

CONSTRUCTION PLANS FOR NW PRYOR ROAD SIGNAL IMPROVEMENTS & LANE WIDENING STREETS OF WEST PRYOR LEE'S SUMMIT, MISSOURI

OCTOBER 2019
RECORD DRAWINGS

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1-800-344-7483
(TOLL FREE)

MISSOURI ONE CALL SYSTEM, INC.

UTILITY STATEMENT:

THE UNDERGROUND UTILITIES SHOWN HEREON ARE FROM FIELD SURVEY INFORMATION OF ONE-CALL LOCATED UTILITIES, FIELD SURVEY INFORMATION OF ABOVE GROUND OBSERVABLE EVIDENCE, AND/OR THE SCALING AND PLOTTING OF EXISTING UTILITY MAPS AND DRAWINGS AVAILABLE TO THE SURVEYOR AT THE TIME OF SURVEY. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. FURTHERMORE, THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES BY EXCAVATION UNLESS OTHERWISE NOTED ON THIS SURVEY.

CAUTION - NOTICE TO CONTRACTOR

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXPOSE EXISTING UTILITIES AT LOCATIONS OF POSSIBLE CONFLICTS PRIOR TO ANY CONSTRUCTION.

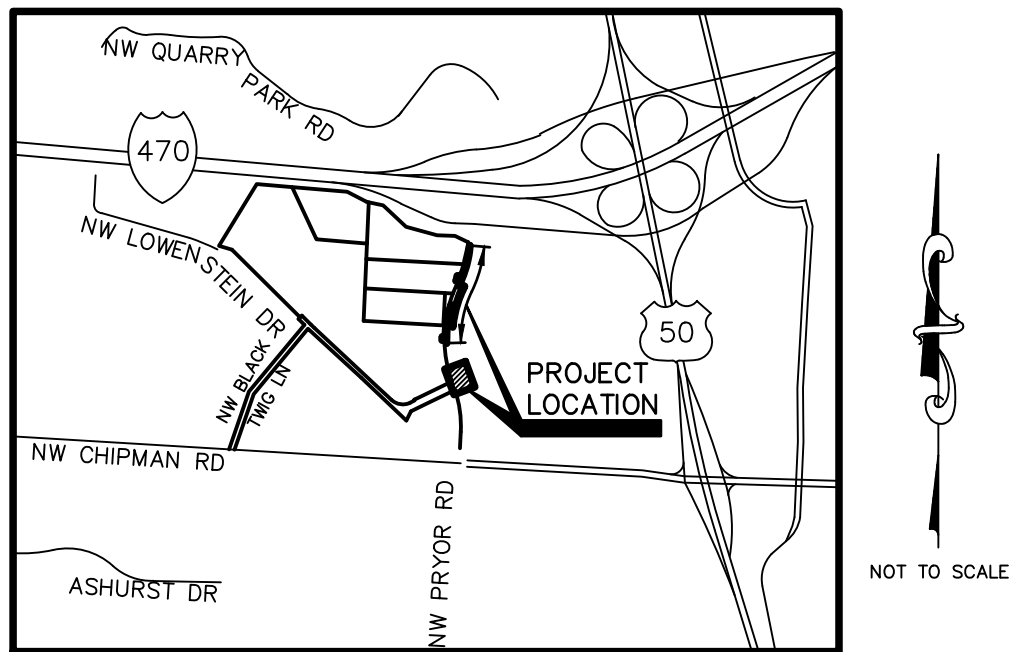
SAFETY NOTICE TO CONTRACTOR

IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

WARRANTY / DISCLAIMER

THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER KAW VALLEY ENGINEERING, INC. NOR ITS PERSONNEL CAN OR DO WARRANTY THESE DESIGNS OR PLANS AS CONSTRUCTED, EXCEPT IN THE SPECIFIC CASES WHERE KAW VALLEY ENGINEERING PERSONNEL INSPECT AND CONTROL THE PHYSICAL CONSTRUCTION ON A CONTEMPORARY BASIS AT THE SITE.

SUMMARY OF QUANTITIES			
Paving			
Item No.	Item	Estimated Quantity	Unit
1	2' Curb & Gutter (CG-1)	1080	L.F.
2	Asphaltic Cement Concrete Surface (2")	1240	S.Y.
3	Asphaltic Cement Concrete Surface (7.5")	1240	S.Y.
4	MoDOT Type 5 Base (Pavement)(12")	1570	S.Y.
5	Terragrid SX2020	1570	S.Y.
6	Concrete Entrance (Commercial)(8")	679	S.Y.
7	Aggregate Base (Commercial)(Driveway)(4")	679	S.Y.
8	Handicap Ramp	4	Ea.
9	Modular Block Retaining Wall	34	L.F.
10	Island Concrete (6")	54	S.F.
11	Concrete Sidewalk (4" RCCMB 4K Concrete)	484	S.Y.
12	Detectable Warning pad (pad only)	2'X3'	Size
Erosion Control			
Item No.	Item	Estimated Quantity	Unit
1	Erosion Control Devices, Sedimentation Fence	1148	L.F.
2	Erosion Control Devices, Curb Inlet Protection	4	Ea.
3	Erosion Control Devices, Area Inlet & Junction Box Protection	4	Ea.
4	Seeding	1	S.F.
Pavment Marking & Signing			
Item No.	Item	Estimated Quantity	Unit
1	Pavement Marking, High-build Paint (Solid White)(4")	574	L.F.
2	Pavement Marking, High-build Paint (Dashed White)(Extension)(6")	82	L.F.
3	Pavement Marking, High-build Paint (Solid White)(Cross-walk)(6")	220	L.F.
4	Pavement Marking, Thermoplastic (Solid White)(Stop Bar)(24")	62	L.F.
5	Pavement Marking, Thermoplastic (White)(Left/Right Arrow)	17	Ea.
6	Permanent Signs	32	S.F.
7	Sign Post (Square Steel Tube)	3	L.F.
Storm Sewer			
Item No.	Item	Estimated Quantity	Unit
1	Setback Curb Inlet (4'x3')	1	Ea.
2	RCP (24")/CLASS 3	42	L.F.
Traffic Signal			
Item No.	Item	Estimated Quantity	Unit
1	Traffic Signal System, NW Pryor & NW Summit Woods	1	L.S.
2	Traffic Signal System, NW Pryor & NW Lowenstein	1	L.S.
3	Interconnect Cable	2370	L.F.
Lighting			
Item	Item	Estimated Quantity	Unit
1	Light Pole Base	2	Ea.



LOCATION MAP
CITY OF LEE'S SUMMIT, MISSOURI

I certify that I have reviewed the as built information provided on these record drawings and take no exception to the information provided by Emery Sapp and Sons, Inc.

Leon D. Osborn

September 2, 2020

APPROVED THIS _____ DAY OF _____, _____

OWNER: MATT PENNINGTON

APPROVED THIS _____ DAY OF _____, _____

CITY APPROVAL

OWNER:
STREETS OF WEST PRYOR, LLC
7200 WEST 132ND STREET
OVERLAND PARK, KS 66213
CONTACT: MATT PENNINGTON
email: matt@drakekc.com

DEVELOPER:
STREETS OF WEST PRYOR, LLC
7200 WEST 132ND STREET
OVERLAND PARK, KS 66213
AGENT: DAVID N. OLSON
email: daveolson@monarchprojectllc.com

PREPARED BY:
KAW VALLEY ENGINEERING, INC.
2319 N. JACKSON
JUNCTION CITY, KS 66441
785-762-5040
CONTACT: LEON D OSBOURN
EMAIL: ldo@kveg.com

DATUM BENCHMARK:
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING OPUS PROJECTS ON PROJECT CONTROL.

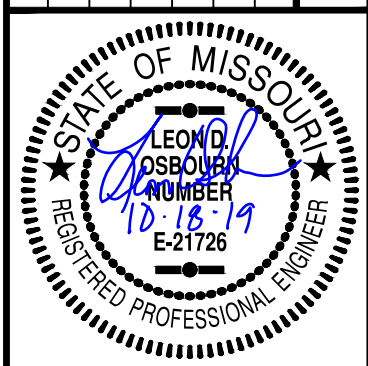
BENCHMARKS:
BM #1: CHISELED "SQUARE" ON TOP OF CURB POINT OF INTERSECTION OF WEST PARK PARKING LOT AT EAST DRIVE ENTRANCE. ELEV=984.97

BM #2: CHISELED "SQUARE" ON NORTHWEST CORNER AREA INLET, 25'± EAST OF CURB LINE AND ON-LINE WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90° BEND IN ROAD. ELEV=970.98

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- DEMOLITION PLAN
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- NW PRYOR DRIVE WIDENING - PLAN & PROFILE
- NW PRYOR DRIVE WIDENING - PLAN & PROFILE
- DRIVE AT PROPOSED PRIVATE ROAD - A & C - PLAN & PROFILE
- GEOMETRIC LAYOUT - NW PRYOR ROAD AT NW SUMMIT WOODS
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- ADA RAMPS
- GRADING PLAN
- EROSION CONTROL PLAN
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- NW PRYOR ROAD PAVEMENT MARKING PLAN
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- TRAFFIC SIGNAL QUANTITIES - NW PRYOR RD & NW LOWENSTEIN DR
- TRAFFIC SIGNAL PLAN - NW PRYOR RD & NW SUMMIT WOODS
- TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD & NW SUMMIT WOODS
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- TRAFFIC INTERCONNECT PLAN, NOTES AND WIRING
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- SIGN AND TRAFFIC CONTROL DETAIL SHEET
- STREET DETAIL SHEET
- STREET DETAIL SHEET
- TRAFFIC SIGNAL STANDARD CITY DETAILS
- TRAFFIC SIGNAL STANDARD CITY DETAILS
- TRAFFIC SIGNAL & STREET LIGHT STANDARD CITY DETAILS
- STREET LIGHT STANDARD CITY DETAILS
- EROSION CONTROL DETAIL SHEET
- SHEET NOT USED
- NW PRYOR ROAD - CROSS-SECTIONS
- NW PRYOR ROAD - CROSS-SECTIONS

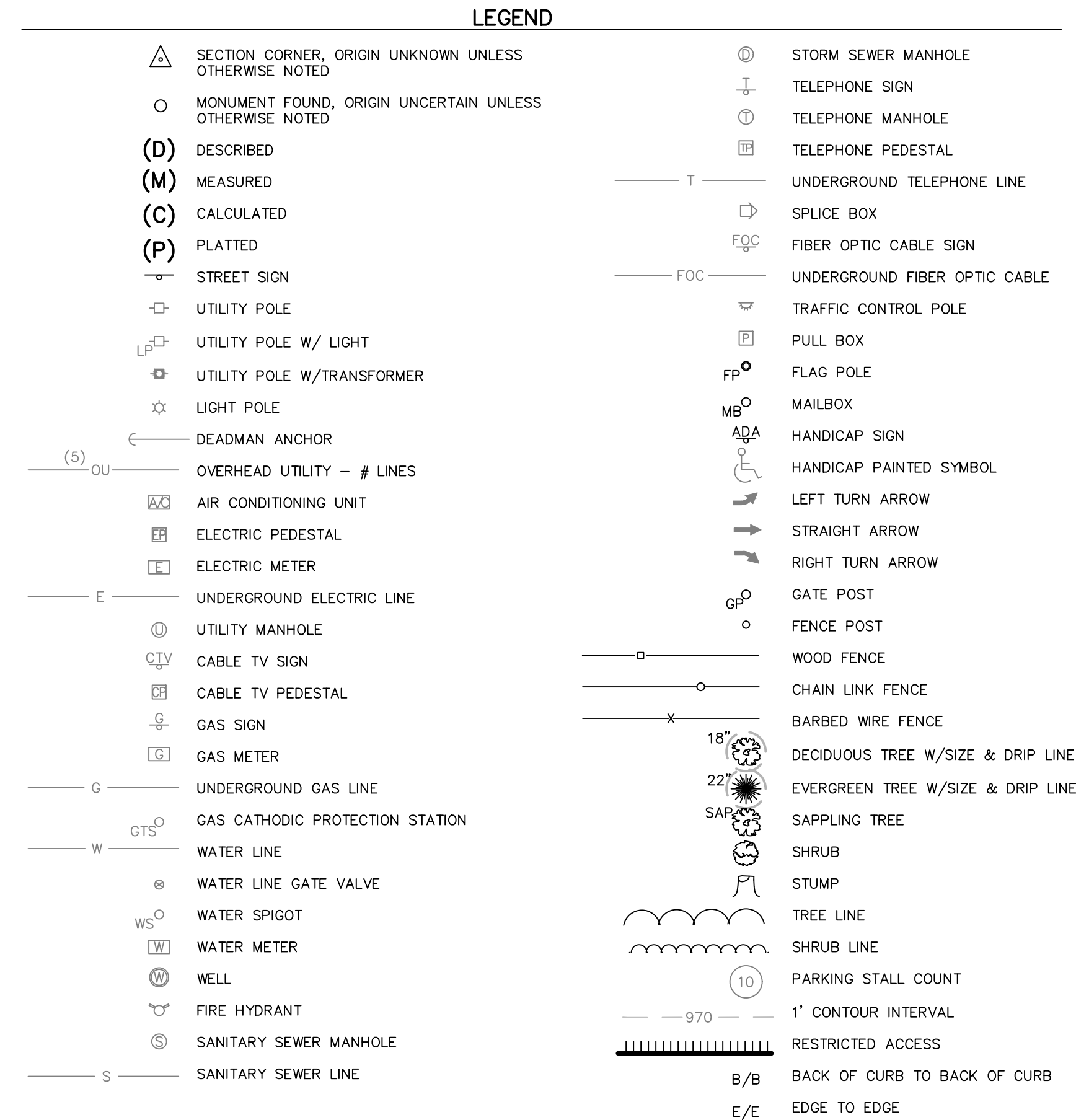
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4	10-3-19	REVISED PER CITY COMMENTS	LDO	JT
3	8-14-19	REVISED PER CITY COMMENT	LDO	LDO
2	7-24-19	REVISED PER CITY COMMENTS	LDO	JT
1	7-2-19	REVISED PER CITY COMMENTS	LDO	LDO
0	5-23-19	INITIAL ISSUE	LDO	JT

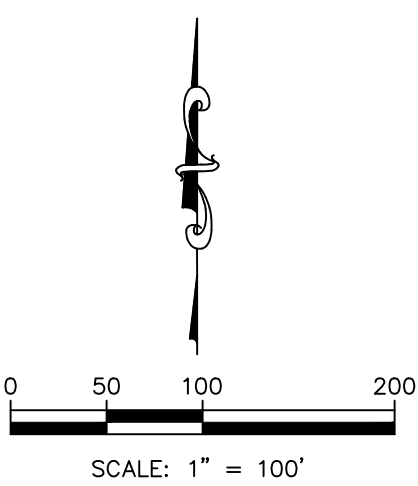


LEON D. OSBOURN
ENGINEER
MO # 021726

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jdo@kveg.com | www.kveg.com
KAW VALLEY ENGINEERING
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES
EXPRESS 12/31/19

STREETS OF WEST PRYOR NW& NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING TITLE SHEET	
PROJ. NO. A14-7067-1		DESIGNER LDO DRAWN BY JT/BKR	
CFN 7067-1SIG-TS		SHEET 1 REV 5	



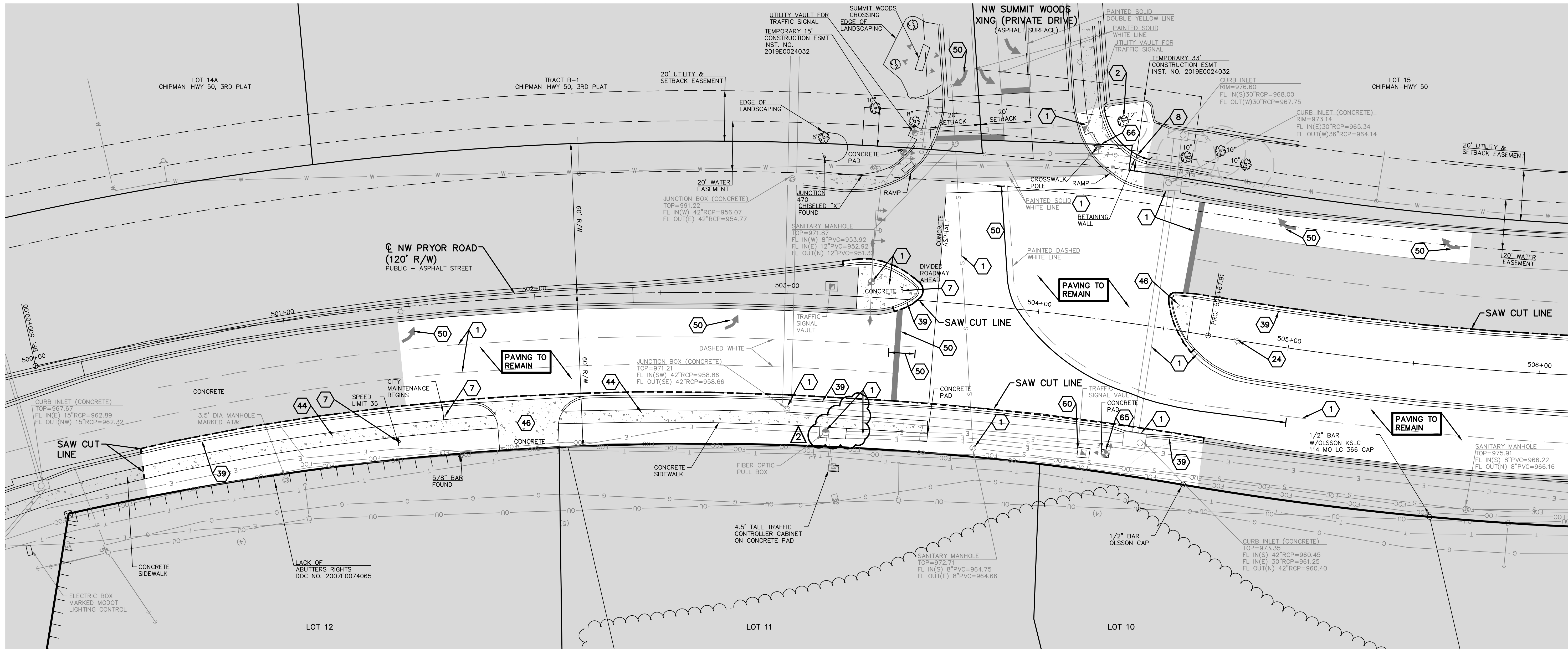


THIS DRAWING SHALL NOT BE UTILIZED BY ANY PERSON, FIRM, OR CORPORATION IN WHOLE OR IN PART WITHOUT THE SPECIFIC PERMISSION OF KAW VALLEY ENGINEERING, INC.



NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING GENERAL LAYOUT SHEET

PROJ. NO.	
A14_7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
7067-1SIG_GLS	
SHEET	REV
3	0



- > TO REMAIN
- > REMOVE TREE (SEE PLANS)
- > TREES TO REMAIN
- > SIGN TO BE RELOCATED
- > PORTION OF RETAINING WALL TO BE REMOVED (SEE PLANS)
- > TO BE REMOVED BY ELECTRIC COMPANY
- > STREET LIGHT TO BE RELOCATED (SEE PLANS)
- > CONTRACTOR TO REMOVE (CURBS)
- > CONTRACTOR TO REMOVE (SIDEWALK)
- > CONTRACTOR TO REMOVE (PAVING)
- > CONTRACTOR TO SAND BLAST STRIPING (SEE PAVEMENT MARKING SHEETS)
- > TRAFFIC POLE TO BE RELOCATED. (SEE PLANS)
- > TRAFFIC POLE TO BE REMOVED AND RETURNED TO THE CITY OF LEES SUMMIT

NOT A PART OF DEMOLITION ACTIVITIES

--- SAW CUT LIMITS

DATUM BENCHMARK:
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING
OPUS PROJECTS ON PROJECT CONTROL.

BENCHMARKS:
BM #1: CHISELED "SQUARE" ON TOP OF CURB P
OF INTERSECTION OF WEST PARK PARKING LOT A
EAST DRIVE ENTRANCE. ELEV=984.97

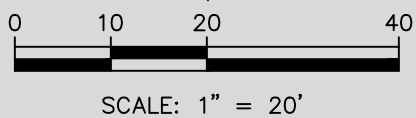
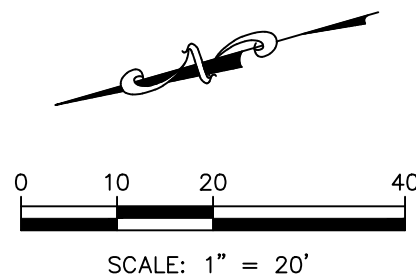
BM #2: CHISELED "SQUARE" ON NORTHWEST COR
AREA INLET, 25'± EAST OF CURB LINE AND ON-
WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90°
BEND IN ROAD. ELEV=970.98

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SAFETY NOTICE TO CONTRACTOR
IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

1. CONTRACTOR SHALL VERIFY SITE CONDITIONS PRIOR TO BIDDING. CONTRACTOR SHALL REMOVE ALL BUILDINGS, UTILITIES, PAVEMENT, FOUNDATIONS, FENCES, CURBS AND ALL OTHER STRUCTURES FROM WITHIN PROPERTY LINES EXCEPT AS DESIGNATED "TO REMAIN" OR "TO BE REMOVED BY OTHERS", IN ACCORDANCE WITH THE SPECIFICATIONS AND THE CITY OF LEE'S SUMMIT AND STATE REGULATIONS. SITE CONDITIONS SHOWN WERE AS OF MARCH 30, 2018.
2. ALL UTILITY PIPE LINES TO BE ABANDONED SHALL BE PLUGGED PER CITY AND STATE REGULATIONS.
3. DRIVES, PAVING AND OTHER STRUCTURES ON STREET OR HIGHWAY RIGHT-OF-WAY SHALL BE REMOVED AS NECESSARY TO CONSTRUCT IMPROVEMENTS SHOWN ON THESE PLANS. REMOVAL AND DISPOSAL SHALL BE IN CONFORMANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.
4. ALL PAVING WITHIN PROPERTY TO BE REMOVED AND DISPOSED OF IN CONFORMANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.
5. ALL HAZARDOUS ASBESTOS AND OTHER HAZARDOUS MATERIALS MUST BE IDENTIFIED AND REMOVED PRIOR TO ANY BUILDING DEMOLITION, IN STRICT CONFORMANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.
6. CONTRACTOR SHALL VERIFY THAT ALL UTILITIES TO EXISTING STRUCTURES HAVE BEEN DISCONNECTED PRIOR TO COMMENCING DEMOLITION.

7. EXISTING POWER LINES AND APPURTENANCES TO BE RELOCATED BY KANSAS CITY POWER & LIGHT.
8. TREE LINES AND INDIVIDUAL TREES SHOWN ARE BASED ON ORIGINAL SURVEY. INITIAL CLEARING AND GRUBBING HAS BEEN COMPLETED. CONTRACTOR SHALL REMOVE ANY ADDITIONAL TREES AND SHRUB IN AREAS INDICATED FOR GRADING AND DEMOLITION.
9. COORDINATE WITH KAW VALLEY ENGINEERING PRIOR TO REMOVAL OF SECTION MONUMENTS AND PROPERTY PINS. KAW VALLEY ENGINEERING WILL RESET NECESSARY MONUMENTS WITHIN THE DISTURBED AREAS FOLLOWING DEMOLITION. CARE SHALL BE TAKEN TO PRESERVE PROPERTY PINS ALONG THE OUTSIDE PERIMETER OF THE SITE.
10. CONTRACTOR SHALL OBTAIN DEMOLITION PERMITS FOR EACH INDIVIDUAL HOUSE WITHIN THE AREA SHOWN. ALL SERVICE LINE SHUT-OFFS, WELL PLUGGING, SEPTIC TANK REMOVALS AND OTHER UTILITY REMOVALS SHALL BE HANDLED IN ACCORDANCE WITH STATE AND CITY CODES.
11. KCP&L TRANSMISSION MAIN SHALL REMAIN IN PLACE DURING DEMOLITION. FOLLOW ALL KCP&L REQUIREMENTS CONCERNING WORK IN THEIR EASEMENT AND IN PROXIMITY TO THEIR LINES, INCLUDING PROTECTION OF POLES AND SAFE WORKING DISTANCES FROM LINES.



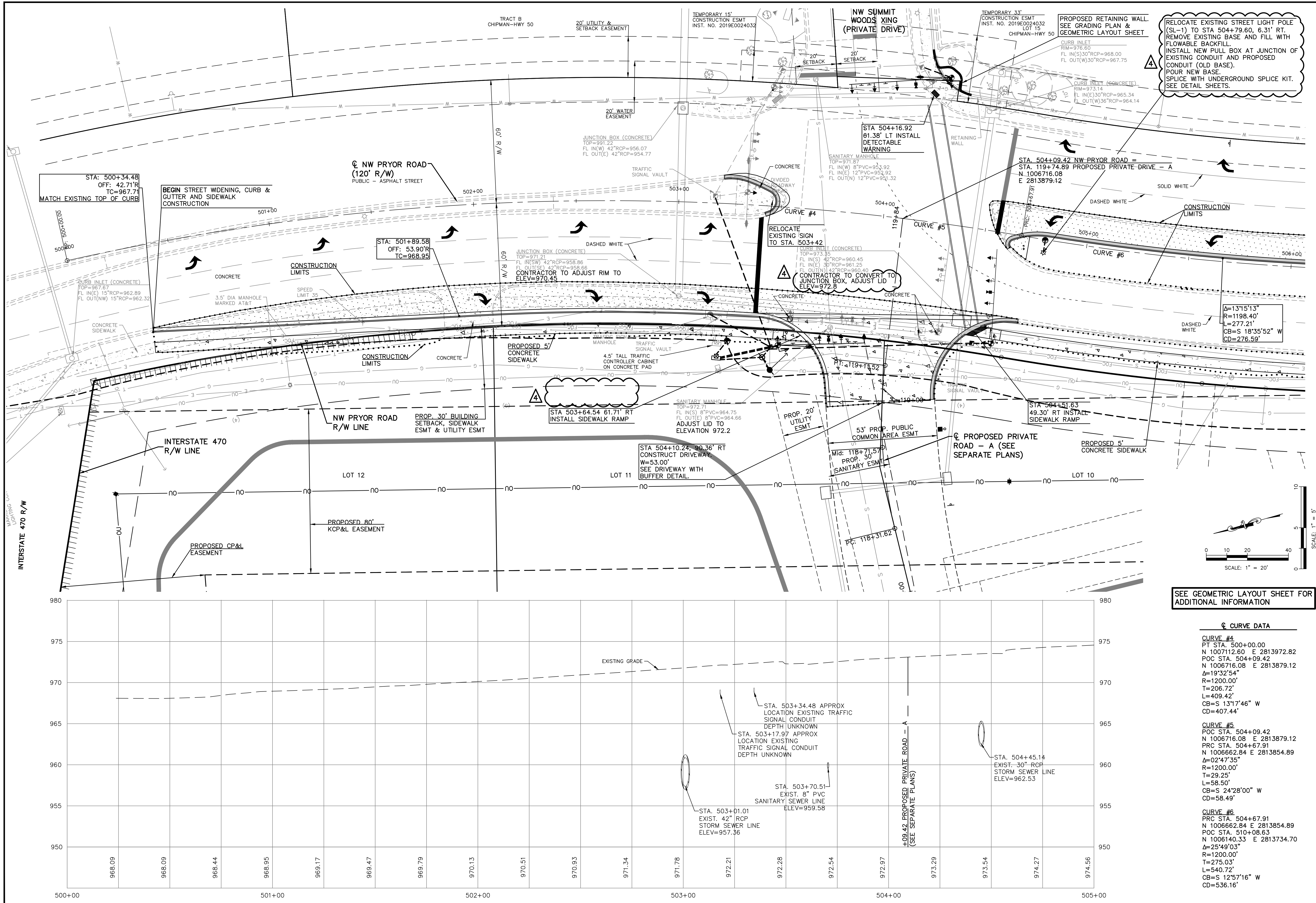
- LEGEND**
- | | |
|---|-------------------------------------|
|  | NOT A PART OF DEMOLITION ACTIVITIES |
|  | SAW CUT LIMITS |

DATUM BENCHMARK:
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING
OPUS PROJECTS ON PROJECT CONTROL.

