

SITE LOCATION MAP (NOT TO SCALE)

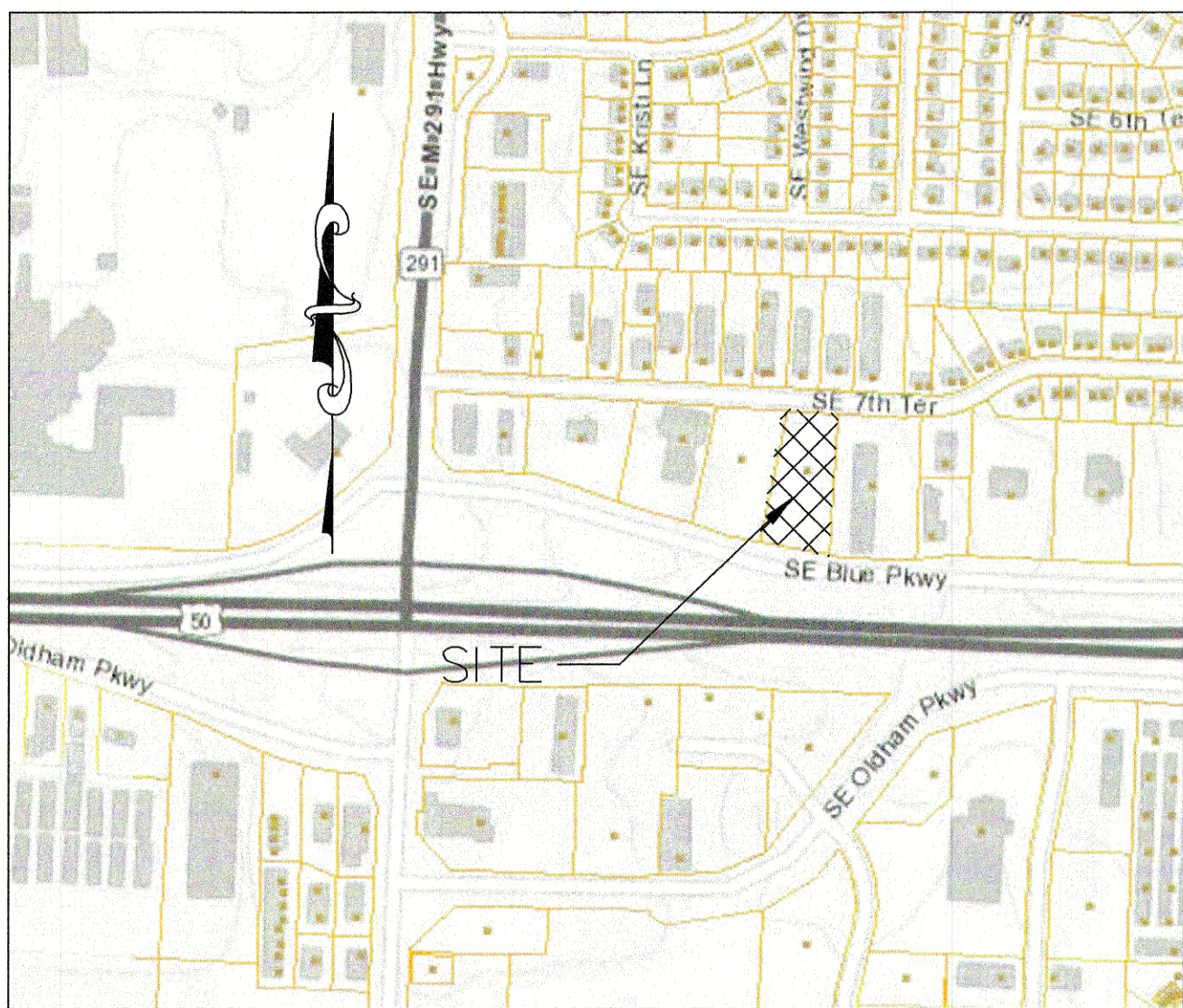


TABLE A ITEMS:

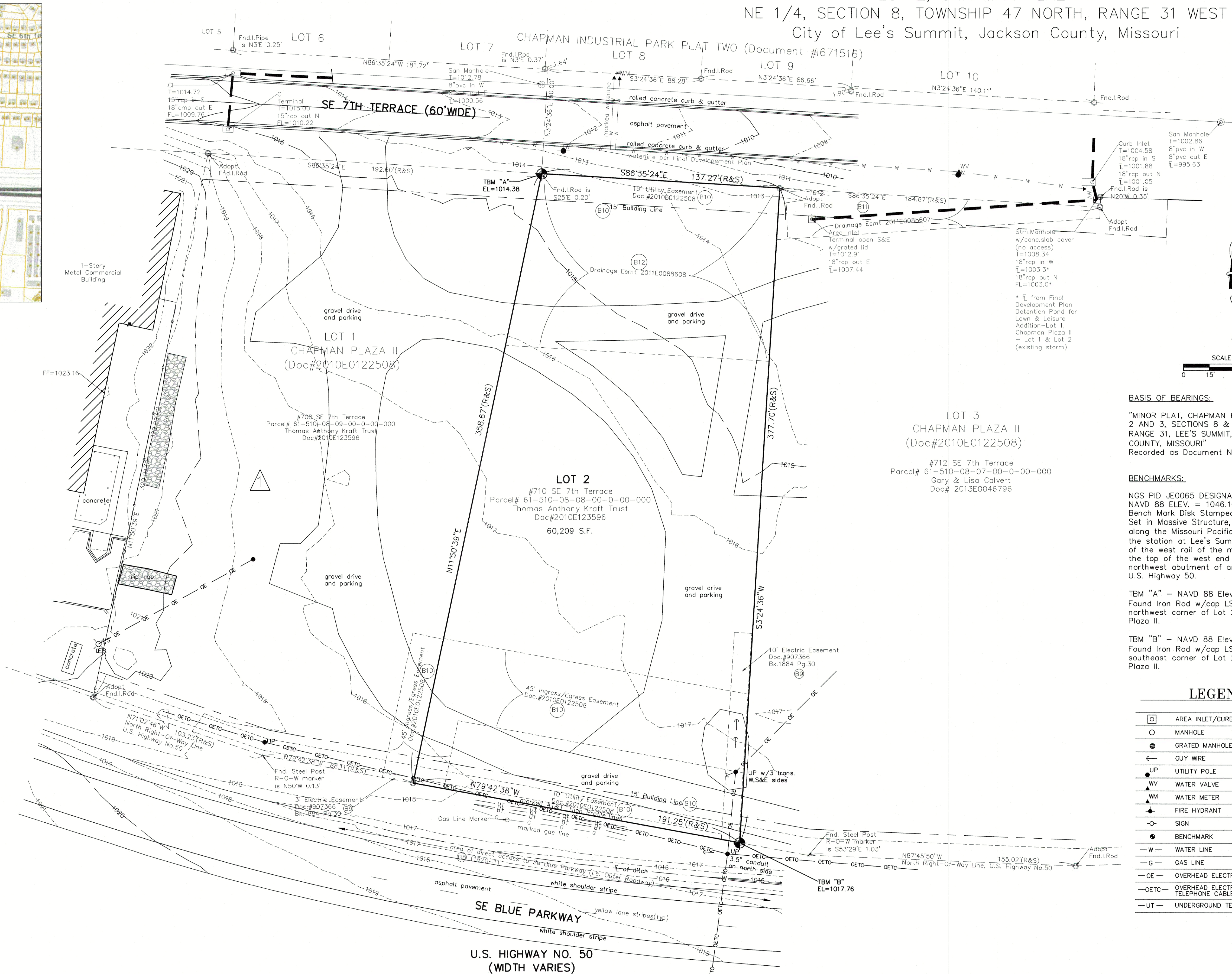
- Monuments set as shown
- Address - 710 SE 7th Terrace, Lee's Summit, MO 64063
- Flood Zone X - Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRM) 29183C0185G effective 1/20/2016 and 29189C0205G effective 1/20/2016
- There are 60,209 square feet in Lot 2
- Vertical datum is NAVD 88 (See Benchmark Note)
- No zoning letter provided
- No zoning letter provided
- There is no building on property
- Substantial features are shown
- There is no designated parking on property
- There were no division or party walls designated by the client.
- Utilities as shown
- The names of adjoining property owners are shown as of the date of the survey from the Assessor's records online.
- Site is approximately 0.2 miles east of SE Missouri 291 Highway along SE 7th Terrace.
- There is no new construction on or adjacent to site, however, there is a "Final Development Plan, Concept Development Plan, for Lawn & Leisure Addition - Lot 1 and Chapman Plaza II - Lot 1 & Lot 2", by Hamilton & Sterrett, Inc., Engineers, Land Surveyors & Planners, Signed & Sealed by Leslie R. Hamilton, MO E-22616, dated March 13, 2015. Plans depict proposed parking, building and detention.
- There was no evidence of changes in street right of way lines, and none made available to the surveyor by the controlling jurisdiction. There is no evidence of recent street or sidewalk construction or repairs observed during the process of conducting the field work.

Chicago Title Insurance Company Commitment No. 211007
Dated April 19, 2021

Description:
Lot 2, CHAPMAN PLAZA II, a subdivision in Lee's Summit, Jackson County, Missouri, according to the recorded plat thereof.

Chicago Title Insurance Company Commitment No. 211007
Dated April 19, 2021
Schedule B, Part II, Exceptions, Item #s

- Rights of way, and easement, together with abutter's rights of direct access to State of Missouri, Doc# 882977 in Bk 1820 pg 1 - property has direct access along the South line of Lot 2 to Se Blue Parkway, as shown, temporary easement mentioned for landscaping should be no longer in effect.
- Electric line easement to Missouri Public Service Company Doc# 907336 in Bk 1884 pg 30, as shown
- Boundaries, building lines, easements and dedications on Chapman Plaza II, Doc# 2010E122508, as shown
- Easement for storm drainage, Doc# 2011E0088607, as shown
- Easement for storm drainage, Doc# 2011E0088608, as shown



REVISION 6/23/2021
Added topography through Lot 1 of CHAPMAN PLAZA II.

To: Freeland & Kaufmann, Inc. and Chicago Title Insurance Company

This is to certify that the map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standards Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6a, 6b, 7a, 8, 9, 10, 11a, 13, 14, 16 & 17 of Table A thereof.
The fieldwork was completed on April 29, 2021

Date of Plot of Map: May 14, 2021

PRELIMINARY

MARK WILEY MO PLS 2437

BASIS OF BEARINGS:

"MINOR PLAT, CHAPMAN PLAZA II, LOTS 1, 2 AND 3, SECTIONS 8 & 9 TOWNSHIP 47, RANGE 31, LEE'S SUMMIT, JACKSON COUNTY, MISSOURI"
Recorded as Document No.2010E0122508

BENCHMARKS:

NGS PID JE0065 DESIGNATION X 205
NAVD 88 ELEV. = 1046.10
Bench Mark Disk Stamped X 205 1946
Set in Massive Structure, 0.6 miles south along the Missouri Pacific Railroad from the station at Lee's Summit, 19.1' west of the west rail of the main track, set in the top of the west end of the northwest abutment of an overpass over U.S. Highway 50.

TBM "A" - NAVD 88 Elev. = 1014.38
Found Iron Rod w/cap LS 371 D at northwest corner of Lot 2, Chapman Plaza II.

TBM "B" - NAVD 88 Elev. = 1017.76
Found Iron Rod w/cap LS 371 D at southeast corner of Lot 2, Chapman Plaza II.

LEGEND

	AREA INLET/CURB INLET
	MANHOLE
	GRATED MANHOLE
	GUY WIRE
	UTILITY POLE
	WATER VALVE
	WATER METER
	FIRE HYDRANT
	SIGN
	BENCHMARK
	WATER LINE
	GAS LINE
	OVERHEAD ELECTRIC
	OVERHEAD ELECTRIC TELEPHONE CABLE
	UNDERGROUND TELEPHONE

ALTA/NSPS SURVEY

LOT 2, CHAPMAN PLAZA II

NE 1/4, SECTION 8, TOWNSHIP 47 NORTH,

RANGE 31 WEST

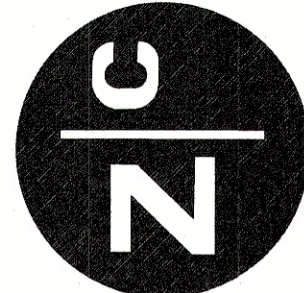
City of Lee's Summit, Jackson County, Missouri

REVISIONS	
DATE	DESCRIPTION
6/01/21	REV.

DATE	2021/05/04
CHECKED BY	MMW
DRAWN BY	AGK
SHEET NO.	1_of_1
JOB NO.	211029

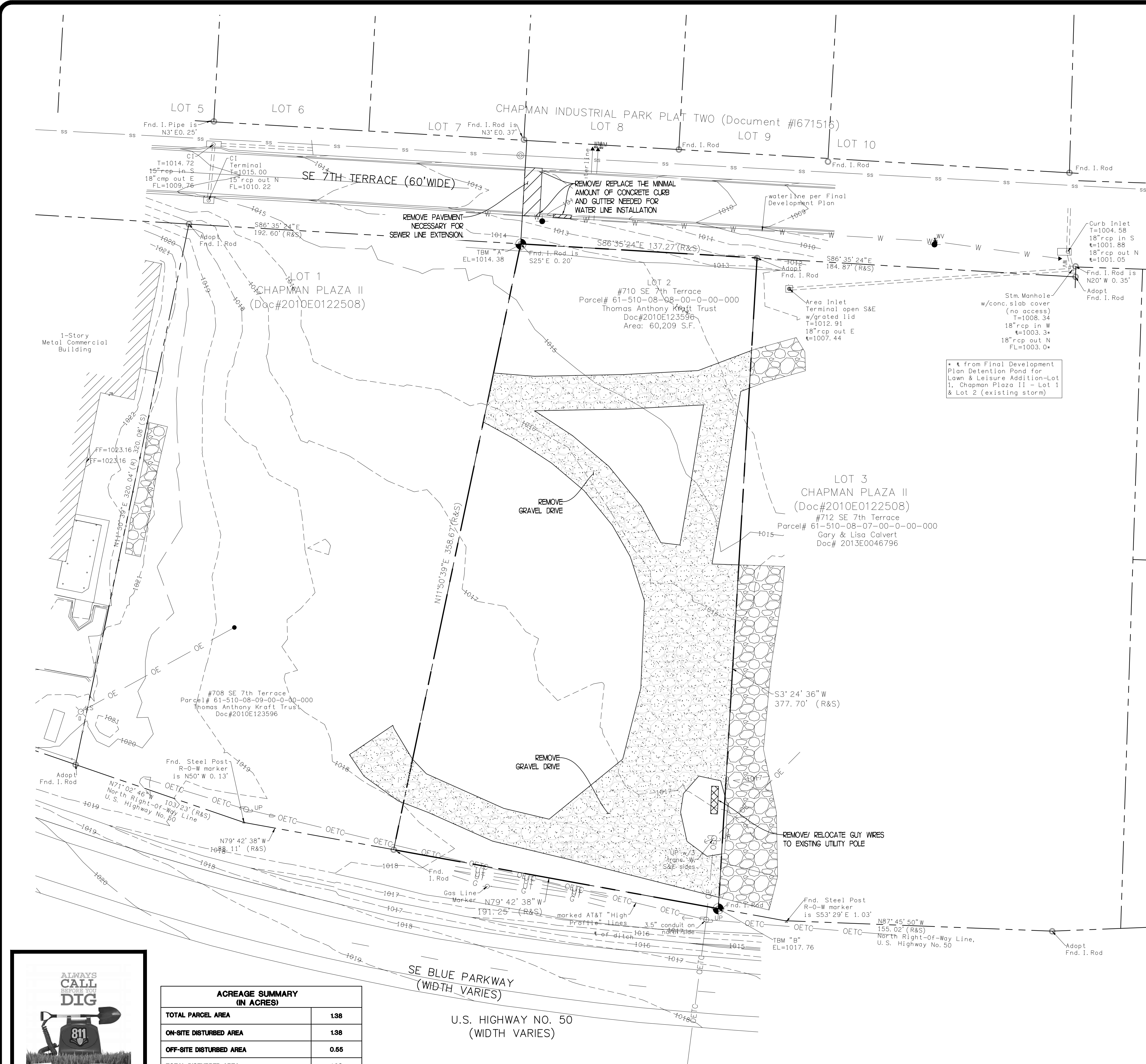
HEIDEMAN | ASSOCIATES, INC.

Consulting
Civil
Electrical
Mechanical
Engineers
Land Surveying



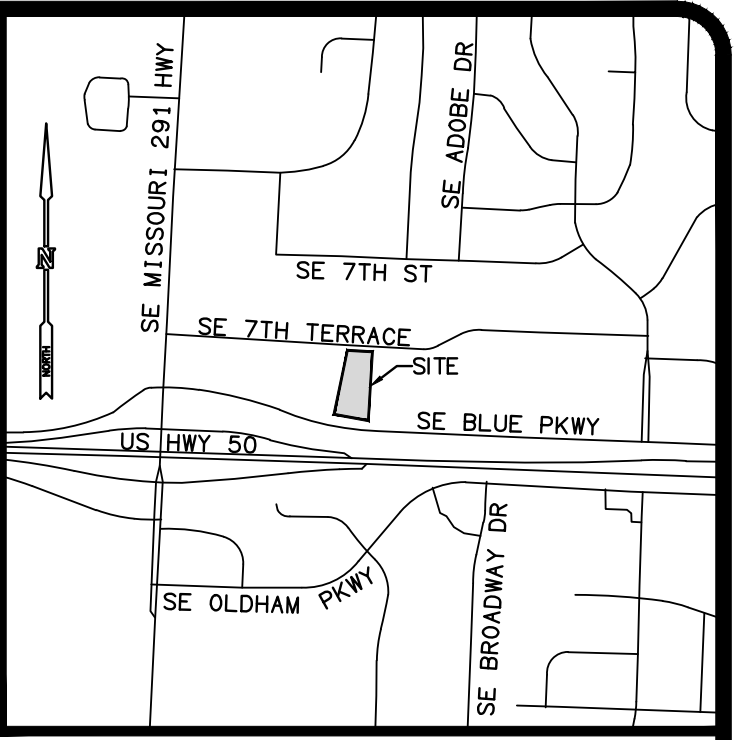
240 LARKIN WILLIAMS INDUSTRIAL COURT
LEE'S SUMMIT, MO 64063
Phone: 636-492-3200
Fax: 636-492-3202

Heideman + Associates, Inc. Professional Land Surveying Corporation Missouri State Certificate of Authority #000281



LEGEND

SYMBOL	DESCRIPTION (TO BE REMOVED)
[Pattern]	GRAVEL DEMO
[Pattern]	ASPHALT/ CONCRETE DEMO
[Pattern]	UTILITY DEMO



- DEMOLITION PLAN NOTES:**
1. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL LOCAL AND STATE PERMITS REQUIRED FOR DEMOLITION WORK.
 2. THE CONTRACTOR SHALL INDEMNIFY AND HOLD HARMLESS THE OWNER AND/OR ENGINEER FOR ANY AND ALL INJURIES AND/OR DAMAGES TO PERSONNEL, EQUIPMENT AND/OR EXISTING FACILITIES IN THE DEMOLITION AND CONSTRUCTION DESCRIBED IN THE PLANS AND SPECIFICATIONS.
 3. EXISTING CONDITIONS AS DEPICTED ON THESE PLANS ARE GENERAL AND ILLUSTRATIVE IN NATURE AND DO NOT INCLUDE MECHANICAL, ELECTRICAL AND MISCELLANEOUS STRUCTURES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO EXAMINE THE SITE AND BE FAMILIAR WITH EXISTING CONDITIONS PRIOR TO BIDDING ON THE DEMOLITION WORK FOR THIS PROJECT. IF CONDITIONS ENCOUNTERED DURING EXAMINATION ARE SIGNIFICANTLY DIFFERENT THAN THOSE SHOWN, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY.
 4. ALL EXISTING ABOVE AND BELOW GROUND STRUCTURES WITHIN THE LIMITS OF NEW CONSTRUCTION SHALL BE RAZED UNLESS NOTED OTHERWISE WITHIN THIS CONSTRUCTION SET, ARCHITECTURAL PLANS AND/OR PROJECT SPECIFICATIONS. THIS INCLUDES FOUNDATION SLABS, WALLS, AND FOOTINGS.
 5. ALL DEMOLITION WASTE AND CONSTRUCTION DEBRIS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF IN A STATE APPROVED WASTE SITE AND IN ACCORDANCE WITH ALL LOCAL AND STATE CODES AND PERMIT REQUIREMENTS.
 6. ALL UTILITY REMOVAL, RELOCATION, CUTTING, CAPPING AND/OR ABANDONMENT SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANY.
 7. THE BURNING OF CLEARED MATERIAL AND DEBRIS SHALL NOT BE ALLOWED UNLESS CONTRACTOR GETS WRITTEN AUTHORIZATION FROM THE AUTHORITIES.
 8. UTILITY CONTACTS ARE LISTED ON THE COVER SHEET.
 9. EROSION AND SEDIMENTATION CONTROL MEASURES AROUND AREAS OF DEMOLITION SHALL BE INSTALLED PRIOR TO INITIATION OF DEMOLITION ACTIVITIES. REFER TO SWPP PLAN FOR DETAILS.
 10. ASBESTOS OR HAZARDOUS MATERIALS, IF FOUND ON SITE, SHALL BE REMOVED BY A LICENSED HAZARDOUS MATERIALS CONTRACTOR. CONTRACTOR SHALL NOTIFY OWNER IMMEDIATELY IF HAZARDOUS MATERIALS ARE ENCOUNTERED.
 11. CONTRACTOR SHALL PROTECT ALL CORNER PINS, MONUMENTS, PROPERTY CORNERS, AND BENCHMARKS DURING DEMOLITION ACTIVITIES. IF DISTURBED, CONTRACTOR SHALL HAVE DISTURBED ITEMS RESET BY A LICENSED SURVEYOR AT NO ADDITIONAL COST TO THE OWNER.
 12. CONTRACTOR SHALL ADHERE TO ALL LOCAL, STATE, FEDERAL AND OSHA REGULATIONS WHEN OPERATING DEMOLITION EQUIPMENT AROUND UTILITIES.
 13. CONTRACTOR SHALL PROVIDE AND MAINTAIN TRAFFIC CONTROL MEASURES IN ACCORDANCE WITH THE NCDOT STANDARDS, AND AS REQUIRED BY LOCAL AGENCIES WHEN WORKING IN AND/OR ALONG STREETS, ROADS, HIGHWAYS, ETC. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN APPROVAL AND COORDINATE WITH LOCAL AND/OR STATE AGENCIES REGARDING THE NEED, EXTENT, AND LIMITATIONS ASSOCIATED WITH INSTALLING AND MAINTAINING TRAFFIC CONTROL MEASURES.
 14. CONTRACTOR SHALL PROTECT AT ALL TIMES ADJACENT STRUCTURES AND ITEMS FROM DAMAGE DUE TO DEMOLITION ACTIVITIES.
 15. DEMOLITION CONTRACTOR SHALL COORDINATE EXISTING FACILITIES UTILITY DISCONNECTS WITH THE OWNER'S REPRESENTATIVE A MINIMUM 7 DAYS PRIOR TO ANTICIPATED DEMOLITION OF STRUCTURES.
 16. CONTRACTOR SHALL REFER TO OTHER PLANS WITHIN THIS CONSTRUCTION SET FOR OTHER PERTINENT INFORMATION.
 17. CONTRACTOR SHALL REMOVE AIR CONDITION UNITS / FREON IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS.
 18. ALL UTILITY OR SERVICE LINES THAT UNDERLIE THE PROPOSED STRUCTURES SHALL BE REMOVED. ALL STORM AND SANITARY SEWER LINES THAT LIE BENEATH A HWY LINE EXTENDING FROM THE BASE OF FOUNDATIONS FOR FREE STANDING FOUNDATIONS FOR LOAD BEARING WALLS SHOULD BE REMOVED OR PRESSURE-GROUTED IN-PLACE.

NOTE:

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2. DEMOLITION CONTRACTOR SHALL COORDINATE EXISTING FACILITIES UTILITIES DISCONNECTS WITH OWNER'S REPRESENTATIVE A MINIMUM 7 DAYS PRIOR TO ANTICIPATED DEMOLITION OF STRUCTURES.
3. CONTRACTOR SHALL REFERENCE THE GEOTECHNICAL REPORT PREPARED BY INTERTEK PSI DATED MAY 21, 2021 FOR INFORMATION ON THE REMOVAL OF EXISTING IN SITU MATERIALS AND HARDSCAPE.

ACREAGE SUMMARY (IN ACRES)	
TOTAL PARCEL AREA	1.38
ON-SITE DISTURBED AREA	1.38
OFF-SITE DISTURBED AREA	0.55
TOTAL DISTURBED AREA	1.93
IMPERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY & INCLUDES GRAVEL)	0.48
PERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY)	0.90
IMPERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.90
PERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.48
IMPERVIOUS AREA INCREASE (PRE VS POST) (ON-SITE ONLY)	0.42
IMPERVIOUS PERCENTAGE (AT POST-CONSTRUCTION STATUS)	65.2%

ALWAYS CALL BEFORE YOU DIG

811

EVERY DIGGING JOB REQUIRES A CALL - EVEN SMALL PROJECTS LIKE PLANTING TREES OR SHEDS. IF YOU HIT AN UNDERGROUND UTILITY LINE WHILE DIGGING, YOU CAN HARM YOURSELF OR THOSE AROUND YOU, DISRUPT SERVICE TO AN ENTIRE NEIGHBORHOOD AND POTENTIALLY BE RESPONSIBLE FOR FINES AND REPAIR COSTS.

SMART DIGGING MEANS CALLING 811 BEFORE EACH JOB. WHETHER YOU ARE A HOMEOWNER OR A PROFESSIONAL EXCAVATOR, ONE CALL TO 811 GETS YOUR UNDERGROUND UTILITY LINES MARKED FOR FREE.

REVISIONS	BY

FREELAND and KAUFFMAN, INC.

Engineers * Landscape Architects

209 West Stone Avenue
Greenville, South Carolina 29609

864-233-5497
fax 864-233-8916



CALIBER COLLISION

710 SE BLUE PARKWAY
LEE'S SUMMIT, MO 64063

CROSS DEVELOPMENT, CC LEE'S SUMMIT, LLC
4388 MARSH RIDGE ROAD
CARROLLTON, TX 75010
TEL (214) 614-8252

DRAWN
BAC
CHECKED
TMB
DATE
1/17/2021
SCALE

DRAWING

3

DEMOLITION PLAN

SCALE 1" = 30'

ALWAYS CALL 811 BEFORE YOU DIG

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DISPOSAL OF SEDIMENT:
ALL SEDIMENT REMOVED FROM SILT FENCE, INLET PROTECTION, SEDIMENT POND, AND TRAPS, ETC. DURING THE COURSE OF CONSTRUCTION, SHALL BE PLACED ON THE TOPSOIL STOCKPILE AND RESPIRED AT THE END OF THE PROJECT. SEDIMENT REMOVED FROM THESE ITEMS AFTER THE SITE HAS BEEN SEEDDED, SHALL BE HAULED OFF SITE AND DISPOSED OF AT AN APPROVED, LICENSED SPILL AREA THAT IS IN FULL COMPLIANCE WITH THE NPDES REGULATIONS. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY APPROVALS NECESSARY TO ALLOW FOR THE DISPOSAL OF THIS MATERIAL IF HAULED OFF-SITE.

REFER TO THE EROSION CONTROL DETAIL SHEETS FOR ALL EROSION CONTROL NOTES, MAINTENANCE REQUIREMENTS, ETC.

CONTRACTOR TO REFERENCE LANDSCAPE PLAN FOR INFORMATION ON STABILIZATION TYPE FOR SEED, SOD, AND MULCH.

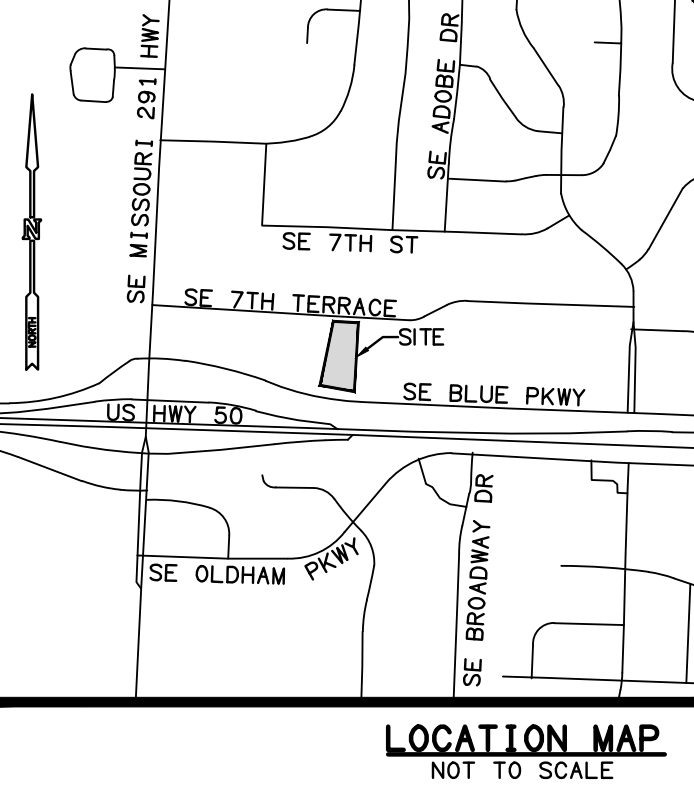
CONTRACTOR SHALL USE EXTREME CAUTION WHILE WORKING UNDERNEATH OVERHEAD POWER LINES. ANY DAMAGE TO THE EXISTING UTILITY POLES AND UTILITY LINES SHALL BE REPAIRED/REPLACED AT THE CONTRACTOR'S EXPENSE.

