIRE HYDRANT		PARKING STALL COUNT
	SANITARY SEWER MANHOLE		1' CONTOUR INTERVAL
	SANITARY SEWER LINE		RESTRICTED ACCESS
			BACK OF CURB TO BACK OF CURB
			EDGE TO EDGE

<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		<b>PROJ. NO.</b> <b>A14 7067-1</b>			
<b>DESIGNER</b> <b>LDO</b>		<b>DRAWN BY</b> <b>JT/BKR</b>			
<b>CFN</b> <b>7067-1SIG_TYP</b>					
<b>SHEET</b> <b>2</b>		<b>REV</b> <b>2</b>			

THIS DRAWING SHALL NOT BE UTILIZED BY ANY PERSON, FIRM, OR CORPORATION IN WHOLE OR IN PART WITHOUT THE SPECIFIC PERMISSION OF KAW VALLEY ENGINEERING, INC.





REV	DATE	DESCRIPTION	INITIAL	ISSUE	DATE	DESCRIPTION	INITIAL	ISSUE	DATE	DESCRIPTION
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO	CHK				

LEON D. OSBOURN  
ENGINEER  
MO # 021726

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JUNCTION CITY, KANSAS 66441  
PH. (785) 762-5040 | FAX (785) 762-7744  
joe@kve.com | www.kve.com

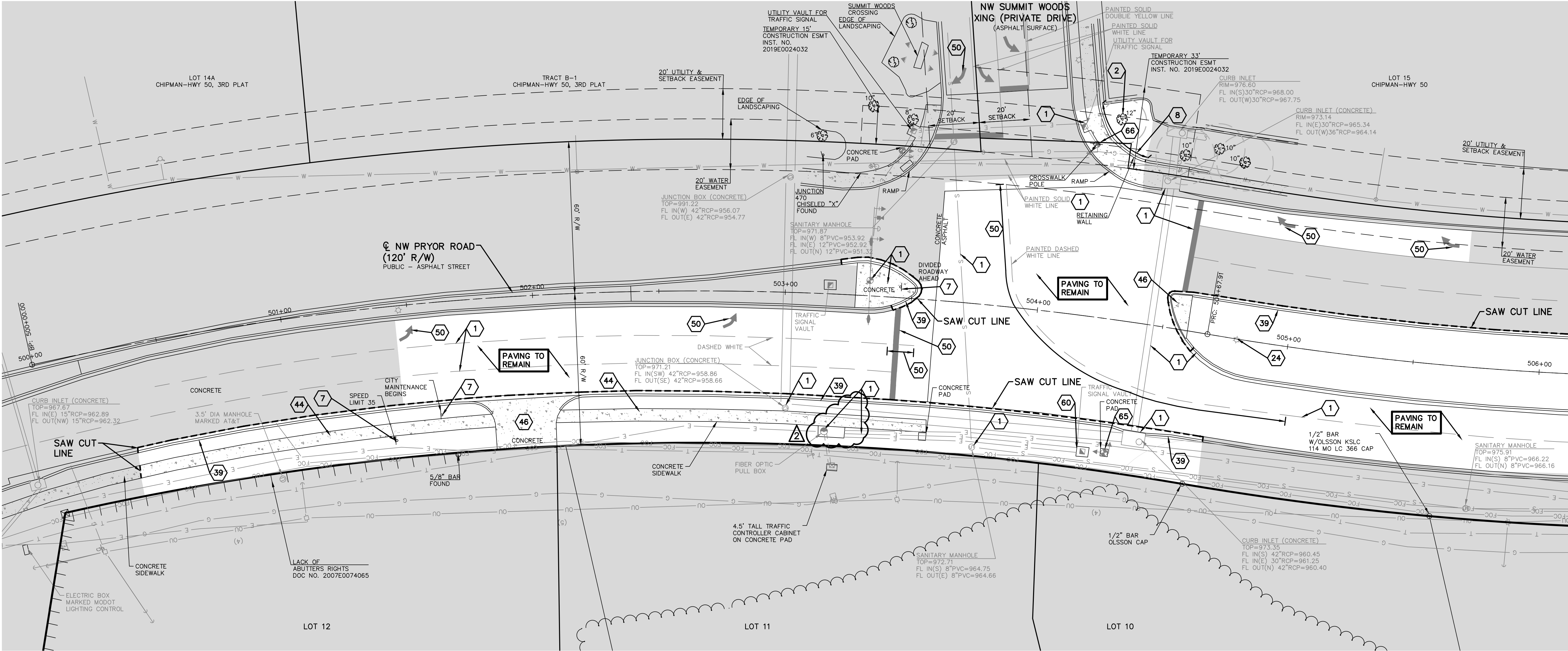
**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN MISSOURI. STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19.

**STREETS OF WEST PRYOR**  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
GENERAL LAYOUT SHEET**

PROJ. NO. <b>A14-7067-1</b>	
DESIGNER <b>LDO</b>	DRAWN BY <b>JT/BKR</b>
CFN <b>7067-1SIG_GLS</b>	REV
SHEET <b>3</b>	<b>0</b>





**DEMOLITION NOTES:**

- |    |  |
|----|--|
| 1  | TO REMAIN  |
| 2  | REMOVE TREE (SEE PLANS)  |
| 3  | TREES TO REMAIN  |
| 7  | SIGN TO BE RELOCATED   |
| 8  | PORTION OF RETAINING WALL TO BE REMOVED (SEE PLANS)                |
| 11 | TO BE REMOVED BY ELECTRIC COMPANY                                  |
| 24 | STREET LIGHT TO BE RELOCATED (SEE PLANS)                           |
| 39 | CONTRACTOR TO REMOVE (CURBS)                                       |
| 44 | CONTRACTOR TO REMOVE (SIDEWALK)                                    |
| 46 | CONTRACTOR TO REMOVE (PAVING)                                      |
| 50 | CONTRACTOR TO SAND BLAST STRIPING (SEE PAVEMENT MARKING SHEETS)    |
| 65 | TRAFFIC POLE TO BE RELOCATED. (SEE PLANS)                          |
| 66 | TRAFFIC POLE TO BE REMOVED AND RETURNED TO THE CITY OF LEES SUMMIT |

## LEGEND

- NOT A PART OF DEMOLITION ACTIVITIES
- SAW CUT LIMITS

**DATUM BENCHMARK:**  
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING  
OPUS PROJECTS ON PROJECT CONTROL.

**BENCHMARKS:**  
BM #1: CHISELED "SQUARE" ON TOP OF CURB POINT  
OF INTERSECTION OF WEST PARK PARKING LOT AT  
EAST DRIVE ENTRANCE. ELEV=984.97

BM #2: CHISELED "SQUARE" ON NORTHWEST CORNER  
AREA INLET, 25'± EAST OF CURB LINE AND ON-LINE  
WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90°  
BEND IN ROAD. ELEV=970.98

**CAUTION – NOTICE TO CONTRACTOR**  
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**SAFETY NOTICE TO CONTRACTOR**  
IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

GENERAL NOTES:

1. CONTRACTOR SHALL VERIFY SITE CONDITIONS PRIOR TO BIDDING. CONTRACTOR SHALL REMOVE ALL BUILDINGS, UTILITIES, PAVEMENT, FOUNDATIONS, FENCES, CURBS AND ALL OTHER STRUCTURES FROM WITHIN PROPERTY LINES EXCEPT AS DESIGNATED "TO REMAIN" OR "TO BE REMOVED BY OTHERS", IN ACCORDANCE WITH THE SPECIFICATIONS AND THE CITY OF LEE'S SUMMIT AND STATE REGULATIONS. SITE CONDITIONS SHOWN WERE AS OF MARCH 30, 2018.

2. ALL UTILITY PIPE LINES TO BE ABANDONED SHALL BE PLUGGED PER CITY AND STATE REGULATIONS.

3. DRIVES, PAVING AND OTHER STRUCTURES ON STREET OR HIGHWAY RIGHT-OF-WAY SHALL BE REMOVED AS NECESSARY TO CONSTRUCT IMPROVEMENTS SHOWN ON THESE PLANS. REMOVAL AND DISPOSAL SHALL BE IN CONFORMANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.

4. ALL PAVING WITHIN PROPERTY TO BE REMOVED AND DISPOSED OF IN CONFORMANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.

5. ALL HAZARDOUS ASBESTOS AND OTHER HAZARDOUS MATERIALS MUST BE IDENTIFIED AND REMOVED PRIOR TO ANY BUILDING DEMOLITION, IN STRICT CONFORMANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.

6. CONTRACTOR SHALL VERIFY THAT ALL UTILITIES TO EXISTING STRUCTURES HAVE BEEN DISCONNECTED PRIOR TO COMMENCING DEMOLITION.

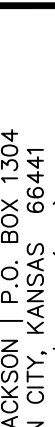
7. EXISTING POWER LINES AND APPURTENANCES TO BE RELOCATED BY KANSAS CITY POWER & LIGHT.

8. TREE LINES AND INDIVIDUAL TREES SHOWN ARE BASED ON ORIGINAL SURVEY. INITIAL CLEARING AND GRUBBING HAS BEEN COMPLETED. CONTRACTOR SHALL REMOVE ANY ADDITIONAL TREES AND SHRUB IN AREAS INDICATED FOR GRADING AND DEMOLITION.

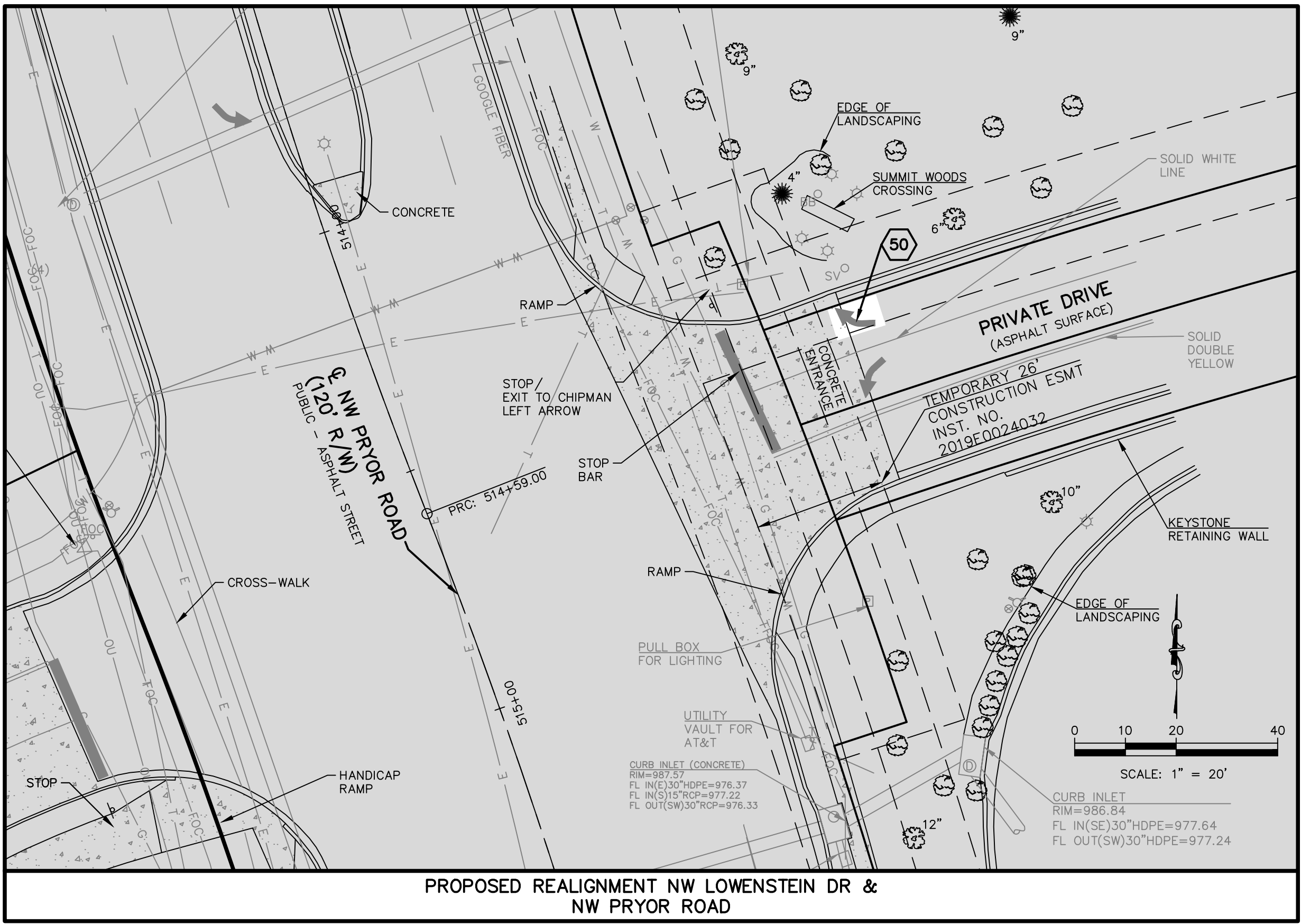
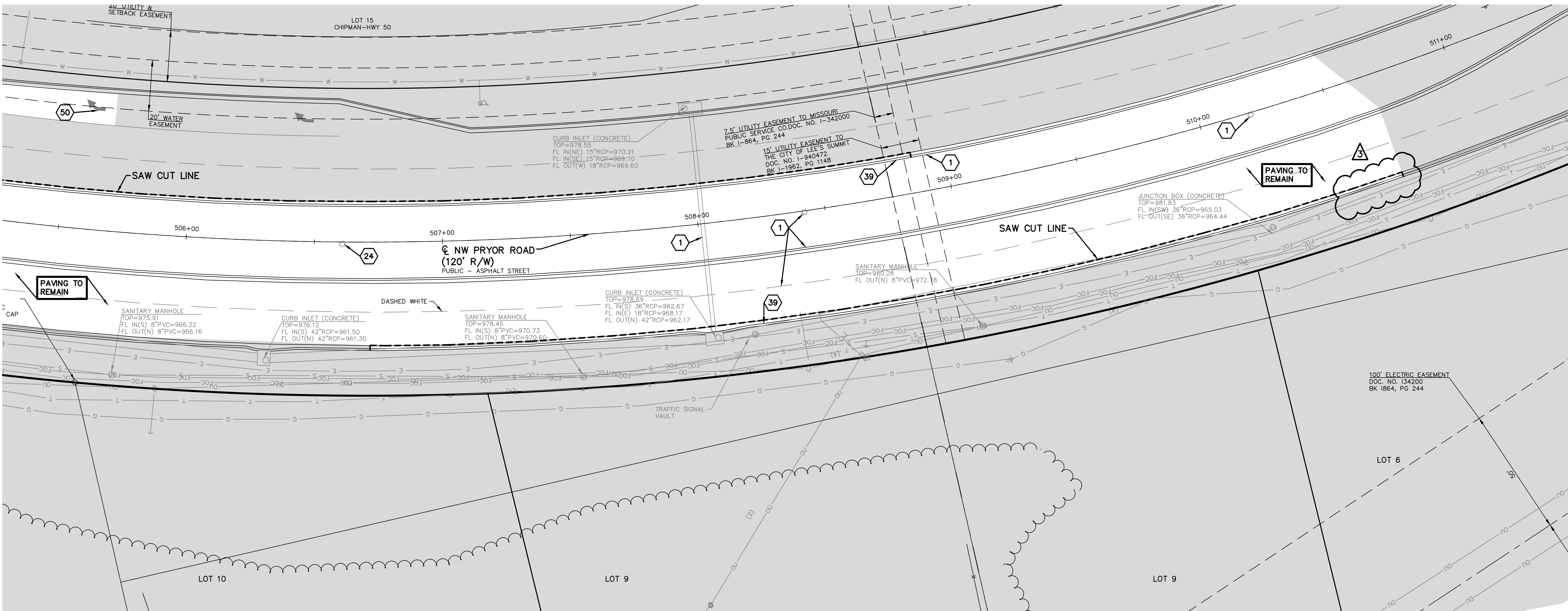
9. COORDINATE WITH KAW VALLEY ENGINEERING PRIOR TO REMOVAL OF SECTION MONUMENTS AND PROPERTY PINS. KAW VALLEY ENGINEERING WILL RESET NECESSARY MONUMENTS WITHIN THE DISTURBED AREAS FOLLOWING DEMOLITION. CARE SHALL BE TAKEN TO PRESERVE PROPERTY PINS ALONG THE OUTSIDE PERIMETER OF THE SITE.

10. CONTRACTOR SHALL OBTAIN DEMOLITION PERMITS FOR EACH INDIVIDUAL HOUSE WITHIN THE AREA SHOWN. ALL SERVICE LINE SHUT-OFFS, WELL PLUGGING, SEPTIC TANK REMOVALS AND OTHER UTILITY REMOVALS SHALL BE HANDLED IN ACCORDANCE WITH STATE AND CITY CODES.

11. KCP&L TRANSMISSION MAIN SHALL REMAIN IN PLACE DURING DEMOLITION. FOLLOW ALL KCP&L REQUIREMENTS CONCERNING WORK IN THEIR EASEMENT AND IN PROXIMITY TO THEIR LINES, INCLUDING PROTECTION OF POLES AND SAFE WORKING DISTANCES FROM LINES.

<b>STREETS OF WEST PRYOR</b> <b>NW/4 NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		PROJ. NO. <b>A14-7067-1</b>	
		DESIGNER <b>LDO</b>	DRAWN BY <b>JT/KBR</b>
		C/F/N <b>7067-1SG DEMO</b>	
		SHEET <b>4</b>	REV <b>2</b>
 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000642. EXPIRES 12/31/19.		LEON D. OSBOURN ENGINEER MO # 021726	
		2310 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX. (785) 762-7744 jck@kveeng.com   www.kveeng.com	
		2 8-14-19 REVISED PER CITY COMMENTS	
		1 7-2-19 REVISED PER CITY COMMENTS	
0 5-23-19 INITIAL ISSUE		LDO JT LDO	
REV DATE DESCRIPTION		DSN DWN CHK	





- ### DEMOLITION NOTES:
- |    |  |
|----|--|
| 1  | TO REMAIN  |
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### LEGEND

NOT A PART OF DEMOLITION ACTIVITIES


--- SAW CUT LIMITS

**DATUM BENCHMARK:**  
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OPUS PROJECTS ON PROJECT CONTROL.

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OF INTERSECTION OF WEST PARK PARKING LOT A  
EAST DRIVE ENTRANCE. ELEV=984.97

BM #2: CHISELED "SQUARE" ON NORTHWEST COR  
AREA INLET, 25'± EAST OF CURB LINE AND ON-1  
WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90'  
BEND IN ROAD. ELEV=970.98

<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		PROJ. NO. <b>A14-7067-1</b>	
		DESIGNER <b>LDO</b>	DRAWN BY <b>JT/BKR</b>
CFIN <b>7067-1SG_DEMO</b>		REV	
SHEET <b>5</b>		REV <b>3</b>	

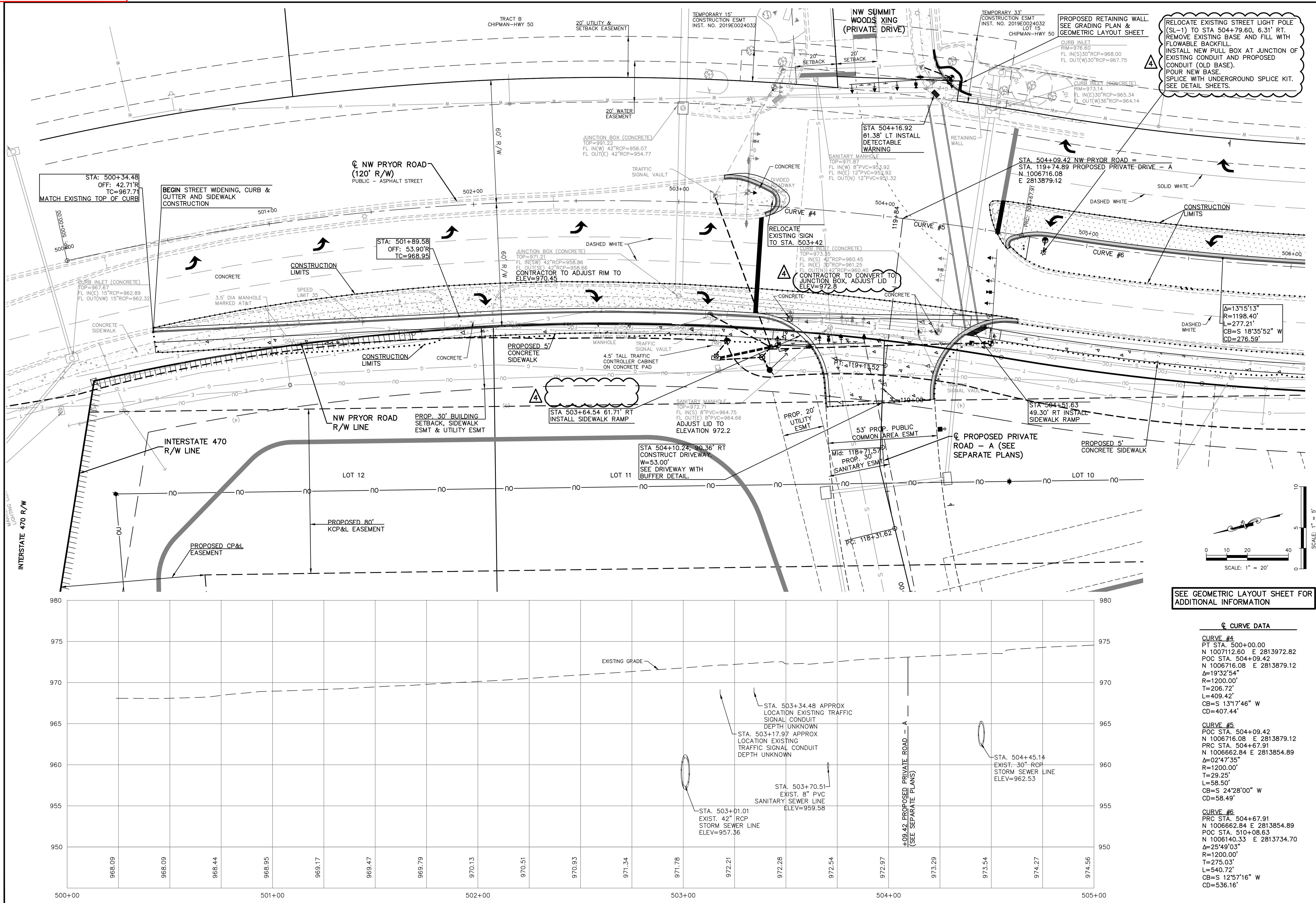




**KAW VALLEY ENGINEERING**  
 KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000642. EXPIRES 12/31/19

2319 N. JACKSON | P.O. BOX 1304  
 JUNCTION CITY, KANSAS 66441  
 PH. (785) 762-5040 | FAX (785) 762-7744  
 jk@kveg.com | www.kveg.com

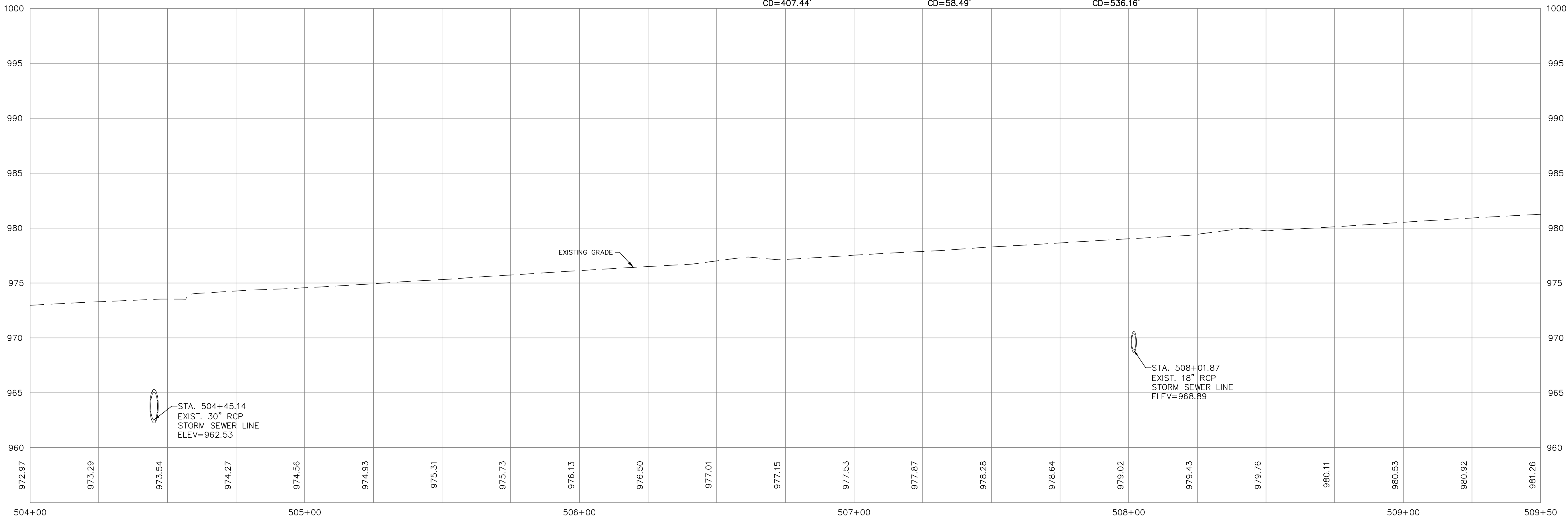
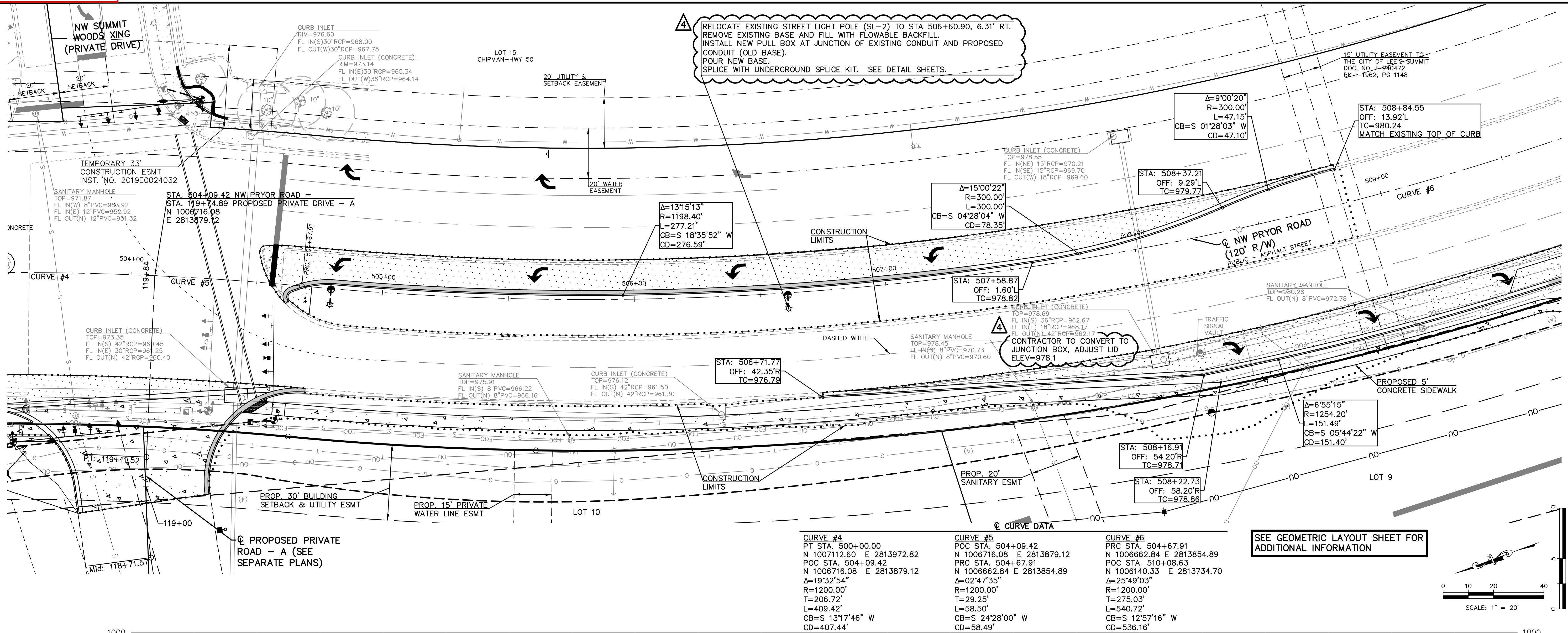
LEON D. OSBOURN									
ENGINEER									
MO # 021726									
		3	10-3-19	REVISED PER CITY COMMENTS	LDO	JT	LDO		
		2	8-14-19	REVISED PER CITY COMMENTS	LDO	JT	LDO		
		1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO		
		0	5-23-19	INITIAL ISSUE	LDO	JT	LDO		
		REV	DATE	DESCRIPTION	DSN	DWN	CHK		





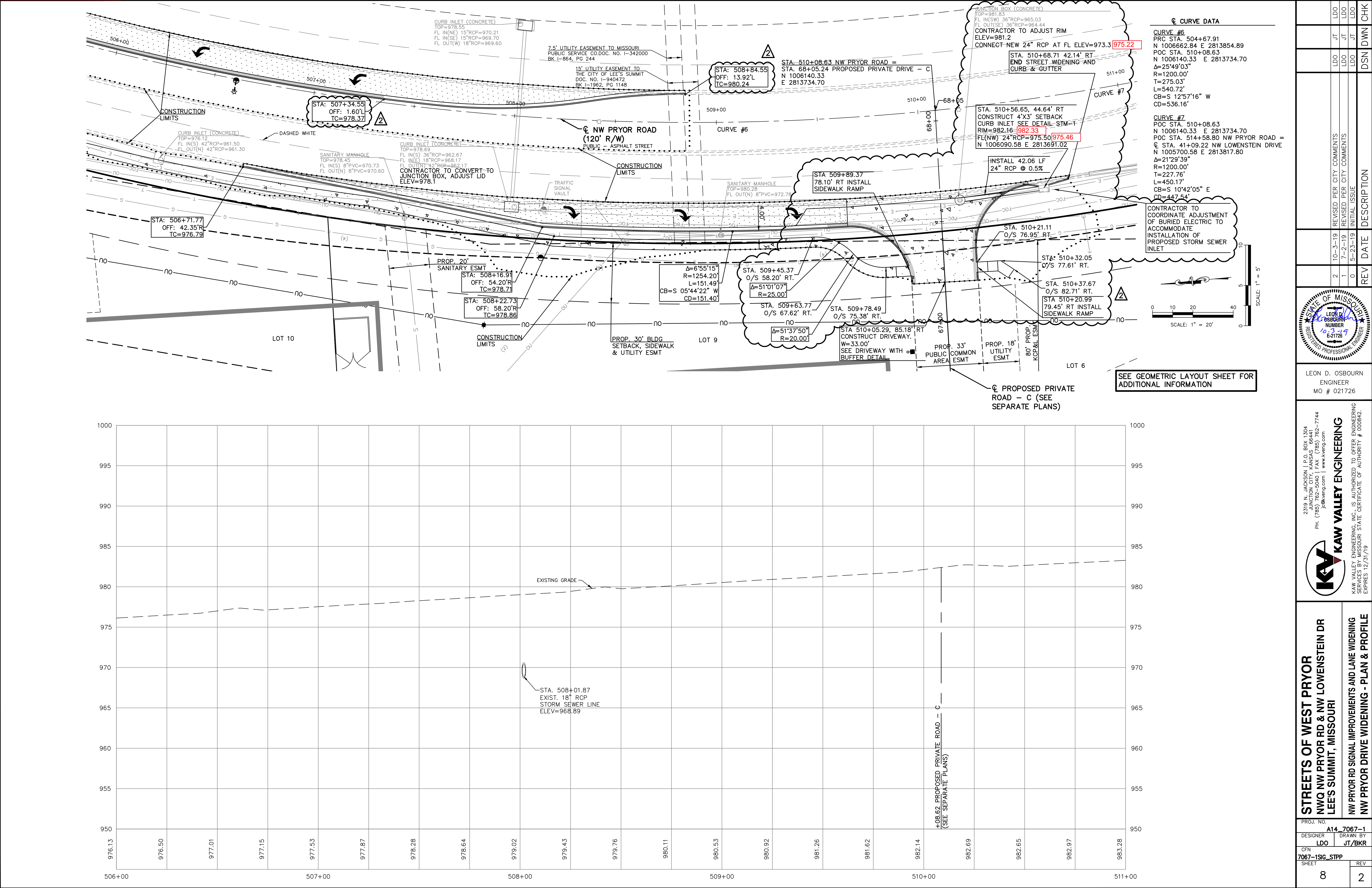
<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		2319 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX (785) 762-7744 <a href="http://jckweng.com">jckweng.com</a>   <a href="http://www.kvw.com">www.kvw.com</a>		 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19	
<b>PROJ. NO.</b> <b>A14_7067-1</b>		<b>DESIGNER</b> <b>LDG</b>		<b>DRAWN BY</b> <b>JT/BKR</b>	
<b>CFN</b> <b>7067-1SG STPP</b>		<b>SHEET</b> <b>6</b>		<b>REV</b> <b>4</b>	
<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>				<b>REV</b> <b>DATE</b> <b>DESCRIPTION</b> 4   10-3-19   REVISED PER CITY COMMENTS 3   8-14-19   REVISED PER CITY COMMENT 2   7-24-19   REVISED PER CITY COMMENTS 1   7-2-19   REVISED PER CITY COMMENTS 0   5-23-19   INITIAL ISSUE	
<b>PROJ. NO.</b> <b>A14_7067-1</b>		<b>DESIGNER</b> <b>LDG</b>		<b>DRAWN BY</b> <b>JT/BKR</b>	
<b>CFN</b> <b>7067-1SG STPP</b>		<b>SHEET</b> <b>6</b>		<b>REV</b> <b>4</b>	





<b>STREETS OF WEST PRYOR</b>											
<b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b>											
<b>LEE'S SUMMIT, MISSOURI</b>											
<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b>											
<b>NW PRYOR DRIVE WIDENING - PLAN &amp; PROFILE</b>											
PROJ. NO.											
A14 7067-1											
DESIGNER						DRAWN BY					
LDO						JT/BKR					
CFN											
7067-1SG_STPP											
SHEET						REV					
7											
						3					





THIS DRAWING SHALL NOT BE UTILIZED BY ANY PERSON, FIRM, OR CORPORATION IN WHOLE OR IN PART WITHOUT THE SPECIFIC PERMISSION OF KAW VALLEY ENGINEERING, INC.