BENCHMARKS:
BM #1: CHISELED "SQUARE" ON TOP OF CURB PO
OF INTERSECTION OF WEST PARK PARKING LOT AT
EAST DRIVE ENTRANCE. ELEV=984.97

BM #2: CHISELED "SQUARE" ON NORTHWEST CORN
AREA INLET, 25'± EAST OF CURB LINE AND ON-LI
WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90'
BEND IN ROAD. ELEV=970.98

STREETS OF WEST PRYOR											
NWQ NW PRYOR RD & NW LOWENSTEIN DR											
LEE'S SUMMIT, MISSOURI											
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING											
DEMOLITION PLAN											
PROJ. NO. A14-7067-1											
DESIGNER LDO						DRAWN BY JT/BKR					
CFN											
SHEET 5						REV 3					
7067-SIG_DEMO											



SEE GEOMETRIC LAYOUT SHEET FOR ADDITIONAL INFORMATION

C CURVE DATA			
CURVE #4			
PT STA.	500+00.00		
N	1007112.60	E	2813972.82
POC STA.	504+09.42		
N	1006716.08	E	2813879.12
Δ=19°32'54"			
R=1200.00'			
T=206.72'			
L=409.42'			
CB=S 13°17'46" W			
CD=407.44'			
CURVE #5			
POC STA.	504+09.42		
N	1006716.08	E	2813879.12
PRC STA.	504+67.91		
N	1006662.84	E	2813854.89
Δ=02°47'35"			
R=1200.00'			
T=29.25'			
L=58.50'			
CB=S 24°28'00" W			
CD=58.49'			
CURVE #6			
PRC STA.	504+67.91		
N	1006662.84	E	2813854.89
POC STA.	510+08.63		
N	1006140.33	E	2813734.70
Δ=25°49'03"			
R=1200.00'			
T=275.03'			
L=540.72'			
CB=S 12°57'16" W			
CD=536.16'			

PROJ. NO.	A14 7067-1	
DESIGNER	LDO	
DRAWN BY	JT/BKR	
CFN	7067-1SG_STPP	
SHEET	6	
REV	4	
DATE		
DESCRIPTION		
REV	DATE	DESCRIPTION
4	10-3-19	REVISED PER CITY COMMENTS
3	8-14-19	REVISED PER CITY COMMENT
2	7-24-19	REVISED PER CITY COMMENTS
1	7-2-19	REVISED PER CITY COMMENTS
0	5-23-19	INITIAL ISSUE
DSN	DWN	CHK

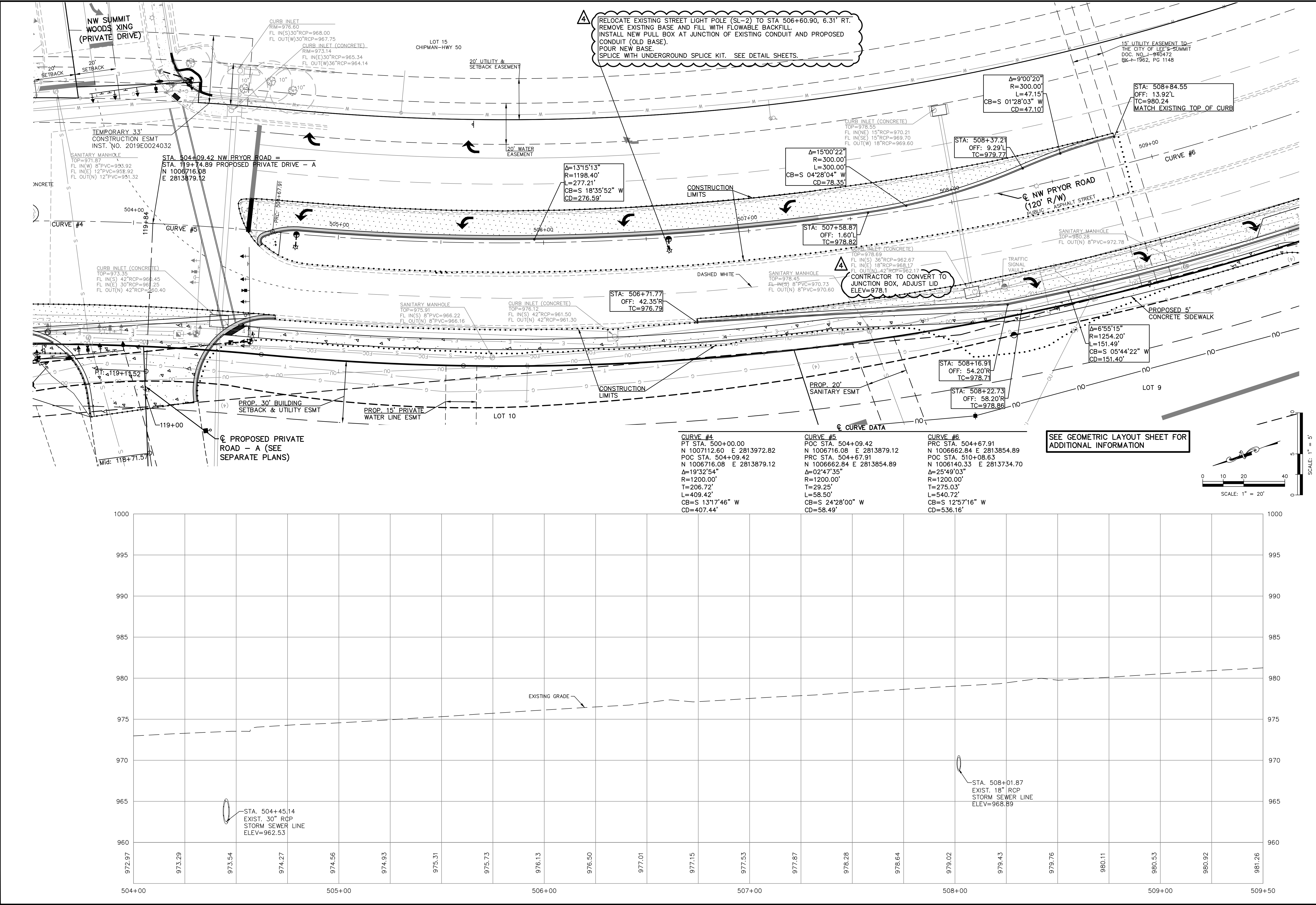
STATE OF MISSOURI
LEON D. OSBOURN
ENGINEER
MO # 021726

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JUNCTION CITY, KANSAS 66441
PH. (785) 762-5040 | FAX (785) 762-7744
joe@kveeng.com | www.kveeng.com

KAW VALLEY ENGINEERING
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES
MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842
EXPIRES 12/31/19

STREETS OF WEST PRYOR
NWQ NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
NW PRYOR DRIVE WIDENING - PLAN & PROFILE



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REV	DATE	DESCRIPTION	CHK
0	5-23-19	INITIAL ISSUE	DWN
1	7-2-19	REVISED PER CITY COMMENTS	LD
2	7-24-19	REVISED PER CITY COMMENTS	LD
3	10-3-19	REVISED PER CITY COMMENT	LD

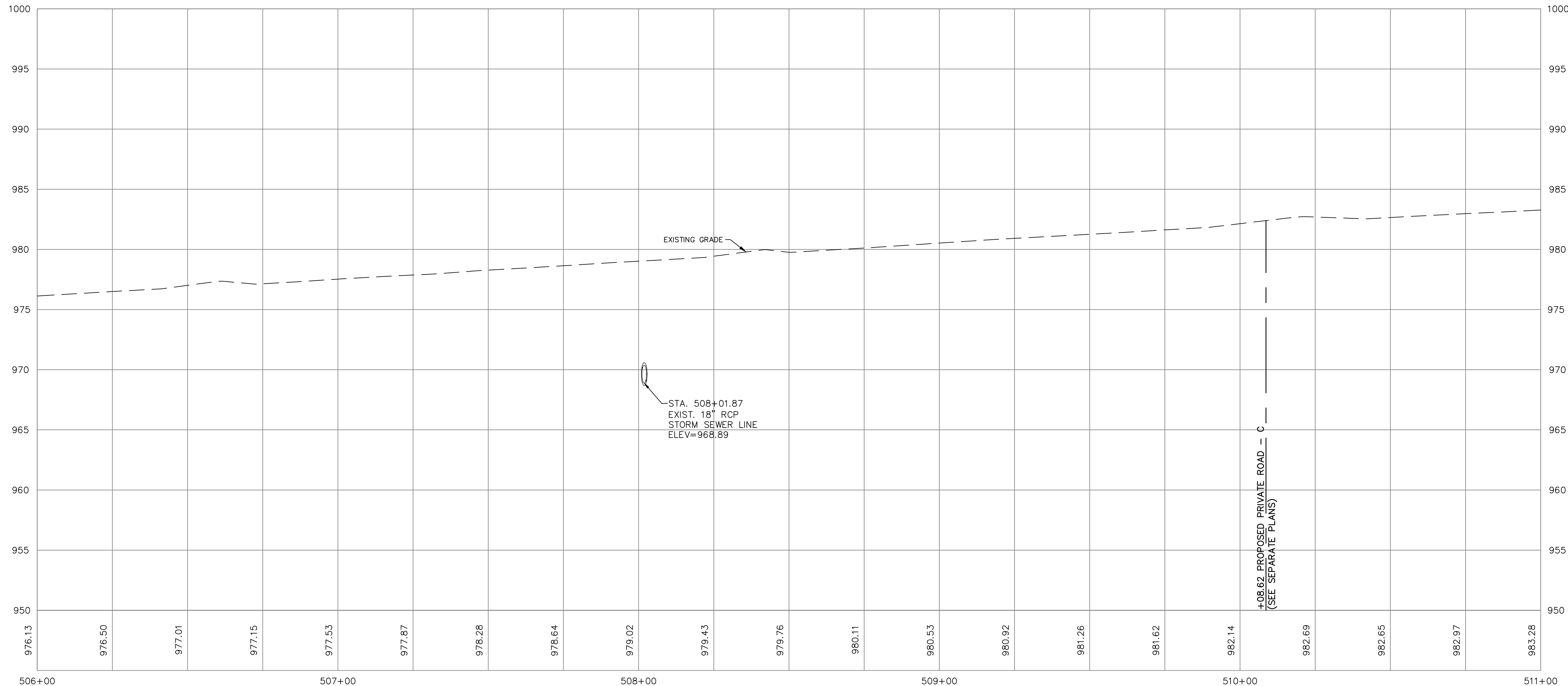
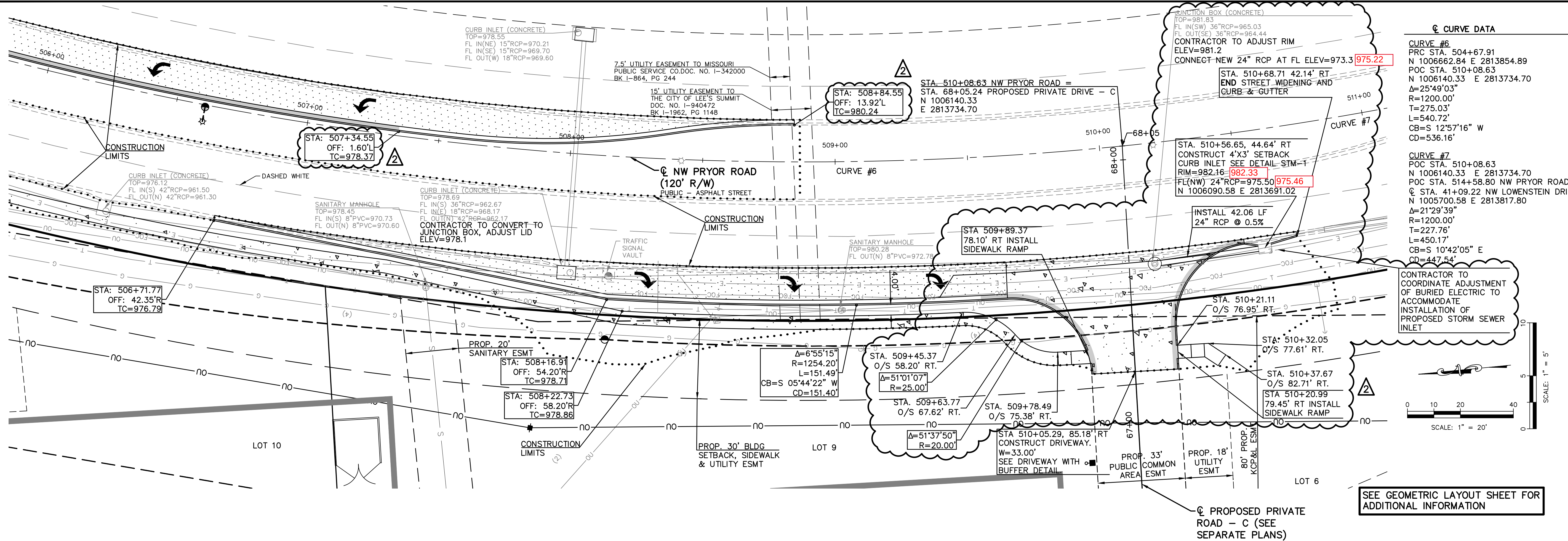
LEON D. OSBOURN
ENGINEER
MO # 021726

KAW VALLEY ENGINEERING
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI UNDER LICENSE # 000842.
EXPIRES 12/31/19

STREETS OF WEST PRYOR
NWQ NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
NW PRYOR DRIVE WIDENING - PLAN & PROFILE

PROJ. NO.	A14-7067-1
DESIGNER	LD
DRAWN BY	JT/BKR
CFN	
7067-1SIC-STPP	
SHEET	7
REV	3



REV	DATE	DESCRIPTION	CHK
2	10-3-19	REVISED PER CITY COMMENTS	LDL
1	7-2-19	REVISED PER CITY COMMENTS	LDL
0	5-23-19	INITIAL ISSUE	LDL

STATE OF MISSOURI
LEON D. OSBOURN
REGISTERED PROFESSIONAL ENGINEER
NUMBER 003379
E-21726

LEON D. OSBOURN
ENGINEER
MO # 021726

2319 N. JACKSON | P.O. BOX 1304
JUNCTION CITY, KANSAS 66441
PH. (785) 762-5040 | FAX (785) 762-7744
joe@kveeng.com | www.kveeng.com

KAW VALLEY ENGINEERING
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19.

STREETS OF WEST PRYOR
NWQ NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

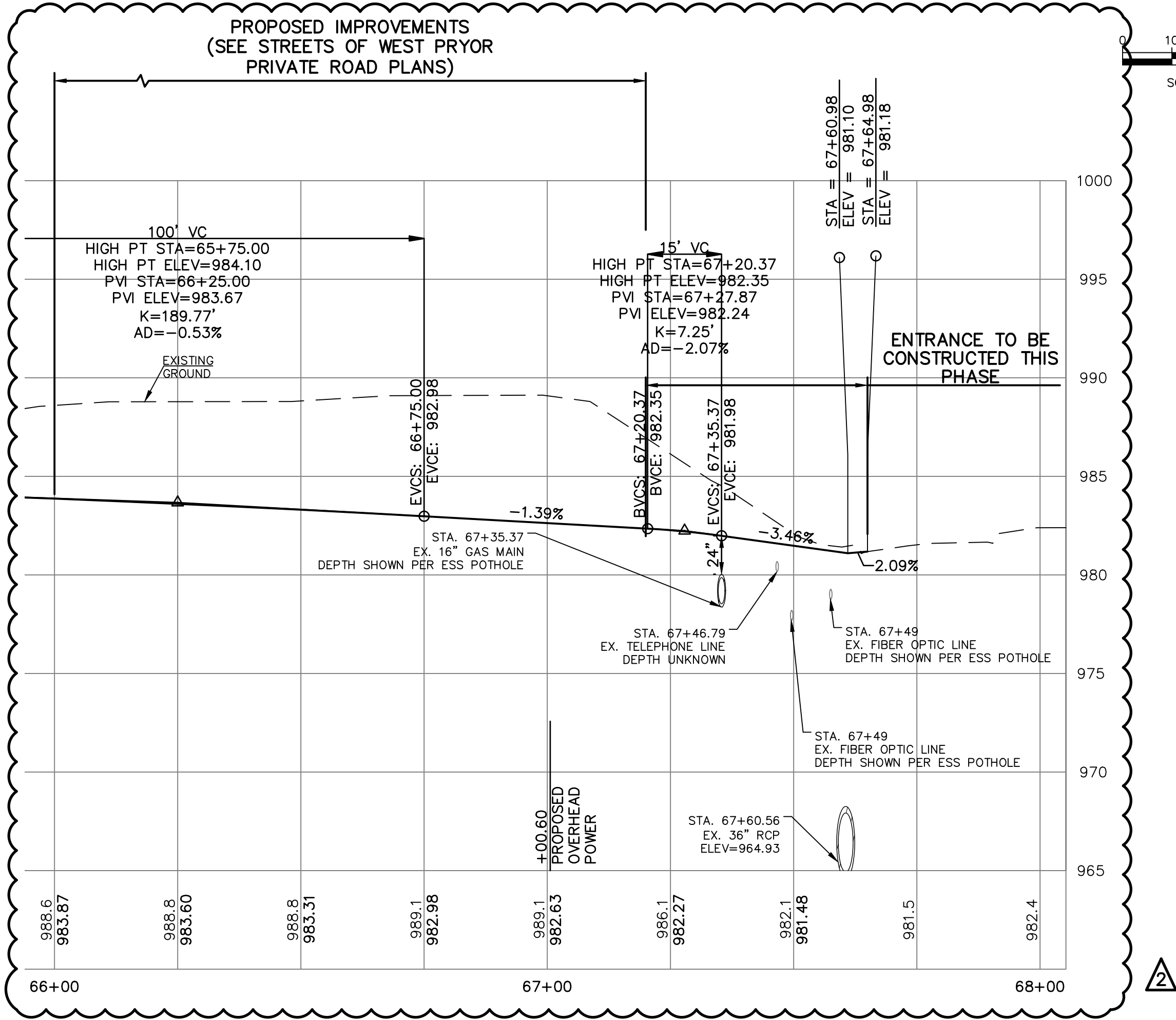
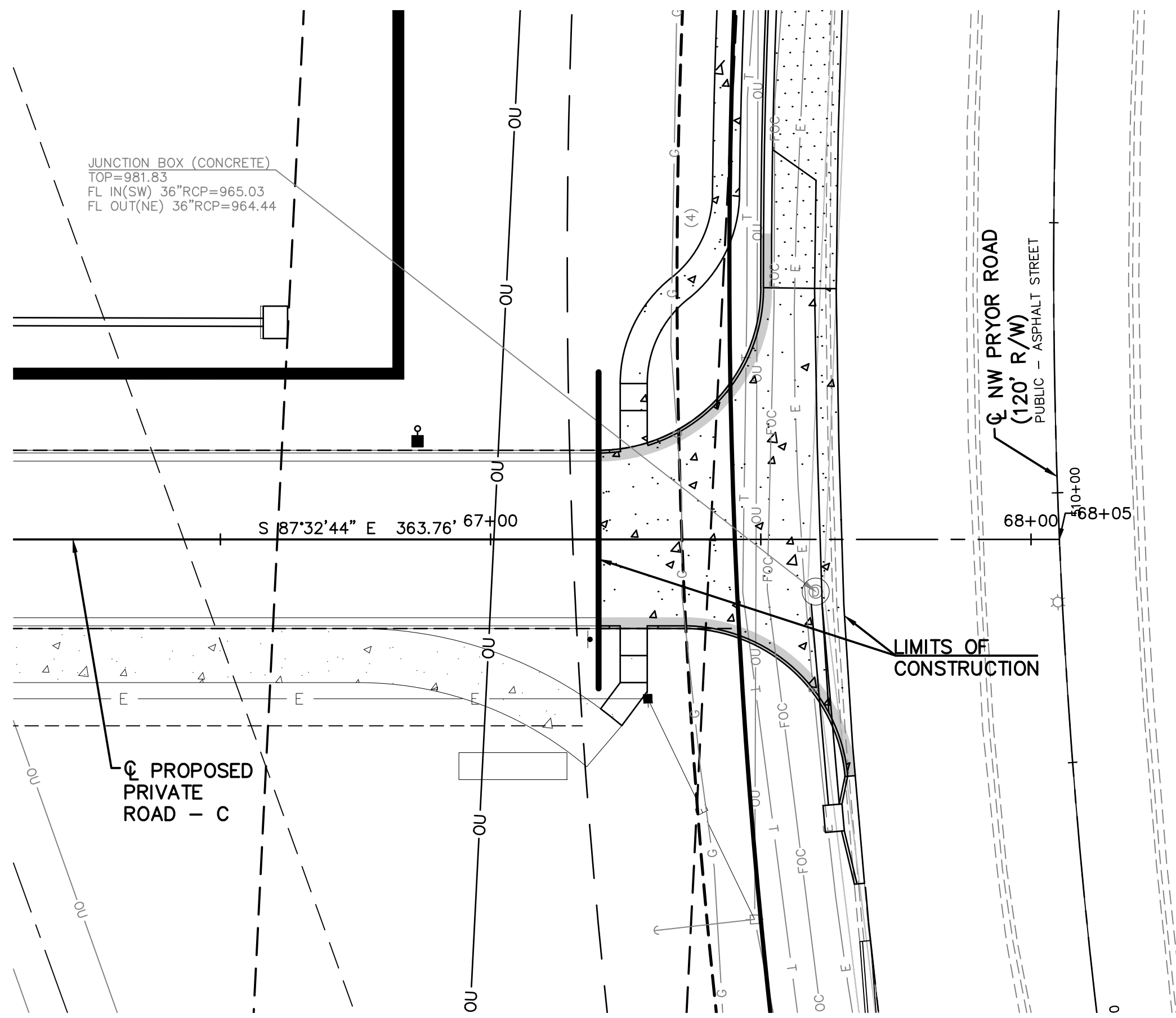
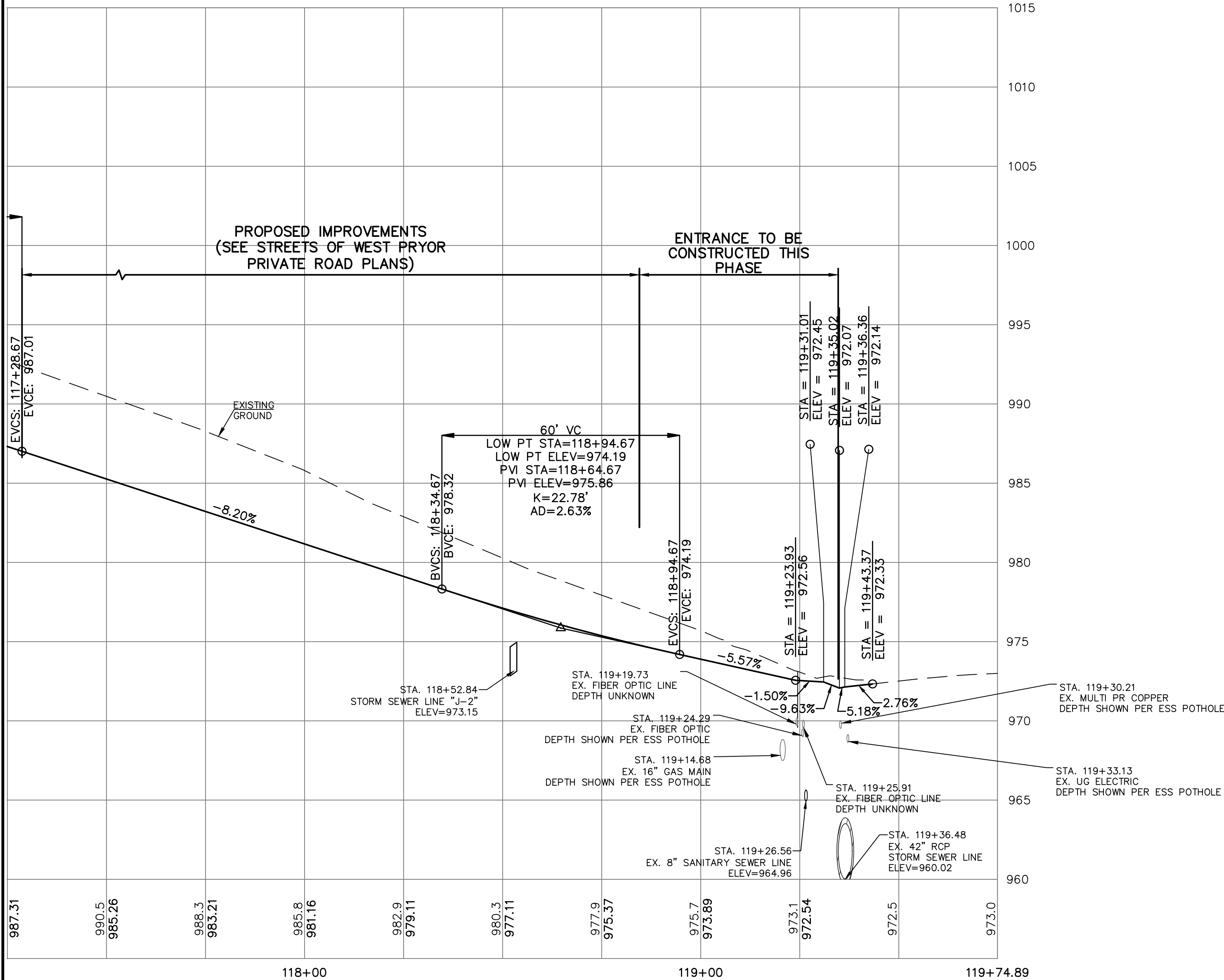
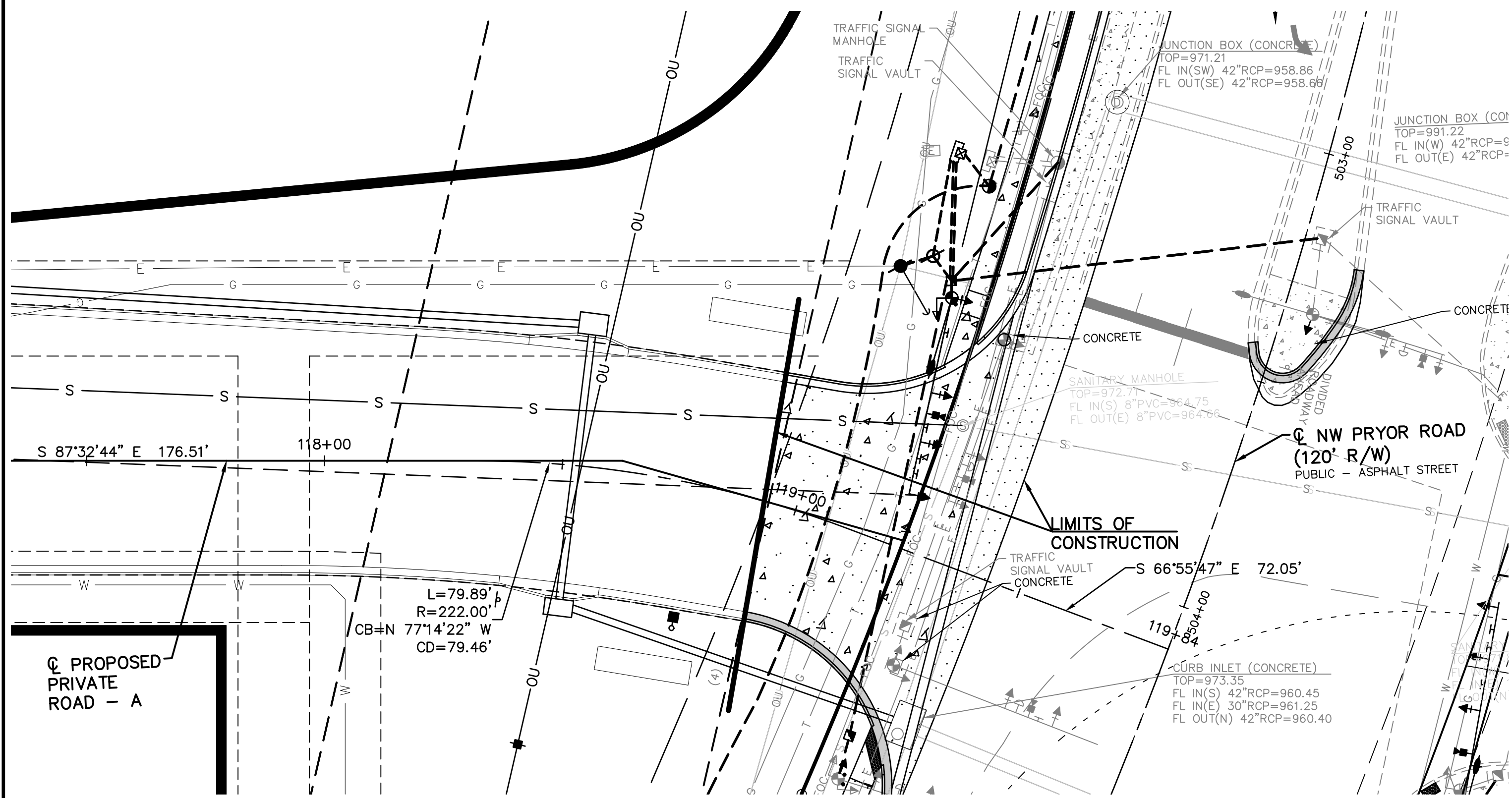
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
NW PRYOR DRIVE WIDENING - PLAN & PROFILE

