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UTILITY CONTACTS:

LEE'S SUMMIT PUBLIC WORKS:
SPECTRUM:
EVERGY:
SPIRE:
CENTURYLINK:

(816) 969-1800
(888) 406-7063
(816) 220-5213
(816) 969-2200
(816) 320-4795

UTILITIES

THE INFORMATION CONCERNING LOCATIONS OF UNDERGROUND UTILITIES SHOWN HEREON WHICH ARE NOT VISIBLE FROM THE SURFACE, HAS BEEN TAKEN FROM THE RECORDS AND FIELD LOCATIONS OF THE VARIOUS UTILITY COMPANIES AND HAS NOT BEEN FIELD VERIFIED BY THIS COMPANY. THESE LOCATIONS ARE NOT TO BE CONSTRUED AS ACCURATE OR EXACT.

ABBREVIATION TABLE:

TC: TOP OF CURB
SW: SIDEWALK
PV: PAVEMENT
FV: FIELD VERIFY
ME: MATCH EXISTING
CI: CURB INLET
FI: FIELD INLET
JB: JUNCTION BOX
MH: MANHOLE
PC: POINT OF CURVATURE
PT: POINT OF TANGENCY
PI: POINT OF INTERSECTION
BP: BEGIN PAVEMENT
EP: END PAVEMENT

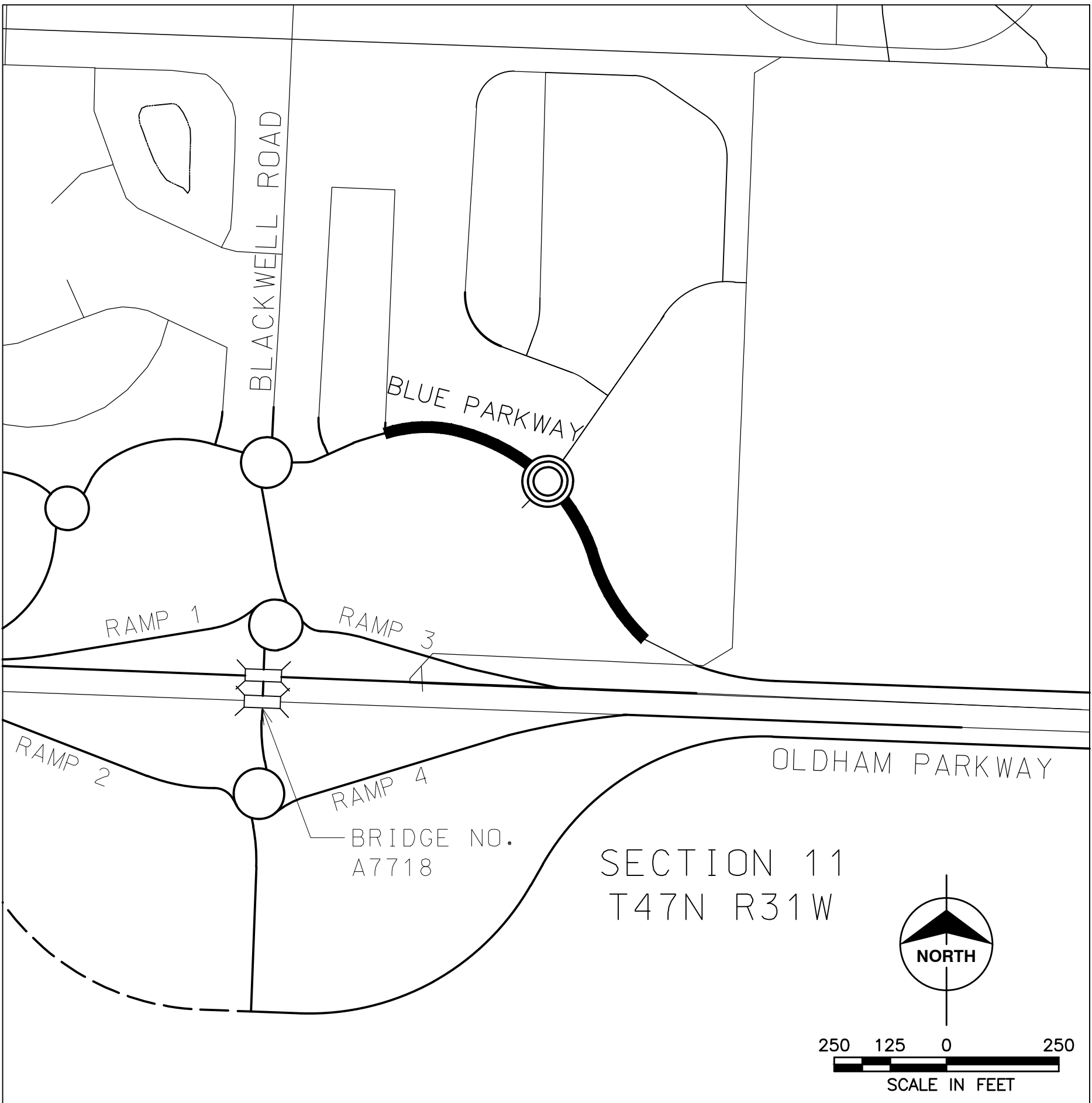
PROJECT INFORMATION:

LENGTH OF PROJECT:

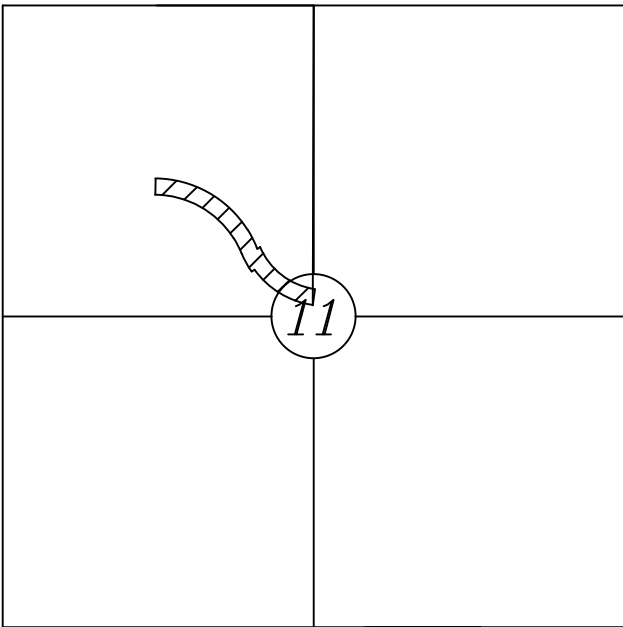
DESIGN DESIGNATION
POSTED SPEED: 40 MPH
CLASSIFICATION: COMMERCIAL COLLECTOR

LEGEND:

- NEW ASPHALT PAVEMENT
- NEW PORTLAND CEMENT CONCRETE SIDEWALK
- NEW PCC PAVEMENT
- NEW STAMPED PCC PAVEMENT
- CONCRETE SIDEWALK
- NEW TYPE CG-1 CURB & GUTTER
- NEW FLUSH CURB
- NEW CHAIN LINK FENCE
- PROPERTY LINE
- PROPOSED TOPO MAJOR
- PROPOSED TOPO MINOR
- EXISTING TOPO MAJOR
- EXISTING TOPO MINOR
- PROPOSED DRAINAGE AREAS
- EXISTING CABLE TV
- EXISTING ELECTRIC
- EXISTING WATER
- EXISTING STORM SEWER
- EXISTING POWER POLE
- EXISTING GAS METER
- EXISTING WATER METER



VICINITY MAP



S11 T47N R31W
SECTION MAP
N.T.S.

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32	INTERSECTION SIGHT DISTANCE

TOTAL NUMBER OF SHEETS = 32

ENGINEER'S CERTIFICATION:

I HEREBY CERTIFY THAT THIS PROJECT HAS BEEN DESIGNED AND THESE PLANS PREPARED IN ACCORDANCE WITH THE CURRENT DESIGN CRITERIA OF THE CITY OF LEE'S SUMMIT, MISSOURI AND THE STATE OF MISSOURI. I FURTHER CERTIFY THAT THESE PLANS WERE IN ACCORDANCE TO AASHTO STANDARDS.

☒ NOT FOR CONSTRUCTION
☐ FOR CONSTRUCTION



Certificates of Authority
Architecture: MO 318 / KS 13
Engineering: MO 4 / KS 241
Land Surveying: MO 123 / KS 36

Jadrienne S. Rodell-Tipton, PE
PE-2021032725 (MISSOURI #)

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PREPARED FOR:
GRIFFIN RILEY
PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

REVISIONS NO.	DATE / DESCRIPTION
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

PRELIMINARY

COVER

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GENERAL NOTES:

- THE CONSTRUCTION COVERED BY THESE PLANS SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI, IN CURRENT USAGE AND ALL SUPPLEMENTS THERE TO.
- REFER TO THE CURRENT VERSION OF THE KC METRO CHAPTER AMERICAN PUBLIC WORKS ASSOCIATION, STANDARD SPECIFICATIONS & INSTALLATION FOR THE GUIDING REFERENCE.
- DO NOT SCALE THESE DRAWINGS.
- NO GEOLOGICAL INVESTIGATION WAS PERFORMED ON THIS SITE.
- CONTRACTOR IS RESPONSIBLE FOR ALL PERMITS, BONDS, AND INSURANCE REQUIRED BY THE CITY.
- THE UTILITY LOCATIONS SHOWN ON THESE PLANS ARE TAKEN FROM UTILITY COMPANY RECORDS AND ARE APPROXIMATE ONLY. THEY DO NOT CONSTITUTE ACTUAL FIELD LOCATIONS. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION.
- CLEARING AND GRUBBING OPERATIONS AND DISPOSAL OF ALL DEBRIS THEREFROM SHALL BE PERFORMED BY THE CONTRACTOR IN STRICT ACCORDANCE WITH ALL STATE AND LOCAL CODES.
- THE DEVELOPER / OWNER SHALL CONTROL EROSION AND SILTATION DURING ALL PHASES OF CONSTRUCTION, AND SHALL KEEP THE STREETS CLEAN OF MUD AND DEBRIS.
- ALL EXCESS MATERIAL SHALL BE REMOVED LEGALLY FROM SITE AND DISPOSED OF OFF SITE.
- TRAFFIC CONTROL AND MAINTENANCE OF TRAFFIC DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE PUBLIC WORKS DEPARTMENT.
- EROSION CONTROL MEASURES SHALL BE PROVIDED AT ALL LOCATIONS WHERE DRAINAGE IS LEAVING THE PROJECT SITE. THE EROSION CONTROL PLAN SHOWS MINIMUM EROSION CONTROL MEASURES TO BE PROVIDED. ADDITIONAL SITE SPECIFIC MEASURES MAY BE NECESSARY AND SHALL BE PROVIDED BY THE DEVELOPER / OWNER, AT THE CONTRACTOR'S EXPENSE.
- ANY EXISTING OR NEW STORM SEWER INLETS IN USE DURING DEMOLITION, GRADING OR CONSTRUCTION SHALL HAVE INLET PROTECTION.
- THE CONTRACTOR SHALL FIELD VERIFY ALL SITE CONDITIONS AND SHALL REPORT ANY DISCREPANCIES BETWEEN ACTUAL AND PLAN SHOWN CONDITIONS TO THE ENGINEER PRIOR TO BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND QUANTITIES SHOWN ON THESE PLANS AND SHALL REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO COMMENCING ANY RELATED WORK.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR DETERMINING THE LOCATION OF ALL EXISTING UTILITIES AND UNDERGROUND INSTALLATIONS, INCLUDING SERVICE CONNECTIONS, IN ADVANCE OF EXCAVATION OR TRENCHING, AND PROTECT THE SAME AS REQUIRED TO MAINTAIN GOOD OPERATING CONDITION.
- THE CONTRACTOR SHALL USE HIS OWN INFORMATION AND NOT RELY UPON ANY INFORMATION SHOWN ON THE DRAWINGS CONCERNING EXISTING UNDERGROUND INSTALLATIONS.
- ANY DELAY, ADDITIONAL WORK, OR EXTRA COST TO THE CONTRACTOR CAUSED BY OR RESULTING FROM DAMAGE TO EXISTING UNDERGROUND INSTALLATIONS SHALL NOT CONSTITUTE A CLAIM FOR EXTRA WORK, ADDITIONAL PAYMENT, OR DAMAGES. ALL DAMAGE TO EXISTING UTILITIES INCLUDING SERVICE CONNECTIONS SHALL RE REPAIRED BY AND AT THE EXPENSE OF THE CONTRACTOR.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES AND OBTAIN ALL NECESSARY INSPECTIONS THROUGHOUT THE CONSTRUCTION ACTIVITIES.
- ALL EXCAVATION SHALL BE UNCLASSIFIED. NO SEPARATE PAYMENT WILL BE MADE FOR ROCK EXCAVATION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, INCLUDING BUT NOT LIMITED TO ALL UTILITIES, STORM DRAINAGE, AND SIGNS AS REQUIRED, ALL WORK SHALL BE IN ACCORDANCE WITH THE GOVERNING AUTHORITIES' SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN THE CONTRACTOR'S CONTRACT WITH THE OWNER. ADDITIONALLY, ALL EXISTING UTILITY TOPS SHALL BE ADJUSTED TO FINISHED GRADE.
- REMOVAL OF EXISTING PAVING AND/OR BORING AT THE CONTRACTOR'S DISCRETION SHALL BE INCLUDED AS A PART OF ALL UTILITY INSTALLATIONS WHERE APPLICABLE AT THE CONTRACTOR'S EXPENSE AS WELL AS REPLACEMENT/REPAIR OF ALL DISTURBED MATERIALS IN ACCORDANCE WITH LOCAL SPECIFICATIONS AND CODES.
- THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION SCHEDULES AND ACTIVITIES WITH THE APPROPRIATE UTILITY OWNER AND ADJACENT PROPERTY OWNERS TO MINIMIZE DISRUPTION TO ADJACENT PROPERTY OWNERS INCLUDING VEHICULAR ACCESS.
- THE CONTRACTOR SHALL COORDINATE ALL UTILITY WORK, INCLUDING DEMOLITION AND REMOVAL, WITH THE APPROPRIATE UTILITY COMPANIES AND SERVICE PROVIDERS PRIOR TO DISCONTINUATION OF SERVICE. UTILITIES NOT NOTED FOR DEMOLITION SHALL REMAIN IN SERVICE AT ALL TIMES.
- THE CONTRACTOR SHALL MAINTAIN EXISTING UTILITY SERVICE TO ALL ADJOINING PROPERTIES UNTIL THE RELOCATED UTILITIES ARE INSPECTED AND APPROVED.
- ALL EXISTING UTILITIES SHALL BE REMOVED BACK TO THE CLOSEST STRUCTURE AND CAPPED AT THAT LOCATION UNLESS OTHERWISE INDICATED IN THESE PLANS.
- REMOVE ALL TREES, GRASS, WEEDS, ROOTS, AND OTHER DEBRIS FROM THE AREA TO BE EXCAVATED, FILLED OR GRADED.
- IF EXCAVATED MATERIAL IS UNSUITABLE FOR COMPACTION, AS DETERMINED BY THE GEOTECHNICAL ENGINEER, THE CONTRACTOR SHALL FURNISH SUITABLE BORROW.
- ALL SLOPES, CUT OR FILL, SHALL BE GRADED TO MAXIMUM FINISH SLOPE OF THREE (3) FEET HORIZONTAL TO ONE (1) FOOT VERTICAL. NO GRADED SLOPE SHALL EXCEED 3:1 WITHOUT SPECIFIC SLOPE PLANTING OR REINFORCEMENT.
- SITE SHALL BE GRADED TO ENSURE DRAINAGE OF WATER FROM ALL SURFACES.
- THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL SURFACE AND GROUNDWATER CONTROL MEASURES.
- GRADES NOT OTHERWISE INDICATED ON THE PLANS SHALL BE UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE GIVEN. ABRUPT CHANGES IN SLOPES SHALL BE WELL ROUNDED.
- STORM DRAINAGE SYSTEMS WITHIN THE PROJECT AREA ARE TO BE COMPLETELY CLEANED AT THE COMPLETION OF THE PROJECT.
- EXISTING TREES WHERE INDICATED SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES. ALL TREE PROTECTION FENCING TO BE INSPECTED DAILY AND ALL GRADING ACTIVITIES TO REMAIN OUTSIDE THE DRIP LINES.
- ALL TREE PROTECTION MEASURES SHALL BE INSTALLED PRIOR TO GRADING.
- ALL SOILS UNDERCUTTING, OVER EXCAVATION, UNDER DRAIN INSTALLATION, AND ROCK FILLS SHALL BE DETERMINED AND DIRECTED BY THE SOILS ENGINEER.
- FILL AREAS TO BE COMPACTED TO 95% STANDARD PROCTOR MINIMUM UNLESS OTHERWISE INDICATED BY GEOTECHNICAL ENGINEER.
- UNLESS OTHERWISE INDICATED, ALL DISTURBED SOIL AREAS TO RECEIVE 6 INCHES OF TOPSOIL AND TO BE SEEDED AND MULCHED.
- THE CONTRACTOR SHALL PERFORM ALL CUTTING, PATCHING, AND REPAIRING AS REQUIRED TO PERFORM ALL OF THE WORK INDICATED ON THE DRAWINGS, AND ALL OTHER WORK THAT MAY BE REQUIRED TO COMPLETE THE JOB.

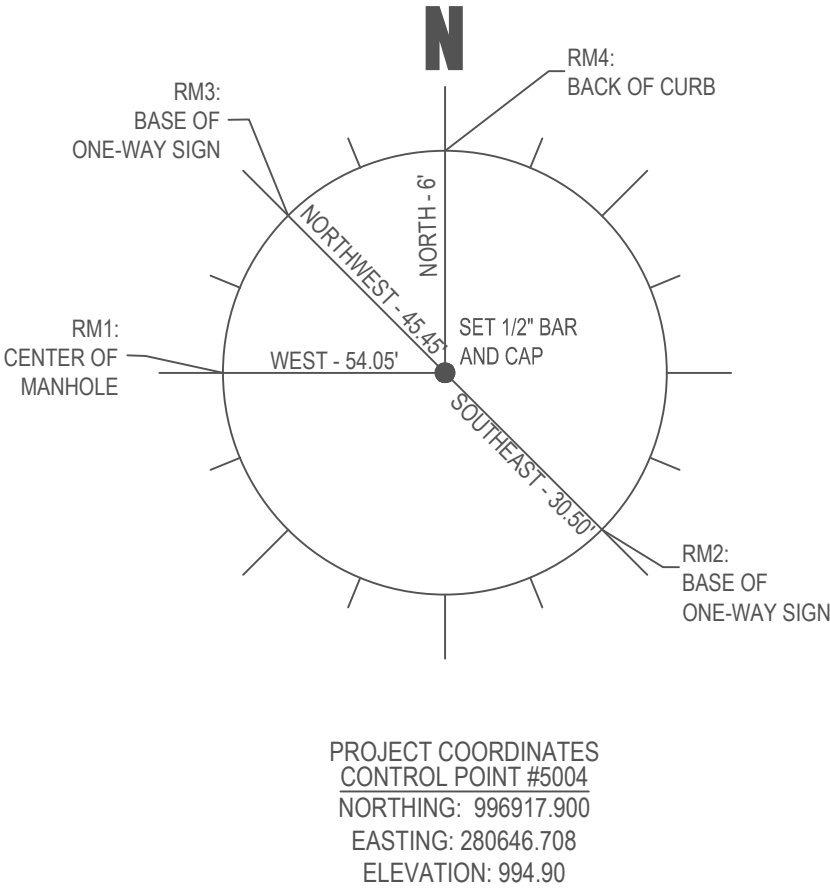
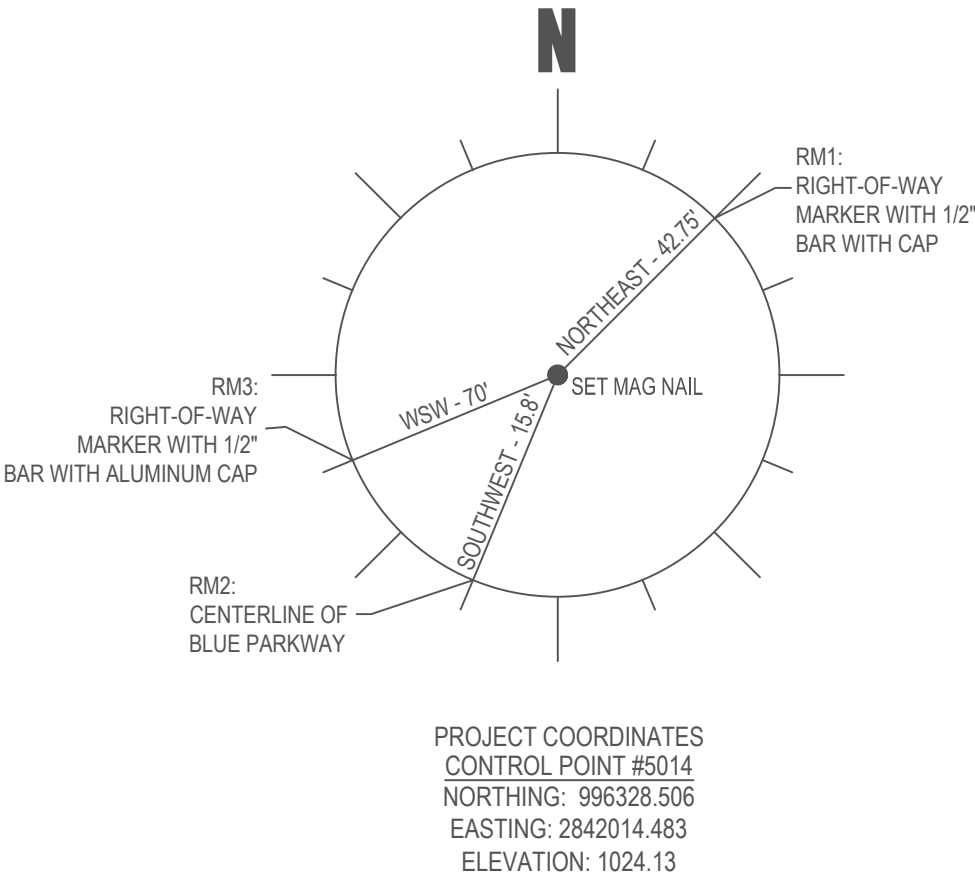
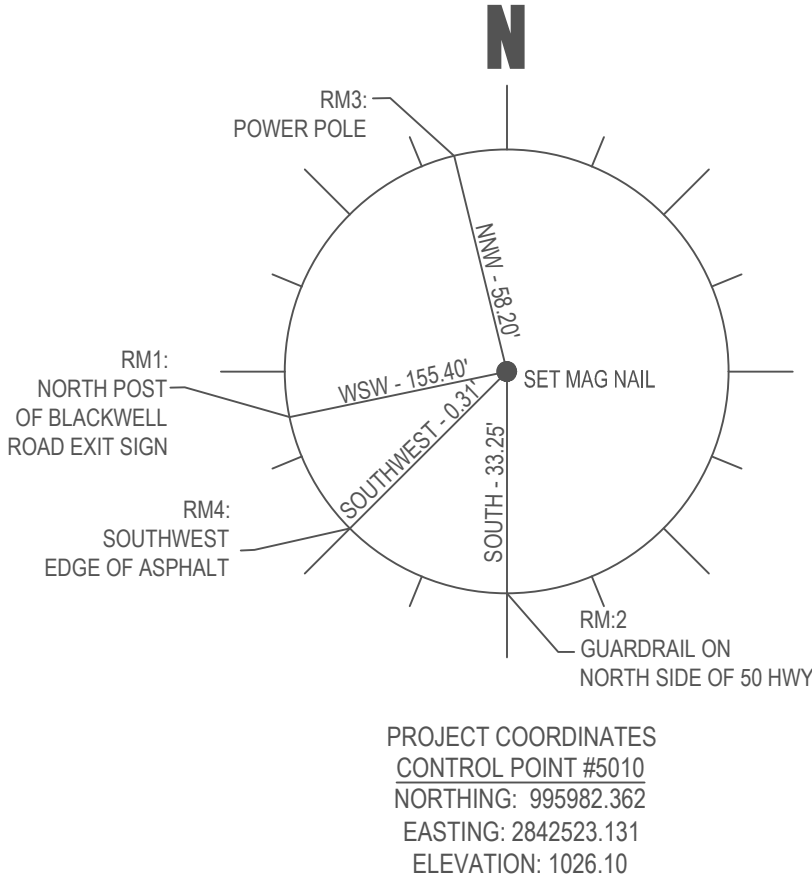
STREETS & STORM SEWER:

- SPECIFICATIONS, MATERIALS, AND CONSTRUCTION, SHALL BE PER THE REQUIREMENTS OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT AND AS AMENDED BY THE ADOPTION OF CERTAIN PARTS OF THE KANSAS CITY METROPOLITAN CHAPTER APWA FOR STREET AND STORM SEWER SPECIFICATIONS AND STANDARDS.
- HIGH DENSITY PLASTIC PIPE (HDPE) SHALL CONFORM TO A.A.S.H.T.O. M-294.
- REINFORCED CONCRETE PIPE (RCP) SHALL CONFORM TO ASTM DESIGNATION C-76-62T (CLASS III).
- CURB RETURN RADIUS, 15' MINIMUM UNLESS SHOWN OTHERWISE.
- THE TOP 6" OF ROADWAY SUBGRADE SHALL BE UNDERCUT AND COMPACTED TO MINIMUM 95% OF MAXIMUM DENSITY AT OPTIMUM MOISTURE AS DETERMINED BY AASHO T99, METHOD B. CONTRACTOR SHALL PROVIDE FOR MOISTURE-DENSITY AND RELATIVE DENSITY TESTS ON ROADWAY SUBGRADE BY AN ACCEPTED TESTING FIRM. CONTRACTOR SHALL PROVIDE FOR IN-PLACE DENSITY TEST ON COMPACTED SUBGRADE BY AN ACCEPTED TESTING FIRM. IN-PLACE DENSITY TEST SHALL BE CONDUCTED EVERY 50-FEET ALONG THE PROPOSED ROADWAY. CONTRACTOR SHALL PROVIDE TESTING RESULTS TO ENGINEER.
- ALL UTILITY CROSSING SHALL BE BACKFILLED WITH AB3 OR FLOWABLE FILL.
- END SECTIONS FOR CONCRETE PIPE SHALL BE FLARED END SECTIONS OF THE PIPE MANUFACTURER'S STANDARD DESIGN, AND SHALL MEET ALL APPLICAPBLE REQUIREMENTS OF ASTM C 76 FOR CLASS II OR HIGHER CLASSES OF PIPE.
- TOE WALLS: FLARED END SECTIONS FOR CONCRETE AND STEEL PIPE SHALL BE SET ON A CONCRETE TOE WALL CENTERED ON THE END OF THE SECTION. TOE WALLS SHALL BE 8" THICK X 24" DEEP X THE WIDTH OF THE END SECTION.
- END SECTIONS FOR HDPE PIPE SHALL BE HDPE MEETING ASTM D3350 MINIMUM CELL CLASSIFICATION 213320C.

REFERENCE BEARING:

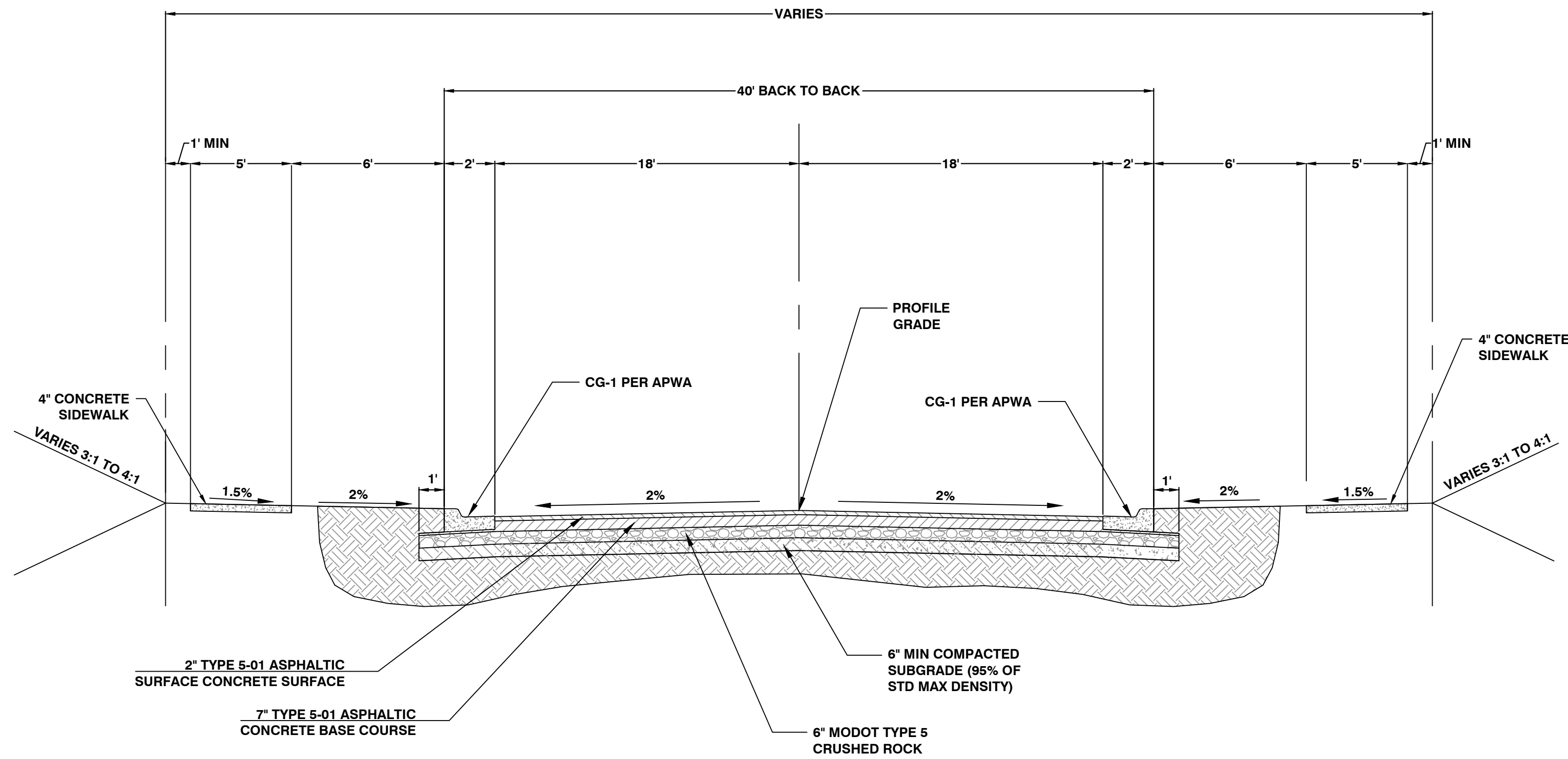
BEARINGS SHOWN HEREON ARE GRID BEARINGS BASED ON THE MISSOURI COORDINATE SYSTEM OF 1983, WEST ZONE AND TIED TO CONTROL MONUMENT "JA-45", 2003 ADJUSTMENT (N: 303273.664 METERS, E: 863885.886 METERS) OF THE MISSOURI GEOGRAPHIC REFERENCE SYSTEM, AS DEVELOPED FROM GPS OBSERVATIONS, A GRID FACTOR OF 0.9998986 WAS USED.