REV	DATE	DESCRIPTION	CHK
2	10-3-19	REVISED PER CITY COMMENTS	LDL
1	7-2-19	REVISED PER CITY COMMENTS	LDL
0	5-23-19	INITIAL ISSUE	LDL

LEON D. OSBOURN  
ENGINEER  
MO # 021726

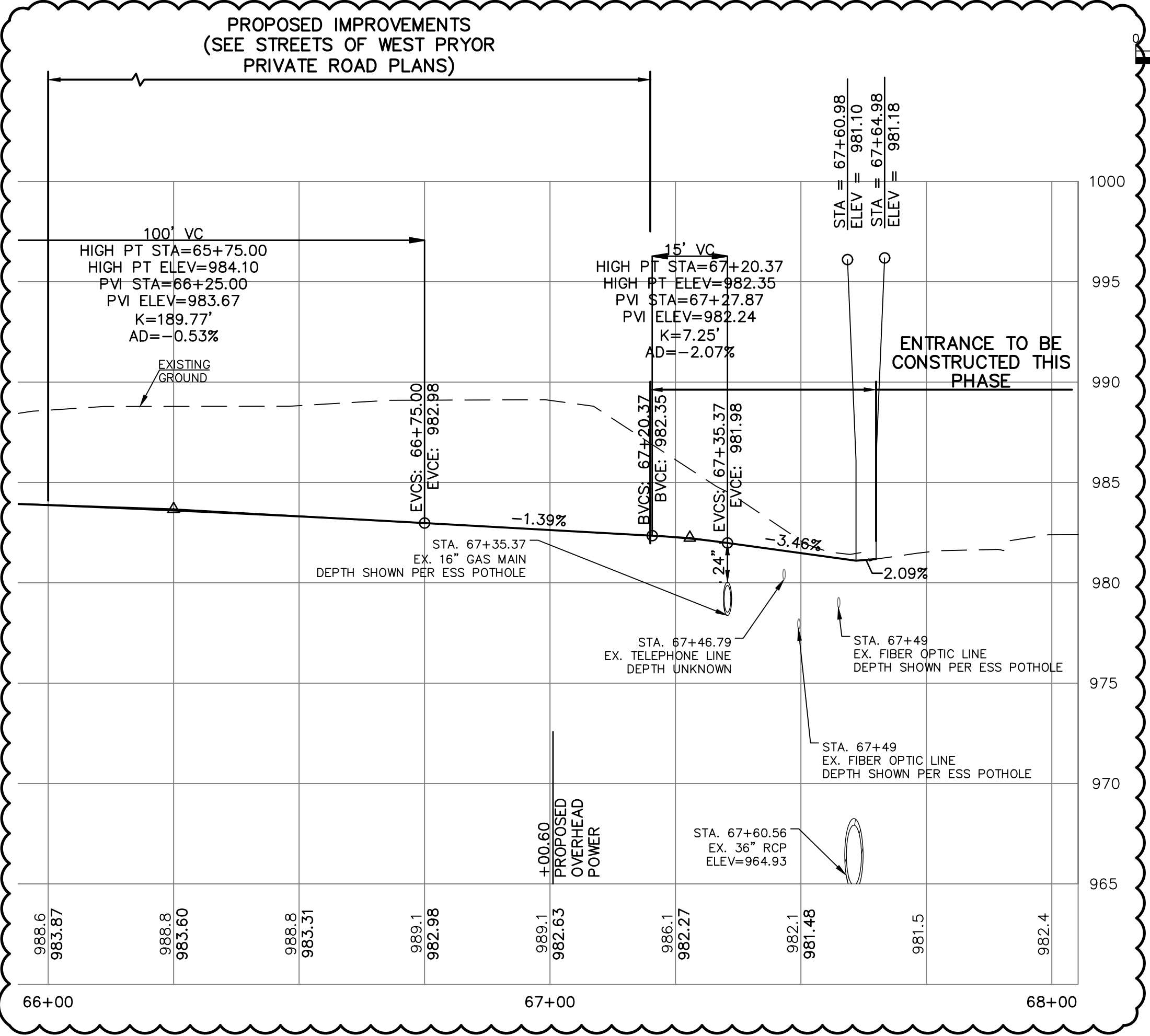
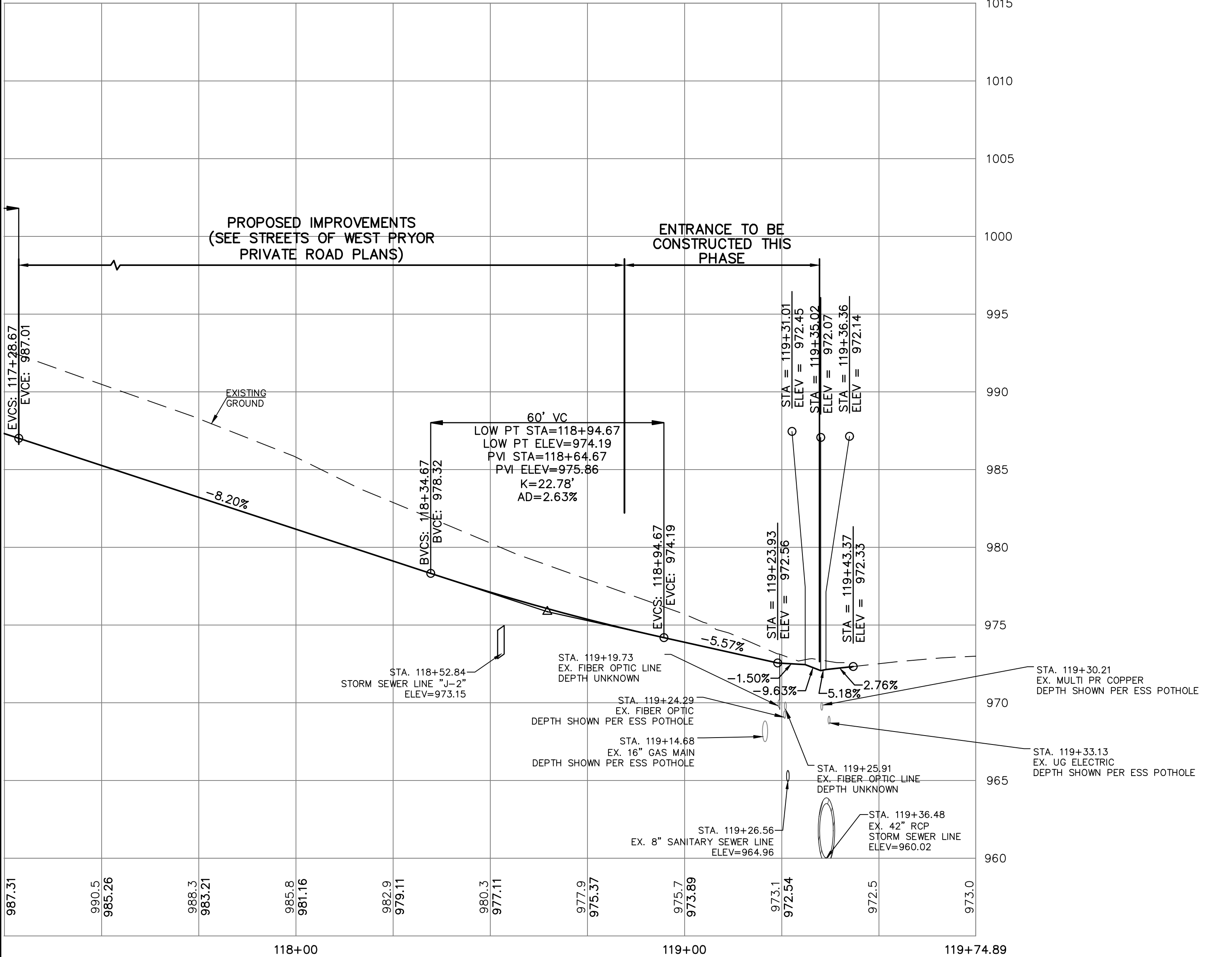
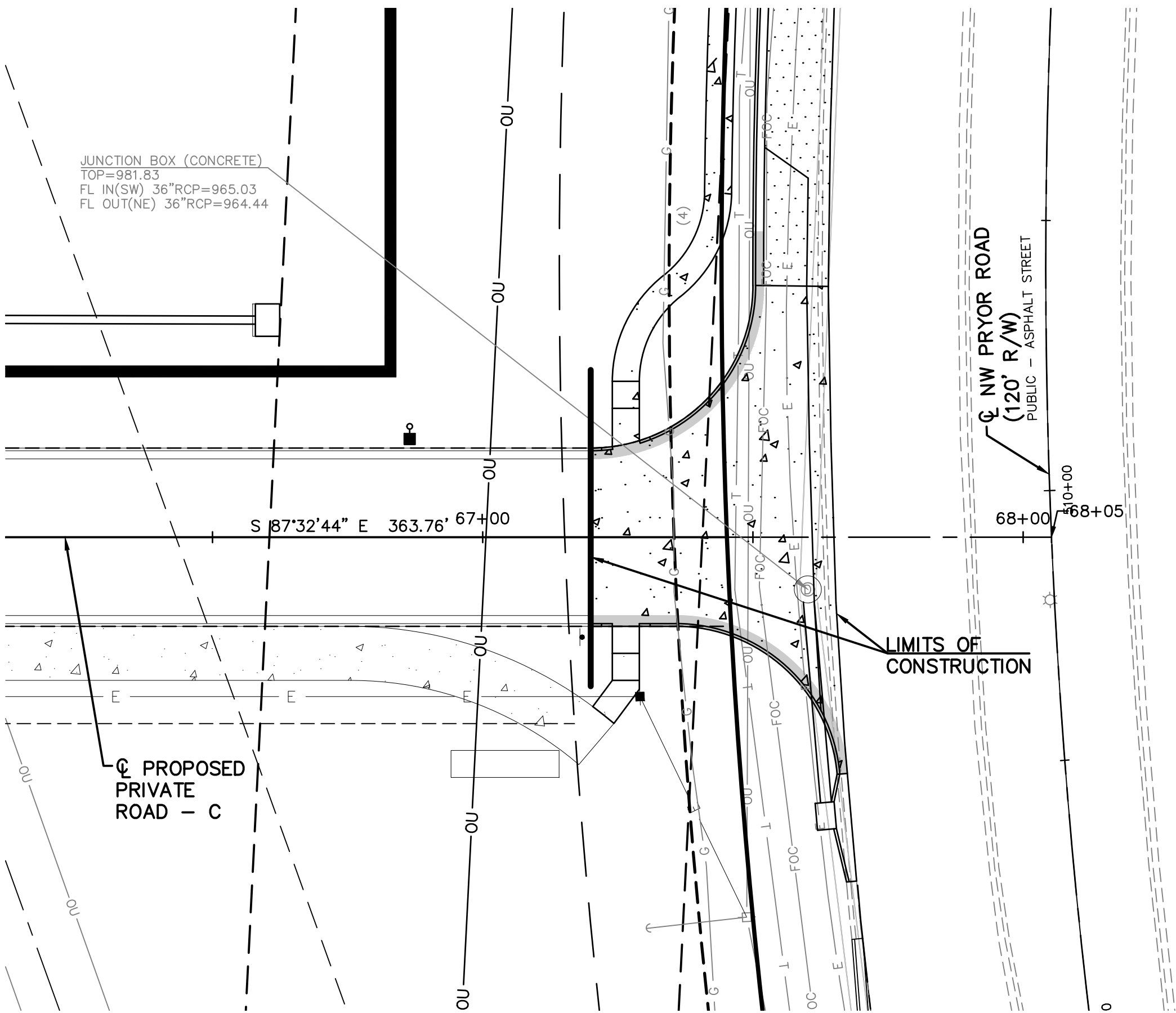
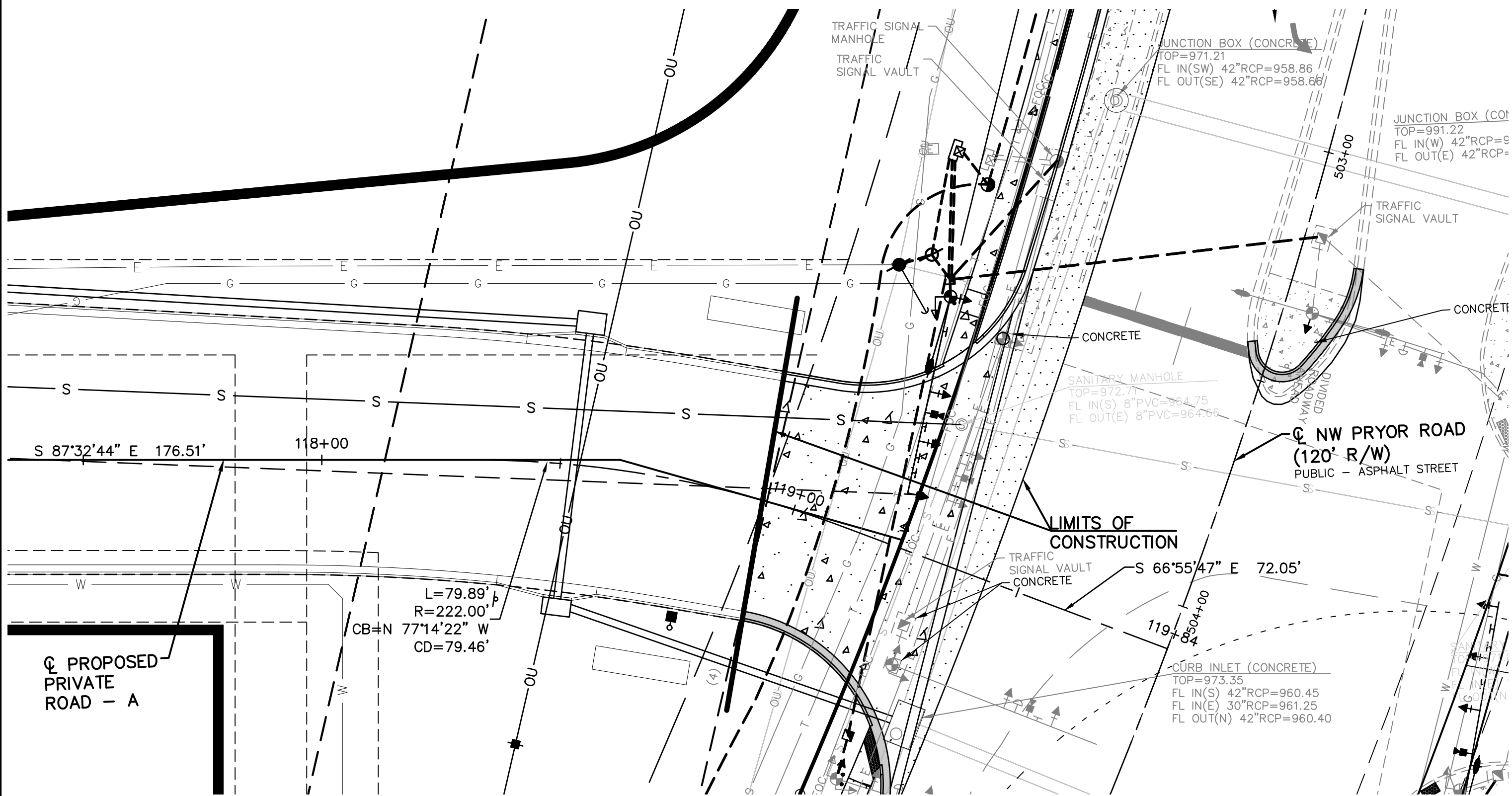
**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. EXPIRES 12/31/19.

**STREETS OF WEST PRYOR**  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING**  
**NW PRYOR DRIVE WIDENING - PLAN & PROFILE**

PROJ. NO.	A14-7067-1
DESIGNER	LDL
DRAWN BY	JT/BKR
CFN	7067-1SIC_STPP
SHEET	8
REV	2





STATE OF MISSOURI  
LEON D. OSBOURN  
REGISTERED PROFESSIONAL ENGINEER  
NUMBER 05319  
EXPIRES 12/31/19

LEON D. OSBOURN  
ENGINEER  
MO # 021726

2319 N. JACKSON | P.O. BOX 1304  
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**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING  
SERVICES IN THE STATE OF MISSOURI UNDER LICENSE # 000842.  
EXPIRES 12/31/19

REV	DATE	DESCRIPTION	DSN	DWN	CHK
2	10-3-19	REVISED PER FIELD CONDITIONS	LDO	BKR	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
DRIVE AT PROPOSED PRIVATE ROAD - A & C PLAN & PROFILE

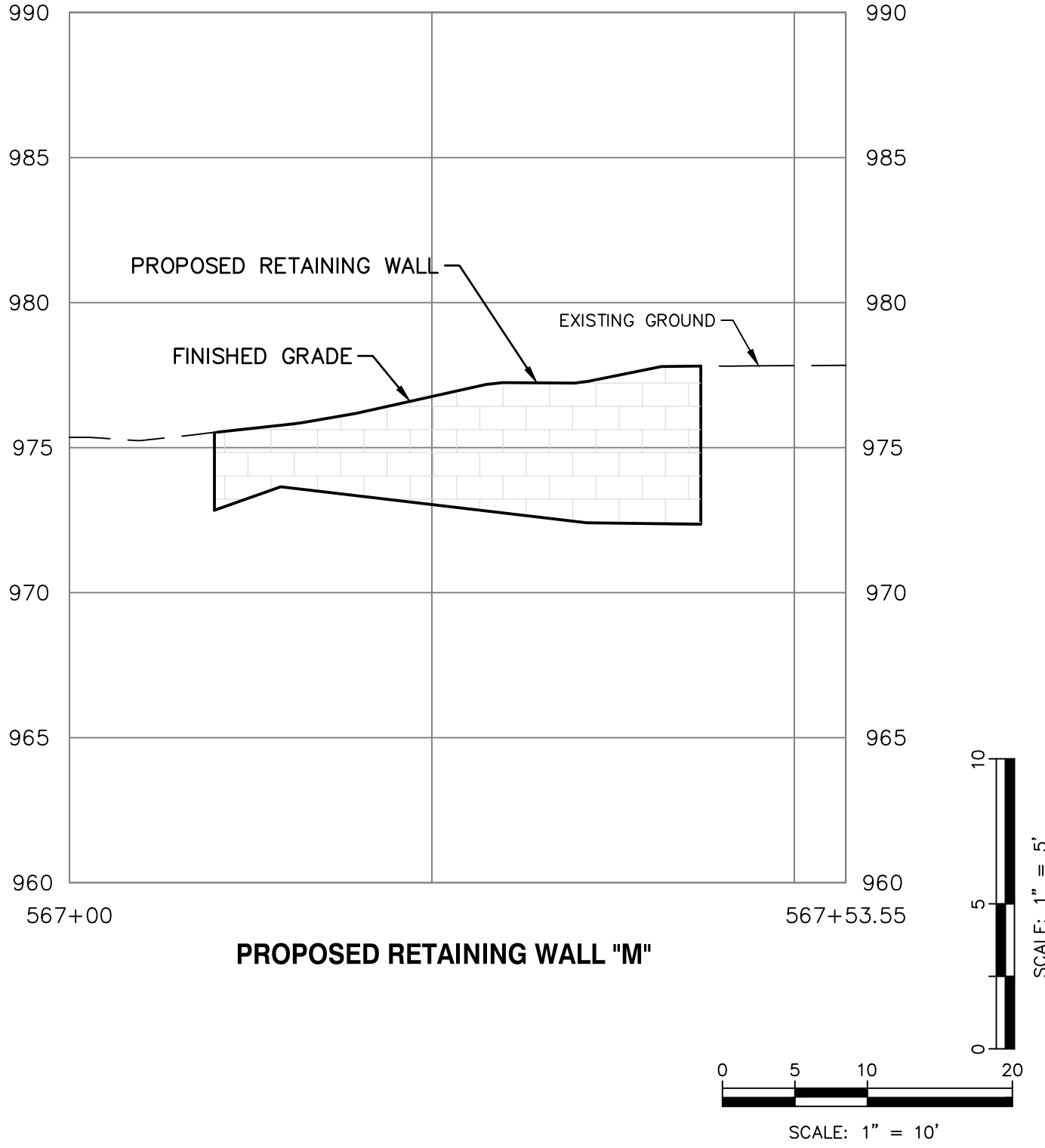
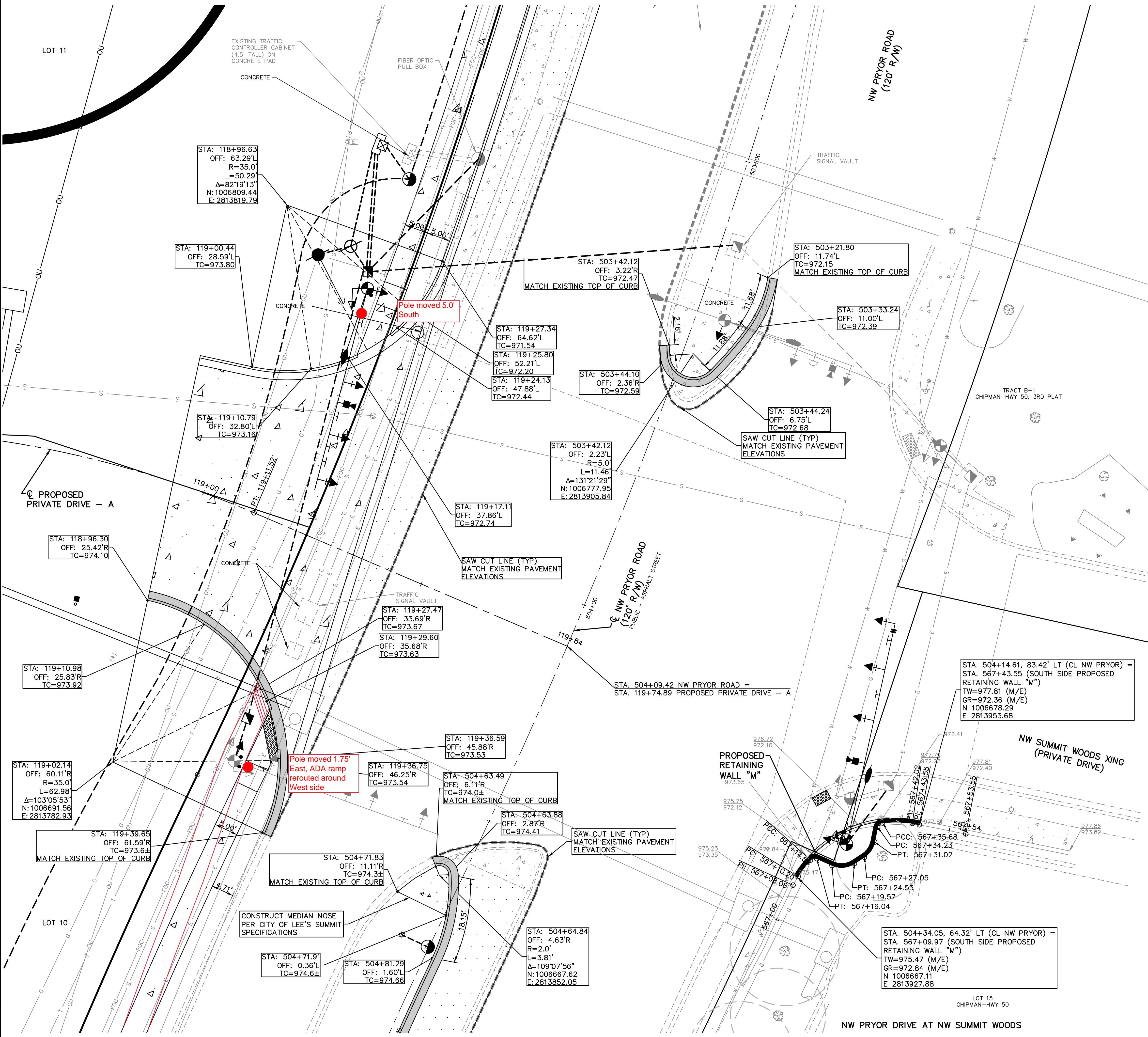
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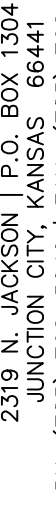
DESIGNER LDO DRAWN BY JT/BKR