PROJ. NO. **A14 7067-1**

DESIGNER **LDL** DRAWN BY **JT/BKR**

CFN **7067-1SIC_STPP**

SHEET **8** REV **2**



PROJ. NO.	A14-7067-1	
DESIGNER	LDO	
CFN	7067-1SIG_DRIVEPP	
SHEET	9	
REV	2	
REV	DATE	DESCRIPTION
2	10-3-19	REVISED PER FIELD CONDITIONS
1	7-2-19	REVISED PER CITY COMMENTS
0	5-23-19	INITIAL ISSUE
DSN	DWN	CHK

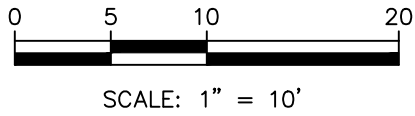
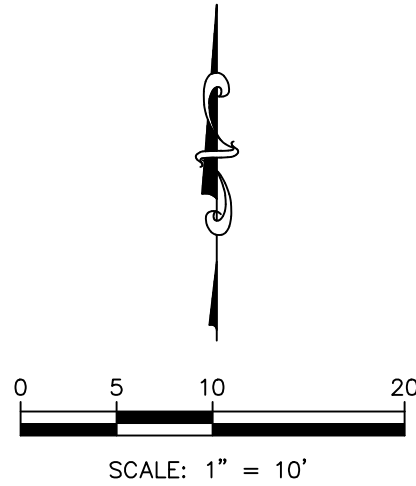
STATE OF MISSOURI
LEON D. OSBOURN
ENGINEER
NUMBER 021726
EXPIRES 12/31/19

2319 N. JACKSON | P.O. BOX 1304
JUNCTION CITY, KANSAS 66441
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joe@kve.com | www.kve.com

KAW VALLEY ENGINEERING
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING
EXPIRES 12/31/19

STREETS OF WEST PRYOR
NWQ NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

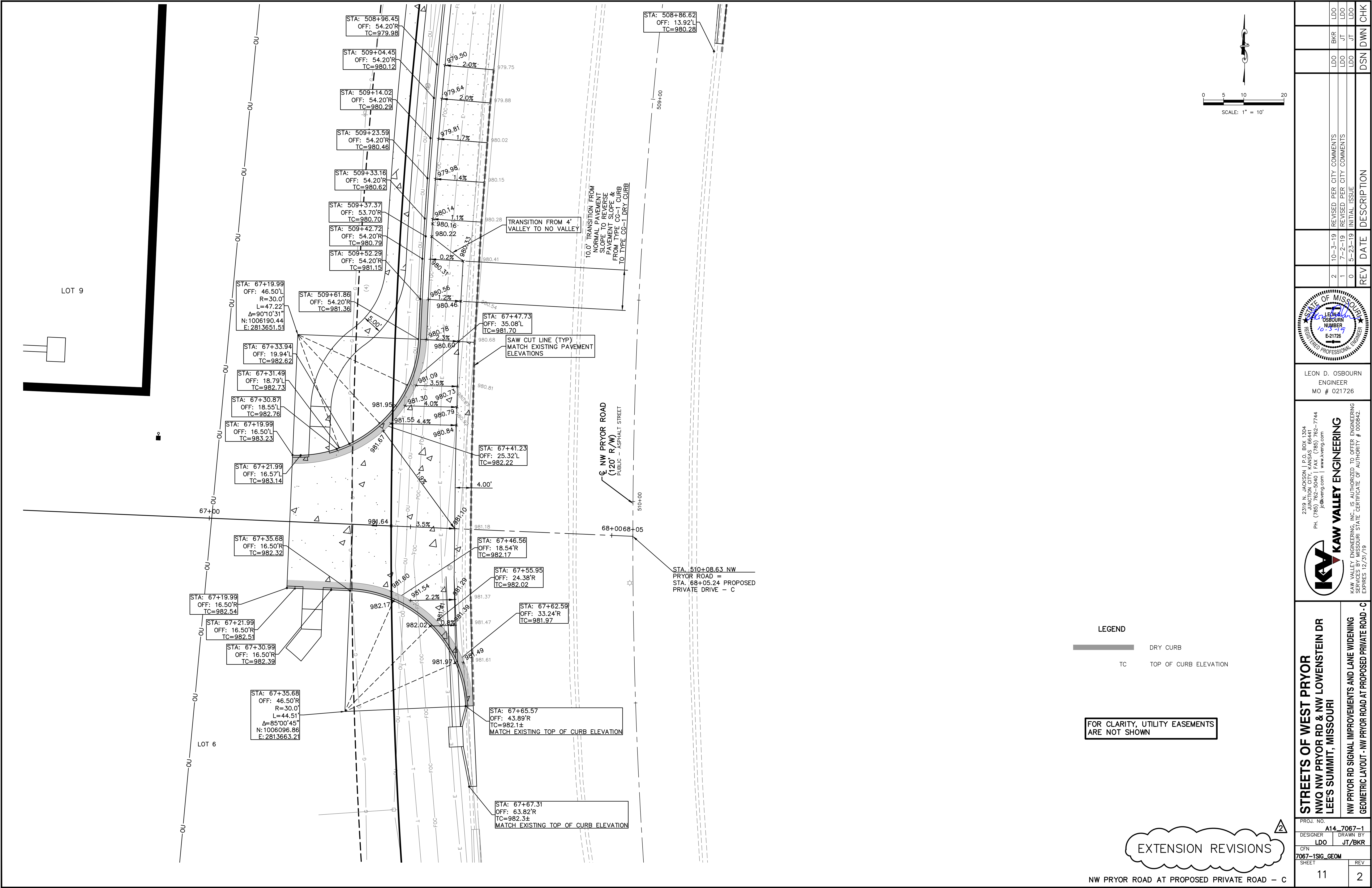
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
DRIVE AT PROPOSED PRIVATE ROAD - A & C PLAN & PROFILE



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PROJ. NO.	
A14_7067-	
DESIGNER	DRAWN BY
LDO	JT/BK



THIS DRAWING SHALL NOT BE UTILIZED BY ANY PERSON, FIRM, OR CORPORATION IN WHOLE OR IN PART WITHOUT THE SPECIFIC PERMISSION OF KAW VALLEY ENGINEERING, INC.

PROJ. NO. A14-7067-1	
DESIGNER LDO	DRAWN BY JT/BKR
CFN	REV
SHEET 11	
7067-1SIG_GEOM	
11	
2	

REV	DATE	DESCRIPTION	CHK
2	10-3-19	REVISED PER CITY COMMENTS	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO
0	5-23-19	INITIAL ISSUE	LDO
			DSN
			DWN
			CHK

STATE OF MISSOURI

LEON D. OSBOURN

ENGINEER

MO # 021726

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KAW VALLEY ENGINEERING

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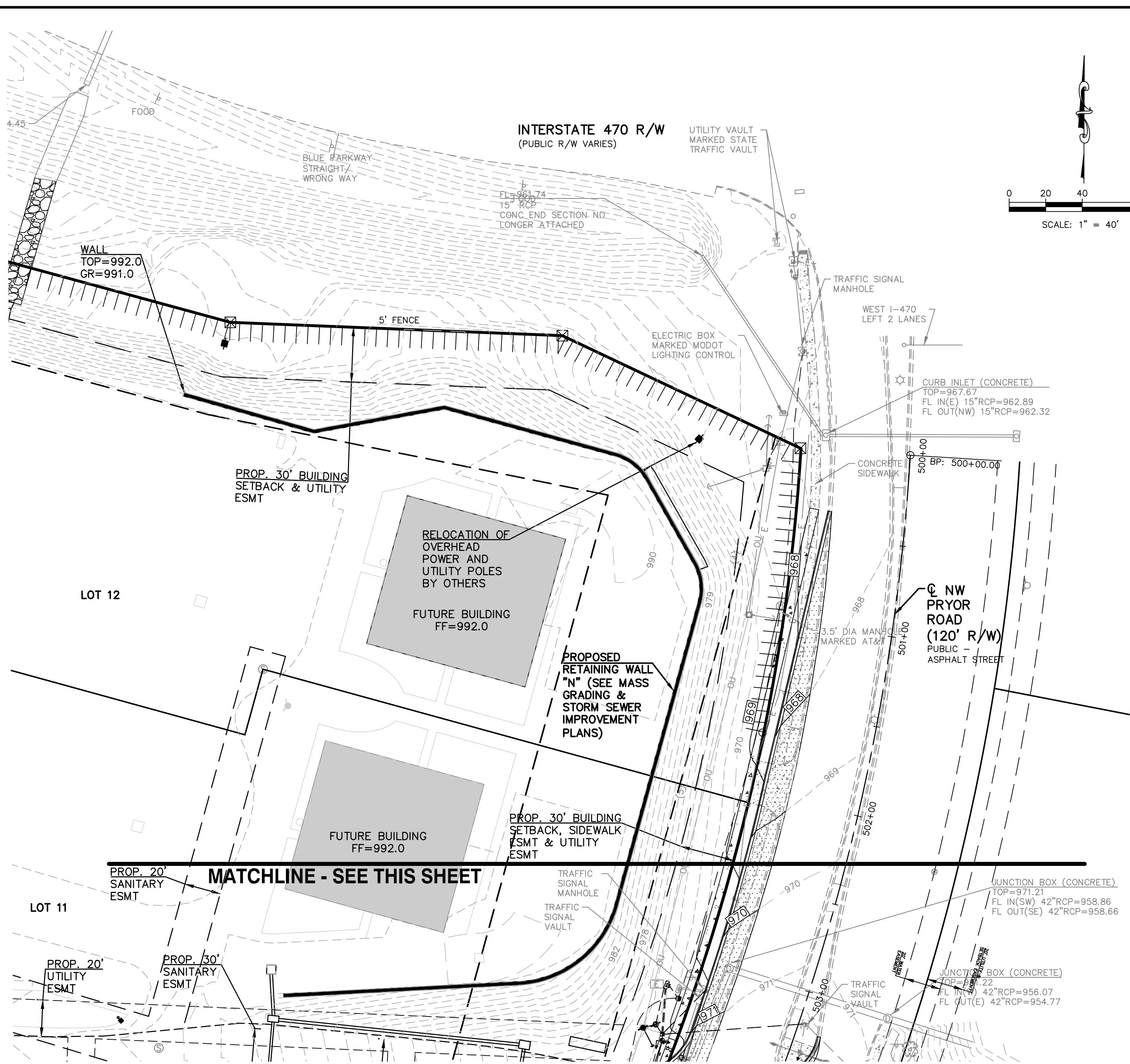
STREETS OF WEST PRYOR

NWQ NW PRYOR RD & NW LOWENSTEIN DR

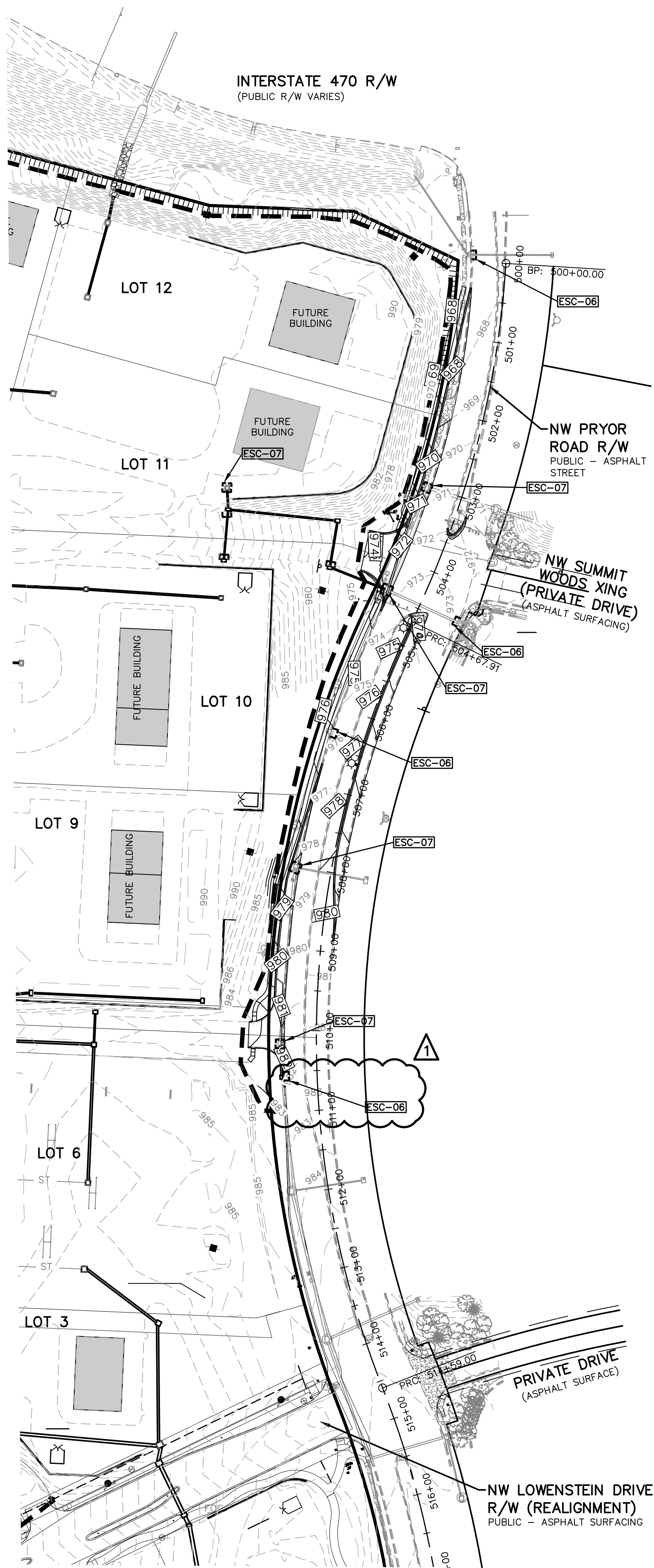
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING

GEOMETRIC LAYOUT - NW PRYOR ROAD AT PROPOSED PRIVATE ROAD - C

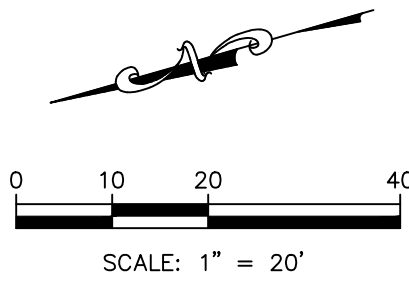
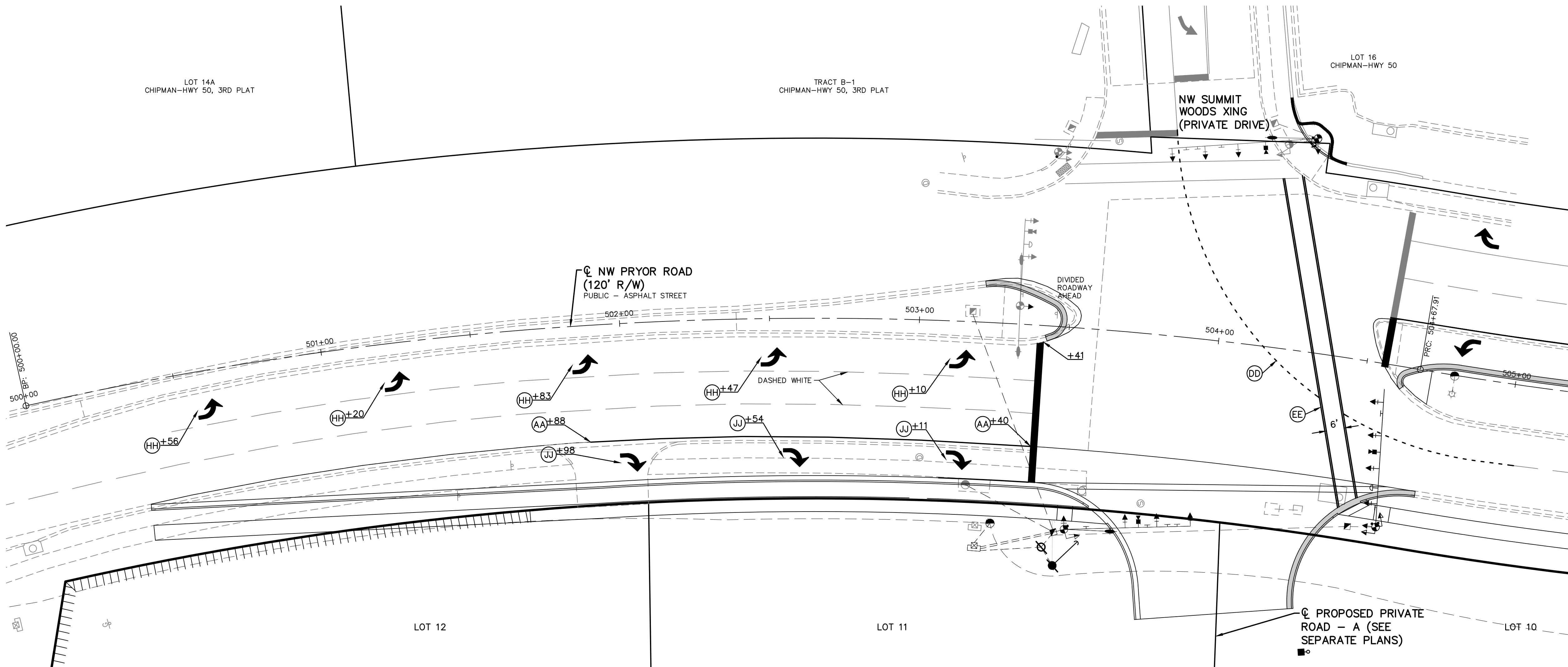


STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING GRADING PLAN	
PROJ. NO. A14_7067-1			
DESIGNER LDO		DRAWN BY JT/BKR	
CFN 7067-1SIG_GP		REV	
SHEET 12		3	



- GENERAL NOTES:
1. PROPERTY LINE IS LIMITS OF CONSTRUCTION EXCEPT AS SHOWN.
 2. THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES AS SHOWN ON THE DRAWINGS PRIOR TO BEGINNING EARTHWORK OPERATIONS.
 3. THE CONTRACTOR SHALL MAINTAIN ALL SILT CONTROL MEASURES DURING CONSTRUCTION.
 4. ALL SILT SHALL REMAIN ON SITE AND SURROUNDING STREETS SHALL BE KEPT CLEAR OF ALL MUD AND DEBRIS.
 5. A SEDIMENTATION BARRIER IS TO BE INSTALLED AS SHOWN.
 6. ACCUMULATED SEDIMENT SHALL BE REMOVED AND THE SEDIMENTATION BARRIERS MAINTAINED AS NEEDED TO PREVENT SEDIMENTATION BYPASS OF THE BARRIER.
 7. SLOPES ARE TO BE LEFT IN A ROUGH CONDITION DURING GRADING.
 8. CURB INLET SEDIMENTATION BARRIERS ARE TO BE INSTALLED AROUND INLETS AND WEIRS WHERE SEDIMENTATION IS A CONCERN. INLET BARRIERS SHALL BE EITHER BLOCK AND GRAVEL, OR SECURED STRAW BALES, OR SILT FENCE.
 9. SEDIMENT IS TO BE REMOVED FROM STORM WATER DRAINAGE SYSTEMS.
 10. RIPRAP IS TO BE INSTALLED AT AREAS OF CONCENTRATED FLOW (I.E. CULVERT OUTLETS).
 11. CONTRACTOR IS RESPONSIBLE FOR INSTALLING ANY ADDITIONAL EROSION CONTROL AS HE/SHE DEEMS NECESSARY.
 12. THE CONTRACTOR SHALL PROVIDE ALL MATERIALS, TOOLS, EQUIPMENT AND LABOR AS NECESSARY TO INSTALL AND MAINTAIN ADEQUATE EROSION AND SILTATION CONTROLS REQUIRED TO PREVENT SOIL EROSION FROM LEAVING THE PROJECT SITE. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO ENSURE THAT METHODS UTILIZED ARE ADEQUATE AND COMPLY WITH REQUIREMENTS OF THE SPECIFICATIONS AND GOVERNMENTAL AGENCIES HAVING JURISDICTION OVER THE WORK.
 13. TEMPORARY SEDIMENT FENCE TO REMAIN UNTIL ADEQUATE VEGETATION IS ESTABLISHED.
 14. MUD AND DEBRIS SHALL BE CLEANED UP AT THE CONCLUSION OF EACH WORKING DAY, OR AFTER EACH RAINFALL IF SILT IS PRESENT.
 15. INSPECTION, MAINTENANCE AND REPAIR OF EROSION CONTROL DEVICES SHALL BE ON GOING THROUGHOUT THE LIFE OF BUILDING CONSTRUCTION TO KEEP THE DEVICES IN OPERABLE CONDITION AT ALL TIMES. ADDITIONAL MEASURES SHALL BE INSTALLED AS REQUIRED BY ACTUAL FIELD CONDITIONS AND/OR GOVERNING INSPECTION AGENCIES.
 16. INSTALL CONSTRUCTION ENTRANCE AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING THE SITE AND AS SHOWN ON PLANS.