EROSION CONTROL MAINTENANCE PLAN:

- ALL SLOPES 3:1 AND STEEPER SHALL RECEIVE A CURLEX BLANKET OR APPROVED EQUAL, IN ADDITION TO HYDROSEEDING. TEMPORARY BERMS MAY BE NEEDED DAILY UNTIL ALL SLOPES ARE BROUGHT TO GRADE.
- EXCEPT AS NOTED BELOW, STABILIZATION SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN SEVEN DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS PERMANENTLY CEASED. WHERE THE INITIATION OF STABILIZATION MEASURES BY THE SEVENTH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS CONDITIONS ALLOW. WHERE CONSTRUCTION ACTIVITY WILL RESUME ON A PORTION OF THE SITE WITHIN 21 DAYS FROM WHEN ACTIVITY CEASED, (E.G. THE TOTAL TIME PERIOD THAT CONSTRUCTION ACTIVITY IS TEMPORARILY HALTED IS LESS THAN 21 DAYS) THEN STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE BY THE SEVENTH DAY AFTER CONSTRUCTION ACTIVITIES HAVE TEMPORARILY CEASED. AREAS WHERE THE SEED HAS FAILED TO GERMINATE ADEQUATELY (UNIFORM PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 70% WITHIN 30 DAYS AFTER SEEDING AND MULCHING MUST BE RE-SEEDDED IMMEDIATELY, OR AS SOON AS WEATHER CONDITIONS ALLOW). PERMANENT STABILIZATION FOR ALL DISTURBED AREAS IS TO BE COMPLETE WITHIN 15 WORKING DAYS OR 90 CALENDAR DAYS (WHICHEVER IS SHORTER) FOLLOWING COMPLETION OF CONSTRUCTION. DIVERSIONS MUST BE STABILIZED PRIOR TO BECOMING FUNCTIONAL.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED EVERY SEVEN (7) DAYS OR AFTER EACH RAINFALL OCCURRENCE THAT EXCEEDS ONE-HALF (0.5) INCH. DAMAGED OR INEFFECTIVE DEVICES SHALL BE REPAIRED AND REPLACED AS NECESSARY.
- PROVIDE SILT FENCE AND/OR OTHER EROSION CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL EROSION CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFF-SITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO THE PAVED ROADWAY FROM CONSTRUCTION AREAS. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT LOADED WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- CONTRACTOR SHALL PROVIDE EROSION CONTROL BLANKETS ON ALL SLOPES STEEPER THAN 3:1. CONTRACTOR HAS THE OPTION TO UTILIZE SOD IN LIEU OF AN EROSION CONTROL BLANKET (TYPICAL).

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	LIMITS OF DISTURBANCE
---	---	CONTOUR LINE
---	---	STORM DRAIN
---	---	TEMPORARY SILT FENCE
---	---	INLET PROTECTION
---	---	ROCK OUTLET
---	---	CONSTRUCTION ENTRANCE
---	---	TEMPORARY SEEDING AREA
---	---	MULCH-PLANTED AREA
---	---	PERMANENT SEEDING AREA



SWPPP CONTACT DURING CONSTRUCTION CROSS DEVELOPMENT ATTN:

DRAINAGE BASIN: MISSOURI RIVER WATERSHED

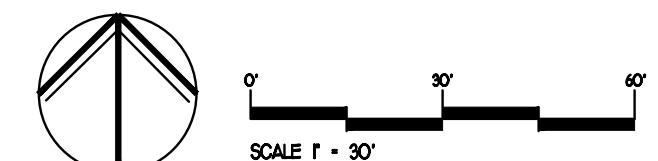
RECEIVING WATERS: EAST FORK LITTLE BLUE RIVER, TRIBUTARY OF THE BLUE RIVER - MISSOURI RIVER WATERSHED

SOILS ON SITE:
• 10082 - ARISBURG-URBAN LAND COMPLEX, 1 TO 5 PERCENT SLOPES (HYDROLOGIC SOIL GROUP C)

SEQUENCE OF CONSTRUCTION:

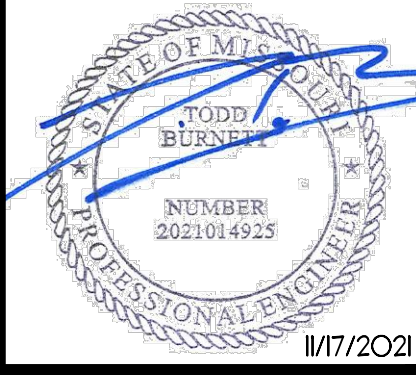
- PHASE I**
- SUBMIT PLANS FOR REVIEW
 - OBTAIN GRADING PERMIT (MAY INCLUDE PRE-CONSTRUCTION CONFERENCE). CONTRACTOR SHALL OBTAIN ALL APPROPRIATE PERMITS REQUIRED FOR DEMOLITION AND LAND DISTURBANCE ACTIVITIES.
 - CONTRACTOR SHALL CONTACT CITY OF LEE'S SUMMIT DEVELOPMENT SERVICES TO SCHEDULE A RECONSTRUCTION MEETING AT 680 969-200.
 - REMOVE THE MINIMAL AMOUNT OF PERIMETER SITE FEATURES IN ORDER TO INSTALL STABILIZED CONSTRUCTION EXIT AND SILT FENCE. ENSURE DRAINAGE IS DIRECTED TOWARDS SILT FENCE.
 - INSTALL INLET PROTECTION AT THE EXISTING STORM STRUCTURE OFF THE NORTH EAST CORNER OF THE PROPERTY ON THE ADJACENT PROPERTY.
 - INSTALL SWPPP SIGN, TEMPORARY STOCKPILE AREA, AND DESIGNATE AN AREA FOR THE PARKING OF CONSTRUCTION VEHICLES.
 - BEGIN DEMOLITION OF THE SITE FEATURES PER THE DEMOLITION PLAN. DO NOT BEGIN DEMOLITION OF THE PAVEMENT AND CONCRETE CURB ALONG SE 7TH TERRACE UNTIL READY TO BEGIN WATER MAIN INSTALLATION CONNECTION.
 - INSTALL CONCRETE WASHOUT AT SITE ENTRANCE/EXIT.
 - INSTALL TEMPORARY SEEDING AND MULCH STABILIZATION TO ALL AREAS WHERE WORK WILL BE CEASED FOR A PERIOD OF MORE THAN 7 DAYS.
- PHASE II**
- CONTINUALLY INSPECT AND MAINTAIN ALL BMP'S. REPAIR AND REPLACE ANY BMP'S AS NEEDED.
 - BEGIN GENERAL GRADING OF SITE. APPLY PERMANENT SEEDING TO ALL AREAS AS SOON AS THEY ARE COMPLETE. DO NOT WAIT UNTIL JOB COMPLETION TO PERMANENTLY SEED DISTURBED AREAS. ADD EROSION CONTROL BLANKETS AS NEEDED FOR ALL AREAS WITH SLOPES THAT EXCEED 3:1.
 - INSTALL UNDERGROUND DETENTION SYSTEM PER CONTECH'S SPECIFICATIONS AND THE GRADING PLAN. INSTALL STORM SEWERS AND STRUCTURES. INLET PROTECTION SHALL BE ADDED AT EACH STRUCTURE IMMEDIATELY AFTER THEY HAVE BEEN CONSTRUCTED. CONNECT TO EXISTING STORM STRUCTURE AS SHOWN, REPAIRING/REPLACING ANY APPURTENANCES AS NEEDED.
 - INSTALL UNDERGROUND UTILITIES AND COORDINATE WITH EACH UTILITY PROVIDER. BRING ALL BUILDING UTILITIES TO THE PROPOSED PAD LOCATION AND CAP FOR FUTURE CONNECTION.
 - INSTALL WATER LINE EXTENSION TO EXISTING MAIN NORTH OF SE 7TH TERRACE. IMMEDIATELY REPAVE ASPHALT AND INSTALL CURB & GUTTER PER CITY OF LEE'S SUMMIT STANDARDS AND SPECS.
 - COMMENCE GRADING OF BUILDING PAD AND AREAS AROUND BUILDING PAD.
 - INSTALL CURB AND GUTTER AND FINE GRADE PARKING AREAS. STONE BASE SHALL BE INSTALLED IN ALL PAVED AREAS AS SOON AS PRACTICABLE. CONSTRUCTION EXIT SHALL BE REMOVED ONLY ONCE THE BUILDING PAD, PARKING LOT, AND DRIVEWAY HAVE BEEN STABILIZED WITH STONE BASE.
 - PAVE SITE AND INSTALL APPROPRIATE INLET PROTECTION DEVICES FOR PAVED AREAS AS WORK PROGRESSES.
 - COMPLETE BUILDING CONSTRUCTION, SIDEWALKS, PLANTINGS, ETC.
 - REMOVE TEMPORARY BMP'S AS SOON AS THEIR CONTRIBUTING DRAINAGE AREAS HAVE BEEN STABILIZED. SEED AND MULCH ANY DISTURBANCE FROM REMOVAL OF TEMPORARY BMP'S.
 - INSPECT/ CLEAN/ FLUSH UNDERGROUND DETENTION SYSTEM. CONTRACTOR SHALL CONTACT MANUFACTURER FOR FINAL INSPECTION AND CERTIFICATION.
 - CONTACT MOHR FOR FINAL INSPECTION AND OWNER FOR COORDINATION OF FILING THE NOTICE OF TERMINATION (NOT).

EROSION CONTROL PHASE I



REVISIONS	BY

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864-233-5497
fax 864-233-8916



CALIBER COLLISION
710 SE BLUE PARKWAY
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CROSS DEVELOPMENT, CC LEE'S SUMMIT, LLC
4388 MARSH RIDGE ROAD
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TEL (214) 614-8252

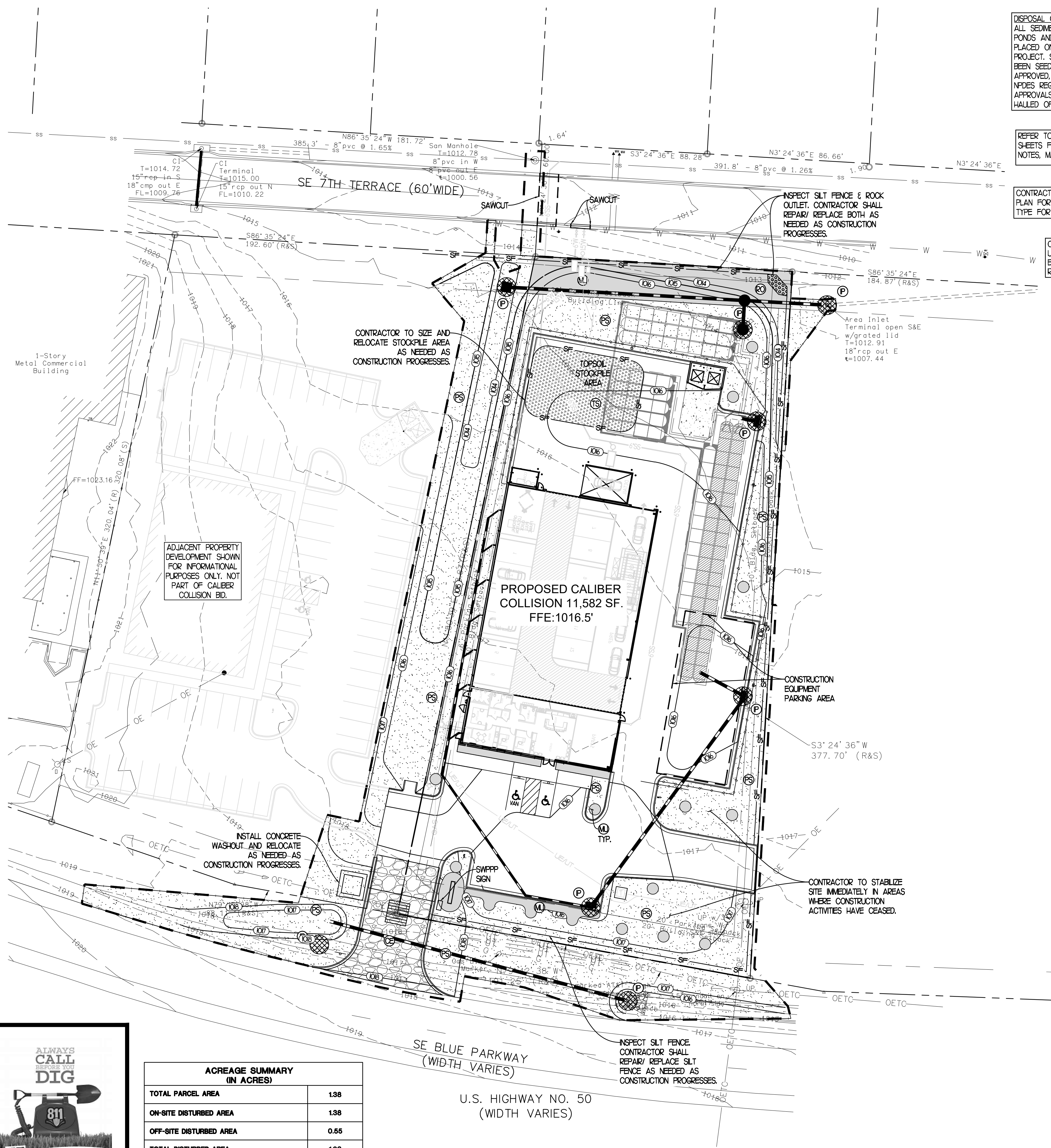
DRAWN	BAC
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SCALE	

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IMPERVIOUS PERCENTAGE (AT POST-CONSTRUCTION STATUS)	65.2%



DISPOSAL OF SEDIMENT:
ALL SEDIMENT REMOVED FROM SILT FENCE, INLET PROTECTION, SEDIMENT PONDS AND TRAPS, ETC. DURING THE COURSE OF CONSTRUCTION, SHALL BE PLACED ON THE TOPSOIL STOCKPILE AND RESPIRED AT THE END OF THE PROJECT. SEDIMENT REMOVED FROM THESE ITEMS AFTER THE SITE HAS BEEN SEED, SHALL BE HAILED OFF SITE AND DISPOSED OF AT AN APPROVED, LICENSED SPOIL AREA THAT IS IN FULL COMPLIANCE WITH THE NPDES REGULATIONS. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY APPROVALS NECESSARY TO ALLOW FOR THE DISPOSAL OF THIS MATERIAL IF HAILED OFF-SITE.

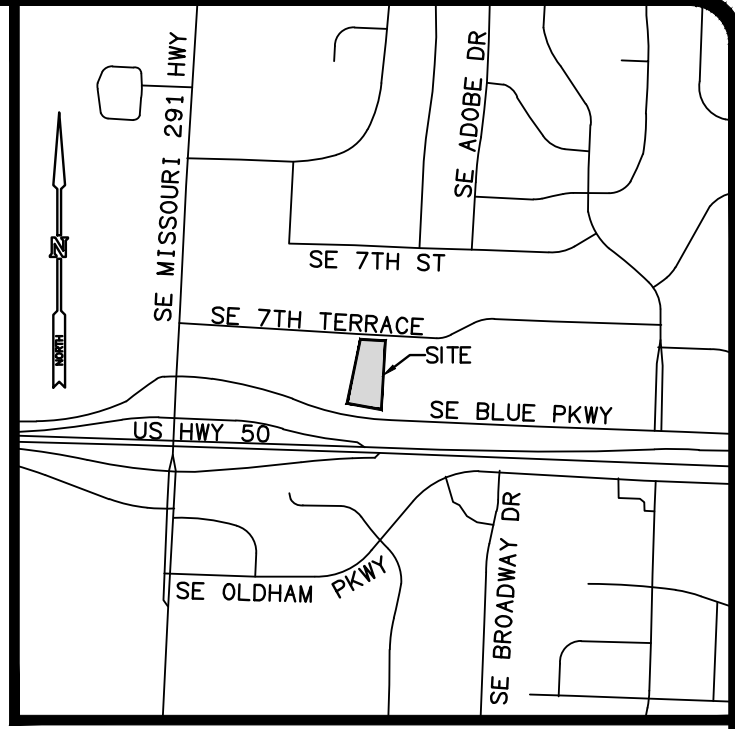
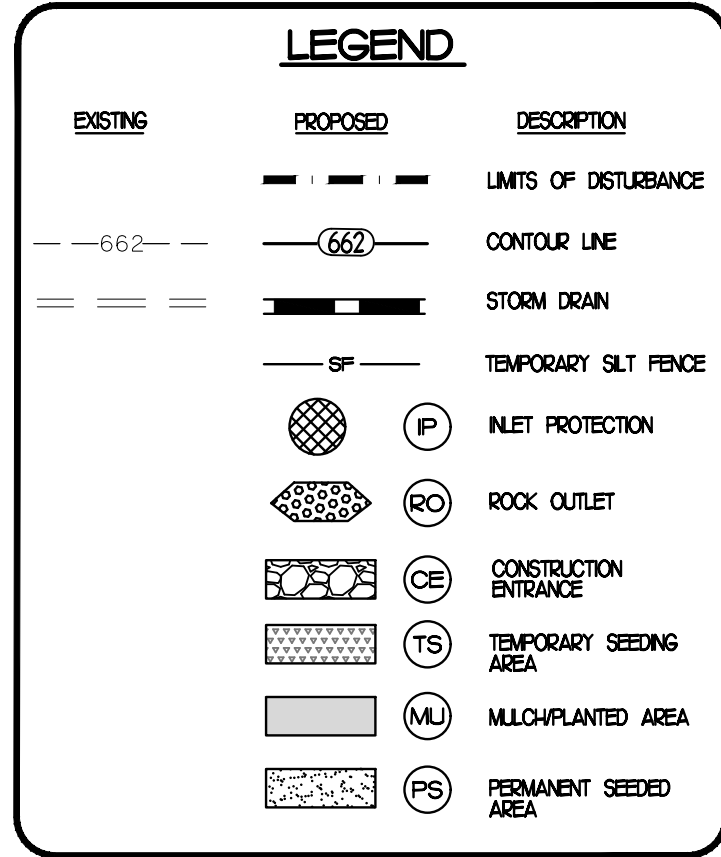
REFER TO THE EROSION CONTROL DETAIL SHEETS FOR ALL EROSION CONTROL NOTES, MAINTENANCE REQUIREMENTS, ETC.

CONTRACTOR TO REFERENCE LANDSCAPE PLAN FOR INFORMATION ON STABILIZATION TYPE FOR SEED, SOD, AND MULCH.

CONTRACTOR SHALL USE EXTREME CAUTION WHILE WORKING UNDERNEATH OVERHEAD POWER LINES. ANY DAMAGE TO THE EXISTING UTILITY POLES AND UTILITY LINES SHALL BE REPAIRED/REPLACED AT THE CONTRACTOR'S EXPENSE.

EROSION CONTROL MAINTENANCE PLAN:

- ALL SLOPES 3:1 AND STEEPER SHALL RECEIVE A CURLEX BLANKET OR APPROVED EQUAL, IN ADDITION TO HYDROSEEDING. TEMPORARY BERMS MAY BE NEEDED DAILY UNTIL ALL SLOPES ARE BROUGHT TO GRADE.
- EXCEPT AS NOTED BELOW, STABILIZATION SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN SEVEN DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS PERMANENTLY CEASED. WHERE THE INITIATION OF STABILIZATION MEASURES BY THE SEVENTH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS CONDITIONS ALLOW. WHERE CONSTRUCTION ACTIVITY WILL RESUME ON A PORTION OF THE SITE WITHIN 21 DAYS FROM WHEN ACTIVITY CEASED, (E.G. THE TOTAL TIME PERIOD THAT CONSTRUCTION ACTIVITY IS TEMPORARILY HALTED IS LESS THAN 21 DAYS) THEN STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE BY THE SEVENTH DAY AFTER CONSTRUCTION ACTIVITIES HAVE TEMPORARILY CEASED. AREAS WHERE THE SEED HAS FAILED TO GERMINATE ADEQUATELY (UNIFORM PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 70% WITHIN 30 DAYS AFTER SEEDING AND MULCHING MUST BE RE-SEEDING IMMEDIATELY, OR AS SOON AS WEATHER CONDITIONS ALLOW). PERMANENT STABILIZATION FOR ALL DISTURBED AREAS IS TO BE COMPLETE WITHIN 15 WORKING DAYS OR 90 CALENDAR DAYS (WHICHEVER IS SHORTER) FOLLOWING COMPLETION OF CONSTRUCTION. DIVERSIONS MUST BE STABILIZED PRIOR TO BECOMING FUNCTIONAL.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED EVERY SEVEN (7) DAYS OR AFTER EACH RAINFALL OCCURRENCE THAT EXCEEDS ONE-HALF (0.5) INCH. DAMAGED OR INEFFECTIVE DEVICES SHALL BE REPAIRED AND REPLACED AS NECESSARY.
- PROVIDE SILT FENCE AND/OR OTHER EROSION CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL EROSION CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFF-SITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO THE PAVED ROADWAY FROM CONSTRUCTION AREAS. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT LOADED WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- CONTRACTOR SHALL PROVIDE EROSION CONTROL BLANKETS ON ALL SLOPES STEEPER THAN 3:1. CONTRACTOR HAS THE OPTION TO UTILIZE SOD IN LIEU OF AN EROSION CONTROL BLANKET (TYPICAL).



SWEEP CONTACT DURING CONSTRUCTION CROSS DEVELOPMENT ATTN:

DRAINAGE BASIN:
MISSOURI RIVER WATERSHED

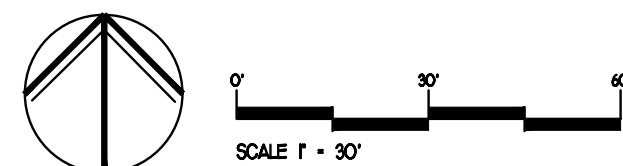
RECEIVING WATERS:
EAST FORK LITTLE BLUE RIVER,
TRIBUTARY OF THE BLUE RIVER -
MISSOURI RIVER WATERSHED

SOILS ON SITE:
• K0082 - ARISBURG-URBAN LAND
COMPLEX, 1 TO 5 PERCENT SLOPES
(HYDROLOGIC SOIL GROUP C)

SEQUENCE OF CONSTRUCTION:

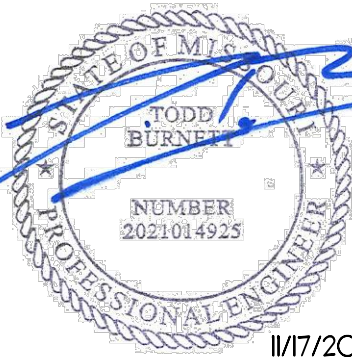
- PHASE I**
- SUBMIT PLANS FOR REVIEW
 - OBTAIN GRADING PERMIT (MAY INCLUDE PRE-CONSTRUCTION CONFERENCE). CONTRACTOR SHALL OBTAIN ALL APPROPRIATE PERMITS REQUIRED FOR DEMOLITION AND LAND DISTURBANCE ACTIVITIES.
 - CONTRACTOR SHALL CONTACT CITY OF LEE'S SUMMIT DEVELOPMENT SERVICES TO SCHEDULE A RECONSTRUCTION MEETING AT (866) 969-2000.
 - REMOVE THE MINIMAL AMOUNT OF PERIMETER SITE FEATURES IN ORDER TO INSTALL STABILIZED CONSTRUCTION EXIT AND SILT FENCE. ENSURE DRAINAGE IS DIRECTED TOWARDS SILT FENCE.
 - INSTALL INLET PROTECTION AT THE EXISTING STORM STRUCTURE OFF THE NORTH EAST CORNER OF THE PROPERTY ON THE ADJACENT PROPERTY.
 - INSTALL SWEEP SIGN, TEMPORARY STOCKPILE AREA, AND DESIGNATE AN AREA FOR THE PARKING OF CONSTRUCTION VEHICLES.
 - BEGIN DEMOLITION OF THE SITE FEATURES PER THE DEMOLITION PLAN. DO NOT BEGIN DEMOLITION OF THE PAVEMENT AND CONCRETE CURB ALONG SE 7TH TERRACE UNTIL READY TO BEGIN WATER MAIN INSTALLATION CONNECTION.
 - INSTALL CONCRETE WASHOUT AT SITE ENTRANCE/EXIT.
 - INSTALL TEMPORARY SEEDING AND MULCH STABILIZATION TO ALL AREAS WHERE WORK WILL BE CEASED FOR A PERIOD OF MORE THAN 7 DAYS.
- PHASE II**
- CONTINUALLY INSPECT AND MAINTAIN ALL BMP'S. REPAIR AND REPLACE ANY BMP'S AS NEEDED.
 - BEGIN GENERAL GRADING OF SITE. APPLY PERMANENT SEEDING TO ALL AREAS AS SOON AS THEY ARE COMPLETE. DO NOT WAIT UNTIL JOB COMPLETION TO PERMANENTLY SEED DISTURBED AREAS. ADD EROSION CONTROL BLANKETS AS NEEDED FOR ALL AREAS WITH SLOPES THAT EXCEED 3:1.
 - INSTALL UNDERGROUND DETENTION SYSTEM PER CONTECH'S SPECIFICATIONS AND THE GRADING PLAN. INSTALL STORM SEWERS AND STRUCTURES. INLET PROTECTION SHALL BE ADDED AT EACH STRUCTURE IMMEDIATELY AFTER THEY HAVE BEEN CONSTRUCTED. CONNECT TO EXISTING STORM STRUCTURE AS SHOWN. REPAIRING/REPLACING ANY APPURTENANCES AS NEEDED.
 - INSTALL UNDERGROUND UTILITIES AND COORDINATE WITH EACH UTILITY PROVIDER. BRING ALL BUILDING'S UTILITIES TO THE PROPOSED PAD LOCATION AND CAP FOR FUTURE CONNECTION.
 - INSTALL WATER LINE EXTENSION TO EXISTING MAIN NORTH OF SE 7TH TERRACE. IMMEDIATELY REPAVE ASPHALT AND INSTALL CURB & GUTTER PER CITY OF LEE'S SUMMIT STANDARDS AND SPECS.
 - COMMENCE GRADING OF BUILDING PAD AND AREAS AROUND BUILDING PAD.
 - INSTALL CURB AND GUTTER AND FINE GRADE PARKING AREAS. STONE BASE SHALL BE INSTALLED IN ALL PAVED AREAS AS SOON AS PRACTICABLE. CONSTRUCTION EXIT SHALL BE REMOVED ONLY ONCE THE BUILDING PAD, PARKING LOT, AND DRIVEWAY HAVE BEEN STABILIZED WITH STONE BASE.
 - PAVE SITE AND INSTALL APPROPRIATE INLET PROTECTION DEVICES FOR PAVED AREAS AS WORK PROGRESSES.
 - COMPLETE BUILDING CONSTRUCTION. SIDEWALKS, PLANTINGS, ETC.
 - REMOVE TEMPORARY BMP'S AS SOON AS THEIR CONTRIBUTING DRAINAGE AREAS HAVE BEEN STABILIZED. SEED AND MULCH ANY DISTURBANCE FROM REMOVAL OF TEMPORARY BMP'S.
 - INSPECT/ CLEAN/ FLUSH UNDERGROUND DETENTION SYSTEM. CONTRACTOR SHALL CONTACT MANUFACTURER FOR FINAL INSPECTION AND CERTIFICATION.
 - CONTACT MOHR FOR FINAL INSPECTION AND OWNER FOR COORDINATION OF FILING THE NOTICE OF TERMINATION (NOT).