CFN 7067-1SIG\_DRIVEPP

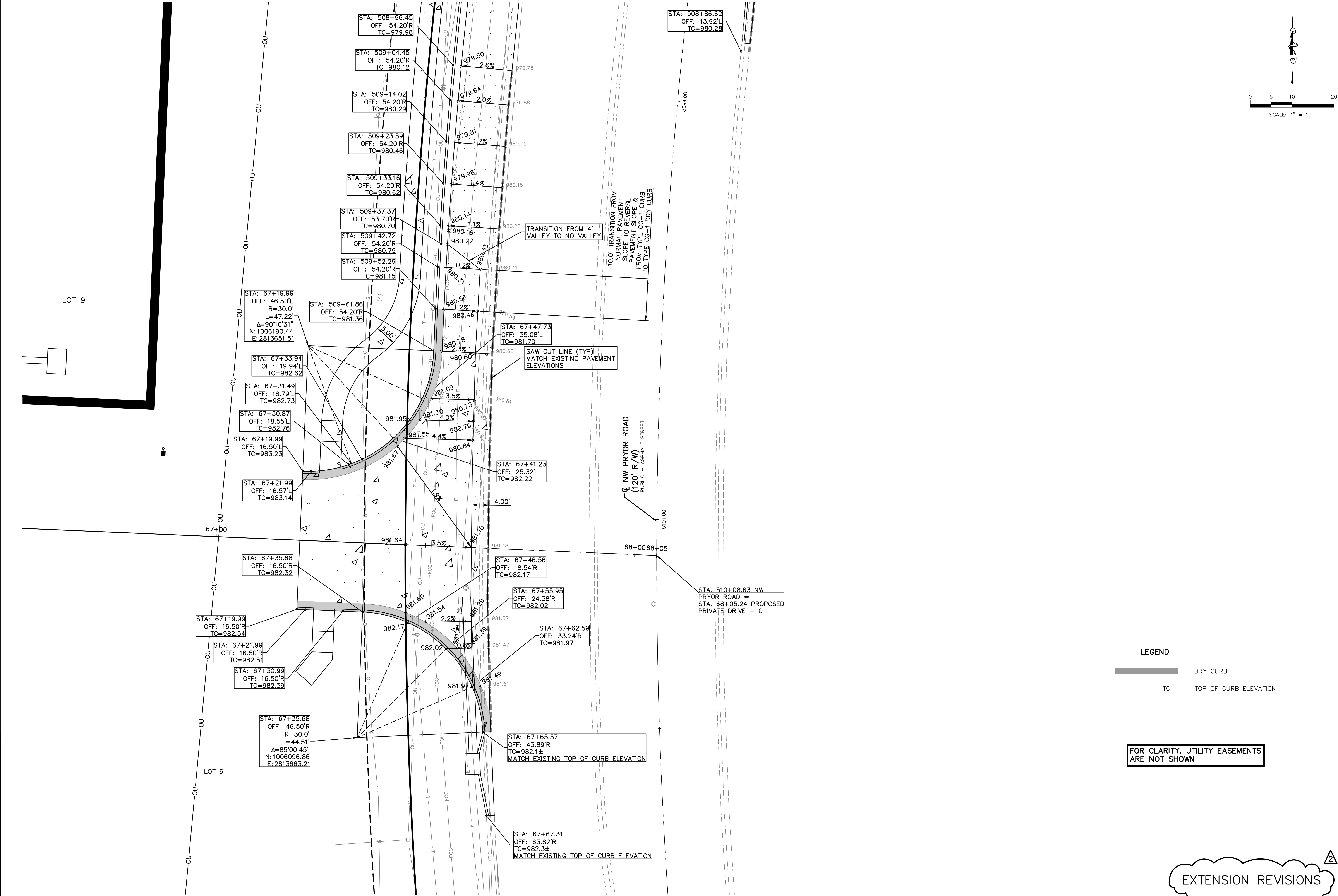
SHEET 9 REV 2





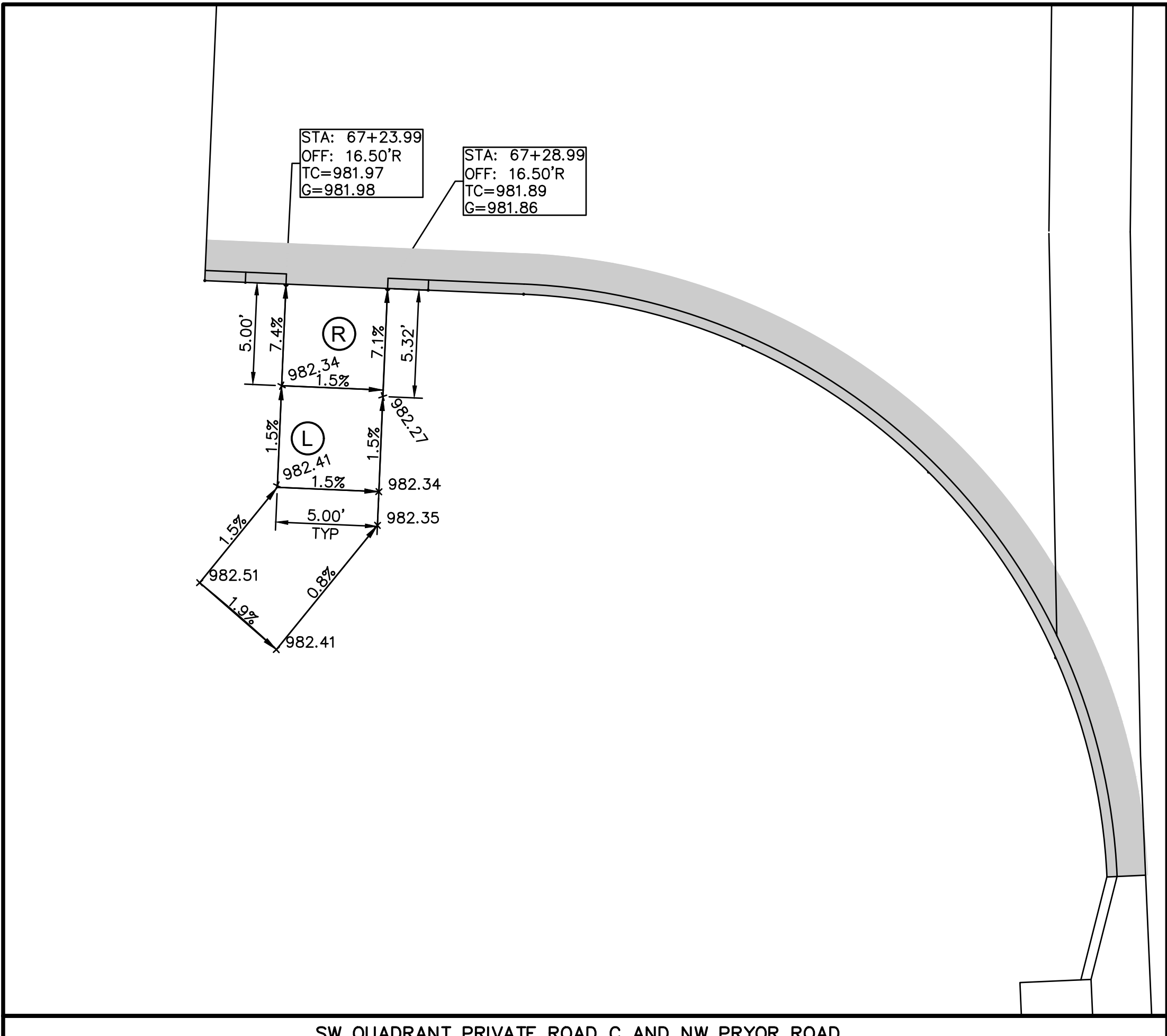
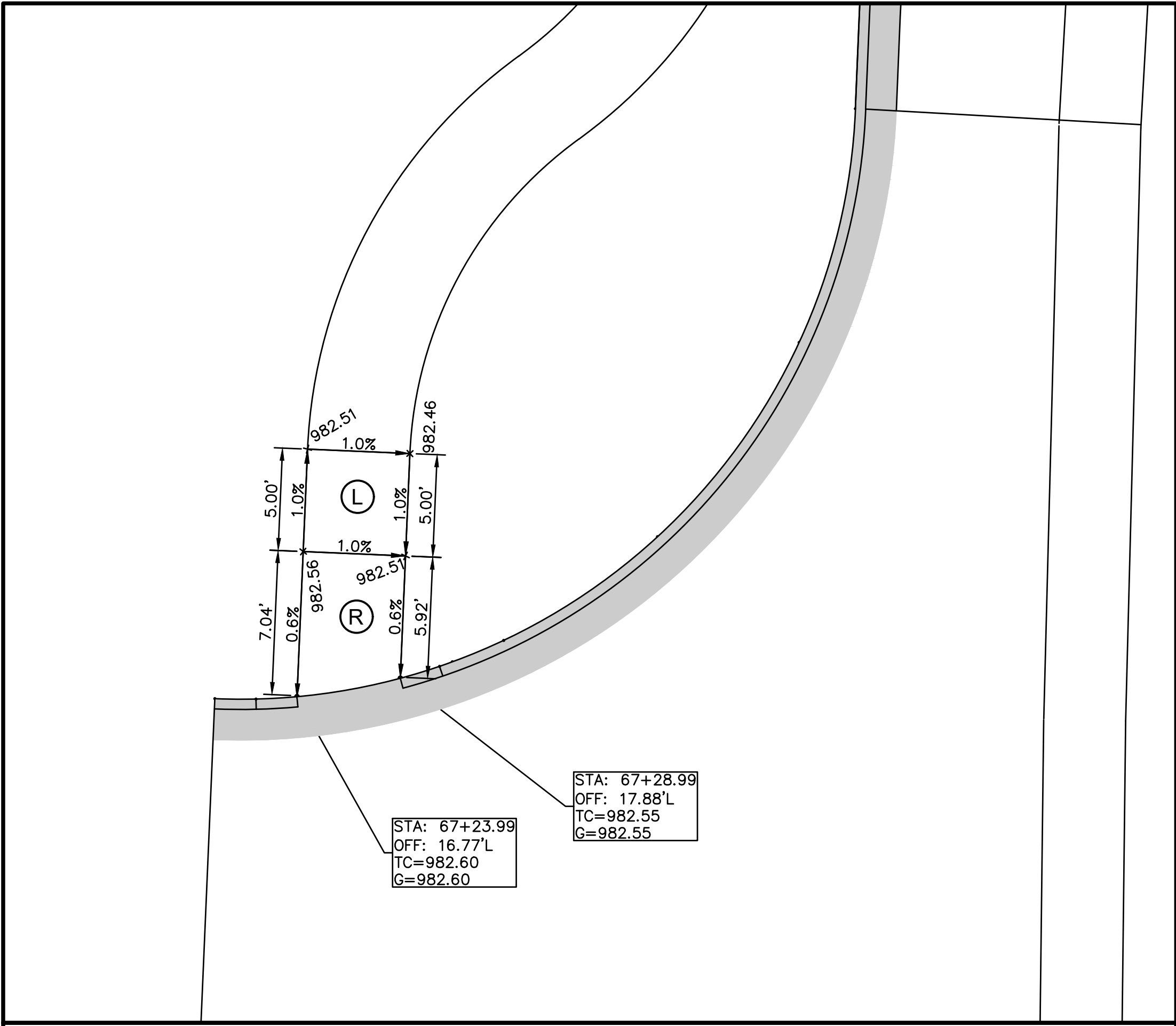
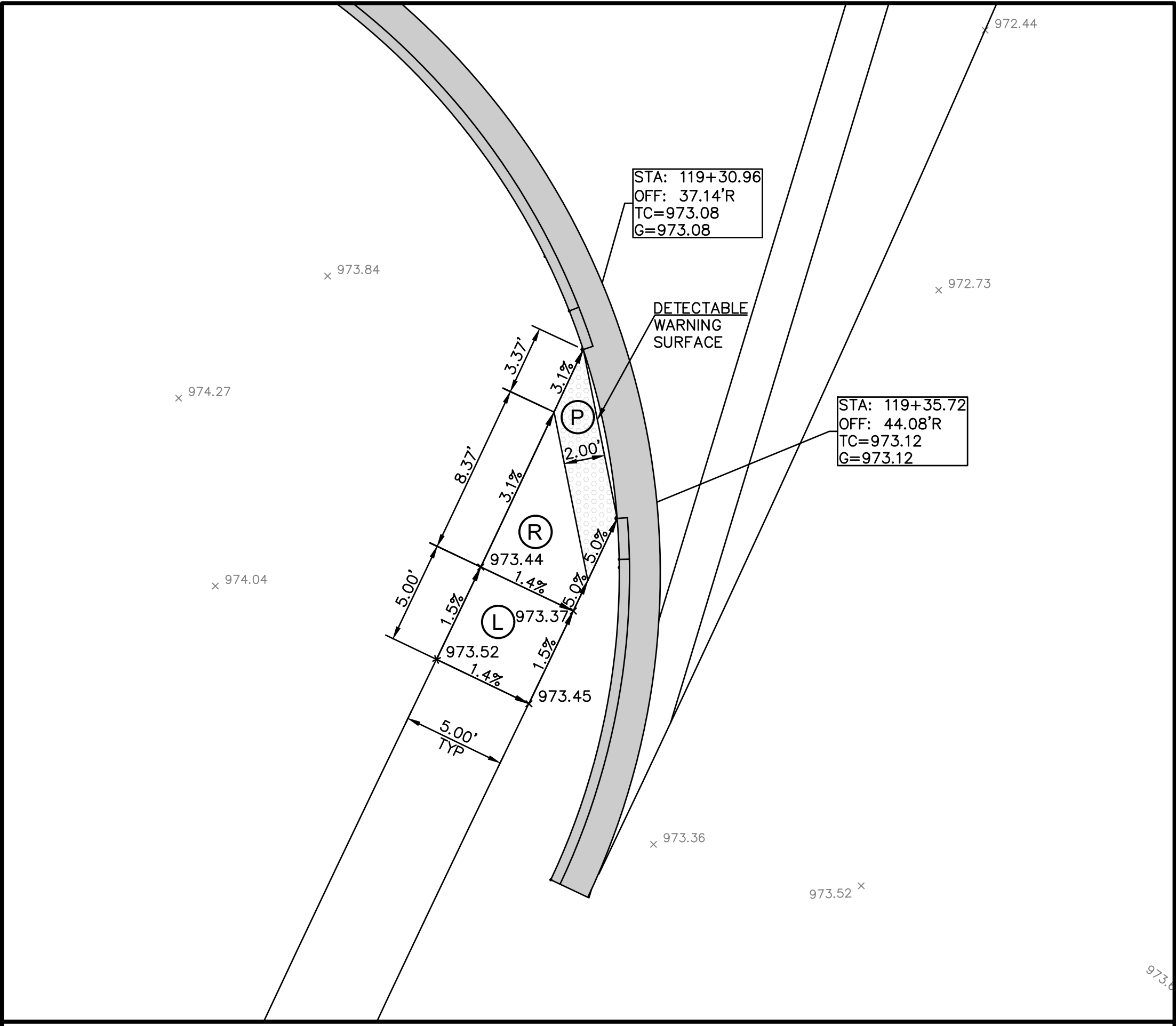
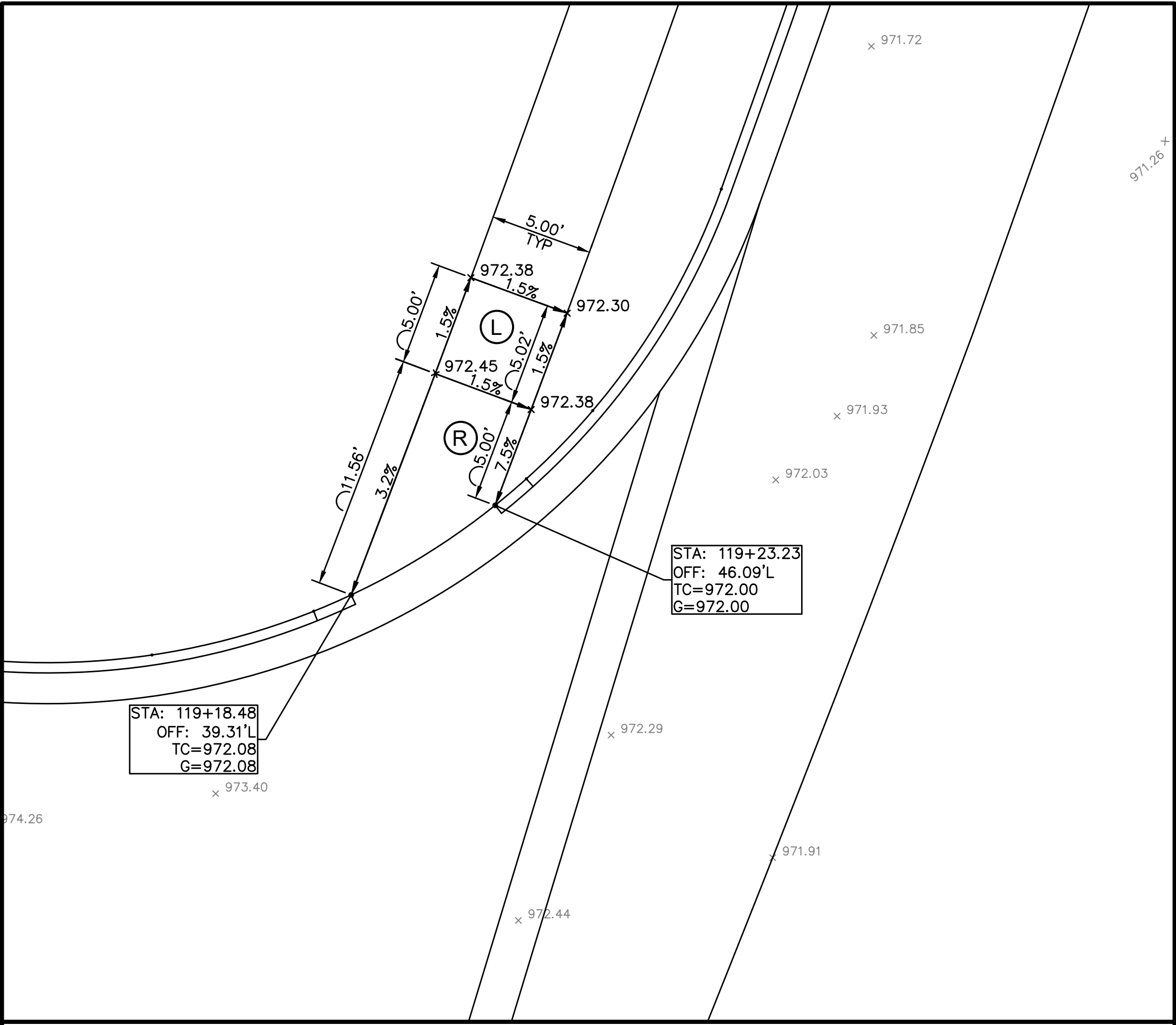
<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19	
PROJ. NO. <b>A14 7067-1</b>		DESIGNER <b>LDO</b> DRAWN BY <b>JT/BKR</b>	
C/FN <b>7067-1SIC GEOM</b>		REV	
SHEET <b>10</b>		<b>2</b>	





STATE OF MISSOURI		LEON D. OSBOURN ENGINEER MO # 021726	
2319 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX. (785) 762-7744 joe@kveeng.com   www.kveeng.com		KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19	
STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING GEOMETRIC LAYOUT - NW PRYOR ROAD AT PROPOSED PRIVATE ROAD - C	
PROJ. NO. A14-7067-1		DESIGNER LDO	
DRAWN BY JT/BKR		CFN	
7067-1SIG_GEOM		REV	
SHEET 11		REV 2	





The drawing shows a cross-section of a sidewalk and curb. From left to right, the components are: a ramp (indicated by a 'P' in a circle), a landing (indicated by an 'L' in a circle), a gutter (indicated by a 'G'), a dry curb (indicated by a thick grey line), and a sidewalk (indicated by a thin grey line). A north arrow points upwards. A scale bar shows 0, 5, and 10 feet, with the text 'SCALE: 1" = 5'' below it. A legend on the right lists the components and their corresponding elevation labels: TC (Top of Curb Elevation), G (Gutter Elevation), R (Ramp), L (Landing), and P (Pan).

**LEGEND**

	DRY CURB
TC	TOP OF CURB ELEVATION
G	GUTTER ELEVATION
	RAMP (7.5% RUNNING SLOPE, 1.5% MAXIMUM CROSS SLOPE)
	LANDING (1.5% MAX IN ALL DIRECTIONS)
	PAN

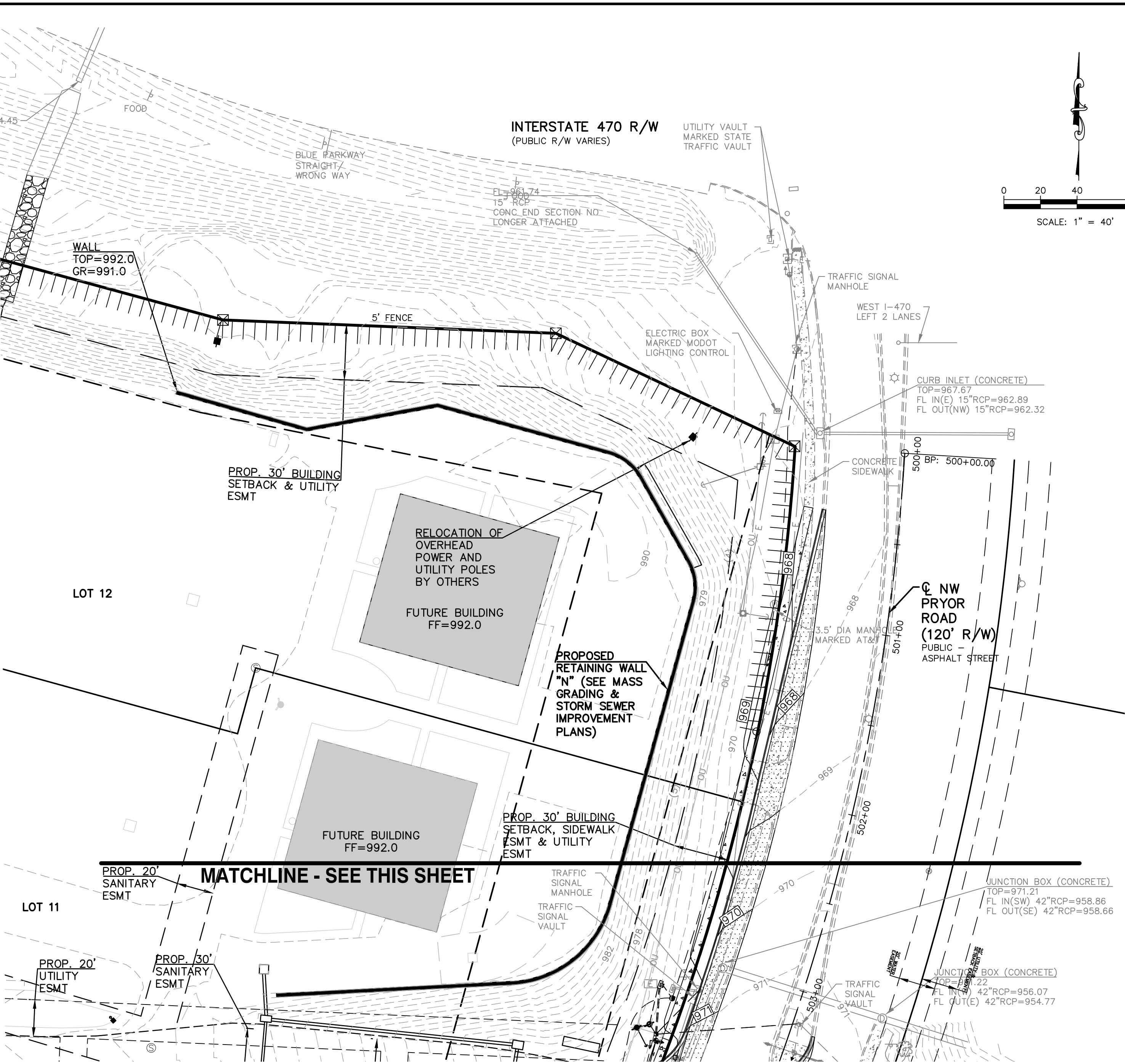
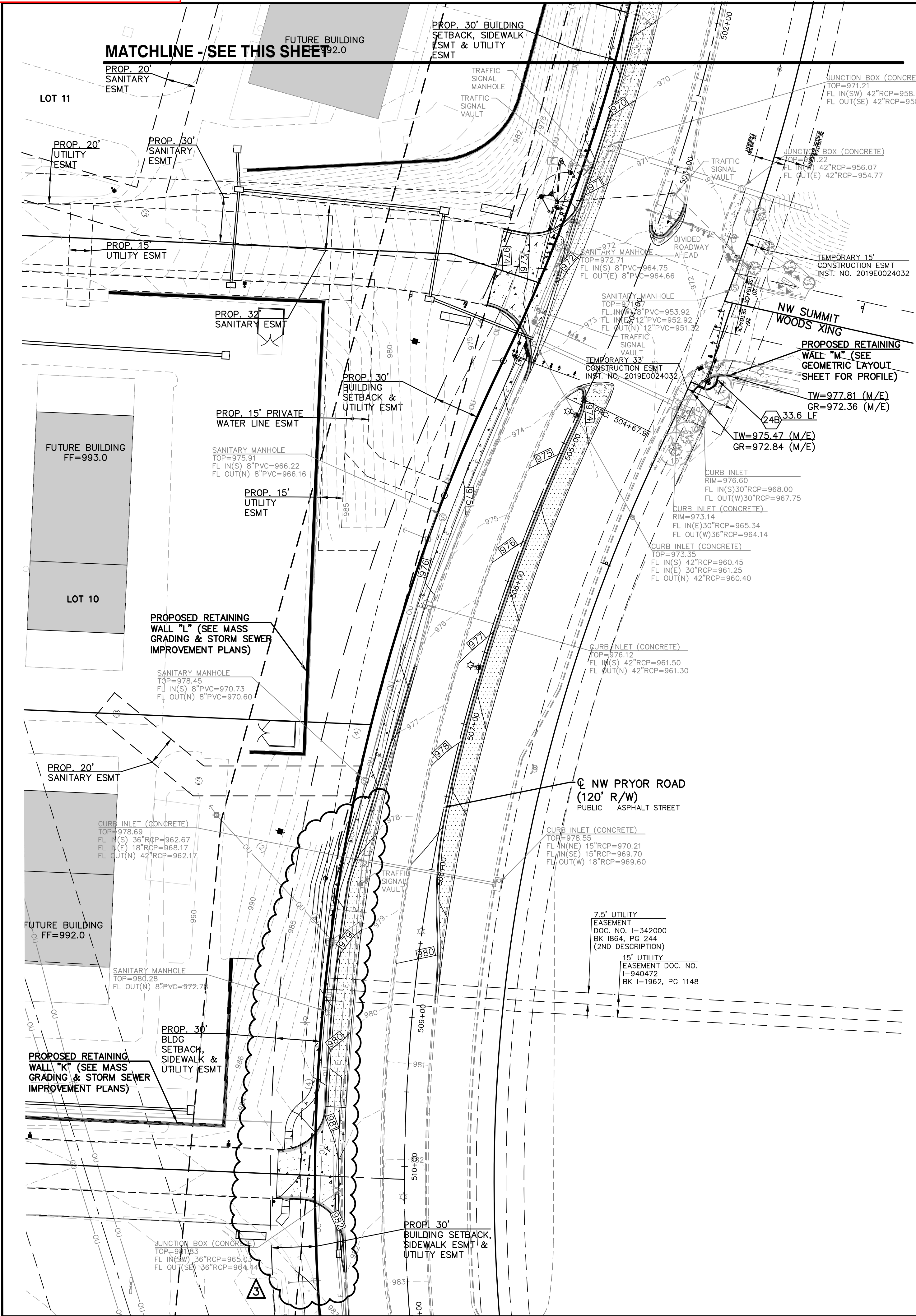
**CONSTRUCTION NOTES:**

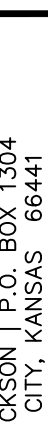

1. SIDEWALK RAMP(S) SHALL BE CONSTRUCTED IN ACCORDANCE WITH LEE'S SUMMIT STANDARD DETAIL GEN-3A. CONSTRUCTION SHALL COMPLY WITH GENERAL NOTES.

**Know what's below.  
Call before you dig.**

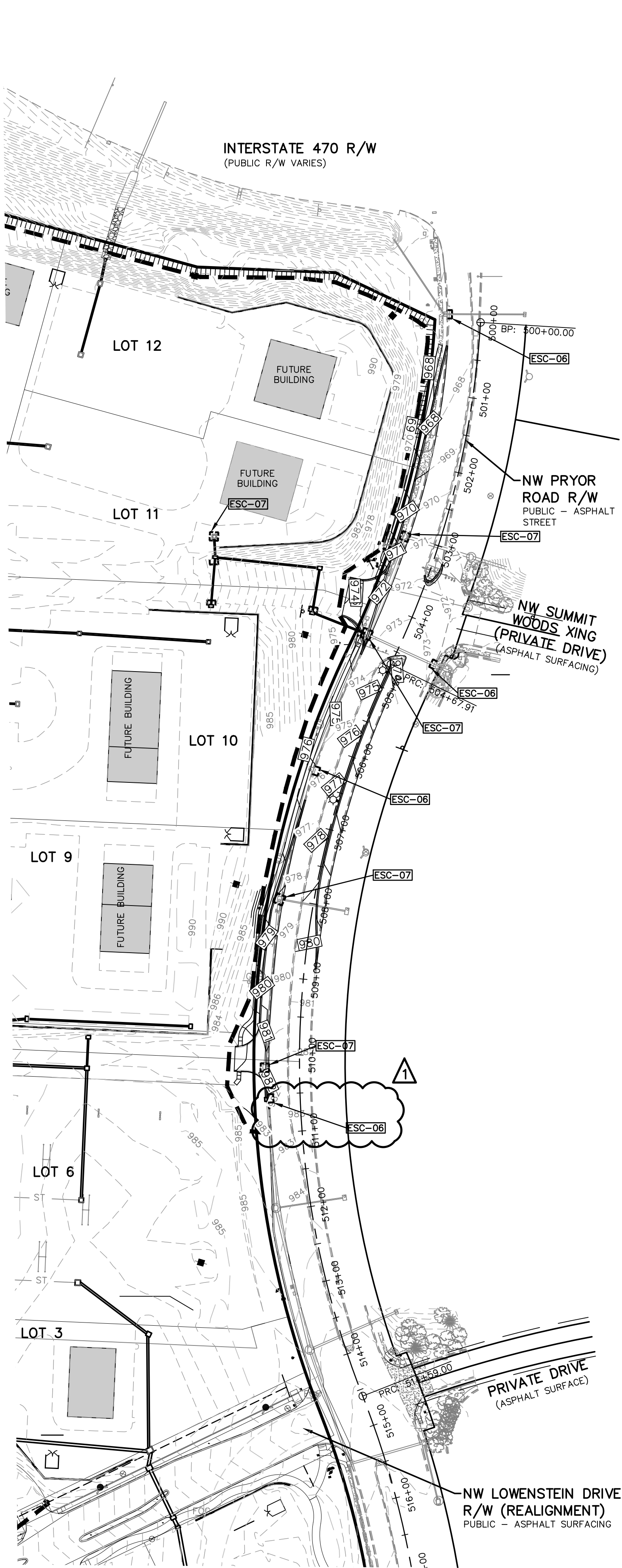
<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b> <b>ADA RAMPS</b>	
PROJ. NO. <b>A14-7067-1</b>		DESIGNER <b>LDQ</b> DRAWN BY <b>JT/BKR</b>	
CFN <b>7067-1SIG_GEOM</b>		SHEET <b>11A</b> REV <b>1</b>	





<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>				2319 N. JACKSON J.C. BOX 1304 JACKSON, MISSOURI 64501 PH. (785) 762-5040   FAX (785) 762-7744 jc@kveg.com   www.kveg.com E-21726							
		KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000942. EXPIRES 12/31/16		LEON D. OSBOURN ENGINEER MO # 021726							
<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b> <b>GRADING PLAN</b>		PROJ. NO. <b>A14-7067-1</b>		DESIGNER <b>LDO</b>		DRAWN BY <b>JT/BKR</b>					
		CFN <b>7067-1SIG_GP</b>		SHEET <b>12</b>		REV <b>3</b>					
								REV DATE DESCRIPTION		DSN DWN CHK	





DETAILS – SEE EROSION CONTROL DETAIL SHEETS FOR THE FOLLOWING DETAILS

ESC-03

SILT FENCE

ESC-06

CURB INLET PROTECTION

ESC-07

AREA INLET AND JUNCTION BOX PROTECTION

EROSION & PROPOSED IMPROVEMENTS LEGEND:

---

970

---

EXISTING GROUND CONTOUR (1' INTERVALS)

970

PROPOSED FINISHED GROUND CONTOUR (1' INTERVALS)

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---

---

SEDIMENTATION FENCE

- GENERAL NOTES:
1. PROPERTY LINE IS LIMITS OF CONSTRUCTION EXCEPT AS SHOWN.

2. THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES AS SHOWN ON THE DRAWINGS PRIOR TO BEGINNING EARTHWORK OPERATIONS.

3. THE CONTRACTOR SHALL MAINTAIN ALL SILT CONTROL MEASURES DURING CONSTRUCTION.

4. ALL SILT SHALL REMAIN ON SITE AND SURROUNDING STREETS SHALL BE KEPT CLEAR OF ALL MUD AND DEBRIS.

5. A SEDIMENTATION BARRIER IS TO BE INSTALLED AS SHOWN.

6. ACCUMULATED SEDIMENT SHALL BE REMOVED AND THE SEDIMENTATION BARRIERS MAINTAINED AS NEEDED TO PREVENT SEDIMENTATION BYPASS OF THE BARRIER.

7. SLOPES ARE TO BE LEFT IN A ROUGH CONDITION DURING GRADING.

8. CURB INLET SEDIMENTATION BARRIERS ARE TO BE INSTALLED AROUND INLETS AND WEIRS WHERE SEDIMENTATION IS A CONCERN. INLET BARRIERS SHALL BE EITHER BLOCK AND GRAVEL, OR SECURED STRAW BALES, OR SILT FENCE.

9. SEDIMENT IS TO BE REMOVED FROM STORM WATER DRAINAGE SYSTEMS.

10. RIPRAP IS TO BE INSTALLED AT AREAS OF CONCENTRATED FLOW (I.E. CULVERT OUTLETS).

11. CONTRACTOR IS RESPONSIBLE FOR INSTALLING ANY ADDITIONAL EROSION CONTROL AS HE/SHE DEEMS NECESSARY.

12. THE CONTRACTOR SHALL PROVIDE ALL MATERIALS, TOOLS, EQUIPMENT AND LABOR AS NECESSARY TO INSTALL AND MAINTAIN ADEQUATE EROSION AND SILTATION CONTROLS REQUIRED TO PREVENT SOIL EROSION FROM LEAVING THE PROJECT SITE. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO ENSURE THAT METHODS UTILIZED ARE ADEQUATE AND COMPLY WITH REQUIREMENTS OF THE SPECIFICATIONS AND GOVERNMENTAL AGENCIES HAVING JURISDICTION OVER THE WORK.

13. TEMPORARY SEDIMENT FENCE TO REMAIN UNTIL ADEQUATE VEGETATION IS ESTABLISHED.

14. MUD AND DEBRIS SHALL BE CLEANED UP AT THE CONCLUSION OF EACH WORKING DAY, OR AFTER EACH RAINFALL IF SILT IS PRESENT.

15. INSPECTION, MAINTENANCE AND REPAIR OF EROSION CONTROL DEVICES SHALL BE ON GOING THROUGHOUT THE LIFE OF BUILDING CONSTRUCTION TO KEEP THE DEVICES IN OPERABLE CONDITION AT ALL TIMES. ADDITIONAL MEASURES SHALL BE INSTALLED AS REQUIRED BY ACTUAL FIELD CONDITIONS AND/OR GOVERNING INSPECTION AGENCIES.

16. INSTALL CONSTRUCTION ENTRANCE AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING THE SITE AND AS SHOWN ON PLANS.