CONTROL POINTS				
GRID FACTOR: 0.9998986 PROJECT COORDINATES				
	GRID NORTHING	GRID EASTING	GROUND NORTHING	GROUND EASTING
5010	303545.249 METERS	866314.931 METERS	995982.362 US FT	2842523.131 US FT
5014	303650.743 METERS	866159.910 METERS	996328.506 US FT	2842014.483 US FT
5004	303830.372 METERS	865743.054 METERS	996917.900 US FT	2840646.708 US FT
HORIZONTAL REFERENCE: STATE PLANE, WEST ZONE, NAD 1983				

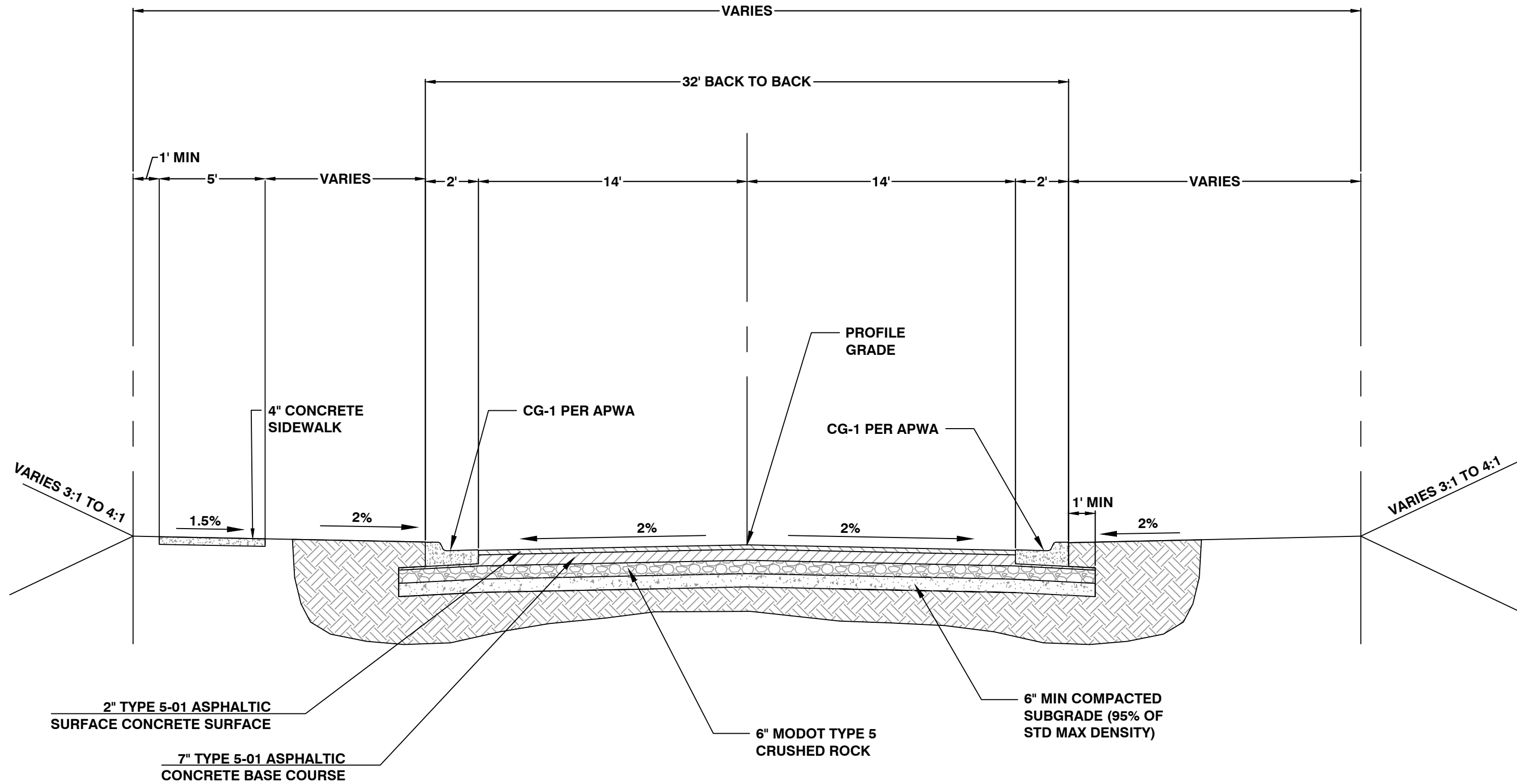


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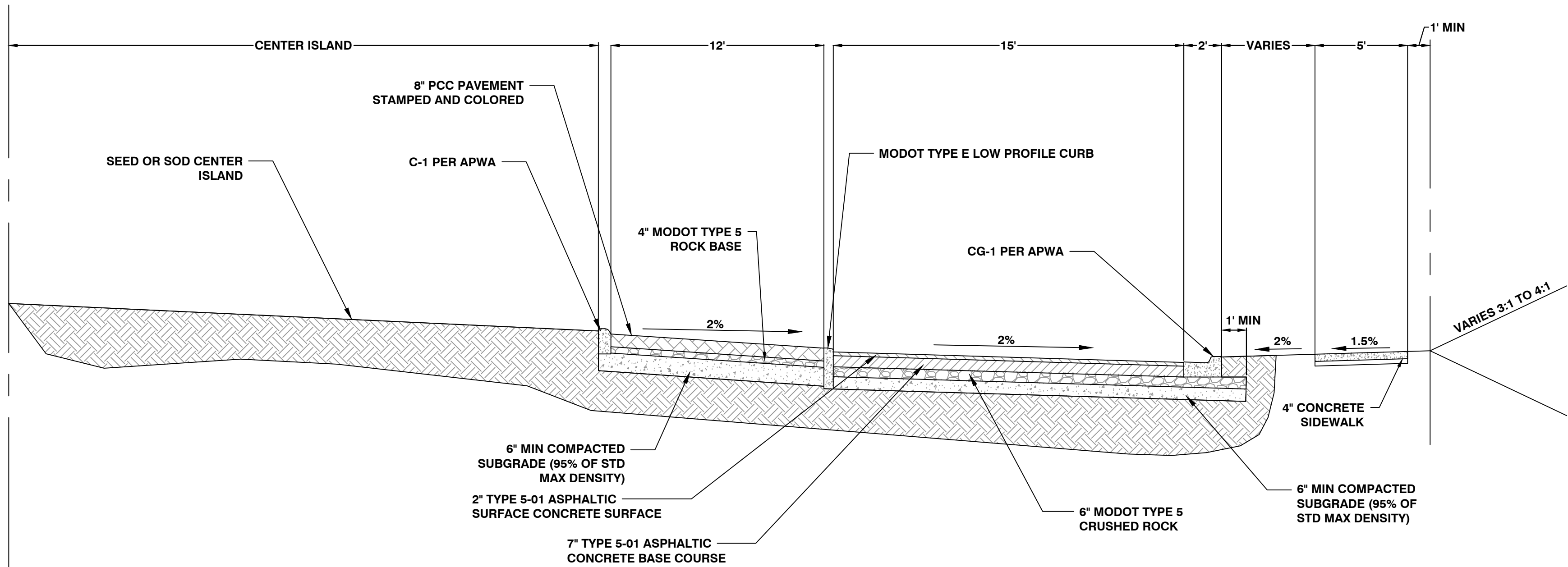
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**TYPICAL SECTION
BLUE PARKWAY
STA 10+00 TO STA 13+80
NTS**



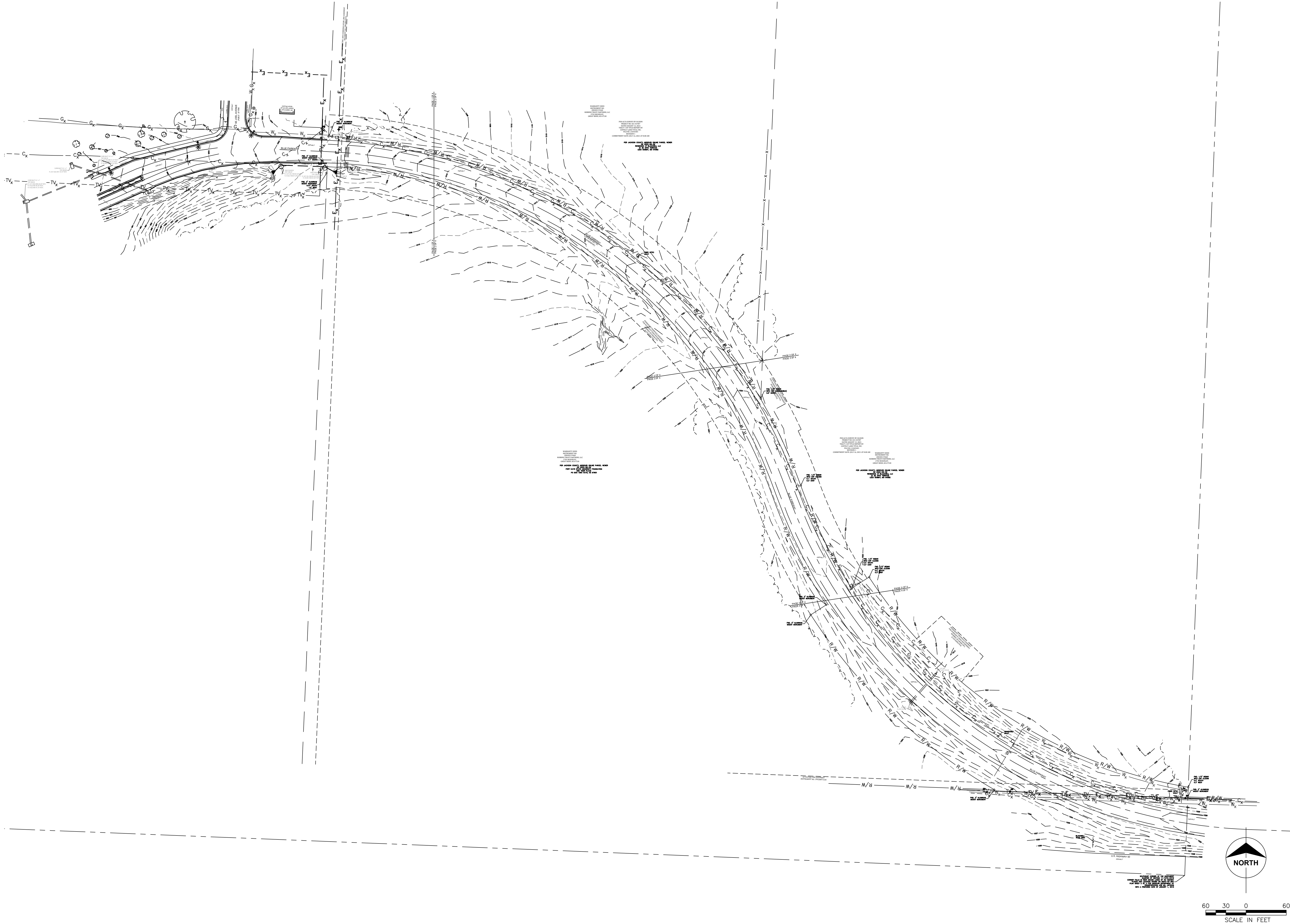
**TYPICAL SECTION
BLUE PARKWAY
STA 17+53 TO STA 27+15
NTS**



**TYPICAL SECTION
ROUNDBOUT**

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Certificates of Authority
Architecture: MO 318 / KS 13
Engineering: MO 4 / KS 241
Land Surveying: MO 123 / KS 36

Jadrienne S. Rodell-Tipton, PE
PE-2021032725 (MISSOURI #)

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PREPARED FOR:
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21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

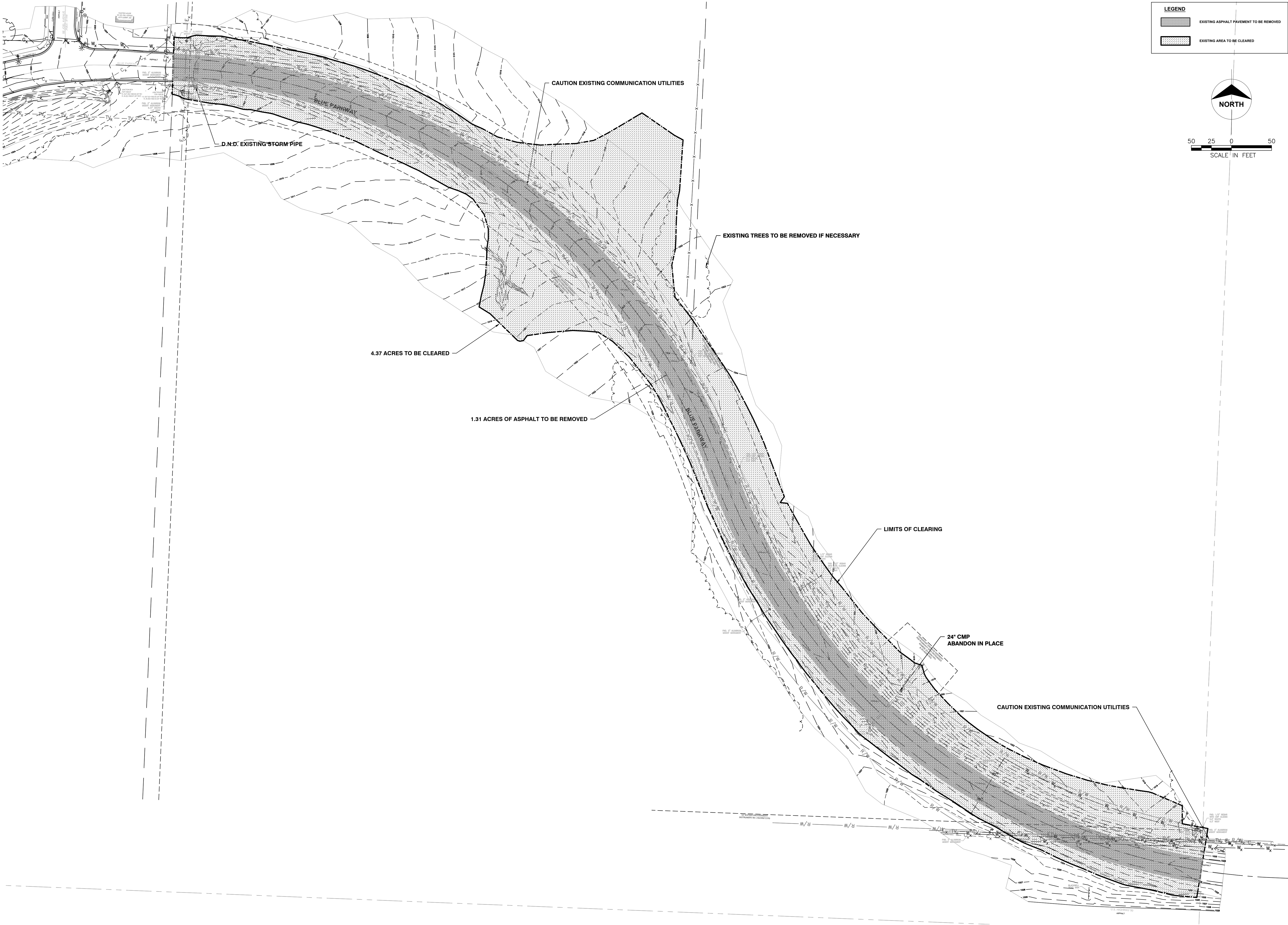
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

PRELIMINARY

EXISTING
CONDITIONS

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PUBLIC IMPROVEMENT PLANS
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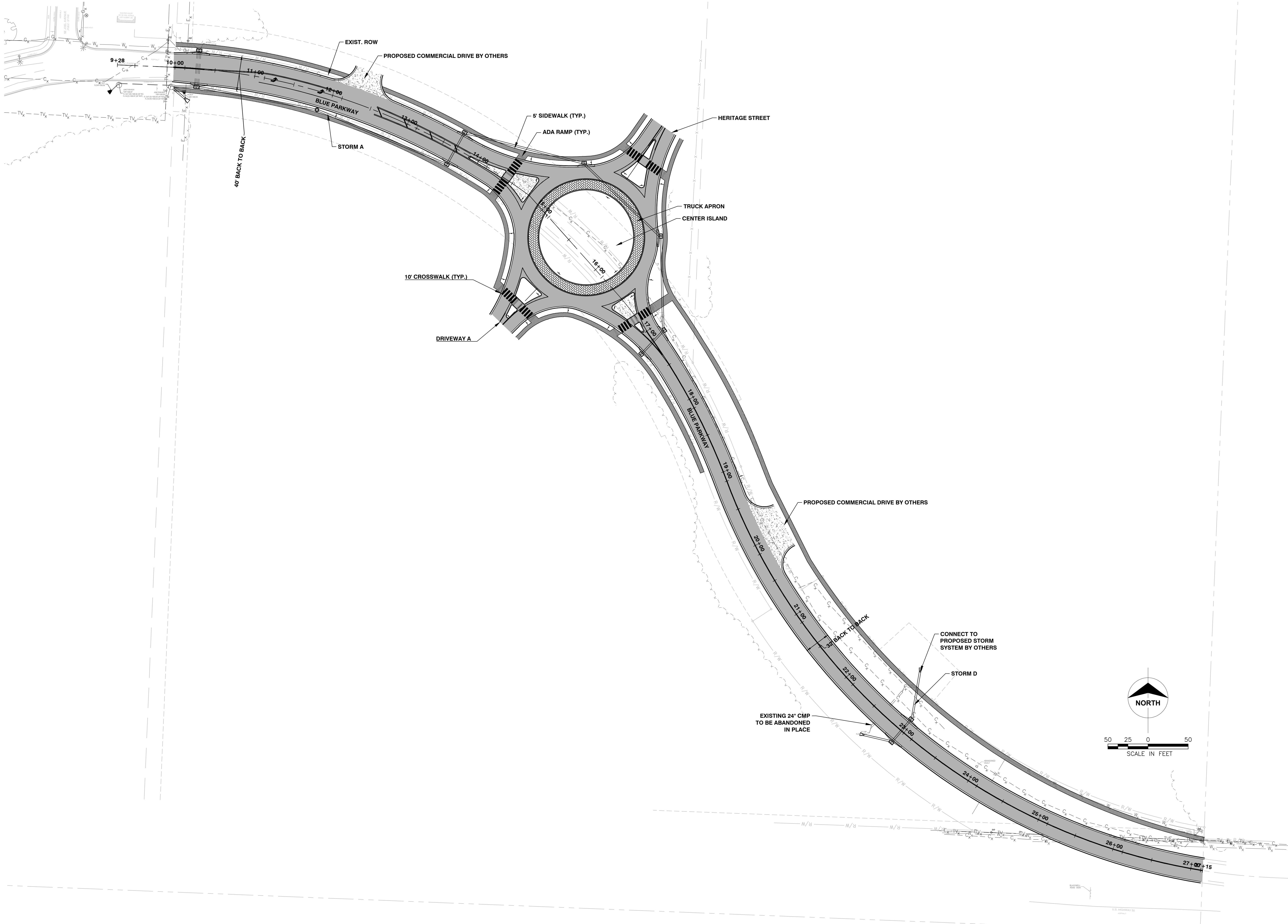
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PRELIMINARY

DEMOLITION
PLAN

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PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

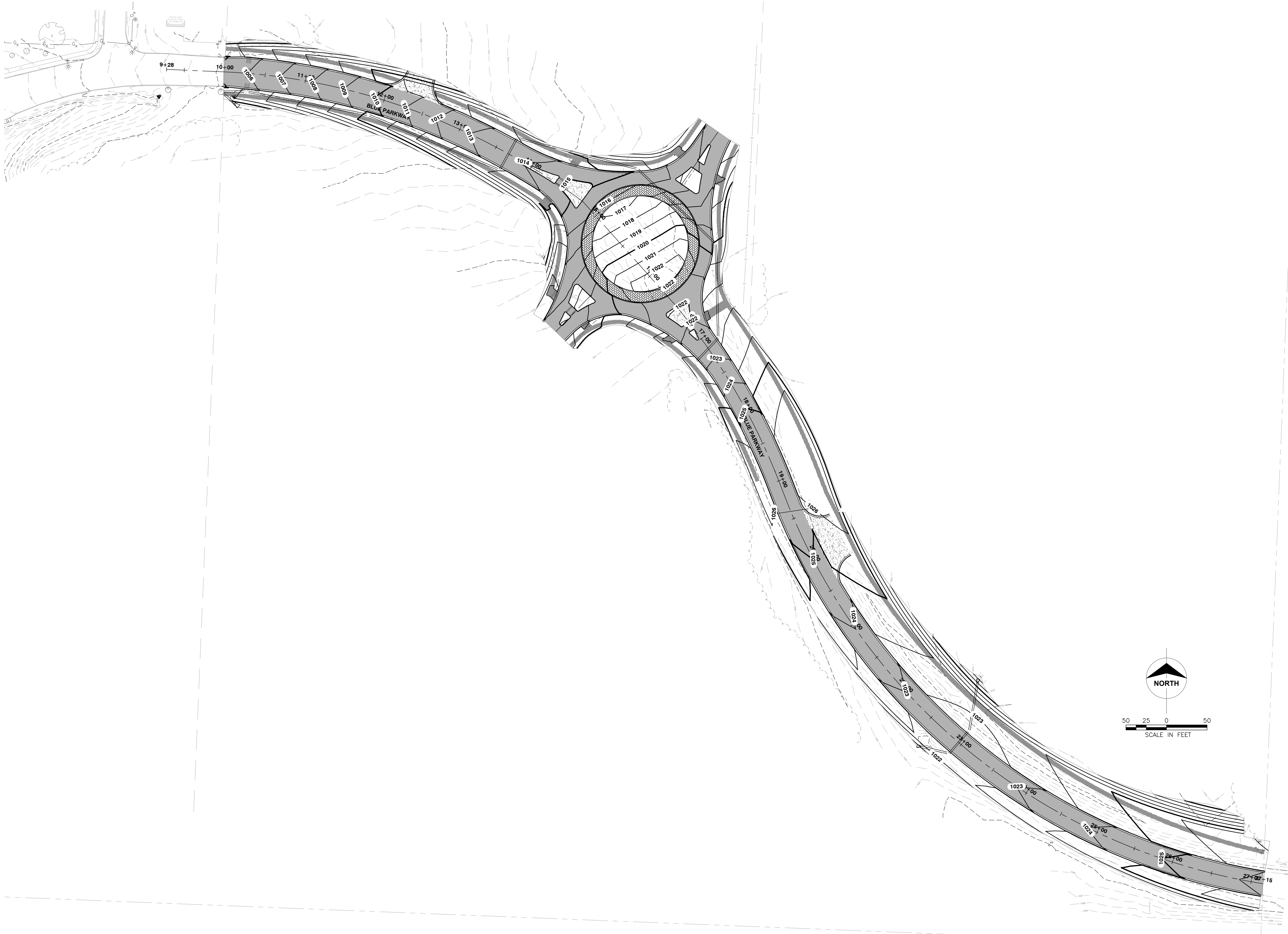
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PRELIMINARY

OVERALL LAYOUT
PLAN

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Certificates of Authority
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Engineering: MO 4 / KS 241
Land Surveying: MO 123 / KS 36

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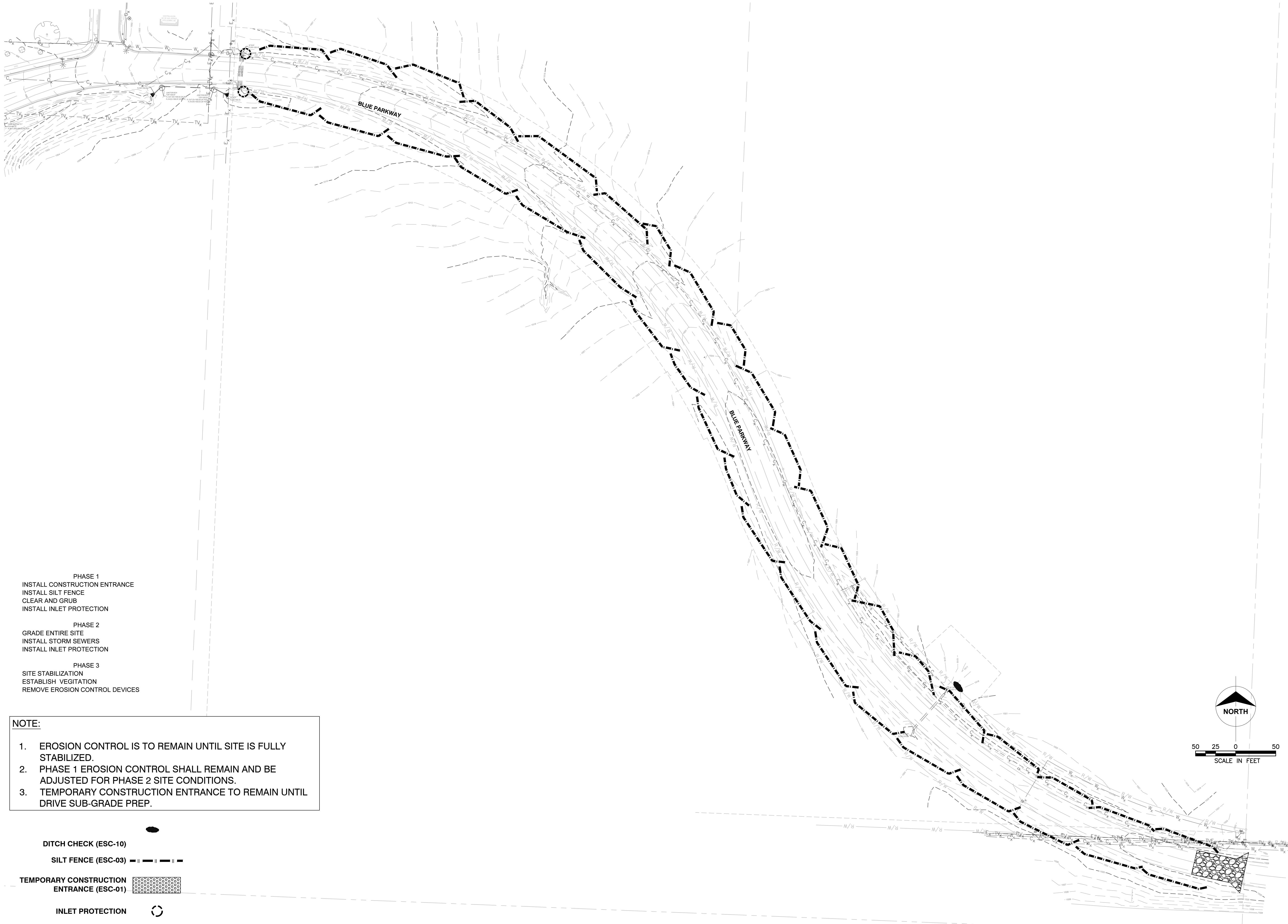
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PRELIMINARY

GRADING PLAN

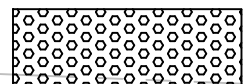
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- PHASE 1
INSTALL CONSTRUCTION ENTRANCE
INSTALL SILT FENCE
CLEAR AND GRUB
INSTALL INLET PROTECTION
- PHASE 2
GRADE ENTIRE SITE
INSTALL STORM SEWERS
INSTALL INLET PROTECTION
- PHASE 3
SITE STABILIZATION
ESTABLISH VEGETATION
REMOVE EROSION CONTROL DEVICES

NOTE:

1. EROSION CONTROL IS TO REMAIN UNTIL SITE IS FULLY STABILIZED.
2. PHASE 1 EROSION CONTROL SHALL REMAIN AND BE ADJUSTED FOR PHASE 2 SITE CONDITIONS.
3. TEMPORARY CONSTRUCTION ENTRANCE TO REMAIN UNTIL DRIVE SUB-GRADE PREP.