EROSION CONTROL PHASE 2



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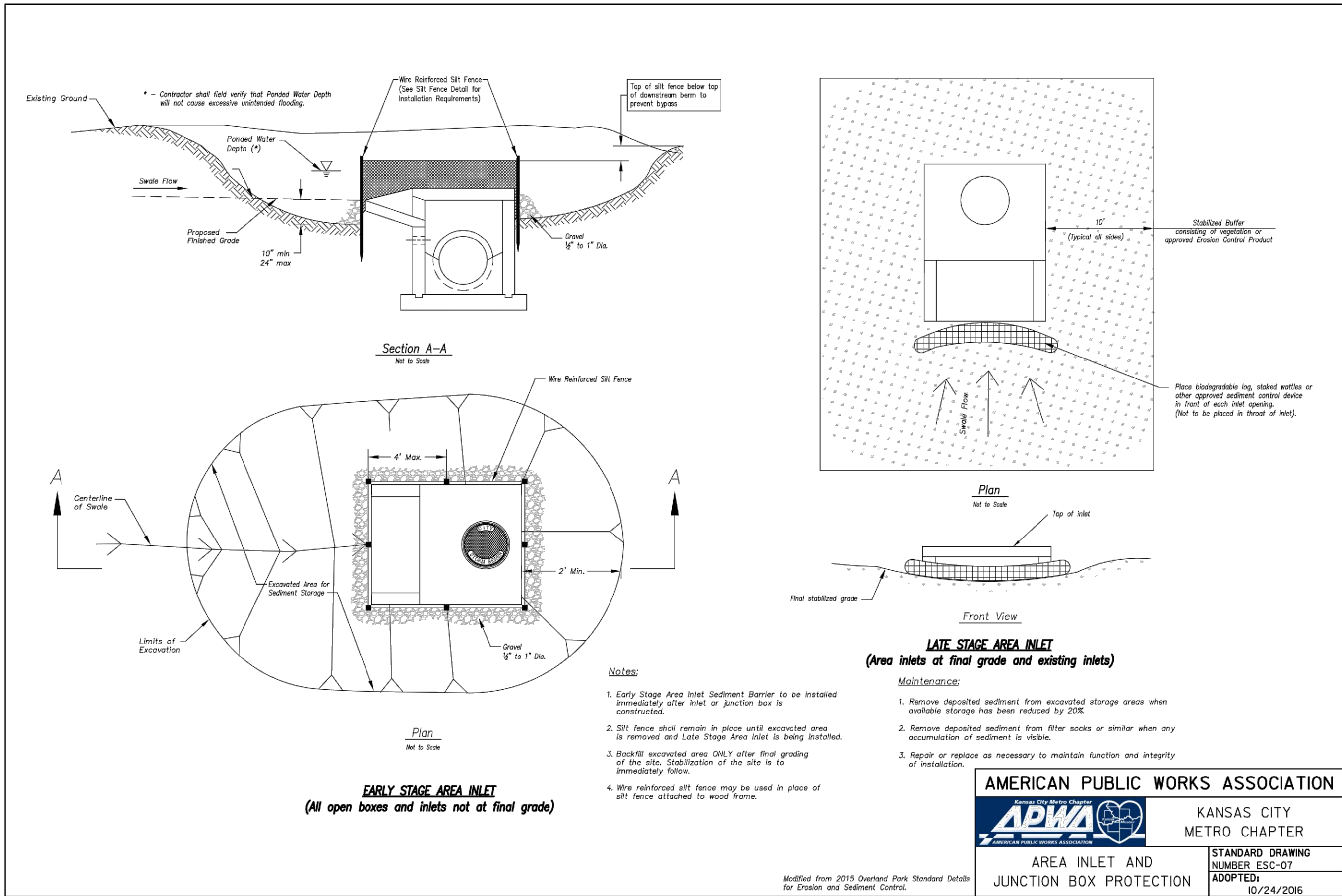
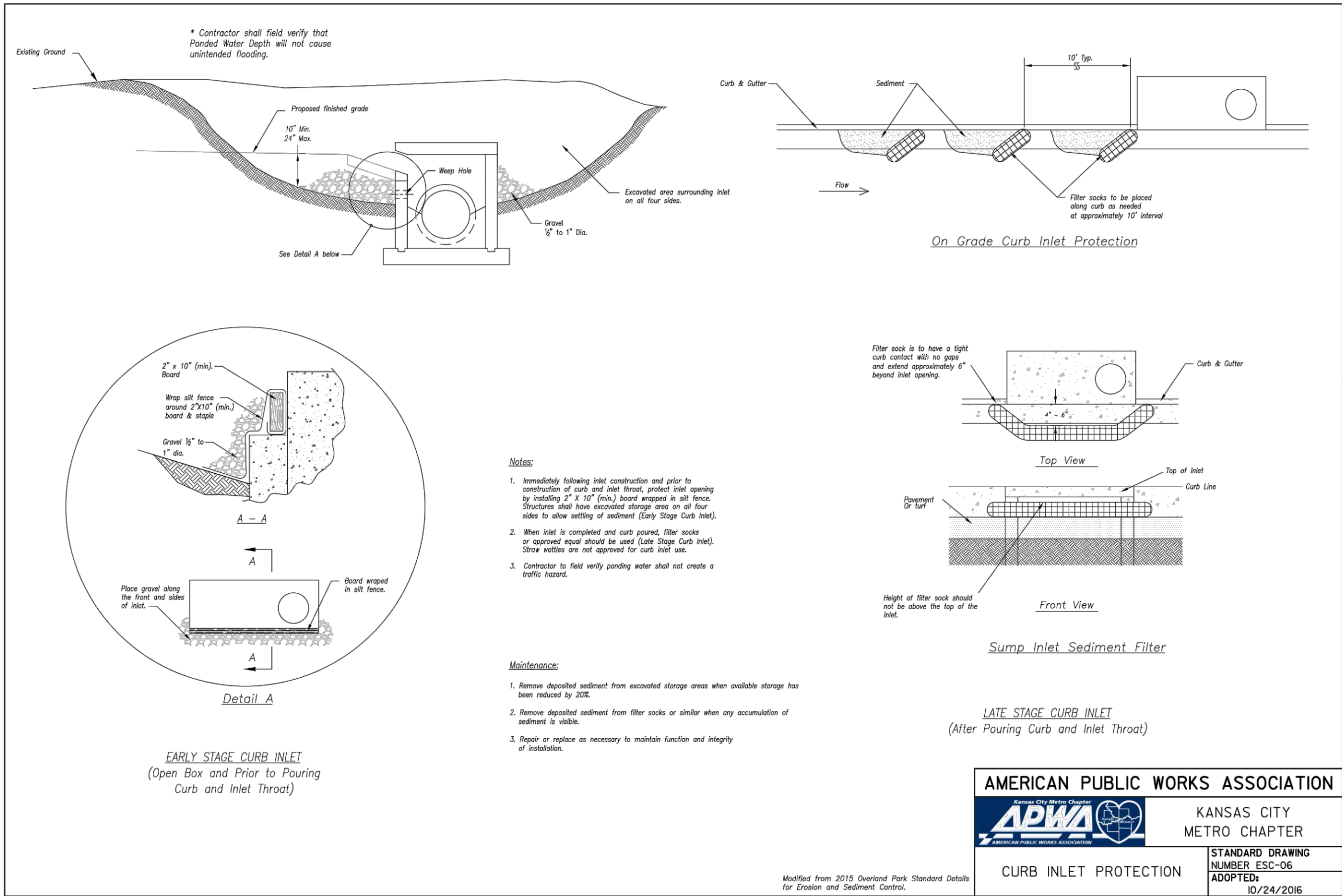
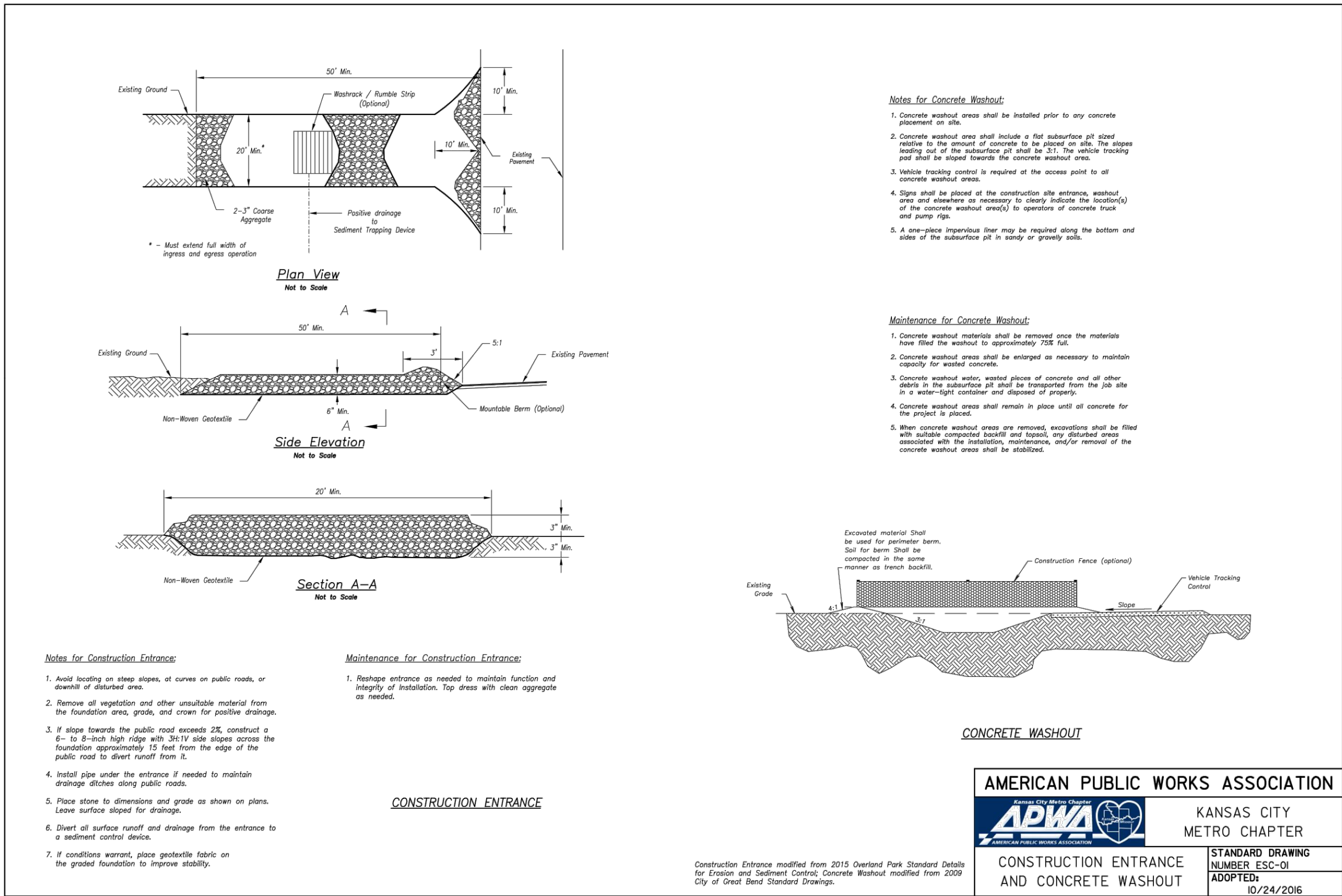


CALIBER COLLISION
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LEE'S SUMMIT, MO 64063
CROSS DEVELOPMENT, CC LEE'S SUMMIT, LLC
4388 MARSH RIDGE ROAD
CARROLLTON, TX 75010
TEL (214) 614-8252

DRAWN
BAC
CHECKED
TMB
DATE
1/17/2021
SCALE

DRAWING

5



REVISIONS	BY

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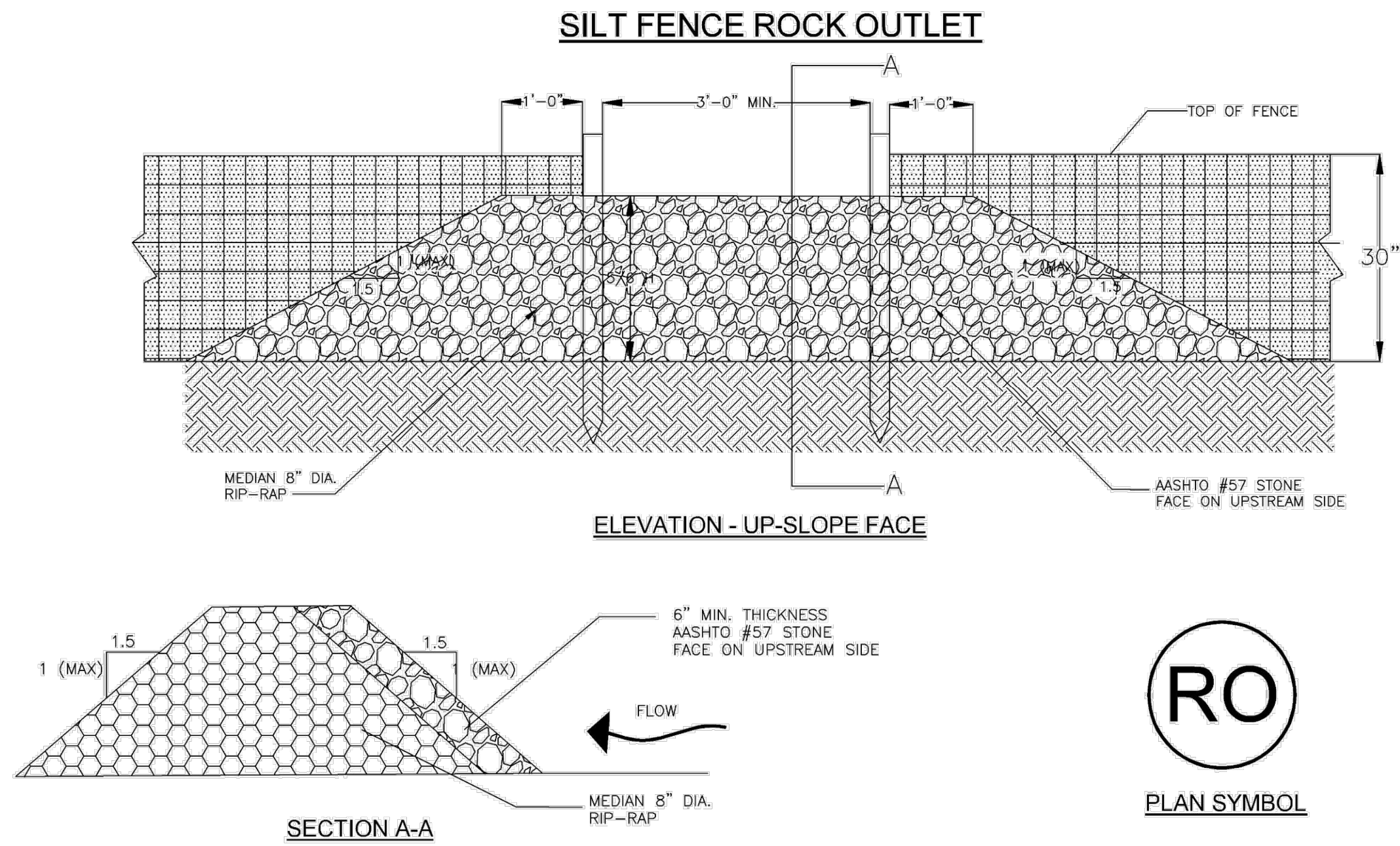
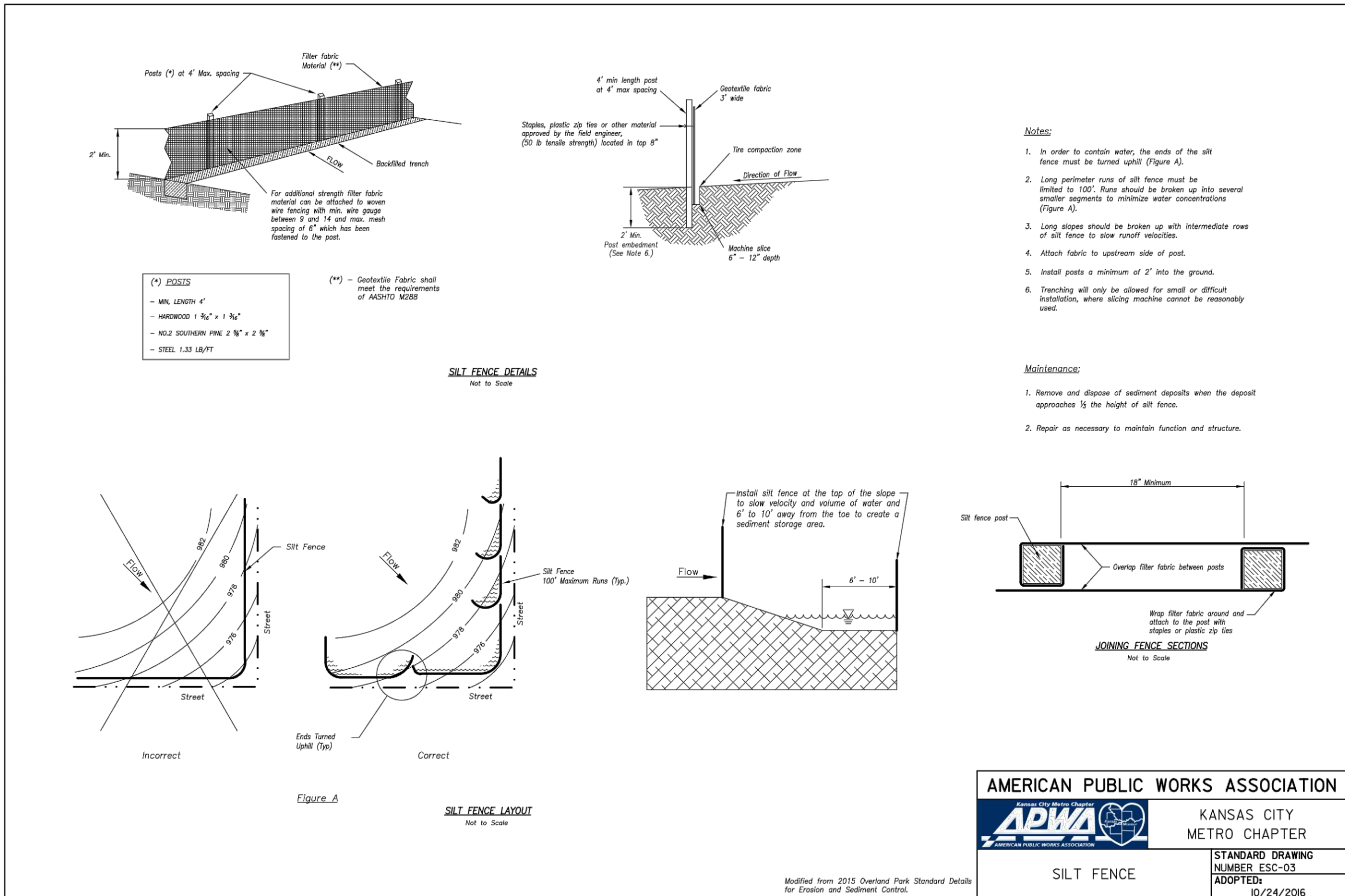


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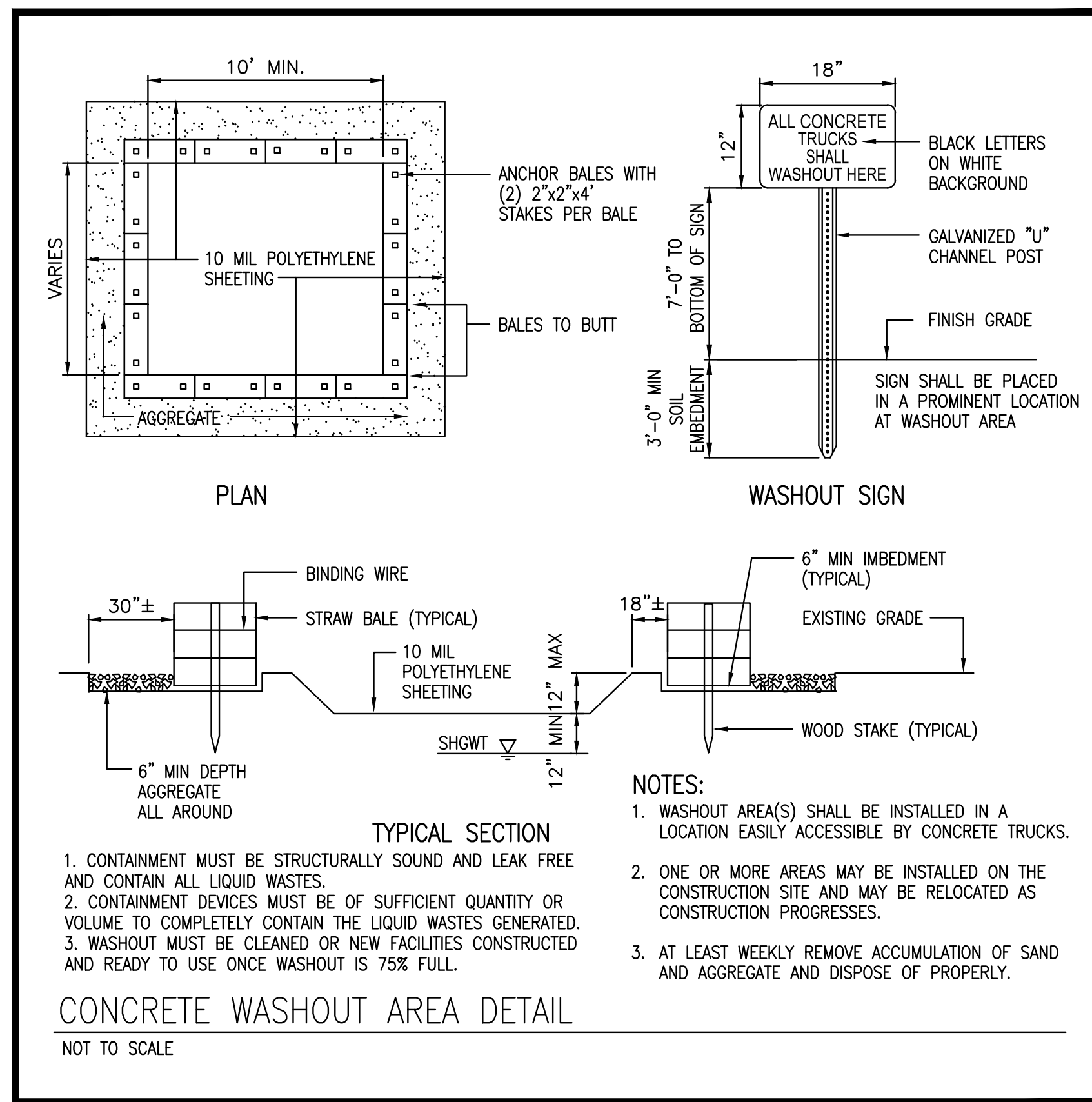
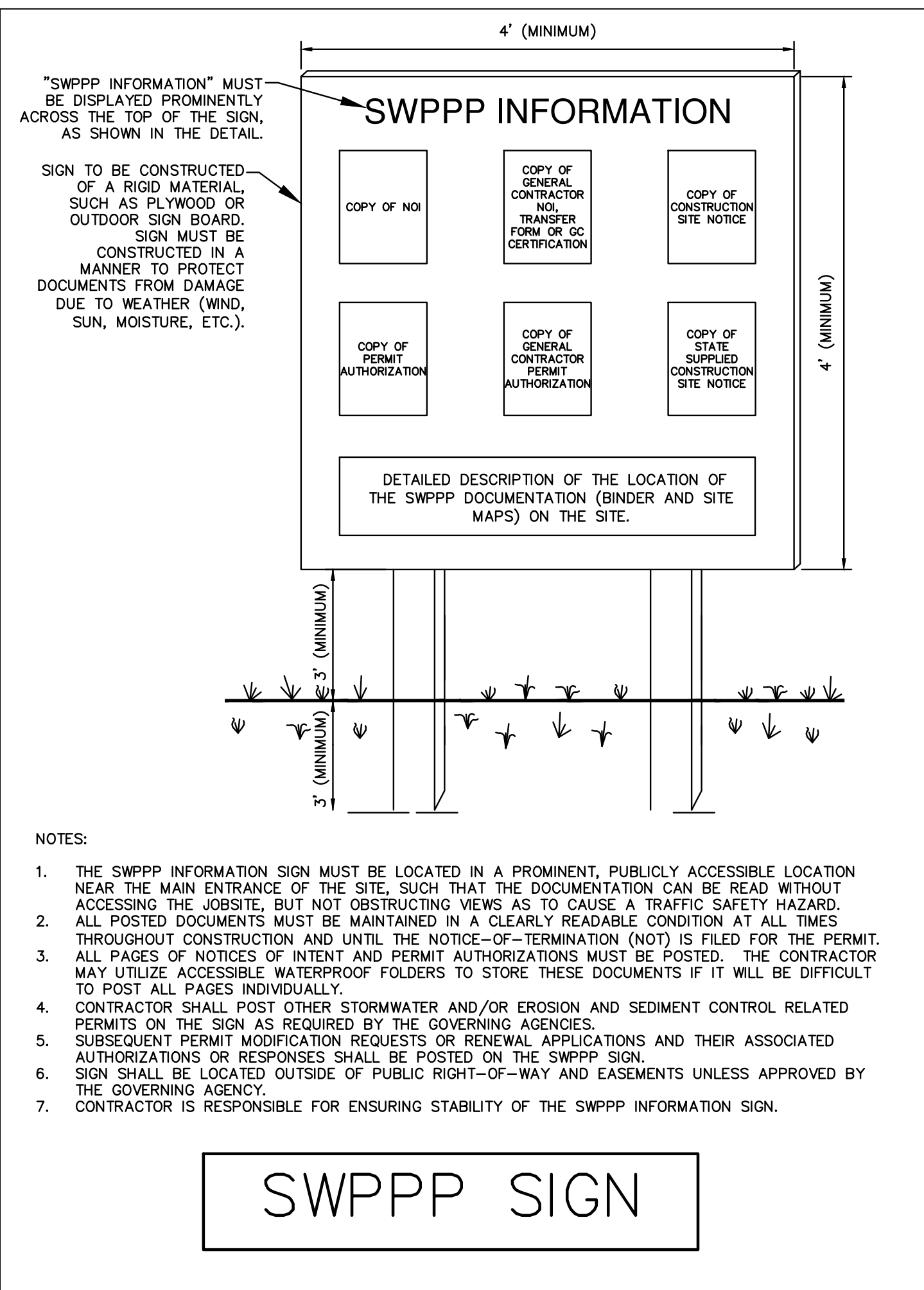
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EROSION CONTROL DETAILS

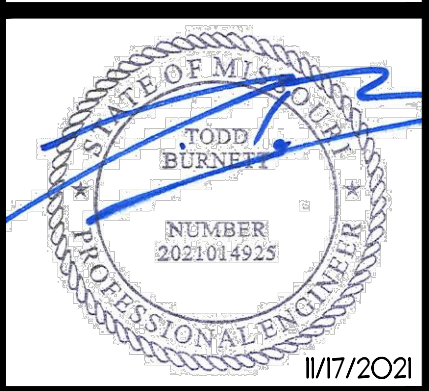


- NOTES:
- WASHED STONE (#57) TO BE REMOVED AND REPLACED ONCE IT BECOMES CLOGGED WITH SEDIMENT.
 - SEDIMENT TO BE REMOVED WHEN ACCUMULATIONS REACH $\frac{1}{3}$ HEIGHT OF SILT FENCE
 - THE KEY TO FUNCTIONAL ROCK OUTLETS IS WEEKLY INSPECTIONS, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.



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SCALE	
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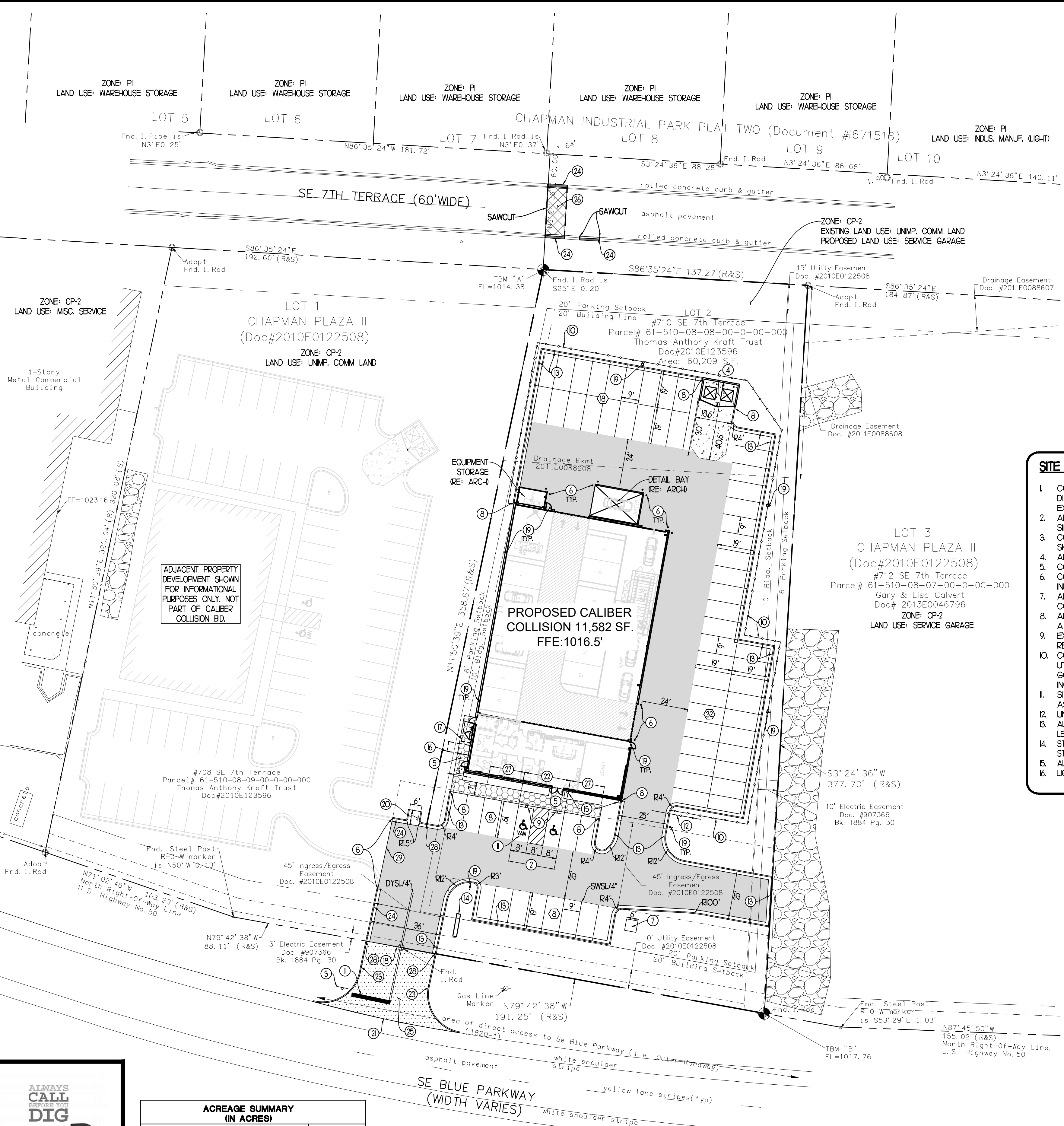
ACREAGE SUMMARY (IN ACRES)	
TOTAL PARCEL AREA	1.38
ON-SITE DISTURBED AREA	1.38
OFF-SITE DISTURBED AREA	0.55
TOTAL DISTURBED AREA	1.93
IMPERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY & INCLUDES GRAVEL)	0.48
PERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY)	0.90
IMPERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.90
PERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.48
IMPERVIOUS AREA INCREASE (PRE VS POST) (ON-SITE ONLY)	0.42
IMPERVIOUS PERCENTAGE (AT POST-CONSTRUCTION STATUS)	65.2%

U.S. HIGHWAY NO. 50
(WIDTH VARIES)

SE BLUE PARKWAY
(WIDTH VARIES)

CITY OF LEE'S SUMMIT LAND USE SCHEDULE	
TOTAL FLOOR AREA:	11582 SF
NUMBER OF DWELLINGS:	1
LAND AREA:	1.38 ACRES
REQUIRED PARKING SPACES:	36
PROVIDED PARKING SPACES:	66
IMPERVIOUS COVERAGE:	0.90 ACRES
FLOOR AREA RATIO (FAR):	0.09
DWELLING UNITS PER ACRE:	0.72
LAND USES TO BE PERMITTED:	1

THERE ARE NO OIL OR GAS WELLS
LOCATED ON THIS PROPERTY PER THE
MISSOURI DNR WELL DATABASE MAPS.