17. AT COMPLETION OF SITE GRADING AND OTHER RELATED CONSTRUCTION ACTIVITIES, ALL DISTURBED AREAS WITHIN THE PROJECT SITE SHALL BE SEEDED, SODDED, OR LANDSCAPED AS SHOWN ON THE LANDSCAPE PLAN WITHIN 14 DAYS.

18. TOPSOIL IS TO BE PLACED IN AREAS UNSUITABLE FOR VEGETATIVE GROWTH.

19. STRIP TOPSOIL PRIOR TO EXCAVATION, STOCKPILE AND SPREAD ONTO DISKED SUBGRADE (4" MIN) A THICKNESS OF 4 INCHES.

20. ROCK LINING (RIPRAP) SHALL BE DURABLE STONE CONTAINING A COMBINED TOTAL OF NOT MORE THAN 10 PERCENT OF EARTH, SAND, SHALE AND NON-DURABLE ROCK. AT LEAST 60 PERCENT OF THE MASS SHALL BE OF PIECES HAVING A MINIMUM WEIGHT OF 150 POUNDS OR MORE PER CUBIC FOOT.

21. THE CONTRACTOR SHALL HAVE THE RESPONSIBILITY FOR RESOLVING COMPLAINTS IN THE EVENT THAT COMPLAINTS OR DAMAGE CLAIMS ARE FILED DUE TO DAMAGES OCCURRING ADJACENT TO OR DOWNSTREAM FROM PROPERTY BY SEDIMENT RESULTING FROM EROSION ON THE PROJECT SITE.

22. GOOD HOUSEKEEPING PRACTICES SHALL BE MAINTAINED ON SITE TO KEEP SOLID WASTE FROM ENTRY INTO WATERS.

23. ALL FUELING FACILITIES PRESENT ON SITE SHALL ADHERE TO APPLICABLE FEDERAL AND STATE REQUIREMENTS CONCERNING UNDERGROUND STORAGE, ABOVE GROUND STORAGE AND DISPENSERS, INCLUDING SPILL PREVENTION, CONTROL AND COUNTER MEASURES.

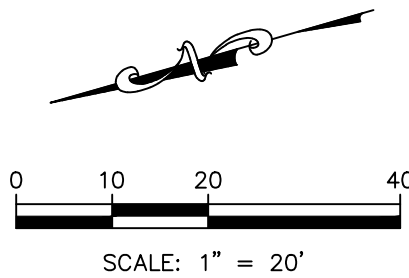
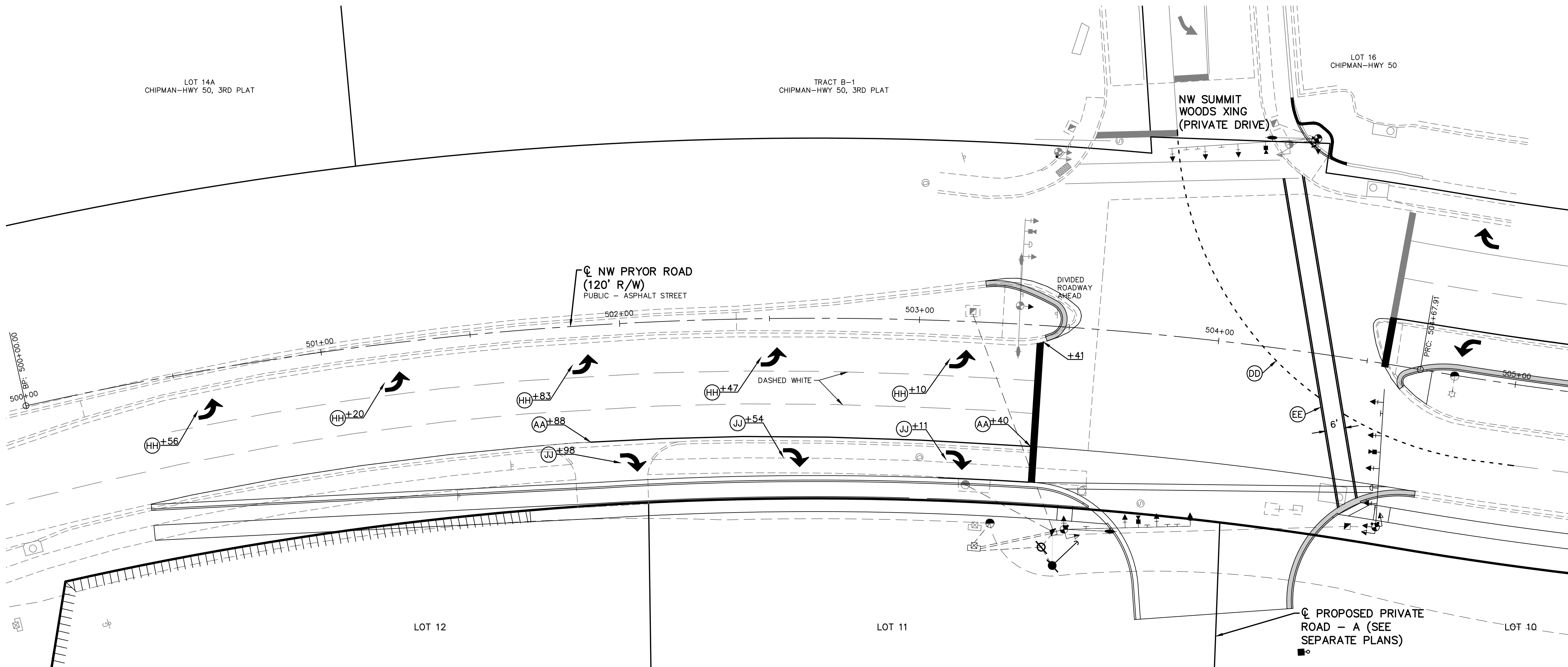
24. RIGHT OF WAY TO BE STABILIZED AS REQUIRED BY APWA SECTION 2400.

25. EROSION CONTROL IS TO BE PLACED IN PHASING AS CONSTRUCTION PROGRESSES.

26. MINIMAL WASHING OF CONCRETE EQUIPMENT ALLOWED, CHUTE ETC. CONCRETE WASHOUT OF THE DRUM IS NOT ALLOWED. ANY PIT/WASHOUT AREA NEEDS TO BE MAINTAINED IN A NON-DISCHARGING MANNER AND ANY WASTE RESIDUE WILL NEED TO BE CLEANED OUT AND REMOVED AT THE END OF PROJECT.

27. EROSION CONTROL SEDIMENT FENCE TO BE INSTALLED 1'-0" BEHIND CURB & GUTTER UPON COMPLETION OF BACKFILL OF CURB IN ALL AREAS WHERE SLOPES FROM LOT DRAIN TOWARDS CURB. UPON COMPLETION OF FINAL GRADING THE TOES OF ALL EMBANKMENTS IN EXCESS OF TWO FEET IN HEIGHT WILL HAVE EROSION CONTROL SEDIMENT FENCE INSTALLED.





**NOTE:**  
1. PAVEMENT MARKING SHALL BE PROVIDED AND INSTALLED IN ACCORDANCE WITH THE CURRENT CITY OF LEE'S SUMMIT PAVEMENT MARKING STANDARDS.  
2. ALL DIMENSIONS REFERENCING DOUBLE YELLOW LINES REFERS TO THE CENTER OF THE SPACE BETWEEN THE TWO DOUBLE YELLOW LINES.  
3. CONTRACTOR TO GRIND OFF CONFLICTING PAVEMENT MARKING.

LIST OF PAVEMENT MARKING	
MARK	ITEM
AA	4" SOLID WHITE LINE (HIGH BUILD PAINT)
DD	6" DASHED WHITE EXTENSION LINE (THERMOPLASTIC)
EE	6" SOLID WHITE CROSS-WALK LINE (THERMOPLASTIC)
GG	24" STOP BAR (THERMOPLASTIC)
HH	TURN ARROW WHITE (LEFT) (THERMOPLASTIC)
JJ	TURN ARROW WHITE (RIGHT) (THERMOPLASTIC)

PROJ. NO. <b>A14_7067-1</b>	
DESIGNER <b>LDO</b>	DRAWN BY <b>JT/BKR</b>
CFN <b>7067-1SIG_PMP</b>	
SHEET <b>14</b>	REV <b>2</b>

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
NW PRYOR ROAD PAVEMENT MARKING PLAN

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joe@kveeng.com | www.kveeng.com

**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI  
EXPIRES 12/31/19

STATE OF MISSOURI  
LEON D. OSBOURN  
NUMBER  
003479  
E-21726  
REGISTERED PROFESSIONAL ENGINEER

LEON D. OSBOURN  
ENGINEER  
MO # 021726

REV	DATE	DESCRIPTION
2	7-24-19	REVISED PER CITY COMMENTS
1	7-2-19	REVISED PER CITY COMMENTS
0	5-23-19	INITIAL ISSUE

	LDO	JT	LDO
	LDO	JT	LDO
	DSN	DWN	CHK

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GENERAL NOTES:

1. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS (APPROVED BY THE CITY OF LEE'S SUMMIT) AND ONE (1) COPY OF THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES.

2. CONSTRUCTION OF THE IMPROVEMENTS SHOWN OR IMPLIED BY THIS SET OF DRAWINGS SHALL NOT BE INITIATED OR ANY PART THEREOF UNDERTAKEN UNTIL THE DIRECTOR OF PUBLIC WORKS OR HIS AGENT IS NOTIFIED OF SUCH INTENT, AND ALL REQUIRED AND PROPERLY EXECUTED BONDS AND CONTRACT AGREEMENTS ARE RECEIVED AND APPROVED BY THE CITY.

3. THE CONSTRUCTION COVERED BY THESE PLANS SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI, IN CURRENT USE. SPECIFICALLY, BUT NOT EXCLUSIVE TO: TRAFFIC SIGNAL SPECIFICATION: SECTION 2900. TRAFFIC SIGNAL STANDARD DRAWINGS: TS-1 THROUGH TS-10.

4. ALL WORKMANSHIP AND MATERIALS SHALL BE SUBJECT TO THE INSPECTION AND APPROVAL OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI.

5. RIGHT-OF-WAY LIMITS SHOULD BE CROSS CHECKED BY THE CONTRACTOR AND APPROVED BY THE FIELD INSPECTOR BEFORE UNDERTAKING ANY EXCAVATIONS AT THE SITE.

6. THE CONTRACTOR SHALL STAKE THE LOCATION OF ALL POLE BASES, PULL BOXES, AND CONTROLLER CABINET BASE, THEN PROVIDE THE CITY ONE WEEK NOTICE PRIOR TO THE START OF CONSTRUCTION, AND SUBSEQUENT CONSTRUCTION ACTIVITIES, FOR INSPECTION AND APPROVAL. THE CONTRACTOR SHALL PROVIDE A WORK SCHEDULE, CONTACT NAMES, AND PHONE NUMBERS.

7. ALL LOCATIONS INDICATED IN DRAWINGS, INCLUDING CONDUIT RUNS ARE SUBJECT TO ADJUSTMENT TO CLEAR OBSTRUCTIONS AND TO MEET SITE CONDITIONS, IF ANY BY THE CITY.

8. EXISTENCE AND LOCATION OF ANY UNDERGROUND OR OVERHEAD FACILITIES SHOWN ON THESE DRAWINGS OR REFERENCE TO ANY SOIL CONDITIONS, IF MADE, ARE APPROXIMATE ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL SITE CONDITIONS AND TO LOCATE ALL UTILITIES, INCLUDING DEPTH, BEFORE STARTING CONSTRUCTION SO THAT ANY ADJUSTMENTS TO DESIGN CAN BE MADE PRIOR TO POLE ORDERING OR FABRICATION. IN ADDITION, THE CONTRACTOR SHALL AVOID DISRUPTION OF SERVICES PROVIDED BY THE UTILITIES AND SHALL INSURE THAT PROPER CLEARANCES (OVERHEAD AND UNDERGROUND) ARE MAINTAINED FOR THE DURATION OF CONSTRUCTION. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UTILITIES.

9. THE CONTRACTOR SHALL COORDINATE WITH THE CITY TRAFFIC ENGINEER FOR ANY NECESSARY CHANGES TO THE TRAFFIC SIGNALS RESULTING FROM EXISTING UTILITIES OR OTHER CONSTRUCTION ISSUES.

10. ANY EQUIPMENT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

11. SIGNAL EQUIPMENT SHALL NOT FORM AN OBSTRUCTION TO THE MOVEMENT OF PEDESTRIAN AND WHEELCHAIR TRAFFIC AND SHALL BE ADA ACCESSIBLE. WHERE SIDEWALKS ARE PRESENT, A MINIMUM CLEAR WIDTH OF 48 INCHES SHALL BE AVAILABLE FOR PEDESTRIAN AND WHEELCHAIR MOVEMENT. PULL BOXES SHALL NOT BE INSTALLED ON WHEELCHAIR RAMPS.

12. CONDUITS TO BE PLACED OUTSIDE OF PAVED AREAS SHALL BE TRENCHED IN PLACE. IF THE PROJECT INCLUDES ROADWAY IMPROVEMENTS, THE CONDUIT SHALL BE TRENCHED AFTER THE ROADWAY ROUGH GRADE IS ESTABLISHED AND PRIOR TO ANY FINAL ROADWAY PAVING, CURB & GUTTER, MEDIAN OR SIDEWALK SECTIONS ARE PLACED. ALL COMPACTION AND BACKFILL SHALL MEET CITY OF LEE'S SUMMIT REQUIREMENTS. AT THE OPTION OF THE CONTRACTOR, CONDUITS MAY BE BORED OUTSIDE PAVED AREAS, BUT THERE WILL BE NO ADJUSTMENT TO THE UNIT PRICES FOR CONDUIT INSTALLATION AND ANY CHANGE IN COST WOULD BE THE CONTRACTOR'S RESPONSIBILITY. ANY CONDUIT BORE OUTSIDE THE PAVED AREAS SHALL BE DONE AFTER ROADWAY IMPROVEMENTS ARE COMPLETE. CONDUITS TO BE PLACED WITHIN THE LIMITS OF PAVEMENT SHALL BE BORED UNLESS OTHERWISE AUTHORIZED BY THE CITY TRAFFIC ENGINEER. IF THE PROJECT INCLUDES ROADWAY IMPROVEMENTS, THE CONDUIT SHALL BE BORED PRIOR TO ANY FINAL ROADWAY PAVING. POTHOLING FOR UTILITIES ON ROAD BORES AFTER FINAL PAVING WILL NOT BE ALLOWED.

13. THE TRAFFIC SIGNAL CONTROLLER, CABINET AND RELATED EQUIPMENT, AS SPECIFIED FOR THIS PROJECT, SHALL BE DELIVERED TO THE CITY FOR TESTING PRIOR TO INSTALLATION. ALL SIGNAL TIMINGS WILL BE PROVIDED BY THE CITY TRAFFIC ENGINEER. THE CONTRACTOR SHALL COORDINATE MATERIAL DELIVERY AND PICK-UP WITH THE PUBLIC WORKS OPERATIONS DEPARTMENT (969-1870) AT LEAST 48 HOURS PRIOR TO TRANSPORTATION. THE CONTRACTOR ASSUMES ALL DAMAGE LIABILITY AND SHOULD INSPECT ALL MATERIALS BEFORE AND AFTER TRANSPORTATION OF EQUIPMENT.

14. THE CONTRACTOR SHALL COORDINATE ALL ELECTRICAL POWER REQUIREMENTS AND CONNECTION ACTIVITIES WITH THE UTILITY COMPANY, INCLUDING LOCATION OF THE METER, CIRCUITRY AND CONNECTION REQUIREMENTS, AND POWER UP THE COMPLETE SYSTEM.

15. ALL DISTURBED SURFACES SHALL BE MADE GOOD TO MATCH EXISTING AT THE CONTRACTOR'S EXPENSE.

16. CONTRACTOR SHALL MAINTAIN AT ALL TIMES ACCESS FOR EMERGENCY VEHICLES AND RESIDENTS ALONG THE ENTIRE PROJECT.

17. SUBSTANTIAL COMPLETION OF THE TRAFFIC SIGNALS SHALL BE DEFINED AS ALL COMPONENTS OF THE TRAFFIC SIGNAL OPERATED FULLY AND SATISFACTORILY WITH RED, YELLOW, AND GREEN CYCLES. SUBSTANTIAL COMPLETION SHALL ALLOW FOR TESTING OF THE SIGNALS, INCLUDING A FLASH PERIOD, PRIOR TO SIGNALS OPERATING WITH CYCLES.

18. FINAL ACCEPTANCE OF TRAFFIC SIGNALS SHALL BE DEFINED AS FINAL WRITTEN APPROVAL AND ACCEPTANCE BY THE CITY, INCLUDING COMPLETION OR CORRECTIONS OF ALL PUNCH LIST ITEMS AND THE TRAFFIC SIGNALS FULLY OPERATIONAL FOR A TIME PERIOD OF FIFTEEN (15) DAYS, WITHOUT ANY PROBLEM, AS NOTED IN THE SPECIFICATIONS. AS-BUILT PLANS SHALL BE SUBMITTED PRIOR TO FINAL ACCEPTANCE BY THE CITY.

19. THE CONTRACTOR SHALL VERIFY AND/OR COORDINATE WITH THE VARIOUS SERVICE PROVIDERS ON THE EXACT LOCATIONS OF METERS, CONNECTION POINTS AND OTHER SPECIFIC PROJECT REQUIREMENTS. THE PROPOSED SERVICE LINE LOCATIONS THAT ARE SHOWN ARE CONCEPTUAL AND ARE NOT WARRANTED.

PROJECT SPECIFIC NOTES:

1. THE CONTRACTOR SHALL SUPPLY THE TRAFFIC SIGNAL CONTROLLER, CABINET, SIGNAL POLES WITH MAST ARMS AND POWER SUPPLY ASSEMBLY, AND THE CONTRACTOR SHALL INSTALL THE EQUIPMENT. THE CONTRACTOR WILL BE RESPONSIBLE FOR SUPPLYING THE SIGNAL PEDESTAL POLES. RE-USE EXISTING IN SYNC COMPONENTS. WILL REQUIRE NEW CAMERA AND CARD.

2. THE VIDEO DETECTION SYSTEM SHALL BE AN IN SYNC ADAPTIVE TRAFFIC CONTROL SYSTEM SUPPLIED BY RHYTHM ENGINEERING, 12351 W 96TH TERRACE, SUITE 107, LENEXA, KANSAS 66215, PHONE (913)227-0603. MATERIALS SUPPLIED BY RHYTHM ENGINEERING SHALL CONSIST OF AN IN SYNC PROCESSOR, VIDEO CAMERAS, TRANSFORMER, RG CABLE, DETECTOR CARDS AND CABLES, SITE EQUIPMENT PANELS, MONITOR, KEYBOARD AND CAMERA MOUNTING HARDWARE.

3. CABLES FOR THE VIDEO DETECTION SYSTEM SHALL BE ONE CONTINUOUS 3c-#14 CABLE AND ONE CONTINUOUS CATEGORY 5E+ CABLE (SHIELDED OUTDOOR RATED) BETWEEN THE CONTROLLER AND EACH CAMERA. THE 3c-#14 CABLE SHALL MEET REQUIREMENTS OF IMSA 20-1. CABLES ARE INCLUDED IN THE LUMP SUM PRICE FOR THE VIDEO DETECTION SYSTEM. NO ADDITIONAL PAYMENTS WILL BE MADE.

CONSTRUCTION SEQUENCING:

1. CONTRACTOR TO OBTAIN AND FOLLOW TEMPORARY TRAFFIC CONTROL PERMIT (TTCP) CONDITION REQUIREMENTS.

2. CONTRACTOR TO SUBMIT FOR APPROVAL BY CITY OF LEE'S SUMMIT TRAFFIC ENGINEER A DETAILED TRAFFIC SEQUENCING PHASING PLAN.

3. CONTRACTOR TO MAINTAIN EXISTING SIGNAL OPERATIONS UNTIL NEW SYSTEM, HAS BEEN TESTED AND OPERATIONAL.

4. PER TTCP, CONTRACTOR TO INSTALL TRAFFIC CONTROL, CHANNELIZATION DEVICES AND TEMPORARY PAVEMENT MARKING AROUND WORK ZONE.

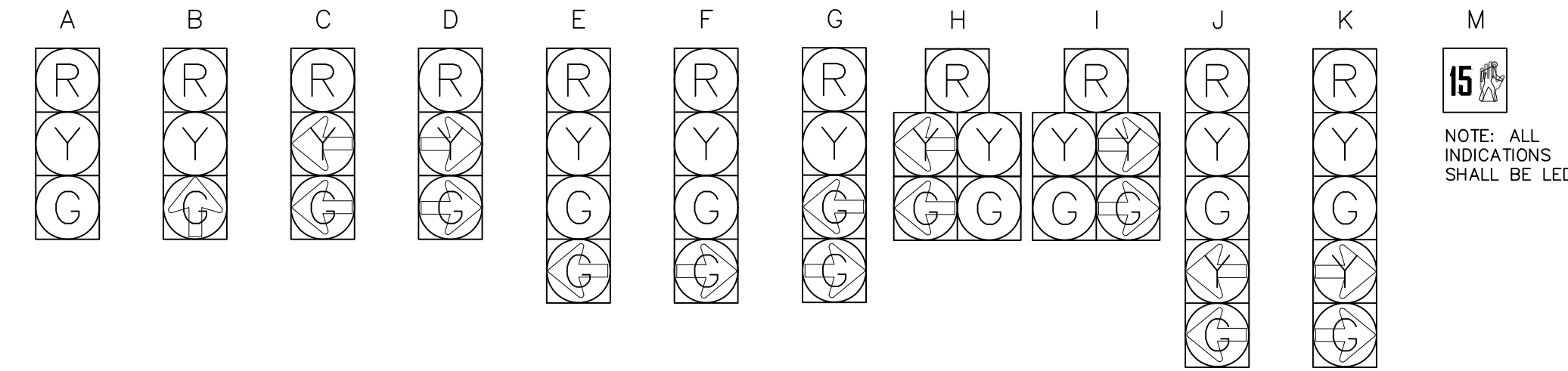
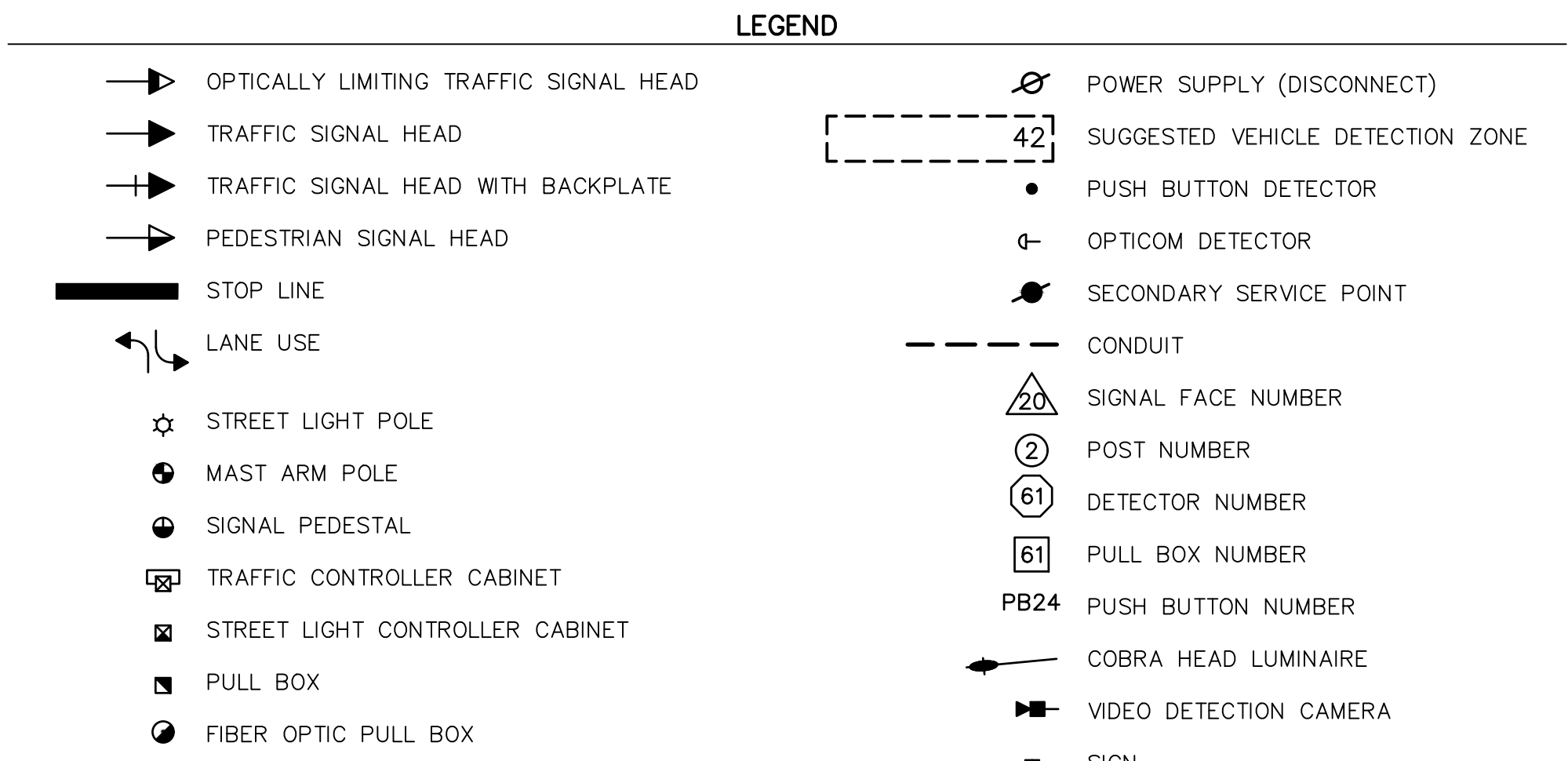
5. CONTRACTOR TO PROVIDE PEDESTRIAN DETOUR PLAN ALONG WITH TRAFFIC SEQUENCING PLAN.

6. CONTRACTOR TO INSTALL CONSTRUCTION FENCING AROUND AREAS WHERE PEDESTRIAN MOVEMENTS MAY CONFLICT WITH CONSTRUCTION ACTIVITIES.

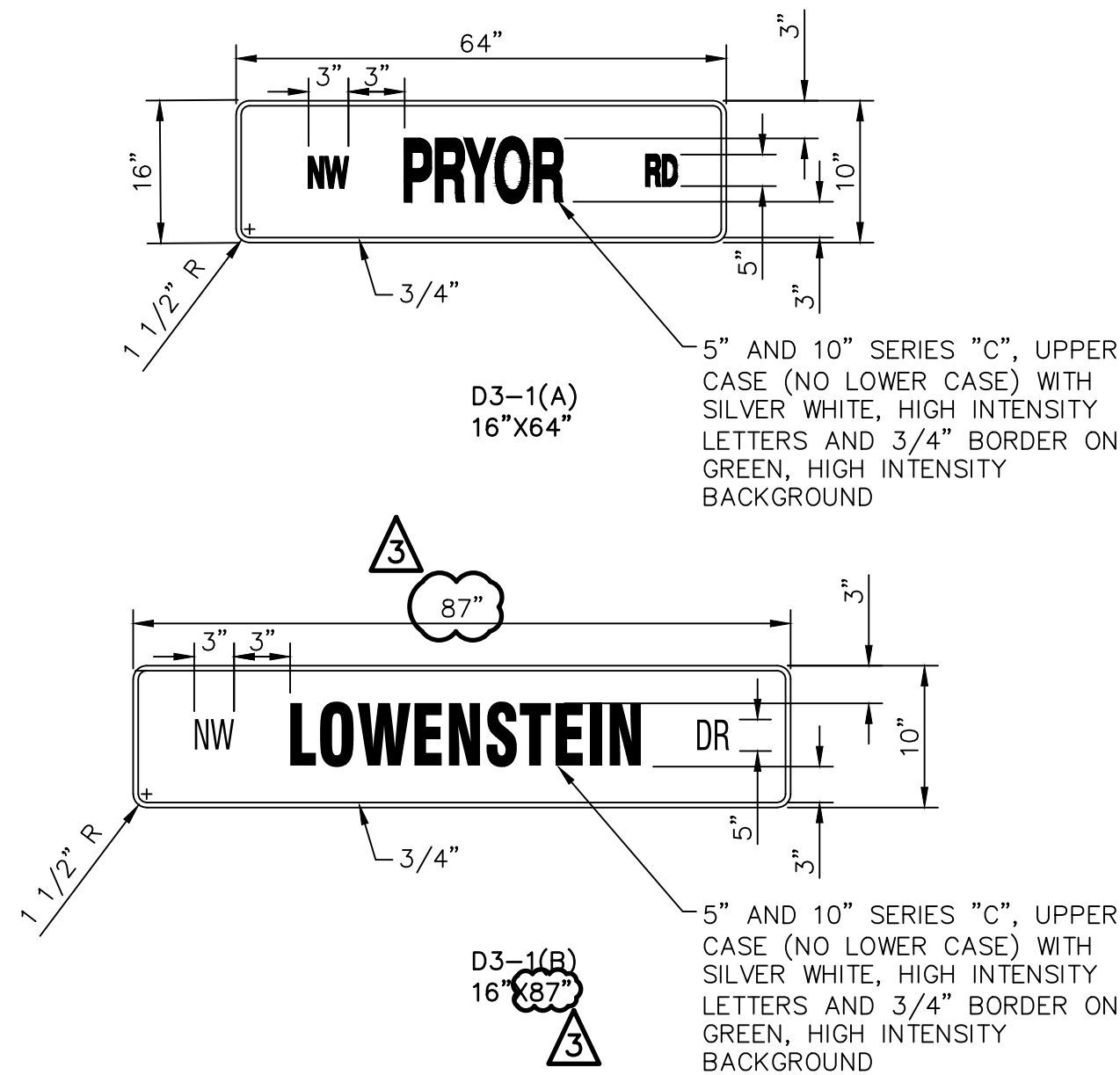
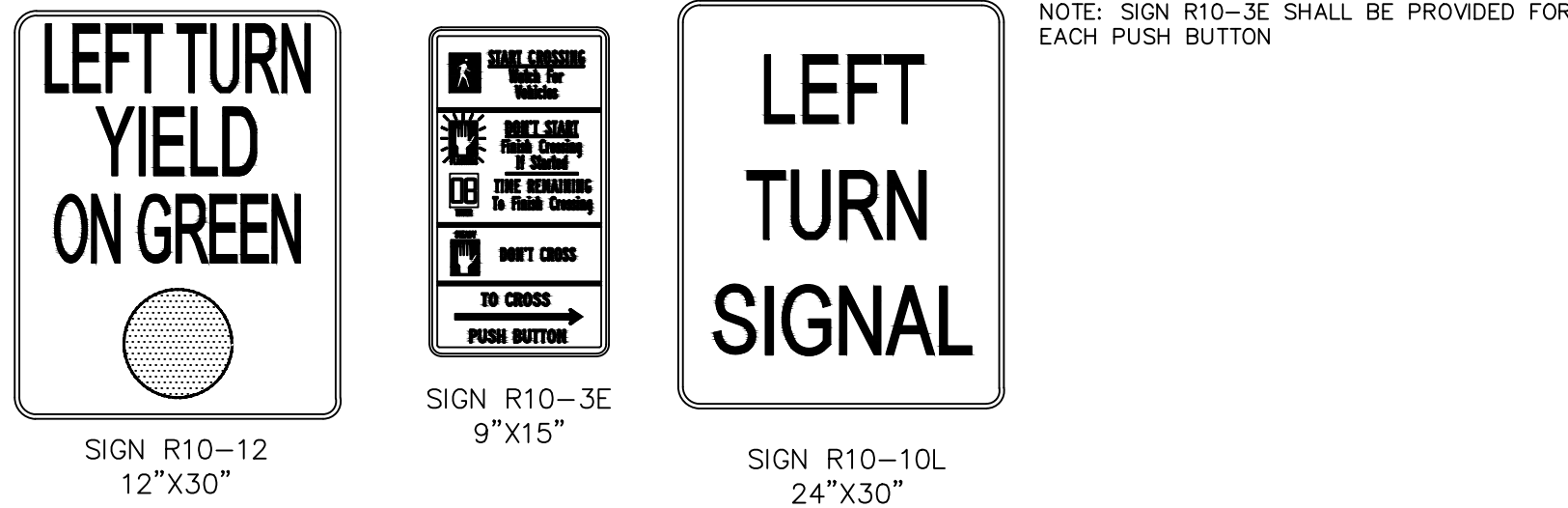
7. NO COLLECTOR OR ARTERIAL STREETS SHALL HAVE LANE(S) REDUCED OR CLOSED BEFORE 9 A.M. AND AFTER 4 P.M. MONDAY THRU FRIDAY. NO WORK ON SATURDAY, SUNDAYS OR HOLIDAYS OBSERVED BY THE CITY.