- DITCH CHECK (ESC-10) 
- SILT FENCE (ESC-03) 
- TEMPORARY CONSTRUCTION ENTRANCE (ESC-01) 
- INLET PROTECTION 

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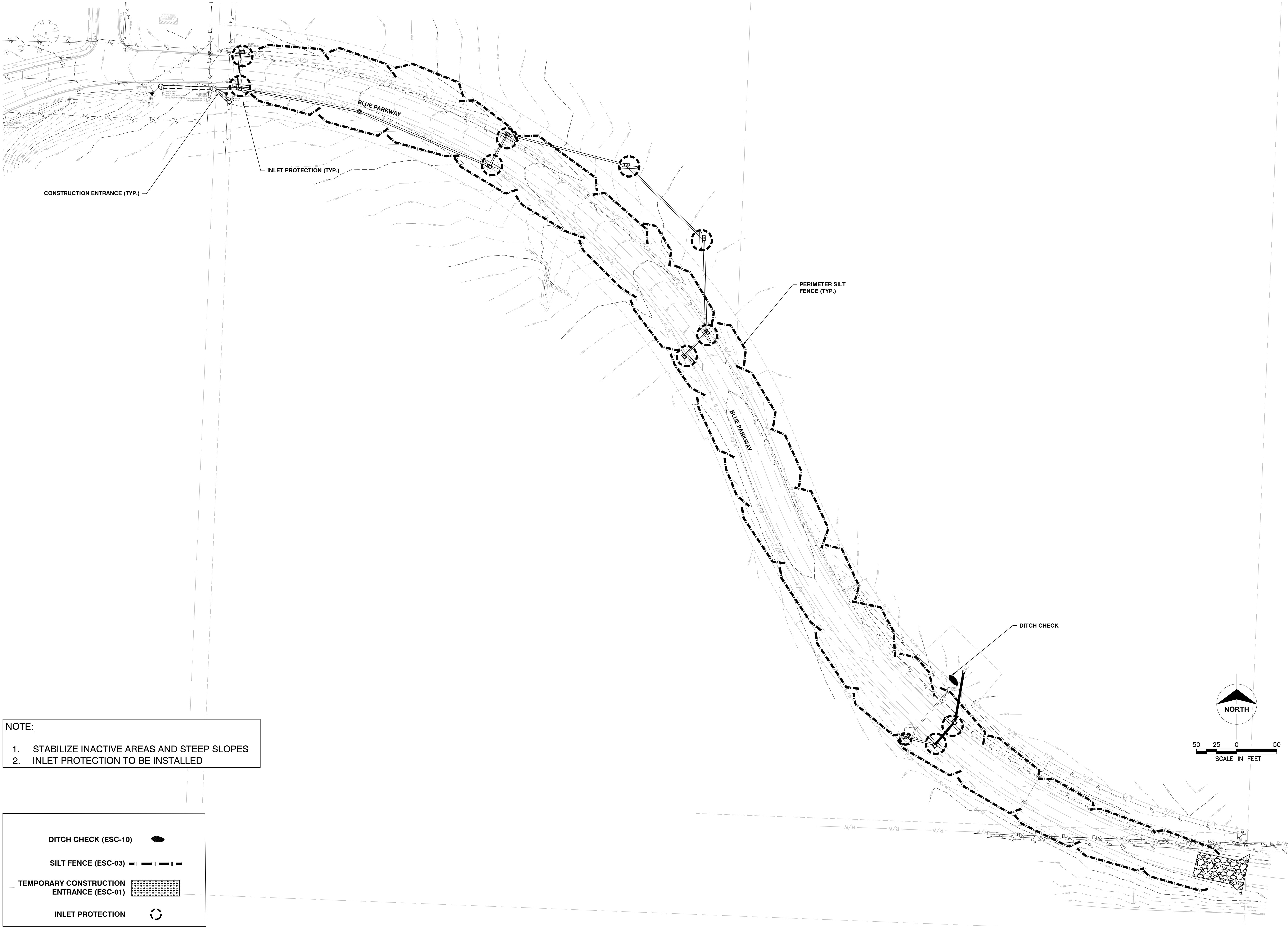
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PRELIMINARY

EROSION
CONTROL PHASE 1

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PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

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EROSION
CONTROL PHASE 2

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BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

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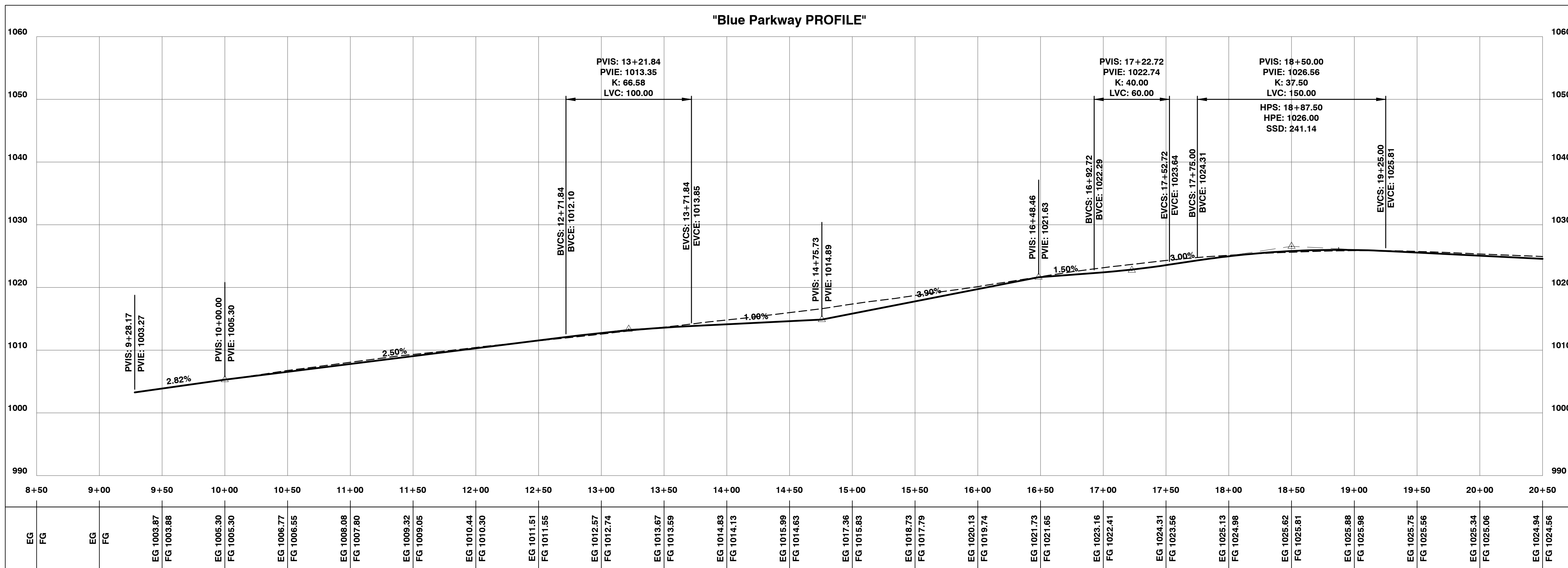
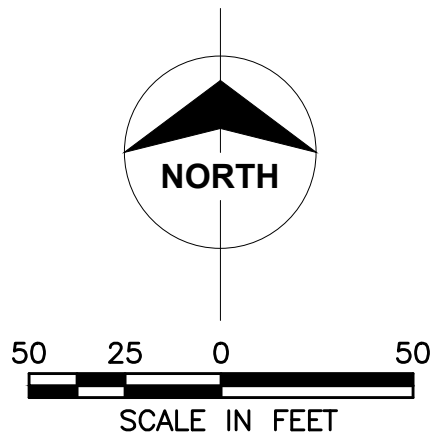
PRELIMINARY

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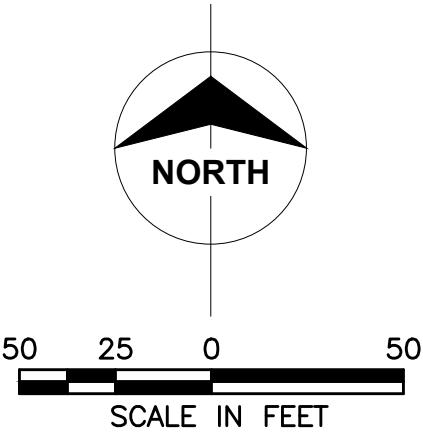
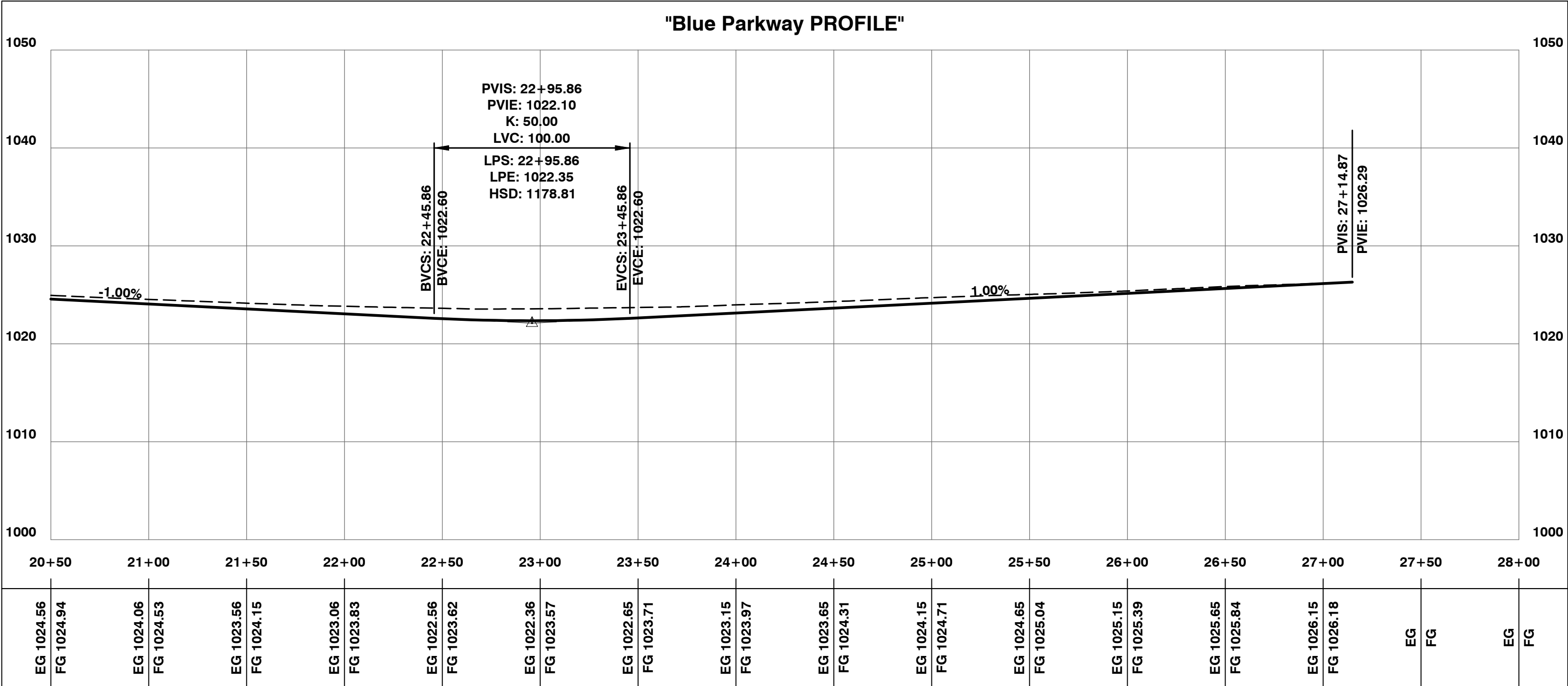
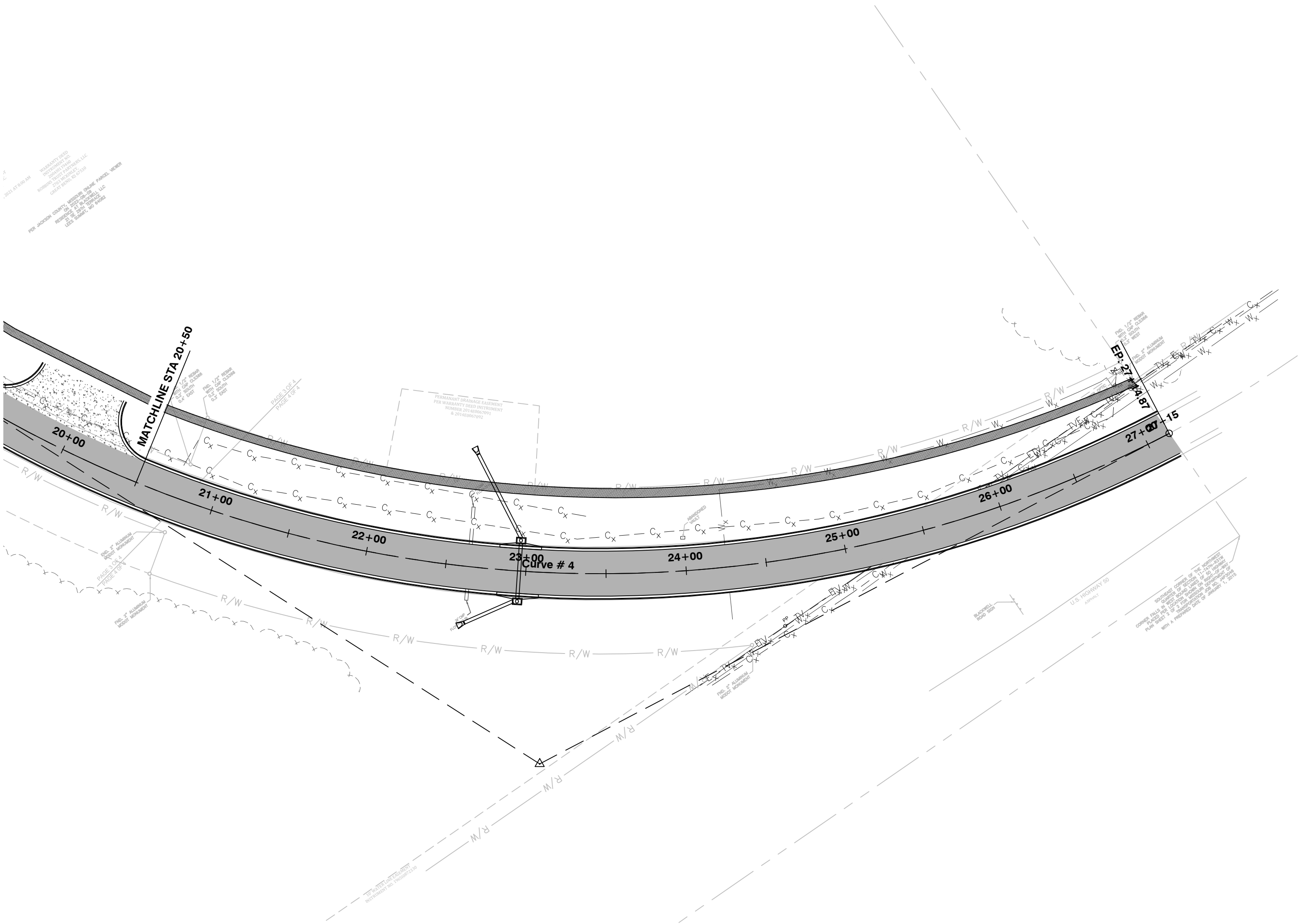


Modified from 2015 Overland Park Standard Details
for Erosion and Sediment Control.

CURVE #3
PI 17+74.91
 Δ 018.13°
T=126.46
L=250.80
N 996615.03
E 2841863.60
R= 792.66



CURVE #4
PI 23+55.45
Δ 060.32°
T=442.74
L=802.16
N 996070.15
E 2842069.93
R= 762.00



Certificates of Authority
Architecture: MO 318 / KS 13
Engineering: MO 4 / KS 241
Land Surveying: MO 123 / KS 36

Jadrienne S. Rodell-Tipton, PE
PE-2021032725 (MISSOURI #)

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PREPARED FOR:
GRIFFIN RILEY
PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

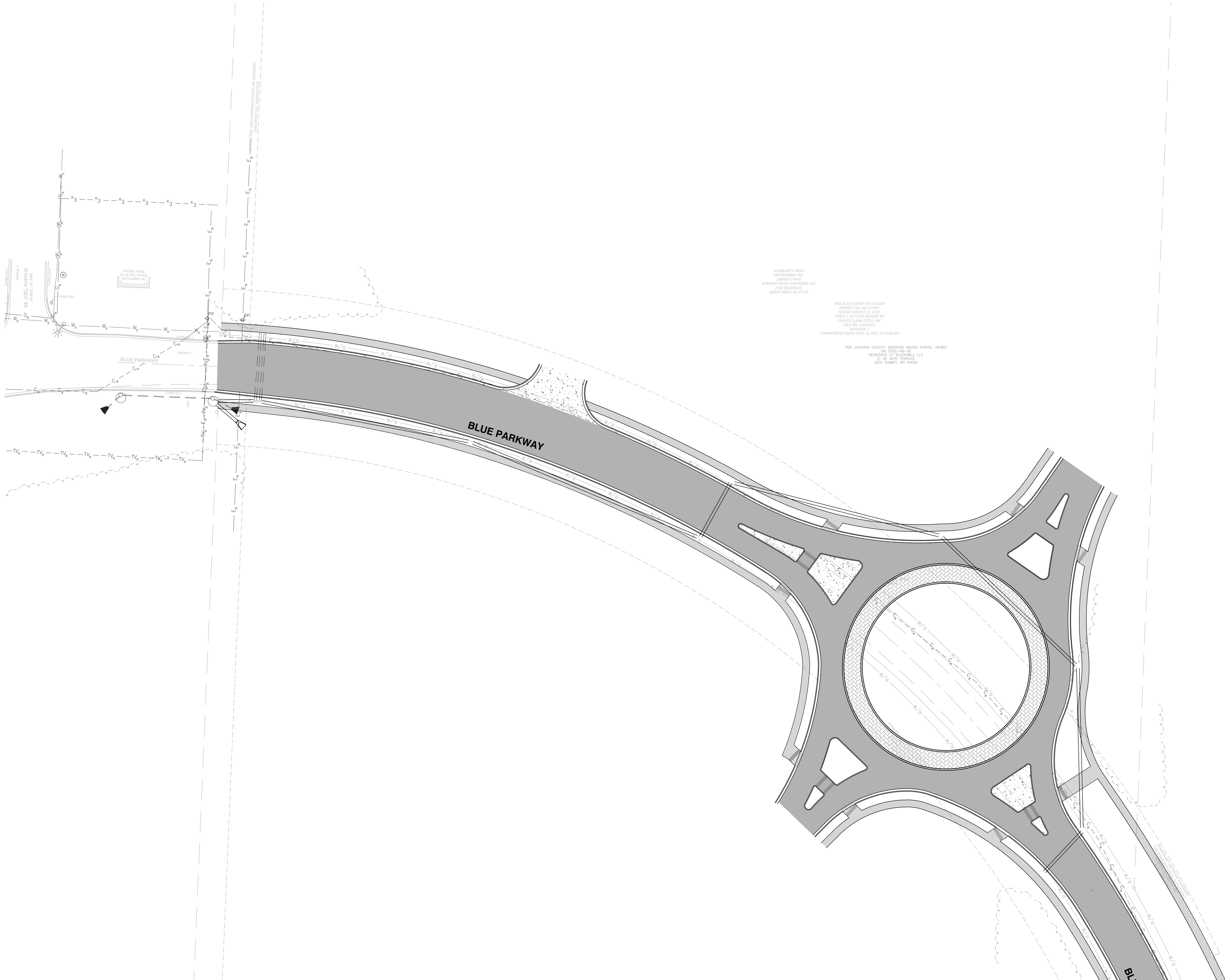
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

PRELIMINARY

STREET PLAN &
PROFILE 2

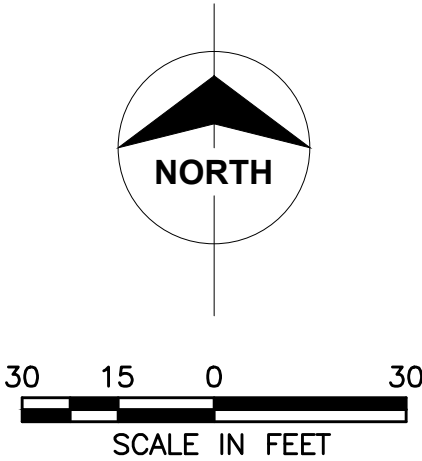
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WARRANTY DEED
INSTRUMENT NO.
20402139460
ROBBINS TRUST PARTNERS, LLC
2703 MCKINLEY,
GREAT BEND, KS 67530

PER ALTA SURVEY BY OLSSON
PROJECT NO. 021-01007
DATED AUGUST 13, 2021
TRACT 1 OF TITLE REPORT BY
CORPUS LAND TITLE, INC.
FILE NO. 21045401
REVISION 1
COMMITMENT DATE: JULY 16, 2021 AT 8:00 AM

PER JACKSON COUNTY, MISSOURI ONLINE PARCEL VIEWER
ON 2022-08-09
RESIDENCE AT BLACKWELL, LLC
21 SE 29TH TERRACE
LEE'S SUMMIT, MO 64082



PER ALTA SURVEY BY OLSSON
PROJECT NO. 021-01007
DATED AUGUST 13, 2021

Jadrienne S. Rodell-Tipton, PE
PE-2021032725 (MISSOURI #)

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21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

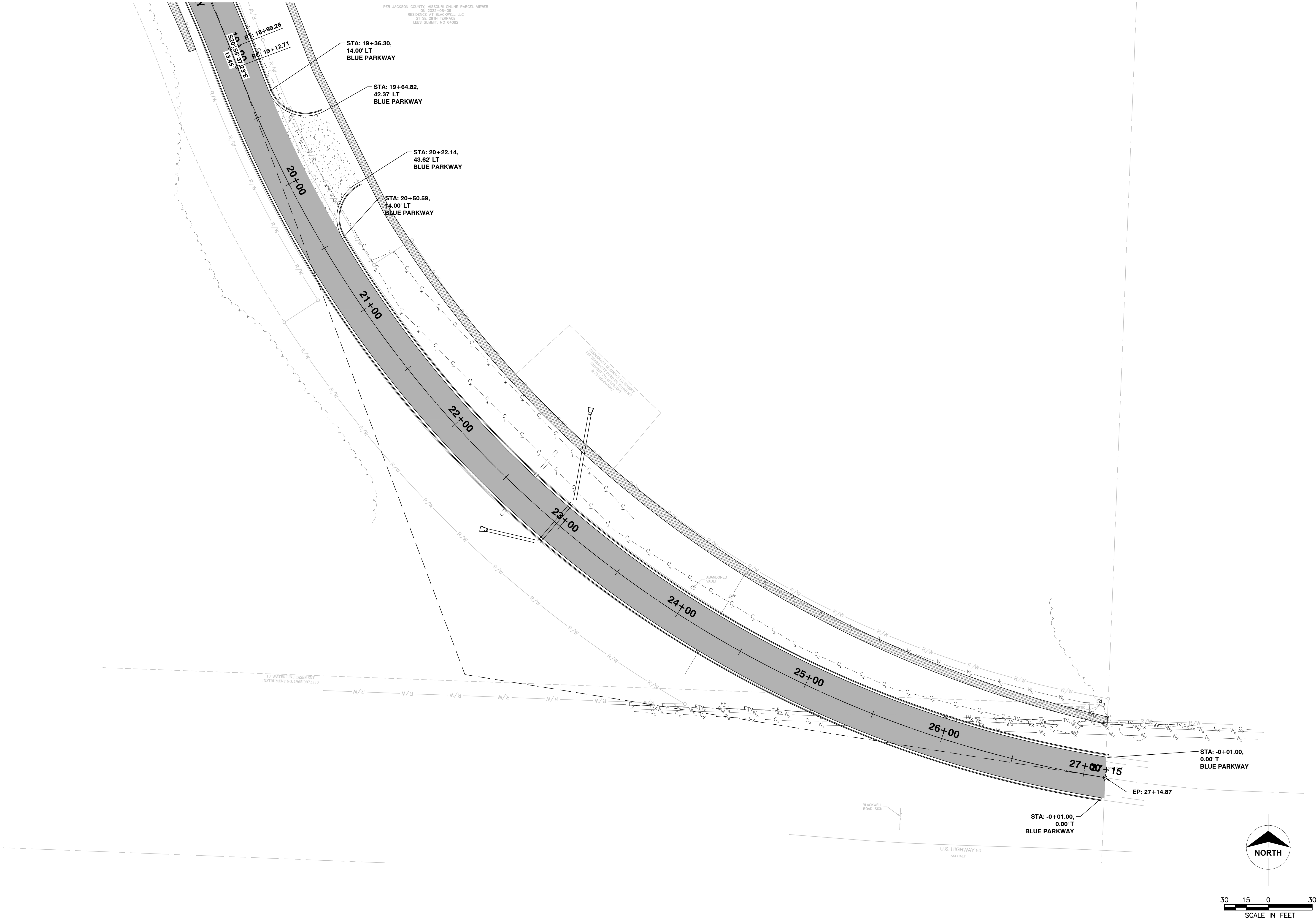
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PROJECT #: 22-2087
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PRELIMINARY

LAYOUT DETAILS 1

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PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