17. AT COMPLETION OF SITE GRADING AND OTHER RELATED CONSTRUCTION ACTIVITIES, ALL DISTURBED AREAS WITHIN THE PROJECT SITE SHALL BE SEEDED, SODDED, OR LANDSCAPED AS SHOWN ON THE LANDSCAPE PLAN WITHIN 14 DAYS.
18. TOPSOIL IS TO BE PLACED IN AREAS UNSUITABLE FOR VEGETATIVE GROWTH.
19. STRIP TOPSOIL PRIOR TO EXCAVATION, STOCKPILE AND SPREAD ONTO DISKED SUBGRADE (4" MIN) A THICKNESS OF 4 INCHES.
20. ROCK LINING (RIPRAP) SHALL BE DURABLE STONE CONTAINING A COMBINED TOTAL OF NOT MORE THAN 10 PERCENT OF EARTH, SAND, SHALE AND NON-DURABLE ROCK. AT LEAST 60 PERCENT OF THE MASS SHALL BE OF PIECES HAVING A MINIMUM WEIGHT OF 150 POUNDS OR MORE PER CUBIC FOOT.
21. THE CONTRACTOR SHALL HAVE THE RESPONSIBILITY FOR RESOLVING COMPLAINTS IN THE EVENT THAT COMPLAINTS OR DAMAGE CLAIMS ARE FILED DUE TO DAMAGES OCCURRING ADJACENT TO OR DOWNSTREAM FROM PROPERTY BY SEDIMENT RESULTING FROM EROSION ON THE PROJECT SITE.
22. GOOD HOUSEKEEPING PRACTICES SHALL BE MAINTAINED ON SITE TO KEEP SOLID WASTE FROM ENTRY INTO WATERS.
23. ALL FUELING FACILITIES PRESENT ON SITE SHALL ADHERE TO APPLICABLE FEDERAL AND STATE REQUIREMENTS CONCERNING UNDERGROUND STORAGE, ABOVE GROUND STORAGE AND DISPENSERS, INCLUDING SPILL PREVENTION, CONTROL AND COUNTER MEASURES.
24. RIGHT OF WAY TO BE STABILIZED AS REQUIRED BY APWA SECTION 2400.
25. EROSION CONTROL IS TO BE PLACED IN PHASING AS CONSTRUCTION PROGRESSES.
26. MINIMAL WASHING OF CONCRETE EQUIPMENT ALLOWED, CHUTE ETC. CONCRETE WASHOUT OF THE DRUM IS NOT ALLOWED. ANY PIT/WASHOUT AREA NEEDS TO BE MAINTAINED IN A NON-DISCHARGING MANNER AND ANY WASTE RESIDUE WILL NEED TO BE CLEANED OUT AND REMOVED AT THE END OF PROJECT.
27. EROSION CONTROL SEDIMENT FENCE TO BE INSTALLED 1'-0" BEHIND CURB & GUTTER UPON COMPLETION OF BACKFILL OF CURB IN ALL AREAS WHERE SLOPES FROM LOT DRAIN TOWARDS CURB. UPON COMPLETION OF FINAL GRADING THE TOES OF ALL EMBANKMENTS IN EXCESS OF TWO FEET IN HEIGHT WILL HAVE EROSION CONTROL SEDIMENT FENCE INSTALLED.



NOTE:

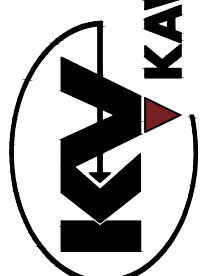
1. PAVEMENT MARKING SHALL BE PROVIDED AND INSTALLED IN ACCORDANCE WITH THE CURRENT CITY OF LEE'S SUMMIT PAVEMENT MARKING STANDARDS.
2. ALL DIMENSIONS REFERENCING DOUBLE YELLOW LINES REFERS TO THE CENTER OF THE SPACE BETWEEN THE TWO DOUBLE YELLOW LINES.
3. CONTRACTOR TO GRIND OFF CONFLICTING PAVEMENT MARKING.

LIST OF PAVEMENT MARKING	
MARK	ITEM
AA	4" SOLID WHITE LINE (HIGH BUILD PAINT)
DD	6" DASHED WHITE EXTENSION LINE (THERMOPLASTIC)
EE	6" SOLID WHITE CROSS-WALK LINE (THERMOPLASTIC)
GG	24" STOP BAR (THERMOPLASTIC)
HH	TURN ARROW WHITE (LEFT) (THERMOPLASTIC)
JJ	TURN ARROW WHITE (RIGHT) (THERMOPLASTIC)

STREETS OF WEST PRYOR
NWQ NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
NW PRYOR ROAD PAVEMENT MARKING PLAN

PROJ. NO. A14_7067-1	
DESIGNER LDO	DRAWN BY JT/BKR
CFN 7067-1SIG_PMP	
SHEET 14	REV 2



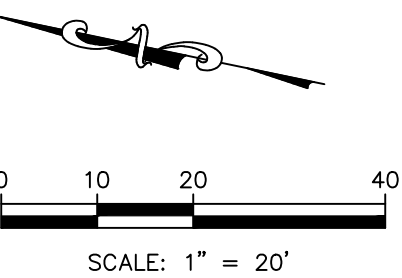
KAW VALLEY ENGINEERING
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LEON D. OSBOURN
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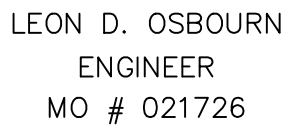


REV	DATE	DESCRIPTION	DSN	DWN	CHK
2	7-24-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO



NOTE:

1. PAVEMENT MARKING SHALL BE PROVIDED AND INSTALLED IN ACCORDANCE WITH THE CURRENT CITY OF LEE'S SUMMIT PAVEMENT MARKING STANDARDS.
2. ALL DIMENSIONS REFERENCING DOUBLE YELLOW LINES REFERS TO THE CENTER OF THE SPACE BETWEEN THE TWO DOUBLE YELLOW LINES.
3. CONTRACTOR TO GRIND OFF CONFLICTING PAVEMENT MARKING.



**SHEE'S OF WEST PRYOR
NNWQ NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI**

PROJ. NO.	
A14_7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
7067-1SIG_PMP	
SHEET	REV
15	2

GENERAL NOTES:

1. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS (APPROVED BY THE CITY OF LEE'S SUMMIT) AND ONE (1) COPY OF THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES.

2. CONSTRUCTION OF THE IMPROVEMENTS SHOWN OR IMPLIED BY THIS SET OF DRAWINGS SHALL NOT BE INITIATED OR ANY PART THEREOF UNDERTAKEN UNTIL THE DIRECTOR OF PUBLIC WORKS OR HIS AGENT IS NOTIFIED OF SUCH INTENT, AND ALL REQUIRED AND PROPERLY EXECUTED BONDS AND CONTRACT AGREEMENTS ARE RECEIVED AND APPROVED BY THE CITY.

3. THE CONSTRUCTION COVERED BY THESE PLANS SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI, IN CURRENT USE. SPECIFICALLY, BUT NOT EXCLUSIVE TO: TRAFFIC SIGNAL SPECIFICATION: SECTION 2900. TRAFFIC SIGNAL STANDARD DRAWINGS: TS-1 THROUGH TS-10.

4. ALL WORKMANSHIP AND MATERIALS SHALL BE SUBJECT TO THE INSPECTION AND APPROVAL OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI.

5. RIGHT-OF-WAY LIMITS SHOULD BE CROSS CHECKED BY THE CONTRACTOR AND APPROVED BY THE FIELD INSPECTOR BEFORE UNDERTAKING ANY EXCAVATIONS AT THE SITE.

6. THE CONTRACTOR SHALL STAKE THE LOCATION OF ALL POLE BASES, PULL BOXES, AND CONTROLLER CABINET BASE, THEN PROVIDE THE CITY ONE WEEK NOTICE PRIOR TO THE START OF CONSTRUCTION, AND SUBSEQUENT CONSTRUCTION ACTIVITIES, FOR INSPECTION AND APPROVAL. THE CONTRACTOR SHALL PROVIDE A WORK SCHEDULE, CONTACT NAMES, AND PHONE NUMBERS.

7. ALL LOCATIONS INDICATED IN DRAWINGS, INCLUDING CONDUIT RUNS ARE SUBJECT TO ADJUSTMENT TO CLEAR OBSTRUCTIONS AND TO MEET SITE CONDITIONS, IF ANY BY THE CITY.

8. EXISTENCE AND LOCATION OF ANY UNDERGROUND OR OVERHEAD FACILITIES SHOWN ON THESE DRAWINGS OR REFERENCE TO ANY SOIL CONDITIONS, IF MADE, ARE APPROXIMATE ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL SITE CONDITIONS AND TO LOCATE ALL UTILITIES, INCLUDING DEPTH, BEFORE STARTING CONSTRUCTION SO THAT ANY ADJUSTMENTS TO DESIGN CAN BE MADE PRIOR TO POLE ORDERING OR FABRICATION. IN ADDITION, THE CONTRACTOR SHALL AVOID DISRUPTION OF SERVICES PROVIDED BY THE UTILITIES AND SHALL INSURE THAT PROPER CLEARANCES (OVERHEAD AND UNDERGROUND) ARE MAINTAINED FOR THE DURATION OF CONSTRUCTION. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UTILITIES.

9. THE CONTRACTOR SHALL COORDINATE WITH THE CITY TRAFFIC ENGINEER FOR ANY NECESSARY CHANGES TO THE TRAFFIC SIGNALS RESULTING FROM EXISTING UTILITIES OR OTHER CONSTRUCTION ISSUES.

10. ANY EQUIPMENT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

11. SIGNAL EQUIPMENT SHALL NOT FORM AN OBSTRUCTION TO THE MOVEMENT OF PEDESTRIAN AND WHEELCHAIR TRAFFIC AND SHALL BE ADA ACCESSIBLE. WHERE SIDEWALKS ARE PRESENT, A MINIMUM CLEAR WIDTH OF 48 INCHES SHALL BE AVAILABLE FOR PEDESTRIAN AND WHEELCHAIR MOVEMENT. PULL BOXES SHALL NOT BE INSTALLED ON WHEELCHAIR RAMPS.

12. CONDUITS TO BE PLACED OUTSIDE OF PAVED AREAS SHALL BE TRENCHED IN PLACE. IF THE PROJECT INCLUDES ROADWAY IMPROVEMENTS, THE CONDUIT SHALL BE TRENCHED AFTER THE ROADWAY ROUGH GRADE IS ESTABLISHED AND PRIOR TO ANY FINAL ROADWAY PAVING, CURB & GUTTER, MEDIAN OR SIDEWALK SECTIONS ARE PLACED. ALL COMPACTION AND BACKFILL SHALL MEET CITY OF LEE'S SUMMIT REQUIREMENTS. AT THE OPTION OF THE CONTRACTOR, CONDUITS MAY BE BORED OUTSIDE PAVED AREAS, BUT THERE WILL BE NO ADJUSTMENT TO THE UNIT PRICES FOR CONDUIT INSTALLATION AND ANY CHANGE IN COST WOULD BE THE CONTRACTOR'S RESPONSIBILITY. ANY CONDUIT BORE OUTSIDE THE PAVED AREAS SHALL BE DONE AFTER ROADWAY IMPROVEMENTS ARE COMPLETE. CONDUITS TO BE PLACED WITHIN THE LIMITS OF PAVEMENT SHALL BE BORED UNLESS OTHERWISE AUTHORIZED BY THE CITY TRAFFIC ENGINEER. IF THE PROJECT INCLUDES ROADWAY IMPROVEMENTS, THE CONDUIT SHALL BE BORED PRIOR TO ANY FINAL ROADWAY PAVING. POTHOLING FOR UTILITIES ON ROAD BORES AFTER FINAL PAVING WILL NOT BE ALLOWED.

13. THE TRAFFIC SIGNAL CONTROLLER, CABINET AND RELATED EQUIPMENT, AS SPECIFIED FOR THIS PROJECT, SHALL BE DELIVERED TO THE CITY FOR TESTING PRIOR TO INSTALLATION. ALL SIGNAL TIMINGS WILL BE PROVIDED BY THE CITY TRAFFIC ENGINEER. THE CONTRACTOR SHALL COORDINATE MATERIAL DELIVERY AND PICK-UP WITH THE PUBLIC WORKS OPERATIONS DEPARTMENT (969-1870) AT LEAST 48 HOURS PRIOR TO TRANSPORTATION. THE CONTRACTOR ASSUMES ALL DAMAGE LIABILITY AND SHOULD INSPECT ALL MATERIALS BEFORE AND AFTER TRANSPORTATION OF EQUIPMENT.

14. THE CONTRACTOR SHALL COORDINATE ALL ELECTRICAL POWER REQUIREMENTS AND CONNECTION ACTIVITIES WITH THE UTILITY COMPANY, INCLUDING LOCATION OF THE METER, CIRCUITRY AND CONNECTION REQUIREMENTS, AND POWER UP THE COMPLETE SYSTEM.

15. ALL DISTURBED SURFACES SHALL BE MADE GOOD TO MATCH EXISTING AT THE CONTRACTOR'S EXPENSE.

16. CONTRACTOR SHALL MAINTAIN AT ALL TIMES ACCESS FOR EMERGENCY VEHICLES AND RESIDENTS ALONG THE ENTIRE PROJECT.

17. SUBSTANTIAL COMPLETION OF THE TRAFFIC SIGNALS SHALL BE DEFINED AS ALL COMPONENTS OF THE TRAFFIC SIGNAL OPERATED FULLY AND SATISFACTORILY WITH RED, YELLOW, AND GREEN CYCLES. SUBSTANTIAL COMPLETION SHALL ALLOW FOR TESTING OF THE SIGNALS, INCLUDING A FLASH PERIOD, PRIOR TO SIGNALS OPERATING WITH CYCLES.

18. FINAL ACCEPTANCE OF TRAFFIC SIGNALS SHALL BE DEFINED AS FINAL WRITTEN APPROVAL AND ACCEPTANCE BY THE CITY, INCLUDING COMPLETION OR CORRECTIONS OF ALL PUNCH LIST ITEMS AND THE TRAFFIC SIGNALS FULLY OPERATIONAL FOR A TIME PERIOD OF FIFTEEN (15) DAYS, WITHOUT ANY PROBLEM, AS NOTED IN THE SPECIFICATIONS. AS-BUILT PLANS SHALL BE SUBMITTED PRIOR TO FINAL ACCEPTANCE BY THE CITY.

19. THE CONTRACTOR SHALL VERIFY AND/OR COORDINATE WITH THE VARIOUS SERVICE PROVIDERS ON THE EXACT LOCATIONS OF METERS, CONNECTION POINTS AND OTHER SPECIFIC PROJECT REQUIREMENTS. THE PROPOSED SERVICE LINE LOCATIONS THAT ARE SHOWN ARE CONCEPTUAL AND ARE NOT WARRANTED.

PROJECT SPECIFIC NOTES:

1. THE CONTRACTOR SHALL SUPPLY THE TRAFFIC SIGNAL CONTROLLER, CABINET, SIGNAL POLES WITH MAST ARMS AND POWER SUPPLY ASSEMBLY, AND THE CONTRACTOR SHALL INSTALL THE EQUIPMENT. THE CONTRACTOR WILL BE RESPONSIBLE FOR SUPPLYING THE SIGNAL PEDESTAL POLES. RE-USE EXISTING IN SYNC COMPONENTS. WILL REQUIRE NEW CAMERA AND CARD.

2. THE VIDEO DETECTION SYSTEM SHALL BE AN IN SYNC ADAPTIVE TRAFFIC CONTROL SYSTEM SUPPLIED BY RHYTHM ENGINEERING, 12351 W 96TH TERRACE, SUITE 107, LENEXA, KANSAS 66215, PHONE (913)227-0603. MATERIALS SUPPLIED BY RHYTHM ENGINEERING SHALL CONSIST OF AN IN SYNC PROCESSOR, VIDEO CAMERAS, TRANSFORMER, RG CABLE, DETECTOR CARDS AND CABLES, SITE EQUIPMENT PANELS, MONITOR, KEYBOARD AND CAMERA MOUNTING HARDWARE.

3. CABLES FOR THE VIDEO DETECTION SYSTEM SHALL BE ONE CONTINUOUS 3c-#14 CABLE AND ONE CONTINUOUS CATEGORY 5E+ CABLE (SHIELDED OUTDOOR RATED) BETWEEN THE CONTROLLER AND EACH CAMERA. THE 3c-#14 CABLE SHALL MEET REQUIREMENTS OF IMSA 20-1. CABLES ARE INCLUDED IN THE LUMP SUM PRICE FOR THE VIDEO DETECTION SYSTEM. NO ADDITIONAL PAYMENTS WILL BE MADE.

CONSTRUCTION SEQUENCING:

1. CONTRACTOR TO OBTAIN AND FOLLOW TEMPORARY TRAFFIC CONTROL PERMIT (TTCP) CONDITION REQUIREMENTS.

2. CONTRACTOR TO SUBMIT FOR APPROVAL BY CITY OF LEE'S SUMMIT TRAFFIC ENGINEER A DETAILED TRAFFIC SEQUENCING PHASING PLAN.

3. CONTRACTOR TO MAINTAIN EXISTING SIGNAL OPERATIONS UNTIL NEW SYSTEM, HAS BEEN TESTED AND OPERATIONAL.

4. PER TTCP, CONTRACTOR TO INSTALL TRAFFIC CONTROL, CHANNELIZATION DEVICES AND TEMPORARY PAVEMENT MARKING AROUND WORK ZONE.

5. CONTRACTOR TO PROVIDE PEDESTRIAN DETOUR PLAN ALONG WITH TRAFFIC SEQUENCING PLAN.

6. CONTRACTOR TO INSTALL CONSTRUCTION FENCING AROUND AREAS WHERE PEDESTRIAN MOVEMENTS MAY CONFLICT WITH CONSTRUCTION ACTIVITIES.

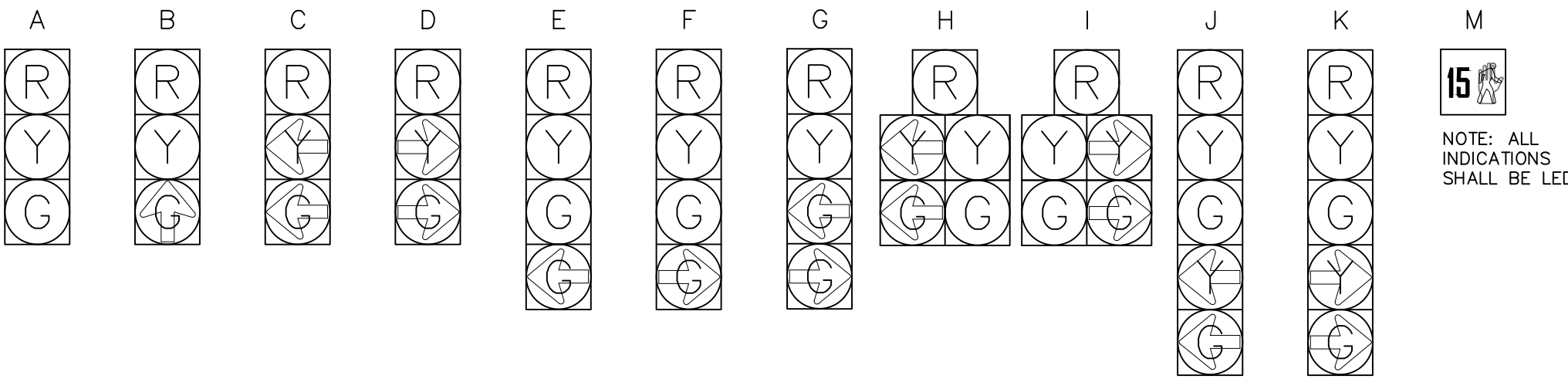
7. NO COLLECTOR OR ARTERIAL STREETS SHALL HAVE LANE(S) REDUCED OR CLOSED BEFORE 9 A.M. AND AFTER 4 P.M. MONDAY THRU FRIDAY. NO WORK ON SATURDAY, SUNDAYS OR HOLIDAYS OBSERVED BY THE CITY.

8. ALL TRAVEL LANES SHALL BE A MINIMUM OF 11.00 FEET WIDE EXCLUDING CURB AND GUTTER. ALL SIDEWALKS SHALL HAVE A MINIMUM CLEAR PATH OF 3.5 FEET. ALL MULTI-USE PATHS SHALL HAVE A MINIMUM CLEAR PATH OF 8.00 FEET.

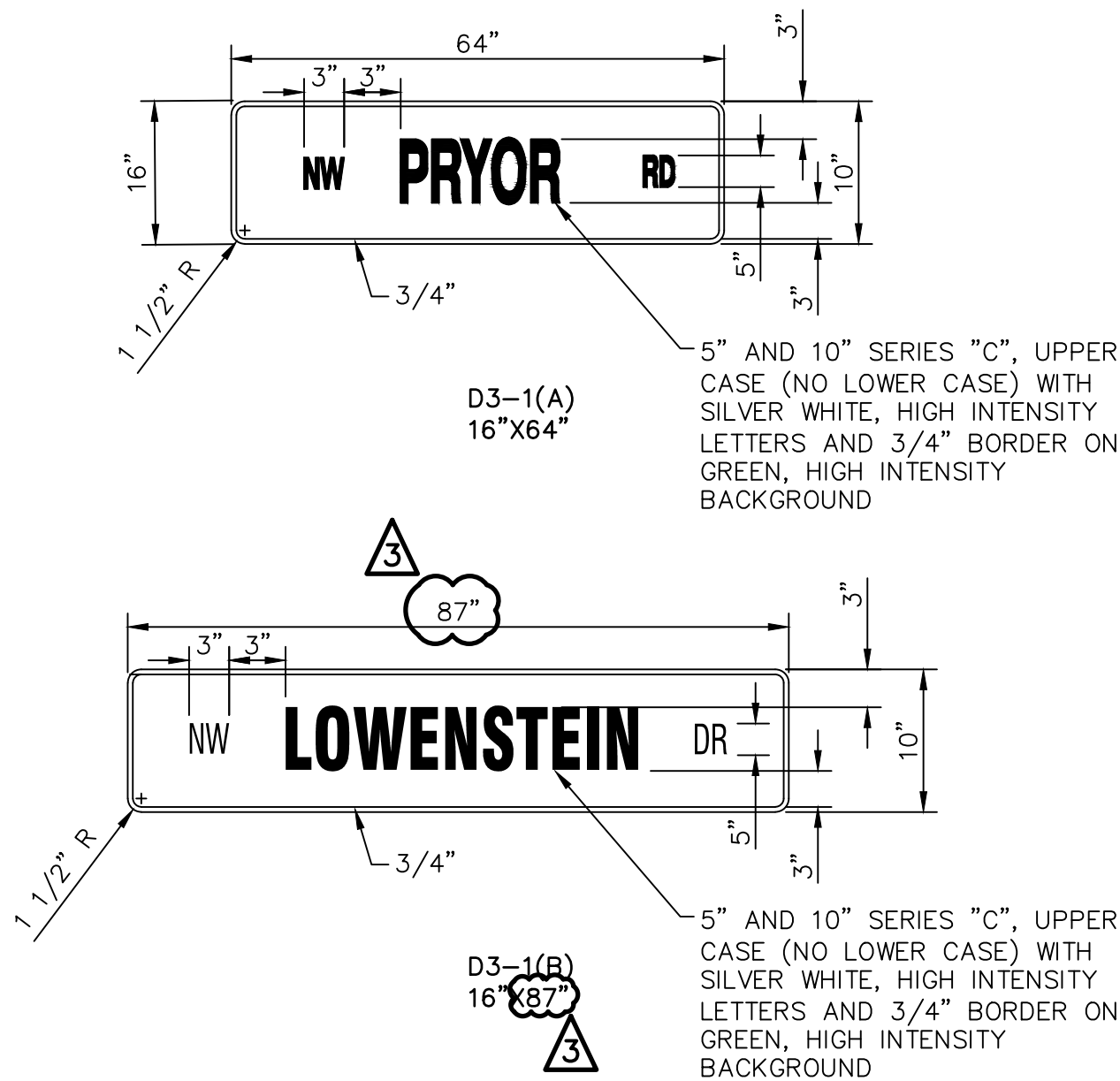
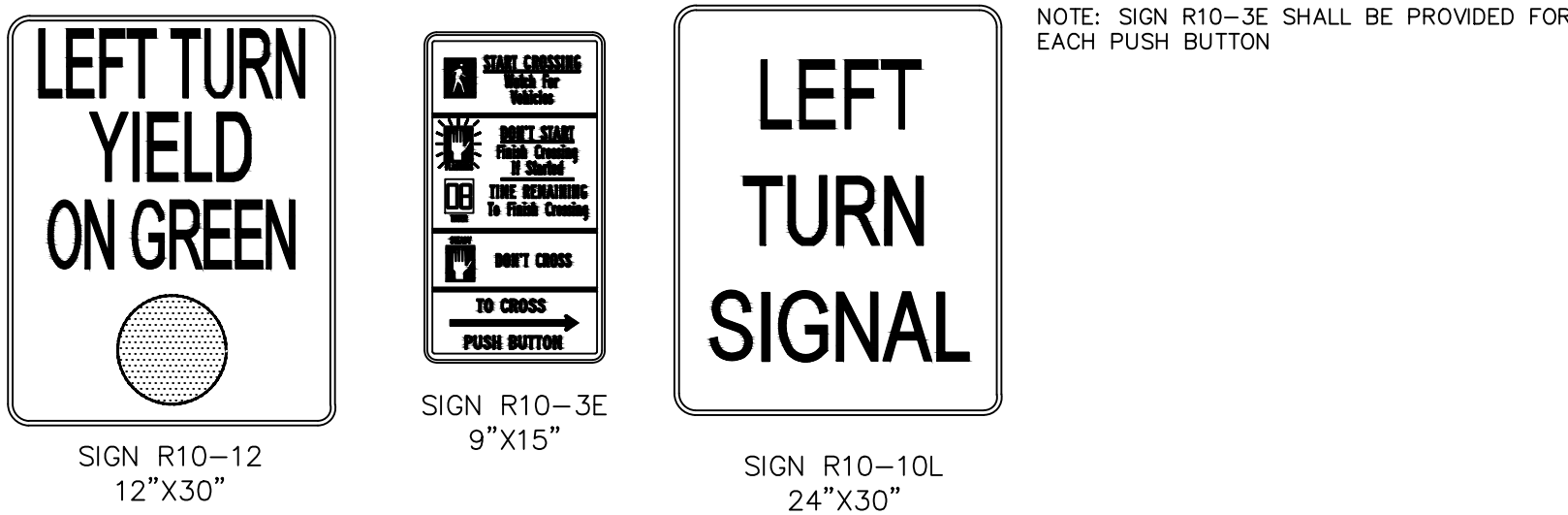
9. CONTRACTOR SHOULD REVIEW TTCP AND MEET REQUIREMENTS PER CITY OF LEE'S SUMMIT REGULATIONS.