8. ALL TRAVEL LANES SHALL BE A MINIMUM OF 11.00 FEET WIDE EXCLUDING CURB AND GUTTER. ALL SIDEWALKS SHALL HAVE A MINIMUM CLEAR PATH OF 3.5 FEET. ALL MULTI-USE PATHS SHALL HAVE A MINIMUM CLEAR PATH OF 8.00 FEET.

9. CONTRACTOR SHOULD REVIEW TTCP AND MEET REQUIREMENTS PER CITY OF LEE'S SUMMIT REGULATIONS.



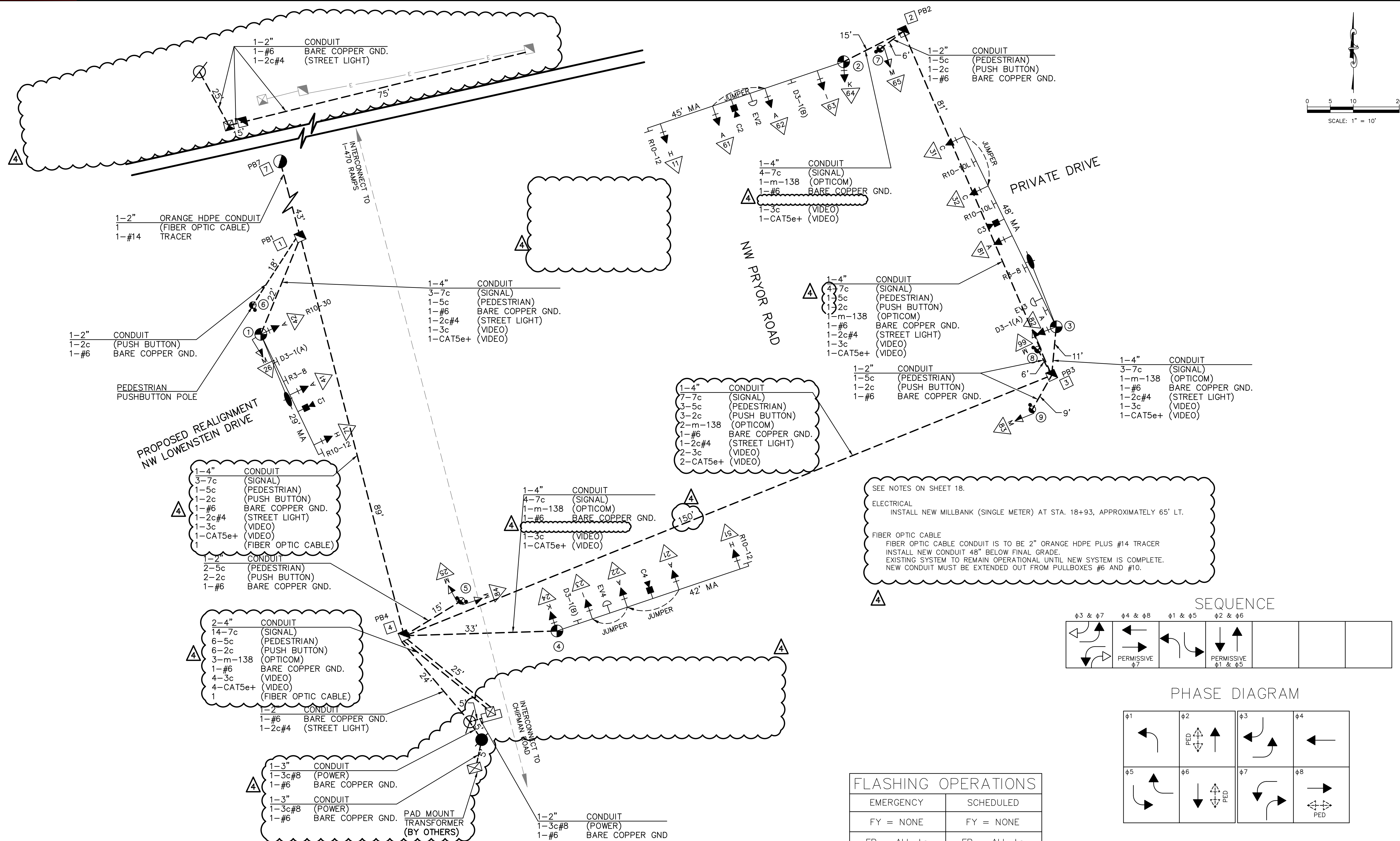
TYPICAL REGULATORY SIGN DETAIL













**WIRING AND PHASING GENERAL NOTES::**

1. THE OUTBOARD SIGNAL HEAD (FURTHEST ON THE MAST ARM FROM THE POLE) FOR EACH PHASE SHALL EACH BE SERVED BY ONE 7c#14 CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. EACH OF THE REMAINING SAME PHASE VEHICLE SIGNAL HEADS LOCATED ON THE MAST ARE SHALL BE CONNECTED TO LIKE PHASE SIGNAL HEADS VIA A 7c#14 CABLE CONNECTED WITHIN THE SIGNAL HEAD TERMINAL BOX. A MAXIMUM OF THREE VEHICLE HEADS MAY BE JOINED TOGETHER AND AN ADDITIONAL SIGNAL HEADS WILL REQUIRE A SEPARATE CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. ALL VEHICULAR SIGNAL HEADS LOCATED ON THE POLE SHALL EACH BE SERVED BY ONE 7c#14 CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. NO CABLE SPLICES ARE ALLOWED, INCLUDING AT THE BASE OF THE POLE AND INSIDE PULL BOXES, EXCEPT FOR STREET LIGHTING CABLES.
2. STREET LIGHTING CABLE, NOT SIGNAL CABLE, MAY BE SPLICED INSIDE OF PULL BOXES USING A SPLIT BOLT CONNECTOR AND RESIN FILLED SPLICE KIT AS DESCRIBED IN SECTION 2800 OF THE TECHNICAL SPECIFICATIONS.
3. A CONTINUOUS 1c #6 AWG BARE SOLID COPPER GROUND WIRE SHALL BE PROVIDED IN ADDITION TO GROUND RODS. ALL GROUNDING AND GROUND RODS SHALL BE TIED TOGETHER USING 1c #6 AWG BARE SOLID COPPER WIRE TO BOND THE SYSTEM.

POWER SUPPLY				
LOCATION	POWER SUPPLY TYPE	CIRCUIT BREAKER TRIP RATINGS		
		SERVICE DISCONNECT (2-POLE)	TRAFFIC SIGNAL (1-POLE)	LIGHTING (2-POLE)
SW CORNER	1 CIRCUIT	40 AMP	30 AMP	15 AMP

FR1	$\phi 1$	$\phi 2$	PED $\phi 2$	$\phi 3$	$\phi 4$	PED $\phi 4$	MONITOR
FR2							
FR1	$\phi 5$	$\phi 6$	PED $\phi 6$	$\phi 7$	$\phi 8$	PED $\phi 8$	
FR2							

STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		 <b>KAW VALLEY ENGINEERING</b> <small>KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19</small>	2318 N. JACKSON   P.O. BOX 1304 JACKSON, MISSOURI 64501 PH. (785) 702-5040   FAX (785) 762-7744 joe@kaweng.com   www.kaweng.com E-21728	LEON D. OSBOURN ENGINEER MO # 021726		REV		DATE		DESCRIPTION	
						4	10-3-19	REVISED PER CITY COMMENTS			
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD & NW LOWENSTEIN DR						3	8-14-19	REVISED PER CITY COMMENTS			
						2	7-24-19	REVISED PER CITY COMMENTS			
						1	7-2-19	REVISED PER CITY COMMENTS			
						0	5-23-19	INITIAL ISSUE			
						19		4			



[illegible]

\* SIGNAL HEAD SPACING (FEET) MAY BE ADJUSTED TO SITE CONDITIONS AS APPROVED BY THE PROJECT INSPECTOR.

SIGNAL HEADS																																						
NO.		HEAD TYPE	OPT. LIMITED	LOUVERS	BACKPLATE	INDICATIONS										WALK	FLASH	DON'T WALK	W/DW	COUNTDOWN INDICATIONS	VISORS		SECTIONS															
POLE	FACE					12" LEDS															TYPE		MOUNTING															
																					TUN.	CUTAWAY																
						R	Y	G	Y←	G←	G↑	Y→	G→	1	2						3	4	5	P	S	P	S	P	M	S	P	M	S	P	M	S	P	M
1	71	H			X	1	1	1	1	1								5																				
	41	A			X	1	1	1	1									3																				
	42	A			X	1	1	1					1	1				5																				
	26	M																																				
2	11	H			X	1	1	1	1	1								5																				
	61	A			X	1	1	1										3																				
	62	A			X	1	1	1										3																				
	63	I			X	1	1	1					1	1				5																				
	64	K			X	1	1	1					1	1				5																				
	65	M																1																				
3	66	M																1																				
	31	C			X	1				1	1																											
	32	C			X	1				1	1																											
	81	A			X	1	1	1																														
	82	A			X	1	1	1																														
	83	M																																				
4	51	H			X	1	1	1	1	1								5																				
	21	A			X	1	1	1																														
	22	A			X	1	1	1																														
	23	I			X	1	1	1					1	1				5																				
	24	K			X	1	1	1					1	1				5																				
5	25	M																1																				
	84	M																1																				
TOTALS						17	15	15	5	5			5	5			6	49																				

P - POLE MOUNTED SIGNAL HEAD

M – MAST ARM MOUNTED SIGNAL HEAD

S - SPAN WIRE MOUNTED SIGNAL HEAD

NOTE:

1. QUANTITIES SHOWN HEREON ARE FOR REFERENCE ONLY.

2. ALIGN FACES OF PEDESTRIAN PUSHBUTTONS PARALLEL TO CROSS-WALK.

3. ALL PULL BOXES LOCATED IN TRAVEL WAY SHALL BE TRAFFIC RATED.

4. THE TRAFFIC SIGNAL SYSTEM SHALL BE COMPLETE AND THE CONTRACTOR SHALL FURNISH AND INSTALL ALL EQUIPMENT AND MATERIALS NECESSARY FOR THE SATISFACTORY OPERATION OF ELECTRICAL APPARATUS AND FOR COMPLETE OPERATION OF THE TRAFFIC SIGNAL SYSTEM WHETHER SPECIFICALLY MENTIONED OR NOT.



[illegible][illegible]

\* CABLE INCLUDED IN LUMP SUM PRICE FOR THE SYSTEM.

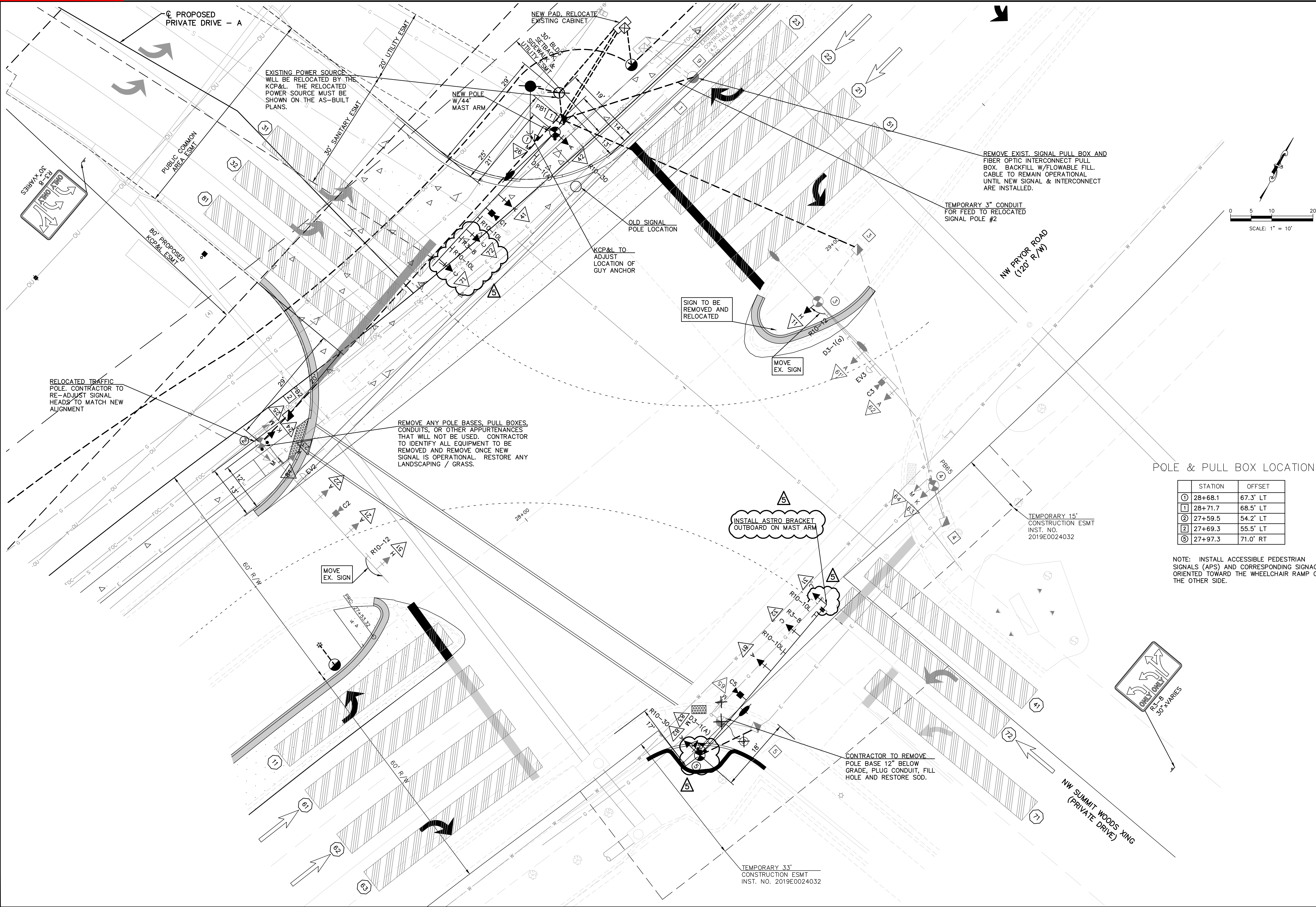
FIBER OPTIC INTERCONNECT CONDUIT & CABLE				
FROM	TO	2" HDPE (ORANGE)	FIBER OPTIC CABLE	#14 STRANDED COPPER TRACER WIRE
[1]	[2]			
[2]	[3]			
[3]	[4]			
[4]	[5]			
[5]	[6]			
[6]	[7]	400'	400'	400'
[7]	[8]	600'	600'	600'
[8]	[9]	515'	515'	515'
[9]	[10]	375'	375'	375'
[7]	[11]	55'	55'	55'
[11]	[4]		89'	
[4]	[X]		25'	
SUBTOTAL (FEET)		1945	2059	1945
BID TOTAL (FEET)		2240	2370	2240

SIGNAL CONDUIT					
	FROM	TO	3" PVC	2" HDPE (GRFV)	4" HDPE
A			10		
				10	
				10	
		4		30	
		4			75
	1	1			30
	1	4			100
	1	6		30	
	2	2			20
	2	3			100
	2	7		12	
	3	3			18
	3	8		12	
	3	9		15	
	4	4			40
	4	5		20	
	4	3			175
	SUBTOTAL (FEET)		10	139	558
	BID TOTAL (FEET)		20	160	640

[illegible]

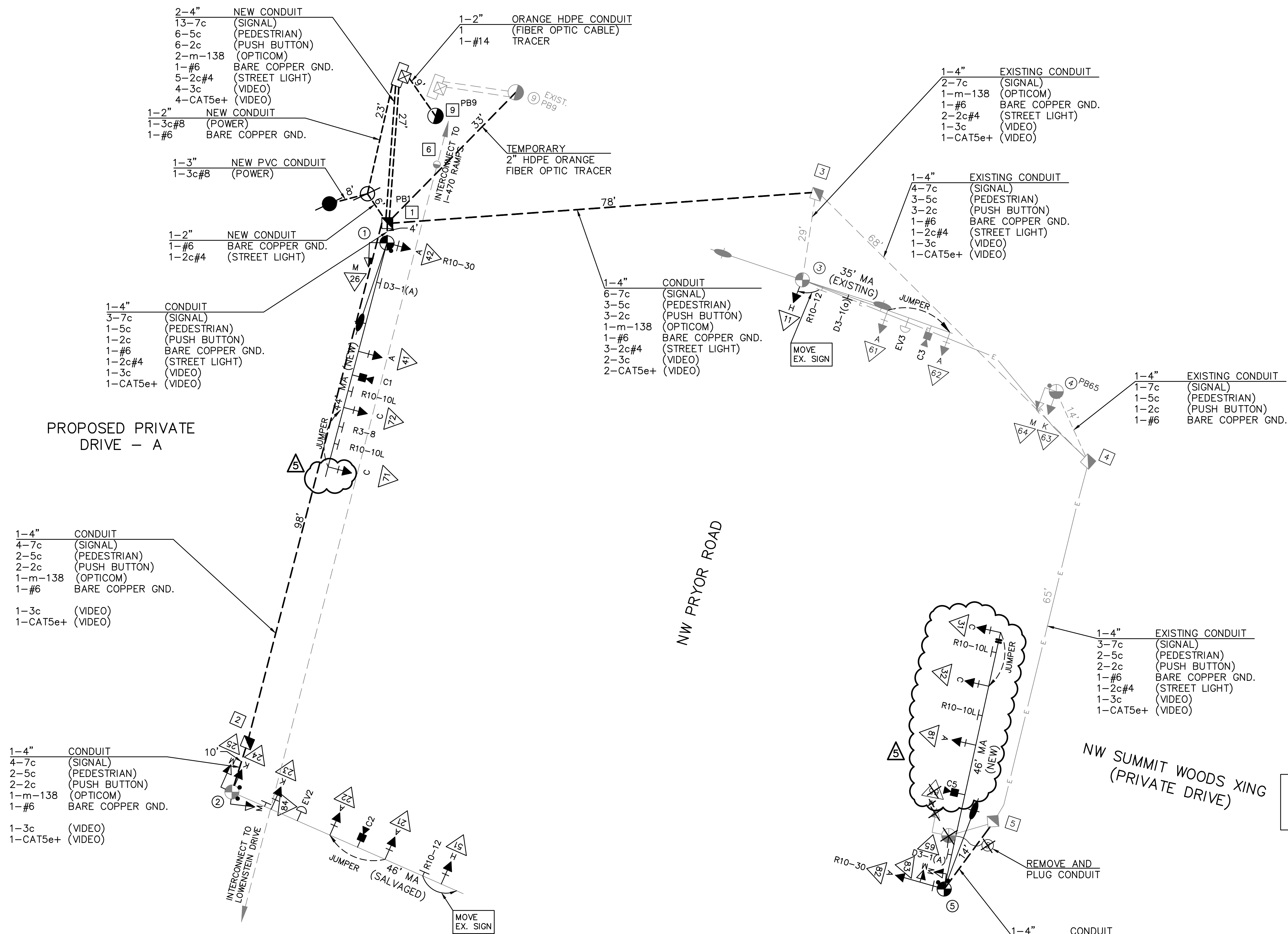
<div>STREETS OF WEST PRYOR NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI</div> <div>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING TRAFFIC SIGNAL QUANTITIES - NW PRYOR RD &amp; NW LOWENSTEIN DR</div>		<div><div>KAW VALLEY ENGINEERING</div><div>KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19</div></div> <div>2316 N. JACKSON   P.O. BOX 1304 JACKSON, MISSOURI 64501 PH. (785) 762-5040   FAX (785) 762-7744 joe@kveng.com   www.kveng.com</div>		<div>LEON D. 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<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		PROJ. NO. <b>A14-7067-1</b> DESIGNER <b>LDO</b> DRAWN BY <b>JT/BKR</b> CFN <b>7067-1SIG_TSP</b> SHEET		<b>21</b>	
<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b> <b>TRAFFIC SIGNAL PLAN - NW PRYOR RD &amp; NW SUMMIT WOODS</b>		REV		<b>5</b>	





FLASHING OPERATIONS	
EMERGENCY	SCHEDULED
FY = NONE	FY = NONE
FR = ALL $\phi$ s	FR = ALL $\phi$ s

POWER SUPPLY				
LOCATION	POWER SUPPLY TYPE	CIRCUIT BREAKER TRIP RATINGS		
		SERVICE DISCONNECT (2-POLE)	TRAFFIC SIGNAL (1-POLE)	LIGHTING (2-POLE)
NW CORNER	1 CIRCUIT	40 AMP	30 AMP	15 AMP

SEQUENCE

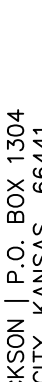

# PHASE DIAGRAM

<p>φ1</p>	<p>φ2</p>	<p>φ3</p>	<p>φ4</p>
<p>φ5</p>	<p>φ6</p>	<p>φ7</p>	<p>φ8</p>

FR1	$\phi 1$	$\phi 2$	PED $\phi 2$	$\phi 3$	$\phi 4$	PED $\phi 4$	MONITOR
FR2							
FR1	$\phi 5$	$\phi 6$	PED $\phi 6$	$\phi 7$	$\phi 8$	PED $\phi 8$	
FR2							

**WIRING AND PHASING GENERAL NOTES:**

1. THE OUTBOARD SIGNAL HEAD (FURTHER ON THE MAST ARM FROM THE POLE) FOR EACH PHASE SHALL EACH BE SERVED BY ONE 7c#14 CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. EACH OF THE REMAINING SAME PHASE VEHICLE SIGNAL HEADS LOCATED ON THE MAST ARE SHALL BE CONNECTED TO LIKE PHASE SIGNAL HEADS VIA A 7c#14 CABLE CONNECTED WITHIN THE SIGNAL HEAD TERMINAL BOX. A MAXIMUM OF THREE VEHICLE HEADS MAY BE JOINED TOGETHER, ANY ADDITIONAL SIGNAL HEADS WOULD REQUIRE A SEPARATE CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. ALL VEHICULAR SIGNAL HEADS LOCATED ON THE POLE SHALL EACH BE SERVED BY ONE 7c#14 CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. NO CABLE SPLICES ARE ALLOWED, INCLUDING AT THE BASE OF THE POLE AND INSIDE PULL BOXES, EXCEPT FOR STREET LIGHTING CABLES.
2. STREET LIGHTING CABLE, NOT SIGNAL CABLE, MAY BE SPLICED INSIDE OF PULL BOXES USING A SPLIT BOLT CONNECTOR AND RESIN FILLED SPLICE KIT AS DESCRIBED IN SECTION 2800 OF THE TECHNICAL SPECIFICATIONS.
3. A CONTINUOUS 1c #6 AWG BARE SOLID COPPER GROUND WIRE SHALL BE PROVIDED IN ADDITION TO GROUND RODS. ALL GROUNDING AND GROUND RODS SHALL BE TIED TOGETHER USING 1c #6 AWG BARE SOLID COPPER WIRE TO BOND THE SYSTEM.
4. ALL SIGNAL HEADS FOR PHASES 3/8 ARE WIRED INDEPENDENTLY TO ALLOW FOR A SINGLE PROTECTED/PERMISSIVE LEFT TURN AND A THROUGH OR FOR DUAL LEFTS.

<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		PROJ. NO. <b>A14 7067-1</b>		 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19		2319 N. JACKSON   P.O. BOX 1304 PH. (765) 762-5040   FAX (765) 762-7744 <a href="http://www.kveng.com">www.kveng.com</a> <a href="mailto:jk@kveng.com">jk@kveng.com</a>		LEON D. OSBOURN ENGINEER MO # 021726				5 10-18-19 REVISED PER CITY COMMENTS		LDO JIT LDO	
		DESIGNER <b>LDO</b>				DRAWN BY <b>JT/BKR</b>		3 8-14-19 REVISED PER CITY COMMENT		LDO JIT LDO					
<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b> <b>TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD &amp; NW SUMMIT WOODS</b>		CFN <b>7067-1SIG_WIRE</b>		2 7-24-19 REVISED PER CITY COMMENTS		LDO JIT LDO									
		SHEET <b>22</b>		REV <b>5</b>		1 7-2-19 REVISED PER CITY COMMENTS		LDO JIT LDO							
						0 5-23-19 INITIAL ISSUE		LDO JIT LDO		REV DATE DESCRIPTION		DSN DWN CHK			



[illegible]

\* SIGNAL HEAD SPACING (FEET) MAY BE ADJUSTED TO SITE CONDITIONS AS APPROVED BY THE PROJECT INSPECTOR.

[illegible]

P - POLE MOUNTED SIGNAL HEAD

M – MAST ARM MOUNTED SIGNAL HEAD

S - SPAN WIRE MOUNTED SIGNAL HEAD

NOTE:

1. QUANTITIES SHOWN HEREON ARE FOR REFERENCE ONLY.

2. ALIGN FACES OF PEDESTRIAN PUSHBUTTONS PARALLEL TO CROSS-WALK.



























3. ALL PULL BOXES LOCATED IN TRAVEL WAY SHALL BE TRAFFIC RATED.

4. THE TRAFFIC SIGNAL SYSTEM SHALL BE COMPLETE AND THE CONTRACTOR SHALL FURNISH AND INSTALL ALL EQUIPMENT AND MATERIALS NECESSARY FOR THE SATISFACTORY OPERATION OF ELECTRICAL APPARATUS AND FOR COMPLETE OPERATION OF THE TRAFFIC SIGNAL SYSTEM WHETHER SPECIFICALLY MENTIONED OR NOT.

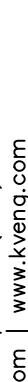
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CONTROLLER AND EQUIPMENT	TOTALS
CABINET AND ACCESSORIES: NEMA TYPE P TS1 CABINET	—EXISTING—
CONTROLLER: TYPE 3608 M 52 EAGLE EPAC COMPLETE PER PLANS, INCLUDING SOFTWARE	—EXISTING—
EMERGENCY VEHICLE DETECTION SYSTEM (COMPLETE): 3M REMOVE FROM PRIVATE DRIVE FOR LOWENSTEIN	—1
VIDEO DETECTION SYSTEM (COMPLETE): INSYNC ADAPTIVE TRAFFIC CONTROL SYSTEM	+1 INSYNC
FIBER DISTRIBUTION UNIT:	
1—CIRCUIT POWER SUPPLY (MODOT TYPE II POWER SUPPLY WITH LIGHTING CIRCUITS)	1
GROUND RODS: REUSE EXISTING	2
PUSHBUTTON DETECTORS	6
FIBER OPTIC DATA LINK SWITCH: KYLAND SICOM 3170	1
GROUND RODS	7
PUSHBUTTON DETECTORS	5

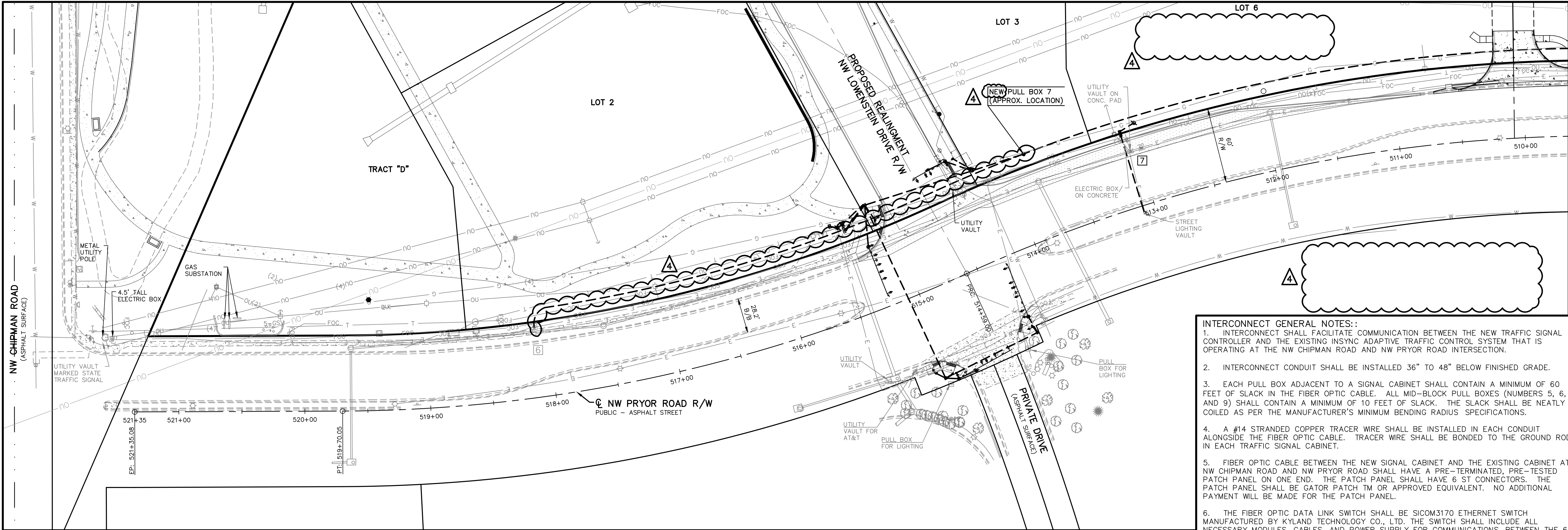
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SIGNAL CONDUIT					
FROM	TO	2" PVC	3" PVC	2" HDPE (GREY)	4" HDPE
			8		
				23	
				6	
					54
					4
					98
					10
					78
					29
					68
					14
					65
					14
SL-1	8A	10			
SL-2	8B	10			
SUBTOTAL (FEET)		50	8	29	434
BID TOTAL (FEET)		60	10	33	500