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PROJECT #: 22-2087
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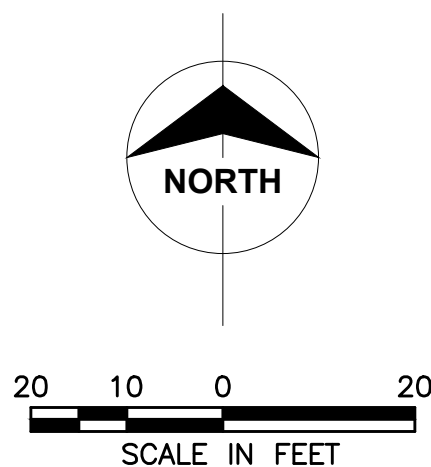
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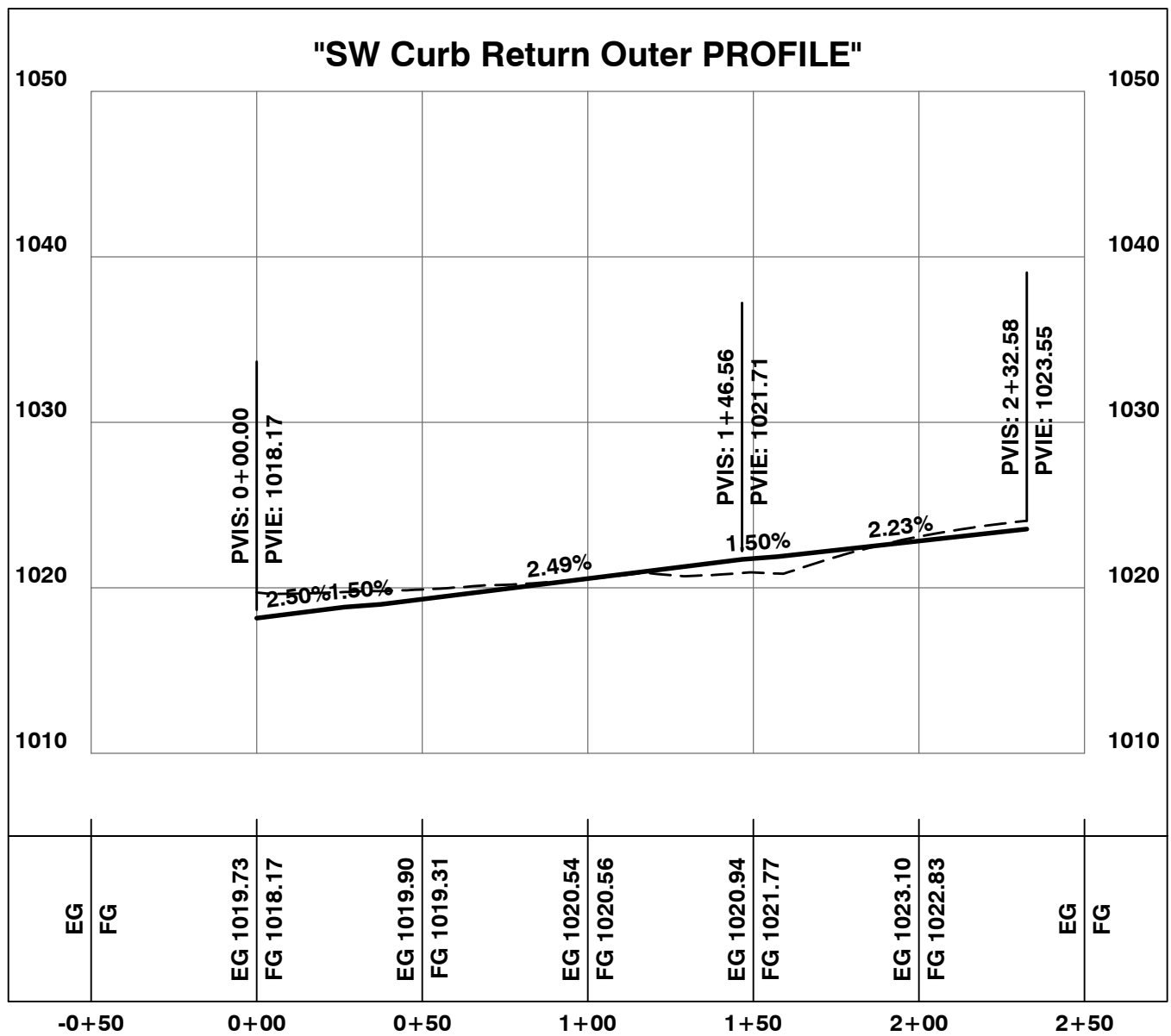
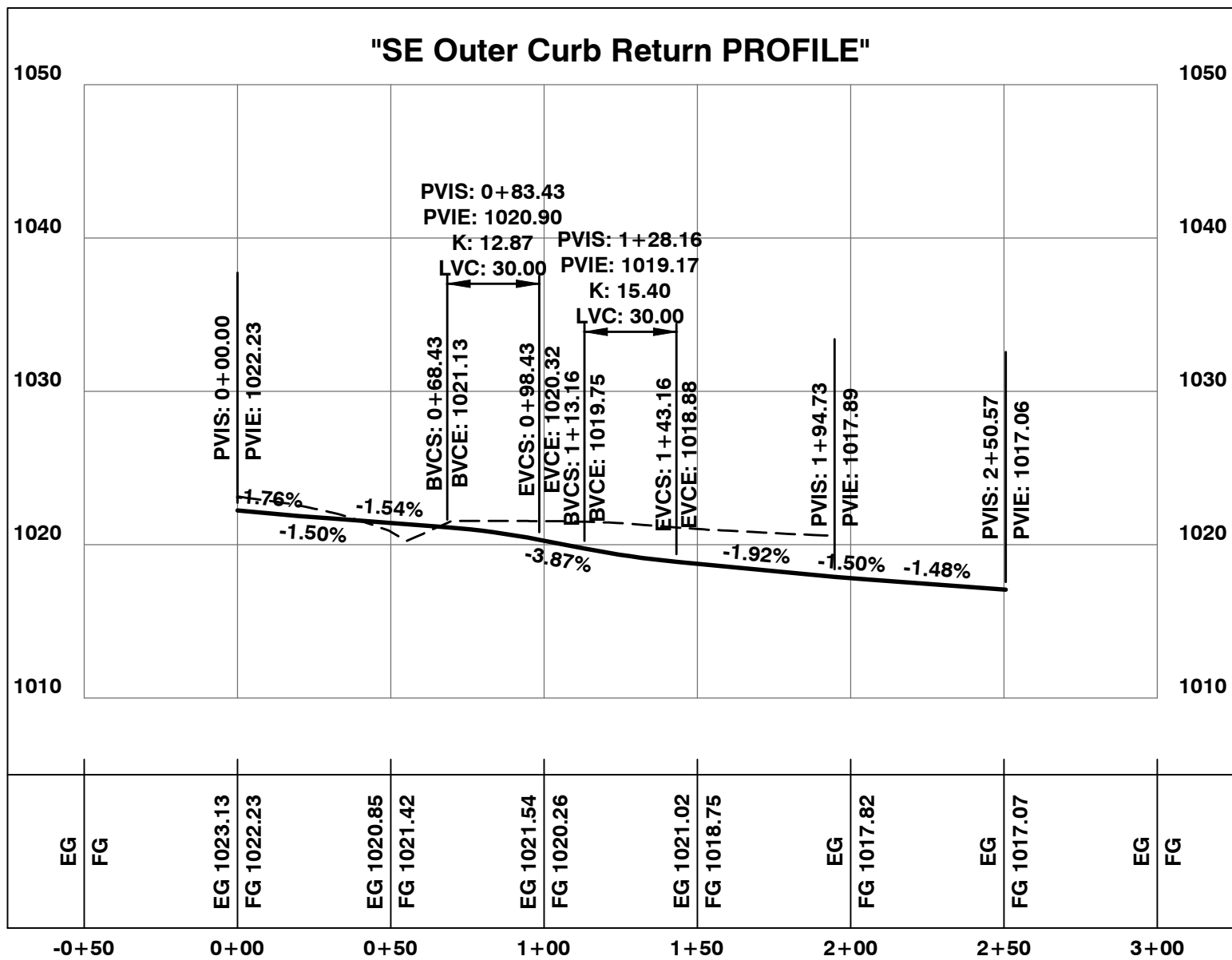
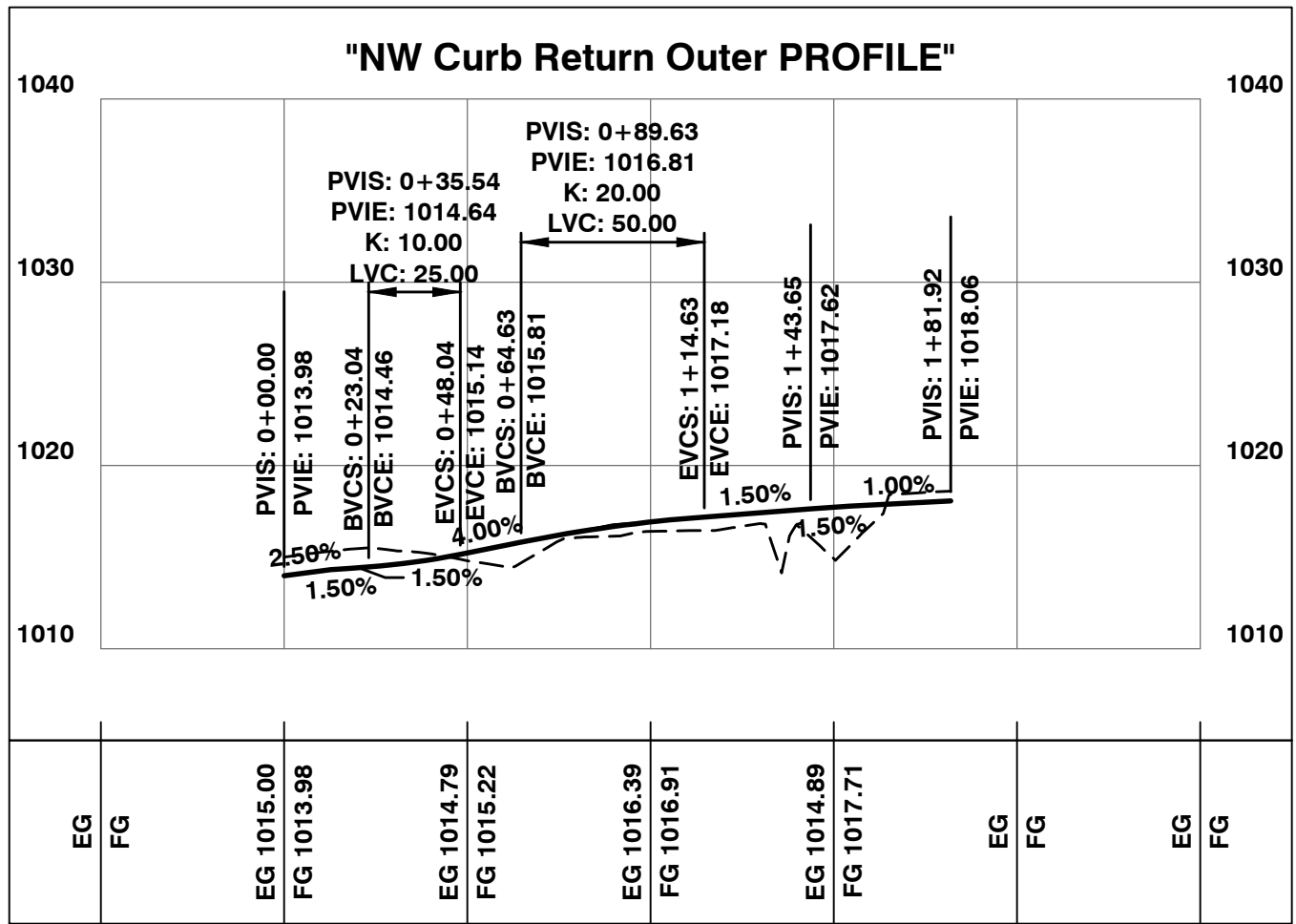
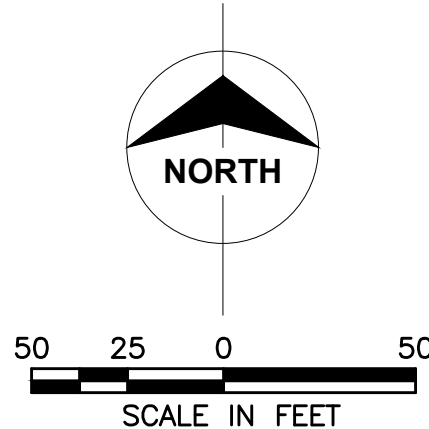
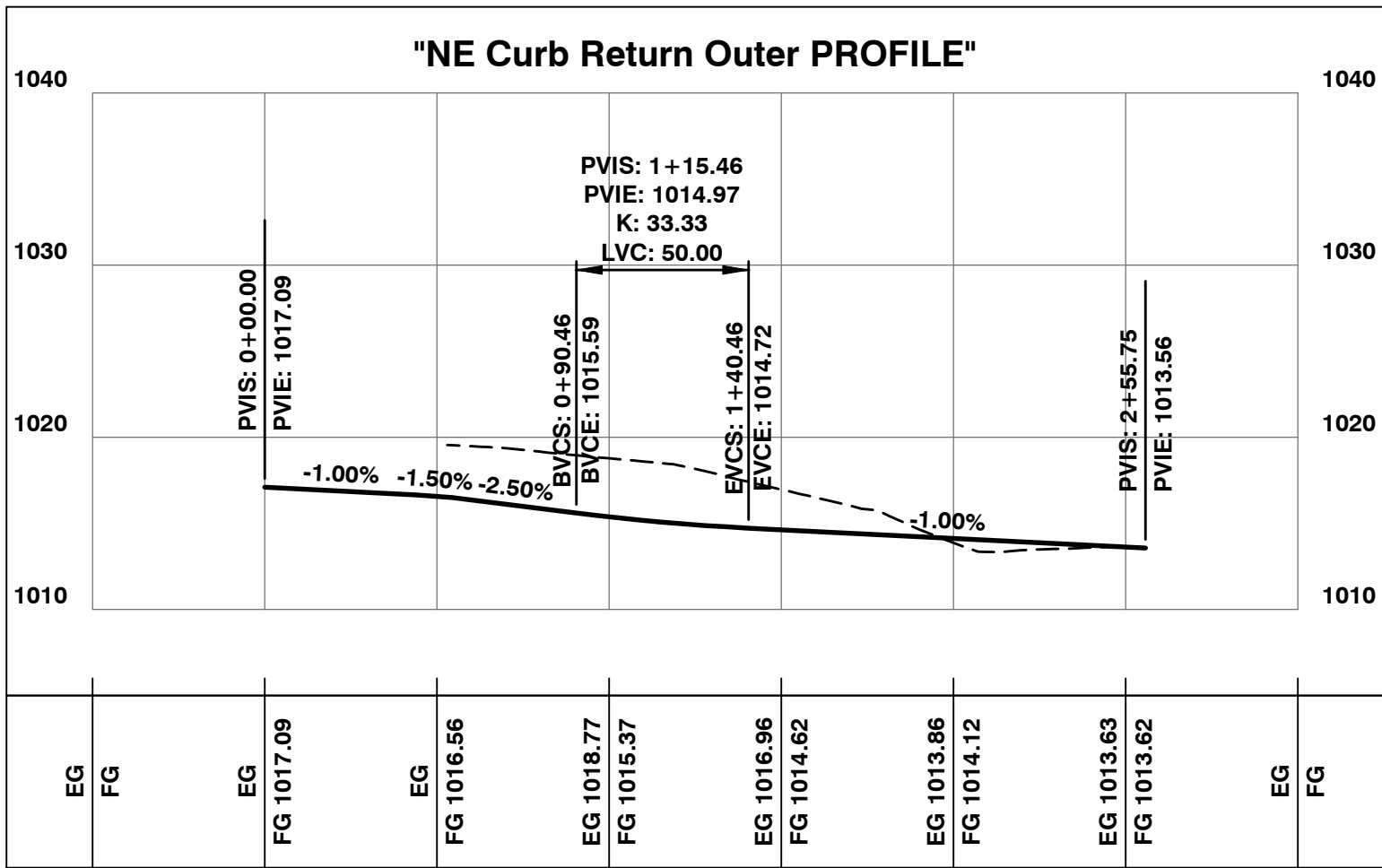
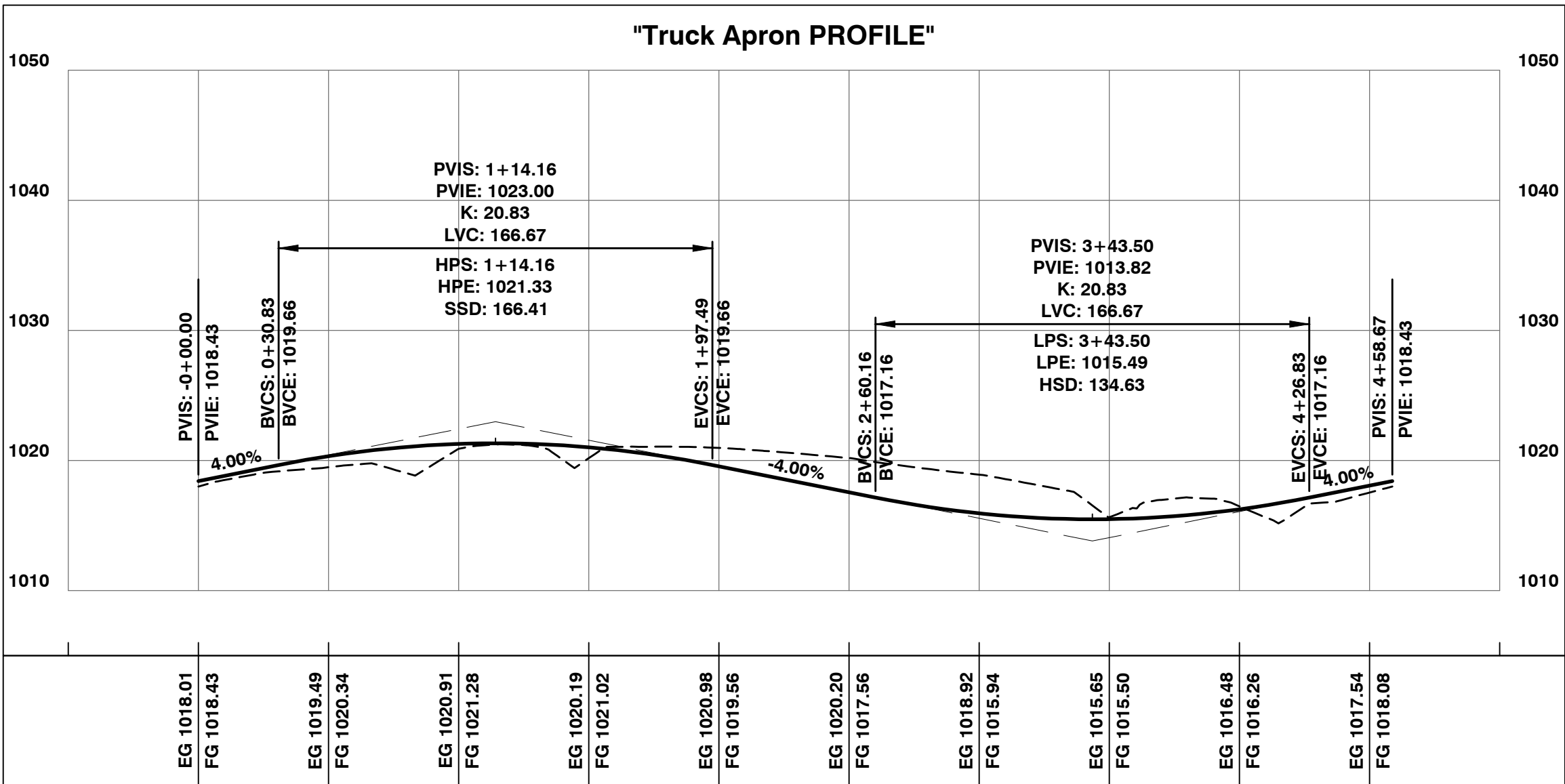
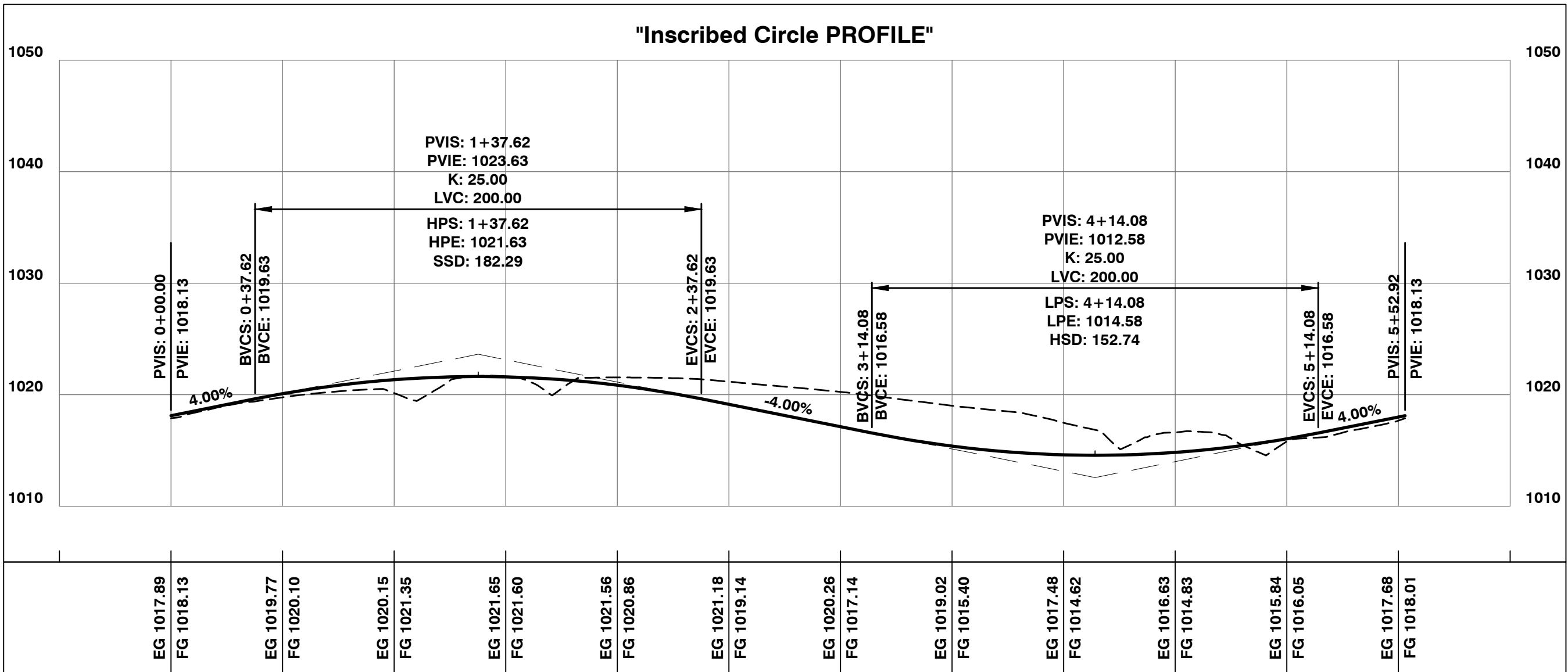
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BLUE PARKWAY
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

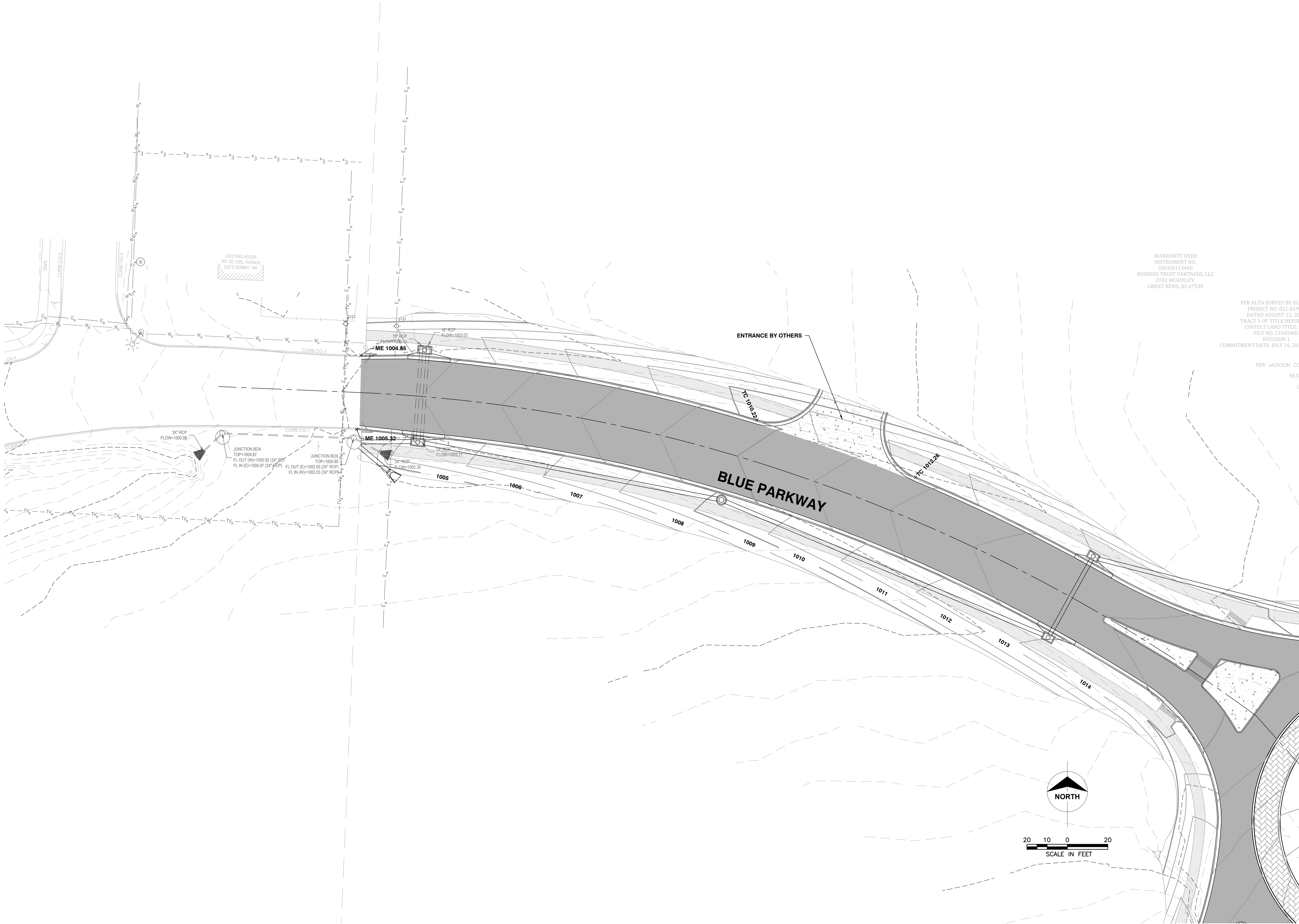
LAYOUT DETAILS 3





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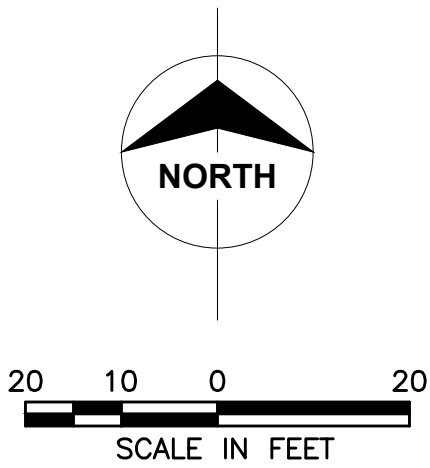
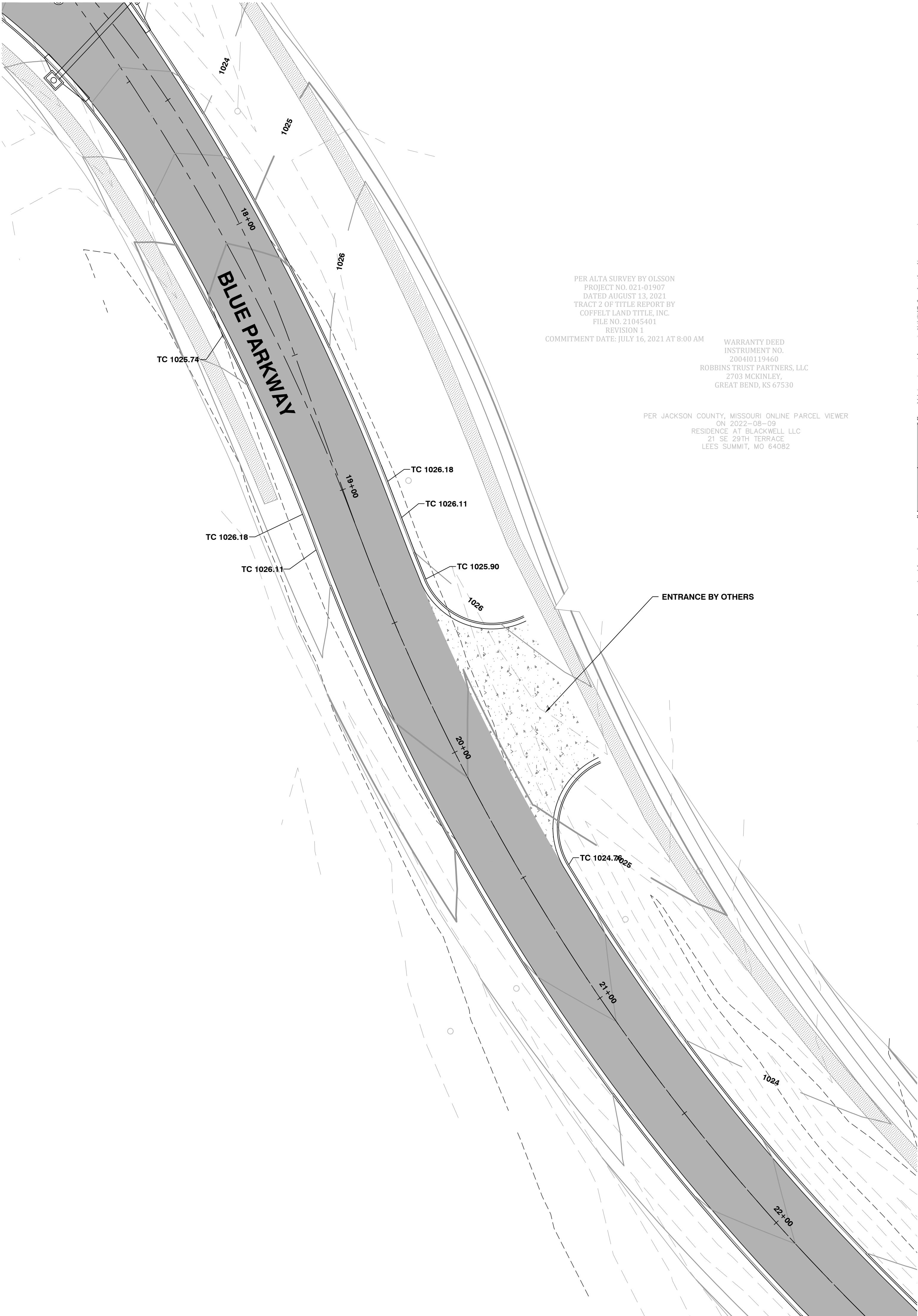
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