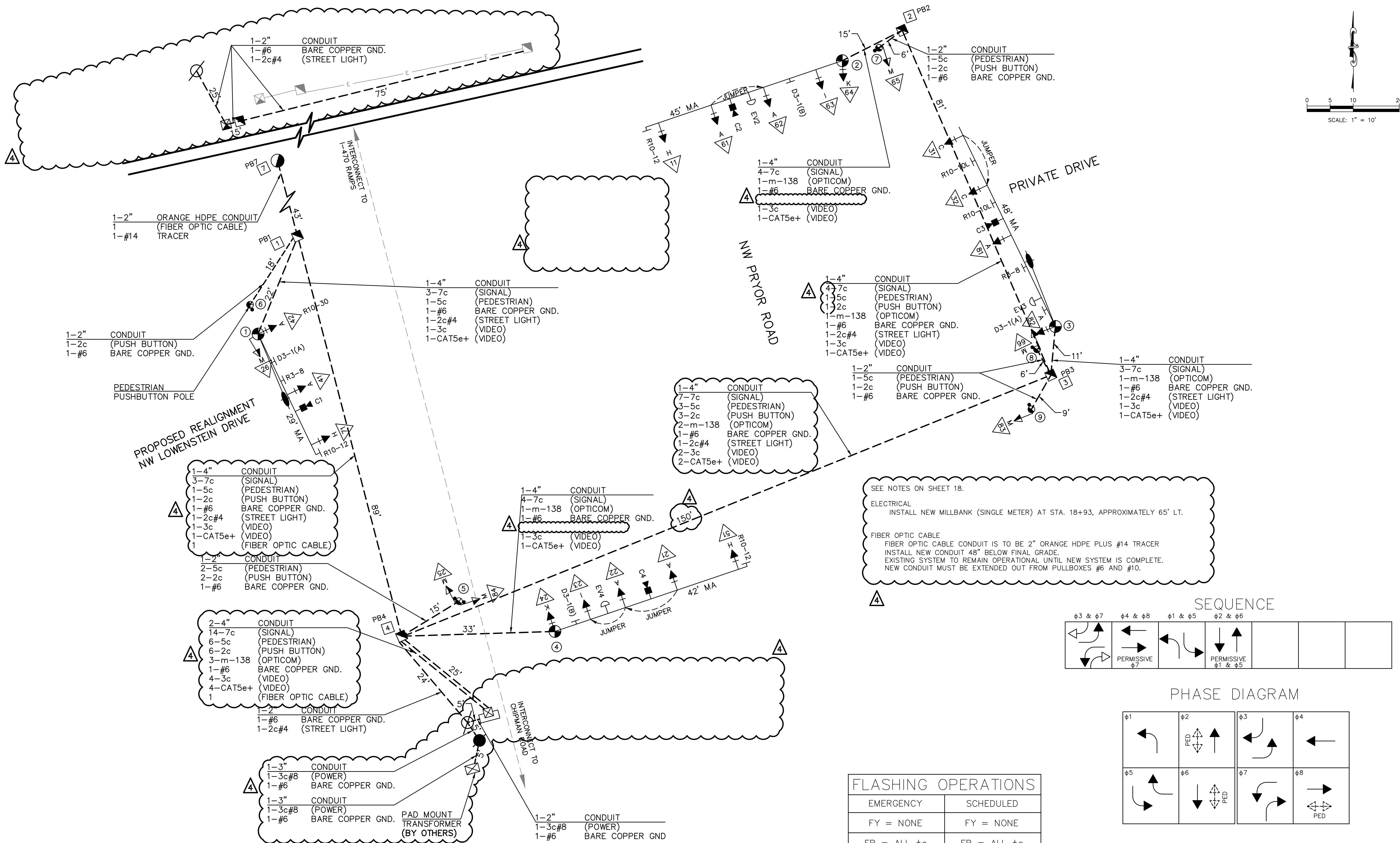
LEGEND

	OPTICALLY LIMITING TRAFFIC SIGNAL HEAD		POWER SUPPLY (DISCONNECT)
	TRAFFIC SIGNAL HEAD		SUGGESTED VEHICLE DETECTION ZONE
	TRAFFIC SIGNAL HEAD WITH BACKPLATE		PUSH BUTTON DETECTOR
	PEDESTRIAN SIGNAL HEAD		OPTICOM DETECTOR
	STOP LINE		SECONDARY SERVICE POINT
	LANE USE		CONDUIT
	STREET LIGHT POLE		SIGNAL FACE NUMBER
	MAST ARM POLE		POST NUMBER
	SIGNAL PEDESTAL		DETECTOR NUMBER
	TRAFFIC CONTROLLER CABINET		PULL BOX NUMBER
	STREET LIGHT CONTROLLER CABINET		PUSH BUTTON NUMBER
	PULL BOX		COBRA HEAD LUMINAIRE
	FIBER OPTIC PULL BOX		VIDEO DETECTION CAMERA
			SIGN



TYPICAL REGULATORY SIGN DETAIL





WIRING AND PHASING GENERAL NOTES::

1. THE OUTBOARD SIGNAL HEAD (FURTHEST ON THE MAST ARM FROM THE POLE) FOR EACH PHASE SHALL EACH BE SERVED BY ONE 7c#14 CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. EACH OF THE REMAINING SAME PHASE VEHICLE SIGNAL HEADS LOCATED ON THE MAST ARE SHALL BE CONNECTED TO LIKE PHASE SIGNAL HEADS VIA A 7c#14 CABLE CONNECTED WITHIN THE SIGNAL HEAD TERMINAL BOX. A MAXIMUM OF THREE VEHICLE HEADS MAY BE JOINED TOGETHER. ANY ADDITIONAL SIGNAL HEADS WOULD REQUIRE A SEPARATE CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. ALL VEHICULAR SIGNAL HEADS LOCATED ON THE POLE SHALL EACH BE SERVED BY ONE 7c#14 CABLE EXTENDING FROM THE HEAD BACK TO THE CONTROLLER. NO CABLE SPLICES ARE ALLOWED, INCLUDING AT THE BASE OF THE POLE AND INSIDE PULL BOXES, EXCEPT FOR STREET LIGHTING CABLES.

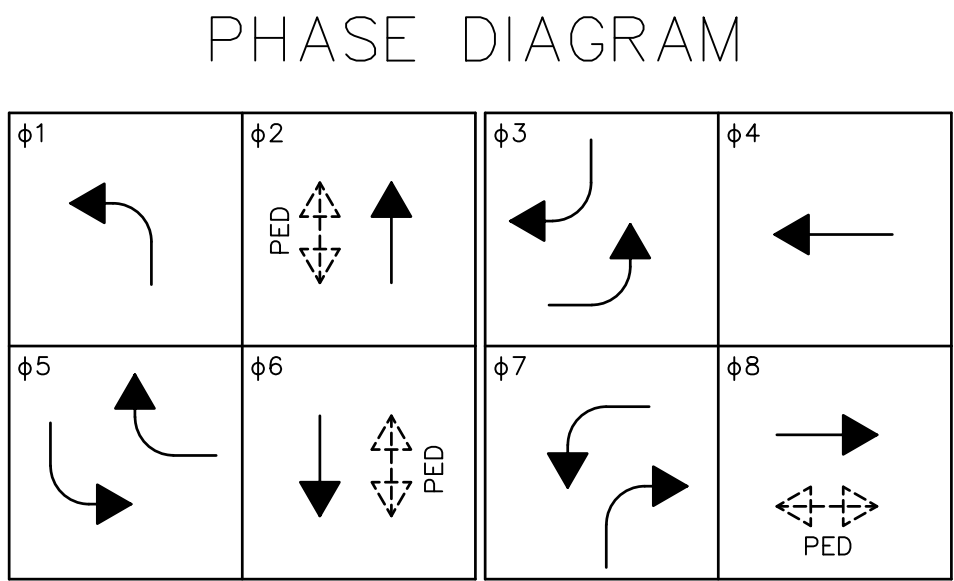
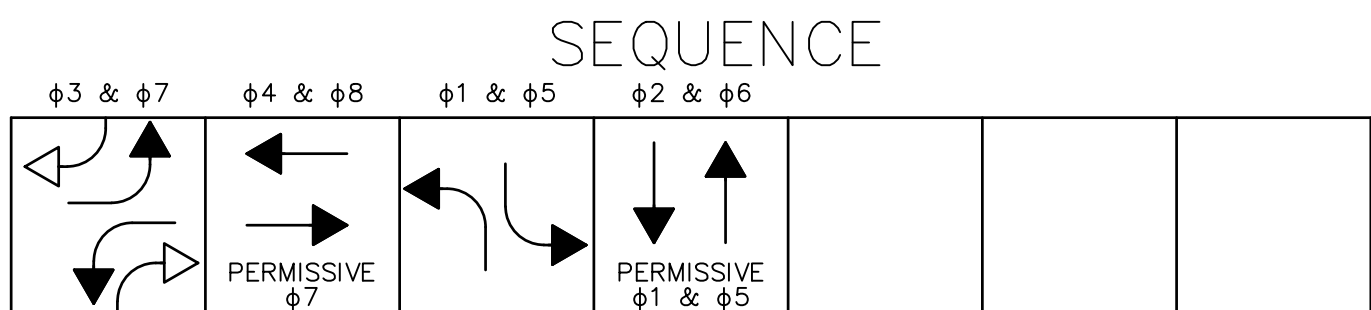
2. STREET LIGHTING CABLE, NOT SIGNAL CABLE, MAY BE SPLICED INSIDE OF PULL BOXES USING A SPLIT BOLT CONNECTOR AND RESIN FILLED SPLICE KIT AS DESCRIBED IN SECTION 2800 OF THE TECHNICAL SPECIFICATIONS.

3. A CONTINUOUS 1c #6 AWG BARE SOLID COPPER GROUND WIRE SHALL BE PROVIDED IN ADDITION TO GROUND RODS. ALL GROUNDING AND GROUND RODS SHALL BE TIED TOGETHER USING 1c #6 AWG BARE SOLID COPPER WIRE TO BOND THE SYSTEM.

FLASHING OPERATIONS	
EMERGENCY	SCHEDULED
FY = NONE	FY = NONE
FR = ALL ϕ s	FR = ALL ϕ s

POWER SUPPLY				
LOCATION	POWER SUPPLY TYPE	CIRCUIT BREAKER TRIP RATINGS		
		SERVICE DISCONNECT (2 POLE)	TRAFFIC SIGNAL (1-POLE)	LIGHTING (2-POLE)
SW CORNER	1 CIRCUIT	40 AMP	30 AMP	15 AMP

FR1	ϕ 1	ϕ 2	PED ϕ 2	ϕ 3	ϕ 4	PED ϕ 4	MONITOR
	ϕ 5	ϕ 6	PED ϕ 6	ϕ 7	ϕ 8	PED ϕ 8	
FR2							



SEE NOTES ON SHEET 18.

ELECTRICAL
INSTALL NEW MILLBANK (SINGLE METER) AT STA. 18+93, APPROXIMATELY 65' LT.

FIBER OPTIC CABLE
FIBER OPTIC CABLE CONDUIT IS TO BE 2" ORANGE HDPE PLUS #14 TRACER
INSTALL NEW CONDUIT 48" BELOW FINAL GRADE.
EXISTING SYSTEM TO REMAIN OPERATIONAL UNTIL NEW SYSTEM IS COMPLETE.
NEW CONDUIT MUST BE EXTENDED OUT FROM PULLBOXES #6 AND #10.

LDO	JT	LDO	JT	LDO	JT	LDO	JT	DSN	CHK
REVISED PER CITY COMMENTS									REV
REVISED PER CITY COMMENTS									DATE
REVISED PER CITY COMMENTS									DESCRIPTION
INITIAL ISSUE									REV
5-23-19									0
7-2-19									1
REVISED PER CITY COMMENTS									2
REVISED PER CITY COMMENTS									3
REVISED PER CITY COMMENTS									4
10-3-19									5
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STATE OF MISSOURI

LEON D. OSBOURN

ENGINEER

MO # 021726

2319 N. JACKSON | P.O. BOX 1304

JUNCTION CITY, KANSAS 66441

PH. (785) 762-5040 | FAX. (785) 762-7744

www.kawvalley.com | www.kawvalley.com

KAW VALLEY ENGINEERING

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. EXPIRES 12/31/19.

STREETS OF WEST PRYOR

NWQ NW PRYOR RD & NW LOWENSTEIN DR

LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING

TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD & NW LOWENSTEIN DR

PROJ. NO. A14-7067-1

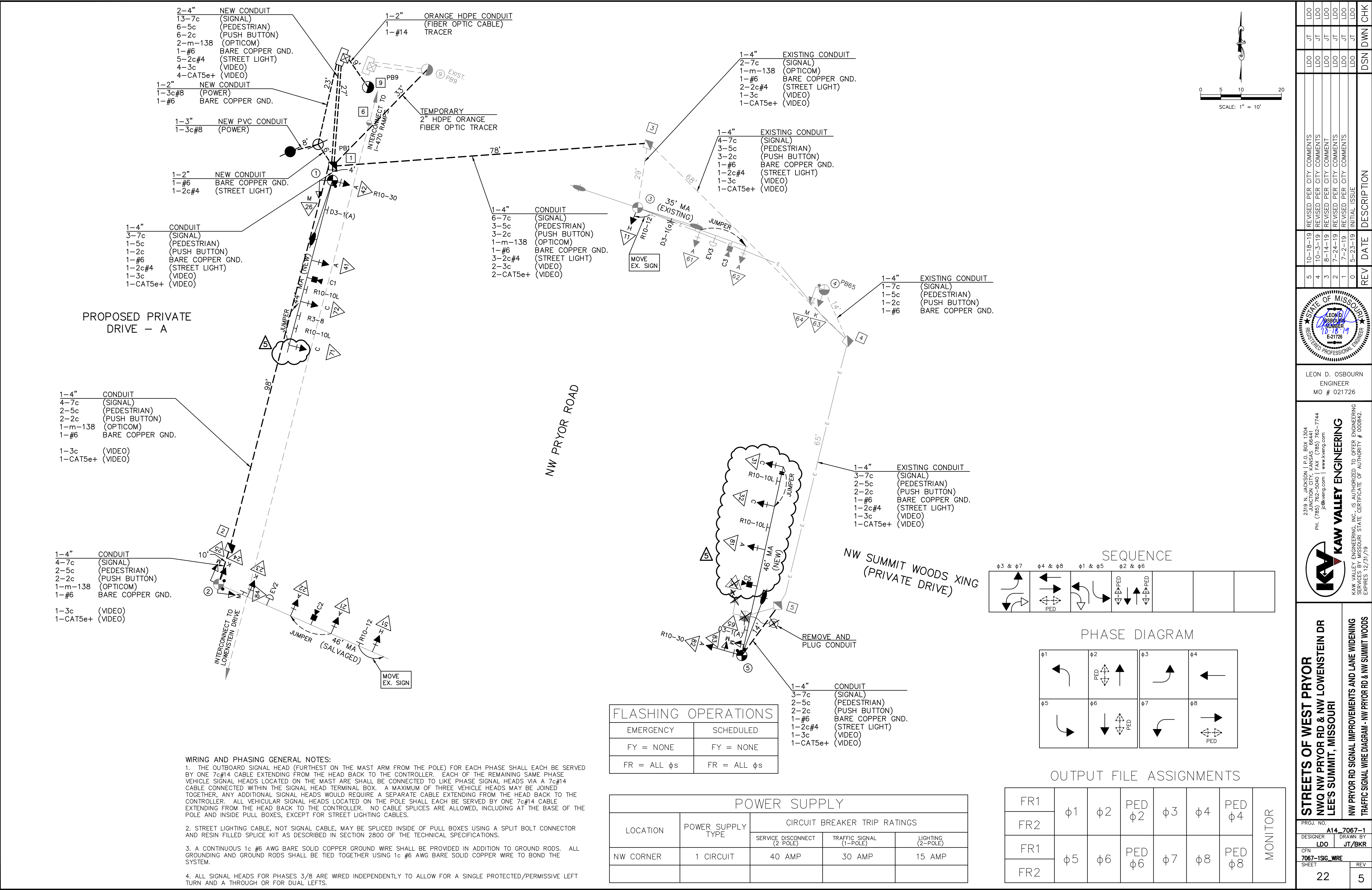
DESIGNER LDO

DRAWN BY JT/BKR

7067-1SIG_WIRE

19

4



REV	DATE	DESCRIPTION	DSN	CHK
5	10-18-19	REVISED PER CITY COMMENTS	LDO	LDO
4	10-3-19	REVISED PER CITY COMMENTS	LDO	LDO
3	8-14-19	REVISED PER CITY COMMENT	LDO	LDO
2	7-24-19	REVISED PER CITY COMMENTS	LDO	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO	LDO
0	5-23-19	INITIAL ISSUE	LDO	LDO



LEON D. OSBOURN
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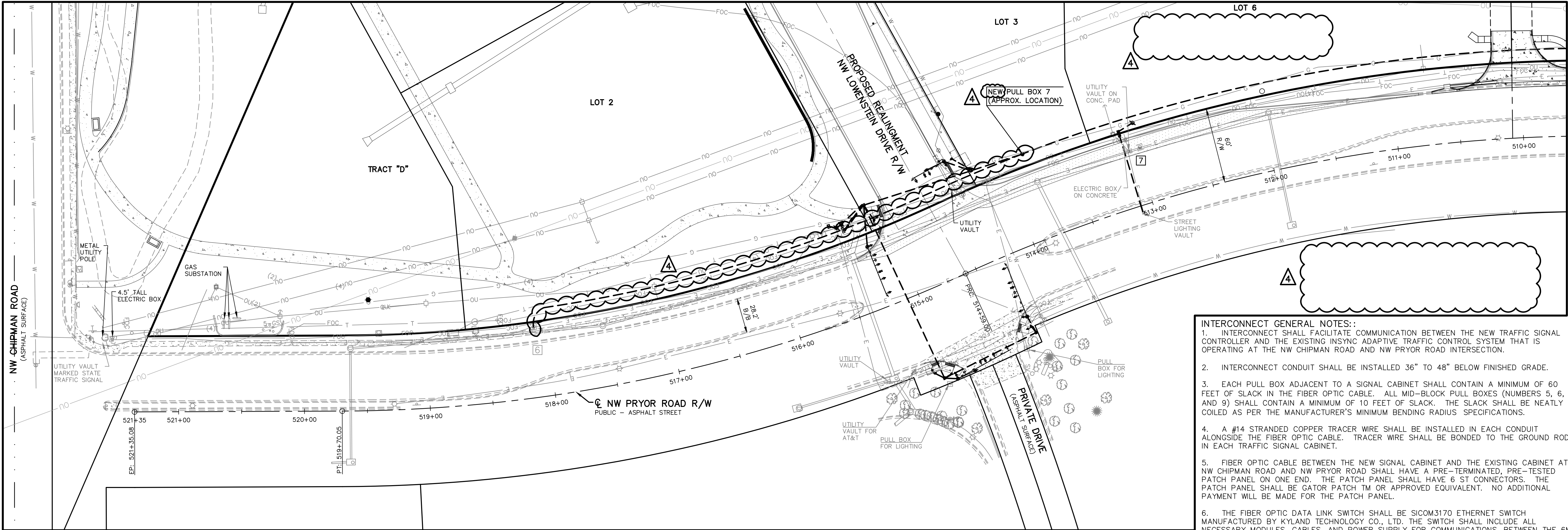
KAW VALLEY ENGINEERING

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842.
EXPIRES 12/31/19

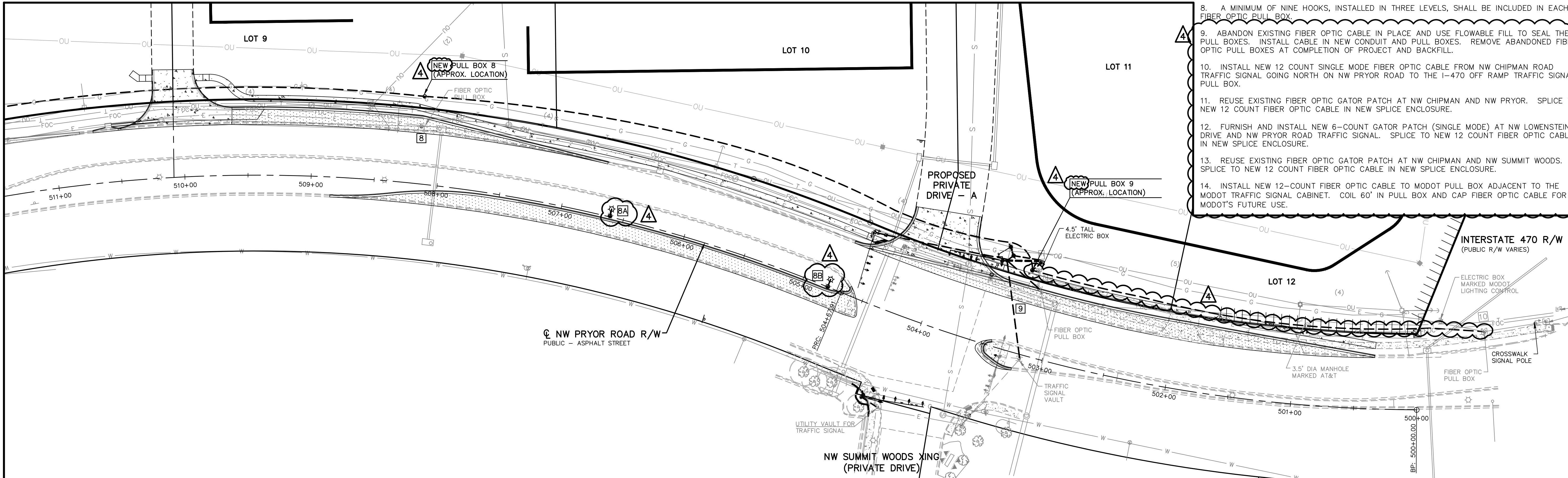
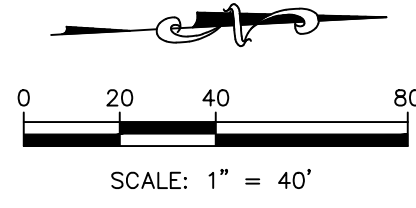
STREETS OF WEST PRYOR
NW & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

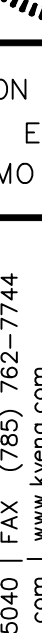
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD & NW SUMMIT WOODS

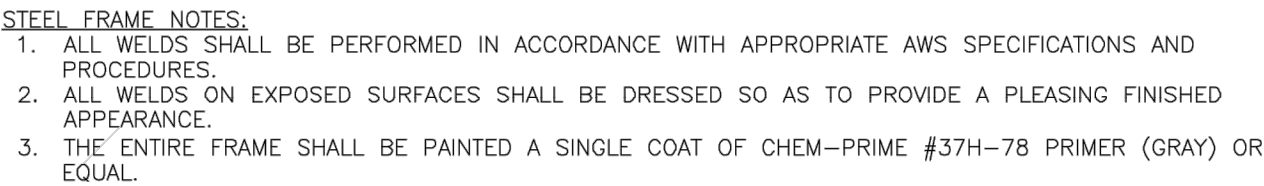
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DESIGNER	LDO	DRAWN BY JT/BKR
CFN	7067-1SIG_WIRE	
SHEET	22	REV 5



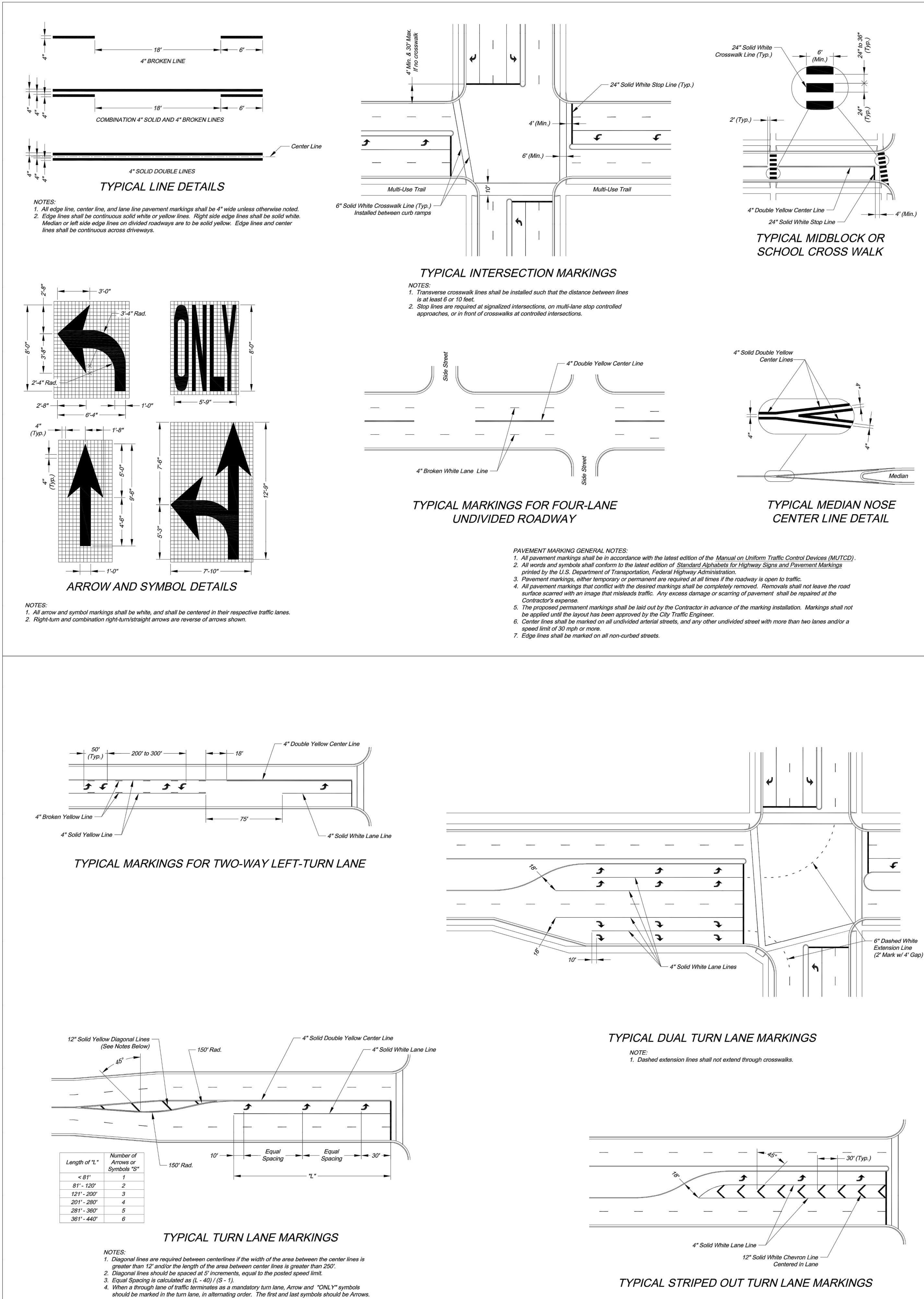
- INTERCONNECT GENERAL NOTES: :**
1. INTERCONNECT SHALL FACILITATE COMMUNICATION BETWEEN THE NEW TRAFFIC SIGNAL CONTROLLER AND THE EXISTING INSYNC ADAPTIVE TRAFFIC CONTROL SYSTEM THAT IS OPERATING AT THE NW CHIPMAN ROAD AND NW PRYOR ROAD INTERSECTION.
 2. INTERCONNECT CONDUIT SHALL BE INSTALLED 36" TO 48" BELOW FINISHED GRADE.
 3. EACH PULL BOX ADJACENT TO A SIGNAL CABINET SHALL CONTAIN A MINIMUM OF 60 FEET OF SLACK IN THE FIBER OPTIC CABLE. ALL MID-BLOCK PULL BOXES (NUMBERS 5, 6, 7, AND 9) SHALL CONTAIN A MINIMUM OF 10 FEET OF SLACK. THE SLACK SHALL BE NEATLY COILED AS PER THE MANUFACTURER'S MINIMUM BENDING RADIUS SPECIFICATIONS.
 4. A #14 STRANDED COPPER TRACER WIRE SHALL BE INSTALLED IN EACH CONDUIT ALONGSIDE THE FIBER OPTIC CABLE. TRACER WIRE SHALL BE BONDED TO THE GROUND ROD IN EACH TRAFFIC SIGNAL CABINET.
 5. FIBER OPTIC CABLE BETWEEN THE NEW SIGNAL CABINET AND THE EXISTING CABINET AT NW CHIPMAN ROAD AND NW PRYOR ROAD SHALL HAVE A PRE-TERMINATED, PRE-TESTED PATCH PANEL ON ONE END. THE PATCH PANEL SHALL HAVE 6 ST CONNECTORS. THE PATCH PANEL SHALL BE GATOR PATCH TM OR APPROVED EQUIVALENT. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE PATCH PANEL.
 6. THE FIBER OPTIC DATA LINK SWITCH SHALL BE SICOM3170 ETHERNET SWITCH MANUFACTURED BY KYLAND TECHNOLOGY CO., LTD. THE SWITCH SHALL INCLUDE ALL NECESSARY MODULES, CABLES, AND POWER SUPPLY FOR COMMUNICATIONS BETWEEN THE 6MM FIBER OPTIC CABLES, SIGNAL CONTROLLER, AND INSYNC PROCESSOR.
 7. FIBER OPTIC CABLE WILL NOT BE INSTALLED INTO THE EXISTING SIGNAL CABINET AT THE I-470 RAMPS. THE SPECIFIED LENGTH OF CABLE WILL BE ROUTED FROM THE NEW SIGNAL CABINET TO THE EXISTING PULL BOX AT STA. 499+44. THE CABLE WILL BE NEATLY COILED AND END CAPPED. MODOT WILL INSTALL THE FIBER INTO THE SIGNAL CABINET AT A LATER DATE.
 8. A MINIMUM OF NINE HOOKS, INSTALLED IN THREE LEVELS, SHALL BE INCLUDED IN EACH FIBER OPTIC PULL BOX.
 9. ABANDON EXISTING FIBER OPTIC CABLE IN PLACE AND USE FLOWABLE FILL TO SEAL THE PULL BOXES. INSTALL CABLE IN NEW CONDUIT AND PULL BOXES. REMOVE ABANDONED FIBER OPTIC PULL BOXES AT COMPLETION OF PROJECT AND BACKFILL.
 10. INSTALL NEW 12 COUNT SINGLE MODE FIBER OPTIC CABLE FROM NW CHIPMAN ROAD TRAFFIC SIGNAL GOING NORTH ON NW PRYOR ROAD TO THE I-470 OFF RAMP TRAFFIC SIGNAL PULL BOX.
 11. REUSE EXISTING FIBER OPTIC GATOR PATCH AT NW CHIPMAN AND NW PRYOR. SPLICE TO NEW 12 COUNT FIBER OPTIC CABLE IN NEW SPLICE ENCLOSURE.
 12. FURNISH AND INSTALL NEW 6-COUNT GATOR PATCH (SINGLE MODE) AT NW LOWENSTEIN DRIVE AND NW PRYOR ROAD TRAFFIC SIGNAL. SPLICE TO NEW 12 COUNT FIBER OPTIC CABLE IN NEW SPLICE ENCLOSURE.
 13. REUSE EXISTING FIBER OPTIC GATOR PATCH AT NW CHIPMAN AND NW SUMMIT WOODS. SPLICE TO NEW 12 COUNT FIBER OPTIC CABLE IN NEW SPLICE ENCLOSURE.
 14. INSTALL NEW 12-COUNT FIBER OPTIC CABLE TO MODOT PULL BOX ADJACENT TO THE MODOT TRAFFIC SIGNAL CABINET. COIL 60' IN PULL BOX AND CAP FIBER OPTIC CABLE FOR MODOT'S FUTURE USE.