ACREAGE SUMMARY AND PARKING SUMMARY:	
ACREAGE SUMMARY:	
TOTAL PARCEL:	1.38 AC.
GROSS FLOOR AREA:	11582 SF
IMPERVIOUS COVER:	0.90 AC. (65%)
PARKING SUMMARY:	
MIN REQUIRED:	3 SPACES PER BAY (EACH BAY CAN COUNT)
12 BAYS X 3 =	36 SPACES REQUIRED
PROVIDED:	16* CUSTOMER SPACES PROVIDED
50 STORAGE SPACES PROVIDED	
2** ADA SPACES PROVIDED	
TOTAL:	66 SPACES TOTAL
NOTES:	
*	GATES FOR STORAGE PARKING SPACES WILL REMAIN OPEN DURING BUSINESS HOURS TO ALLOW FOR OVERFLOW PARKING (SHOULD IT BE NEEDED).
**	TOTAL NUMBER OF ADA PARKING SPACES IS TO BE RESULT OF THE NUMBER OF CUSTOMER SPACES PROVIDED AND NOT TOTAL SPACES PROVIDED.

DEPICTION OF THE DEVELOPMENT ON THE NEIGHBORING PROPERTY TO THE WEST IS SHOWN FOR PLANNING INFORMATION PURPOSES ONLY. THE LAYOUT AND LOCATIONS OF THE SITE FEATURES SHOWN ARE APPROXIMATE AND SHOULD NOT BE CONSIDERED DEFINITE. CONTRACTOR SHALL NOT INCLUDE THE NEIGHBORING DEVELOPMENT IN THE CALIBER COLLISION BID.

LAND USES OF THE ADJACENT PROPERTIES ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. THE LAND USES SHOWN WERE TAKEN FROM THE JACKSON COUNTY GIS.

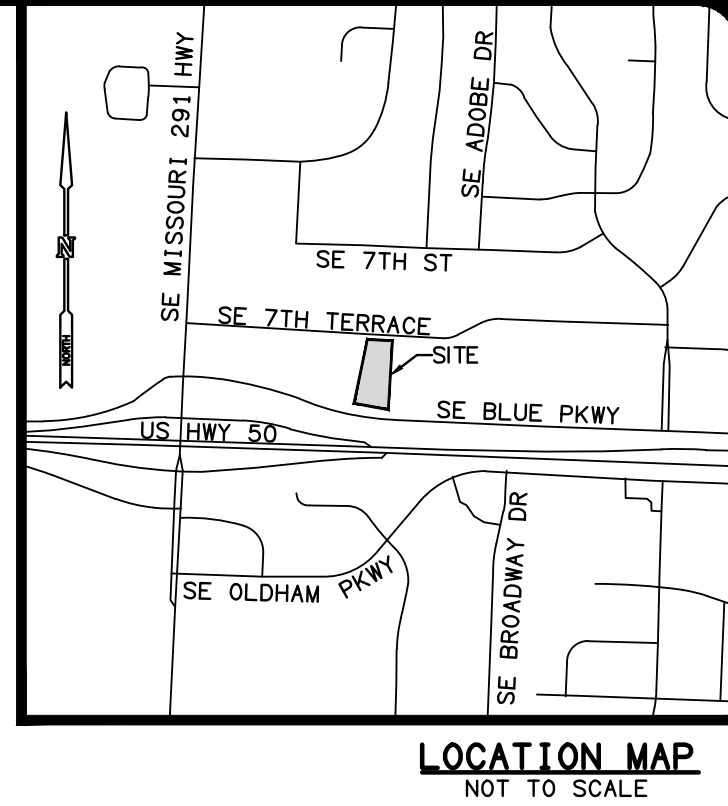
SITE NOTES:

- CONTRACTOR SHALL REFER TO THE CALIBER COLLISION ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS, AND FOR THE EXACT LOCATION OF UTILITY ENTRANCES, ROOF LEADERS, EXIT DOORS, EXIT STOOPS, ETC.
- ALL DIMENSIONS ARE TO BUILDING FACE, FACE OF CURB, CENTERLINE OF PARKING, OR EDGE OF SIDEWALK UNLESS NOTED OTHERWISE.
- CONTRACTOR SHALL PROVIDE ALL LABOR AND MATERIALS FOR THE INSTALLATION OF TRAFFIC SIGNAGE AND PAVEMENT MARKINGS AS SHOWN ON THE CONSTRUCTION PLANS.
- ALL STRIPING SHALL BE WHITE, 4" WIDE, UNLESS NOTED OTHERWISE.
- CONTRACTOR SHALL COORDINATE FINAL LOCATION OF THE MONUMENT SIGN WITH CALIBER COLLISION.
- CONTRACTOR SHALL REFER TO OTHER PLANS WITHIN THIS CONSTRUCTION SET FOR OTHER PERTINENT INFORMATION.
- ALL WORK AND MATERIALS SHALL COMPLY WITH THE CITY OF LEE'S SUMMIT REGULATIONS AND CODES AND OSHA STANDARDS.
- ALL DISTURBED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL, SEED, MULCH AND WATER UNTIL A HEALTHY STAND OF GRASS IS ESTABLISHED, UNLESS NOTED OTHERWISE.
- EXISTING SITE IMPROVEMENTS WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID.
- SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY HEIDEMAN ASSOCIATES, INC.
- UNLESS NOTED OTHERWISE, PAVEMENT SHALL BE STANDARD DUTY ASPHALT.
- ALL HANDICAP SPACES ARE TO RECEIVE A HANDICAP SIGN. ALL HANDICAPPED STALLS SHALL BE PER CITY OF LEE'S SUMMIT STANDARDS. SPACES MARKED "VAN" SHALL RECEIVE A "VAN ACCESSIBLE" SIGN.
- STOP SIGNS SHALL MEET THE CRITERIA OF THE STATE OF MISSOURI DEPT. OF TRANSPORTATION AND MUTCD STANDARDS.
- ALL PARKING SPACES ARE TO BE 9.0' WIDE AND 9.0' TYPICAL UNLESS NOTED OTHERWISE.
- LIGHT POLES ARE SHOWN FOR REFERENCE ONLY. RE: PHOTOMETRIC PLAN.

KEYNOTES:

- 24" WHITE THERMOPLASTIC PAINT STOP BAR (SEE DETAILS)
- ACCESSIBLE PARKING (SEE DETAIL)
- STOP SIGN (SEE DETAIL)
- DUMPSTER ENCLOSURE W/ GATES (RE: ARCH)
- NEW SIDEWALK (SEE DETAIL AND PLAN FOR WIDTH)
- 6" DIAMETER BOLLARD (SEE DETAIL)
- CONCRETE TRANSFORMER PAD (SEE DETAIL)
- TERMINATE CURB AND GUTTER
- HANDICAP PARKING SIGN IN 6' BOLLARD (SEE DETAIL)
- 6" VINYL PRIVACY FENCE (SEE DETAIL)
- ACCESSIBLE SPACE WITH SIGN "VAN" INDICATES VAN ACCESSIBLE, SIGN MOUNTED IN BOLLARD, SEE DETAIL
- 26" SLIDING GATE (RE:ARCH FOR GATE DETAIL)
- "TYPE CG-1" CONCRETE STRAIGHT BACK CURB & GUTTER PER CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATION (SEE DETAIL)
- SITE SIGNAGE, NOT PART OF THE SITE PLAN APPROVAL PERMITTED BY OTHERS UNDER SEPARATE APPLICATION
- KNOX BOX (RE: ARCH)
- 136" X 6" GAS CLEAR SPACE AREA
- 160" X 54" ELECTRICAL CLEAR SPACE AREA
- TRANSITION FROM ON-SITE ASPHALT TO CITY OF LEE'S SUMMIT STANDARD CONCRETE FOR COMMERCIAL DRIVEWAYS. ENSURE SMOOTH TRANSITION.
- SITE LIGHTING (RE: PHOTOMETRIC PLANS)
- CONCRETE FLUME (SEE DETAIL)
- TRANSITION FROM CITY OF LEE'S SUMMIT CONCRETE TO CITY OF LEE'S SUMMIT ASPHALT PAVEMENT. ENSURE SMOOTH TRANSITION.
- SIDEWALK FLUSH WITH ASPHALT
- "TYPE C-1" CONCRETE STRAIGHT CURB PER CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATIONS. SEE CITY DETAIL
- "TYPE CG-2" CONCRETE ROLL BACK CURB & GUTTER PER CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATIONS. SEE CITY DETAIL
- COMMERCIAL DRIVEWAY PER CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATIONS. SEE DETAIL
- ASPHALT PAVING FOR ROADWAY TRENCH REPAIR PER CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATIONS. SEE DETAIL
- SIDEWALK TAPER, TAPER INTEGRAL SIDEWALK FROM A HEIGHT OF 0' TO 6" OVER 18 FEET
- TRANSITION IN CURB/ CURB & GUTTER TYPE. SEE SITE PLAN FOR ADDITIONAL INFORMATION
- EDGE OF ASPHALT TO ADJACENT PROPERTY

PROPERTY DATA	
PN:	61-510-08-00-0-00-000
ADDRESS:	710 SE 7TH TERRACE LEE'S SUMMIT, MO 64063
ZONING & SETBACK DATA	
ZONED: CP-2, PLANNED COMMUNITY COMMERCIAL	
BUILDING SETBACKS:	LANDSCAPE SETBACKS:
FRONT/STREET: 15'	PUBLIC STREET: 20'
SIDE: 10'	SIDE STREET: 6'
REAR: 20'	
MAXIMUM BUILDING HEIGHT: 60'	
PROPOSED BUILDING HEIGHT: 26'	
FLOOD INFORMATION	
FLOOD ZONE "X" (AREA OF MINIMUM FLOOD HAZARD) PER FEMA MAP NO. 29050C0438G EFFECTIVE DATE: JANUARY 20, 2017	

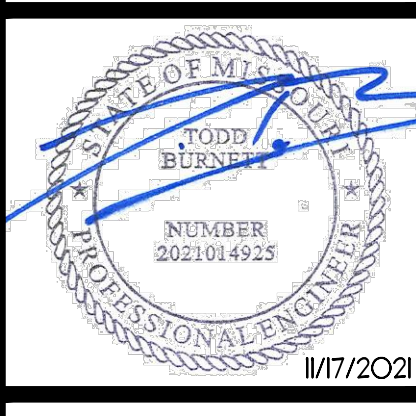


PAINTING STRIPES LEGEND	
SWSL/4"	SINGLE WHITE SOLID LINE / 4" WIDE
DYSL/4"	DOUBLE YELLOW SOLID LINE / 4" WIDE

SYMBOL	DESCRIPTION
	HANDICAPPED SYMBOL
	NUMBER OF PARKING SPACES
	SITE SIGNAGE
	BOLLARD
	6' SCREEN FENCE
	CONCRETE PAVING/SIDEWALK
	HEAVY DUTY CONCRETE
	HEAVY DUTY ASPHALT*
	STANDARD DUTY ASPHALT*
	CITY SPEC'D CONCRETE**
	CITY SPEC'D ASPHALT**
	PAINTED STOP LINE
	SAWCUT LINE
* COORDINATE WITH THE PAVEMENT DESIGN RECOMMENDATIONS IN THE GEOTECH REPORT PREPARED BY INTERTEK PSL DATED MAY 21, 2021.	
** REFERENCE THE CITY OF LEE'S SUMMIT STANDARDS AND SPECIFICATIONS FOR ADDITIONAL INFORMATION ON PAVING REQUIREMENTS.	

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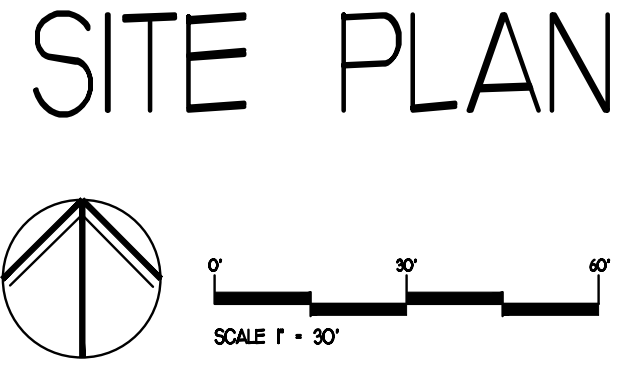