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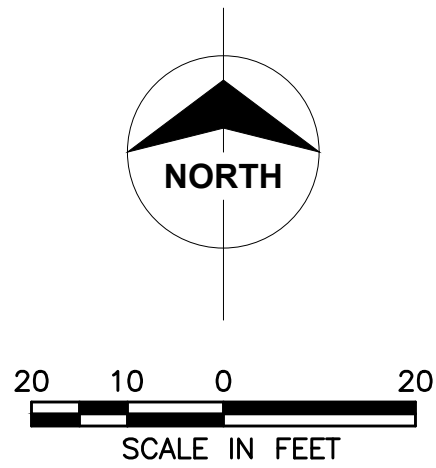
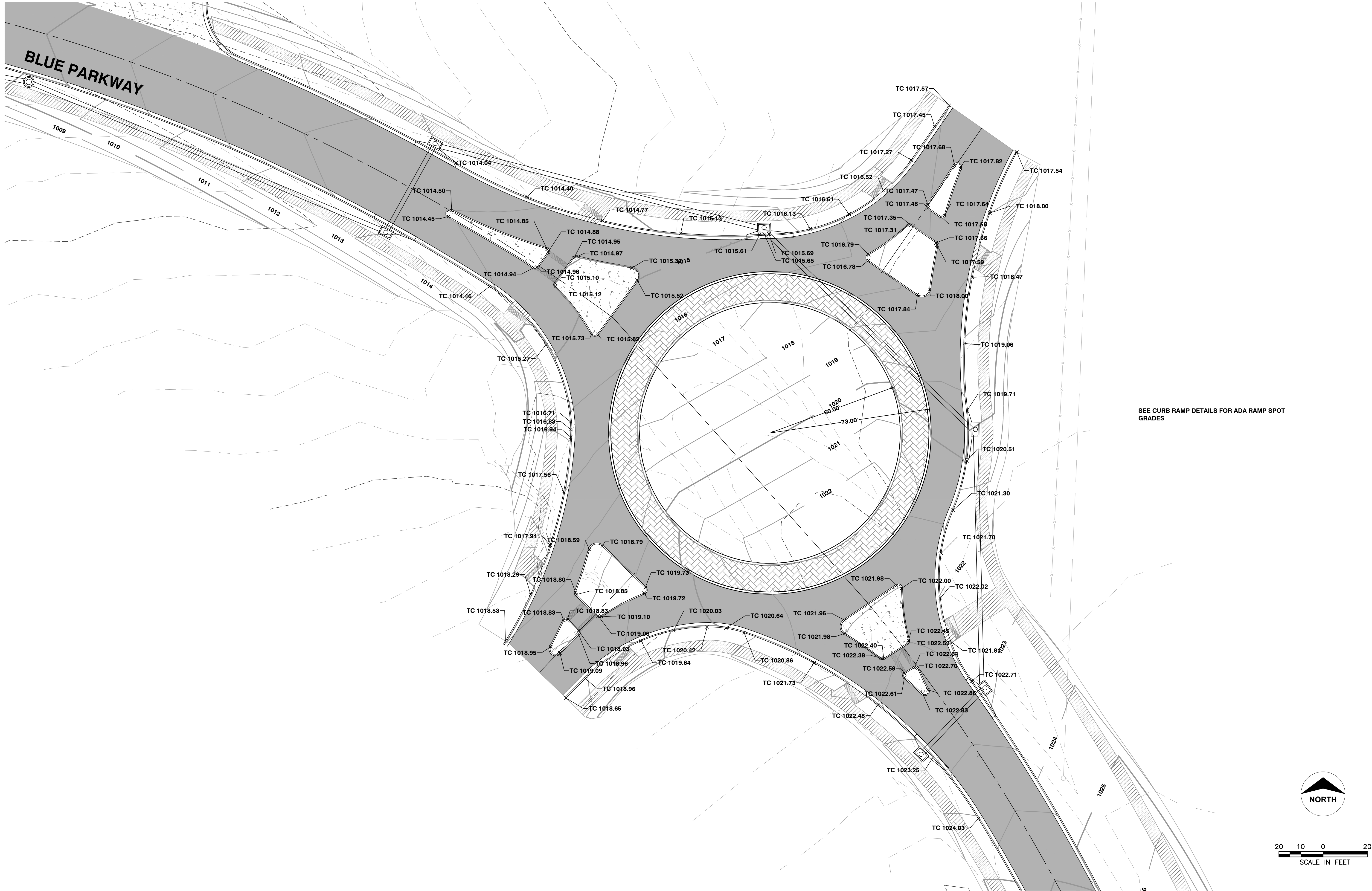
GRADING DETAILS

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Jadrienne S. Rodell-Tipton, PE
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PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

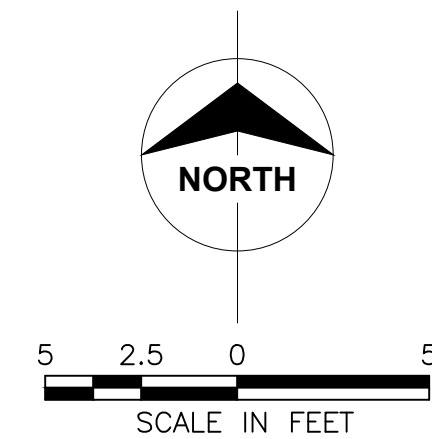
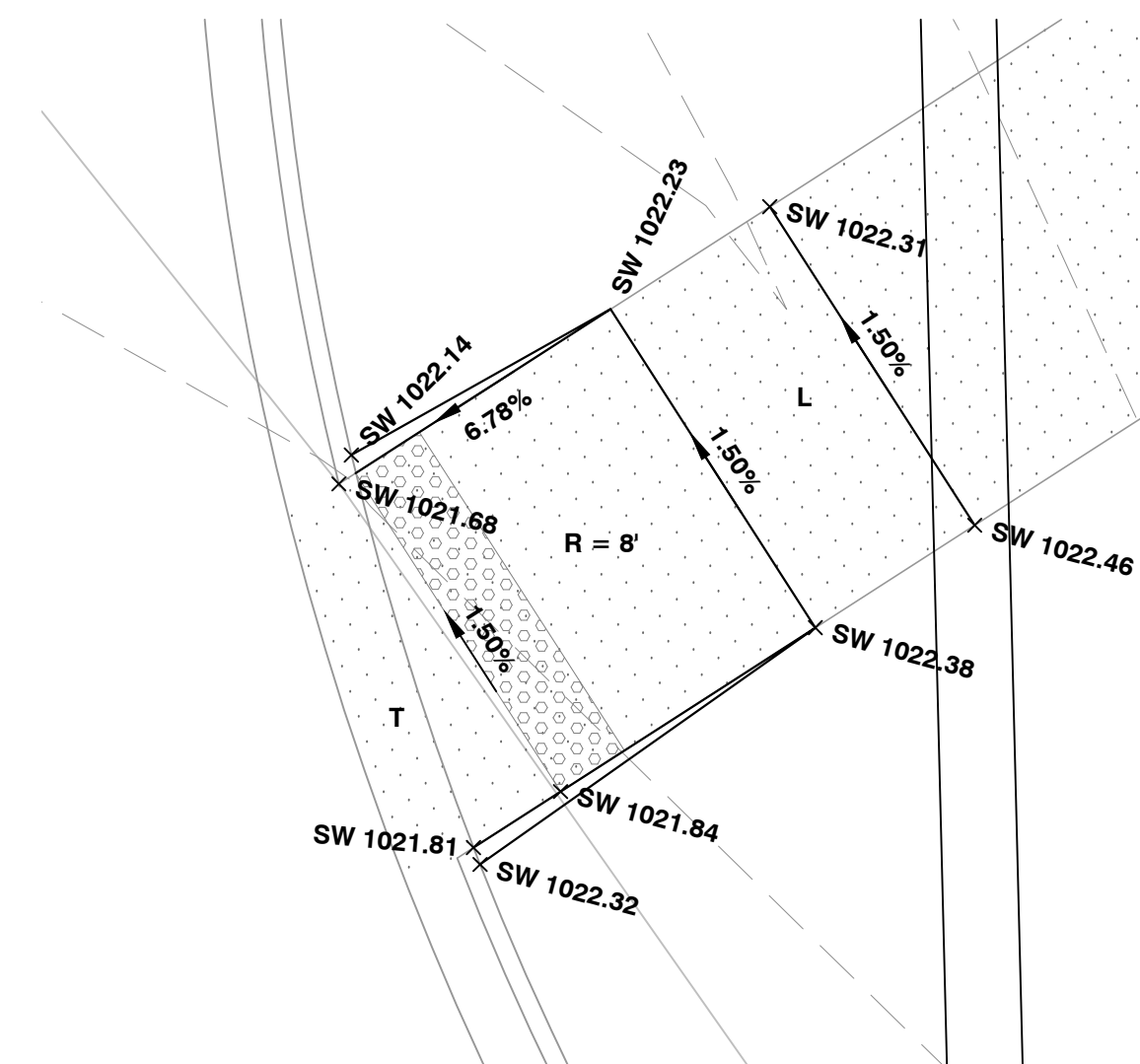
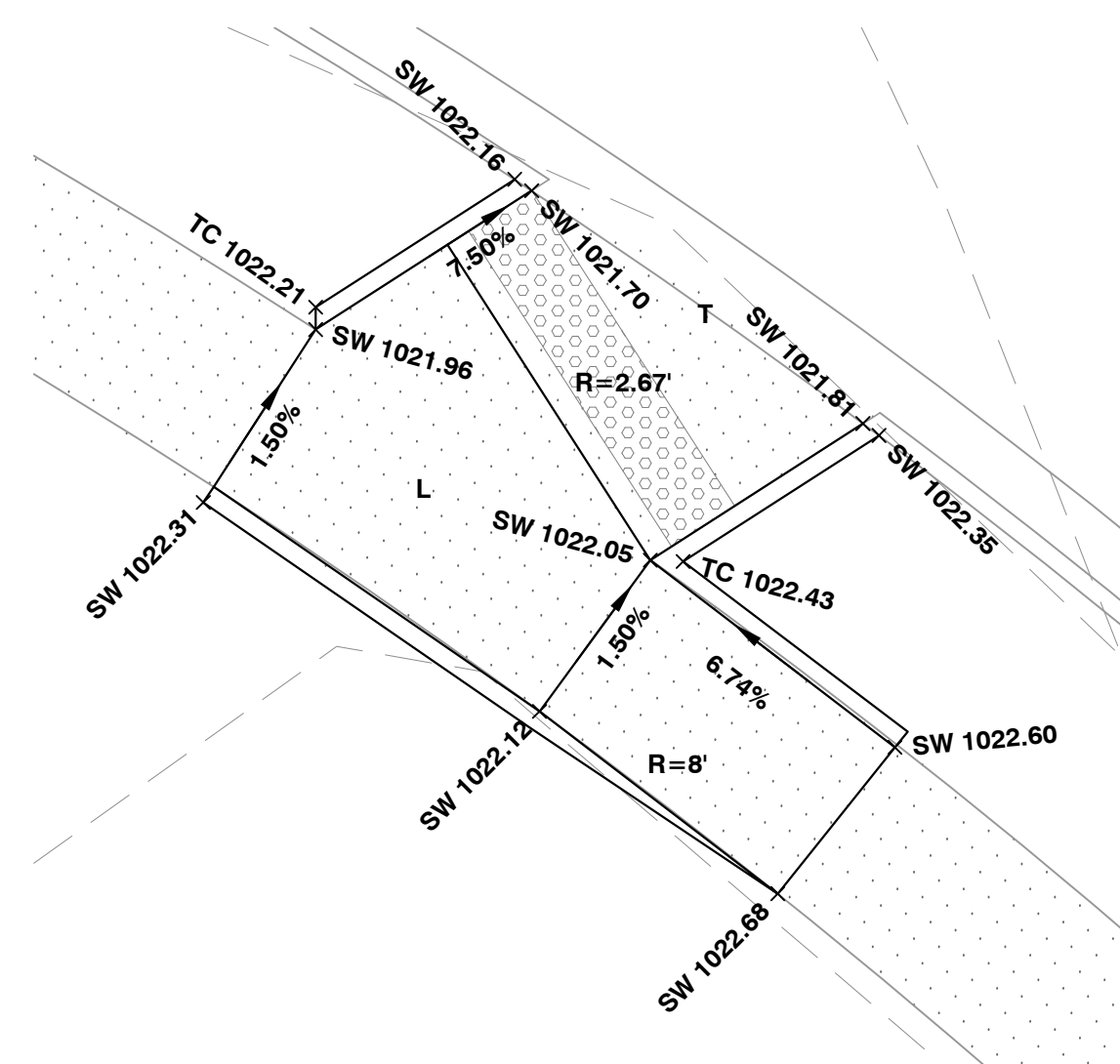
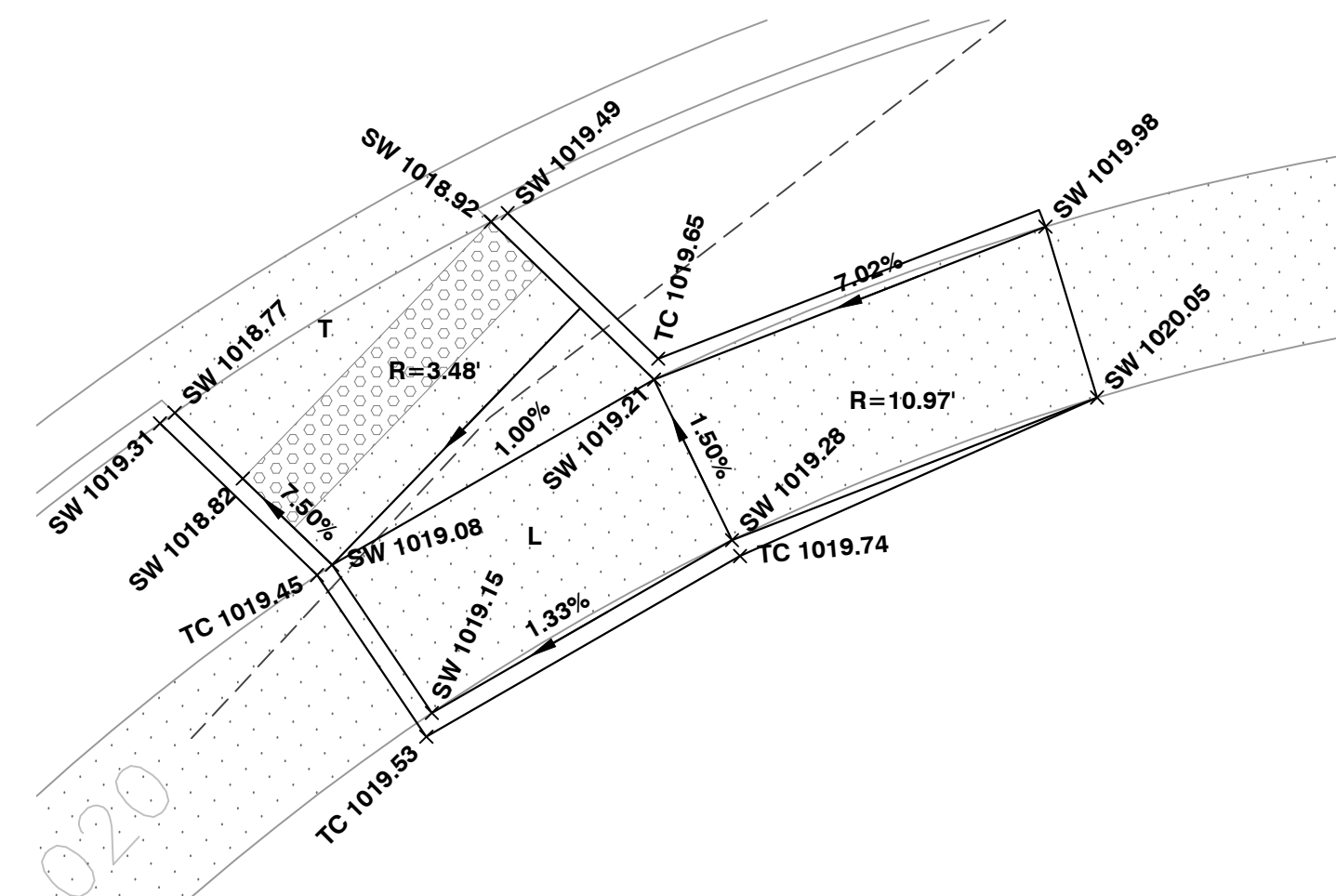
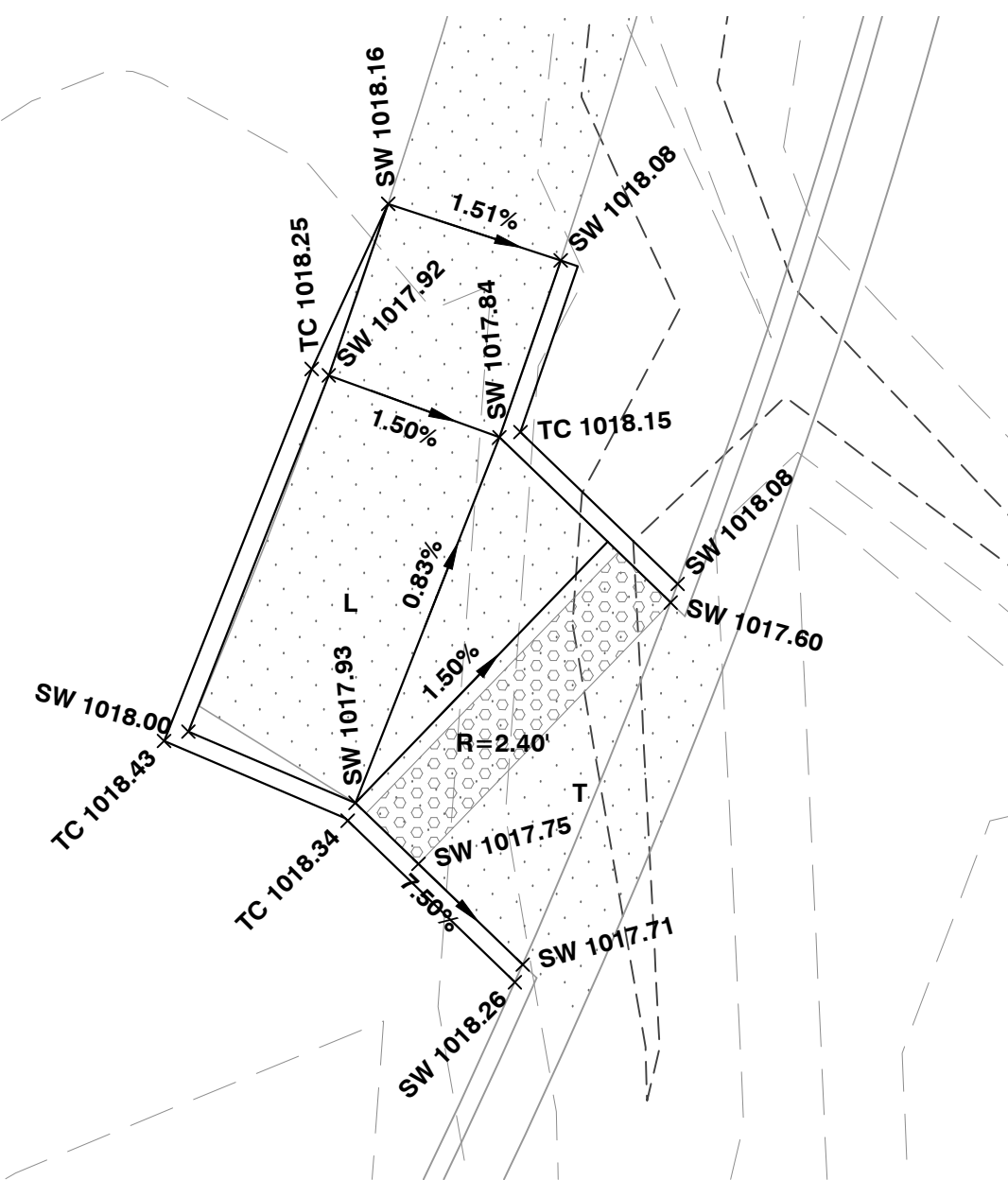
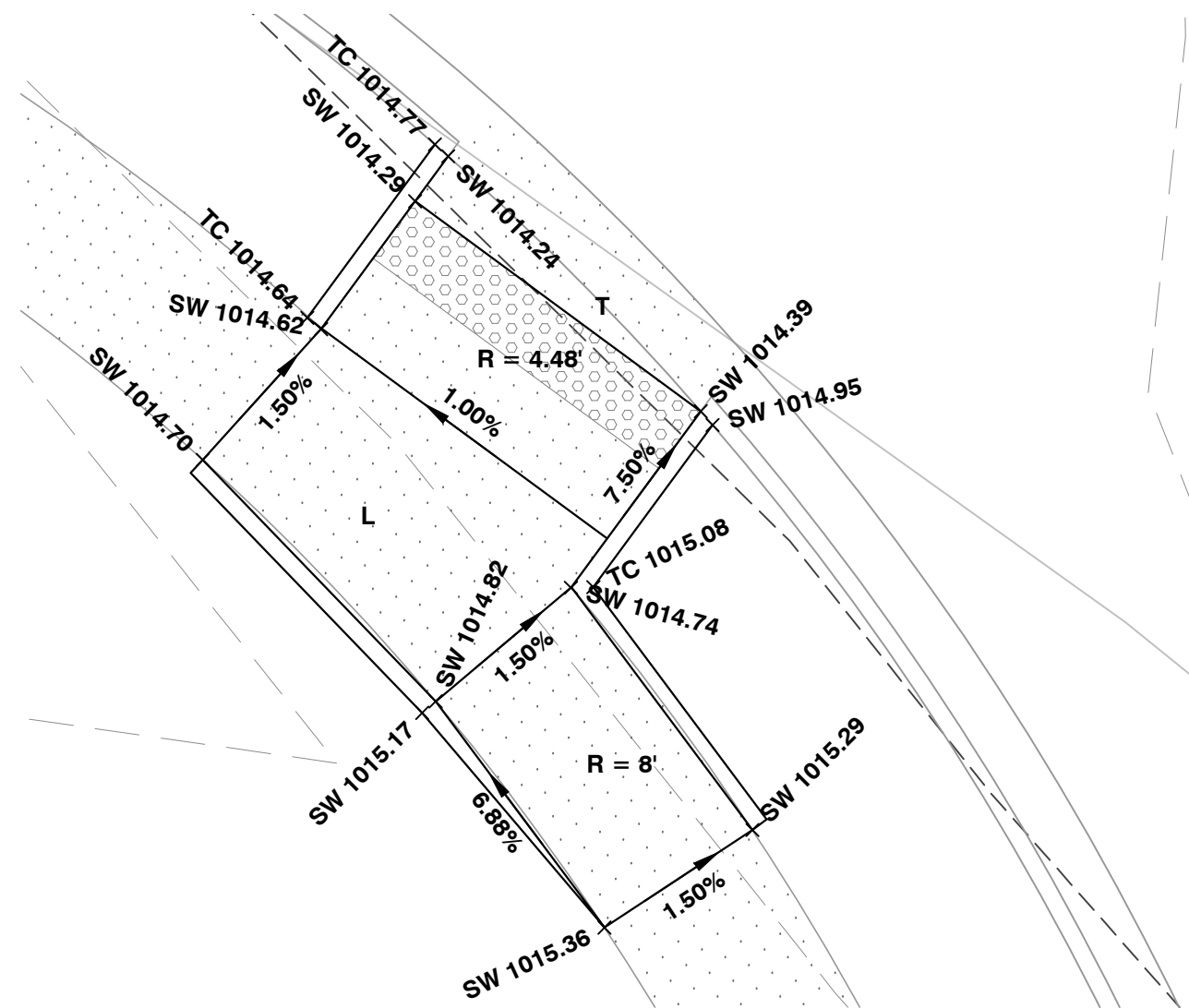
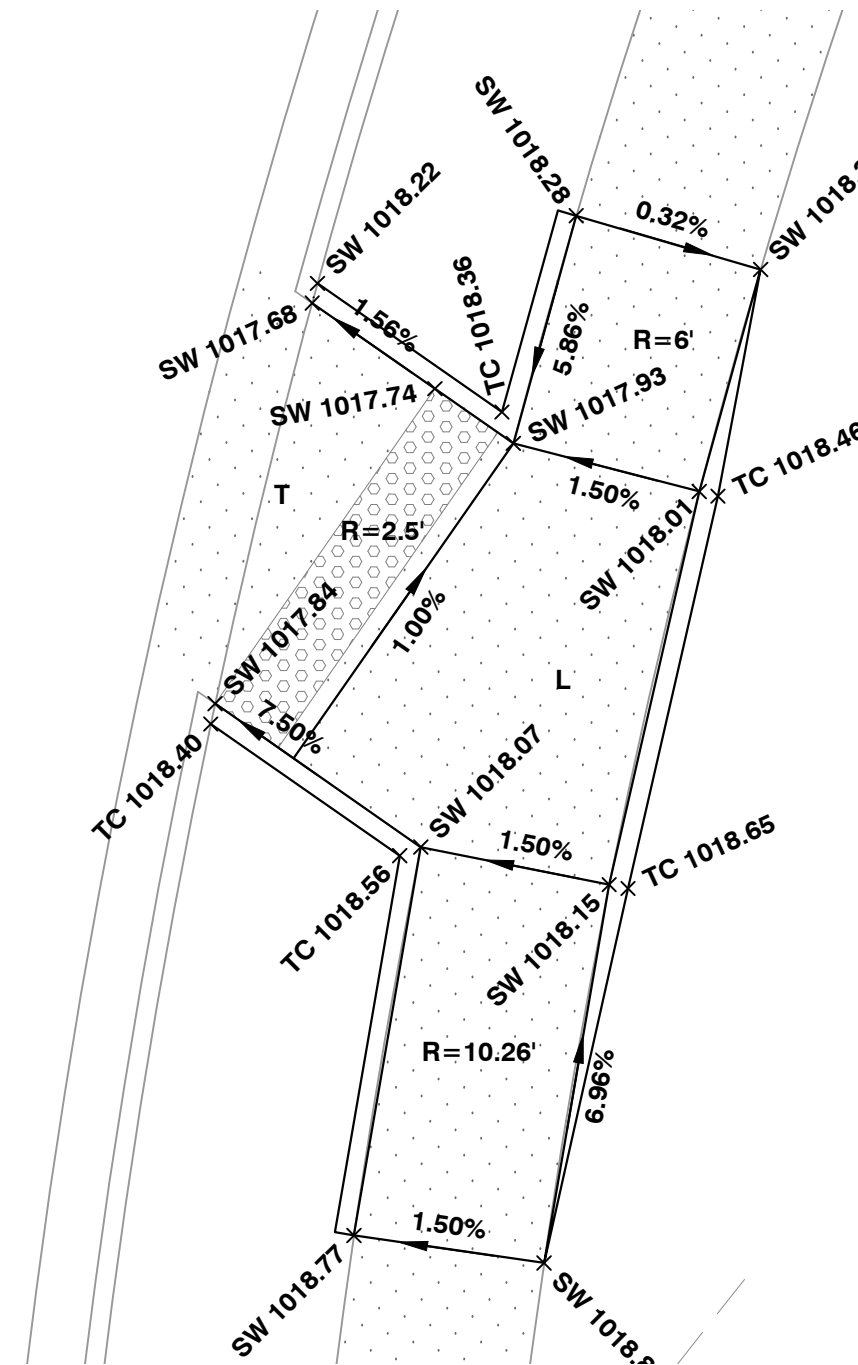
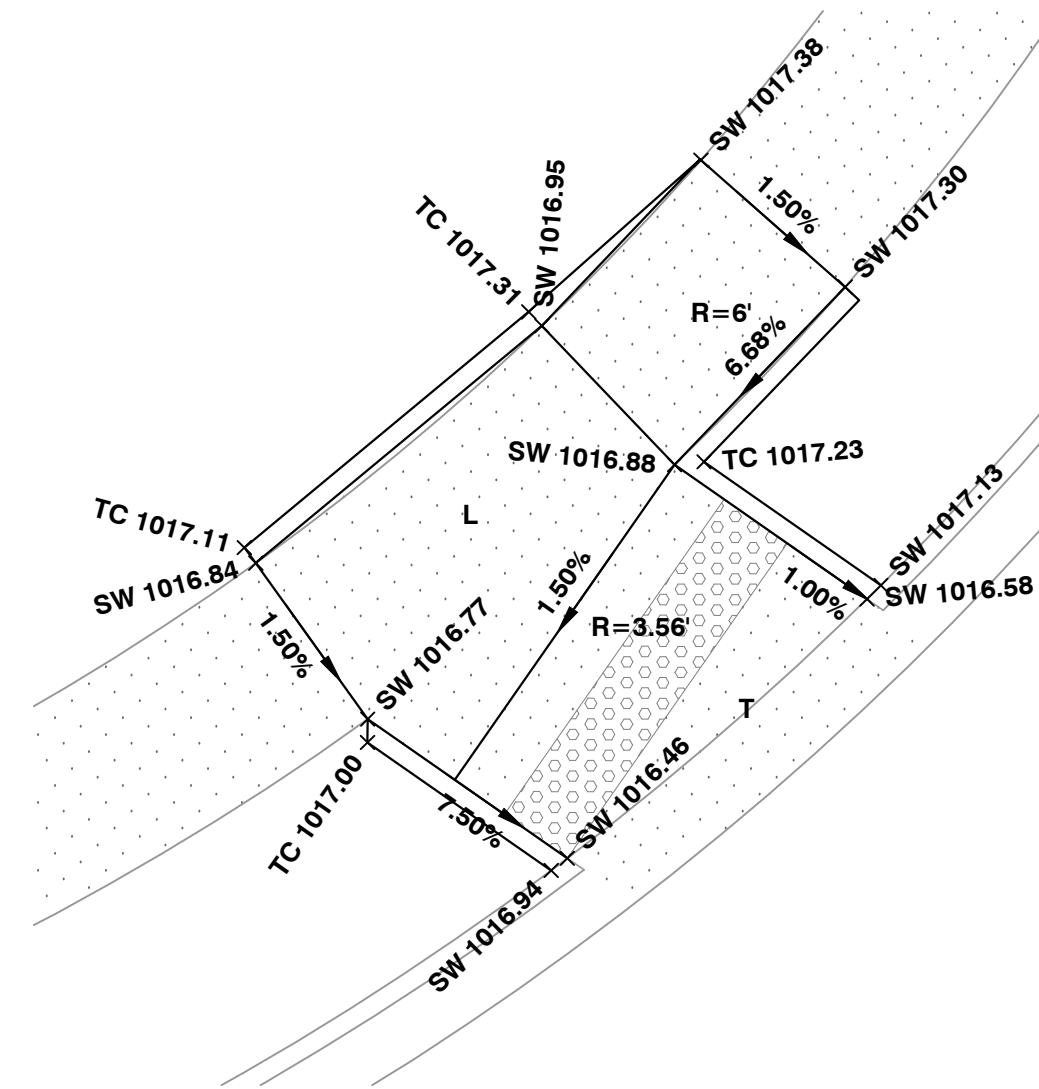
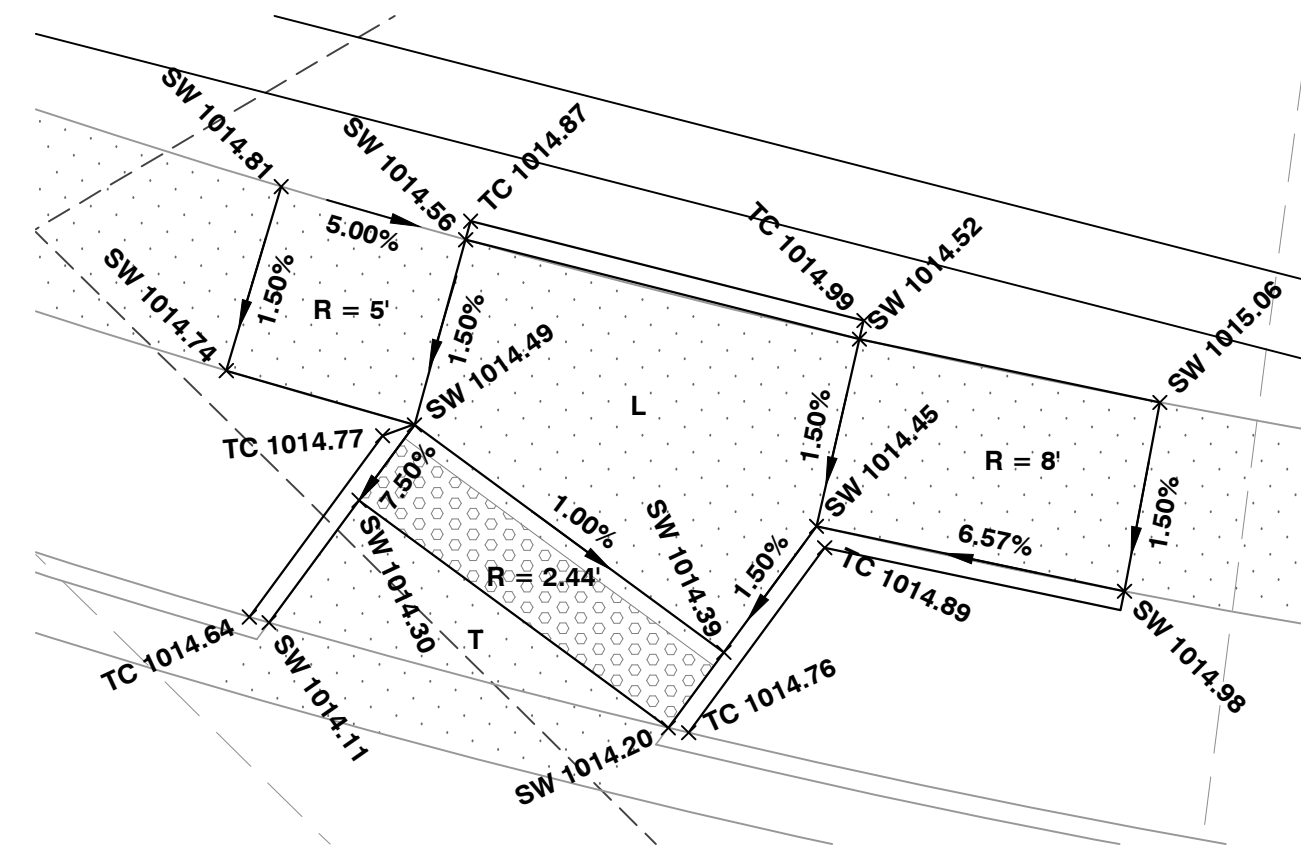
PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