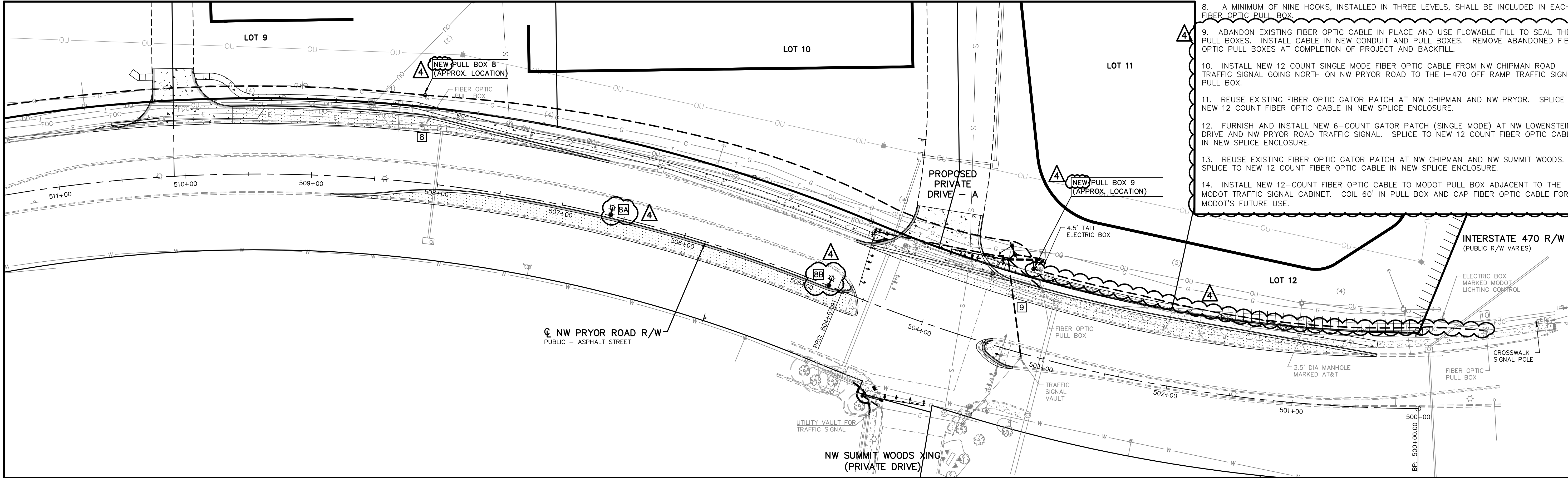
SIGNS				
SIGN	LEGEND	NO.	SF.	TOTAL S.F.
R10-10L	LEFT TURN	4	5.0	20.0
R10-12	LTYOG	2	5.0	10.0
R10-3E	PED CROSSING	6	—	—
R10-30	RTORMYTUT	2	7.5	15.0
D3-1(A)	NW PRYOR RD	1	7.1	7.1
R3-8	LANE USAGE	2		
TOTAL				52.1

STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		K&V KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19		2319 N. JACKSON   P.O. BOX 104 JONESBORO, MO 64501 PH. (785) 762-5040   FAX. (785) 762-7744 jc@kveng.com   www.kveng.com		LEON D. OSBORN ENGINEER MO # 021726				5		10-18-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
										4		10-3-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING TRAFFIC SIGNAL QUANTITIES - NW PRYOR RD & NW SUMMIT WOODS		PROJ. NO. A14 7067-1		DESIGNER LDO		DRAWN BY JT/BKR		CFN		3		8-14-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
										2		7-24-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
SHEET 23		REV		5		0		7-2-19		1		7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
										0		5-23-19	INITIAL ISSUE	LDO	JT	LDO
										REV	DATE	DESCRIPTION	DSN	DWN	CHK	





- INTERCONNECT GENERAL NOTES: :**
1. INTERCONNECT SHALL FACILITATE COMMUNICATION BETWEEN THE NEW TRAFFIC SIGNAL CONTROLLER AND THE EXISTING INSYNC ADAPTIVE TRAFFIC CONTROL SYSTEM THAT IS OPERATING AT THE NW CHIPMAN ROAD AND NW PRYOR ROAD INTERSECTION.
  2. INTERCONNECT CONDUIT SHALL BE INSTALLED 36" TO 48" BELOW FINISHED GRADE.
  3. EACH PULL BOX ADJACENT TO A SIGNAL CABINET SHALL CONTAIN A MINIMUM OF 60 FEET OF SLACK IN THE FIBER OPTIC CABLE. ALL MID-BLOCK PULL BOXES (NUMBERS 5, 6, 7, AND 9) SHALL CONTAIN A MINIMUM OF 10 FEET OF SLACK. THE SLACK SHALL BE NEATLY COILED AS PER THE MANUFACTURER'S MINIMUM BENDING RADIUS SPECIFICATIONS.
  4. A #14 STRANDED COPPER TRACER WIRE SHALL BE INSTALLED IN EACH CONDUIT ALONGSIDE THE FIBER OPTIC CABLE. TRACER WIRE SHALL BE BONDED TO THE GROUND ROD IN EACH TRAFFIC SIGNAL CABINET.
  5. FIBER OPTIC CABLE BETWEEN THE NEW SIGNAL CABINET AND THE EXISTING CABINET AT NW CHIPMAN ROAD AND NW PRYOR ROAD SHALL HAVE A PRE-TERMINATED, PRE-TESTED PATCH PANEL ON ONE END. THE PATCH PANEL SHALL HAVE 6 ST CONNECTORS. THE PATCH PANEL SHALL BE GATOR PATCH TM OR APPROVED EQUIVALENT. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE PATCH PANEL.
  6. THE FIBER OPTIC DATA LINK SWITCH SHALL BE SICOM3170 ETHERNET SWITCH MANUFACTURED BY KYLAND TECHNOLOGY CO., LTD. THE SWITCH SHALL INCLUDE ALL NECESSARY MODULES, CABLES, AND POWER SUPPLY FOR COMMUNICATIONS BETWEEN THE 6MM FIBER OPTIC CABLES, SIGNAL CONTROLLER, AND INSYNC PROCESSOR.
  7. FIBER OPTIC CABLE WILL NOT BE INSTALLED INTO THE EXISTING SIGNAL CABINET AT THE I-470 RAMPS. THE SPECIFIED LENGTH OF CABLE WILL BE ROUTED FROM THE NEW SIGNAL CABINET TO THE EXISTING PULL BOX AT STA. 499+44. THE CABLE WILL BE NEATLY COILED AND END CAPPED. MODOT WILL INSTALL THE FIBER INTO THE SIGNAL CABINET AT A LATER DATE.
  8. A MINIMUM OF NINE HOOKS, INSTALLED IN THREE LEVELS, SHALL BE INCLUDED IN EACH FIBER OPTIC PULL BOX.
  9. ABANDON EXISTING FIBER OPTIC CABLE IN PLACE AND USE FLOWABLE FILL TO SEAL THE PULL BOXES. INSTALL CABLE IN NEW CONDUIT AND PULL BOXES. REMOVE ABANDONED FIBER OPTIC PULL BOXES AT COMPLETION OF PROJECT AND BACKFILL.
  10. INSTALL NEW 12 COUNT SINGLE MODE FIBER OPTIC CABLE FROM NW CHIPMAN ROAD TRAFFIC SIGNAL GOING NORTH ON NW PRYOR ROAD TO THE I-470 OFF RAMP TRAFFIC SIGNAL PULL BOX.
  11. REUSE EXISTING FIBER OPTIC GATOR PATCH AT NW CHIPMAN AND NW PRYOR. SPLICE TO NEW 12 COUNT FIBER OPTIC CABLE IN NEW SPLICE ENCLOSURE.
  12. FURNISH AND INSTALL NEW 6-COUNT GATOR PATCH (SINGLE MODE) AT NW LOWENSTEIN DRIVE AND NW PRYOR ROAD TRAFFIC SIGNAL. SPLICE TO NEW 12 COUNT FIBER OPTIC CABLE IN NEW SPLICE ENCLOSURE.
  13. REUSE EXISTING FIBER OPTIC GATOR PATCH AT NW CHIPMAN AND NW SUMMIT WOODS. SPLICE TO NEW 12 COUNT FIBER OPTIC CABLE IN NEW SPLICE ENCLOSURE.
  14. INSTALL NEW 12-COUNT FIBER OPTIC CABLE TO MODOT PULL BOX ADJACENT TO THE MODOT TRAFFIC SIGNAL CABINET. COIL 60' IN PULL BOX AND CAP FIBER OPTIC CABLE FOR MODOT'S FUTURE USE.



REV	DATE	DESCRIPTION	DSN	DWN	CHK
4	10-3-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
3	8-14-19	REVISED PER CITY COMMENT	LDO	JT	LDO
2	7-24-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO
0					

STATE OF MISSOURI  
LEON D. OSBOURN  
ENGINEER  
NO. 021726

2319 N. JACKSON | P.O. BOX 304  
JUNCTION CITY, KANSAS 66441  
PH. (785) 762-5040 | FAX (785) 762-7744  
joe@kve.com | www.kve.com

**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI  
EXPIRES 12/31/19

**STREETS OF WEST PRYOR**  
NW & NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
TRAFFIC INTERCONNECT PLAN, NOTES AND WIRING

PROJ. NO. **A14-7067-1**

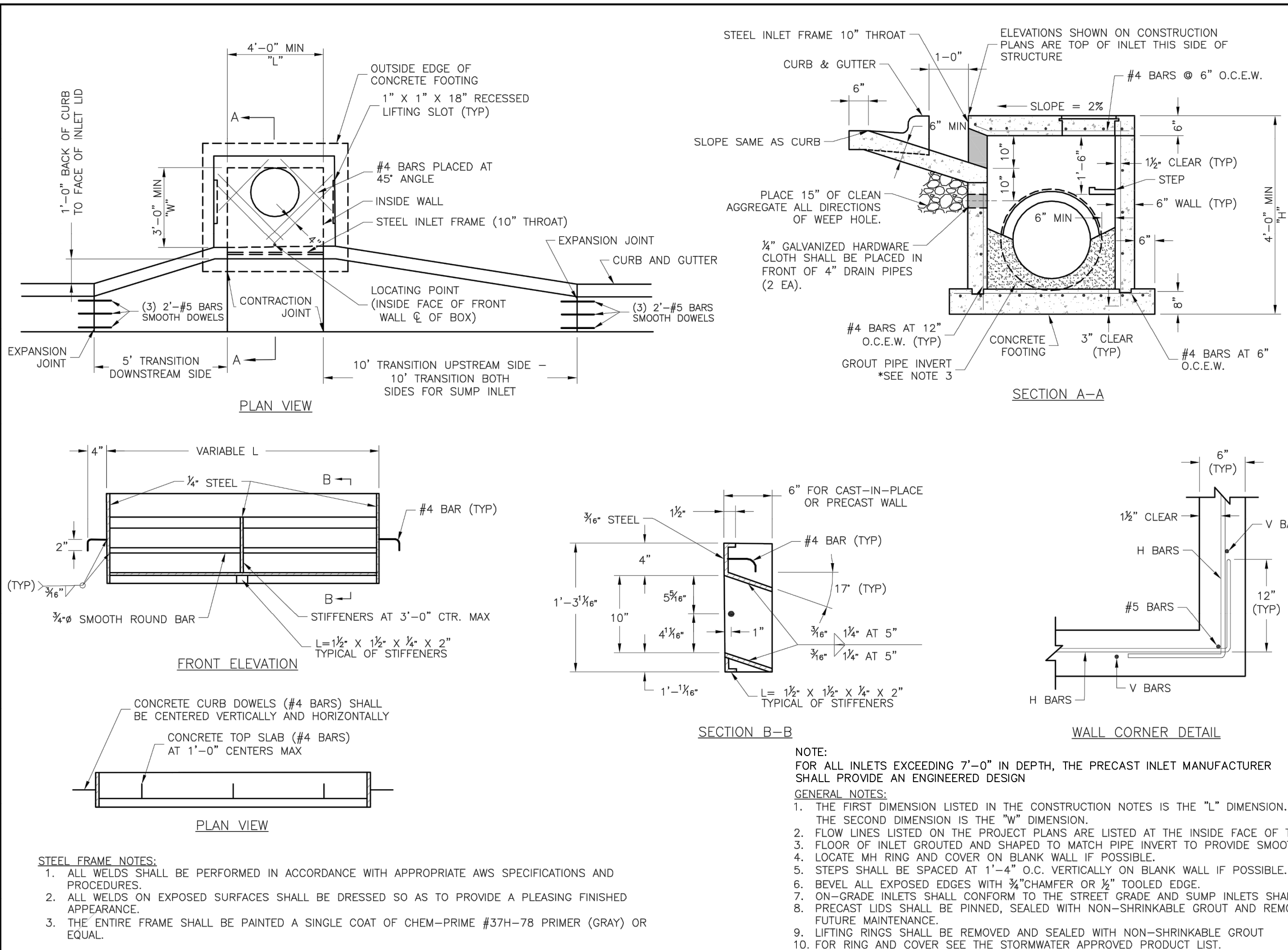
DESIGNER **LDO** DRAWN BY **JT/BKR**

CFN **7067-1SIC\_CONN**

SHEET **24** REV **4**

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ADDED SHEET



LEON D. OSBOURN  
ENGINEER  
MO # 021726

 2319 N. JACKSON | P.O. BOX 1304  
JUNCTION CITY, KANSAS 66441  
PH (785) 762-5040 | FAX (785) 762-7744  
[www.kveeng.com](http://www.kveeng.com)

**KAW VALLEY ENGINEERING**

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 0000842.



**LS** LEE'S SUMMIT  
MISSOURI



GREEN STREET | LEE'S SUMMIT, MO 64063

**STREETS OF WEST PRYOR**  
NW/4 NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
STORM SEWER DETAIL SHEET**

Project: STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO

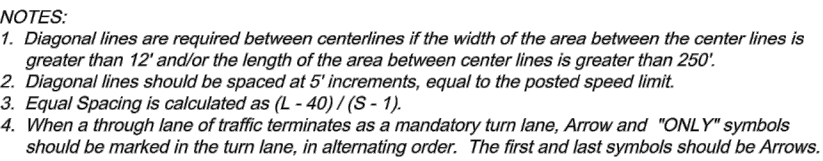
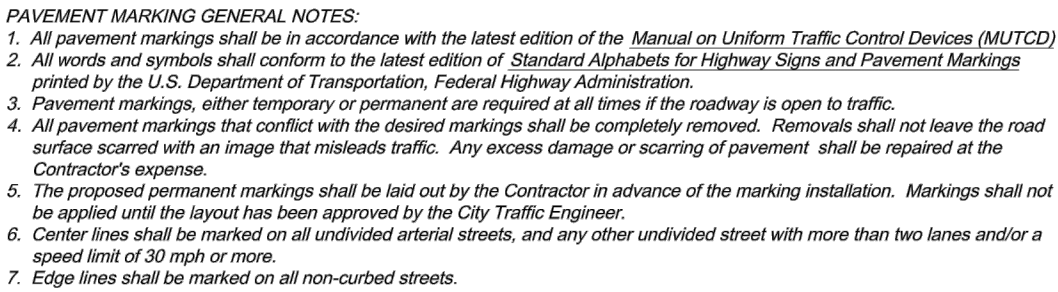
Drawn By: MJF
Checked By: DL
Date: 04/17
Proj. #:

STM

PROJ. NO.		A14_7067-1	
DESIGNER		DRAWN BY	
LDO		JT	
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7067-1SIG_DET			
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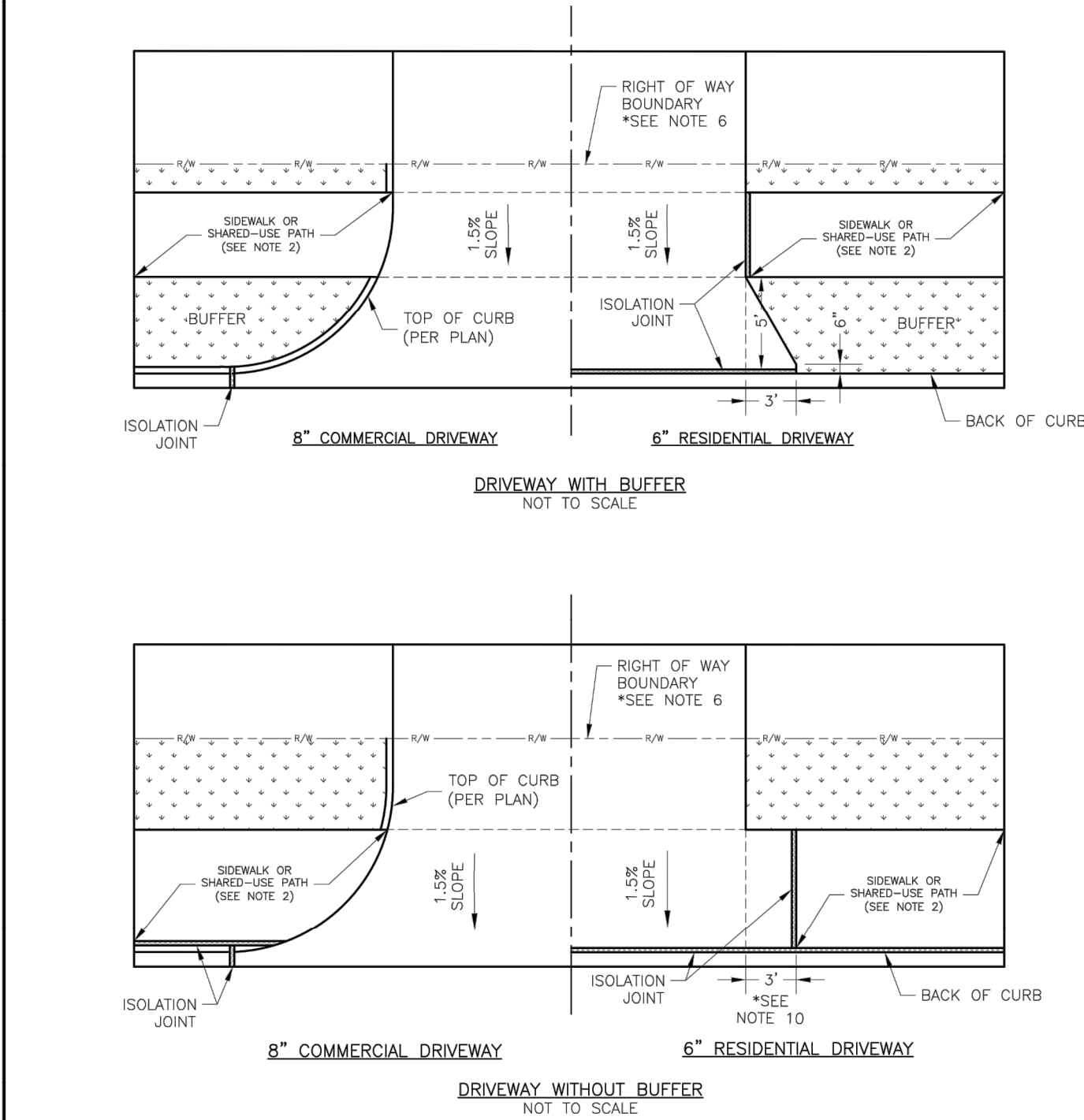


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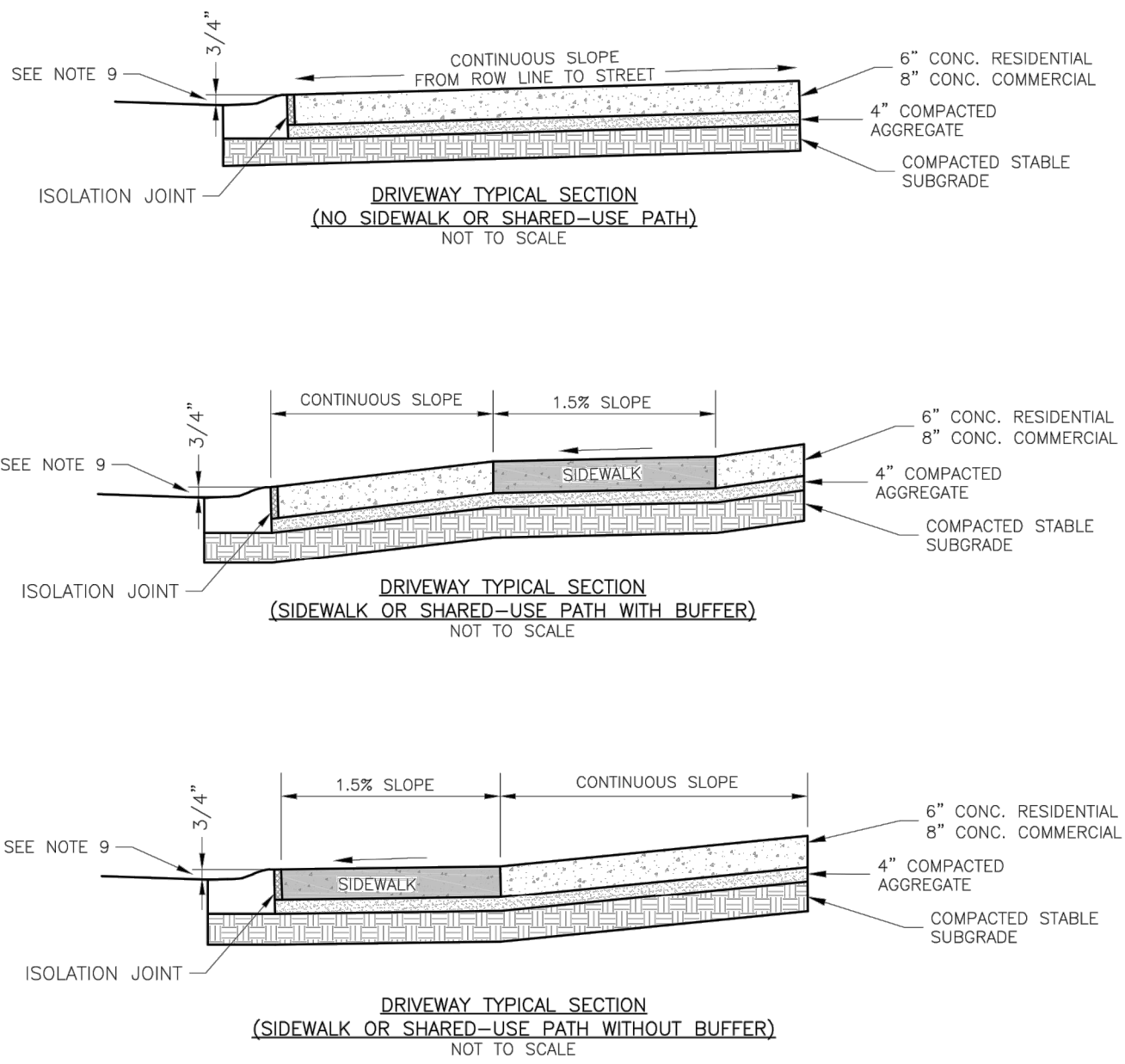








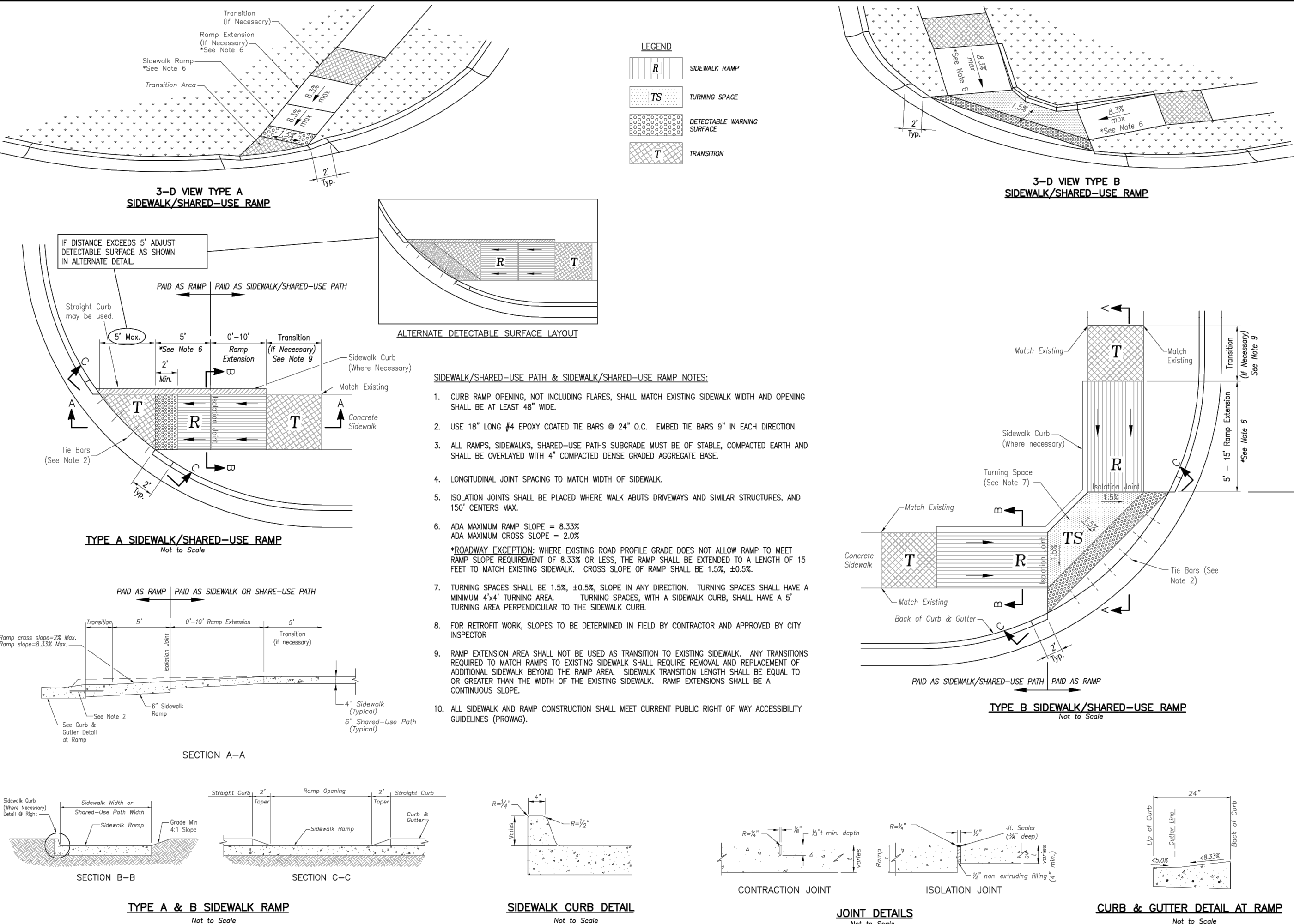
- GENERAL NOTES
- SUBGRADE SHALL BE STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
  - ALL DRIVE APPROACHES SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG) FOR SLOPE REQUIREMENTS WHEN SIDEWALK IS REQUIRED (SEE ADA RAMP RETROFIT DETAIL GEN-35, SIDEWALK/SHARED USE PATH RAMP AT DRIVEWAY DETAIL).
  - JOINT AT BACK OF CURB LINE SHALL BE AN ISOLATION JOINT FOR RESIDENTIAL DRIVEWAYS.
  - KCMMB 4K CONCRETE MIX IS REQUIRED FOR ALL CURBS.
  - COMMERCIAL DRIVEWAYS, IN THE PUBLIC RIGHT OF WAY, SHALL BE KCMMB 4K CONCRETE MIX.
  - RESIDENTIAL DRIVEWAYS, IN THE PUBLIC RIGHT OF WAY, KCMMB 4K CONCRETE MIX IS RECOMMENDED. OTHER CONCRETE MIXES NEEDS TO BE APPROVED BY CITY INSPECTOR.
  - A JOINT MUST BE INSTALLED AT THE RIGHT OF WAY BOUNDARY FOR PROPERTY DELINEATION.
  - WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.
  - 3/4" FROM TOP OF CURB TO FLOWLINE AT DRIVEWAY (TYPE CG-1 CURB ONLY). MUST MAINTAIN ORIGINAL FLOWLINE OF CURB.
  - SIDEWALK ADJOINING CURB SHALL BE 6" THICK, EXTENDING 3' FROM THE DRIVEWAY.
  - THE MAXIMUM WIDTH OF A RESIDENTIAL DRIVEWAY IS 36 FEET WITHIN THE RIGHT OF WAY.