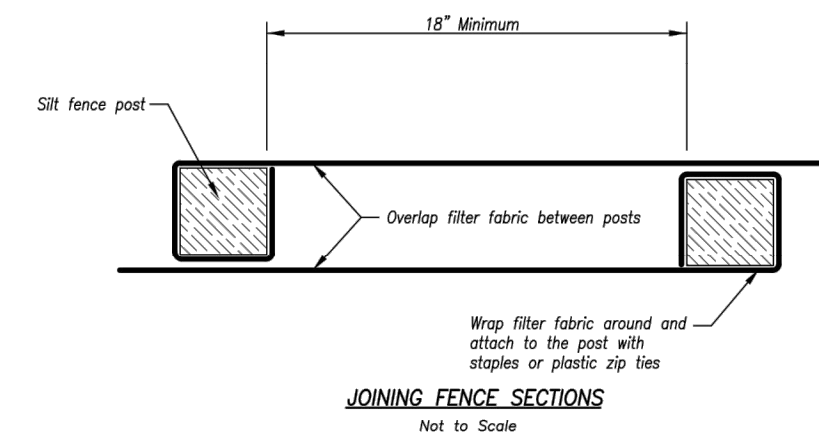
2319 N. JACKSON P.O. BOX 304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040 / 762-7744 joe@kve.com www.kve.com KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI EXPIRES 12/31/19		LEON D. OSBOURN ENGINEER MO # 021726				REV	DATE	DESCRIPTION	DSN	DWN	CHK
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		3	8-14-19	REVISED PER CITY COMMENT	LDO	JT	LDO				
		2	7-24-19	REVISED PER CITY COMMENTS	LDO	JT	LDO				
		1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO				
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO						
PROJ. NO. A14-7067-1											
DESIGNER		LDO	DRAWN BY		JT/BKR						
CFN		7067-1SIC_CONN									
SHEET		24					REV		4		



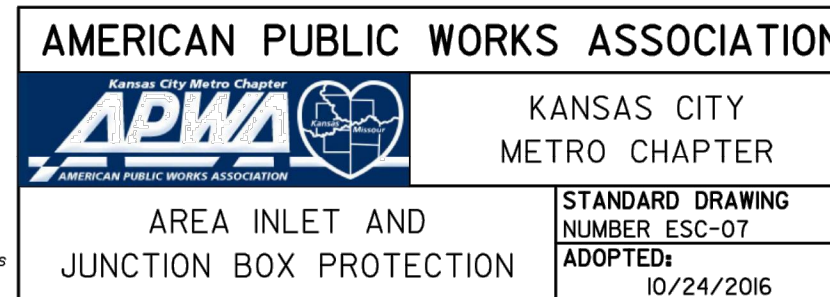
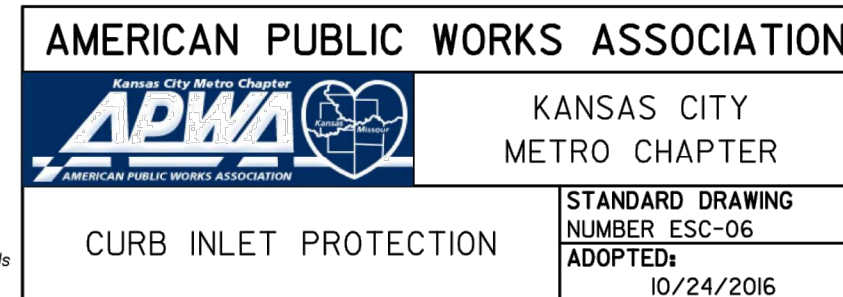
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CITY OF LEE'S SUMMIT PUBLIC ENGINEERING DIVISION 220 SE GREEN STREET LEE'S SUMMIT, MISSOURI 64441 PHONE: (816) 892-1800 FAX: (816) 892-1809		CITY OF LEE'S SUMMIT PUBLIC ENGINEERING DIVISION 220 SE GREEN STREET LEE'S SUMMIT, MISSOURI 64441 PHONE: (816) 892-1800 FAX: (816) 892-1809	
ROADWAY MARKING DETAILS Sheet Name: ROADWAY MARKING DETAILS Project: STANDARD DRAWING PM-1		INTERSECTION MARKING DETAILS Sheet Name: INTERSECTION MARKING DETAILS Project: STANDARD DRAWING PM-2	
Project: ROADWAY MARKING DETAILS Sheet Name: ROADWAY MARKING DETAILS Project: STANDARD DRAWING PM-1		Project: INTERSECTION MARKING DETAILS Sheet Name: INTERSECTION MARKING DETAILS Project: STANDARD DRAWING PM-2	
Drawn By: AS Checked By: JW Date: 09/09/2009 Project: 1 OF 2 1		Drawn By: AS Checked By: JW Date: 09/09/2009 Project: 2 OF 2 2	
2310 N. JACKSON P.O. BOX 304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040 FAX (785) 762-7744 joe@kveg.com www.kveg.com		2310 N. JACKSON P.O. BOX 304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040 FAX (785) 762-7744 joe@kveg.com www.kveg.com	
KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING DESIGN SERVICES IN THE STATE OF MISSOURI EXPIRES 12/31/19		KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING DESIGN SERVICES IN THE STATE OF MISSOURI EXPIRES 12/31/19	
STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI	
PROJ. NO. A14-7067-1 DESIGNER LDO CFN 7067-1SIG_DET SHEET 26		PROJ. NO. A14-7067-1 DESIGNER LDO CFN 7067-1SIG_DET SHEET 26	
DRAWN BY JT/BKR REV 0		DRAWN BY JT/BKR REV 0	
INITIAL ISSUE		INITIAL ISSUE	
DATE 5-23-19		DATE 5-23-19	
DESCRIPTION		DESCRIPTION	
LDO		LDO	
DWN		DWN	
CHK		CHK	



AMERICAN PUBLIC WORKS ASSOCIATION	
Kansas City Metro Chapter 	KANSAS CITY METRO CHAPTER
SILT FENCE	STANDARD DRAWING NUMBER ESC-03 ADOPTED: 10/24/2016