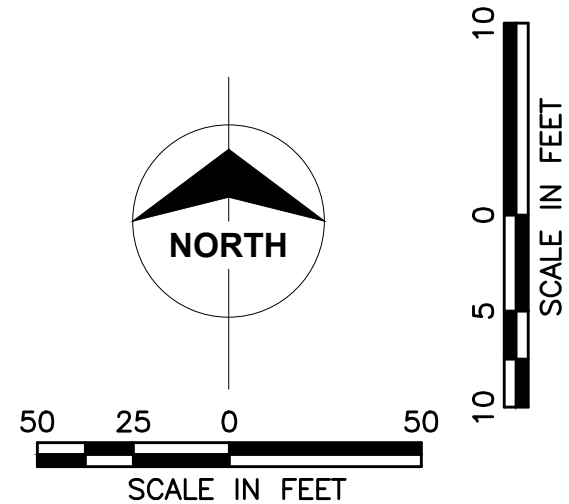
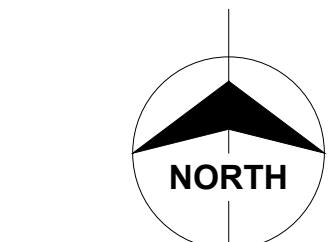
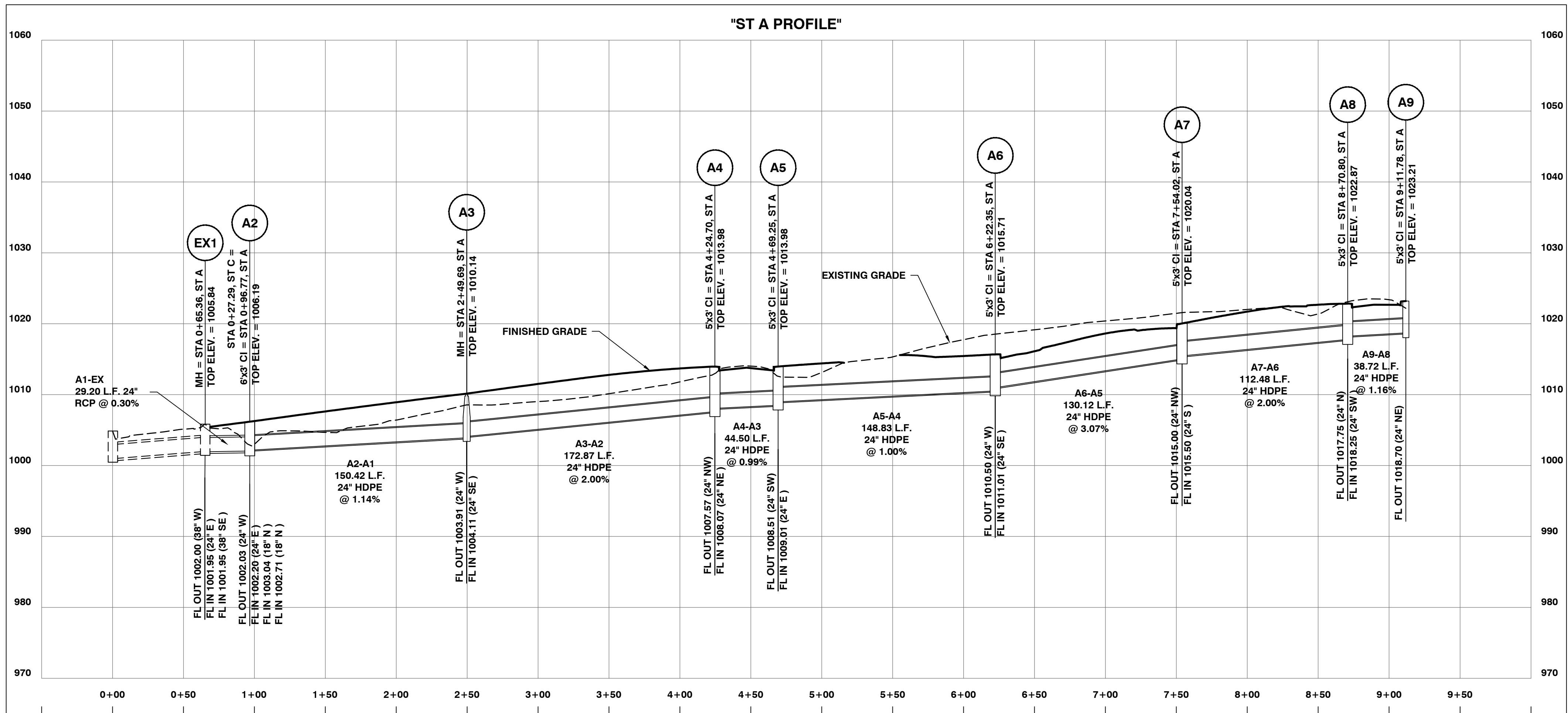
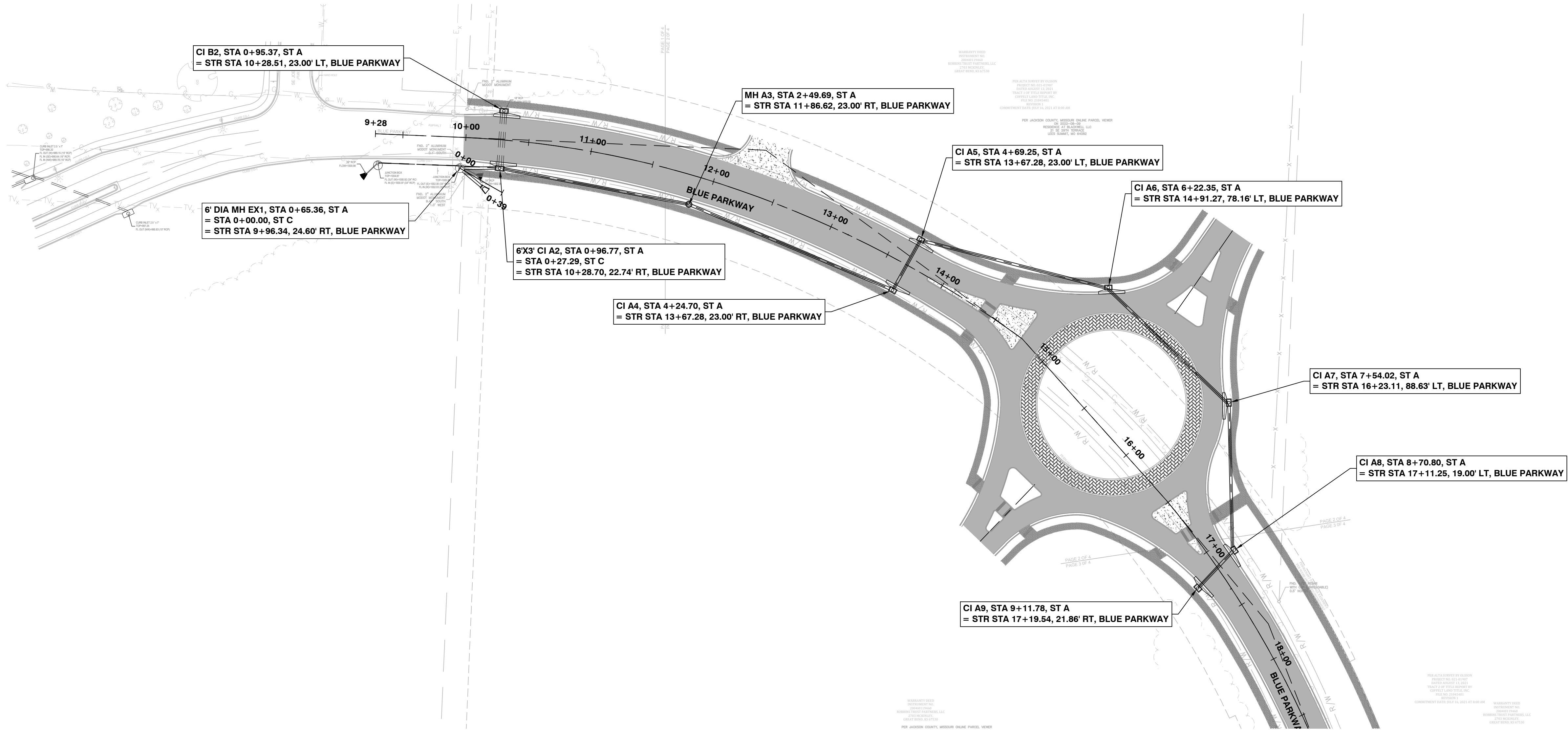
PRELIMINARY

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PE-2021032725 (MISSOURI #)
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PREPARED FOR:
GRIFFIN RILEY
PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

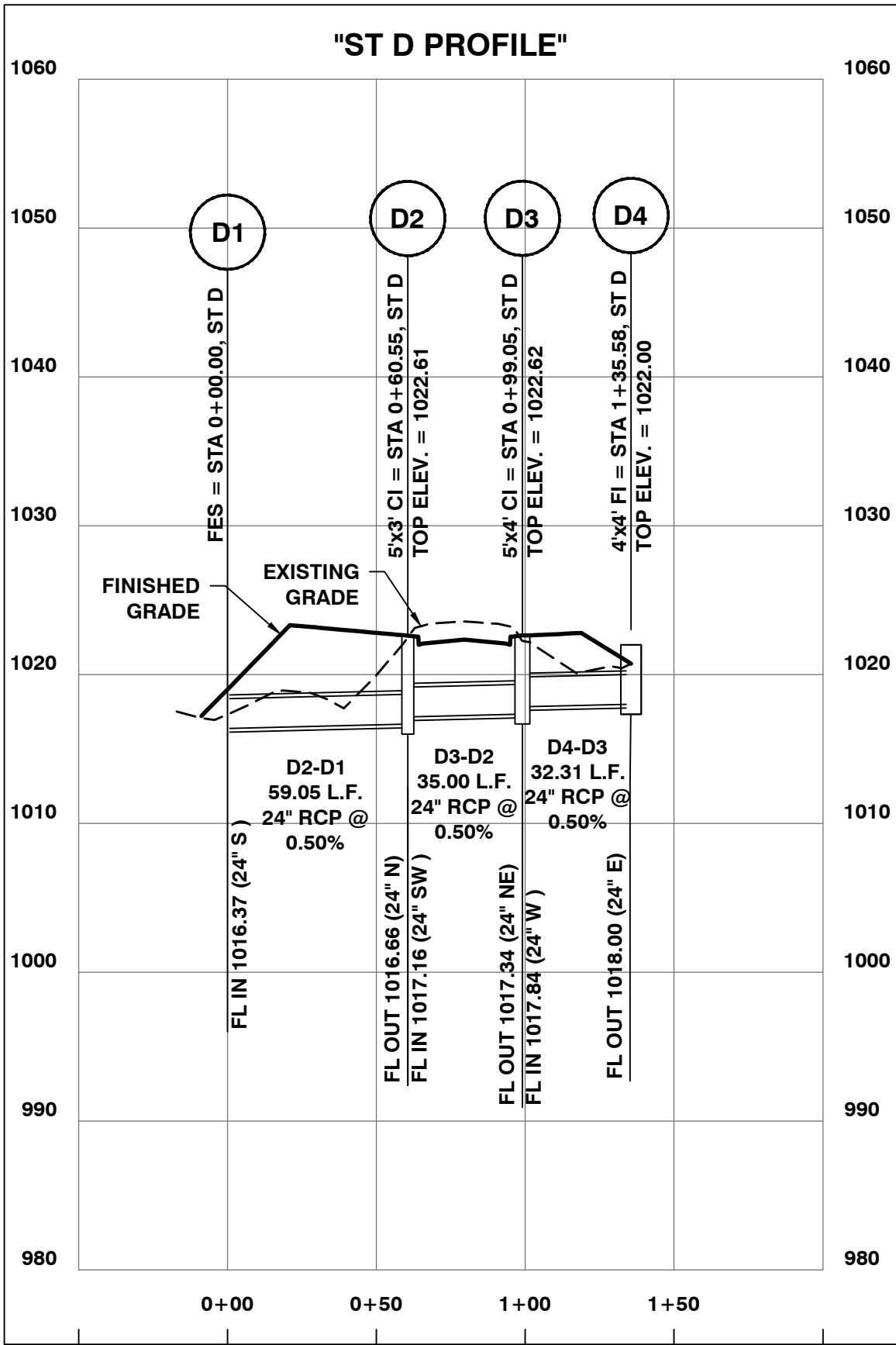
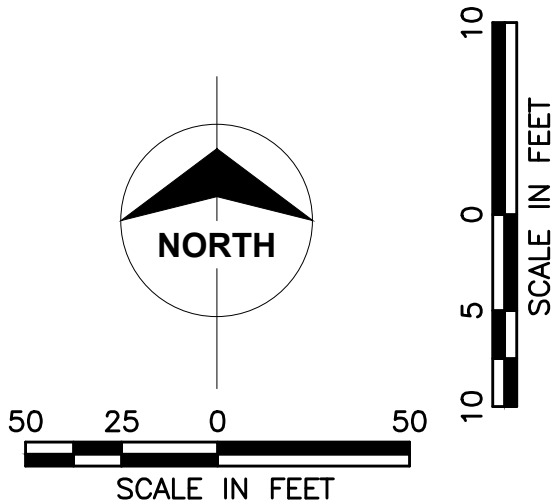
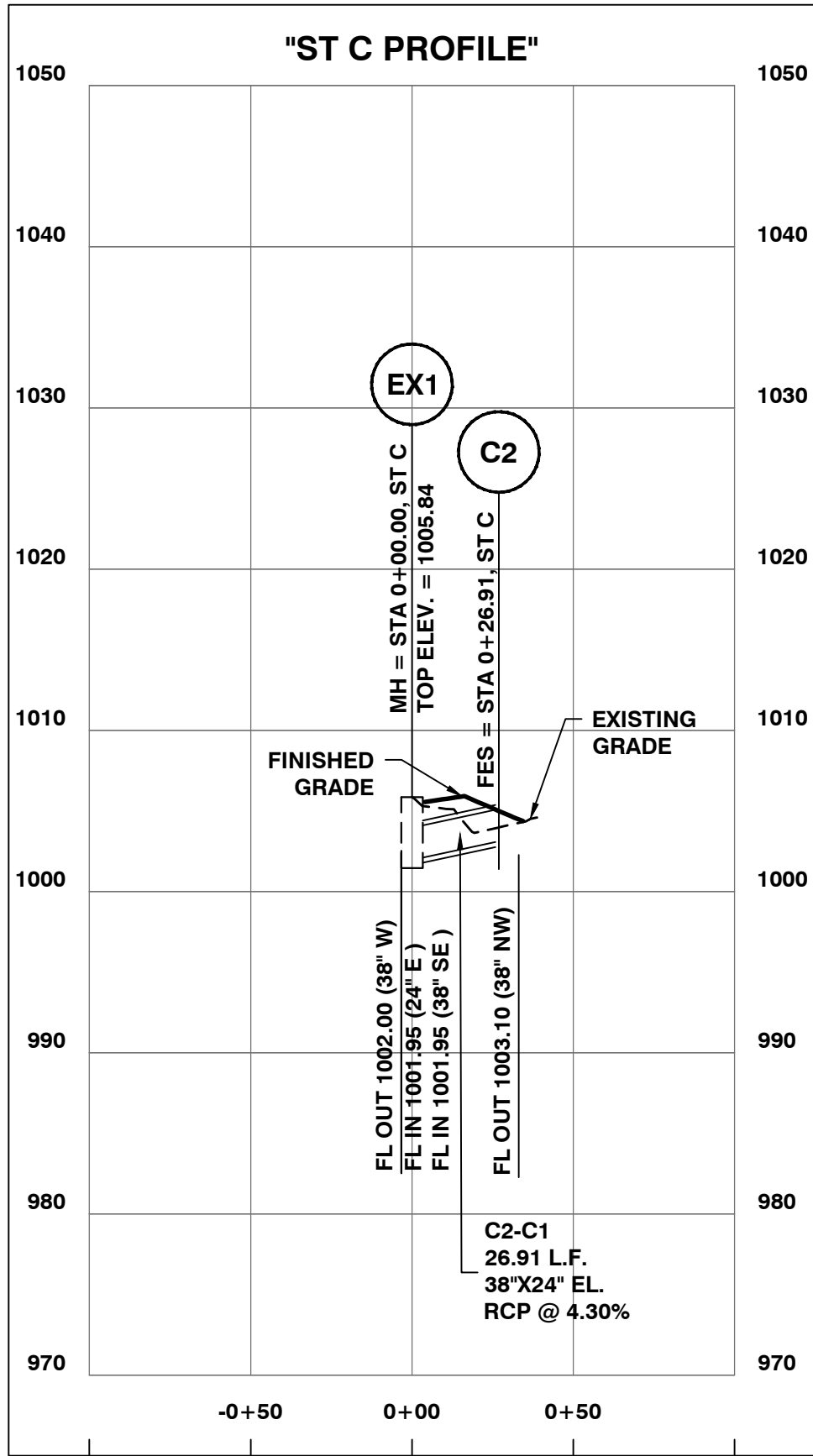
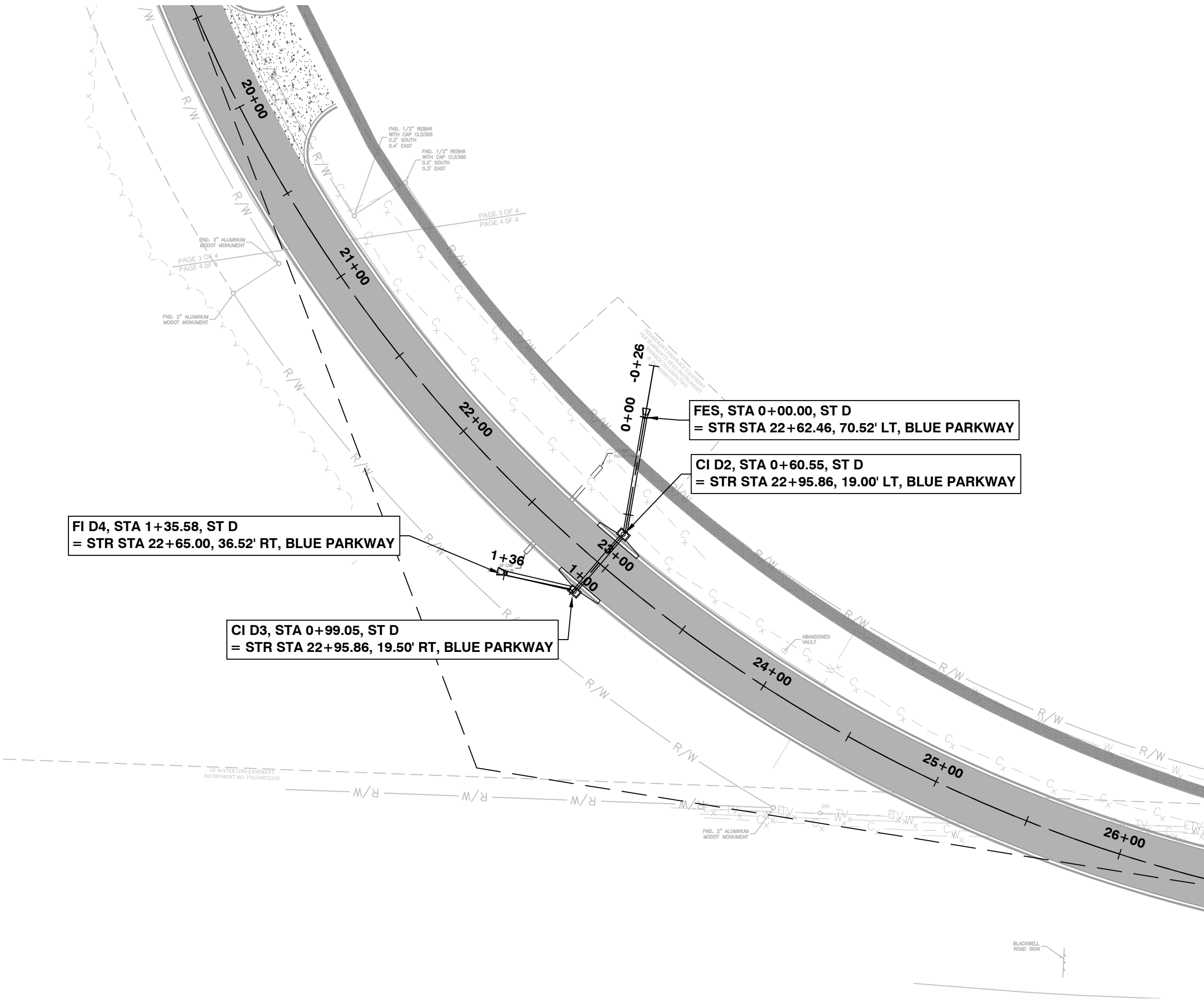
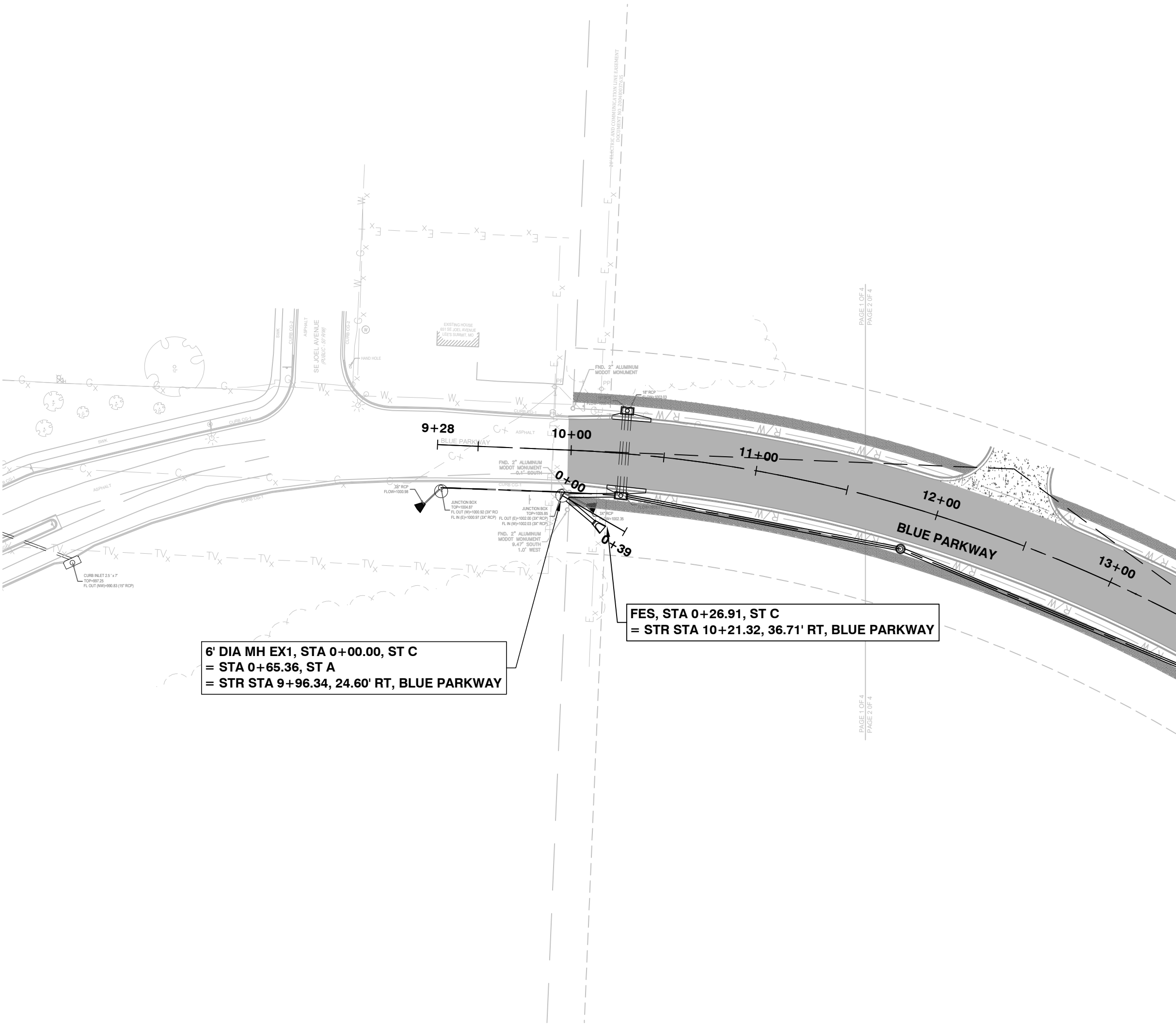
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