LEE'S SUMMIT MISSOURI

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO  
DRIVEWAY DETAIL

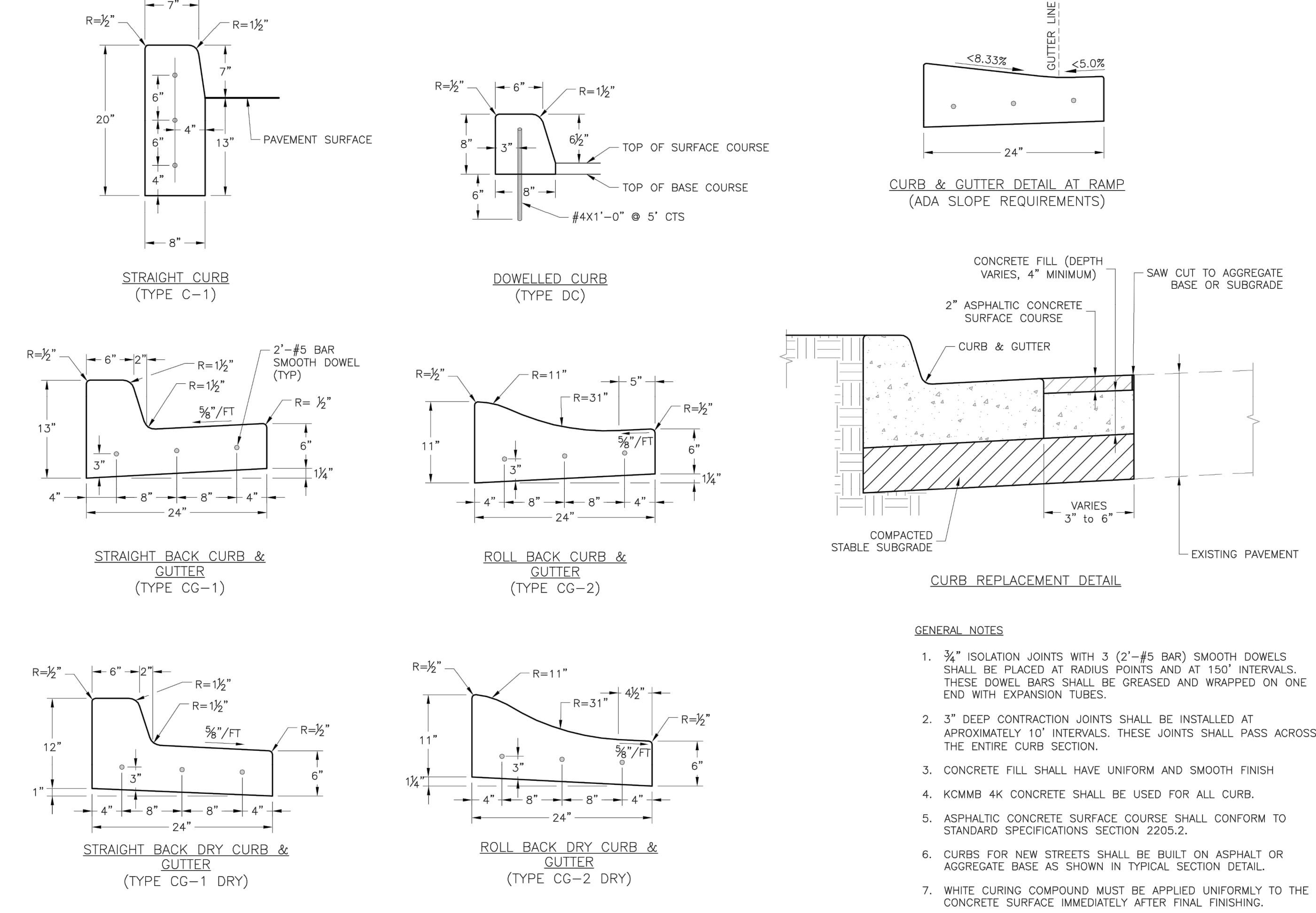
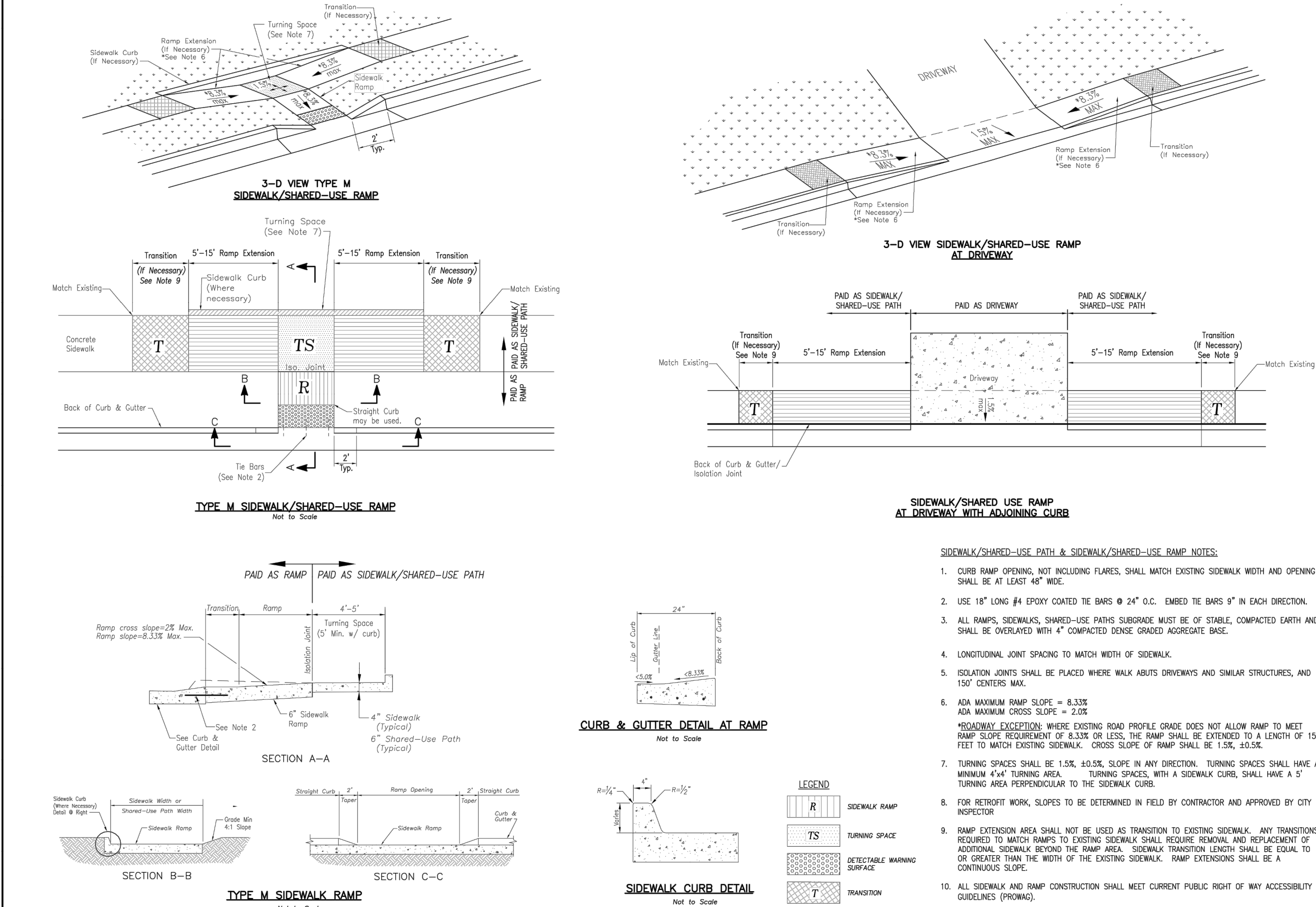
GEN-1



LEE'S SUMMIT MISSOURI

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO  
ADA RAMP RETROFIT DETAIL

GEN-3B



LEE'S SUMMIT MISSOURI

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO  
ADA RAMP RETROFIT DETAIL

GEN-3A

LEE'S SUMMIT MISSOURI

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO  
CURB & GUTTER DETAIL

GEN-4



LEON D. OSBOURN  
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PH. (785) 762-5040 | FAX (785) 762-7744  
joe@kvw.com | www.kvw.com

KAW VALLEY ENGINEERING

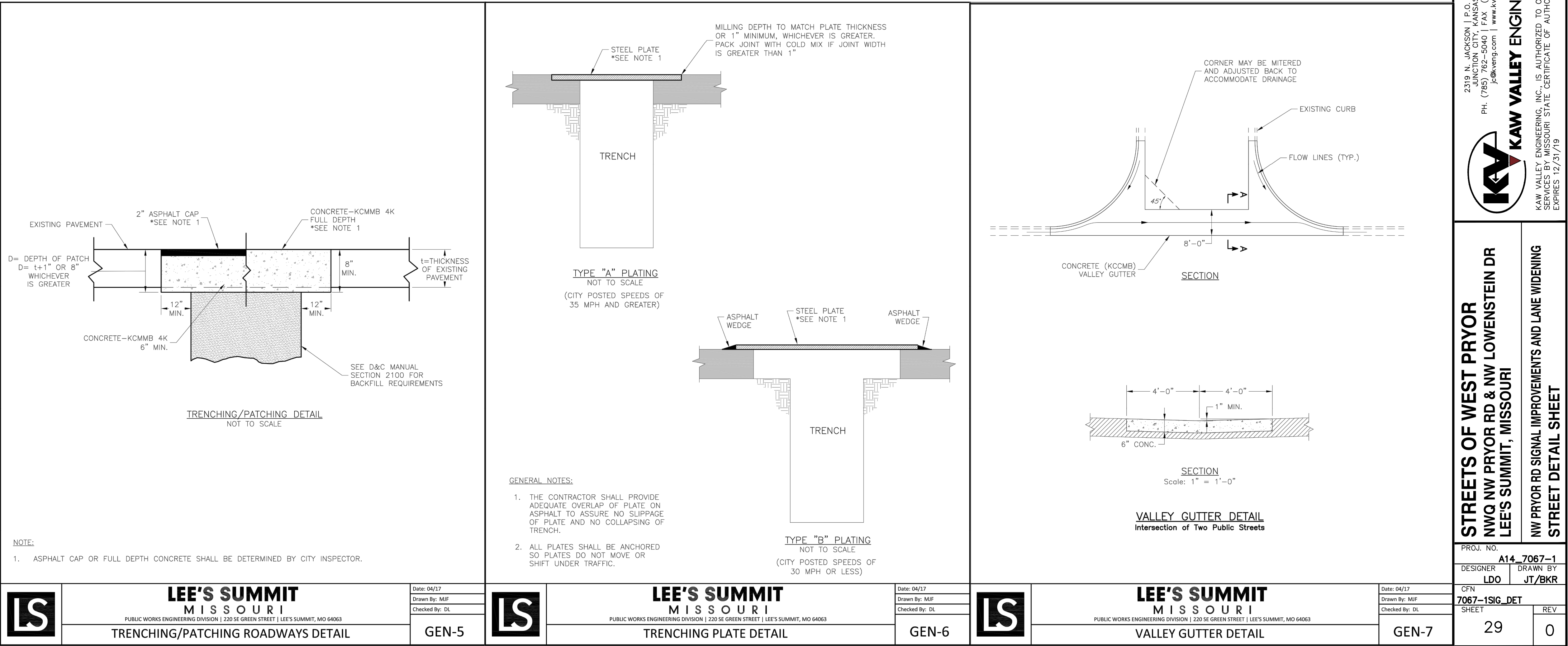
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES  
MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842  
EXPIRES 12/31/19

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

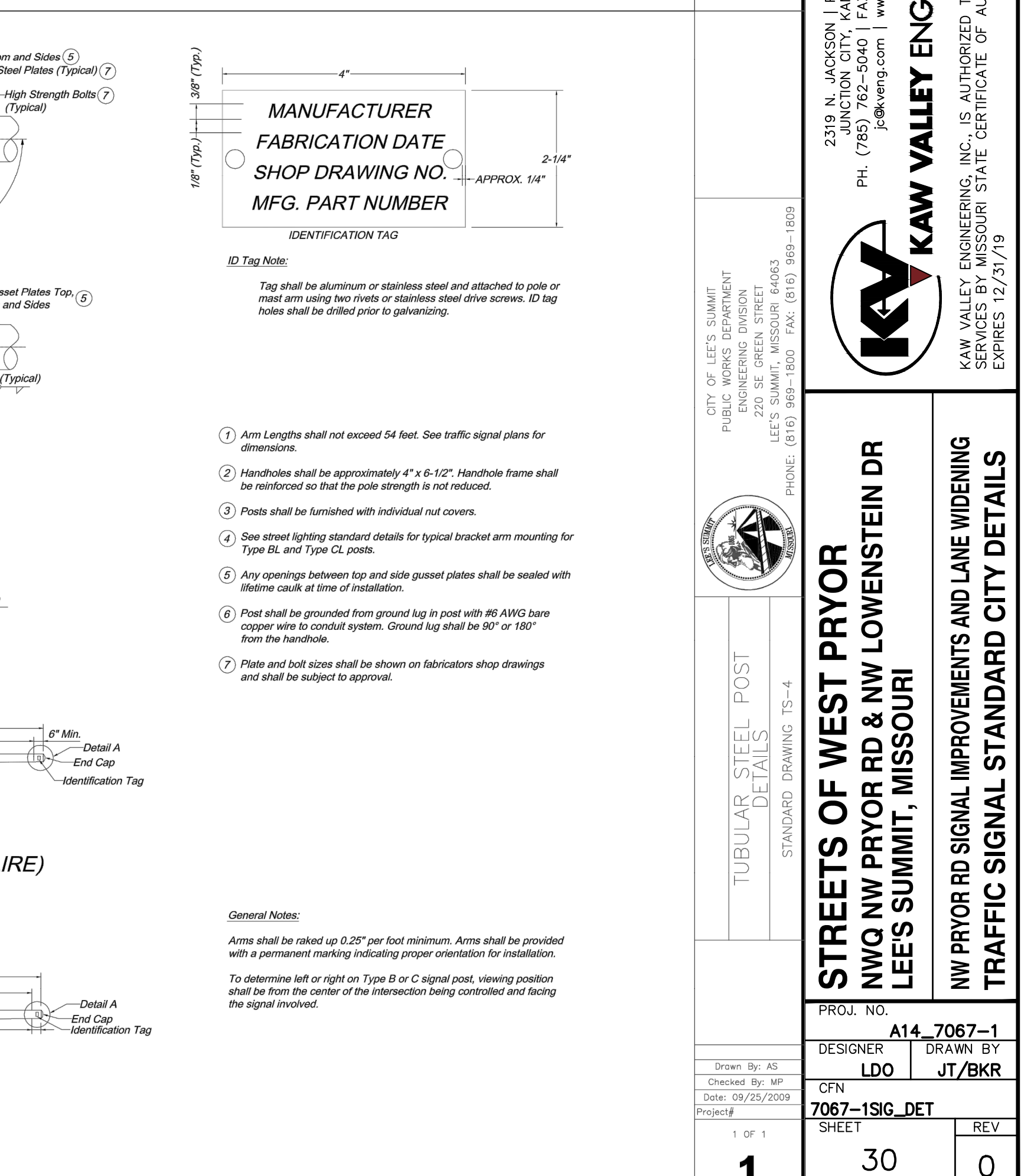
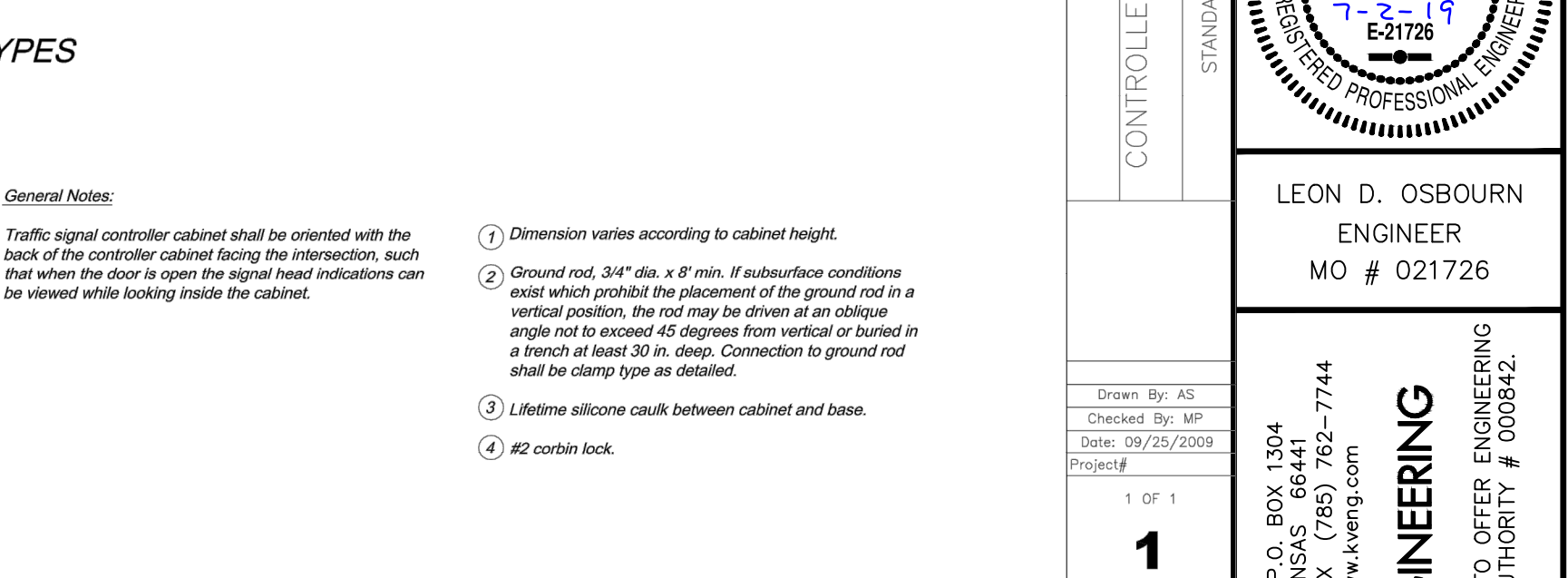
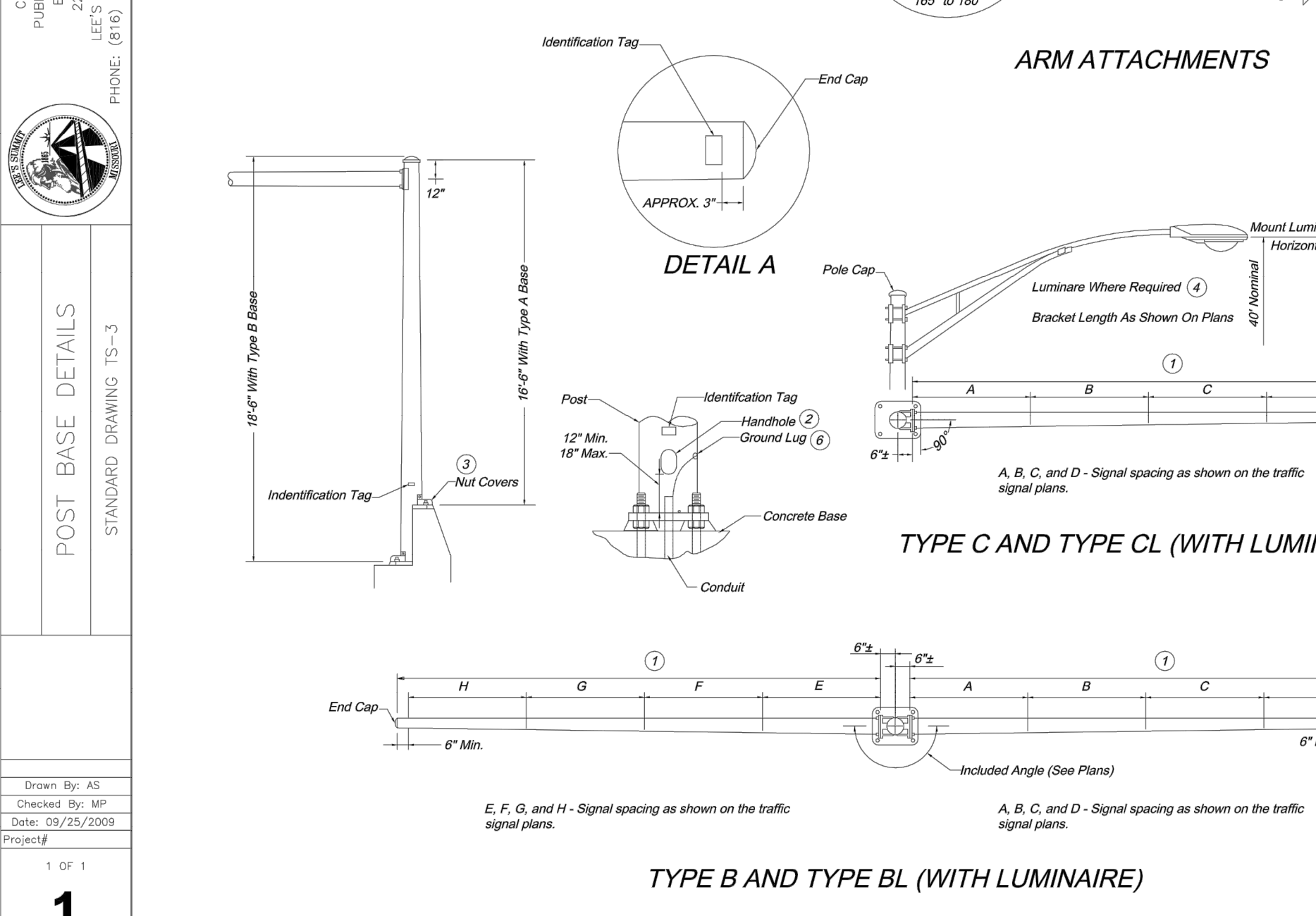
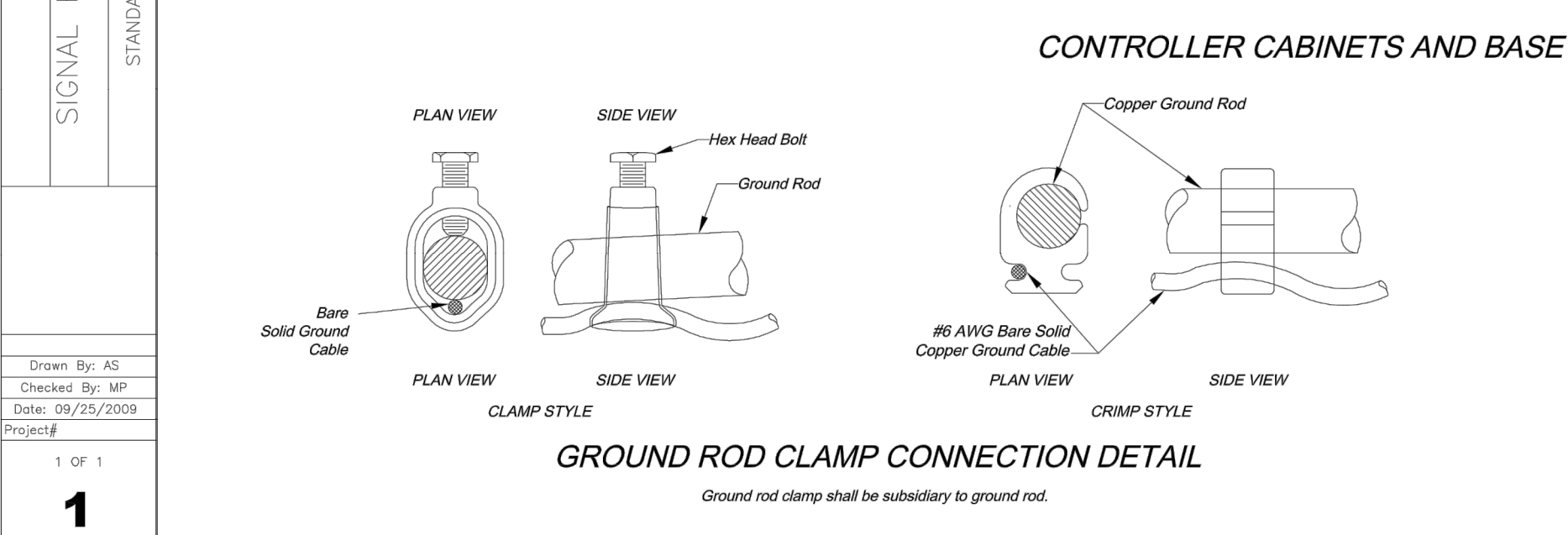
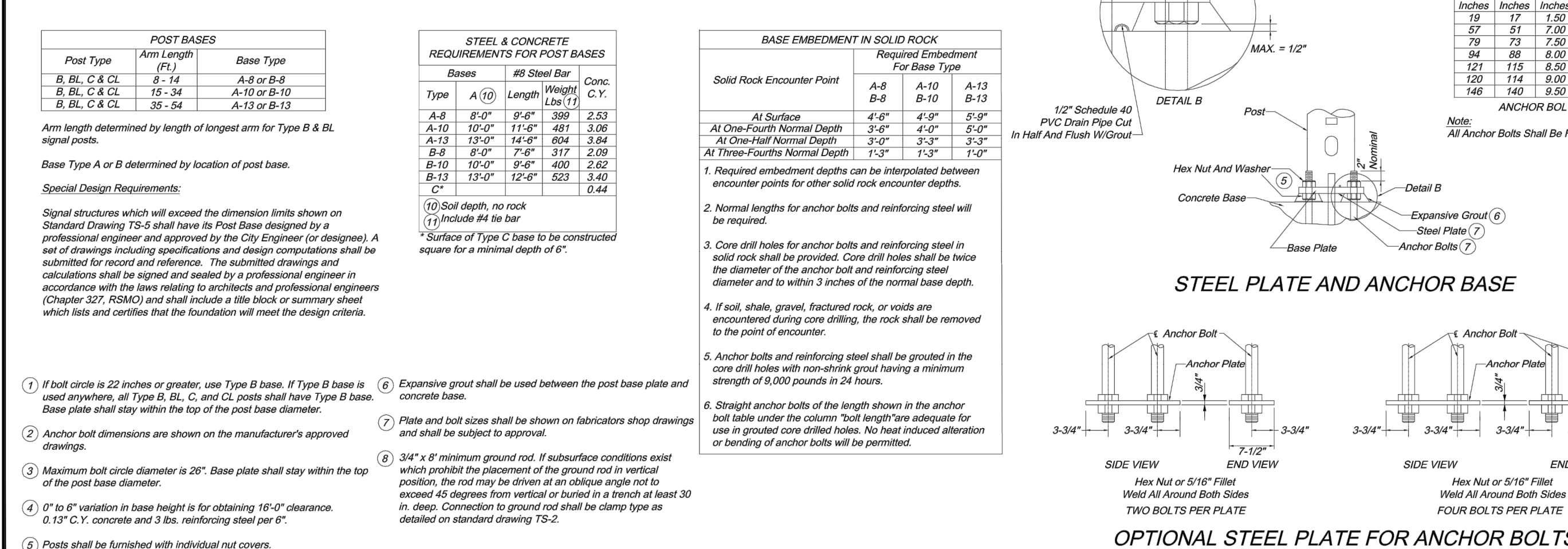
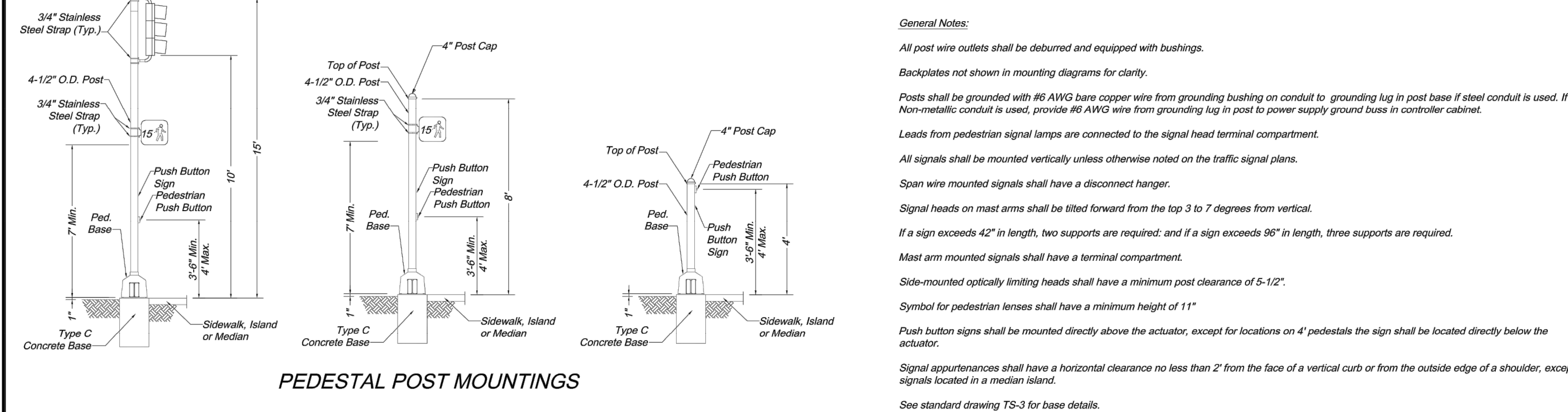
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
STREET DETAIL SHEET

PROJ. NO. A14-7067-1  
DESIGNER LDO  
DRAWN BY JT/BKR  
CFN  
7067-1SIG\_DET  
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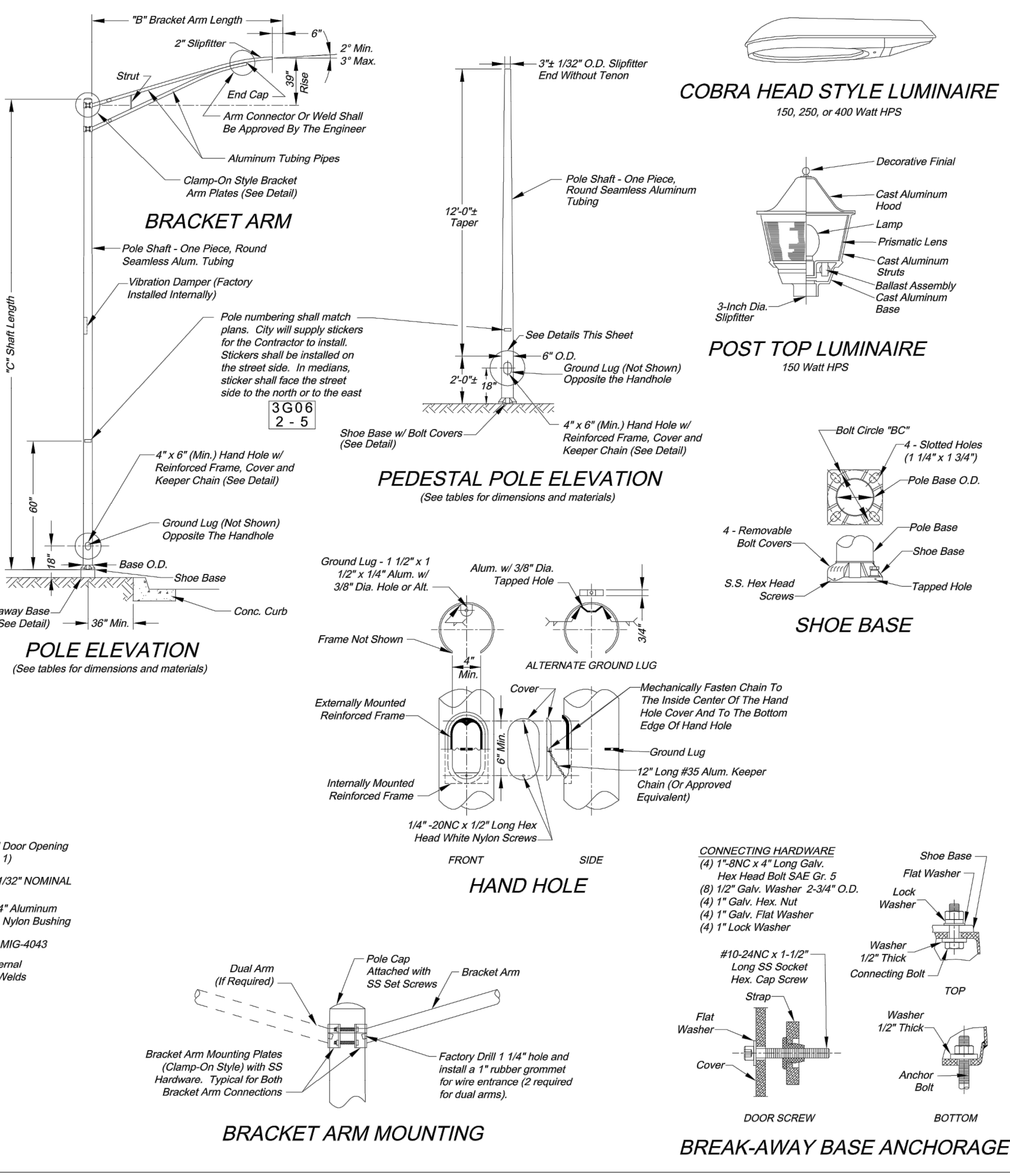
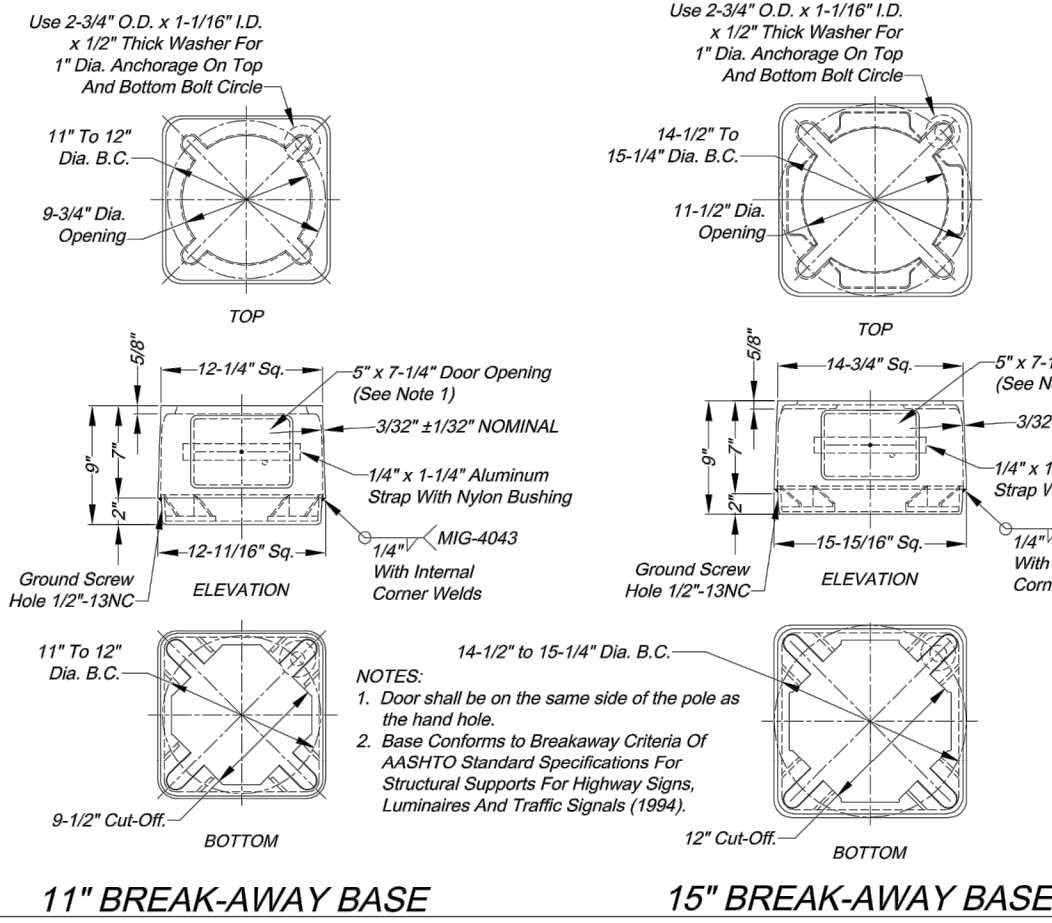
STREET LIGHT POLE, BRACKET ARM, AND BREAK-AWAY BASE

POLE TYPE	MOUNTING HEIGHT (A)	BRACKET ARMS		POLE SHAFT		SHOE BASE		ANCHOR BOLTS	
		LENGTH (B)	ARM 2	BASE O.D.	MIN. WALL THICKNESS (C)	SHAFT LENGTH (D)	BOLT CIRCLE (E)	DIAMETER	LENGTH
P14	14'	-	-	6"	3"	0.156"	14'-0"	9.5"	0.75" 10NC
P30S	30'	6" or 10"	6" or 10"	6"	6"	0.188"	26'-6"	11.0"	1.00" 8NC
P30D	30'	6" or 10"	6" or 10"	6"	6"	0.219"	26'-6"	11.0"	1.00" 8NC
P40S	40'	6" or 10" or 15"	6" or 10" or 15"	6"	6"	0.219"	38'-6"	11.5"	1.00" 8NC
P40D	40'	6" or 10" or 15" or 18"	6" or 10" or 15" or 18"	6"	6"	0.219"	38'-6"	14.5"	1.00" 8NC

- NOTES:
- All poles, arms, and miscellaneous equipment shall conform to these details and as specified by the latest city standard specifications.
  - Pole shaft shall have a satin ground finish.
  - All hardware (bolts, nuts, washers but not including anchor bolts) not otherwise specifically designated in the specifications or details shall be 305-series stainless steel conforming to ASTM A193 or A194.
  - Anchor bolts shall be used with concrete bases. Anchor bolts shall be steel with 50,000 PSI minimum yield, top 10" min. galvanized, including 8 nuts and 8 flat washers galvanized to ASTM A153 standards. Galvanized hex head bolts (see pole foundation sheet) shall be used with screw-in anchor bases. 4 bolts, 4 nuts and 8 flat washers to provided with each anchor.
  - All welding is to be done with 4043 weld wire. All arms and shafts are to be heat-treated to T8 temper after welding.
  - Anchor bolts shall project above the concrete base as per manufacturer's recommended practices, 2 1/2" to 3".
  - The aluminum street light pole assembly, including anchorage and luminaire, shall comply with the latest city standard specifications and the American Association of State Highway and Transportation Officials (AASHTO) load wind loading.
  - All poles and arms shall be clearly identified by the manufacturer's name, abbreviation, or symbol engraved on the shaft, shoe base, hand hole, or other means such as to be readily visible after installation.