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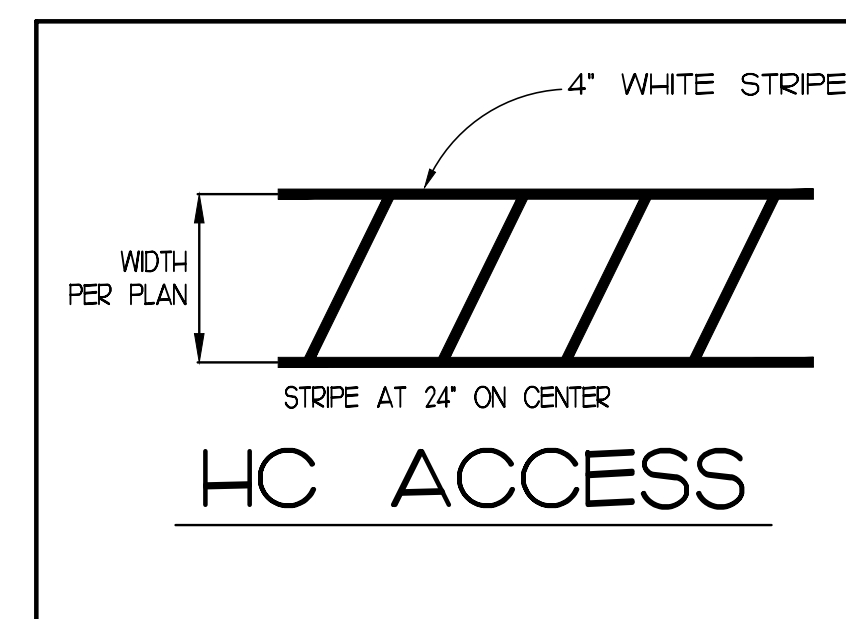
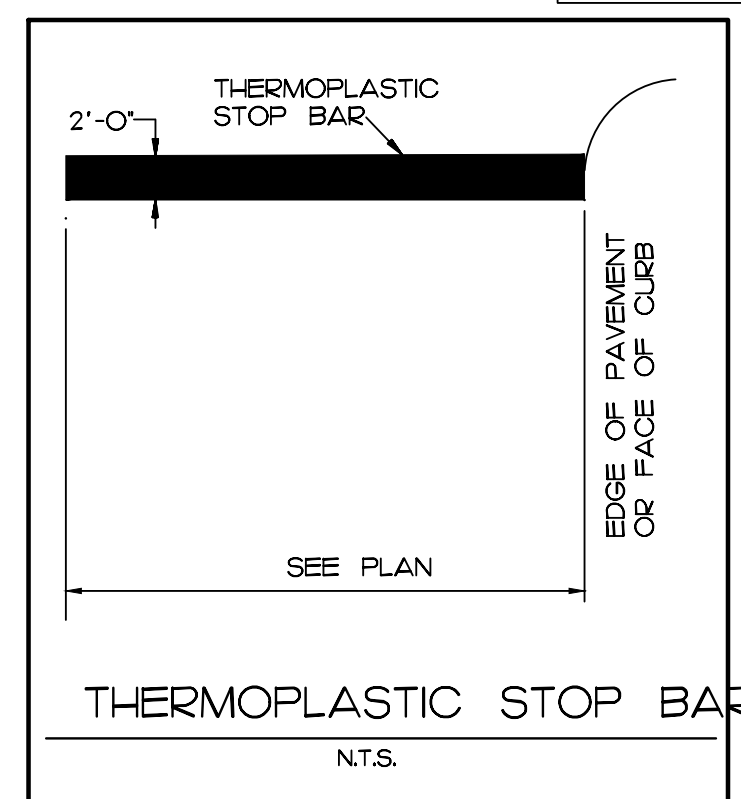
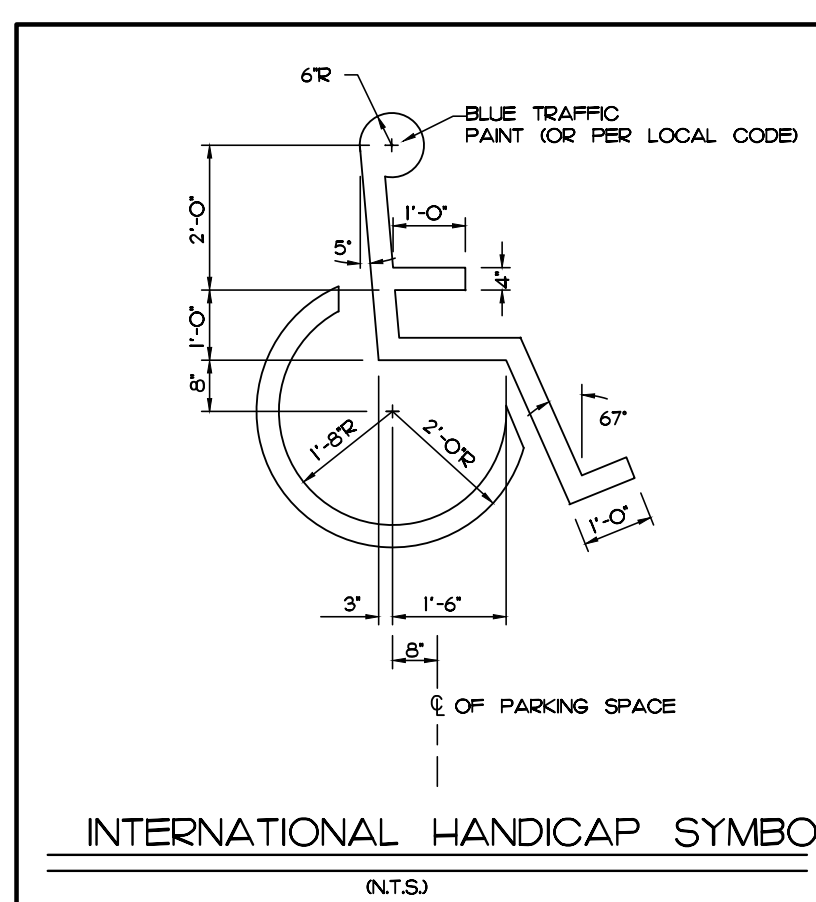
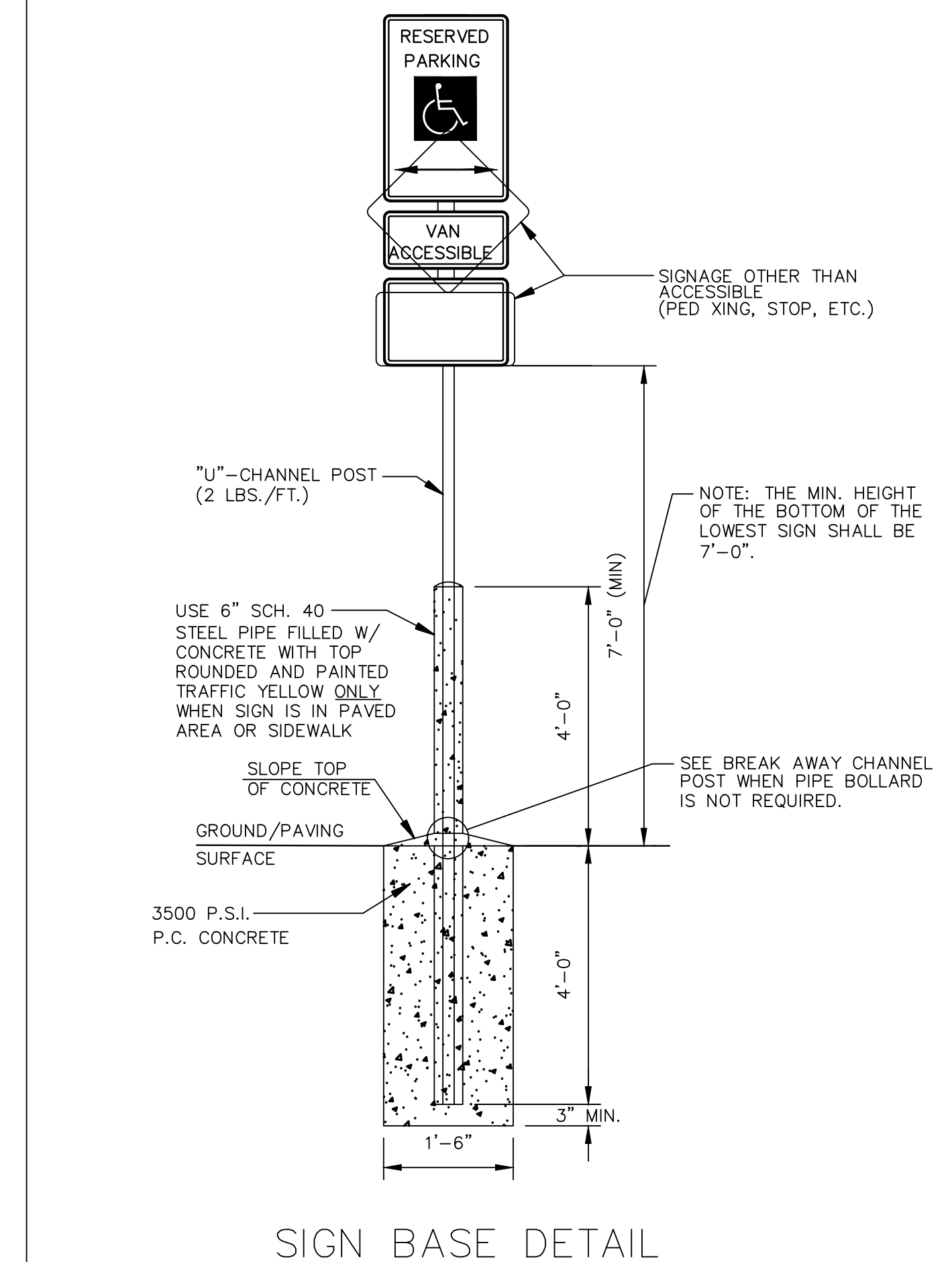
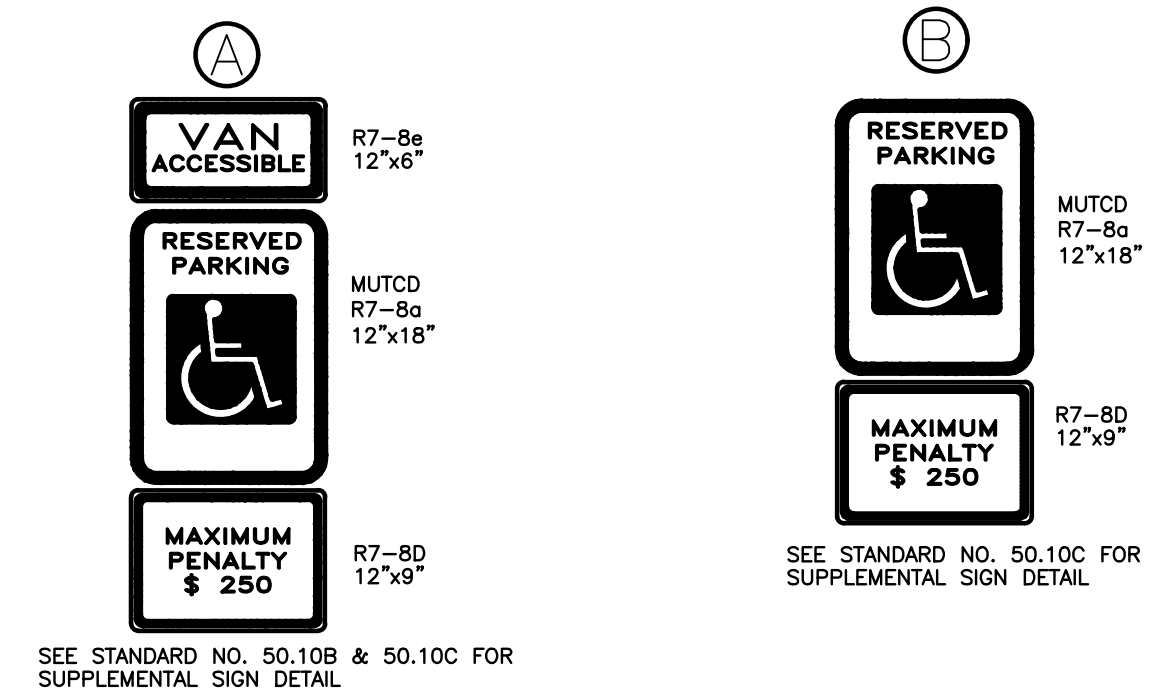
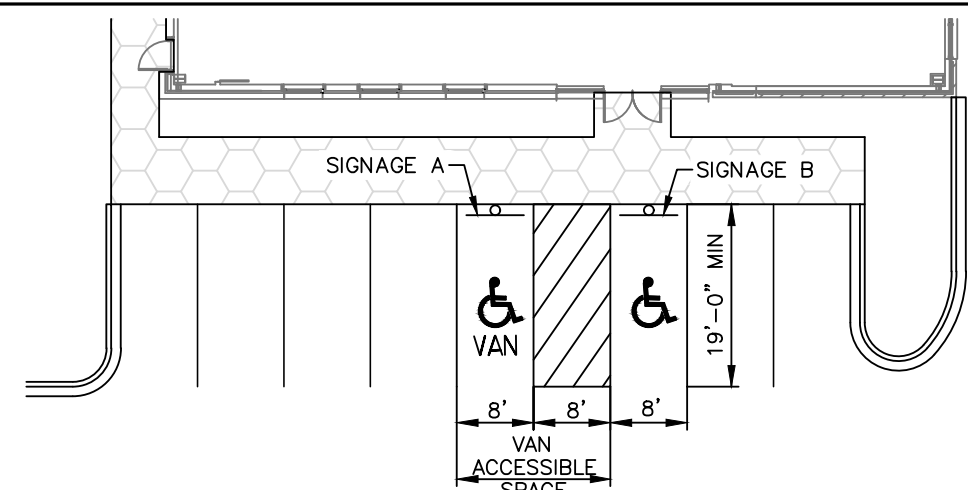
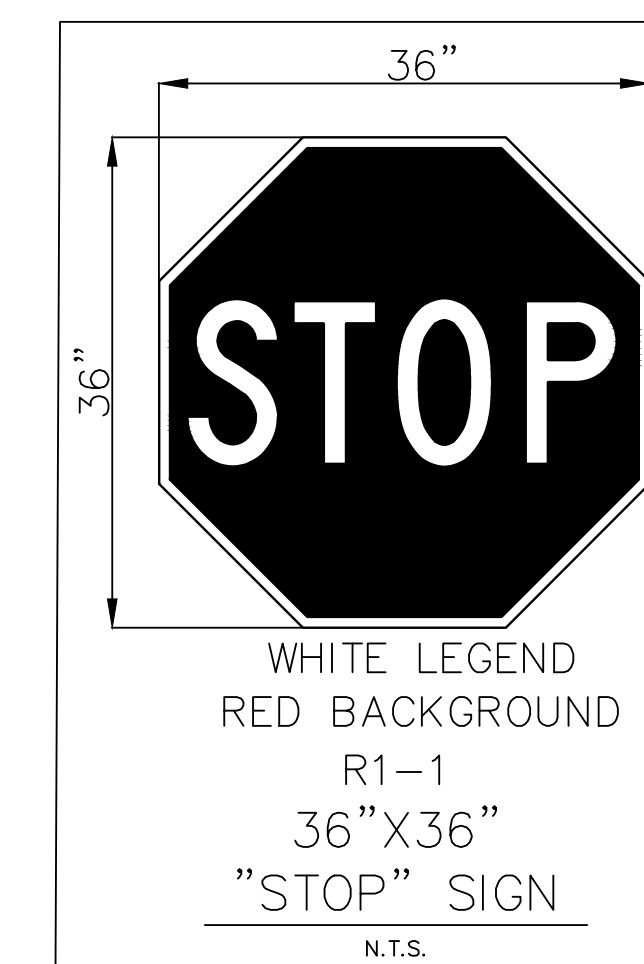
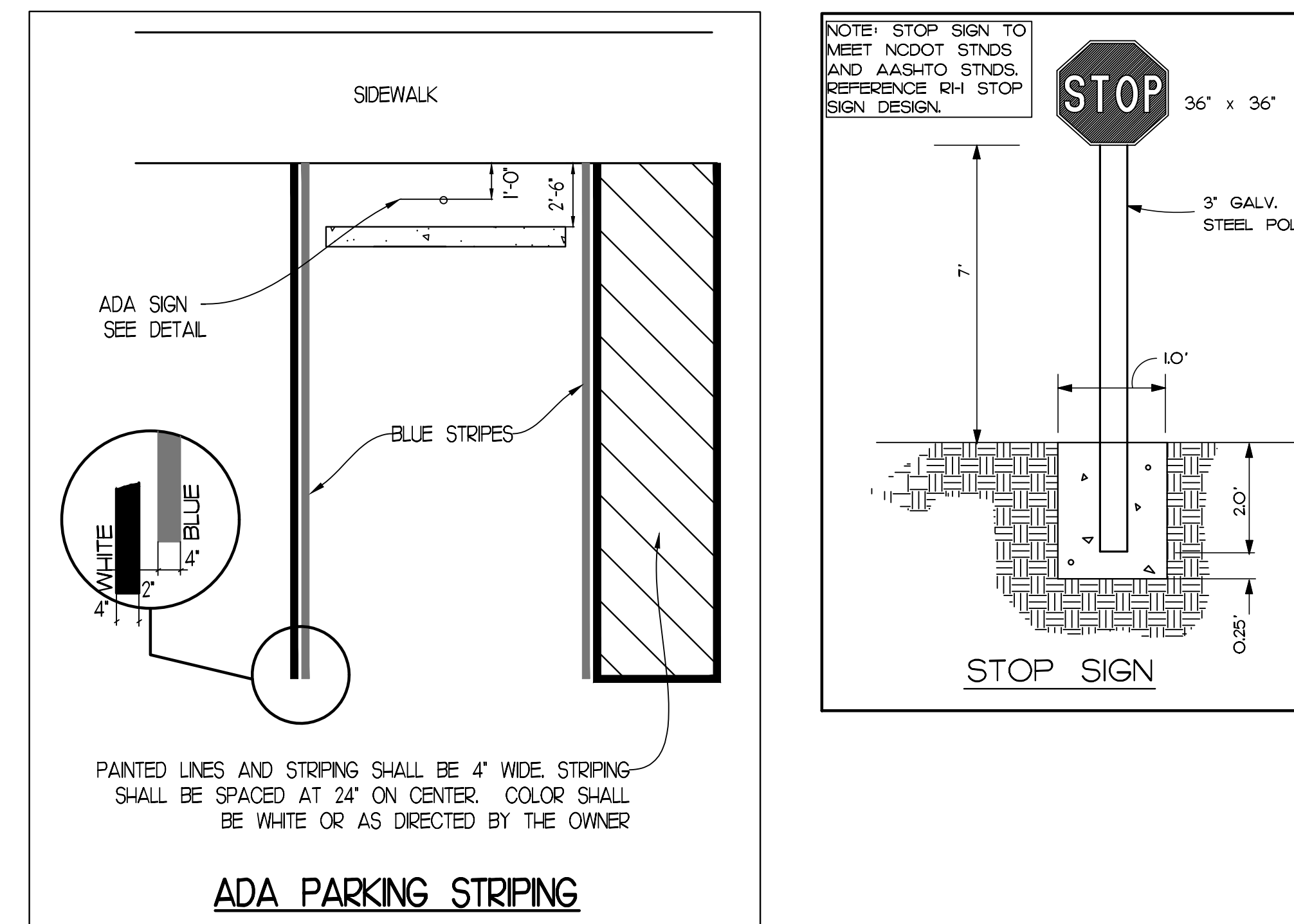
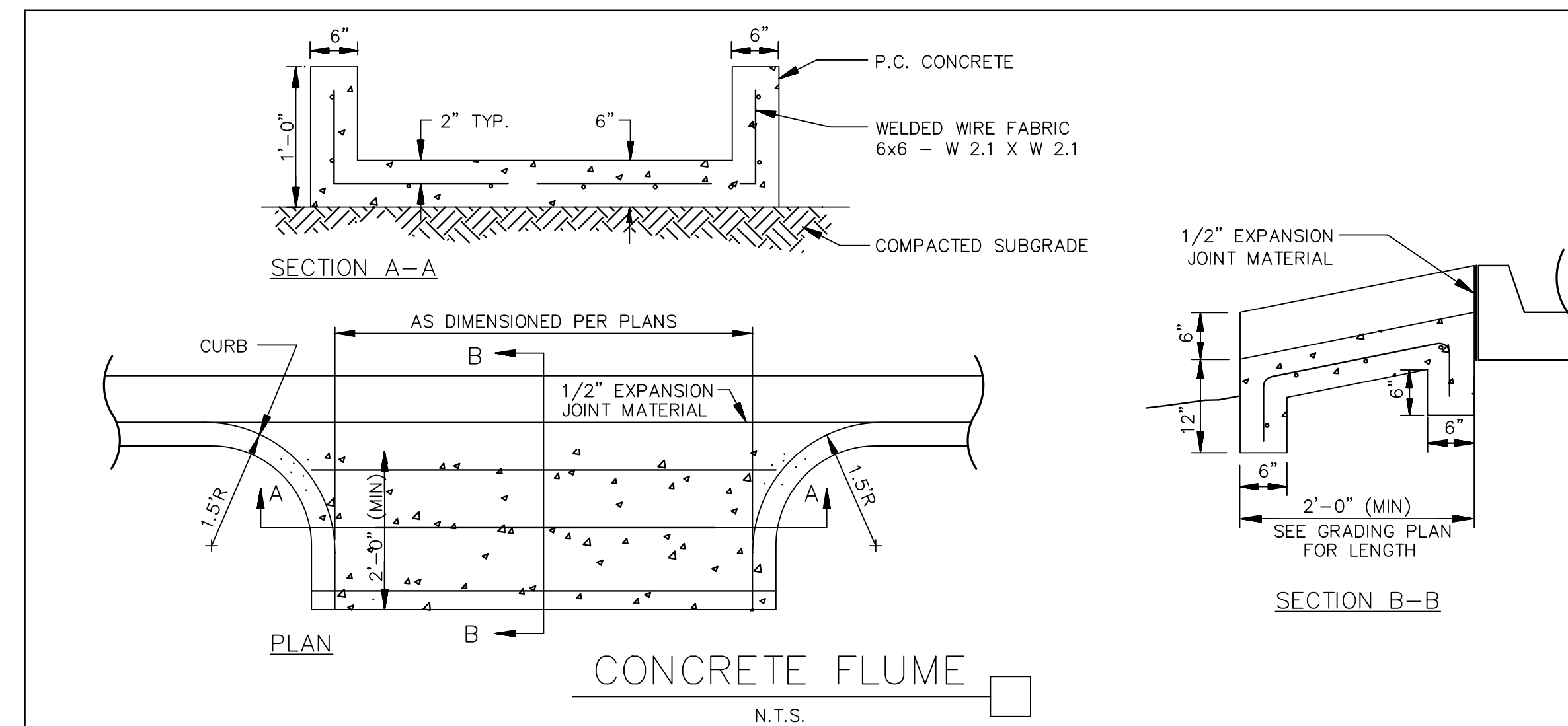
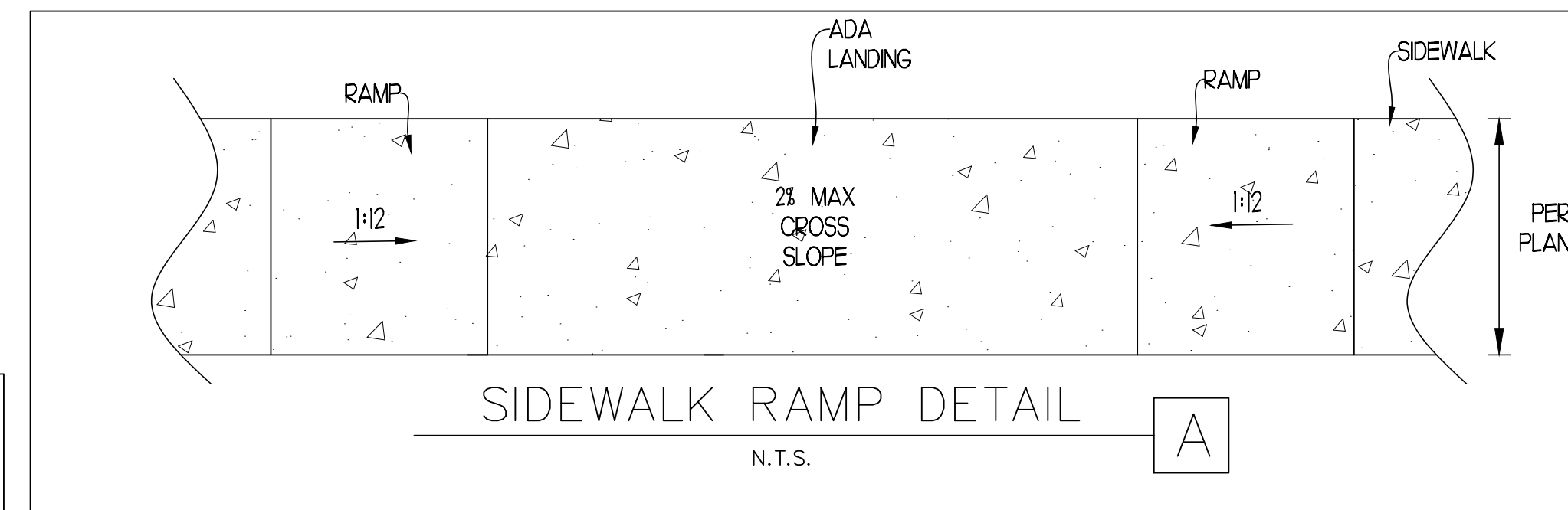
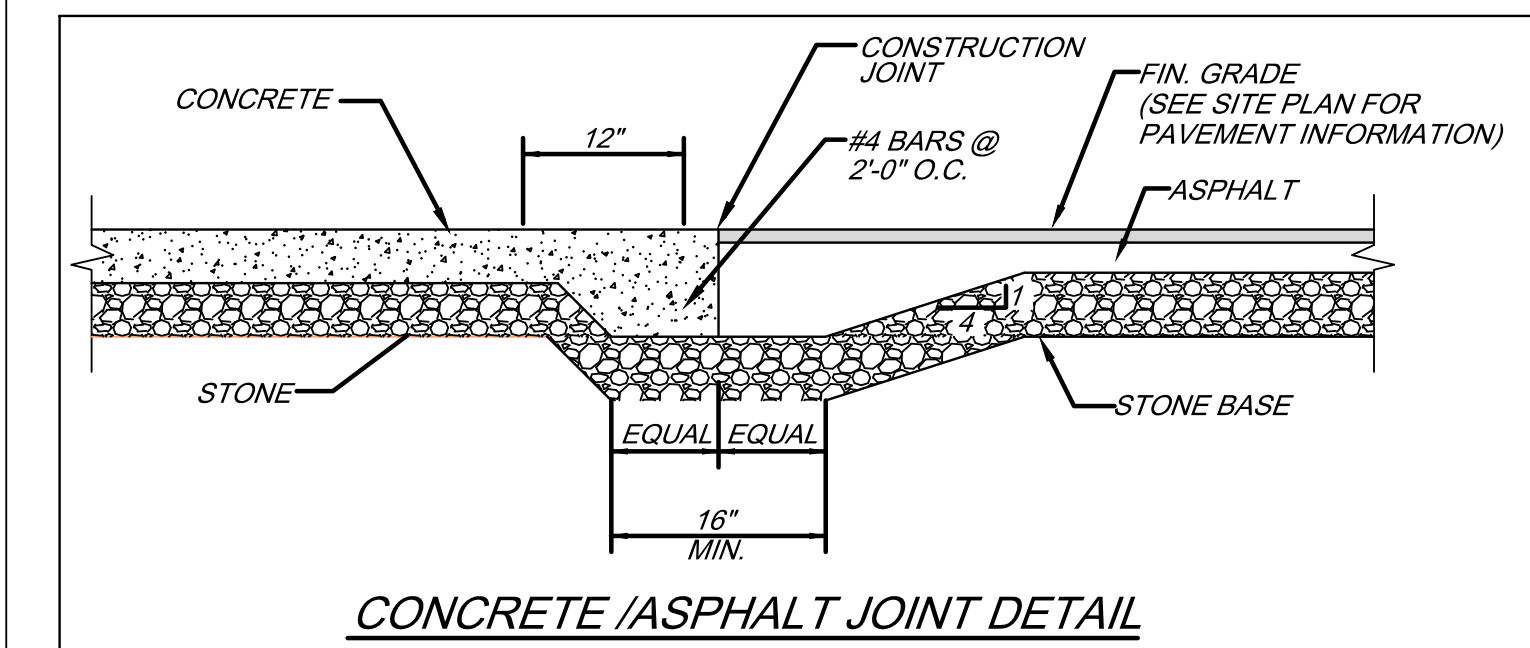
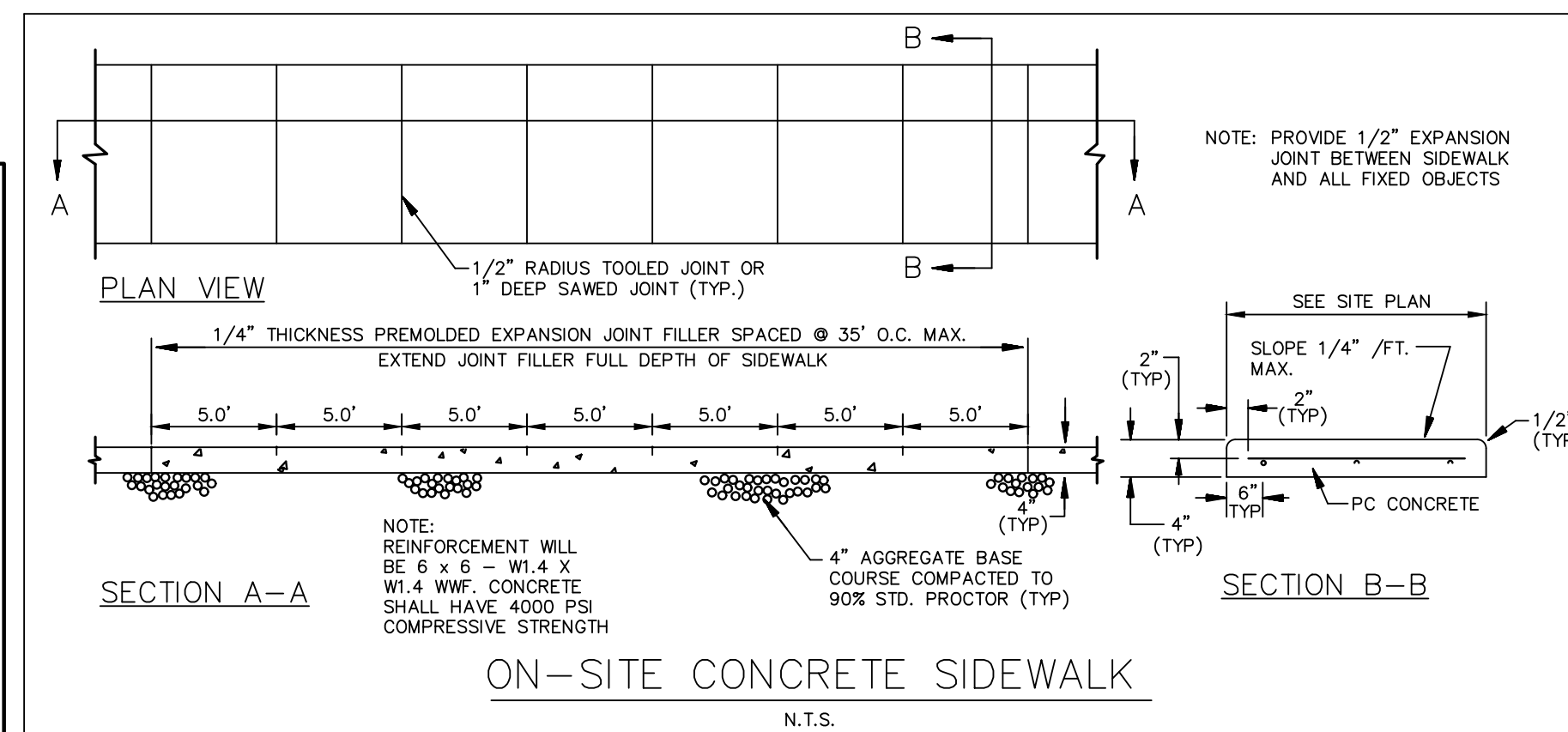
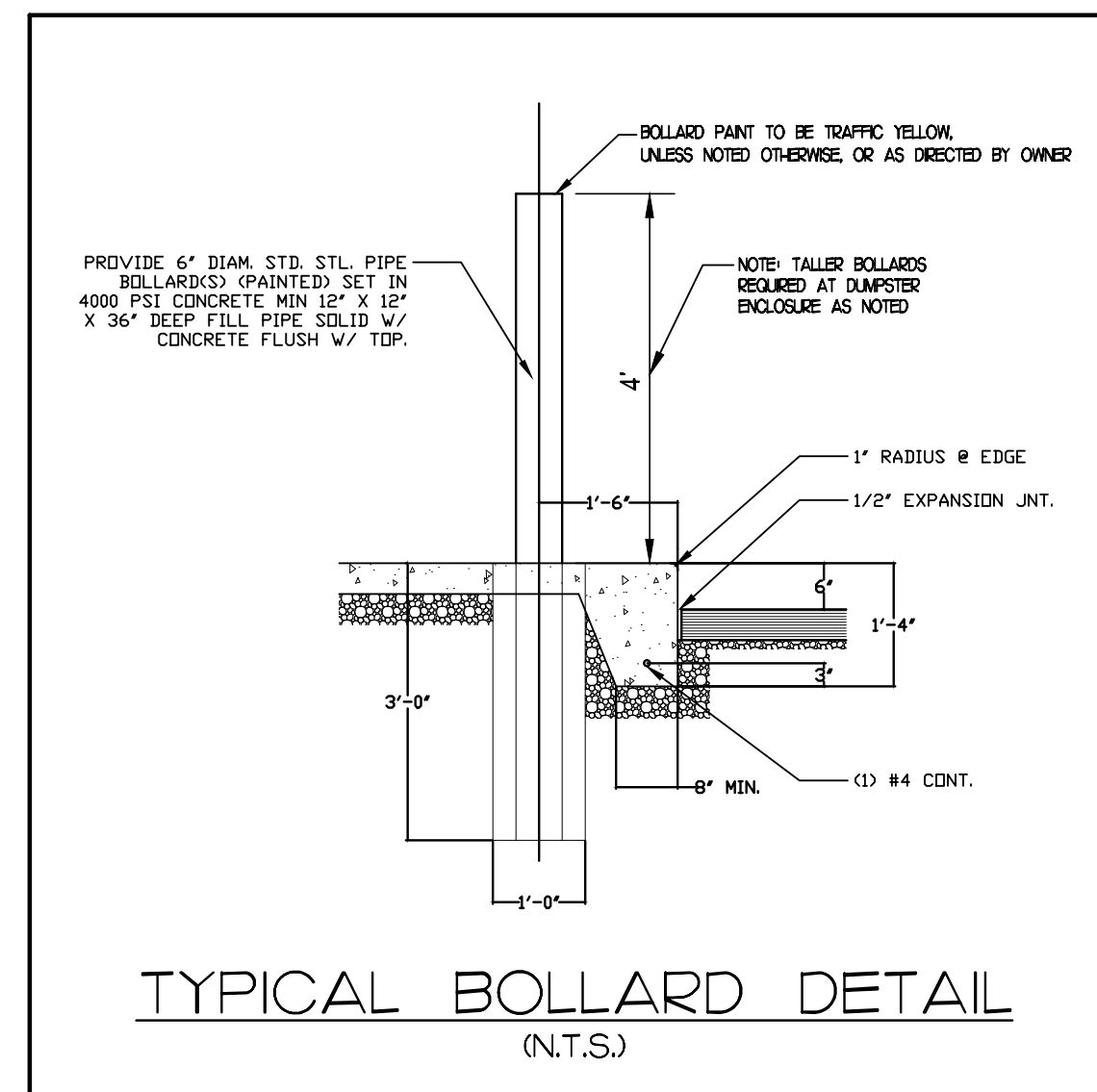
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SITE PLAN

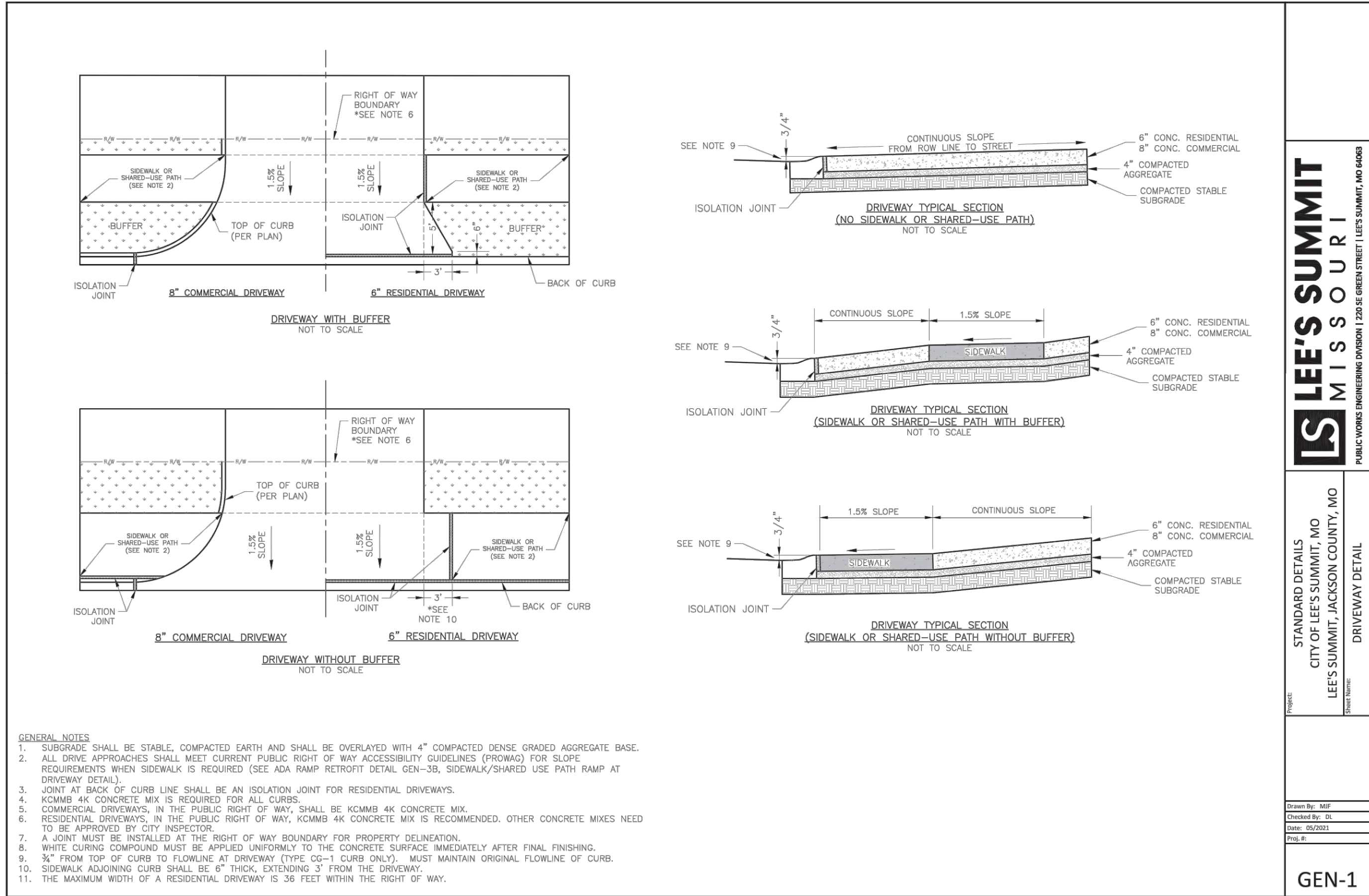
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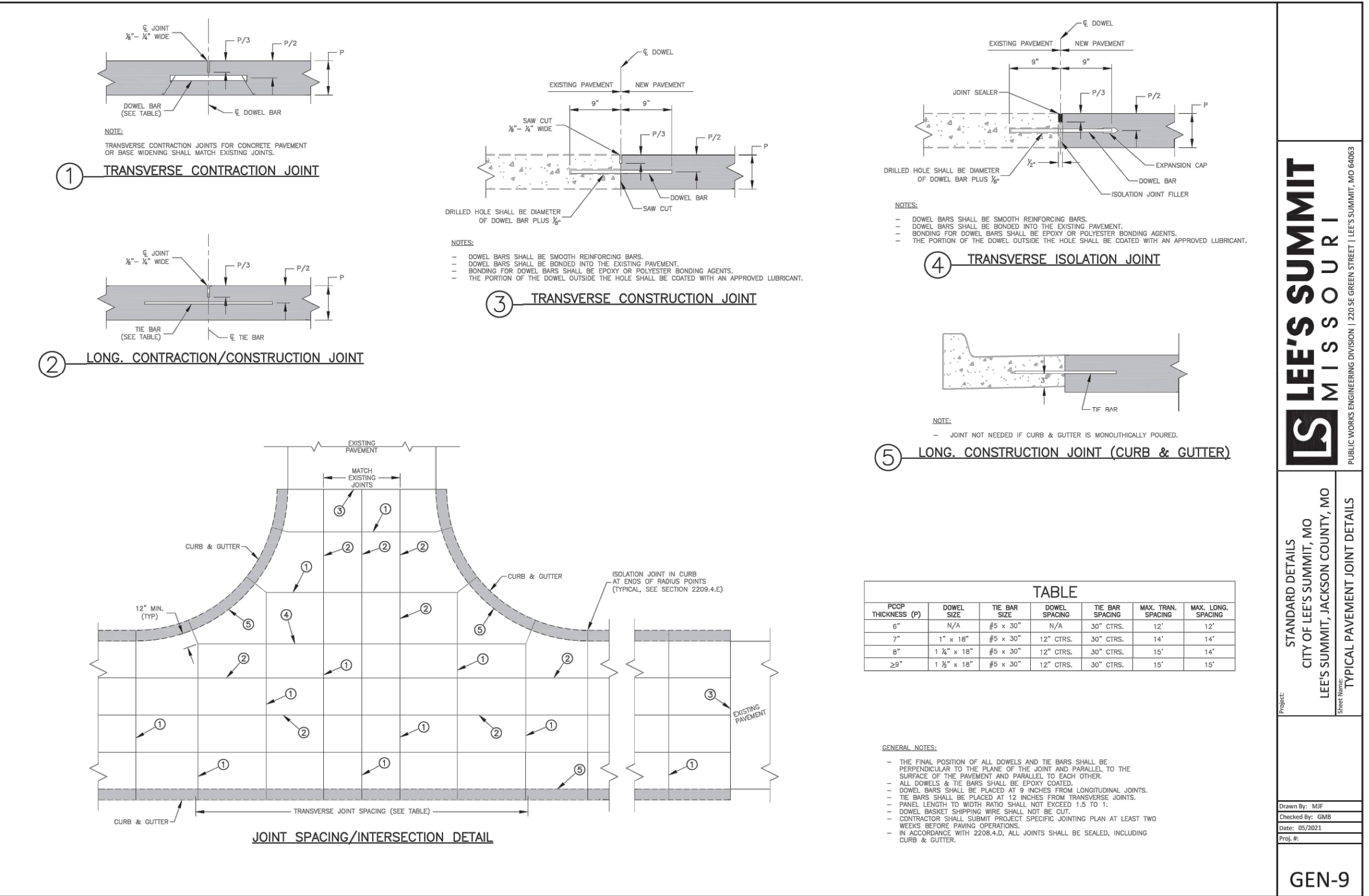
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LEE'S SUMMIT
MISSOURI