PRELIMINARY

STORM SEWER
PLAN & PROFILE

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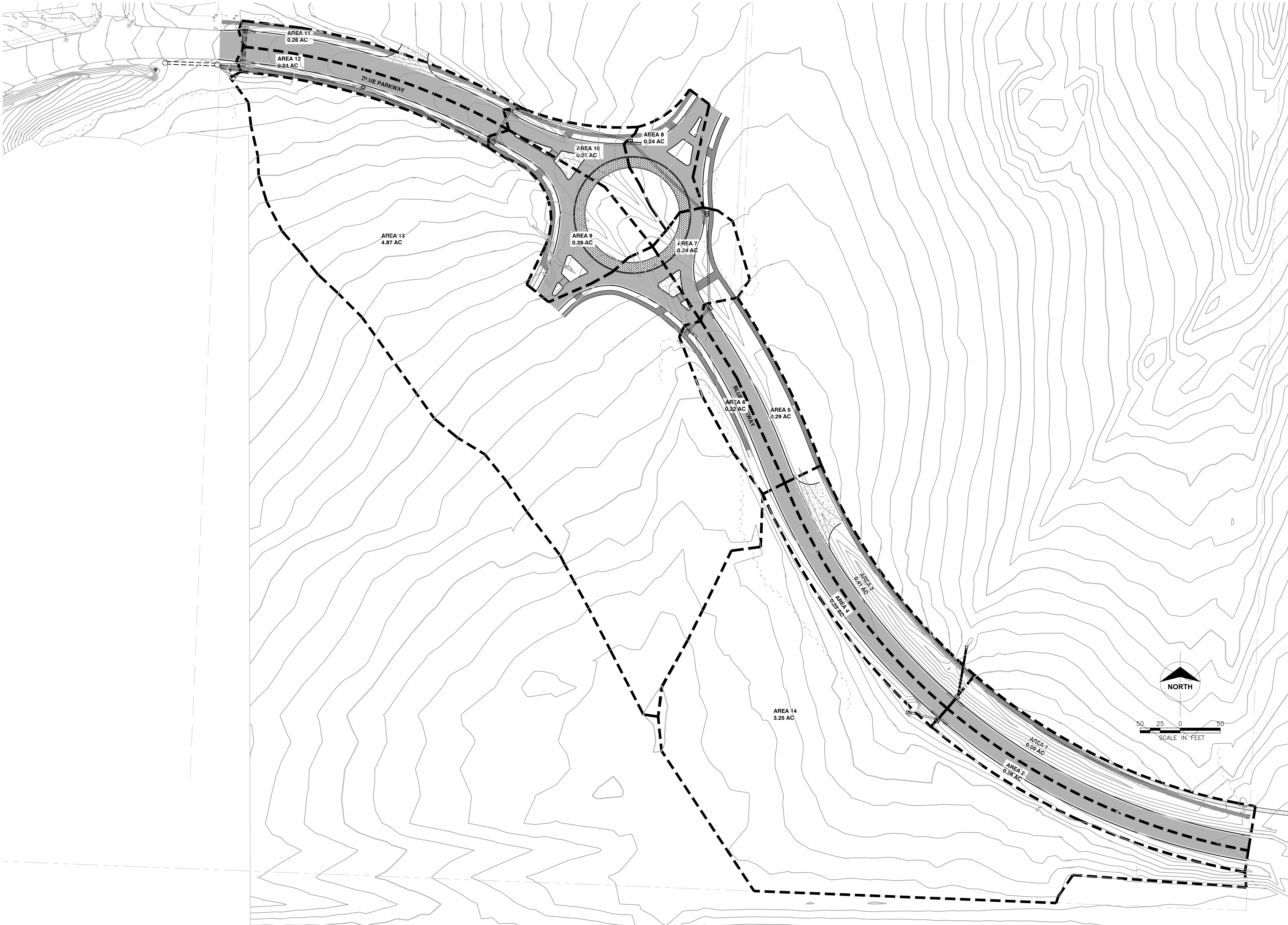
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PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

PRELIMINARY

STORM SEWER
PLAN & PROFILE

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Certificates of Authority
Architecture: MO 318 / KS 13
Engineering: MO 4 / KS 241
Land Surveying: MO 123 / KS 36

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PE-2021032725 (MISSOURI #)

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LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

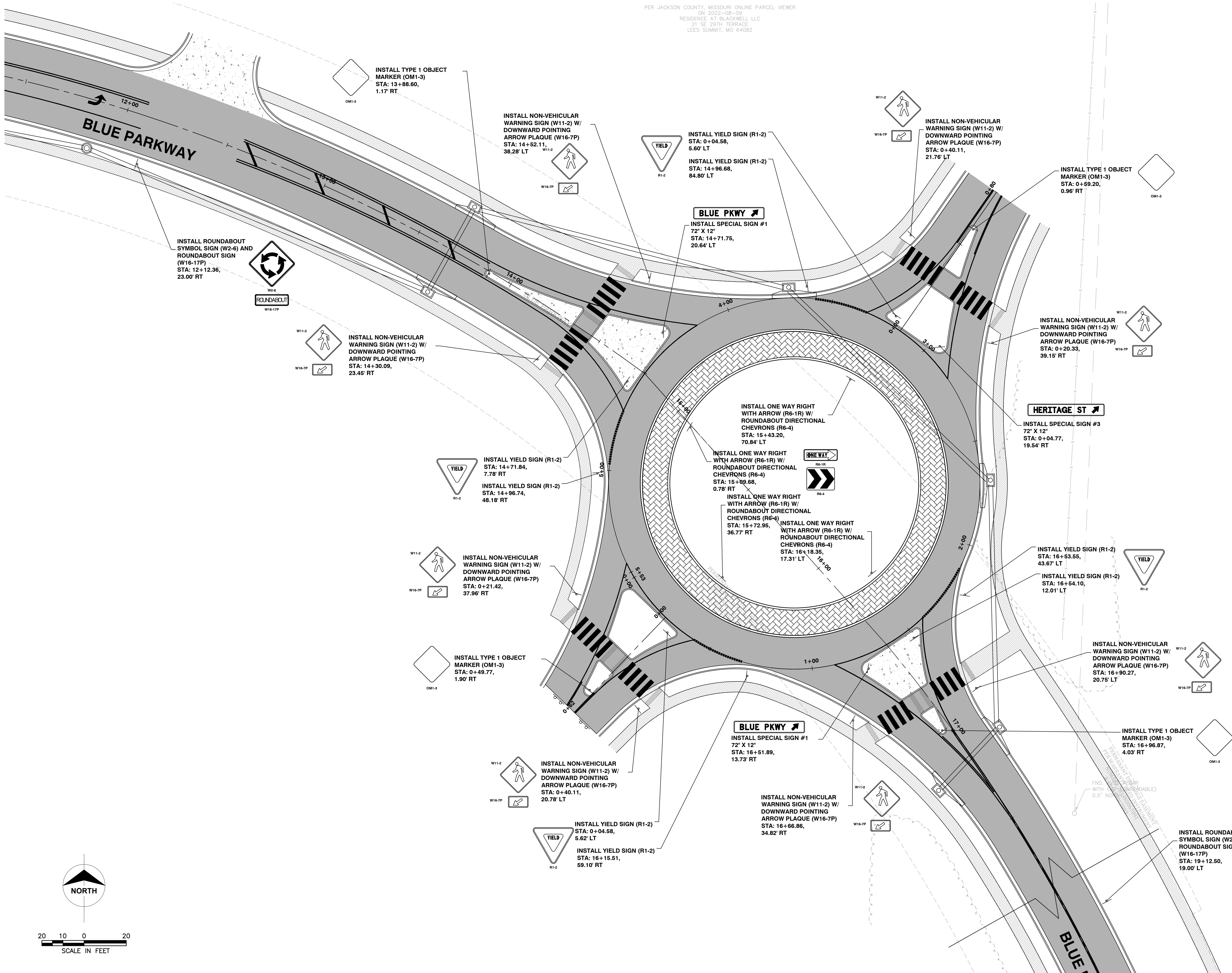
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PROJECT #: 22-2087
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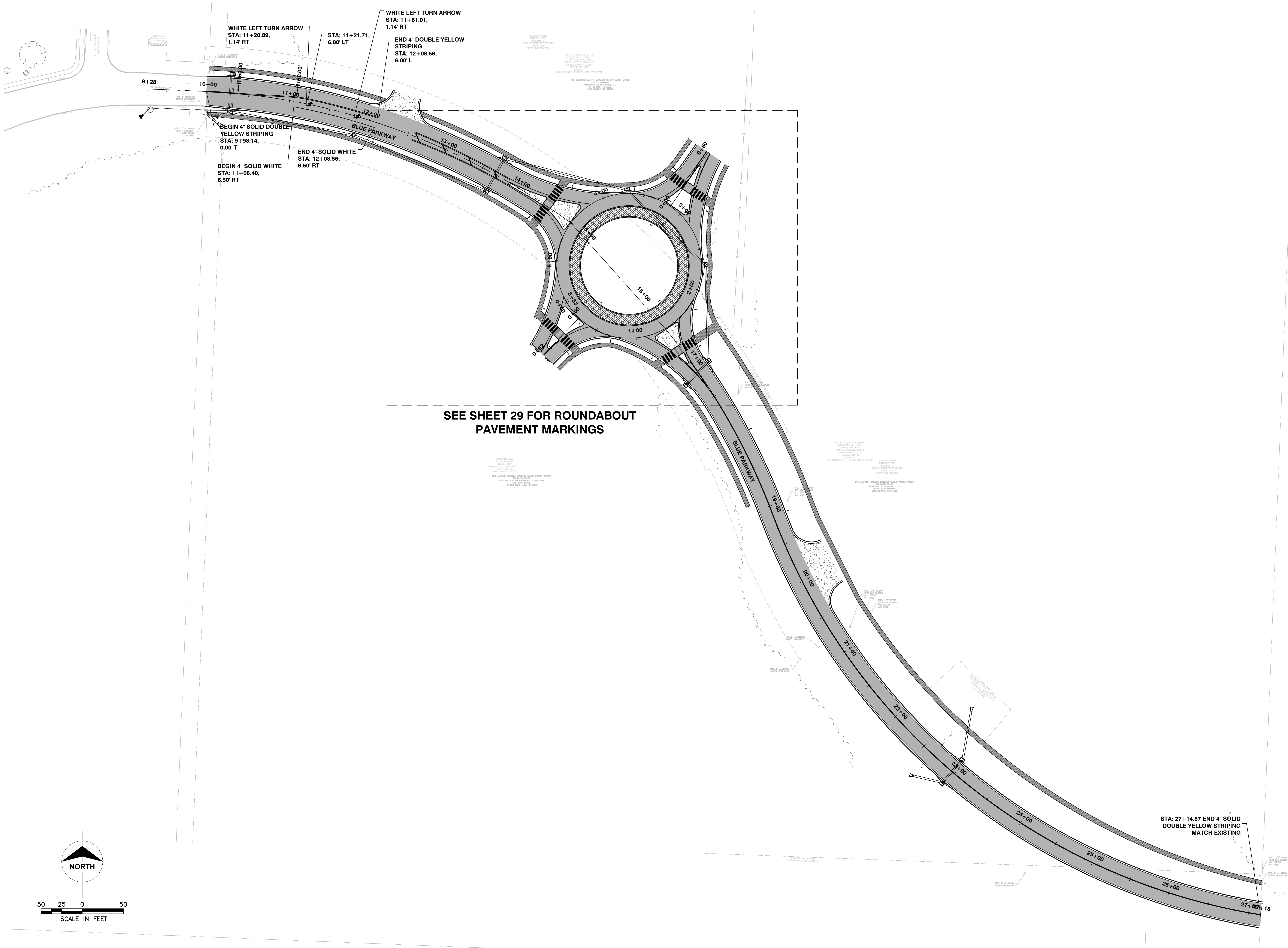
PRELIMINARY

STORM DRAINAGE
MAP

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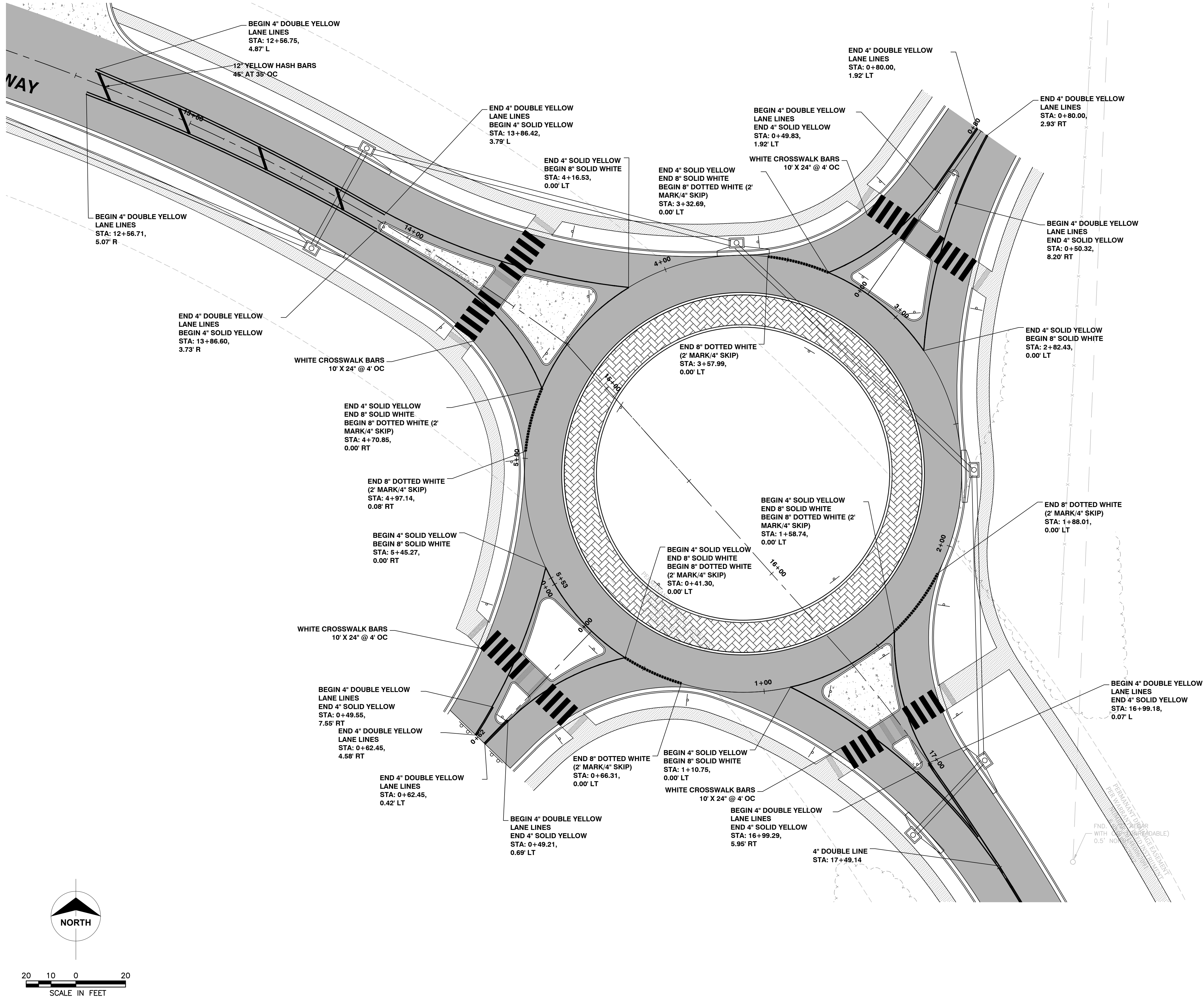
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PROJECT #: 22-2087
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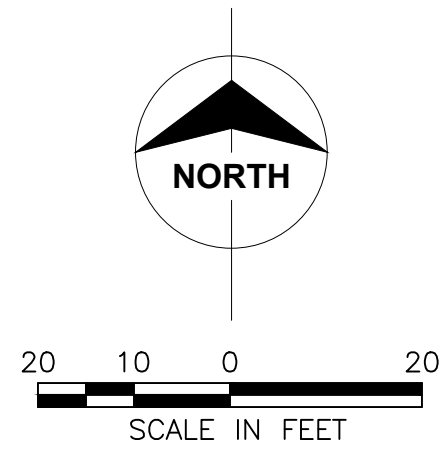
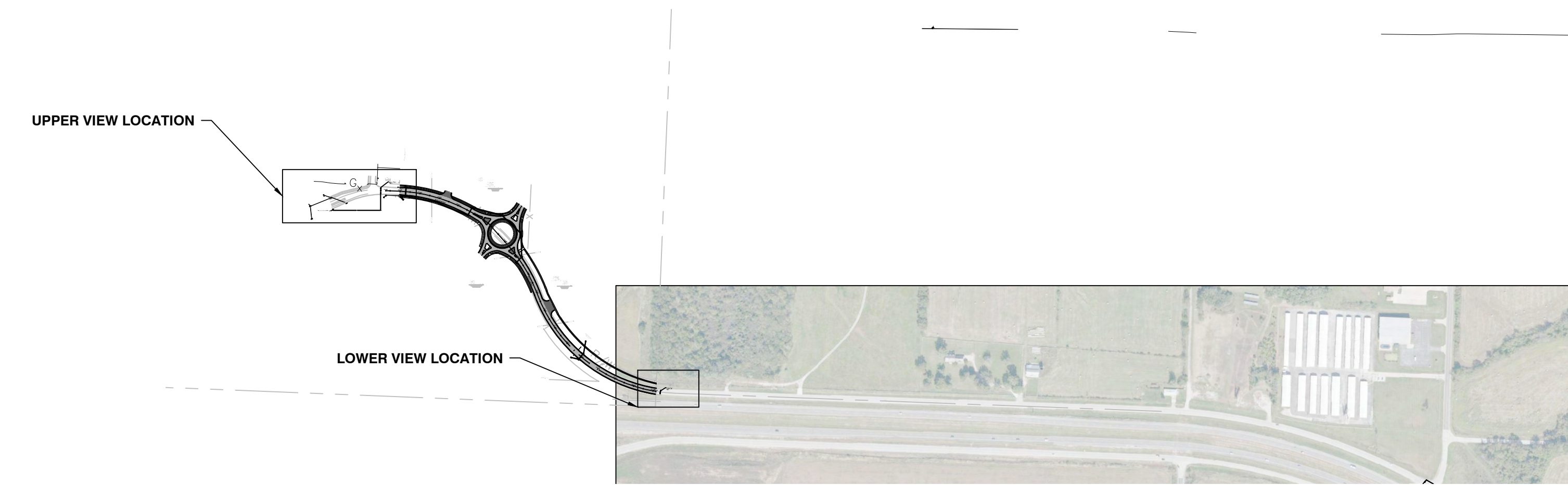
STRIPING PLAN

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PRELIMINARY

TEMPORARY TRAFFIC
CONTROL PLAN

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

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PE-2021032725 (MISSOURI #)

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PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

POWELL **CWM**
ARCHITECTURE/ENGINEERING/SURVEYING
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Certificates of Authority
Architecture: MO 318 / E 13
Engineering: MO 4 / KS 241
Land Surveying: MO 123 / KS 36

P:\Griffin Riley\22-2087 Blue Parkway\Civil\XXXX-XX-XX 100% CD\31 22-2087 Traffic Control Details.dwg, Traffic Control Plans (2), 11/28/2022 1:50:15 PM, DWG To PDF.pc3, ARCH full bleed D (24.00 x 36.00 inches)



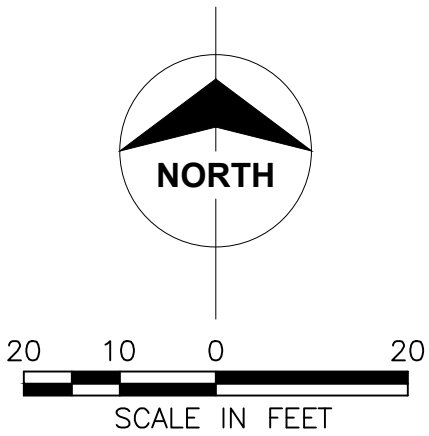
ROAD CLOSED TO THRU
TRAFFIC SIGN (R11-4)
MOUNTED ON MOVEABLE TYPE 3 BARRICADE

350.17'

INSTALL ROAD CLOSED AHEAD SIGN (W20-3)



VIEW LOCATION



REVISIONS
NO. DATE DESCRIPTION

1		
2		
3		
4		
5		
6		

PROJECT #: 22-2087
ISSUE DATE: 11/28/2022

PRELIMINARY

TRAFFIC CONTROL
PLAN

Jadrienne S. Rodell-Tipton, PE
PE-2021032725 (MISSOURI #)

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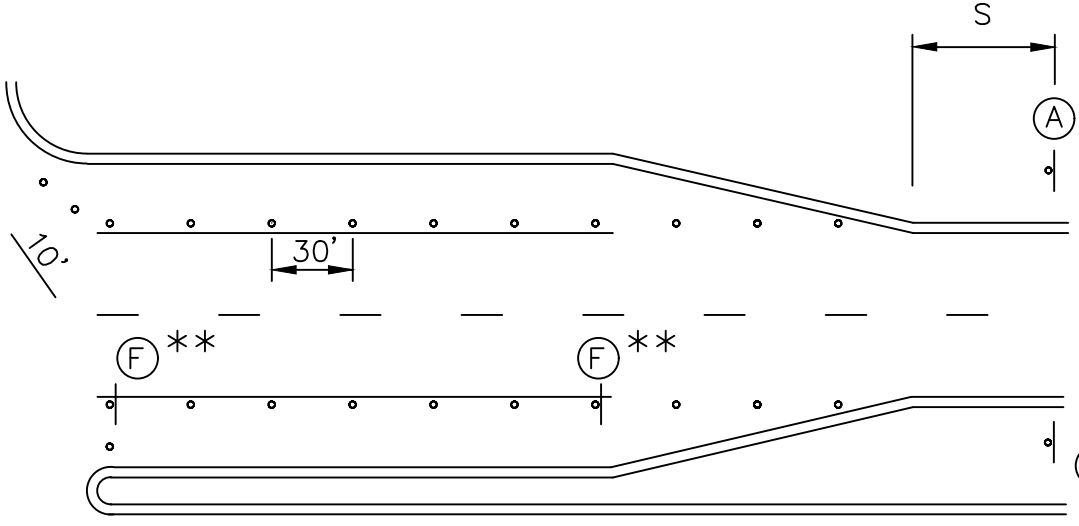
PREPARED FOR:
GRIFFIN RILEY
PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

Certificates of Authority
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Engineering: MO 4 / KS 241
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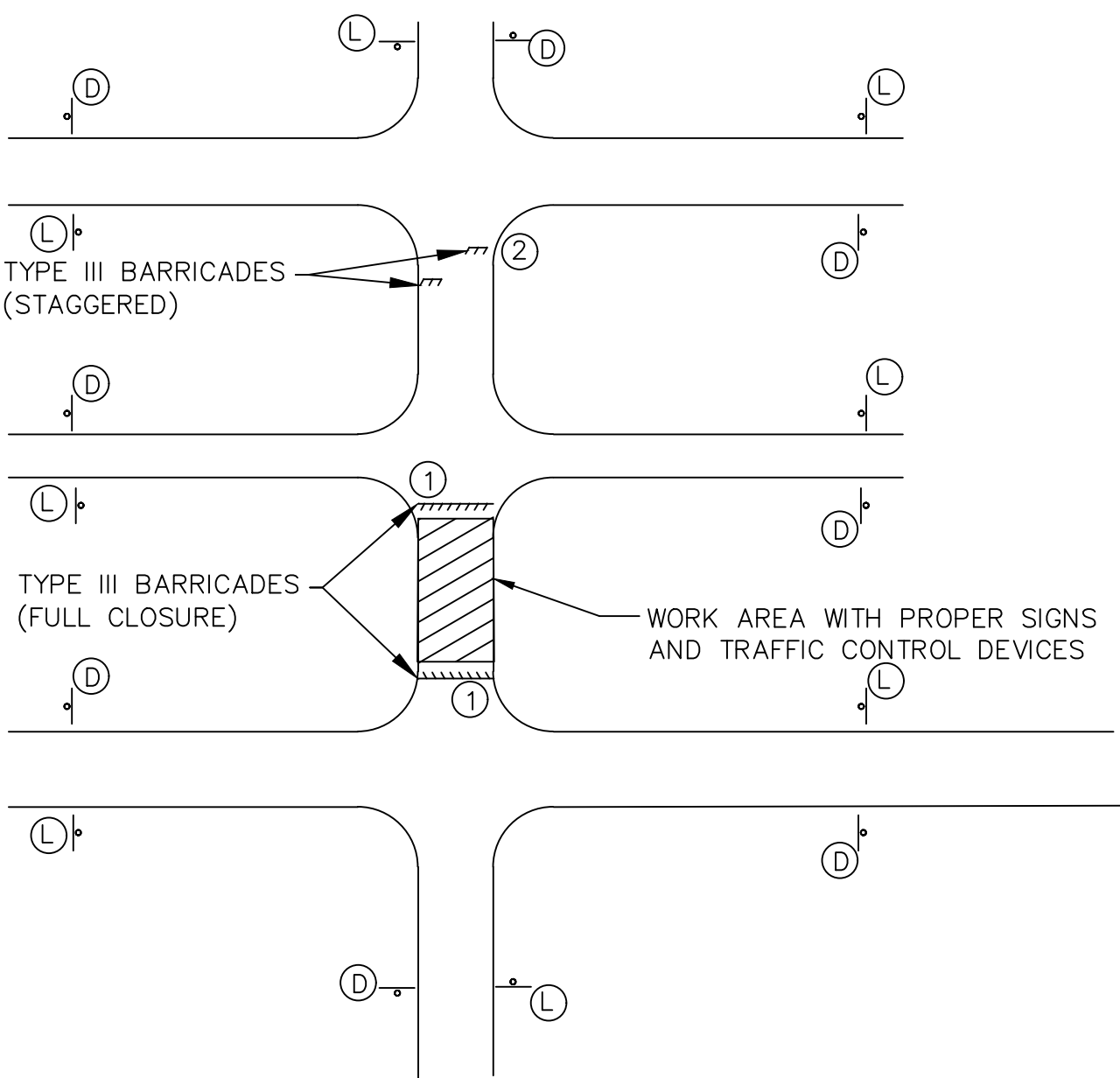
GENERAL NOTES:

- All signs, barricades, drums, markings and other traffic control devices shall conform to the Manual on Uniform Traffic Control Devices (M.U.T.C.D.), most current edition.
- All traffic control devices shall be standard in size, shape, color and message, in good condition, and reflectorized. All signs and barricades in place at night shall be reflectorized with high intensity sheeting. All signs shall be securely mounted with height and lateral location as described in the M.U.T.C.D.
- Warning lights should be used on all channelization devices and warning signs per M.U.T.C.D. Type A Low-Intensity Flashing Warning lights should be mounted on warning signs as well as barricades where used singly to warn users of potentially hazardous areas during nighttime hours. Type B High-Intensity Flashing warning lights should be used during day and nighttime operations to increase awareness of potentially hazardous areas. Type C Steady-Burn lights should be mounted on barricades where used in series to delineate the edge of the travel way.
- Advance Warning Arrow Panels shall be used for all lane closures on multi-lane streets but should not be used in lieu of proper traffic control signs, barricades and channelization devices.
- Flaggers shall be used where indicated on the plans, where construction vehicles interact with normal traffic or where construction activities impose a restriction on traffic, as directed by the Engineer in charge of construction. Where flaggers are used, advance signing shall be erected as shown above or as specified in the M.U.T.C.D. Flaggers shall meet the requirements in the M.U.T.C.D. in regard to character, training, attire and behavior.
- Channelization devices used in the street shall be either drums, cones or Trimline channelizers.
- Traffic control devices not in use or not applicable shall be either covered or removed from the work area.
- The Contractor shall place as many barricades as needed to effectively shield pedestrians and traffic from exposed objects, excavations and construction activities. ADA Pedestrian Barricades shall be installed around all excavations left open during non-construction times or as directed by the Engineer in charge of construction.
- Access shall be maintained to all driveways and side streets unless noted otherwise on the plans.
- Construction materials shall be kept off of sidewalks, consolidated in one location within City Right-Of-Way, and removed daily unless otherwise approved by the Engineer in charge of construction.
- Dirt, mud and other construction debris on streets and sidewalks shall be removed immediately.
- The Contractor shall not perform any work that will restrict traffic in any way between the hours of 7:00 a.m. and 8:30 a.m. or 4:30 p.m. and 6:00 p.m. unless prior approval has been given.
- The Contractor shall be responsible for maintaining all traffic control devices on an around-the-clock basis, whether or not work is actively being pursued and any deficiencies noted shall be corrected immediately.
- The traffic control requirements shown on these plans are minimum requirements only and do not attempt to address in depth the variety of situations that may occur once construction has started. In no way do the requirements shown on these plans relieve the contractor of his responsibility for selecting the proper traffic control devices and implementation procedures that will assure the safety of motorist, pedestrians, and workers at all times. Any additional quantities of traffic control devices necessary to complete the contract or as ordered by the Engineer shall be considered subsidiary to the contract bid price.
- Construction vehicles parked along streets and construction signs shall not restrict sight distance for vehicles exiting at streets or any drives.
- Should the contractor fail to enforce the traffic control plan or fail to clean, replace or otherwise maintain the traffic control devices when directed to do so by the Engineer or representative, the City/Controlling Jurisdiction may take one or more of the following actions:
 - Employ another agency to correct deficiencies in signing or warning devices and deduct the cost from the contractor's pay estimate
 - Suspend all pay Estimates until deficiencies are corrected.
 - Stop the work until deficiencies are corrected.
 - Place the contractor in default.
- The Contractor shall keep roadway closures due to construction activities to a minimum and provide advanced notice to affected users.
- The Contractor shall notify enforcement and emergency agencies before the start of construction to request their cooperation and to provide coordination of services when emergencies arise during the construction at the project site. When the contractor completes this notification with enforcement and emergency agencies, a report shall be furnished to the engineer on the status of incident management.
- The Contractor should, at the request of the Engineer, place reduced speed limit signing and work zone speed limit signing in advance of all active work zone areas. Work zone speed limit reduction shall not be greater than 10 mph throughout construction areas without advanced signing in place. Contractor shall cover or replace all speed limit signs in conflict with the work zone speed limit throughout the construction area.