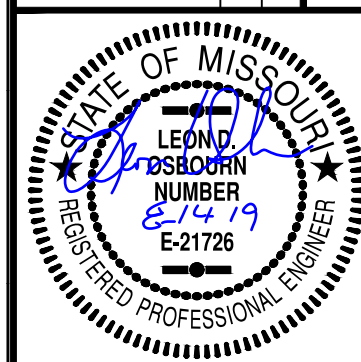


**STREETS OF WEST PRYOR
NW NW NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
EROSION CONTROL DETAIL SHEET**

KAW VALLEY ENGINEERING

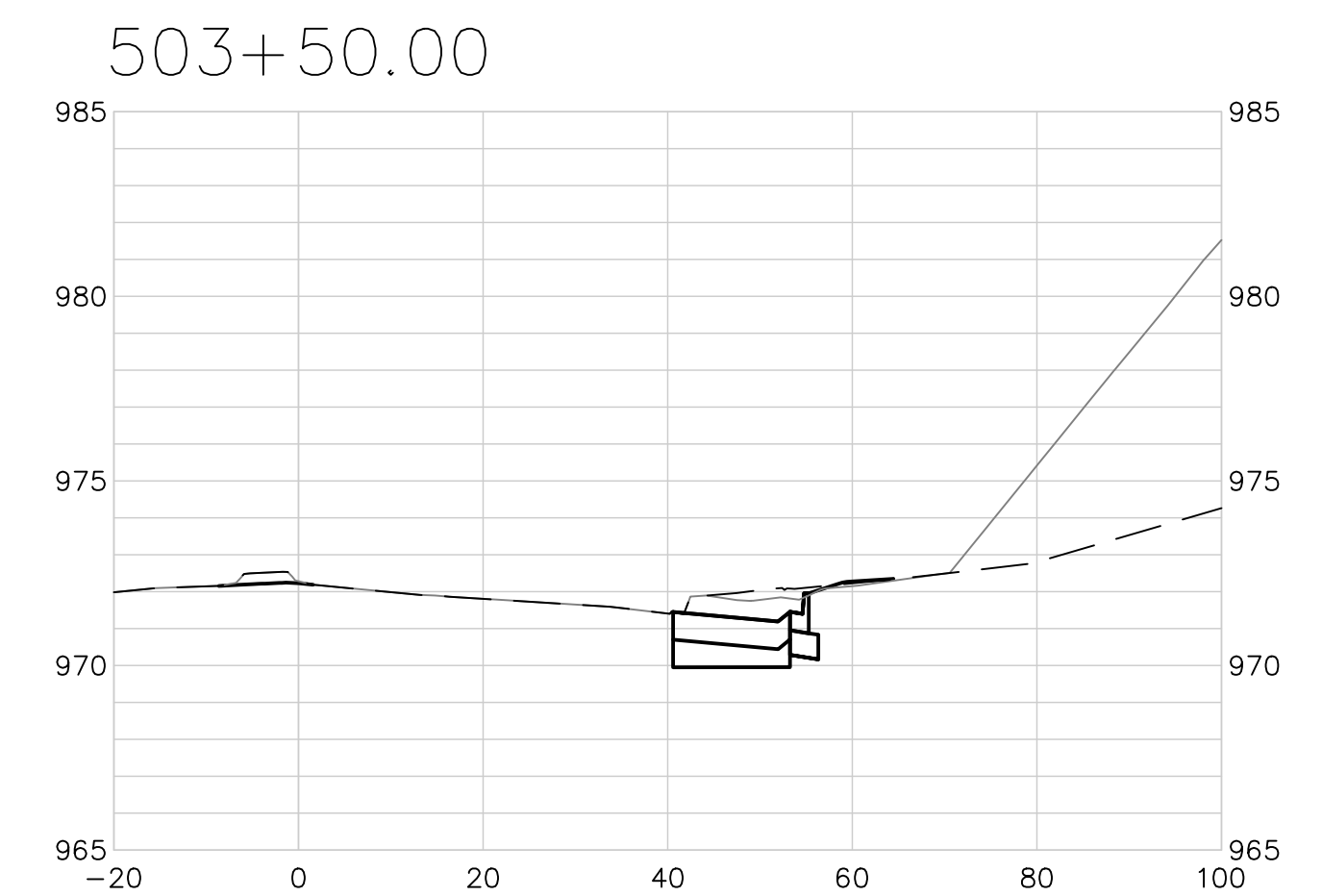
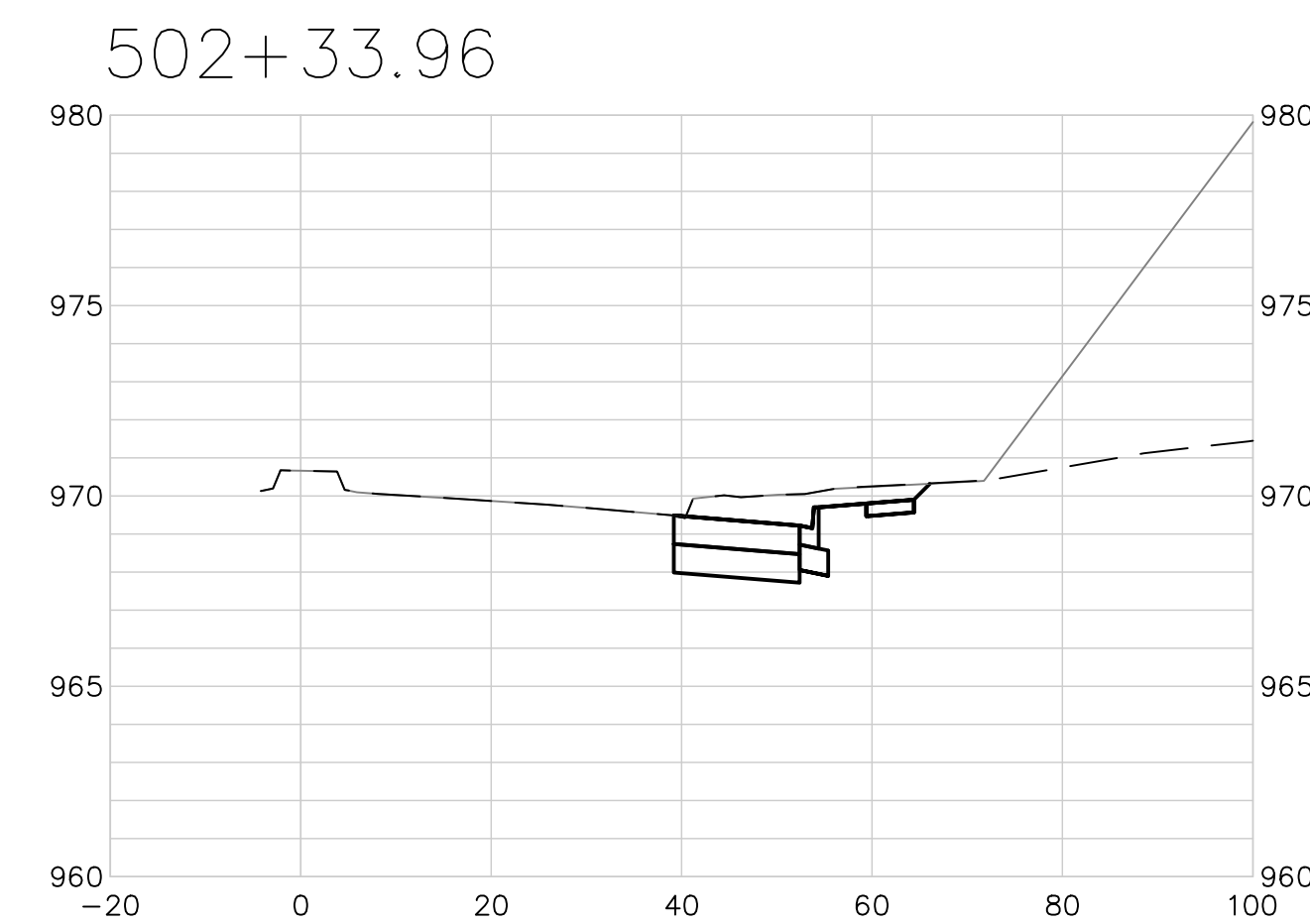
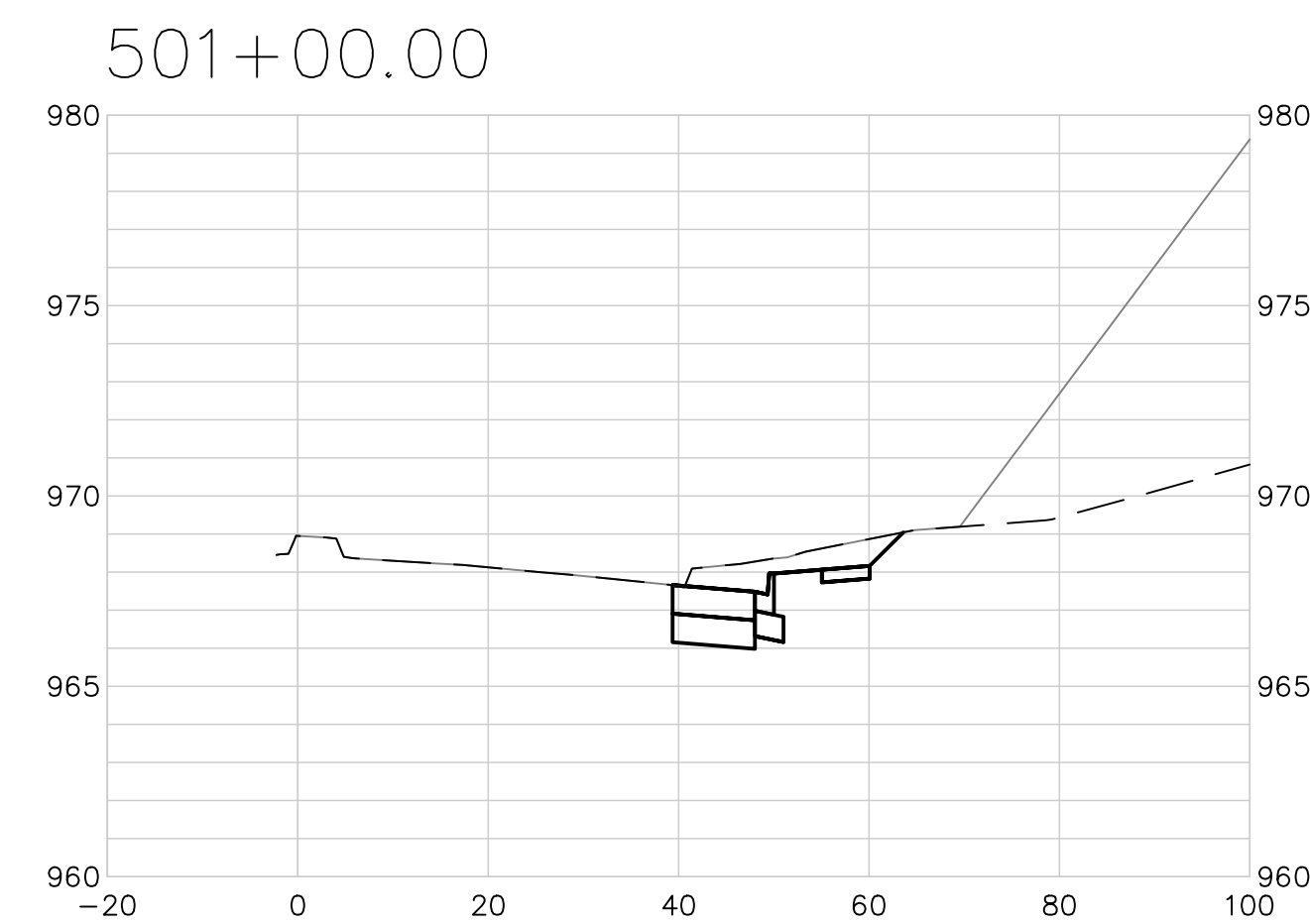
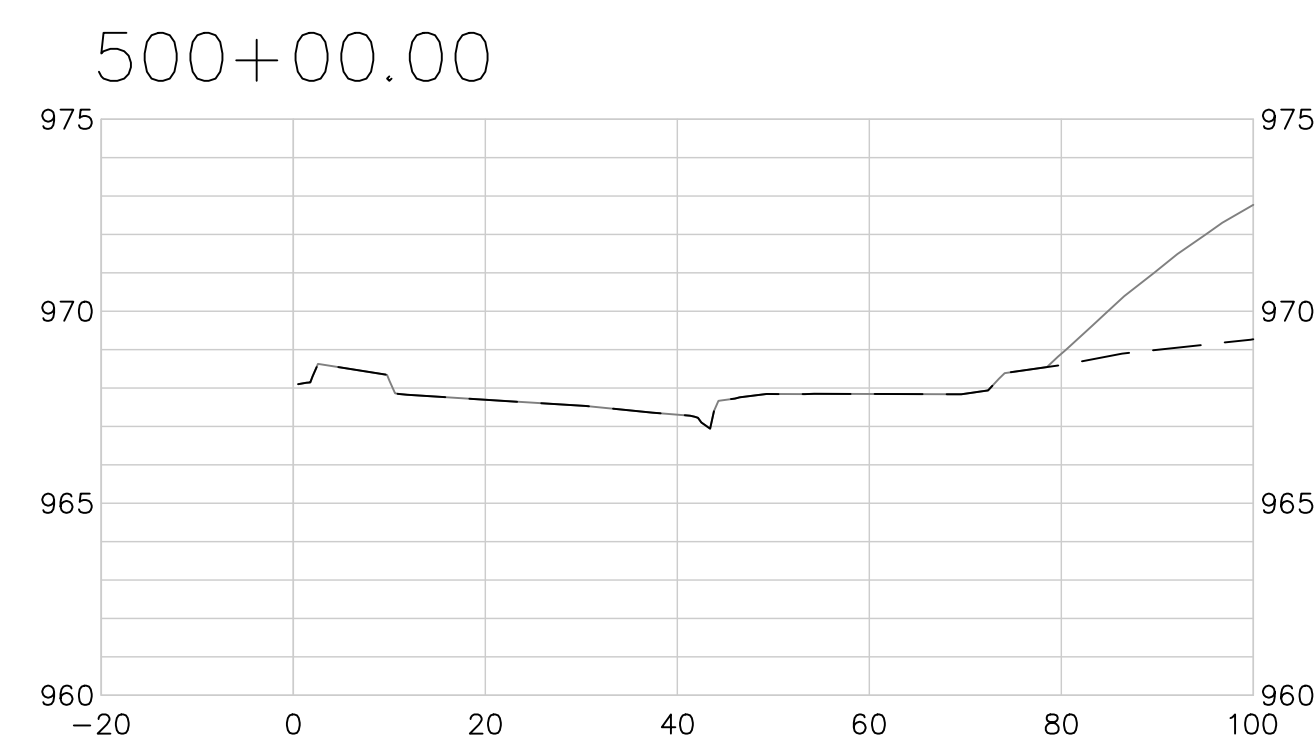
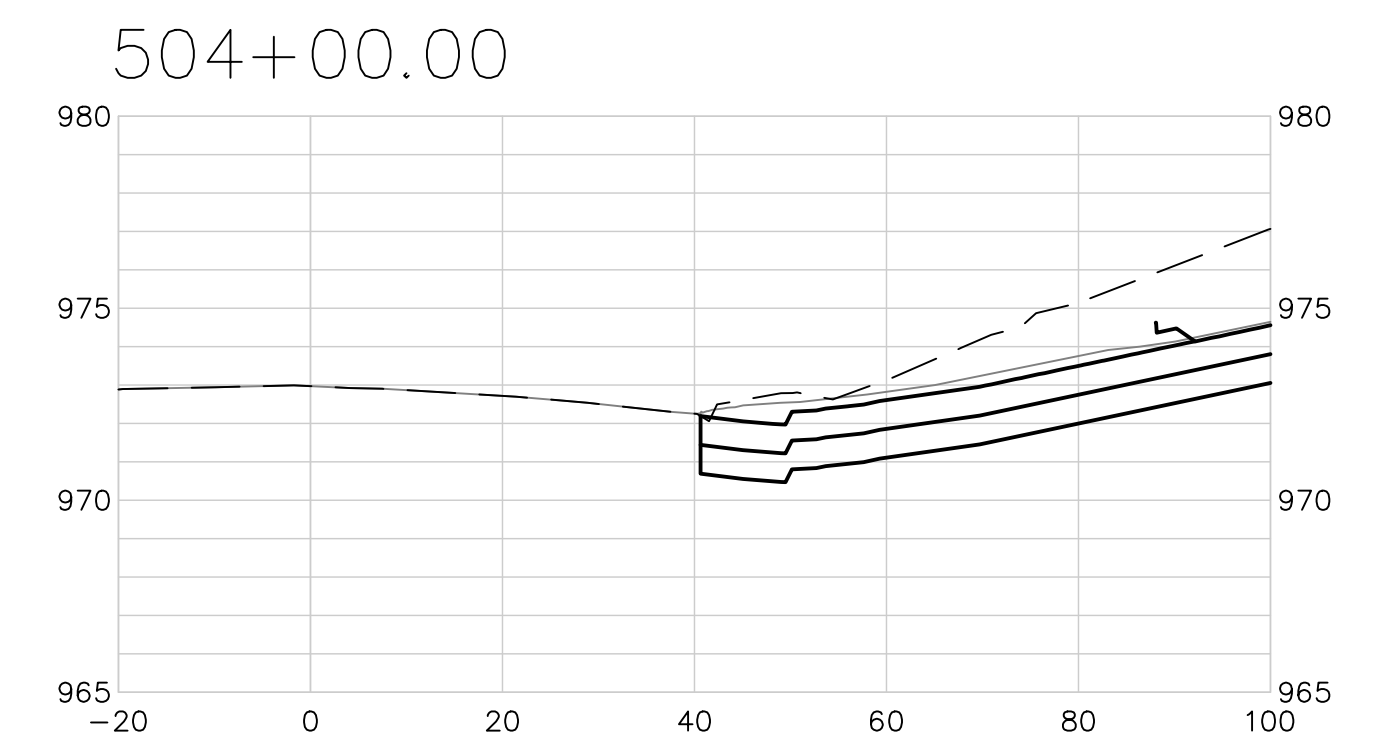
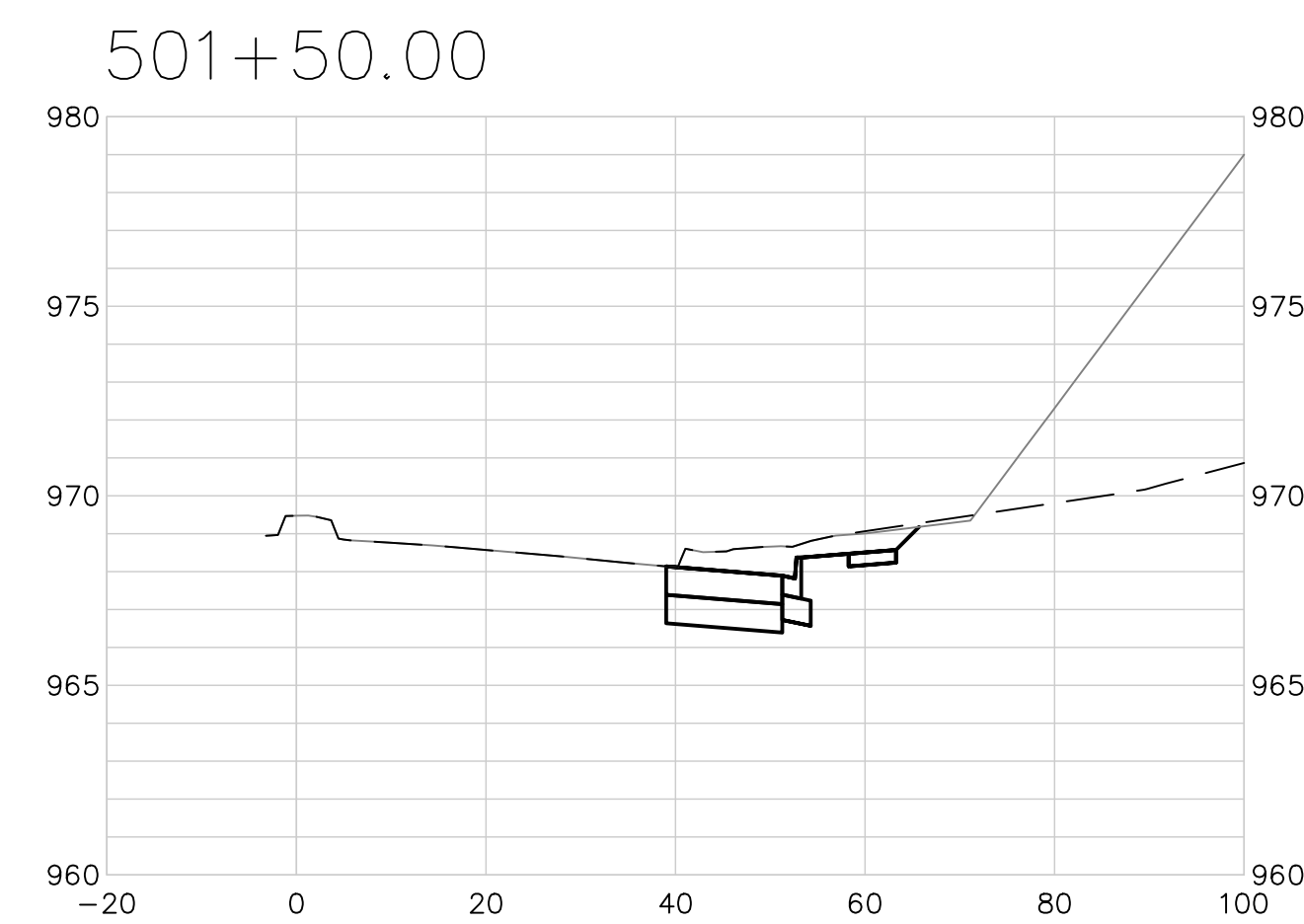
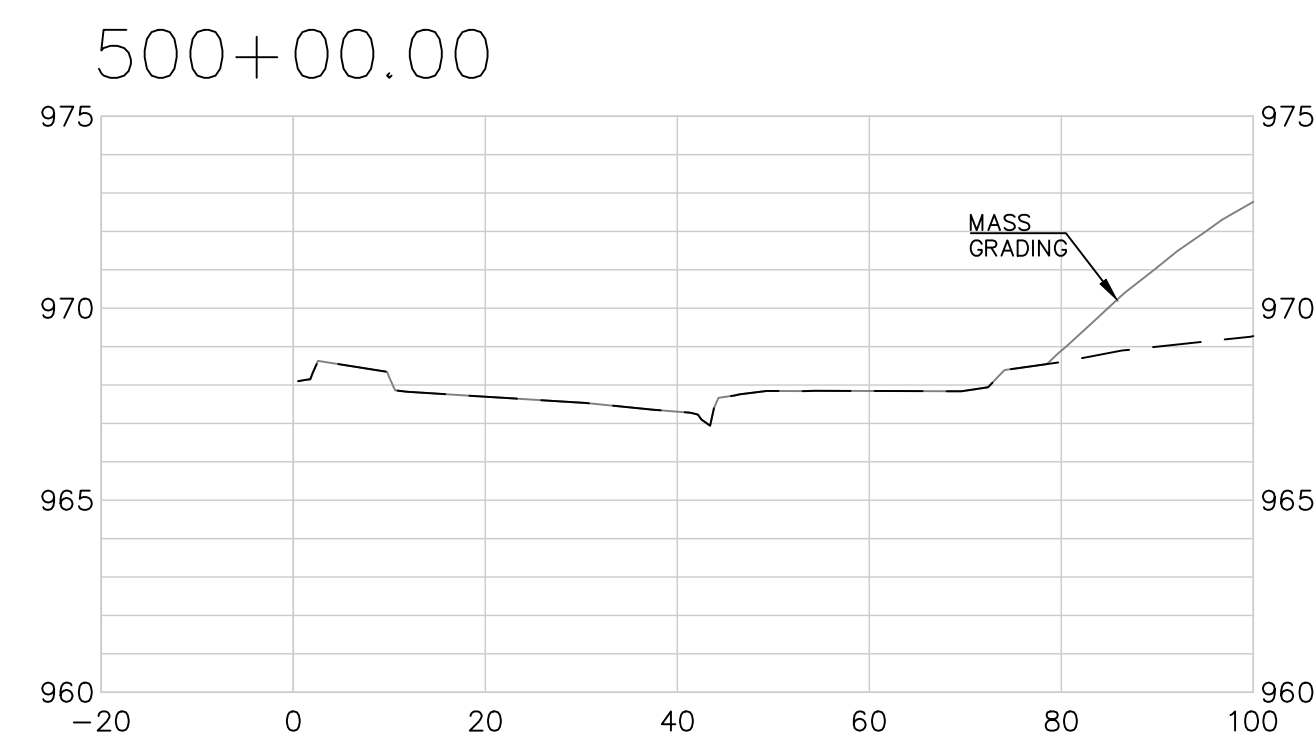
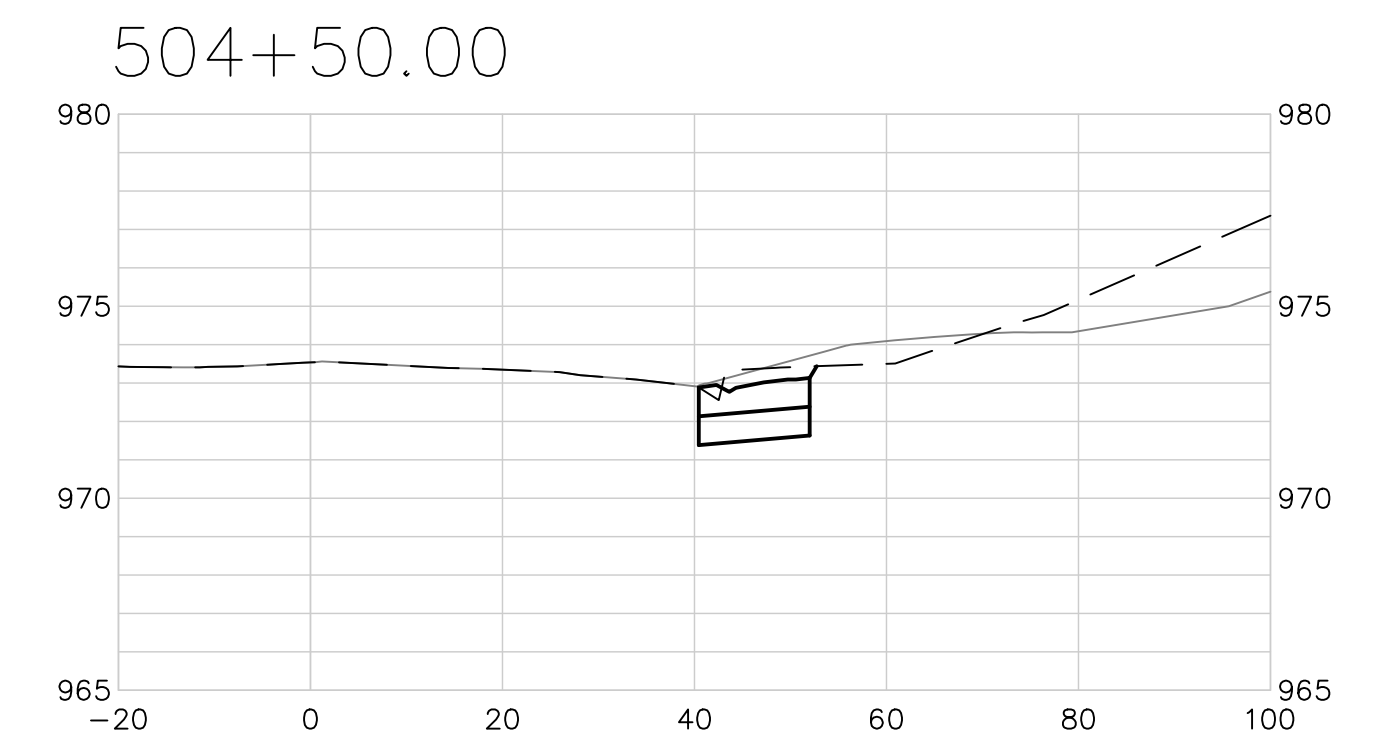
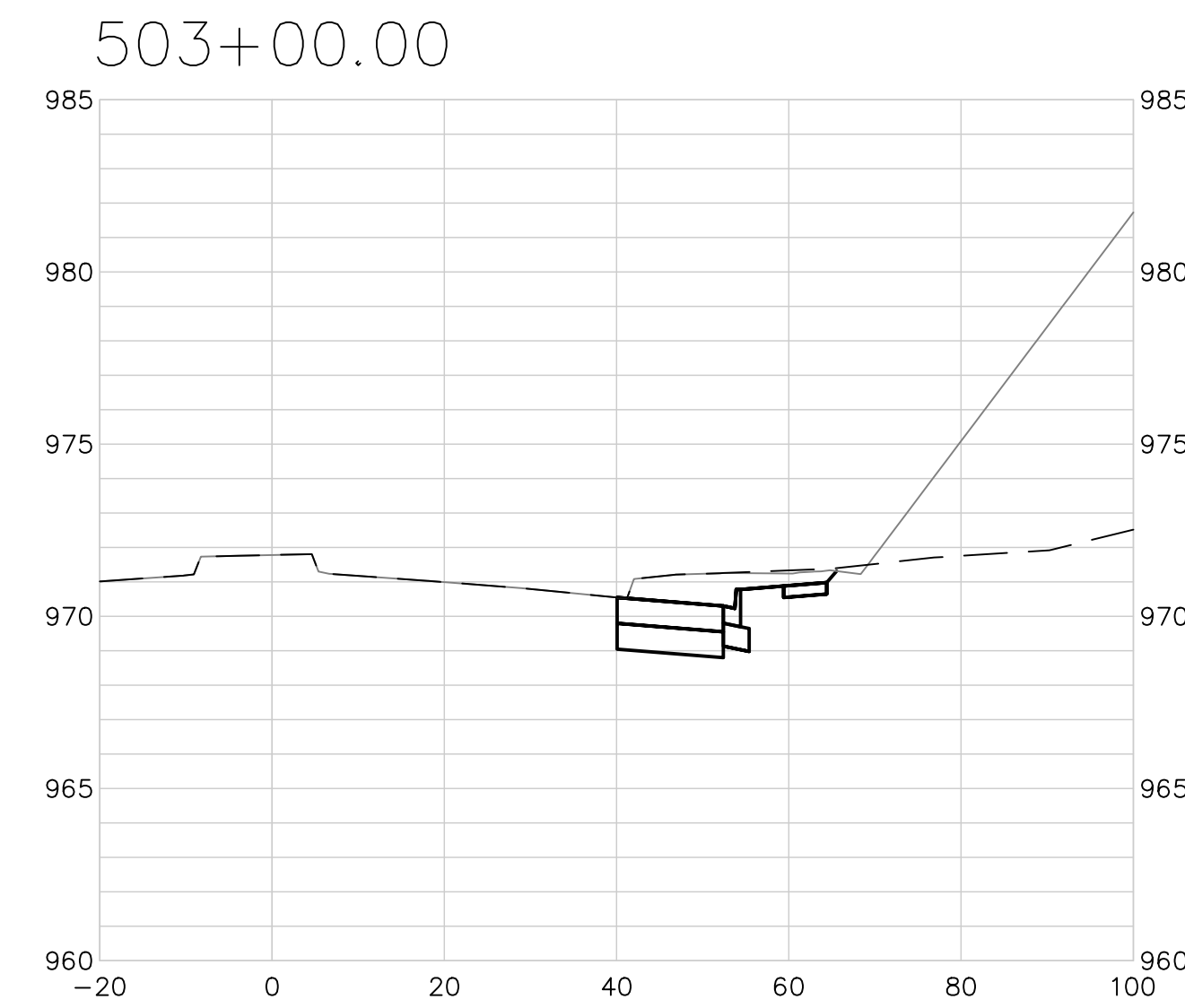
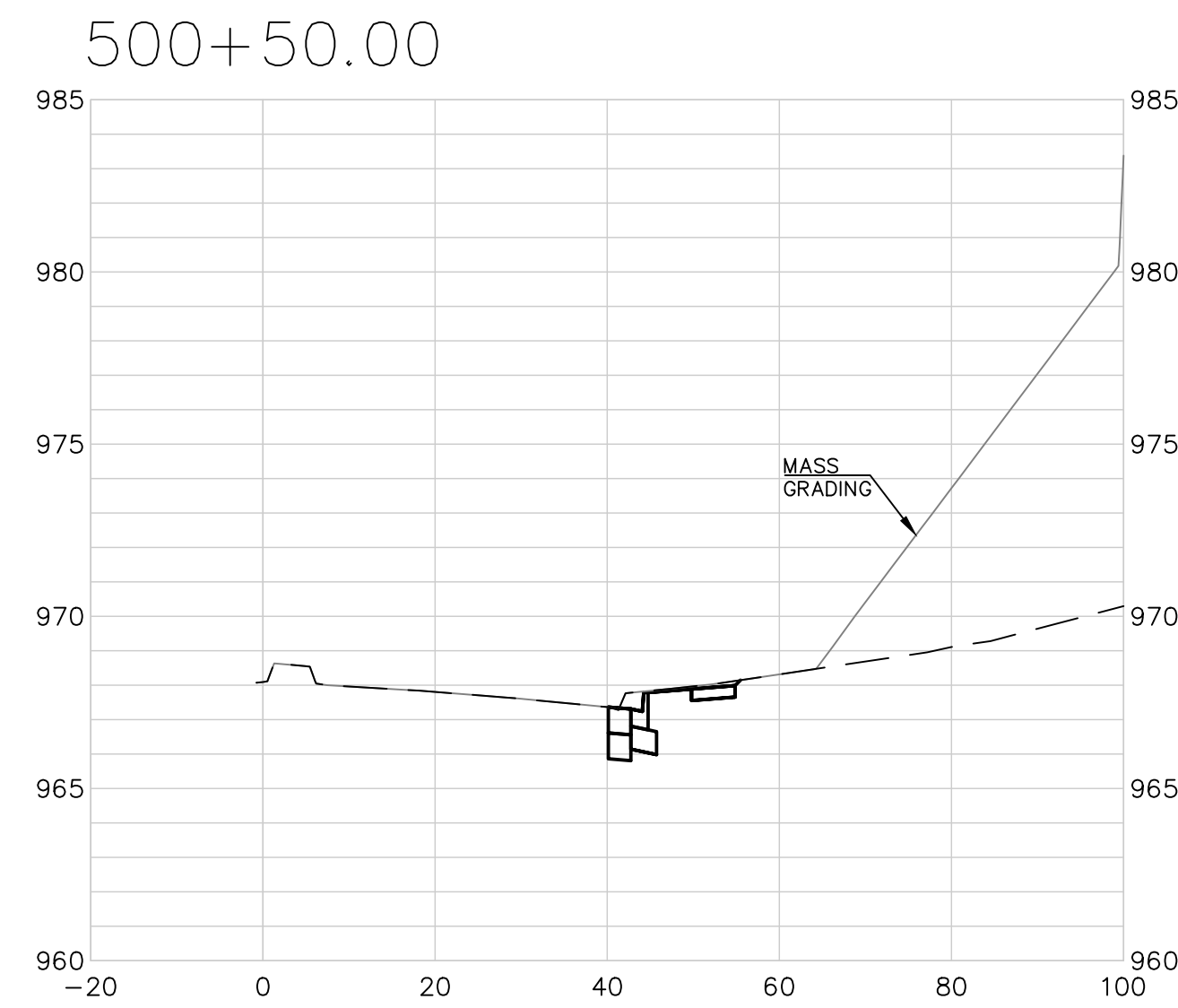
2319 N. JACKSON | P.O. BOX 1304
JUNCTION CITY, KANSAS 66441
PH. (785) 762-5040 | FAX. (785) 762-7744
jke@kve.com | www.kve.com

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES UNDER THE MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 8/31/2011.

[illegible]

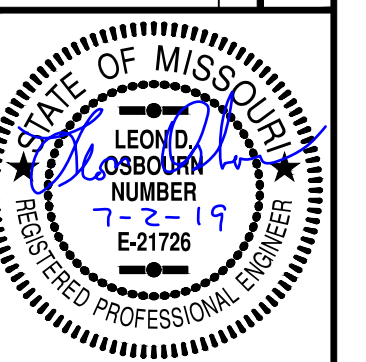
LEON D. OSBOURN
ENGINEER
MO # 021726

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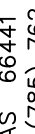


SCALE:
1"=20' HORIZ.
1"=5' VERT.

REV	DATE	DESCRIPTION	DSN	DWN	CHK
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO



LEON D. OSBOURN
ENGINEER
MO # 021726

 **KAW VALLEY ENGINEERING**

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842.