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

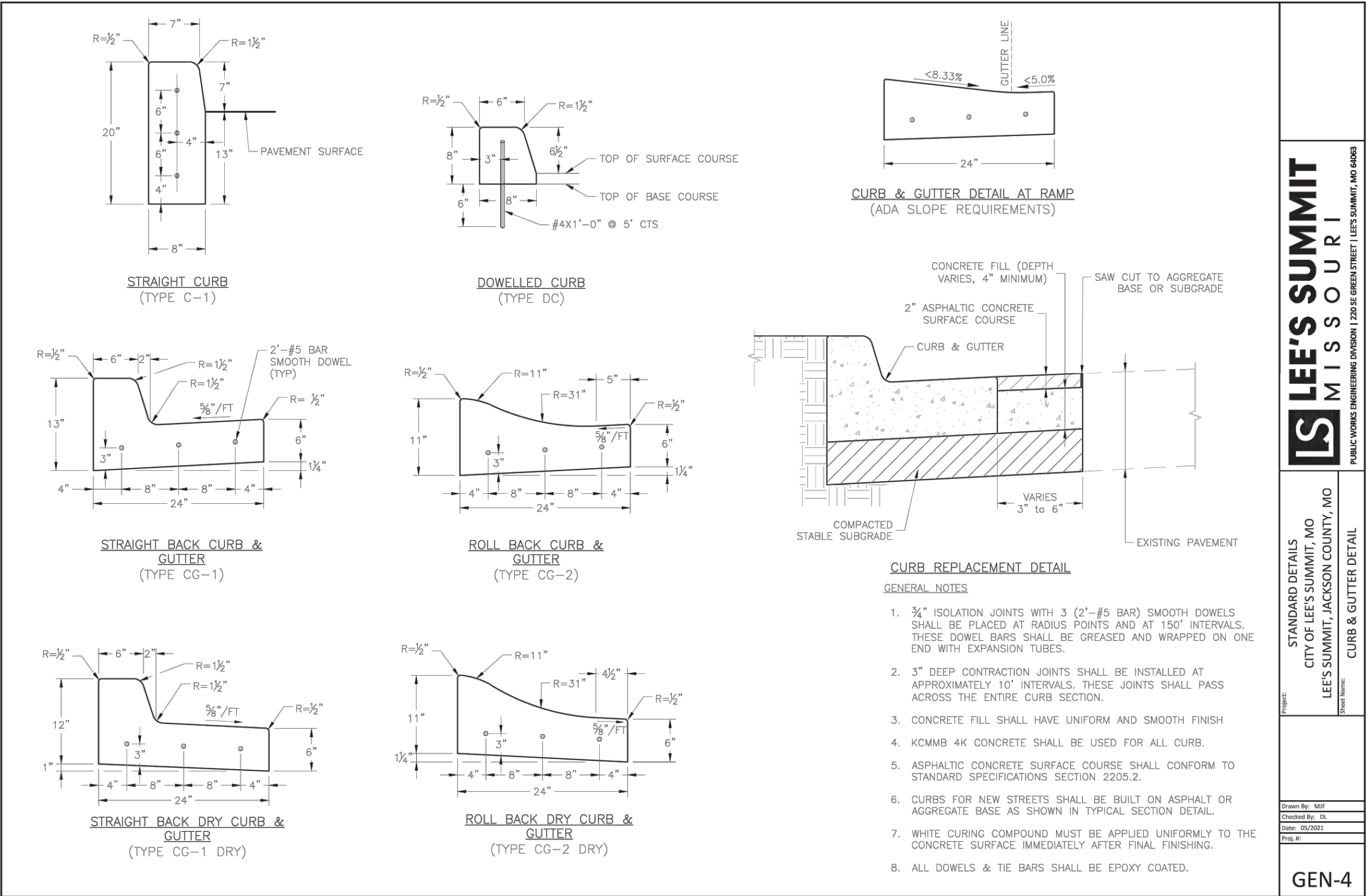
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STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

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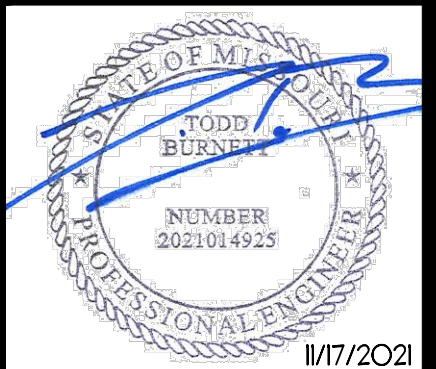
LEE'S SUMMIT
MISSOURI

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

GEN-4

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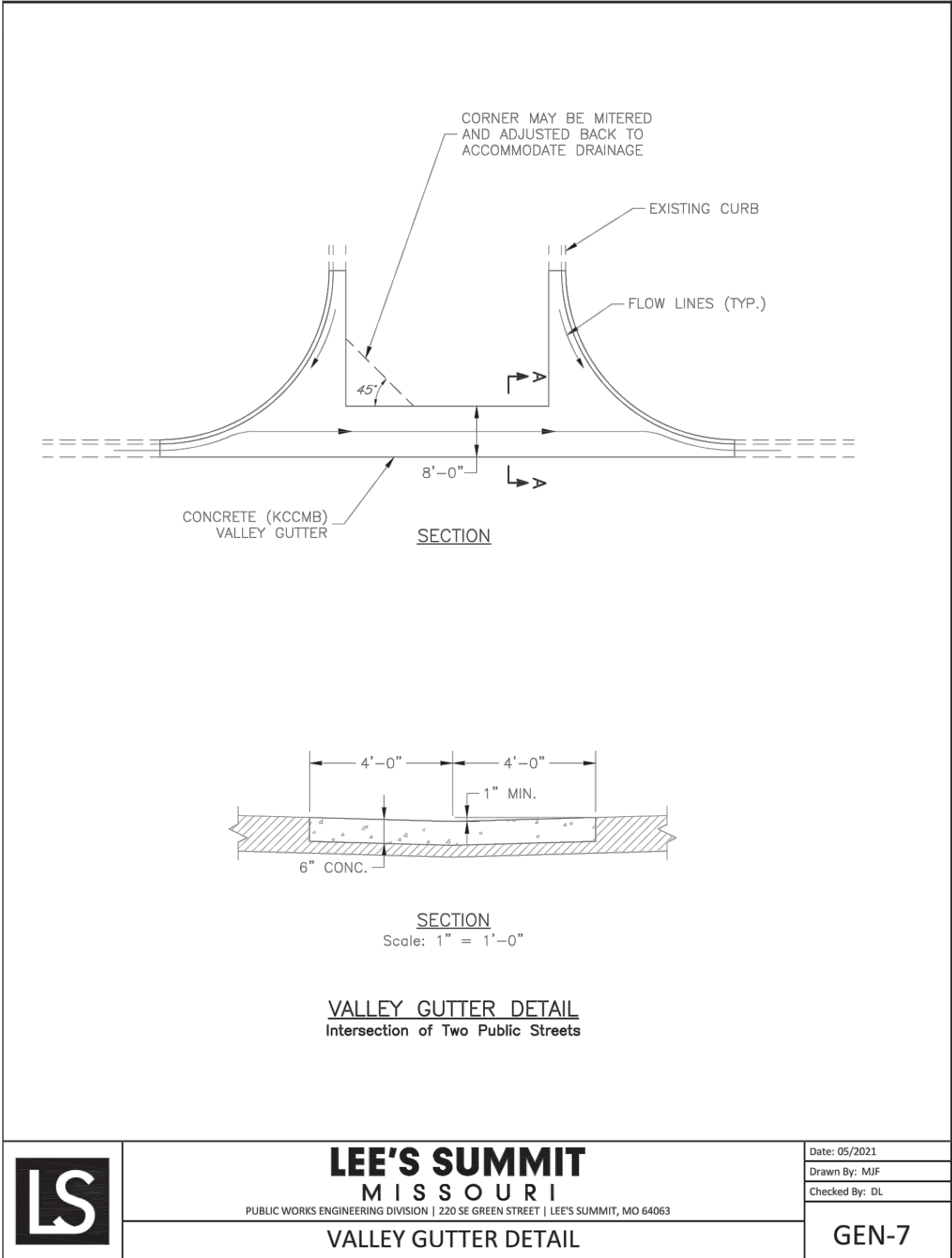
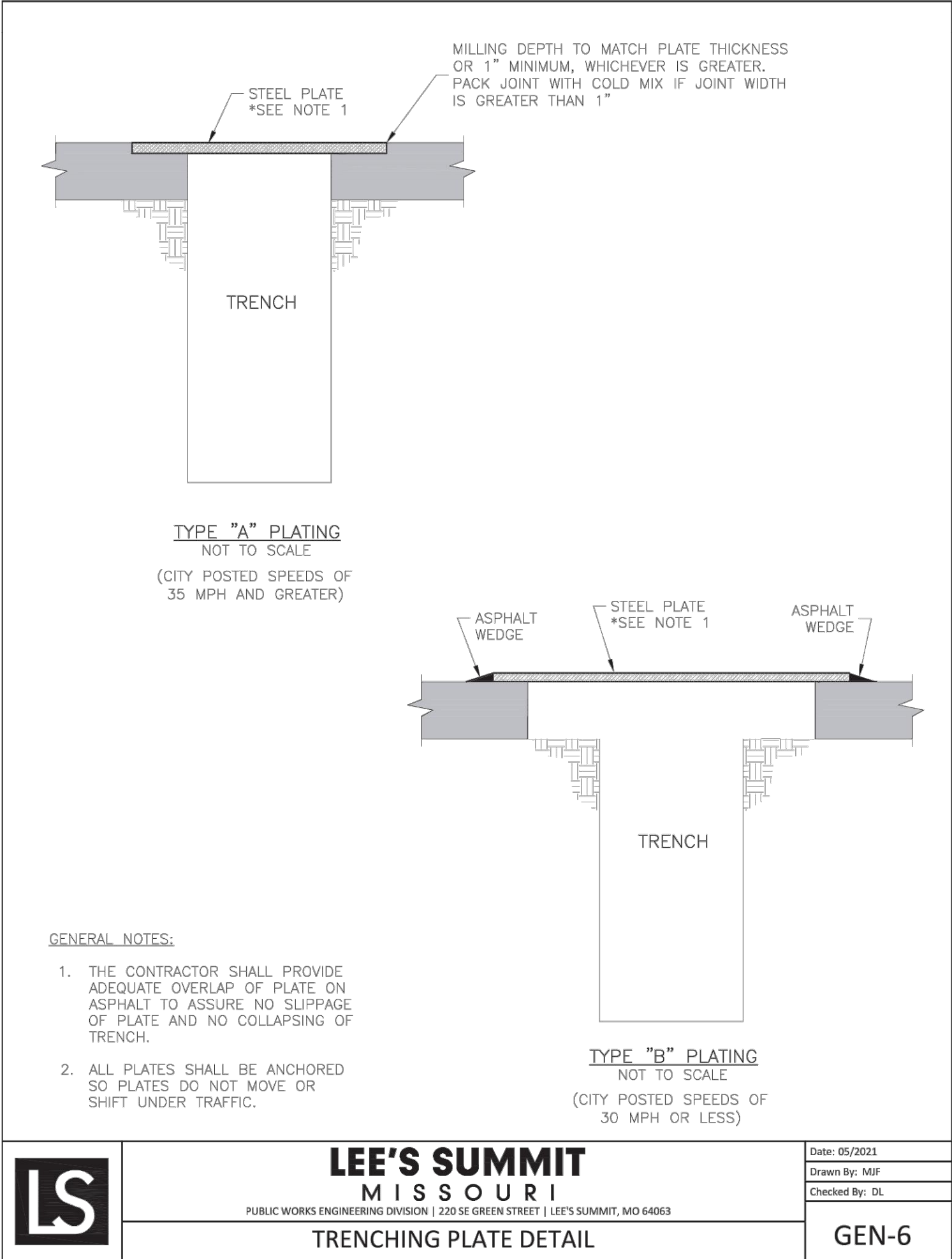
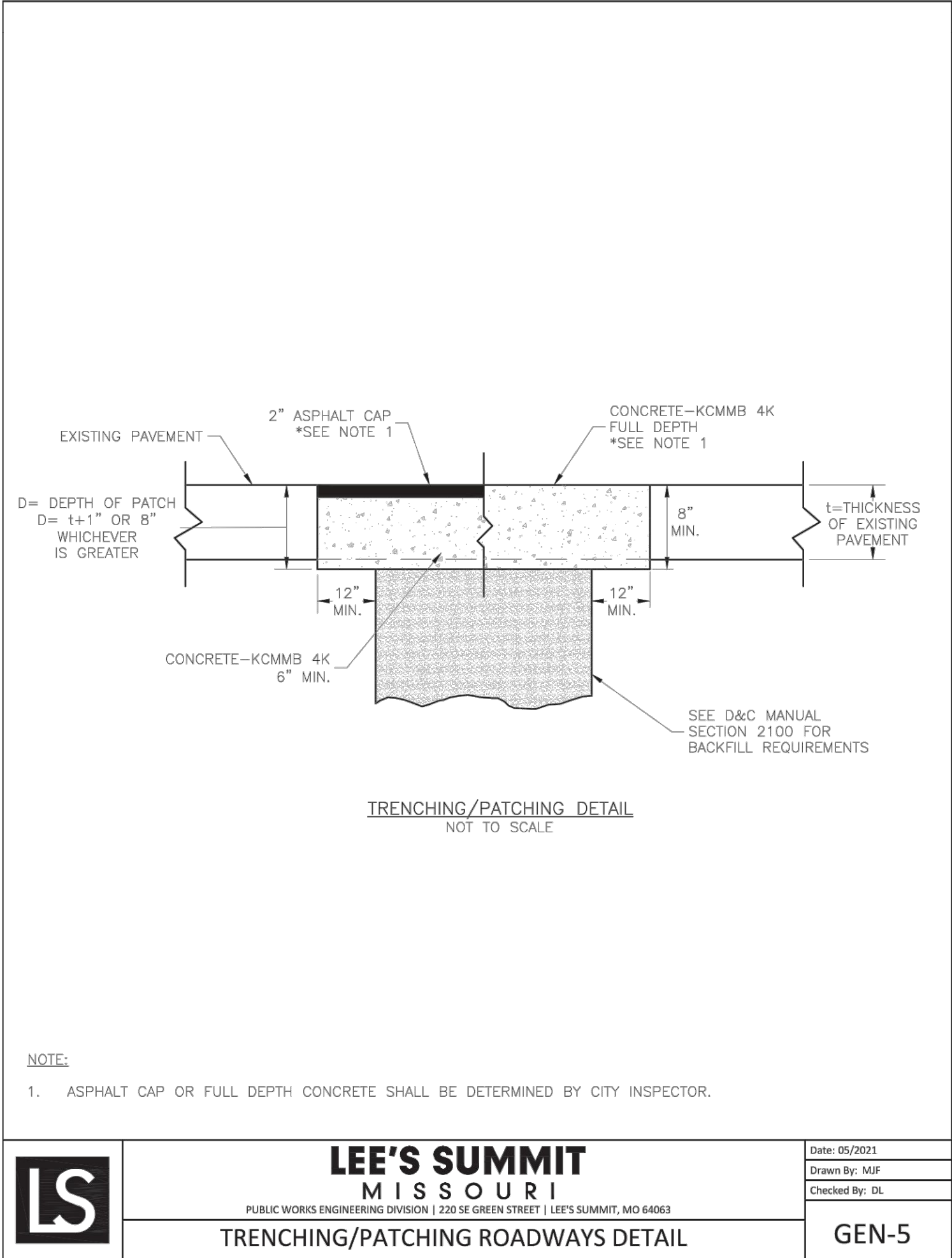
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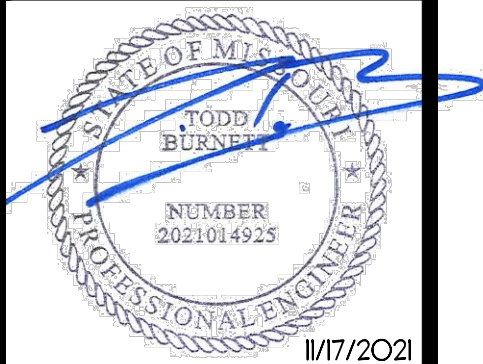
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SITE DETAILS

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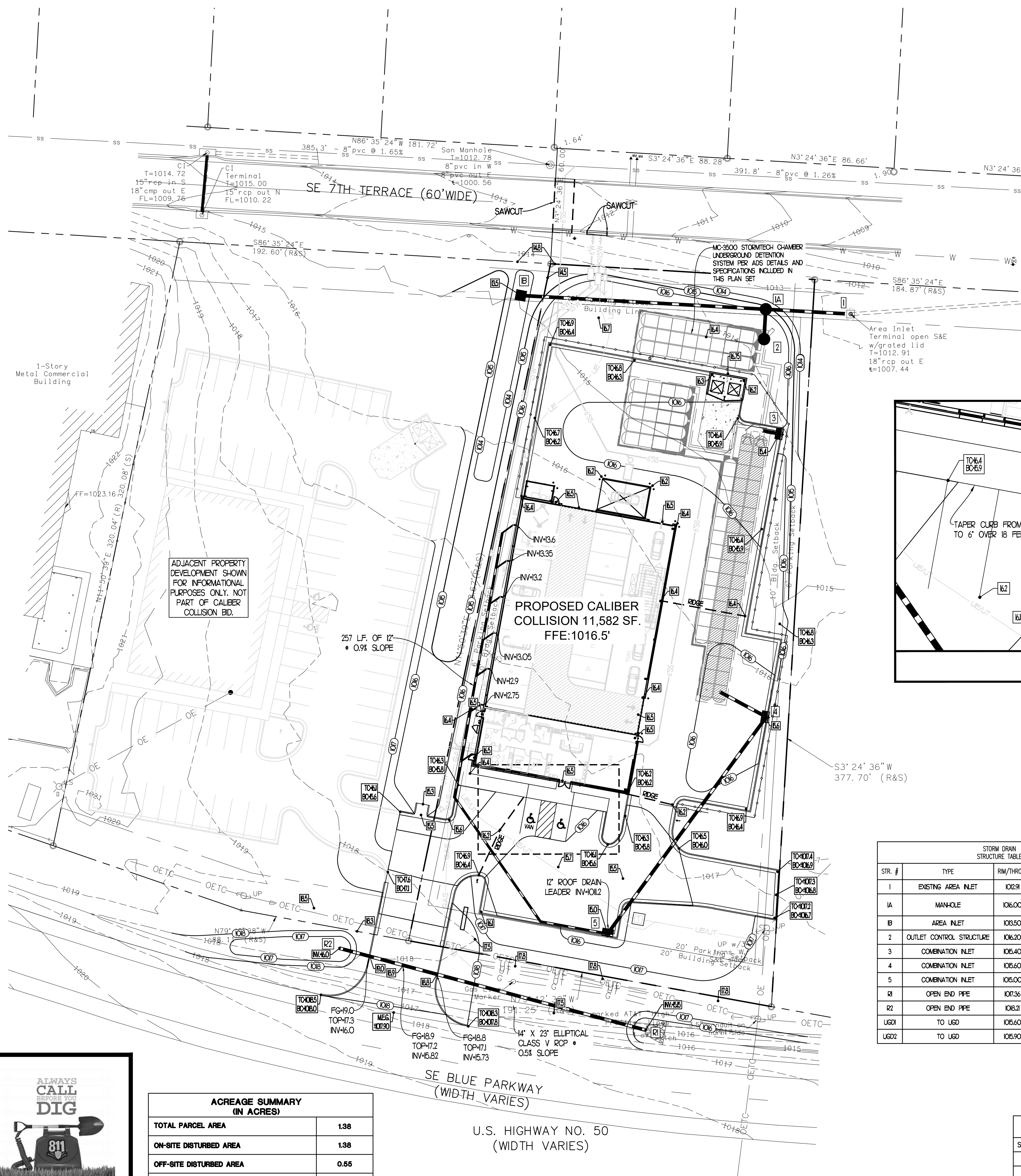
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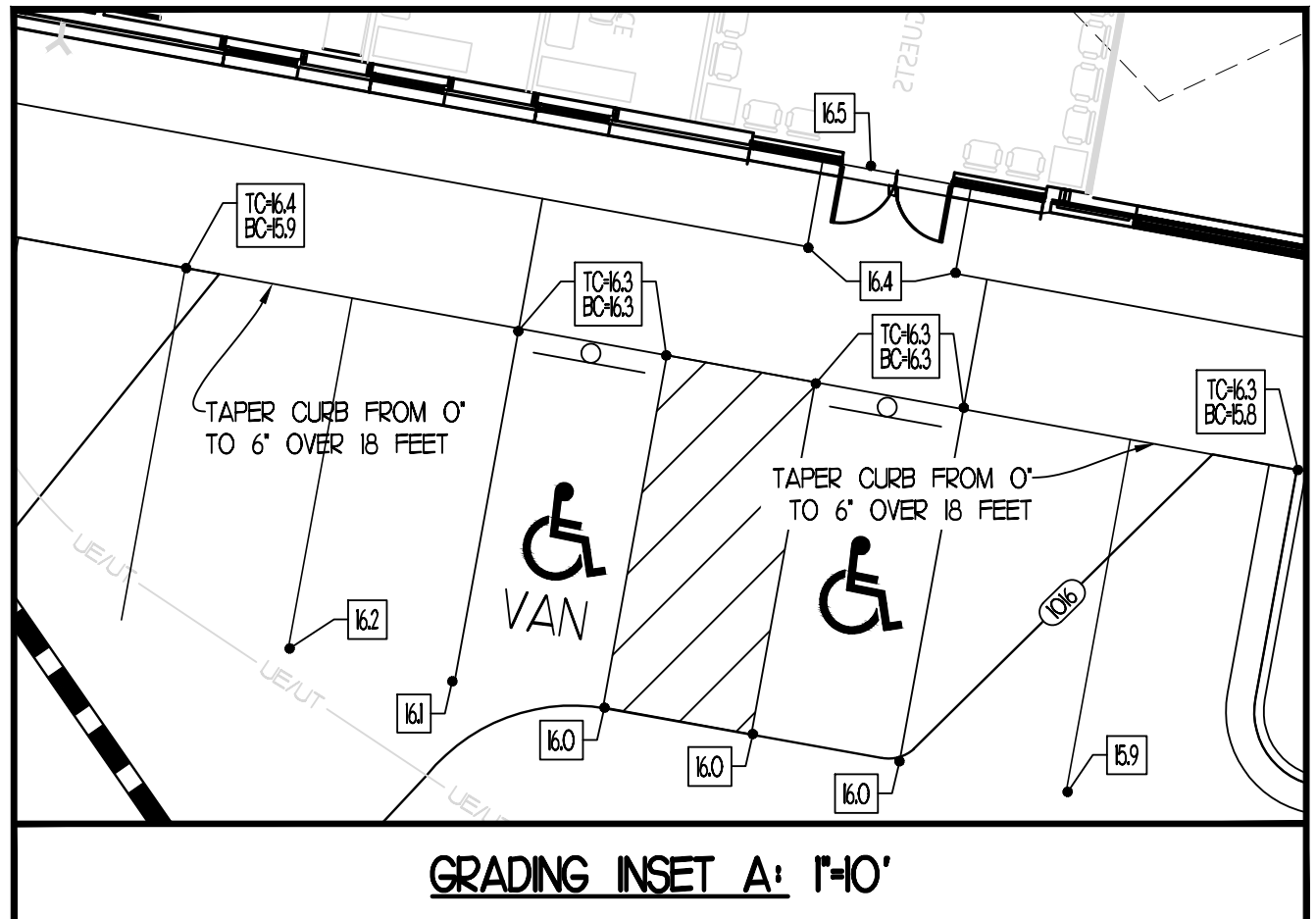
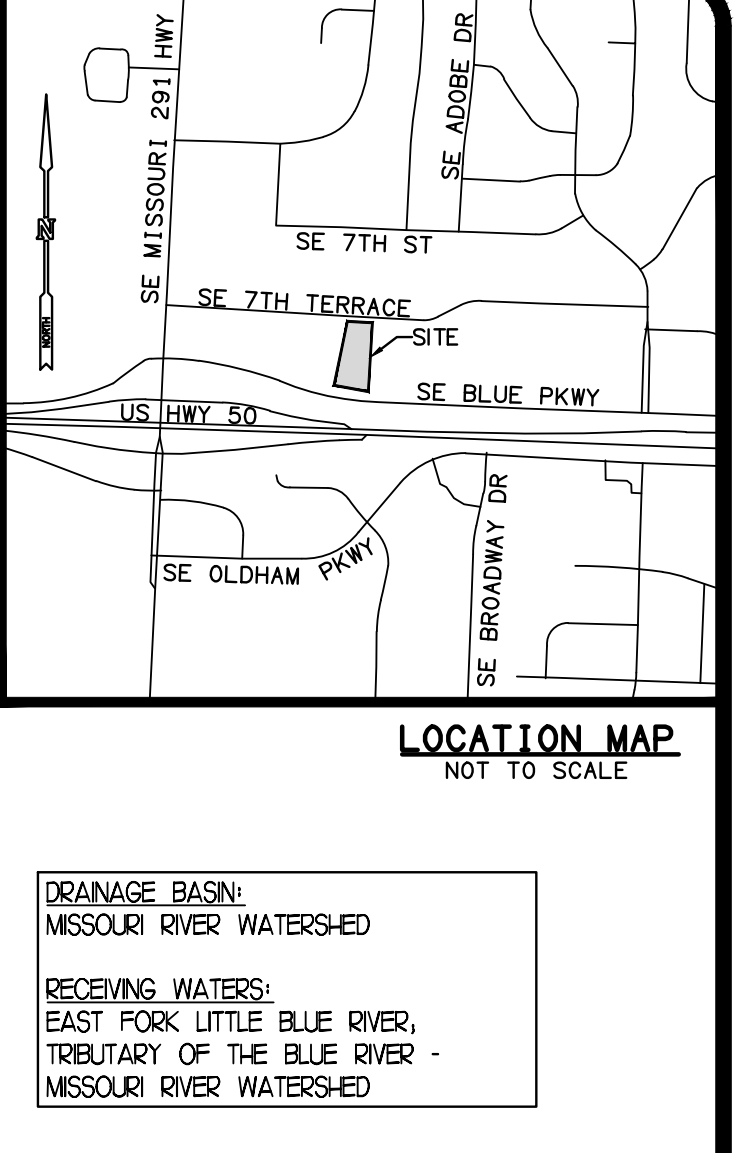
STORM DRAIN NOTES

1. ALL HYDRAULIC DATA FOR THE STORM DRAINAGE PIPES ARE BASED ON THE 100-YEAR DESIGN STORM.
2. ALL MAN-HOLE DIAMETERS AND ALL INLET DIMENSIONS SHALL HAVE A MINIMUM INSIDE DIMENSION OF 4' DIAMETER (OR CIRCULAR) OR 4'x4' (SQUARE). STRUCTURES SHALL BE UP-SIZED AS NEEDED TO ACCOMMODATE THE CONNECTING PIPE SIZES/ ANGLES.
3. STORM DRAIN PIPE SHALL BE CLASS III, WALL 12" RCP IN ACCORDANCE WITH ASTM C-76, UNLESS NOTED OTHERWISE.
4. HIGH DENSITY POLYETHYLENE PIPE (HDPE) MAY BE USED AS AN ALTERNATE. ALL HDPE PIPE JOINTS, AND FITTINGS SHALL MEET THE REQUIREMENTS OF AASHTO DESIGNATIONS M252 AND M254 TYPE S.
5. PIPE LENGTHS ARE BASED ON HORIZONTAL DISTANCE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE AND USED FOR DESIGN ONLY. CONTRACTOR TO FIELD VERIFY ACTUAL PIPE LENGTHS NEEDED FOR CONSTRUCTION.
6. CONTRACTOR IS RESPONSIBLE FOR COMPARING THE STORM DRAIN WITH THE PROFILES FOR THE STORM DRAIN LINES AND INFORMING THE ENGINEER OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO COMMENCEMENT OF CONSTRUCTION.
7. STORM DRAIN PIPE SHALL HAVE WATER TIGHT JOINTS IN ACCORDANCE WITH ASTM C 361, C443, AND D3212.

CONTRACTOR SHALL USE EXTREME CAUTION WHILE WORKING UNDERNEATH OVERHEAD POWER LINES. ANY DAMAGE TO THE EXISTING UTILITY POLES AND UTILITY LINES SHALL BE REPAIRED/REPLACED AT THE CONTRACTOR'S EXPENSE.