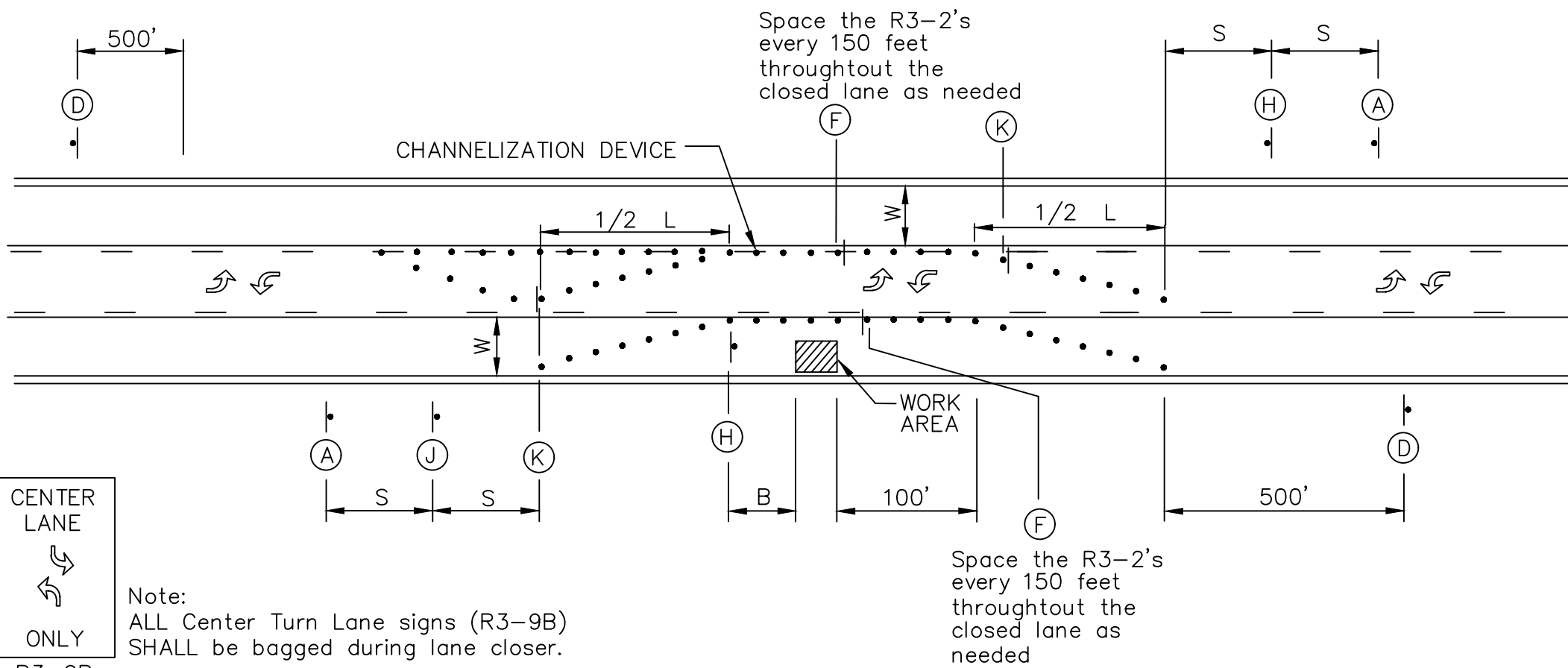


** USE ONLY AS APPROVED BY CITY TRAFFIC ENGINEER

DETAIL 3: TYPICAL RIGHT-TURN LANE AND LEFT-TURN LANE CLOSURES



DETAIL 4: TYPICAL SIDE ROAD SIGNING FOR ROAD CLOSURE



DETAIL 6: TYPICAL LANE CLOSURE - THREE-LANE STREET

Guidelines for length of longitudinal buffer space (B)	
Speed* (mph)	Length (Feet)
20	35
25	55
30	85
35	120
40	170
45	220
50	300

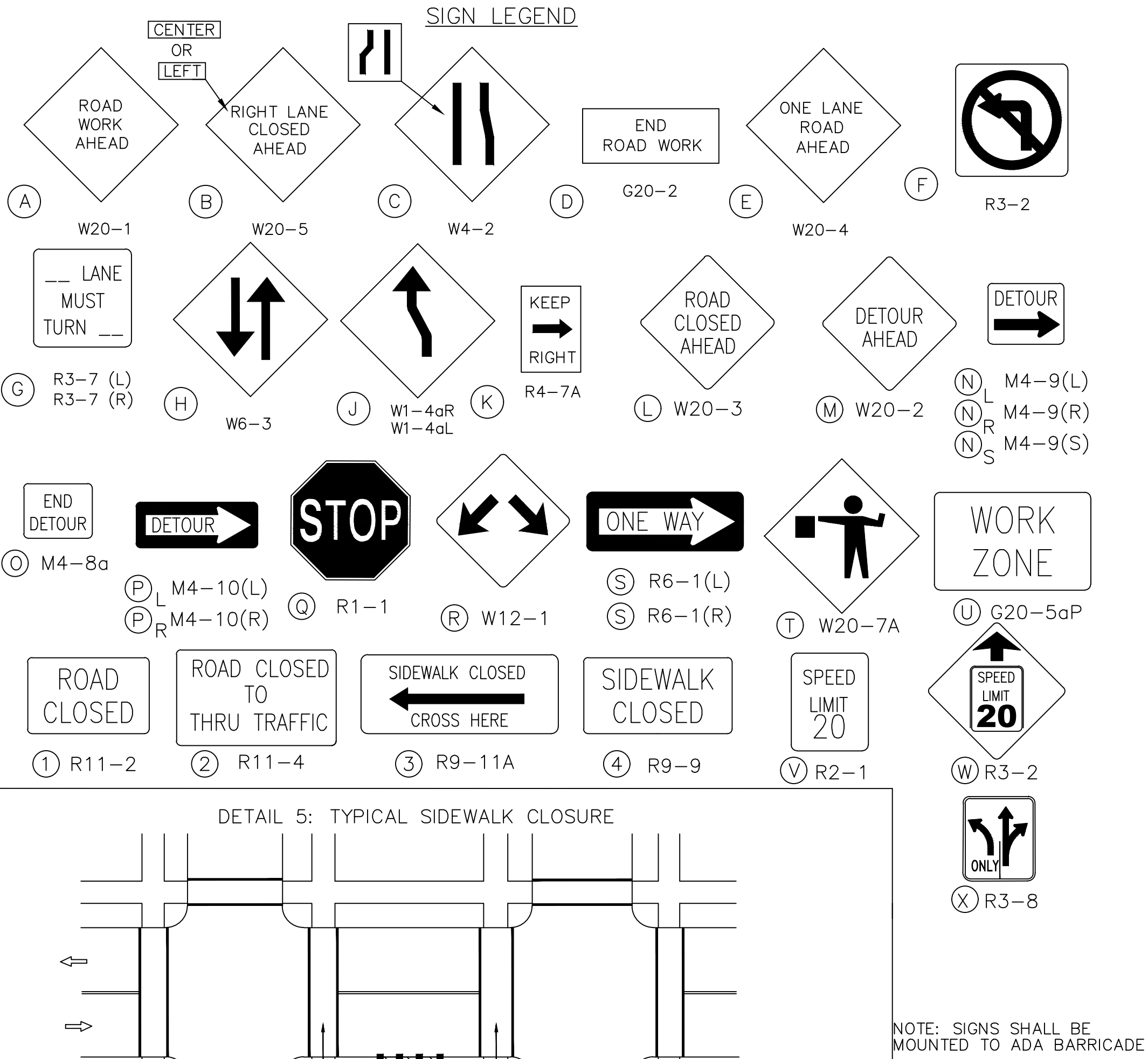
*Posted speed

SIGN SPACING(S)	
Speed Limit M.P.H.	Spacing (Feet)
≤ 30	100
35 - 45	350
≥ 55	500

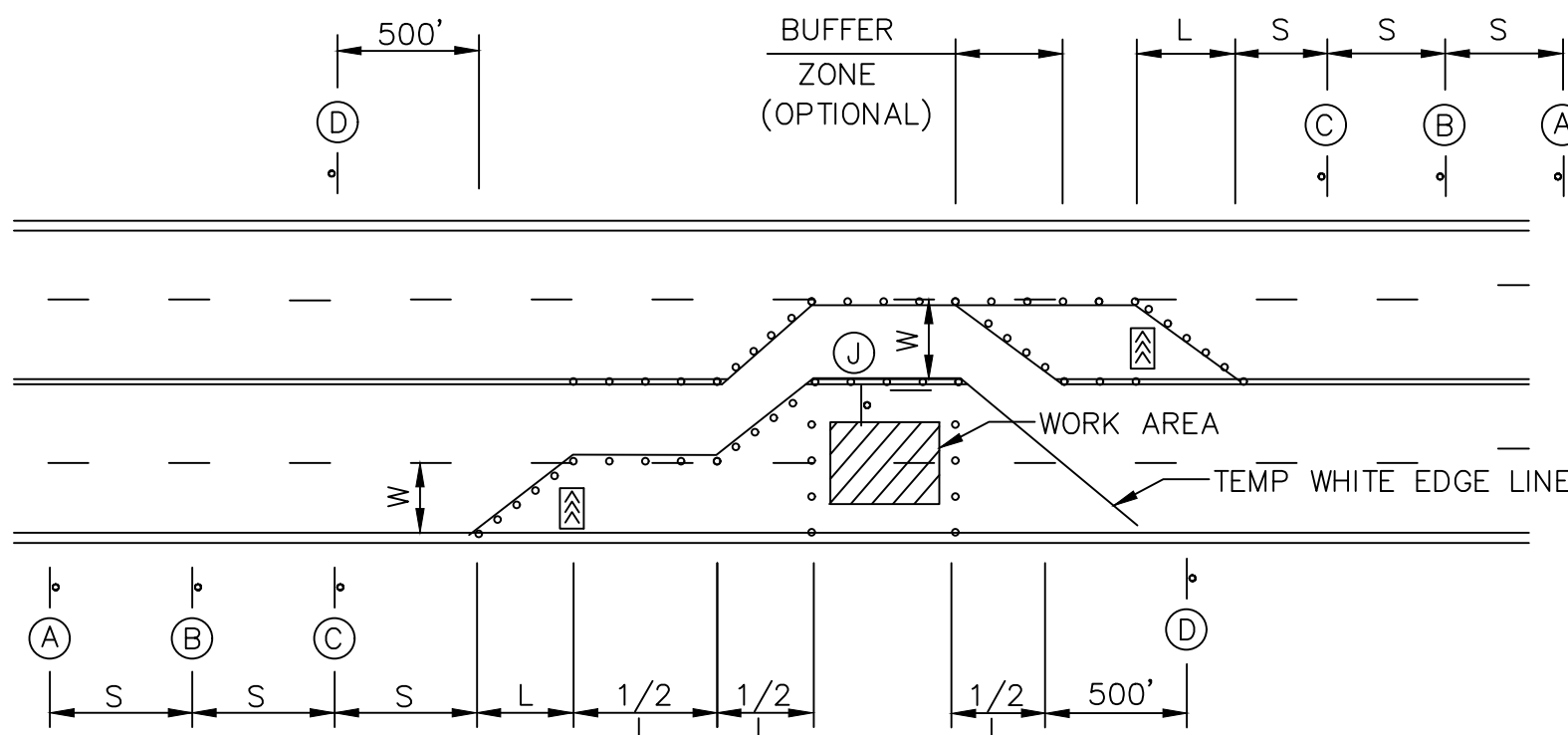
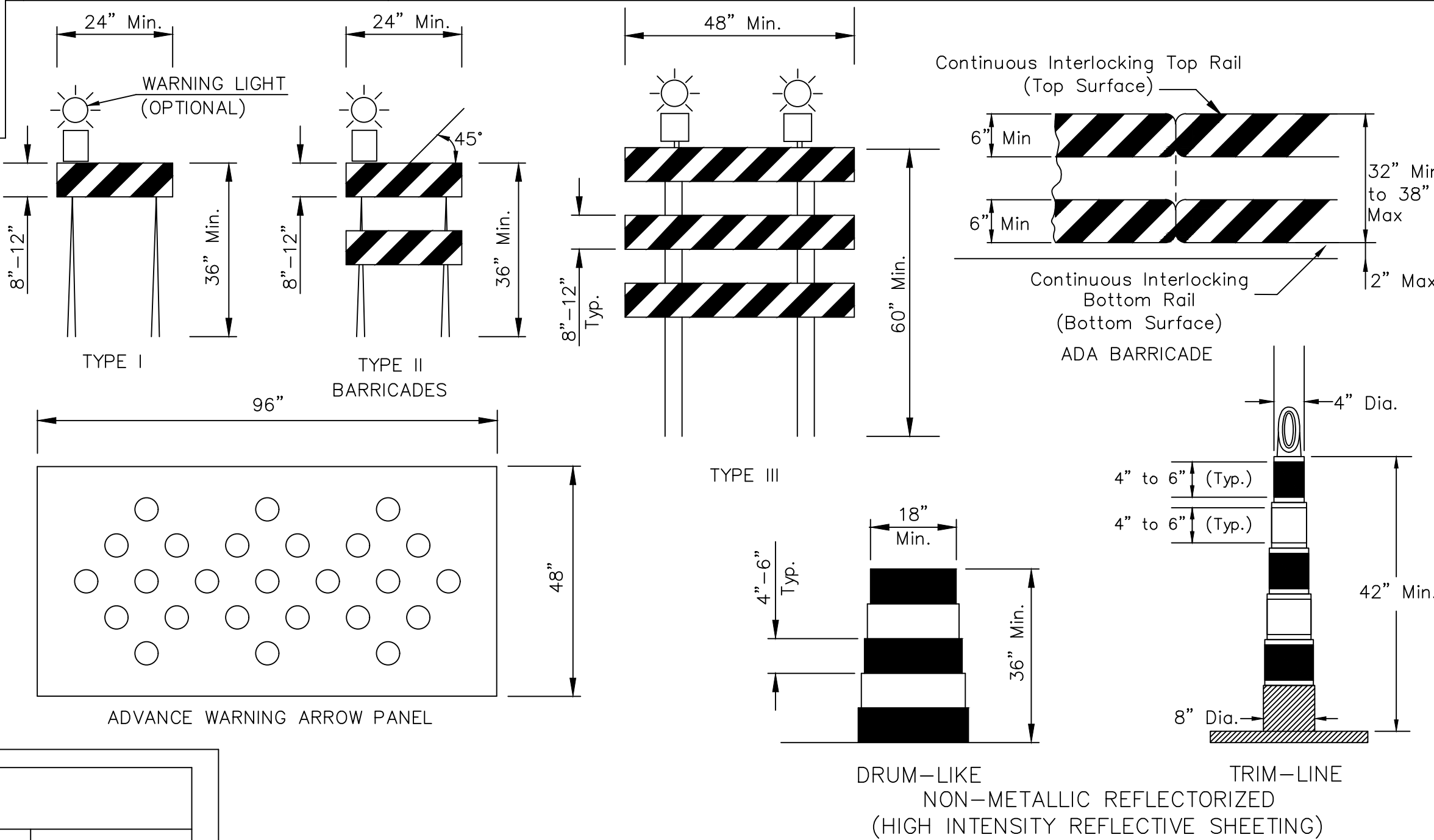
MAXIMUM CHANNELIZATION SPACING IN FEET *		
Speed Limit M.P.H.	Along Taper	After Taper
20	20	40
25	25	50
30	30	60
35	35	70
40	40	80
45	45	90
50	50	100
55	55	100

* The maximum distance in feet between devices in a taper should not exceed 1.0 times the speed limit in MPH.

TAPER DETAIL				
Speed Limit M.P.H.	Minimum Taper Length (L)			Minimum Number of Channelizing Devices for Taper
	Lane Width (W)			
	10	11	12	
20	70	75	80	5
25	105	115	125	6
30	150	165	180	7
35	205	225	245	8
40	270	295	320	9
45	450	495	540	13
50	500	550	600	13
55	550	605	660	13



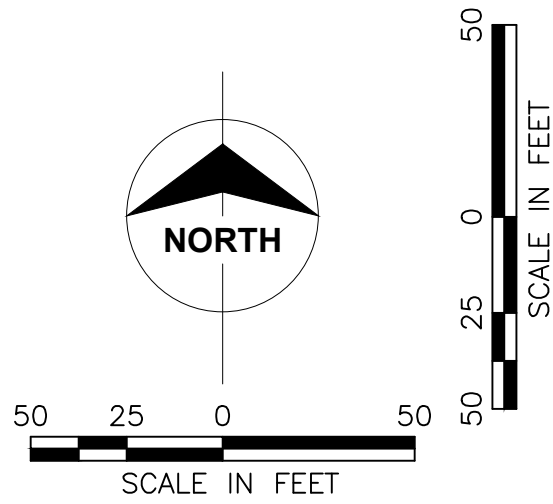
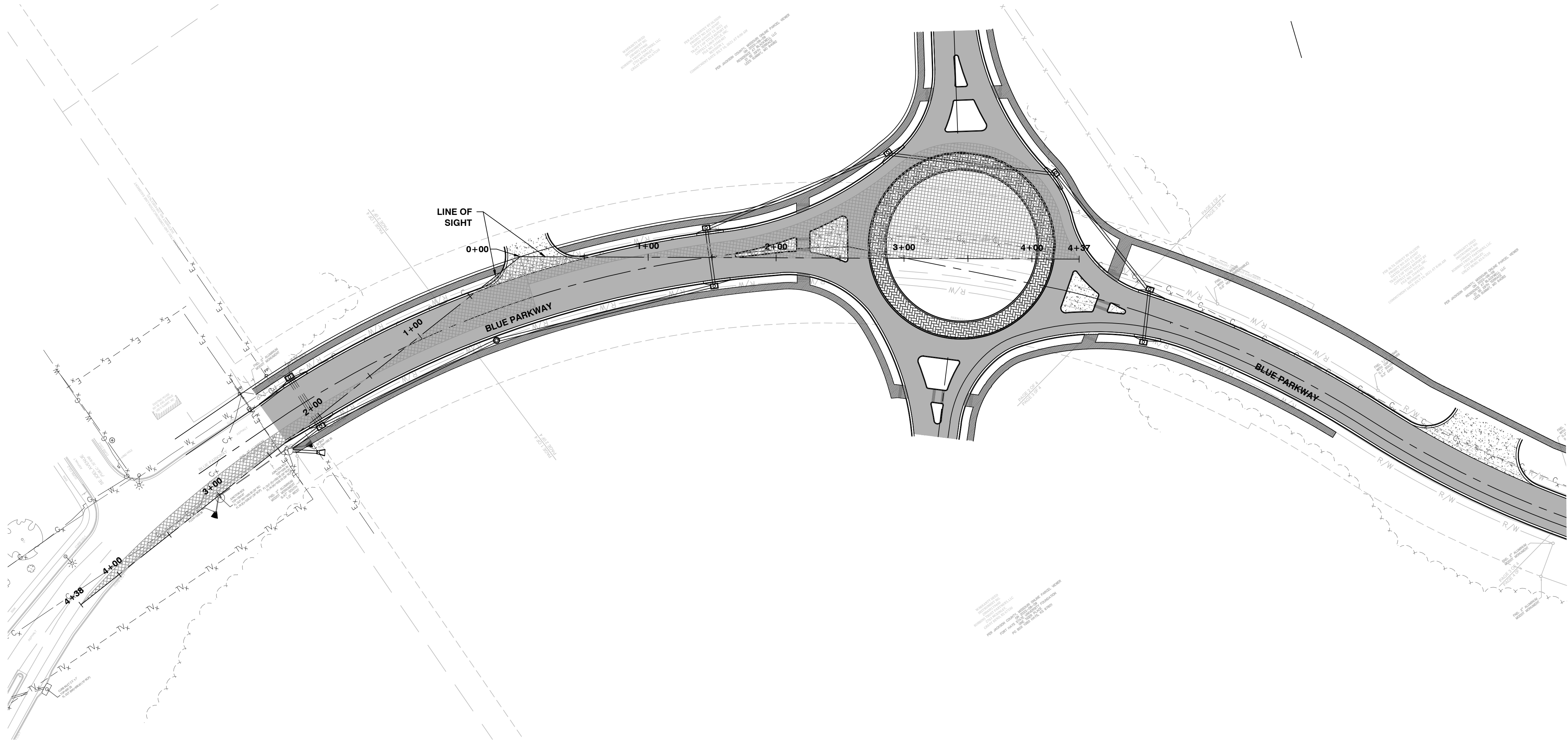
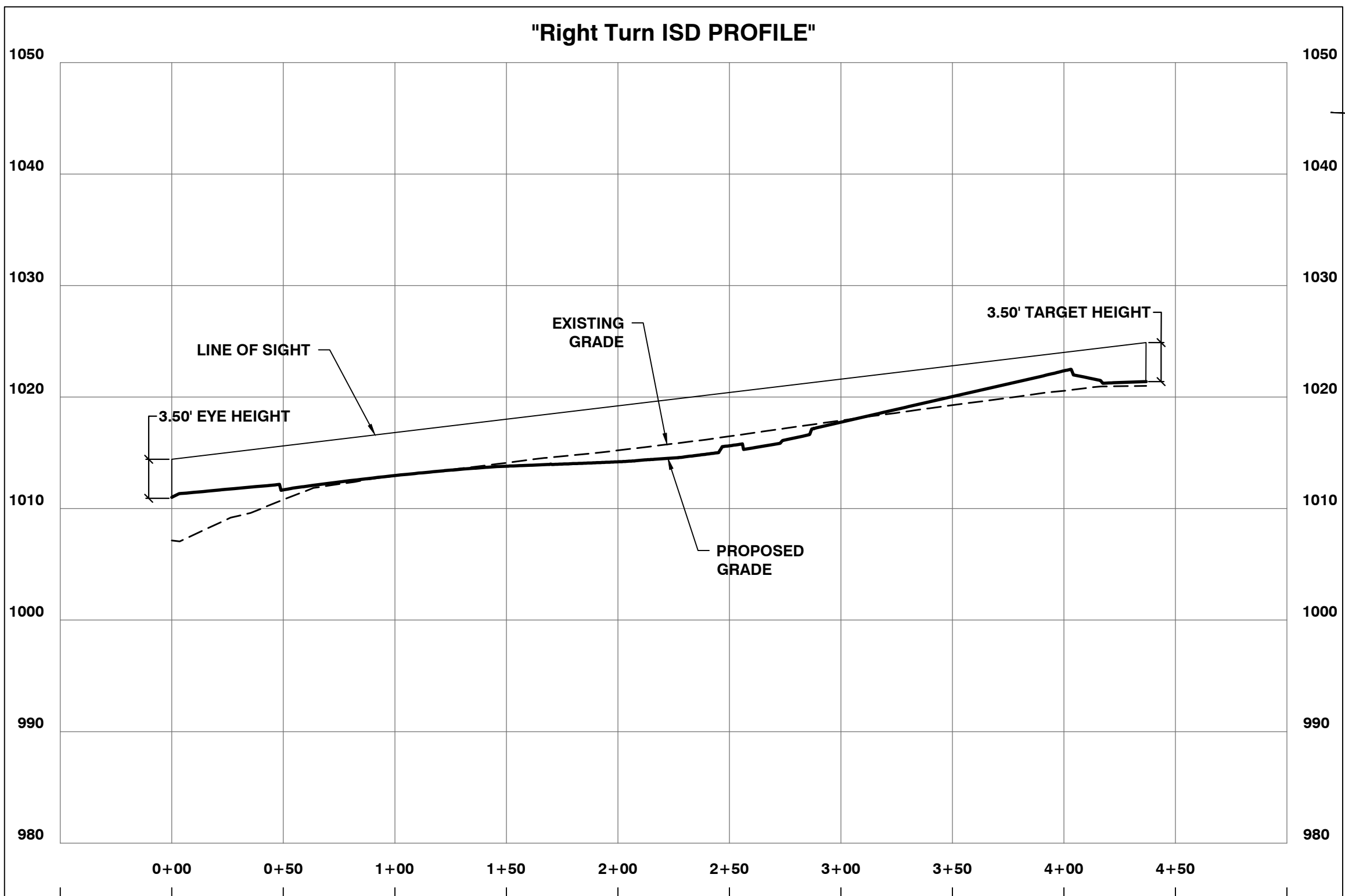
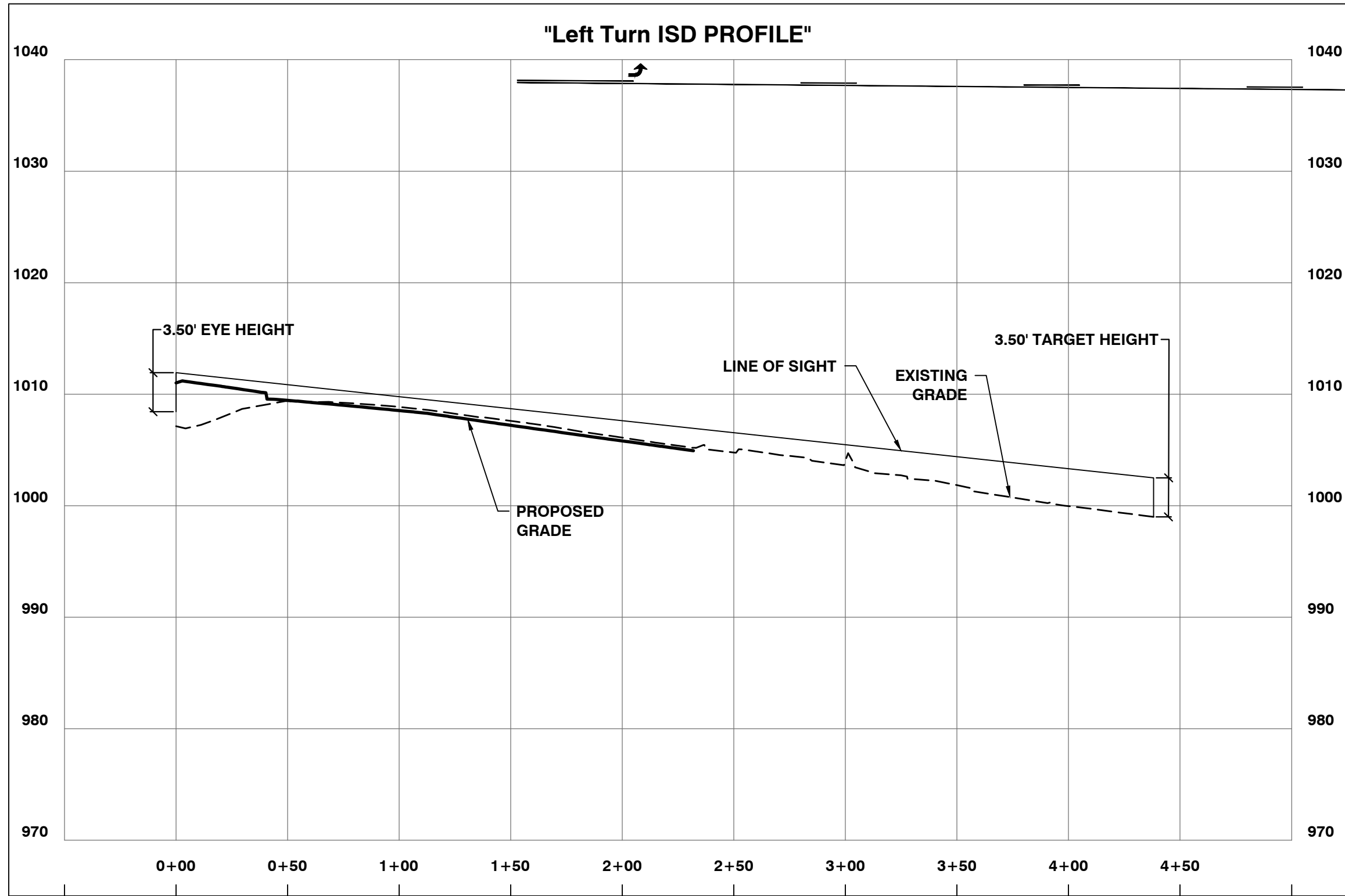
NOTE: SIGNS SHALL BE MOUNTED TO ADA BARRICADE



DETAIL 7: TYPICAL LANE CLOSURE - FOUR-LANE STREET

REVISIONS	NO.	DATE/DESCRIPTION
	1	
	2	
	3	
	4	
	5	
	6	

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PREPARED FOR:
GRIFFIN RILEY
PROPERTY GROUP, LLC
21 SE 29th TERRACE,
LEE'S SUMMIT, MO 64082

PUBLIC IMPROVEMENT PLANS
BLUE PARKWAY
LEE'S SUMMIT, JACKSON COUNTY, MO

REVISIONS	NO.	DATE/DESCRIPTION
1		
2		
3		
4		
5		
6		

PROJECT #: 22-2087
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PRELIMINARY

INTERSECTION
SIGHT DISTANCE