MATERIAL DATA

COMPONENT	ALUMINUM ALLOY DESIGNATION	SPECIFICATION
Shoe Base	356-T6 Cast	ASTM B206 or B108
Breakaway Base	356-T6 Cast	ASTM B108
Bolt Covers	356 or 360, Cast	ASTM B206 or B108
Pole Shaft	6063-T6 Extruded	ASTM B221 or B241
Ground Lug	6061-T6 or 6063-T6, Plate	ASTM B221
Reinforced Handhole Frame	356-T6 or 6061-T6	ASTM B206 or B108
Handhole Cover	6063-T6	ASTM B206, B271 or B241
Bracket Arm & Tying Pipes	6061-T6 or 6063-T6 Extruded	ASTM B221, B241 or B249
Bracket Arm Mounting Plates	6061-T6 or 6063-T6 Extruded	ASTM B206 or B108
Bracket Arm Strut & Arm Connector	356, Cast	ASTM B206 or B108
Pole Cap	356, Cast	ASTM A153
Anchor Bolts	N/A	



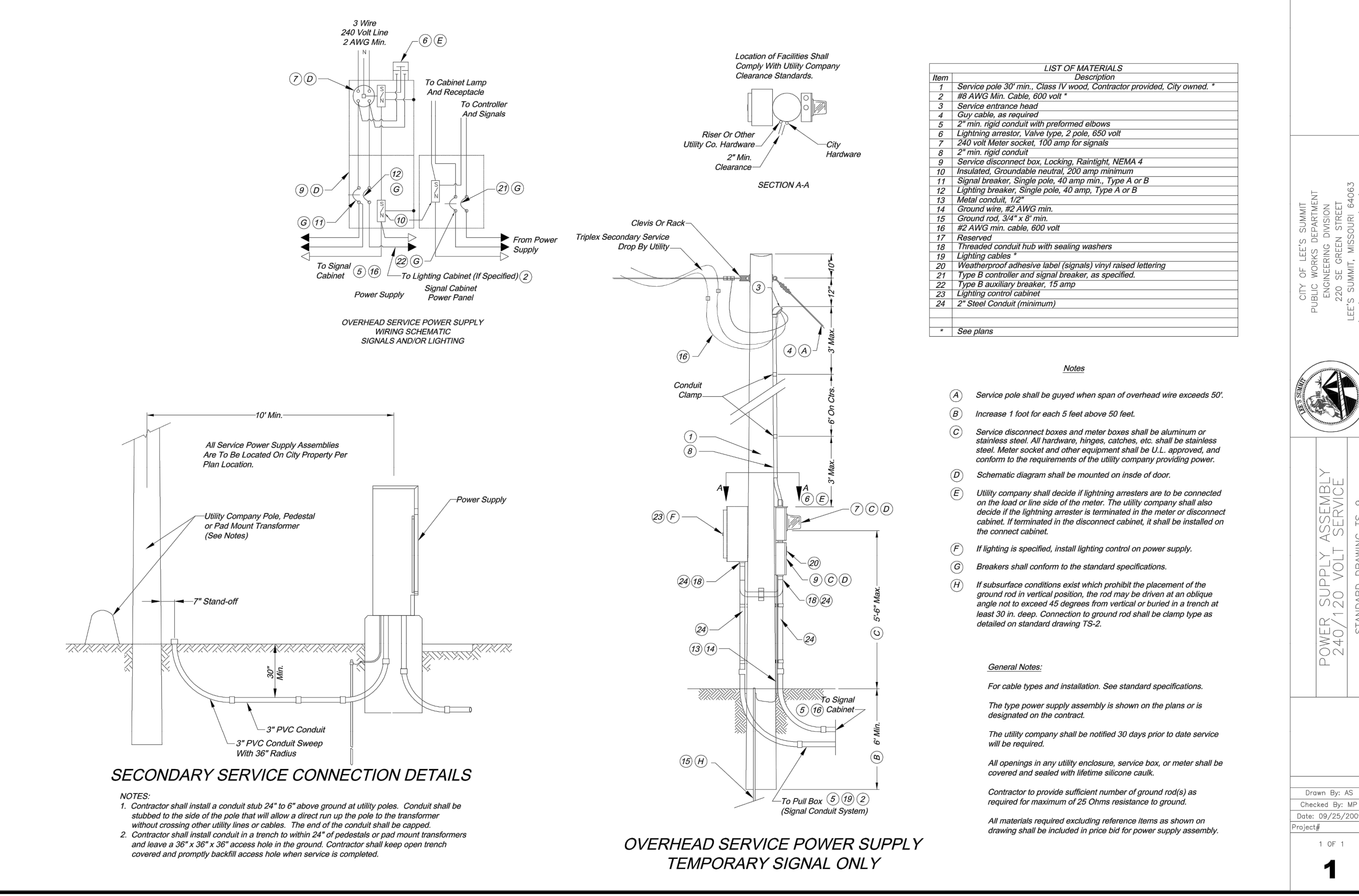
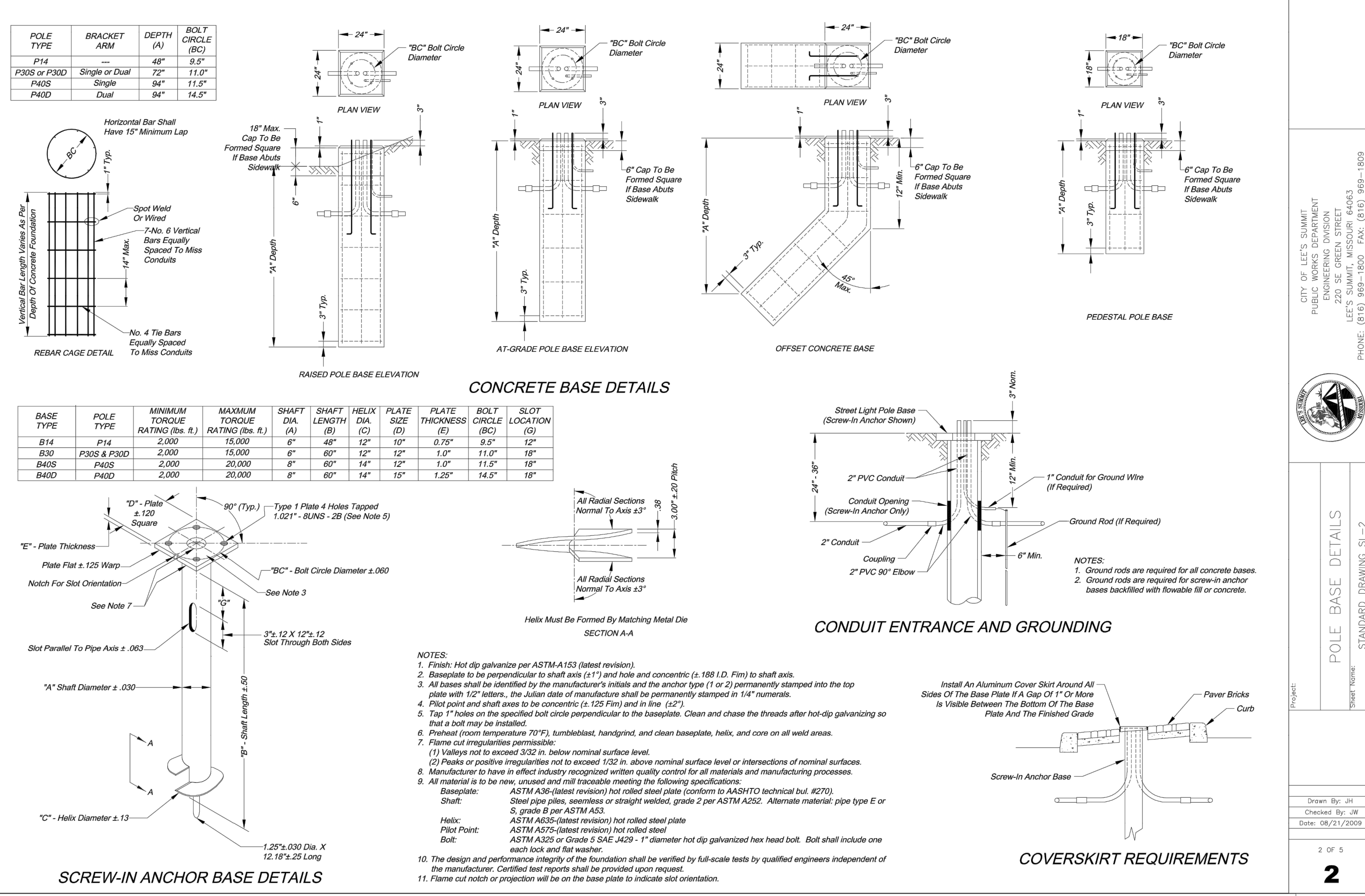
CITY OF LEE'S SUMMIT  
PUBLIC WORKS DEPARTMENT  
220 SE GREEN STREET  
LEE'S SUMMIT, MISSOURI 64083  
PHONE: (816) 869-1600 FAX: (816) 869-1609

PROJECT: STREET LIGHTING  
STANDARD DRAWING SL-1

POLE AND LUMINAIRE DETAILS

Drawn By: JH  
Checked By: JH  
Date: 08/21/2009

1 OF 1



EXTENSIVE REVISIONS

CITY OF LEE'S SUMMIT  
PUBLIC WORKS DEPARTMENT  
ENGINEERING DIVISION  
220 SE GREEN STREET  
LEE'S SUMMIT, MISSOURI 64083  
PHONE: (816) 869-1600 FAX: (816) 869-1609

PROJECT: STREET LIGHTING  
STANDARD DRAWING SL-2

POLE BASE DETAILS

Drawn By: JH  
Checked By: JH  
Date: 08/21/2009

2 OF 2

2

STATE OF MISSOURI  
LEON D. OSBOURN  
REGISTERED PROFESSIONAL ENGINEER  
NO. 021726

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j@kvweng.com | www.kvweng.com

KAW VALLEY ENGINEERING

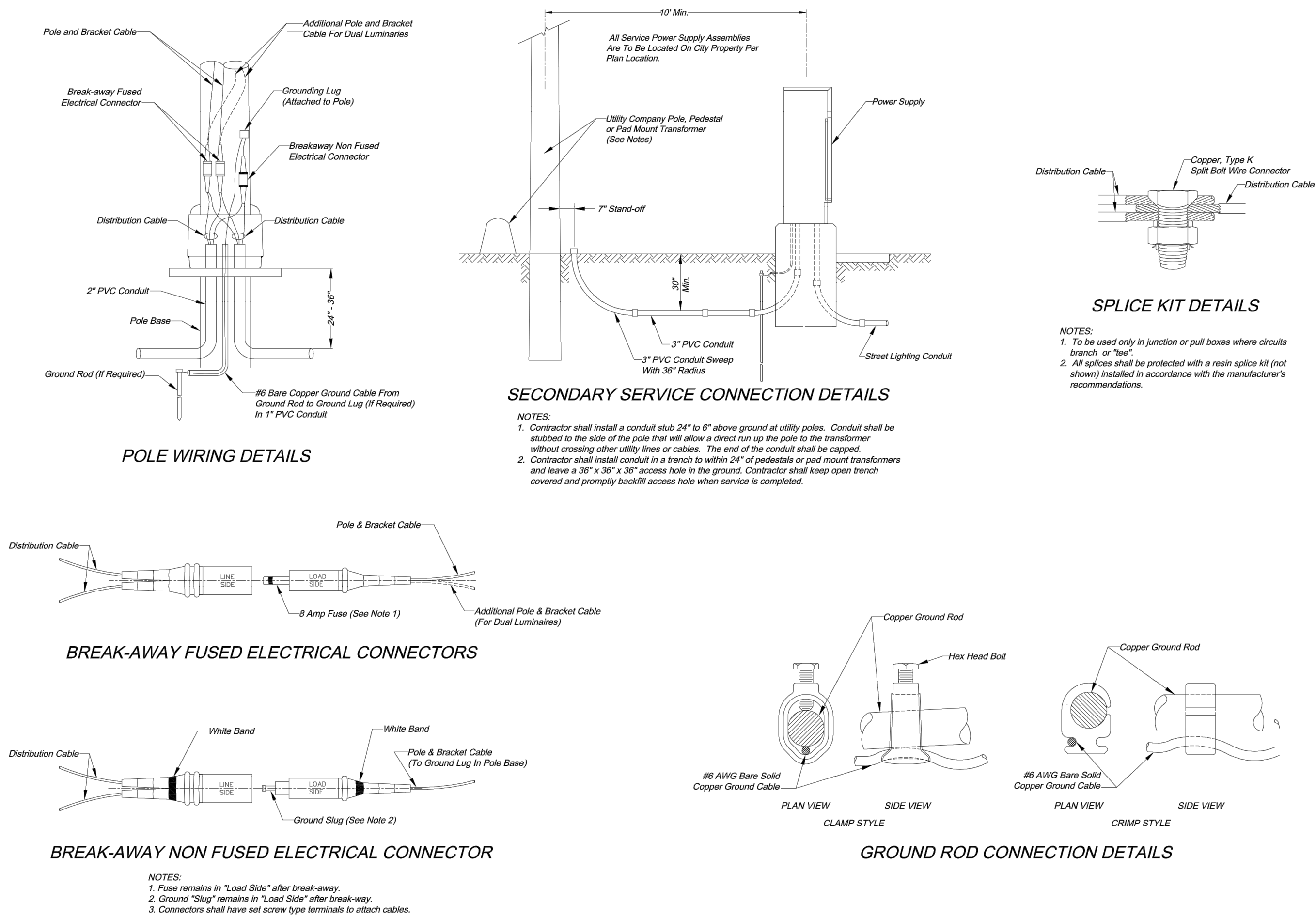
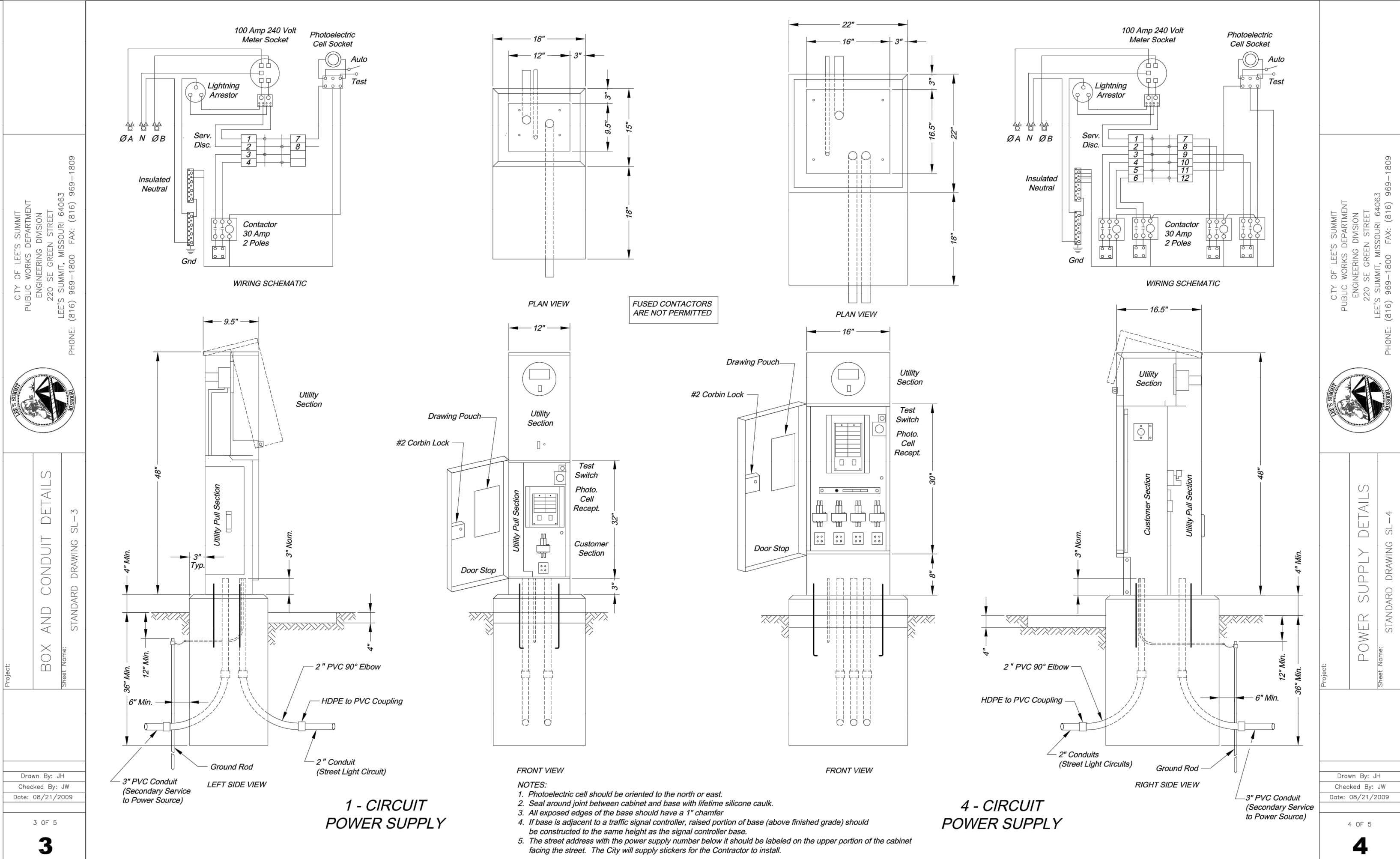
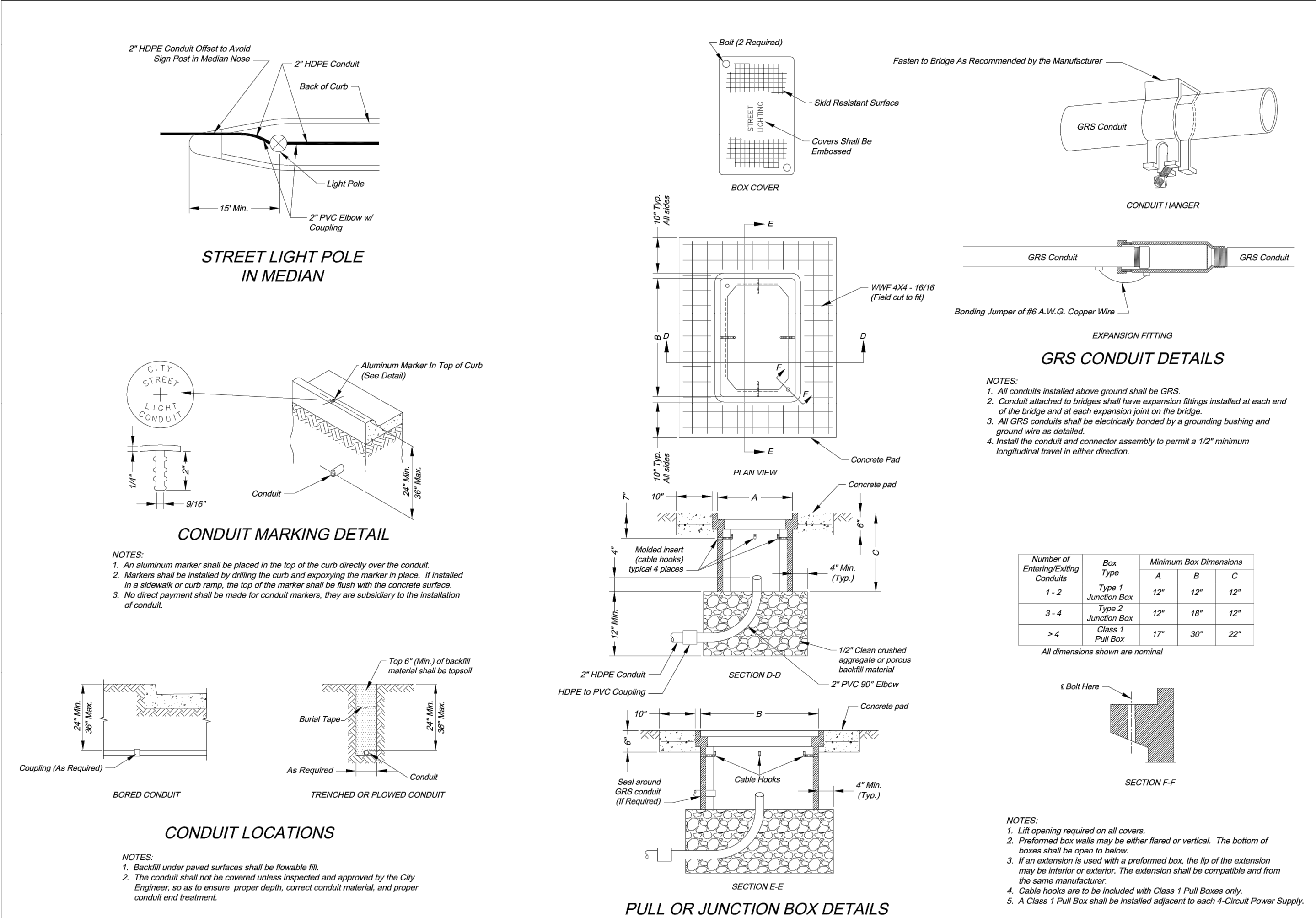
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING  
DESIGN SERVICES TO THE STATE OF MISSOURI EXPIRES 12/31/19

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
TRAFFIC LIGHT STANDARD CITY DETAILS

PROJ. NO. A14-7067-1  
DESIGNER LDO  
DRAWN BY JT/BKR  
CFN 7067-1SIG\_DET  
SHEET 32  
REV 2





EXTENSIVE REVISIONS

PROJ. NO.	A14-7067-1	
DESIGNER	LDO	
DRAWN BY	JT/BKR	
CFN		
SHEET	7067-1SIG_DET	
33	1	
REV	DATE	DESCRIPTION
0	5-23-19	INITIAL ISSUE
1	8-14-19	REVISED PER CITY COMMENTS
LDO	JT	LDO
CHK	DWN	CHK

**KAW VALLEY ENGINEERING**

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JUNCTION CITY, KANSAS 66441  
PH. (785) 762-5040 | FAX (785) 762-7744  
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**KAW VALLEY ENGINEERING**

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000642. EXPIRES 12/31/19

**STREETS OF WEST PRYOR**

NWQ NW PRYOR RD & NW LOWENSTEIN DR

LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING

STREET LIGHT STANDARD CITY DETAILS

**POWER SUPPLY DETAILS**

STANDARD DRAWING SL-4

4 OF 5

4

**ELECTRICAL DETAILS**

STANDARD DRAWING SL-5

5 OF 5

5

**BOX AND CONDUIT DETAILS**

STANDARD DRAWING SL-3

3 OF 5

3

**STREET LIGHT POLE IN MEDIAN**

STANDARD DRAWING SL-1

1 OF 1

1

**CONDUIT MARKING DETAIL**

STANDARD DRAWING SL-2

2 OF 2

2

**CONDUIT LOCATIONS**

STANDARD DRAWING SL-3

3 OF 3

3

**PULL OR JUNCTION BOX DETAILS**

STANDARD DRAWING SL-4

4 OF 4

4

**GRS CONDUIT DETAILS**

STANDARD DRAWING SL-5

5 OF 5

5

**1 - CIRCUIT POWER SUPPLY**

STANDARD DRAWING SL-6

6 OF 6

6

**4 - CIRCUIT POWER SUPPLY**

STANDARD DRAWING SL-7

7 OF 7

7

**SPICE KIT DETAILS**

STANDARD DRAWING SL-8

8 OF 8

8

**POLE WIRING DETAILS**

STANDARD DRAWING SL-9

9 OF 9

9

**BREAK-AWAY FUSED ELECTRICAL CONNECTORS**

STANDARD DRAWING SL-10

10 OF 10

10

**BREAK-AWAY NON FUSED ELECTRICAL CONNECTOR**

STANDARD DRAWING SL-11

11 OF 11

11

**SECONDARY SERVICE CONNECTION DETAILS**

STANDARD DRAWING SL-12

12 OF 12

12

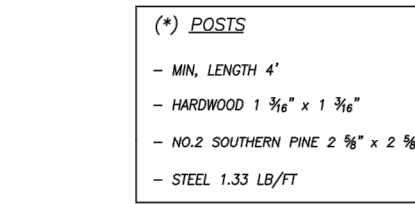
**GROUND ROD CONNECTION DETAILS**

STANDARD DRAWING SL-13

13 OF 13

13

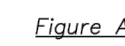




(\*\*) - Geotextile Fabric shall meet the requirements of AASHTO M288

SILT FENCE DETAILS

Not to Scale



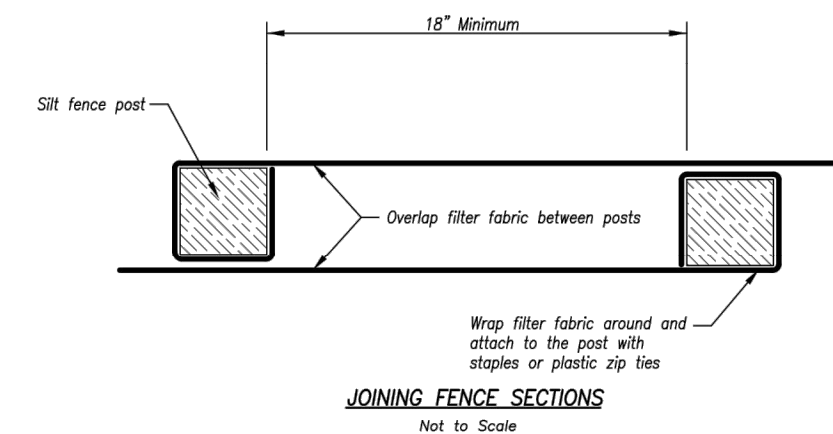
SILT FENCE LAYOUT


Not to Scale

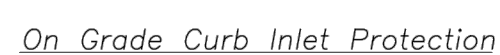
- ### Notes:
1. In order to contain water, the ends of the silt fence must be turned uphill (Figure A).
  2. Long perimeter runs of silt fence must be limited to 100'. Runs should be broken up into several smaller segments to minimize water concentrations (Figure A).
  3. Long slopes should be broken up with intermediate rows of silt fence to slow runoff velocities.
  4. Attach fabric to upstream side of post.
  5. Install posts a minimum of 2' into the ground.
  6. Trenching will only be allowed for small or difficult installation, where slicing machine cannot be reasonably used.

Maintenance:

1. Remove and dispose of sediment deposits when the deposit approaches  $\frac{1}{2}$  the height of silt fence.
2. Repair as necessary to maintain function and structure.



AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
SILT FENCE	STANDARD DRAWING NUMBER ESC-03 ADOPTED: 10/24/2016



EARLY STAGE CURB INLET  
(Open Box and Prior to Pouring  
Curb and Inlet Throat)

- Notes:

1. Immediately following inlet construction and prior to construction of curb and inlet throat, protect inlet opening by installing 2" X 10" (min.) board wrapped in sail fence. Structures shall have excavated storage area on all four sides to allow settling of sediment (Early Stage Curb Inlet).
2. When inlet is completed and curb poured, filter socks or approved equal should be used (Late Stage Curb Inlet). Straw wattles are not approved for curb inlet use.
3. Contractor to field verify ponding water shall not create a traffic hazard.


Maintenance:

1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
3. Repair or replace as necessary to maintain function and integrity of installation.



LATE STAGE CURB INLET

DATE STAGE CORD INLET  
(After Pouring Curb and Inlet Throat)

AMERICAN PUBLIC WORKS ASSOCIATION	
Kansas City Metro Chapter  AMERICAN PUBLIC WORKS ASSOCIATION	KANSAS CITY METRO CHAPTER
CURB INLET PROTECTION	STANDARD DRAWING NUMBER ESC-06 ADOPTED: 10/24/2016

Modified from 2015 Overland Park Standard Details  
for Erosion and Sediment Control.



SECTION A-A



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- ### Notes

1. Early Stage Area Inlet Sediment Barrier to be installed immediately after inlet or junction box is constructed.
2. Silt fence shall remain in place until excavated area is removed and Late Stage Area Inlet is being installed.
3. Backfill excavated area ONLY after final grading of the site. Stabilization of the site is to immediately follow.
4. Wire reinforced silt fence may be used in place of silt fence attached to wood frame.

EARLY STAGE AREA INLET



EARLY STAGE AREA INLET  
(All open boxes and inlets not at final grade)

EXTENSIVE REVISIONS



**DATE STAGE AREA INLET**  
(Area inlets at final grade and existing inlets)

- Maintenance:
1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
  2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
  3. Repair or replace as necessary to maintain function and integrity of installation.

<b>AMERICAN PUBLIC WORKS ASSOCIATION</b> Kansas City Metro Chapter  		<b>KANSAS CITY METRO CHAPTER</b>
AREA INLET AND JUNCTION BOX PROTECTION	<b>STANDARD DRAWING NUMBER ESC-07</b> <b>ADOPTED:</b> 10/24/2016	

Modified from 2015 Overland Park Standard Details  
for Erosion and Sediment Control.