LEGEND

- PROPOSED CONTOURS
- EXISTING CONTOURS
- PHASE 2 CONTOURS (NOT PART OF CALIBER BID)
- SPOT ELEVATION
- MATCH EXISTING GRADE
- PROPOSED STORM DRAIN
- PROPOSED STORM CATCH BASIN
- PROPOSED STORM MANHOLE
- PROPOSED AREA INLET
- PROPOSED HEADWALL
- DRAINAGE FLOW ARROW
- EROSION CONTROL BLANKET



GRADING & DRAINAGE NOTES

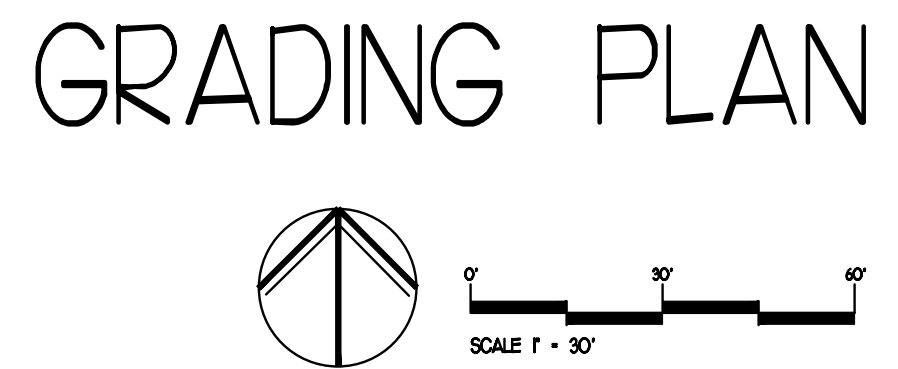
1. ALL SITE WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE PLANS PREPARED BY FREELAND & KAUFFMAN, INC. THE CURRENT REQUIREMENTS OF THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, APPLICABLE SECTIONS OF THE MODOT STANDARD SPECIFICATIONS, AND ALL OTHER PERTINENT FEDERAL AND STATE LAWS.
2. ALL EXISTING STRUCTURES, FENCING, TREES, ETC. WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED AND PROPERLY DISPOSED OF OFF-SITE, UNLESS OTHERWISE NOTED TO REMAIN. BURNING ON-SITE SHALL NOT BE ALLOWED.
3. ALL DRAINAGE STRUCTURES SHALL BE PRE-CAST AND SHALL MEET MODOT MINIMUM REQUIREMENTS.
4. ALL DRAINAGE STRUCTURES AND STORM SEWER PIPES SHALL MEET HEAVY DUTY TRAFFIC (H20) LOADING AND BE INSTALLED ACCORDINGLY.
5. BEFORE EXCAVATION, ALL UNDERGROUND UTILITIES SHALL BE LOCATED IN THE FIELD BY THE PROPER AUTHORITIES. THE CONTRACTOR SHALL NOTIFY "CALL BEFORE YOU DIG" AT THE LOCATION OF ALL UTILITIES AND UNDERGROUND STRUCTURES ARE APPROXIMATE AND MAY NOT BE SHOWN. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXISTENCE AND THE EXACT LOCATION OF ALL UTILITIES AND UNDERGROUND STRUCTURES.
6. SITE GRADING SHALL NOT PROCEED UNTIL EROSION CONTROL MEASURES HAVE BEEN INSTALLED.
7. THE CONTRACTOR SHALL COMPLY AT ALL TIMES WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, PROVISIONS, AND POLICIES GOVERNING SAFETY AND HEALTH, INCLUDING THE FEDERAL CONSTRUCTION SAFETY ACT (PUBLIC LAW 91-554), FEDERAL REGISTER, CHAPTER XVII, PART 1926 OF TITLE 29 REGULATIONS, OCCUPATIONAL SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION, AND SUBSEQUENT PUBLICATIONS UPDATING THESE REGULATIONS.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXAMINING THE AREAS AND CONDITIONS UNDER WHICH THE PROJECT IS TO BE CONSTRUCTED PRIOR TO THE SUBMISSION OF A BID. SUBMISSION OF A BID SHALL BE CONSTRUED TO MEAN THE CONTRACTOR HAS REVIEWED THE SITE AND IS FAMILIAR WITH CONDITIONS AND CONSTRAINTS OF THE SITE.
9. ALL SLOPES SHALL BE 3:1 (HORIZONTAL:VERTICAL) UNLESS NOTED OTHERWISE. NO SLOPES SHALL EXCEED 2:1 IN ANY CASE.
10. AN AS-BUILT DRAWING OF STORM DRAIN LINES AND THE UNDERGROUND DETENTION SYSTEM MUST BE PREPARED BY THE CONTRACTOR AND SUBMITTED TO THE OWNER AND THE CITY OF LEE'S SUMMIT UPON COMPLETION OF THE PROJECT.
11. ALL DISTURBED AREAS WHICH ARE NOT PAVED SHALL BE TOPSOILED, SEEDED AND MULCHED OR LANDSCAPED UNLESS OTHERWISE NOTED IN THE CONSTRUCTION DRAWINGS, SITE SPECIFICATIONS, OR INSTRUCTED BY THE OWNER.
12. CONTRACTOR SHALL REFER TO THE GEOTECHNICAL REPORT, PREPARED BY OTHERS, PRIOR TO INITIATION OF ANY EARTHWORK ACTIVITY.
13. IF ANY EXISTING STRUCTURES OR SITE FEATURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
14. ALL STORM PIPE ENTERING STRUCTURES SHALL BE GROUTED TO ASSURE CONNECTION AT STRUCTURE IS WATERTIGHT. STORM PIPE MATERIALS SHALL MEET ALL CITY OF LEE'S SUMMIT, JACKSON COUNTY, AND MODOT REQUIREMENTS.
15. THE CONTRACTOR SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
16. CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
17. CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
18. TOPOGRAPHIC INFORMATION TAKEN FROM A TOPOGRAPHIC SURVEY BY HEIDMAN ASSOCIATES, INC. IF CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, HE SHALL HAVE MADE, AT HIS EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR AND SUBMIT IT TO THE OWNER FOR REVIEW.
19. ALL STORM STRUCTURES SHALL HAVE A SMOOTH UNIFORM POURED MORTAR INVERT FROM INVERT IN TO INVERT OUT.
20. EXISTING STORM DRAIN PIPES IMMEDIATELY ADJACENT TO THE SITE SHALL BE INSPECTED AND CLEANED TO REMOVE ALL SILT AND DEBRIS.
21. CONTRACTOR TO PROVIDE SHOP DRAWINGS ON ALL STORM SEWER INLETS AND MANHOLES FOR REVIEW PRIOR TO ORDERING.
22. ROOF LEADERS SHALL DISCHARGE TO SPLASH BLOCKS UNLESS SPECIFICALLY NOTED OTHERWISE. RE-ARCH FOR EXACT NUMBER AND LOCATION.
23. ALL SIDEWALKS TO HAVE 2.00% MAXIMUM CROSS SLOPE AND 5.00% MAX LONGITUDINAL SLOPE.
24. SLOPES 3:1 AND STEEPER SHALL BE INSTALLED WITH AN EROSION CONTROL BLANKET (TOURLEX® OR APPROVED EQUAL).
25. INSURE THAT ALL SIDEWALK CURBS LESS THAN 6" HEIGHT OR LESS ARE PAINTED YELLOW PRIOR TO CERTIFICATE OF OCCUPANCY. SURVEYOR SEALED AS-BUILT DRAWINGS OF ALL WATER QUALITY BMP'S AND DETENTION SYSTEMS MUST BE PROVIDED.
26. CONTRACTOR SHALL PROVIDE EROSION CONTROL BLANKETS ON ALL SLOPES STEEPER THAN 3:1. CONTRACTOR HAS THE OPTION TO UTILIZE SOD IN LIEU OF AN EROSION CONTROL BLANKET (TYPICAL).
27. ALL CONSTRUCTED SEVERE SLOPES GREATER THAN 2:1 SHALL BE DESIGNED BY A NORTH CAROLINA REGISTERED PROFESSIONAL ENGINEER. ADDITIONALLY, FOR CONSTRUCTED SEVERE SLOPES GREATER THAN FIVE FEET IN HEIGHT, AN INSPECTION AND A STABILITY CERTIFICATE ARE REQUIRED BY A NORTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WITH GEOTECHNICAL EXPERTISE SUFFICIENT TO PERFORM THE INSPECTION AND STABILITY ANALYSIS. FOR ALL CONSTRUCTED SEVERE SLOPES WITHIN PROPOSED OR EXISTING PUBLIC RIGHTS-OF-WAY, PERIODIC INSPECTIONS AND COMPACTED REPORTS ARE REQUIRED BY A NORTH CAROLINA REGISTERED PROFESSIONAL ENGINEER WITH GEOTECHNICAL EXPERTISE.

STORM DRAIN STRUCTURE TABLE				
STR. #	TYPE	RIM/THROAT	INV. IN	INV. OUT
1	EXISTING AREA INLET	102.9	1007.60 (A)	1008.00 (B)
1A	MANHOLE	106.00	1008.30 (B) 1008.30 (C)	1008.00 (B)
1B	AREA INLET	108.50		1009.60 (A)
2	OUTLET CONTROL STRUCTURE	106.50		1008.50 (A)
3	COMBINATION INLET	106.40		1007.90 (UGD)
4	COMBINATION INLET	106.60	1009.70 (C)	1009.50 (UGD)
5	COMBINATION INLET	105.00		1010.00 (A)
R1	OPEN END PIPE	107.36	106.65 (R2)	
R2	OPEN END PIPE	108.2		106.00 (R)
UGD1	TO UGD	106.60	1007.60 (C)	
UGD2	TO UGD	106.90	1009.20 (A)	

STORM PIPE TABLE					
STORM DRAIN PIPE	SIZE	LENGTH	SLOPE	INV. IN	INV. OUT
1A - 1	18"	44'	0.3%	1008.00'	1007.60'
B - 1A	18"	127'	0.2%	1009.60'	1008.30'
2 - 1A	18"	16'	0.2%	1008.50'	1008.30'
5 - 4	18"	199'	0.03%	1010.00'	1009.70'
R2 - R1	18" BLUE	164'	0.52%	106.00'	106.5'

WATER TIGHT STORM PIPES:
CONTRACTOR TO ENSURE THAT ALL STORM PIPES ARE WATER TIGHT AND TESTED PER ASTM C902-02.

CONTRACTOR TO ENSURE ALL STORM DRAIN PIPE SHALL HAVE WATER TIGHT JOINTS IN ACCORDANCE WITH ASTM C 361, C443 AND D3212.



ACREAGE SUMMARY (IN ACRES)	
TOTAL PARCEL AREA	1.38
ON-SITE DISTURBED AREA	1.38
OFF-SITE DISTURBED AREA	0.65
TOTAL DISTURBED AREA	1.93
IMPERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY & INCLUDES GRAVEL)	0.48
PERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY)	0.90
IMPERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.90
PERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.48
IMPERVIOUS AREA INCREASE (PRE VS POST) (ON-SITE ONLY)	0.42
IMPERVIOUS PERCENTAGE (AT POST-CONSTRUCTION STATUS)	65.2%

ALWAYS CALL 811 BEFORE YOU DIG

EVERY DIGGING JOB REQUIRES A CALL - EVEN SMALL PROJECTS LIKE PLANTING TREES OR SHEDS. IF YOU HIT AN UNDERGROUND UTILITY LINE WHILE DIGGING, YOU CAN HARM YOURSELF OR THOSE AROUND YOU, DISRUPT SERVICE TO AN ENTIRE NEIGHBORHOOD AND POTENTIALLY BE RESPONSIBLE FOR FINES AND REPAIR COSTS.

SMART DIGGING MEANS CALLING 811 BEFORE EACH JOB. WHETHER YOU ARE A HOMEOWNER OR A PROFESSIONAL EXCAVATOR, ONE CALL TO 811 GETS YOUR UNDERGROUND UTILITY LINES MARKED FOR FREE.

REVISIONS	BY

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STORM DRAIN NOTES

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- CONTRACTOR IS RESPONSIBLE FOR COMPARING THE STORM DRAIN WITH THE PROFILES FOR THE STORM DRAIN LINES AND INFORMING THE ENGINEER OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- STORM DRAIN PIPE SHALL HAVE WATER TIGHT JOINTS IN ACCORDANCE WITH ASTM C 361, C443, AND D332.

INLET DRAINAGE AREAS													
TYPE	INLET	AREA (AC)	TC (MIN)	IMPERVIOUS COVERAGE (ACRE)	IMPERVIOUS RUNOFF COEFFICIENT	PERVIOUS COVERAGE (ACRE)	PERVIOUS RUNOFF COEFFICIENT	WEIGHTED RUNOFF COEFFICIENT	T*50	T*100	Q50 (CFS)	Q100 (CFS)	PONDING DEPTH @ INLET PER 100-YR STORM (FT)
PROPOSED	1B *	1.70	5	0.04	0.90	1.66	0.35	0.36	11.30	12.60	6.97	7.77	0.30
	3	0.26	5	0.26	0.90	0.00	0.35	0.90	11.30	12.60	2.64	2.95	0.26
	4	0.15	5	0.15	0.90	0.00	0.35	0.90	11.30	12.60	1.53	1.70	0.20
	5	0.62	5	0.47	0.90	0.15	0.35	0.77	11.30	12.60	5.37	5.99	0.39
	R-2	0.94	5	0.29	0.90	0.65	0.35	0.52	11.30	12.60	5.52	6.16	** NOT CALCULATED
EXISTING	EX-1	0.08	5	0.02	0.90	0.06	0.35	0.49	11.30	12.60	0.44	0.49	** NOT CALCULATED
	EX-2	0.47	5	0.08	0.90	0.39	0.35	0.44	11.30	12.60	2.36	2.63	** NOT CALCULATED

RUNOFF COEFFICIENTS USED TO CALCULATE THE WEIGHTED RUNOFF COEFFICIENT ARE LISTED BELOW

- LAWNS/GRASS: 0.35
- ROOFS/PAVEMENT: 0.90

RAINFALL DATA FROM NOAA ATLAS 14, VOLUME 8, VERSION 2, LEE'S SUMMIT, MISSOURI

- THE AREA SHOWN FOR INLET 1B ASSUMES THAT THE ADJACENT PROPERTY WILL BE DEVELOPED AND CONNECT ITS STORM SEWER SYSTEM TO STRUCTURE 1B. PER THE CITY OF LEE'S SUMMIT CODE, THE MAXIMUM ALLOWED DISCHARGE FROM THE DEVELOPED SITE IS 3 CFS PER DEVELOPED ACRE. ASSUMING 1.70 ACRES OF DEVELOPED AREA, THE MAXIMUM DISCHARGE ALLOWED FROM THE ADJACENT SITE WOULD BE 5.1 CFS WHICH IS LESS THAN THE EXISTING SITE'S UNDEVELOPED CONDITION (WHICH RESULTS IN THE VALUE(S) SHOWN IN THE TABLE ABOVE). TO BE CONSERVATIVE, THE RELEASES FOR INLET 1B ABOVE ARE USED TO ADEQUATELY SIZE THE STORM SEWER PIPES PROPOSED BY THE CALIBER COLLISION DEVELOPMENT.
- THE PONDING DEPTHS FOR THESE INLETS WERE NOT CALCULATED AS THEY ARE A RESULT OF THE EXISTING CONDITIONS OF THE SURROUNDING PROPERTIES AND ADDITIONAL SURVEY INFORMATION WILL BE REQUIRED TO ACCURATELY CALCULATE. ADDITIONALLY, THESE "INLETS" ARE OPEN-ENDED PIPES AND NOT "STORM STRUCTURES". THEIR PONDING DEPTHS ARE NEGLECTED AS THEY CONTAIN CONDITIONS WHERE PONDING IS NOT AN ISSUE (I.E. CONTAINED WITHIN DITCHES)

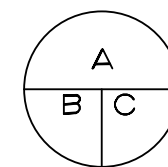
RAINFALL DISTRIBUTION FOR LEE'S SUMMIT, MISSOURI

STORM EVENT	2-YR	10-YR	25-YR	50-YR	100-YR
RAINFALL (INCHES/HR)	5.81	8.35	9.98	11.30	12.60

- THE RAINFALL FOR RAINFALL DISTRIBUTION DATA TAKEN FROM NOAA ATLAS 14, VOLUME 8, VERSION 2 FOR LEE'S SUMMIT, MISSOURI
- STORM DISTRIBUTION TYPE II
- 24-HOUR STORM DISTRIBUTION

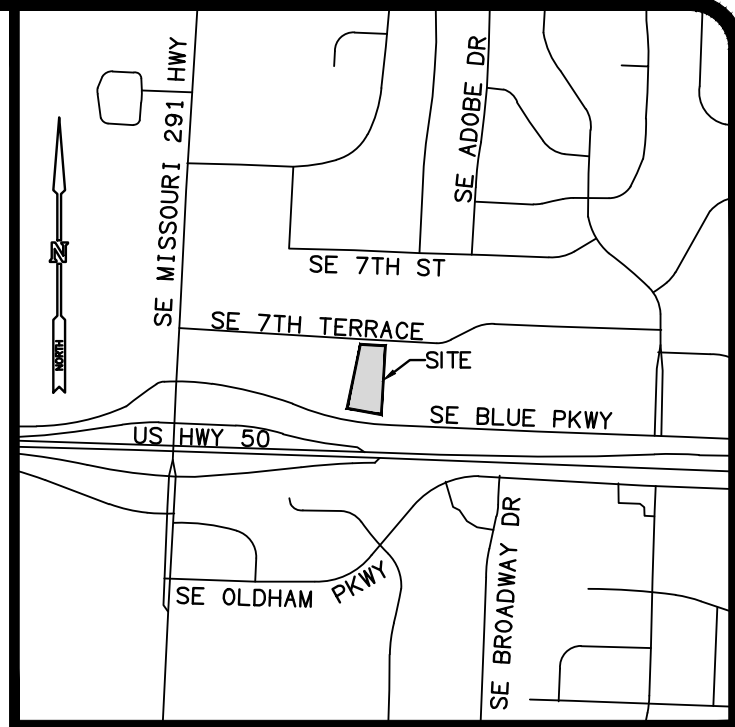
LEGEND

DESCRIPTION



- A = BASIN DESIGNATION
- B = AREA IN ACRES
- C = RUNOFF COEFFICIENT

--- DRAINAGE BASIN BOUNDARY

LOCATION MAP
NOT TO SCALE

WATER TIGHT STORM PIPES:

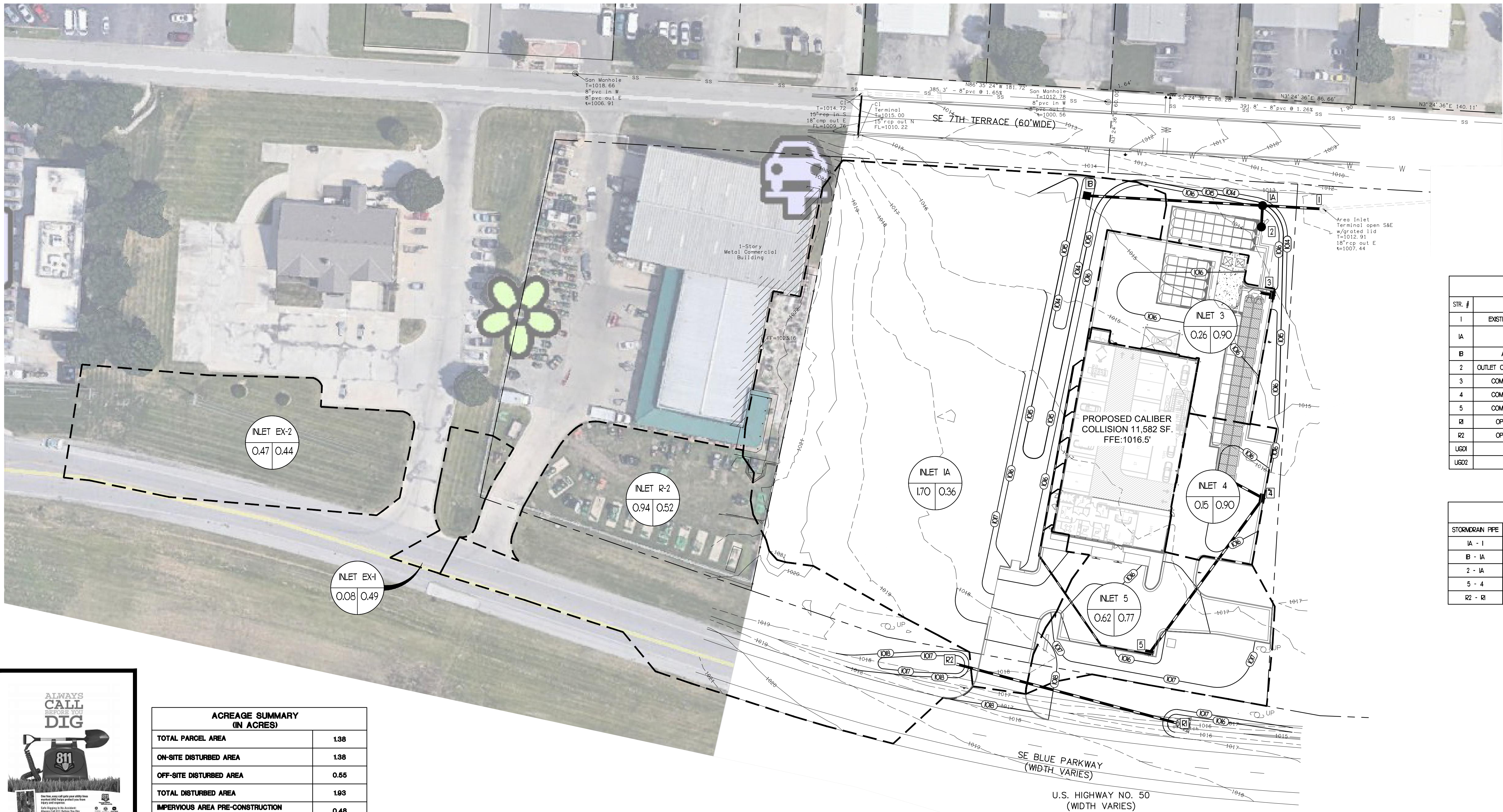
CONTRACTOR TO ENSURE THAT ALL STORM PIPES ARE WATER TIGHT AND TESTED PER ASTM C902-02

CONTRACTOR TO ENSURE ALL STORM DRAIN PIPE SHALL HAVE WATER TIGHT JOINTS IN ACCORDANCE WITH ASTM C 361, C443 AND D332.

DRAINAGE BASIN:

MISSOURI RIVER WATERSHED

RECEIVING WATERS:

EAST FORK LITTLE BLUE RIVER,
TRIBUTARY OF THE BLUE RIVER -
MISSOURI RIVER WATERSHED

STORM DRAIN STRUCTURE TABLE				
STR. #	TYPE	RIM/THROAT	INV. IN	INV. OUT
1	EXISTING AREA INLET	108.21	1007.60 (A)	
1A	MANHOLE	106.00	1008.30 (B) 1008.50 (C)	1008.00 (D)
B	AREA INLET	108.50		1009.60 (A)
2	OUTLET CONTROL STRUCTURE	106.20		1008.50 (A)
3	COMBINATION INLET	106.40		1007.90 (B50)
4	COMBINATION INLET	106.60	1009.70 (C)	1009.50 (B50)
5	COMBINATION INLET	106.00		100.00 (A)
R	OPEN END PIPE	107.36	106.5 (R2)	
R2	OPEN END PIPE	108.3		106.00 (R)
UG01	TO UG0	106.60	1007.80 (C)	
UG02	TO UG0	106.90	1009.20 (A)	

STORM PIPE TABLE				
STORM DRAIN PIPE	SIZE	LENGTH	SLOPE	INV. IN / INV. OUT
1A - 1	18"	44'	1.2%	1008.00' / 1007.60'
B - 1A	18"	27'	1.02%	1009.60' / 1008.50'
2 - 1A	18"	16'	1.2%	1008.50' / 1008.50'
5 - 4	18"	139'	0.93%	1010.00' / 1009.70'
R2 - R1	14" ELIP.	64'	0.52%	106.00' / 106.5'

ACREAGE SUMMARY (IN ACRES)	
TOTAL PARCEL AREA	1.38
ON-SITE DISTURBED AREA	1.38
OFF-SITE DISTURBED AREA	0.55
TOTAL DISTURBED AREA	1.93
IMPERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY & INCLUDES GRAVEL)	0.48
PERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY)	0.90
IMPERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.90
PERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.48
IMPERVIOUS AREA INCREASE (PRE VS POST) (ON-SITE ONLY)	0.42
IMPERVIOUS PERCENTAGE (AT POST-CONSTRUCTION STATUS)	65.2%

ALWAYS CALL BEFORE YOU DIG

811

THE ONLY WAY TO SAFELY AND LEGALLY DIG IS TO CALL 811 BEFORE YOU DIG. IT'S THE ONLY WAY TO FIND OUT WHERE ALL THE UNDERGROUND UTILITIES ARE. CALLING 811 BEFORE YOU DIG IS THE ONLY WAY TO AVOID ACCIDENTS, INJURIES, AND DEATHS. CALLING 811 BEFORE YOU DIG IS THE ONLY WAY TO AVOID FINES AND REPAIR COSTS.

EVERY DIGGING JOB REQUIRES A CALL - EVEN SMALL PROJECTS LIKE PLANTING TREES OR SHRUBS. IF YOU HIT AN UNDERGROUND UTILITY LINE WHILE DIGGING, YOU CAN HARM YOURSELF OR THOSE AROUND YOU, DISRUPT SERVICE TO AN ENTIRE NEIGHBORHOOD AND POTENTIALLY BE RESPONSIBLE FOR FINES AND REPAIR COSTS.

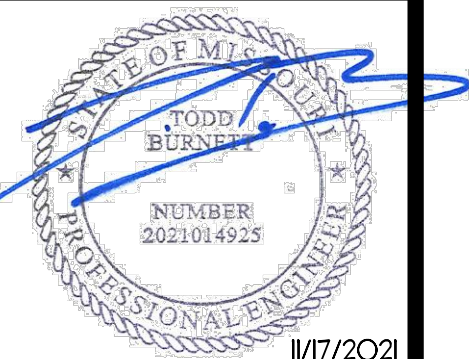
SMART DIGGING MEANS CALLING 811 BEFORE EACH JOB. WHETHER YOU ARE A HOMEOWNER OR A PROFESSIONAL EXCAVATOR, ONE CALL TO 811 GETS YOUR UNDERGROUND UTILITY LINES MARKED FOR FREE.

REVISIONS

BY

FREELAND and KAUFFMAN, INC.

Engineers * Landscape Architects
209 West Stone Avenue
Greenville, South Carolina 29609
864-233-5497
fax 864-233-8916



1/17/2021

CALIBER COLLISION
710 SE BLUE PARKWAY
LEE'S SUMMIT, MO 64063

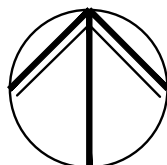
CROSS DEVELOPMENT, CC LEE'S SUMMIT, LLC
4388 MARSH RIDGE ROAD
CARROLLTON, TX 75010
TEL (214) 614-8252

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CHECKED
TMB
DATE
1/17/2021
SCALE

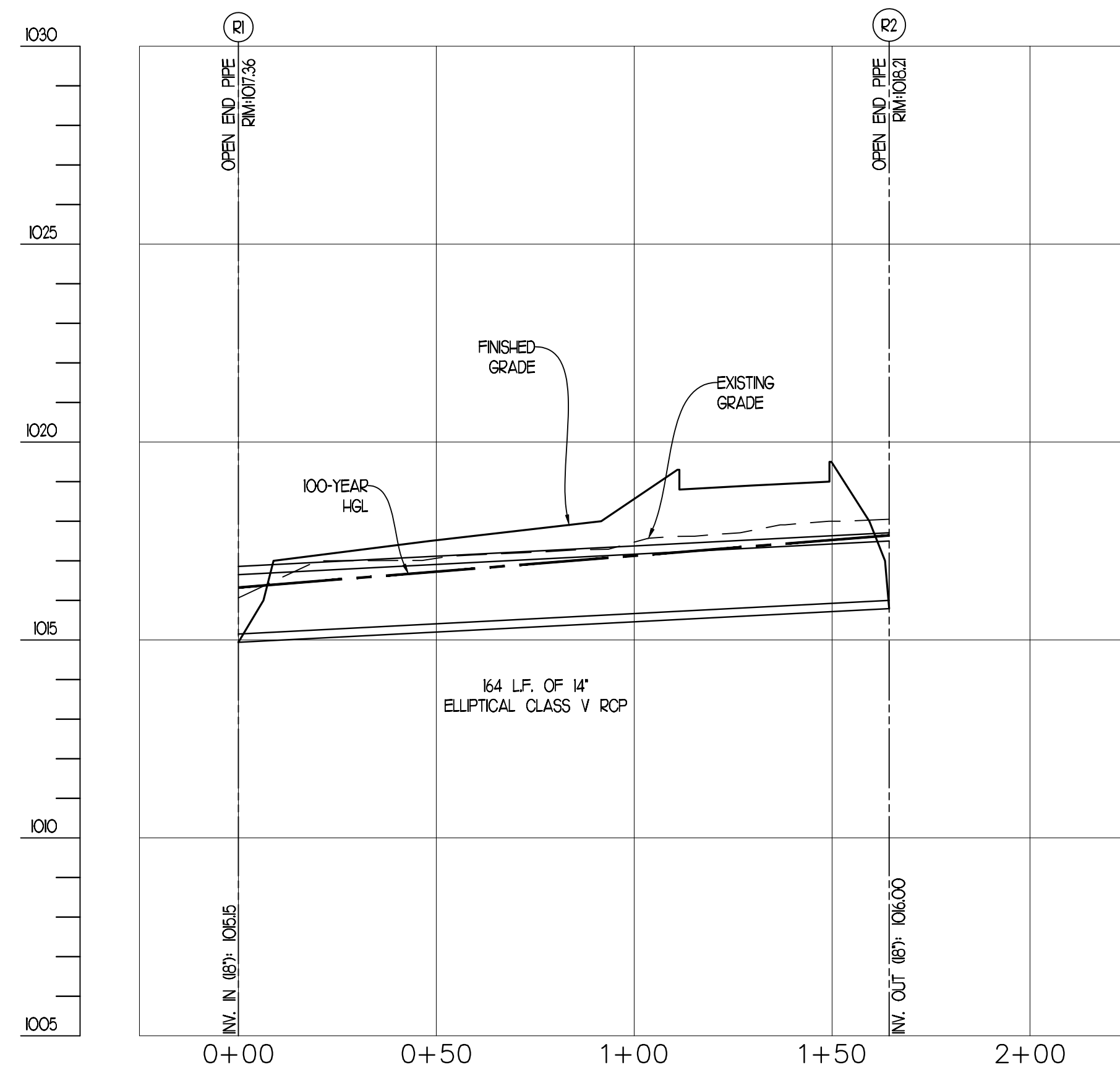
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14