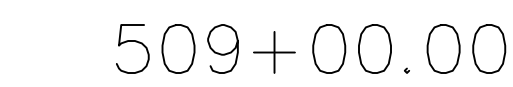
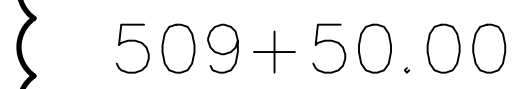
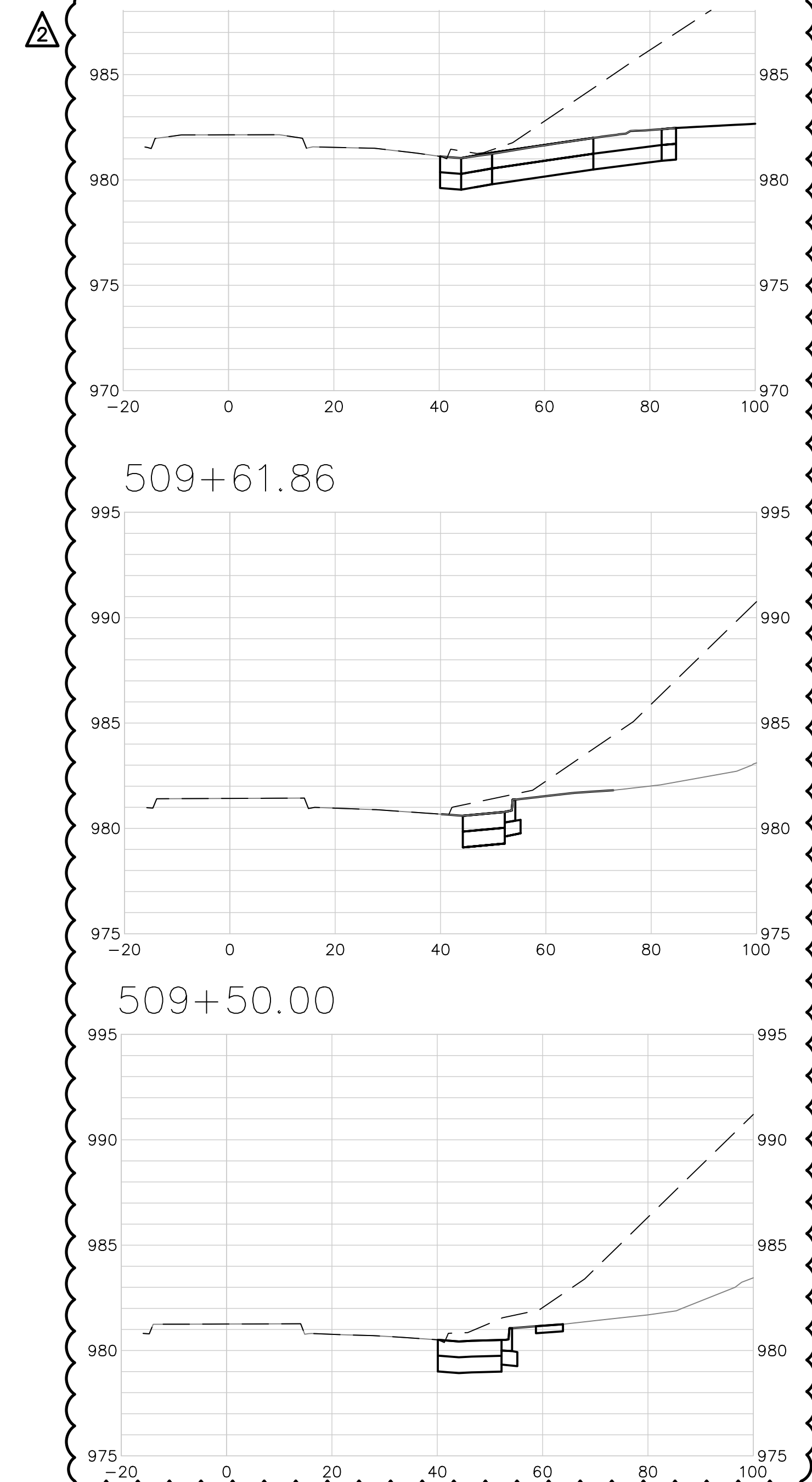
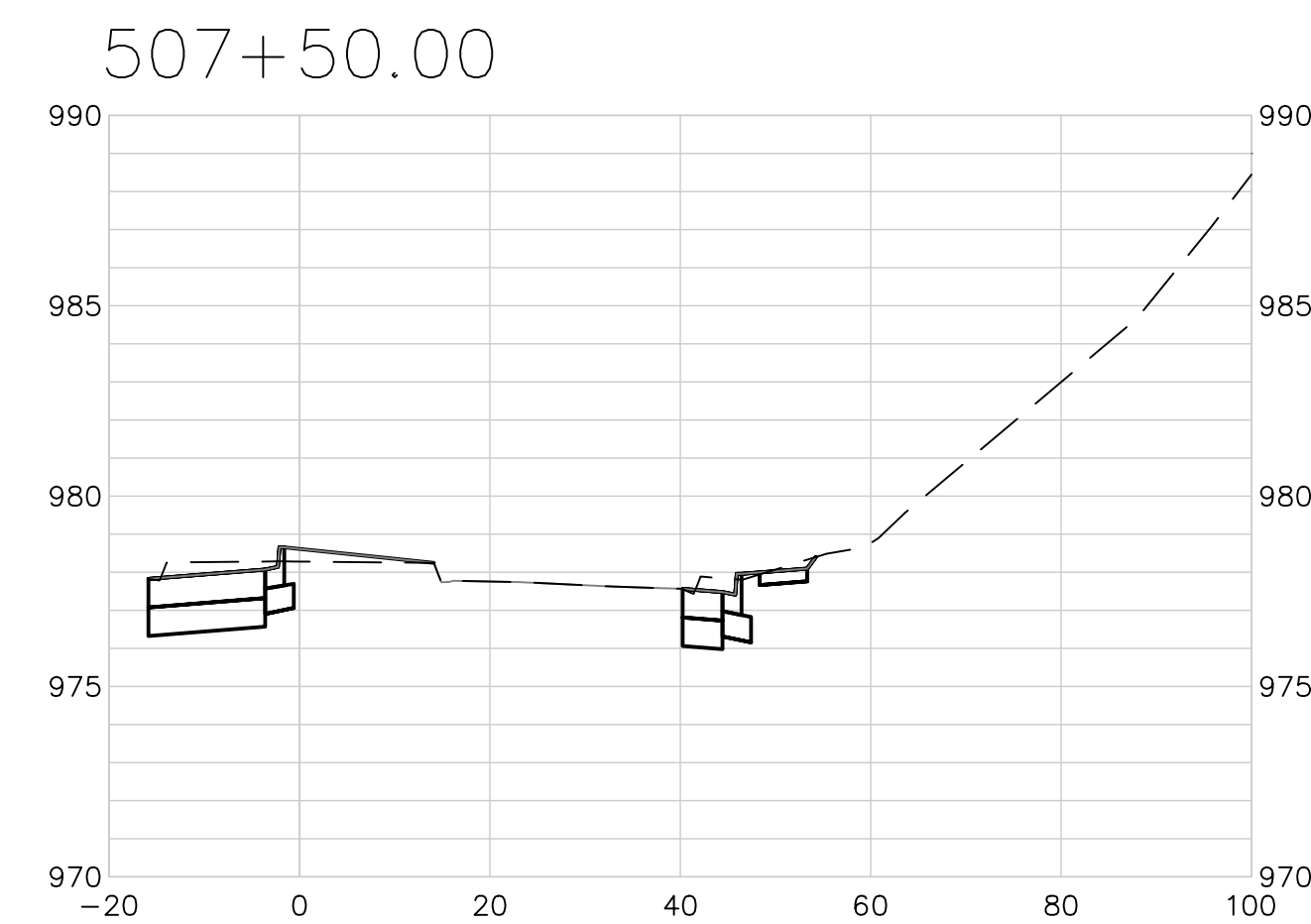
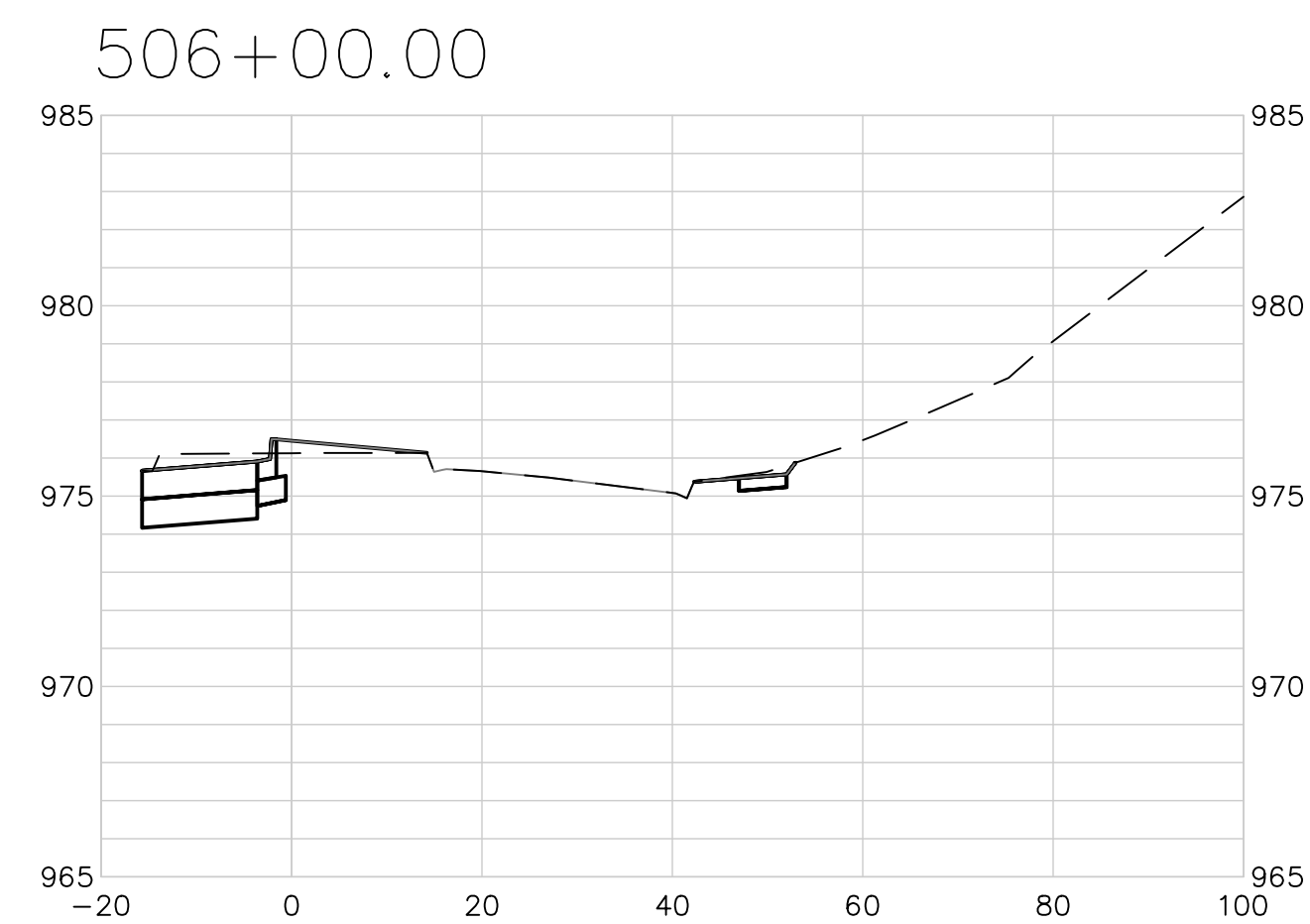
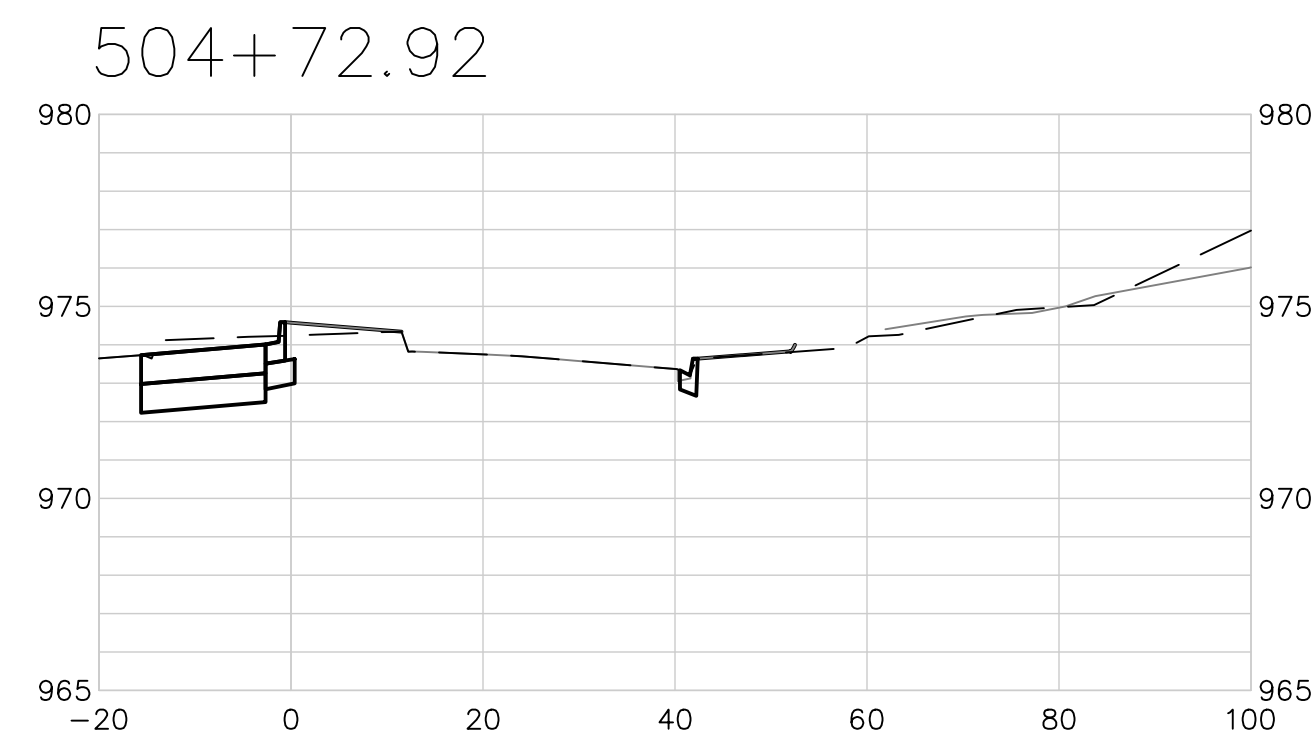
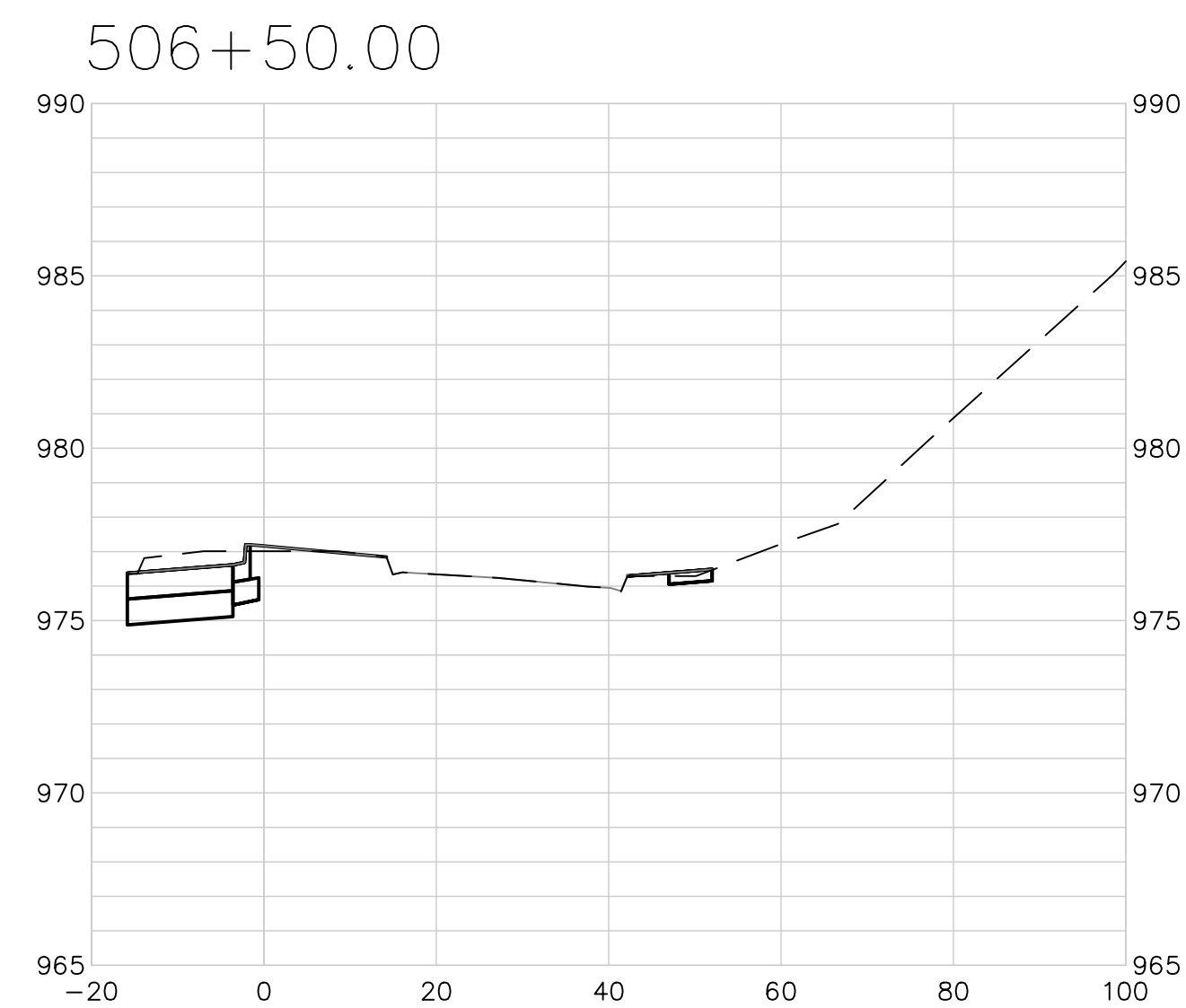
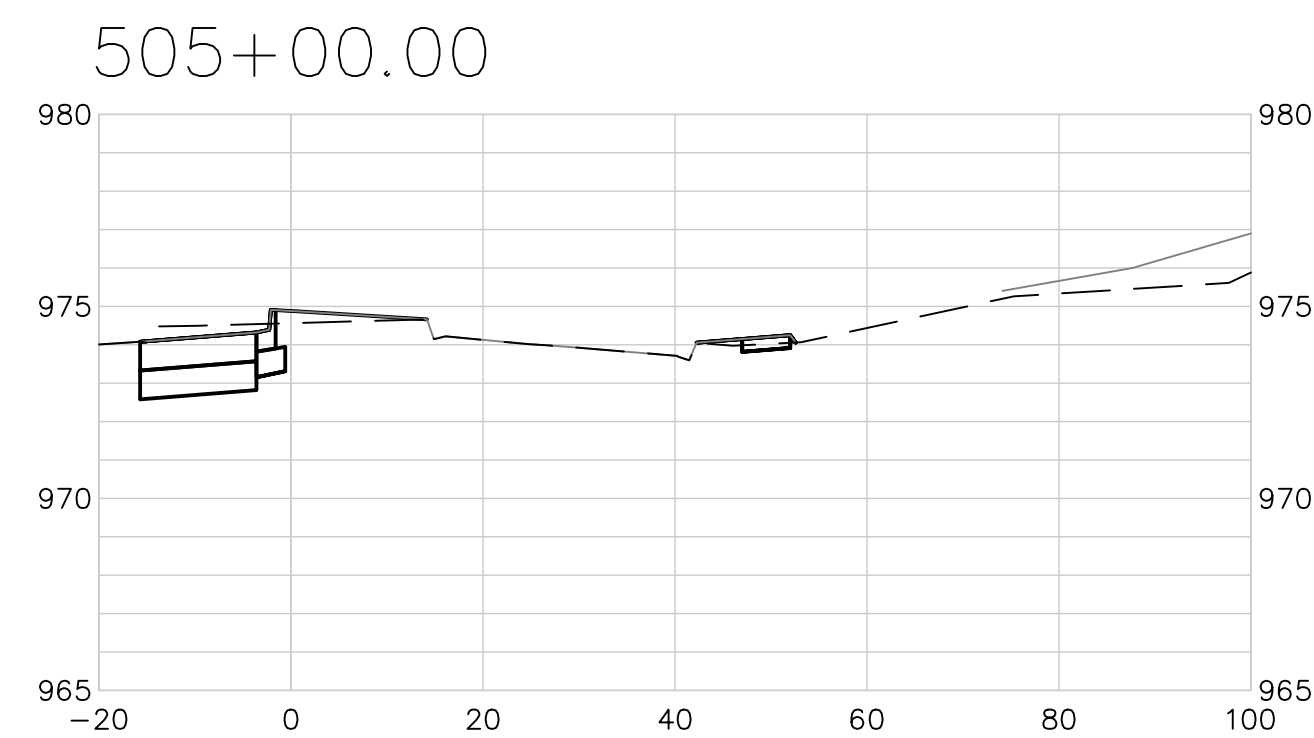
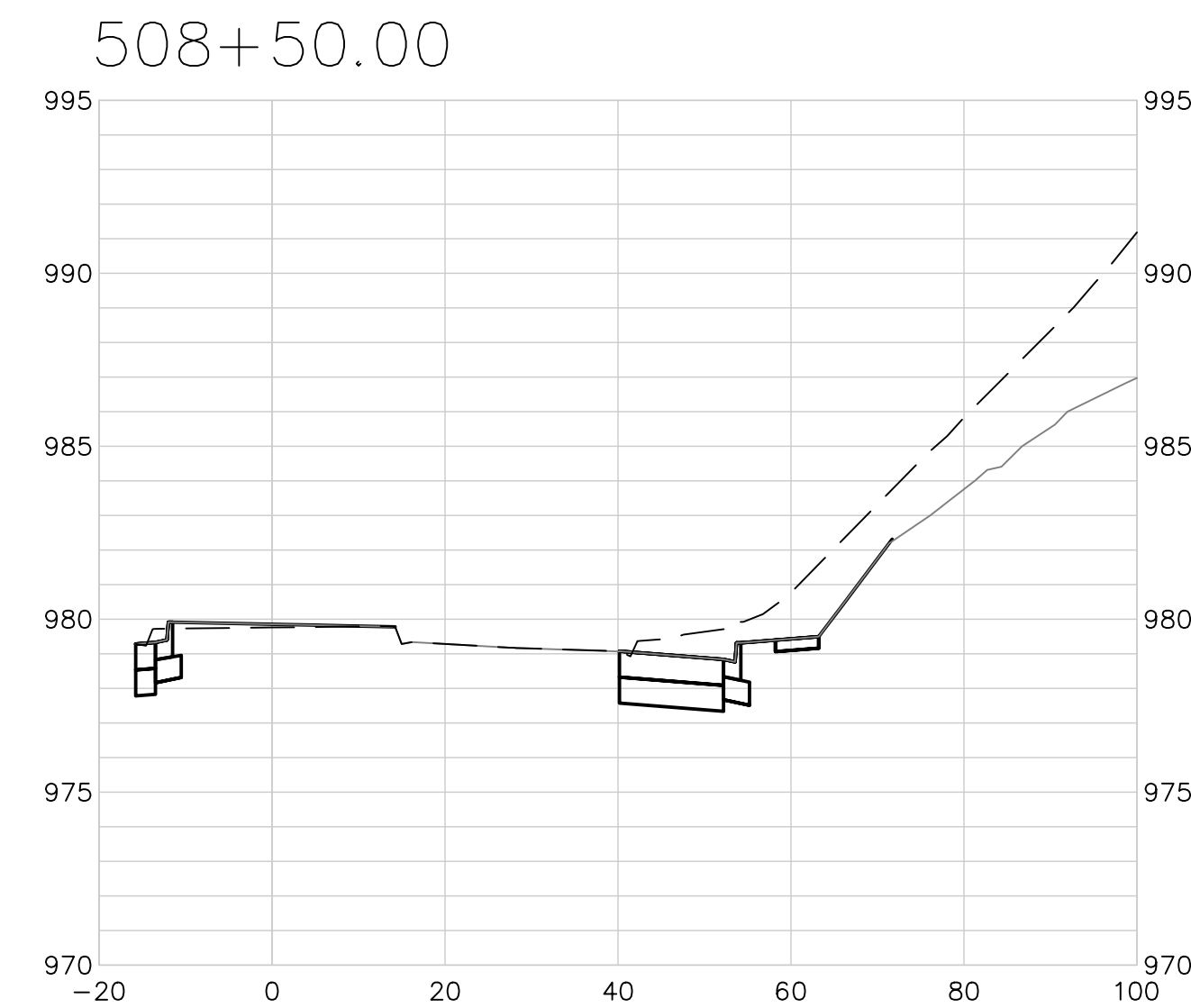
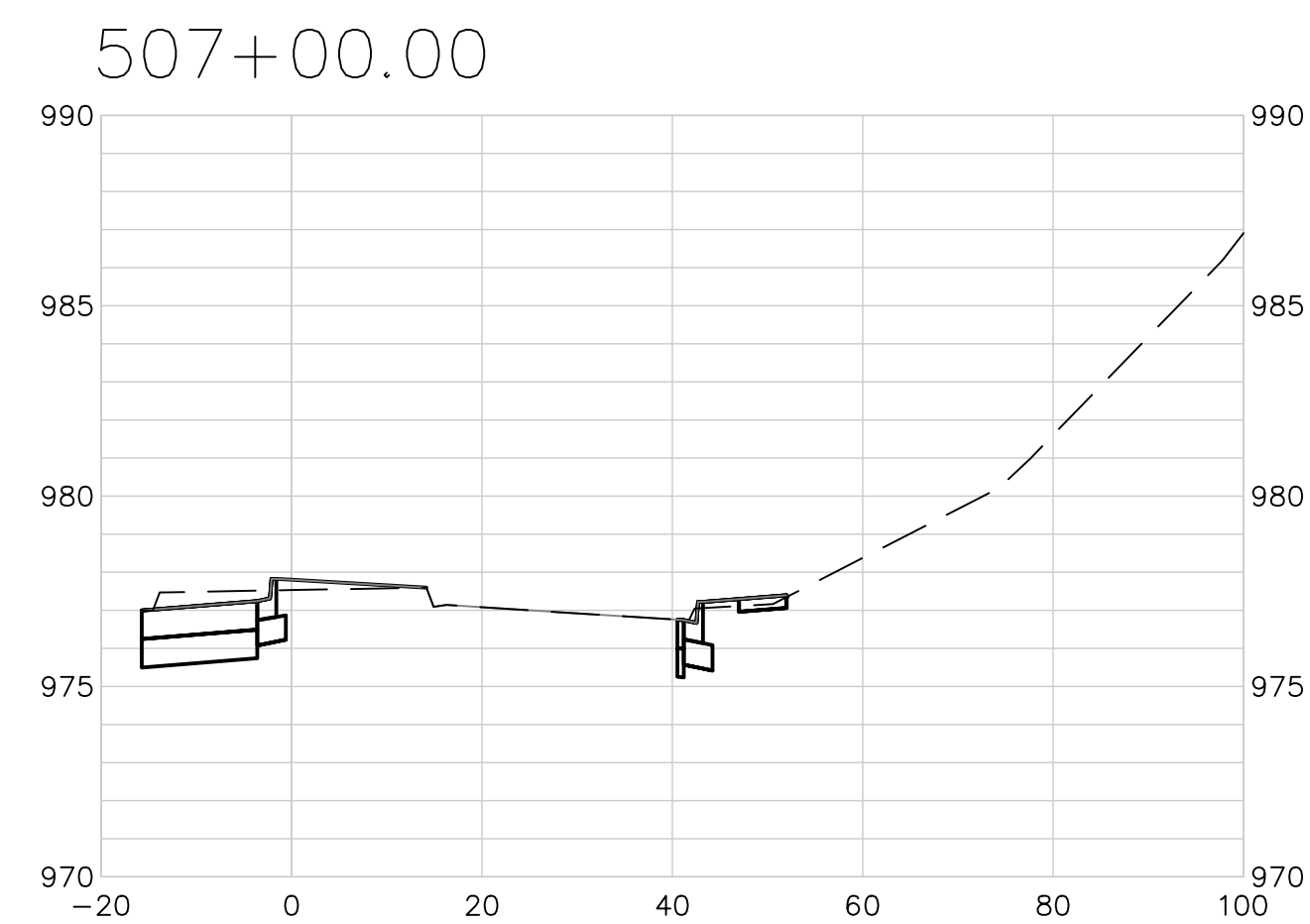
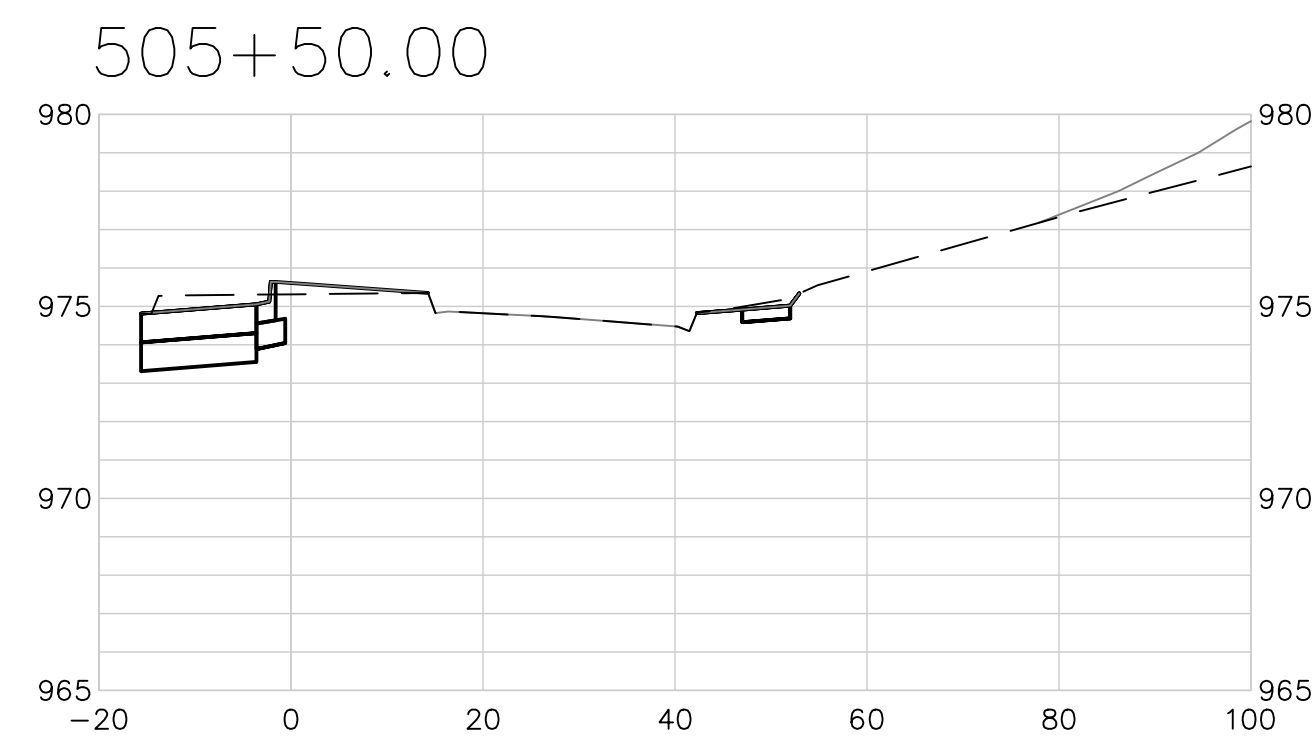
EXPIRES 12/31/19

2319 N. JACKSON | P.O. BOX 1304
JUNCTION CITY, KANSAS 66441
PH. (785) 762-5040 | FAX (785) 762-7744
p-kveeng.com | www.kveeng.com

STREETS OF WEST PRYOR
NWQW NW PRYOR RD & NW LOWENSTEIN DR
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING
NW PRYOR ROAD - CROSS-SECTIONS

PROJ. NO.	
A14-7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
067-1SIG_STPP	
SHEET	REV
36	0



SCALE:
1"=20' HORIZ.
1"=5' VERT.

STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		2319 N. JACKSON P.O. BOX 1304 JACKSON, MISSOURI 64301-0304 PH. (785) 762-5040 FAX (785) 762-7744 www.kvweng.com jack@kvweng.com							
PROJ. NO. A14-7067-1 DESIGNER LDO CFN 7067-1SIG_STTP		KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19		LEON D. OSBOURN ENGINEER MO # 021726		REVISED PER CITY COMMENTS REVISED PER CITY COMMENTS INITIAL ISSUE			
SHEET 37		DRAWN BY JT/BKR REV		2 10-3-19 1 7-2-19 0 5-23-19		REV DATE DESCRIPTION		LDO BKR LDO JT LDO LDO DSN DWN CHK	