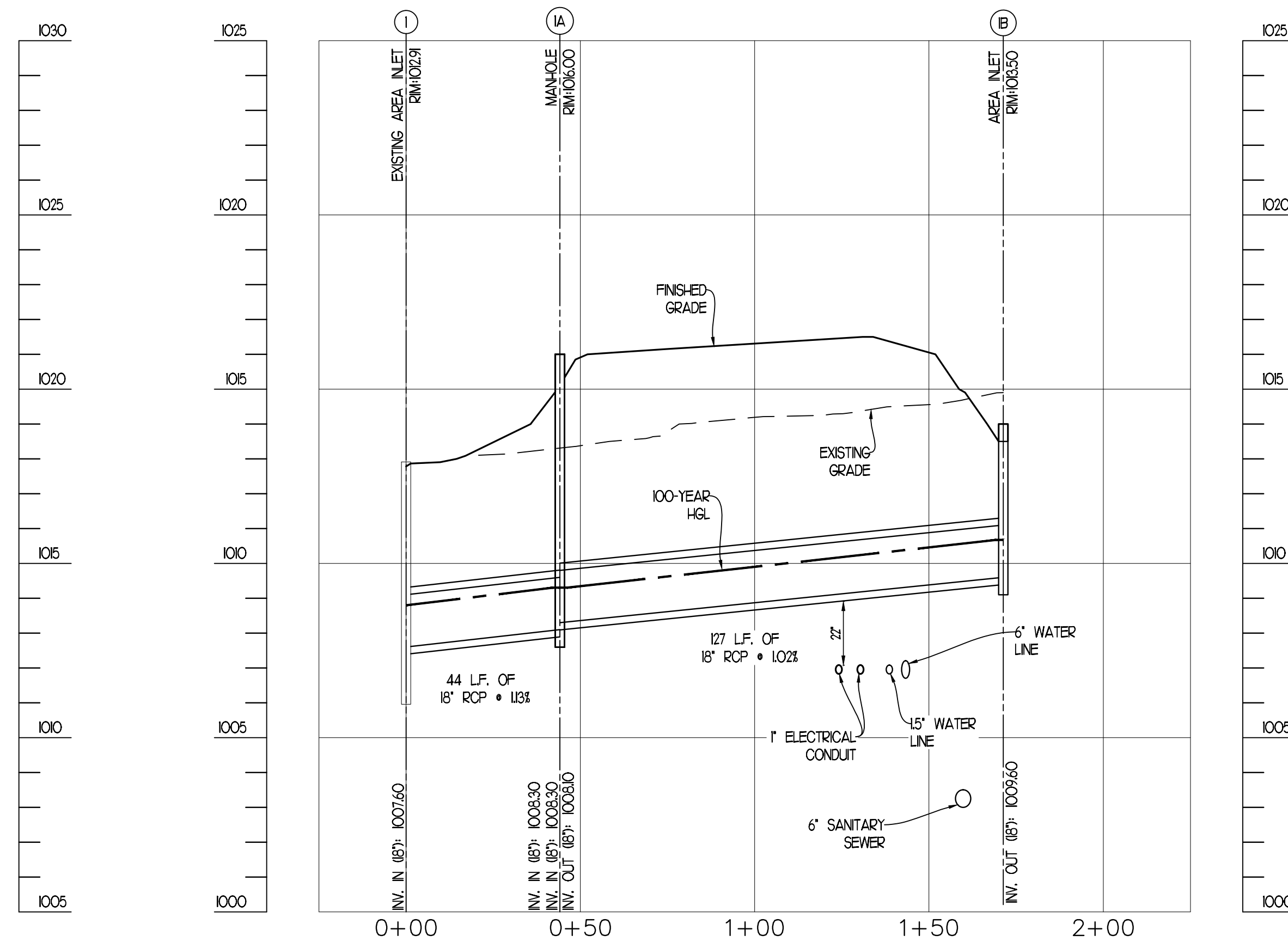
INLET AREA MAP



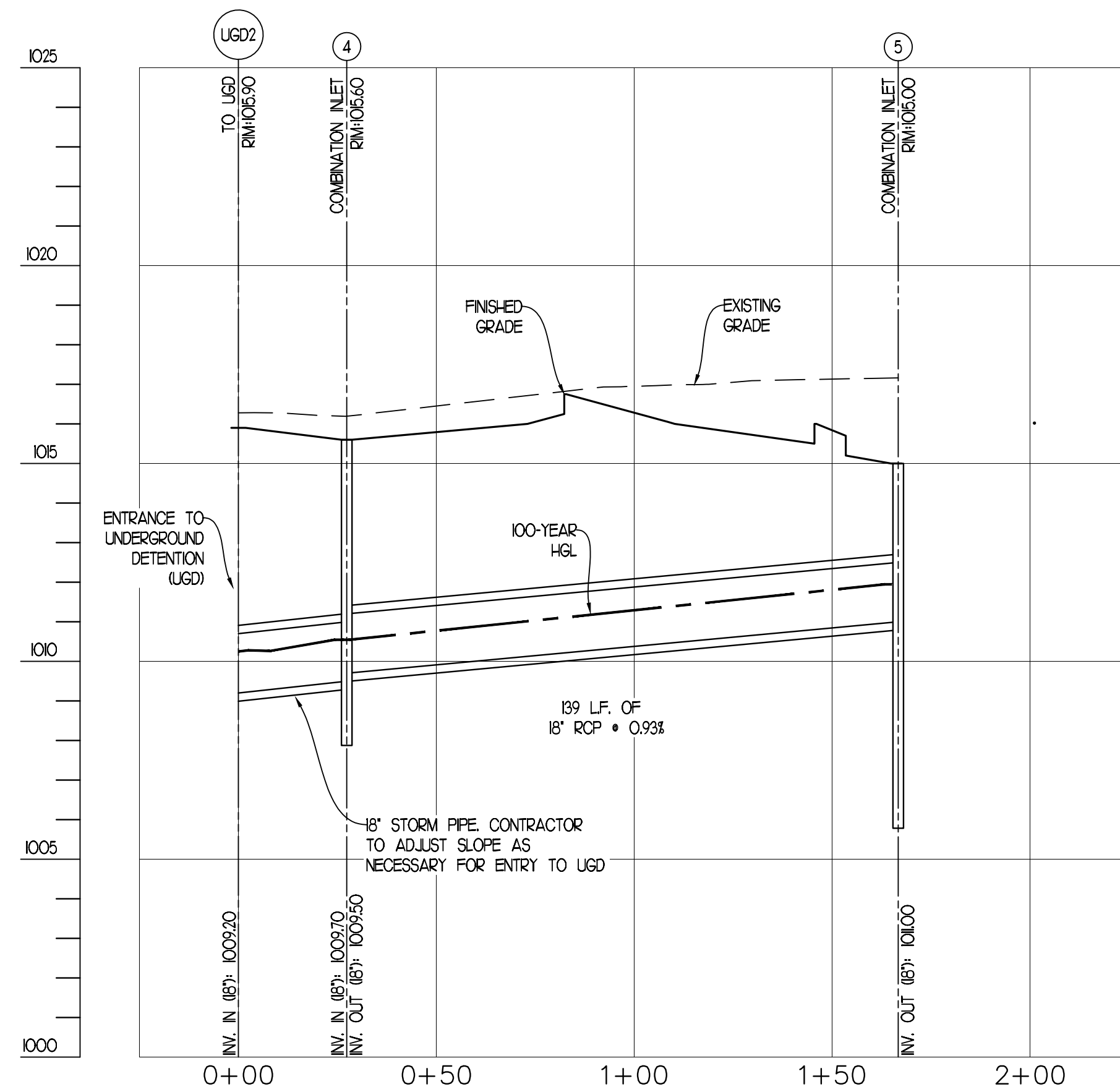
SCALE 1" = 30'



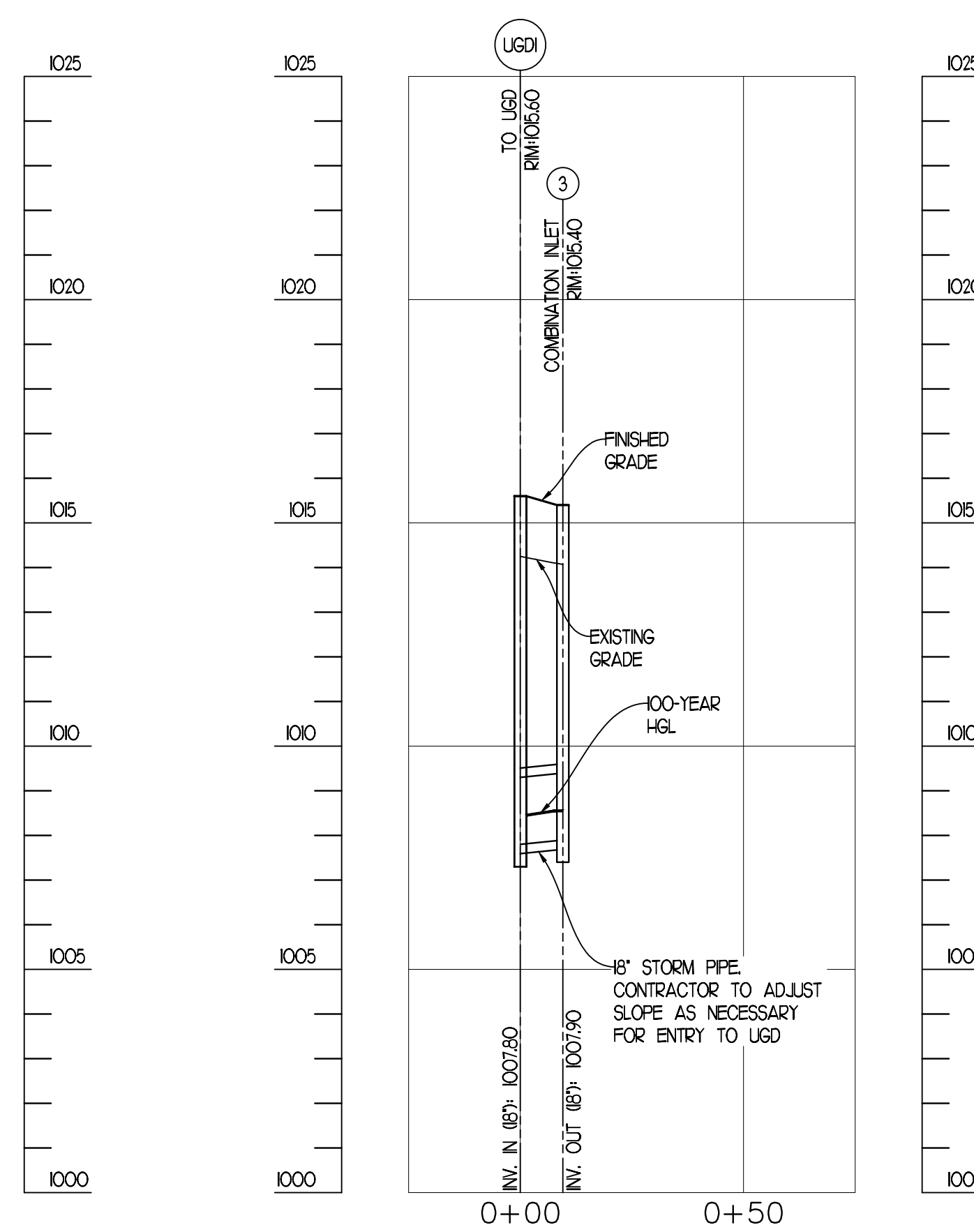
STORM DRAIN PROFILE R1 TO R2



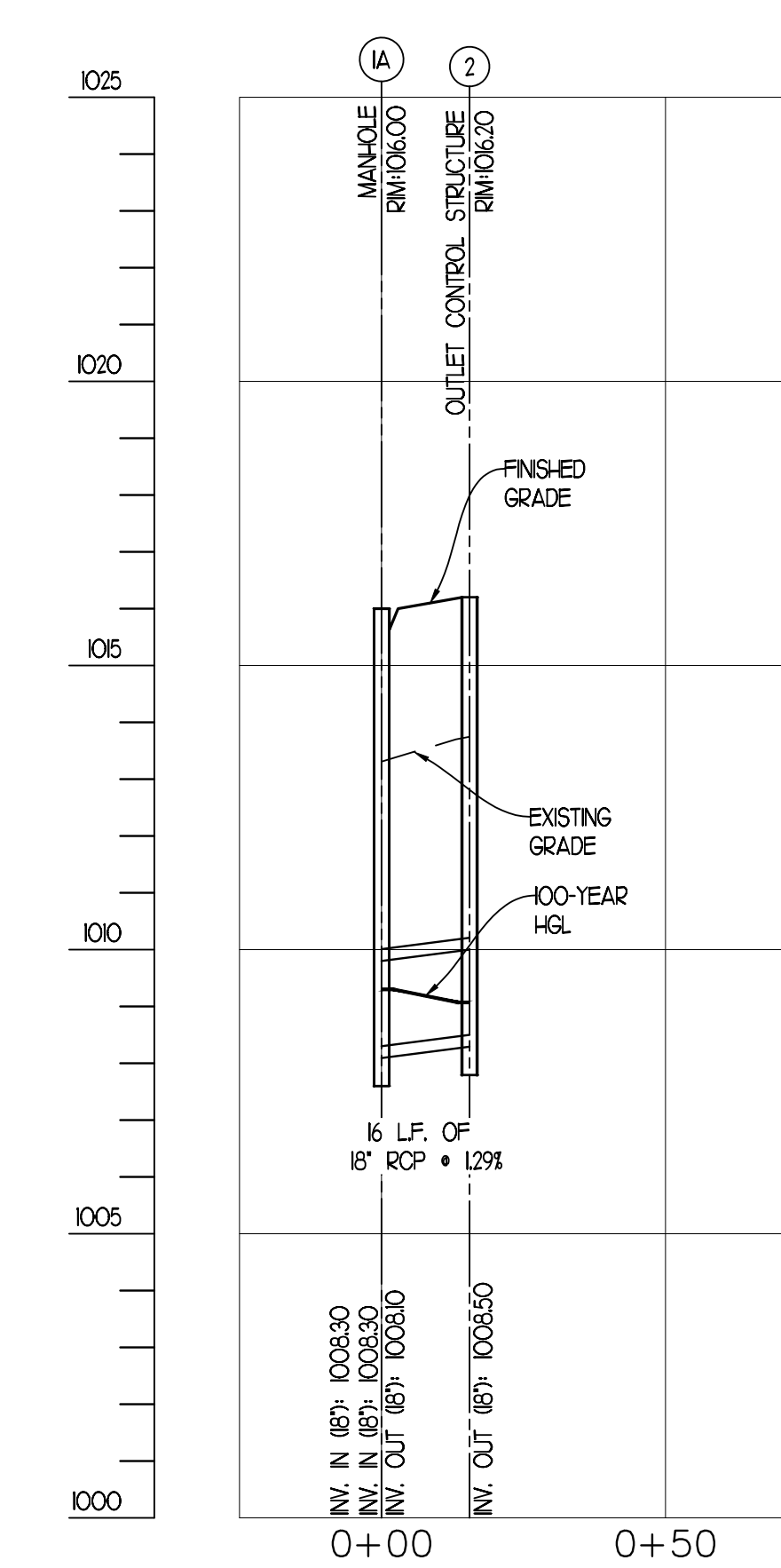
STORM DRAIN PROFILE I TO IB



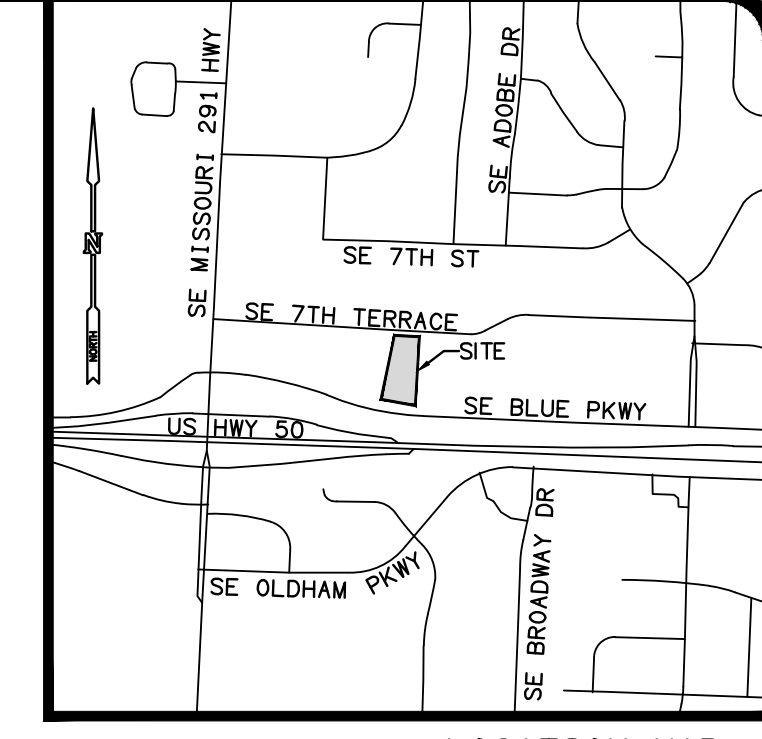
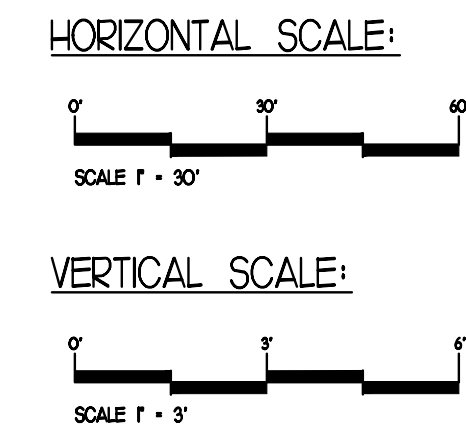
STORM DRAIN PROFILE UGD TO 5



STORM PROFILE UGD TO 3



STORM DRAIN PROFILE IA TO 2



LOCATION MAP
NOT TO SCALE

- STORM DRAIN NOTES**
1. ALL HYDRAULIC DATA FOR THE STORM DRAINAGE PIPES ARE BASED ON THE 100-YEAR DESIGN STORM.
 2. ALL INLET DIMENSIONS SHALL HAVE A MINIMUM INSIDE DIMENSION OF 4\"/>
 3. STORM DRAIN PIPE SHALL BE CLASS III, WALL 18\"/>
 4. HIGH DENSITY POLYETHYLENE PIPE (HDPE) MAY BE USED AS AN ALTERNATE, ALL HDPE PIPE JOINTS, AND FITTINGS SHALL MEET THE REQUIREMENTS OF AASHTO DESIGNATIONS M252 AND M294 TYPE S.
 5. PIPE LENGTHS ARE BASED ON HORIZONTAL DISTANCE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE AND USED FOR DESIGN ONLY. CONTRACTOR TO FIELD VERIFY ACTUAL PIPE LENGTHS NEEDED FOR CONSTRUCTION.
 6. CONTRACTOR IS RESPONSIBLE FOR COMPARING THE STORM DRAIN WITH THE PROFILES FOR THE STORM DRAIN LINES AND INFORMING THE ENGINEER OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO COMMENCEMENT OF CONSTRUCTION.
 7. STORM DRAIN PIPE SHALL HAVE WATER TIGHT JOINTS IN ACCORDANCE WITH ASTM C 361, C443, AND D322.

STORM DRAIN STRUCTURE TABLE				
STR. #	TYPE	RM/THROAT	INV. IN	INV. OUT
1	EXISTING AREA INLET	102.91	1007.60 (A)	
IA	MANHOLE	106.00	1008.30 (B) 1008.30 (D)	1008.80 (D)
IB	AREA INLET	106.50		1009.60 (A)
2	OUTLET CONTROL STRUCTURE	106.20		1008.50 (A)
3	COMBINATION INLET	106.40		1007.90 (UGD)
4	COMBINATION INLET	106.60	1009.70 (C)	1009.50 (UGD)
5	COMBINATION INLET	106.00		1010.00 (A)
R1	OPEN END PIPE	107.36	1016.5 (R2)	
R2	OPEN END PIPE	106.2		1016.00 (R)
UGD1	TO UGD	106.60	1007.80 (C)	
UGD2	TO UGD	106.90	1009.20 (A)	

STORM PIPE TABLE					
STORM/DRAIN PIPE	SIZE	LENGTH	SLOPE	INV. IN	INV. OUT
1A - 1	18"	44'	1.8%	1008.10'	1007.60'
1B - 1A	18"	127'	1.02%	1009.60'	1008.30'
2 - 1A	18"	16'	1.29%	1008.50'	1008.30'
5 - 4	18"	139'	0.93%	1010.00'	1009.70'
R2 - R1	18" ELL.	164'	0.52%	1016.00'	1016.5'

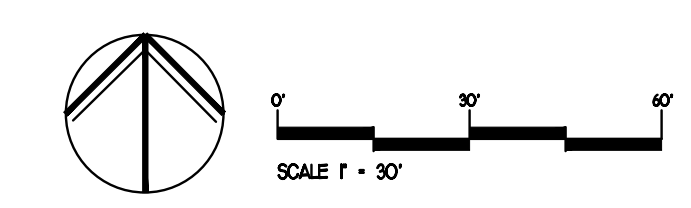
WATER TIGHT STORM PIPES:
CONTRACTOR TO ENSURE THAT ALL STORM PIPES ARE WATER TIGHT AND TESTED PER ASTM C902-02.

CONTRACTOR TO ENSURE ALL STORM DRAIN PIPE SHALL HAVE WATER TIGHT JOINTS IN ACCORDANCE WITH ASTM C 361, C443 AND D322.

DRAINAGE BASIN:
MISSOURI RIVER WATERSHED

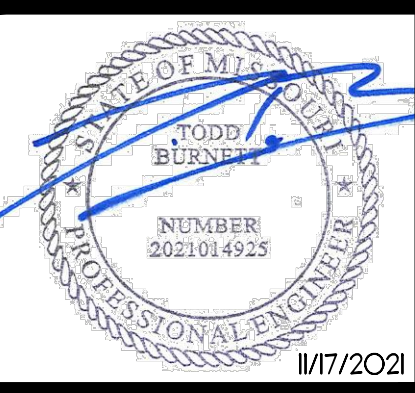
RECEIVING WATERS:
EAST FORK LITTLE BLUE RIVER,
TRIBUTARY OF THE BLUE RIVER -
MISSOURI RIVER WATERSHED

STORM PROFILES



REVISIONS	BY

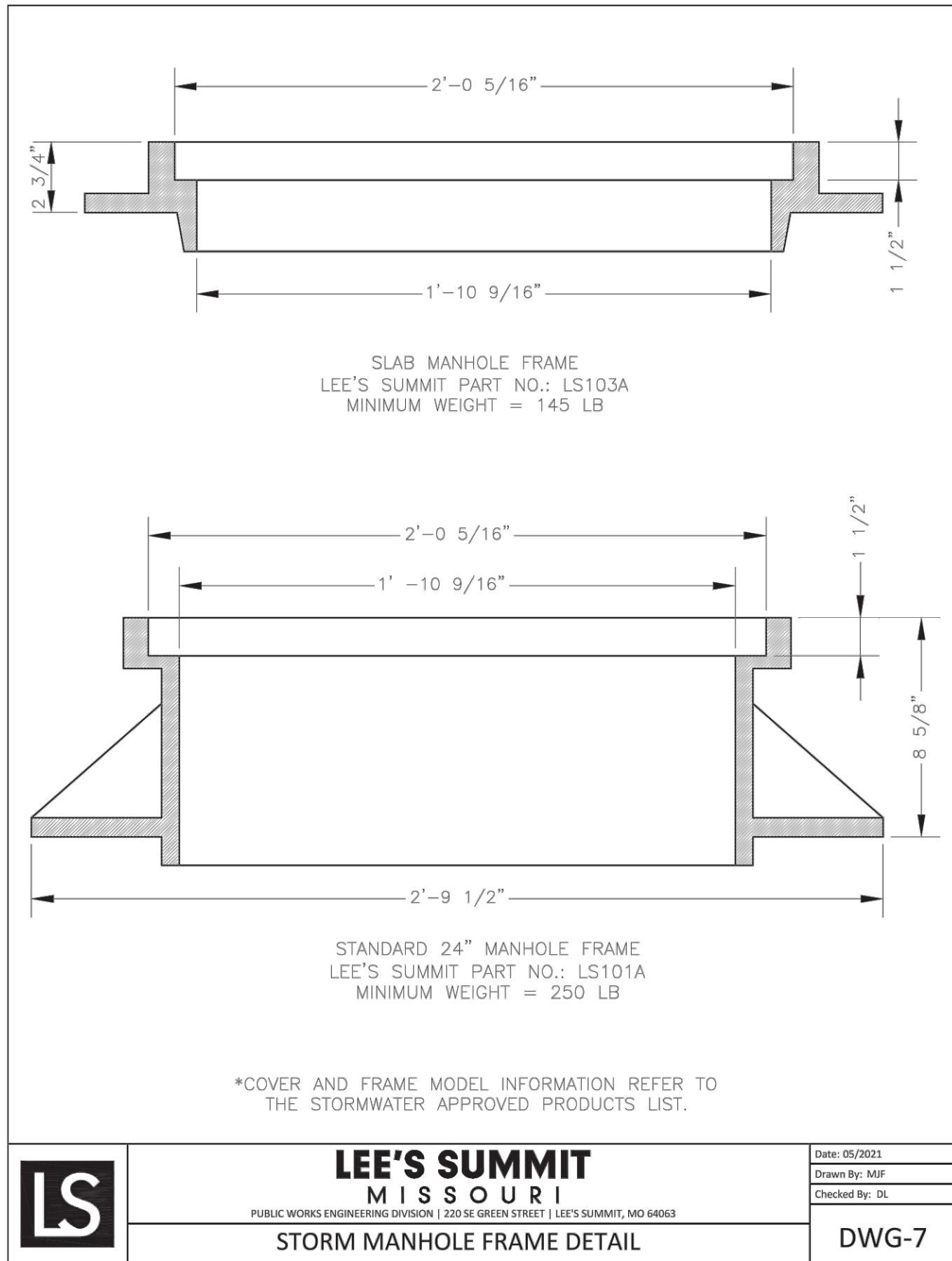
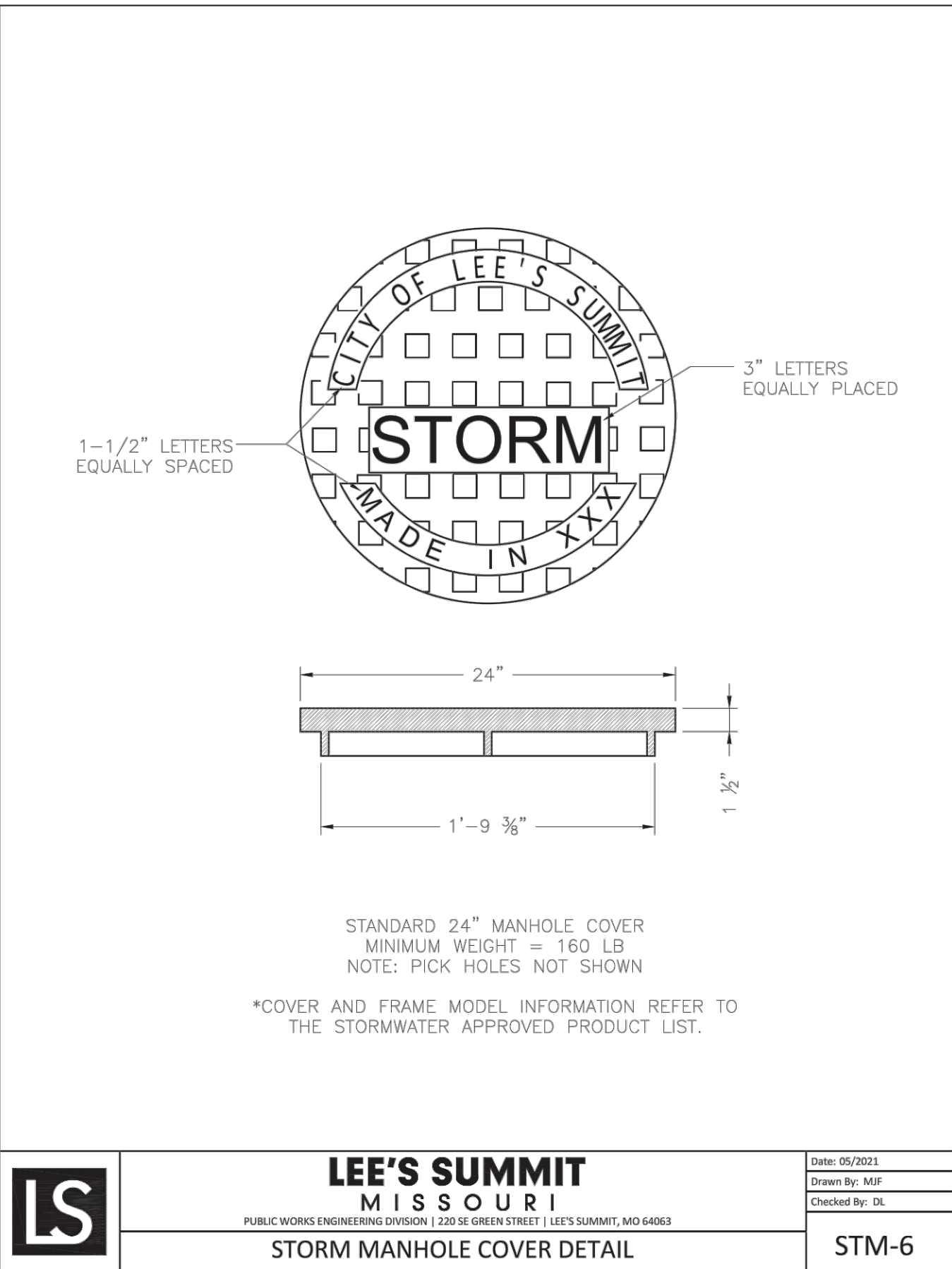
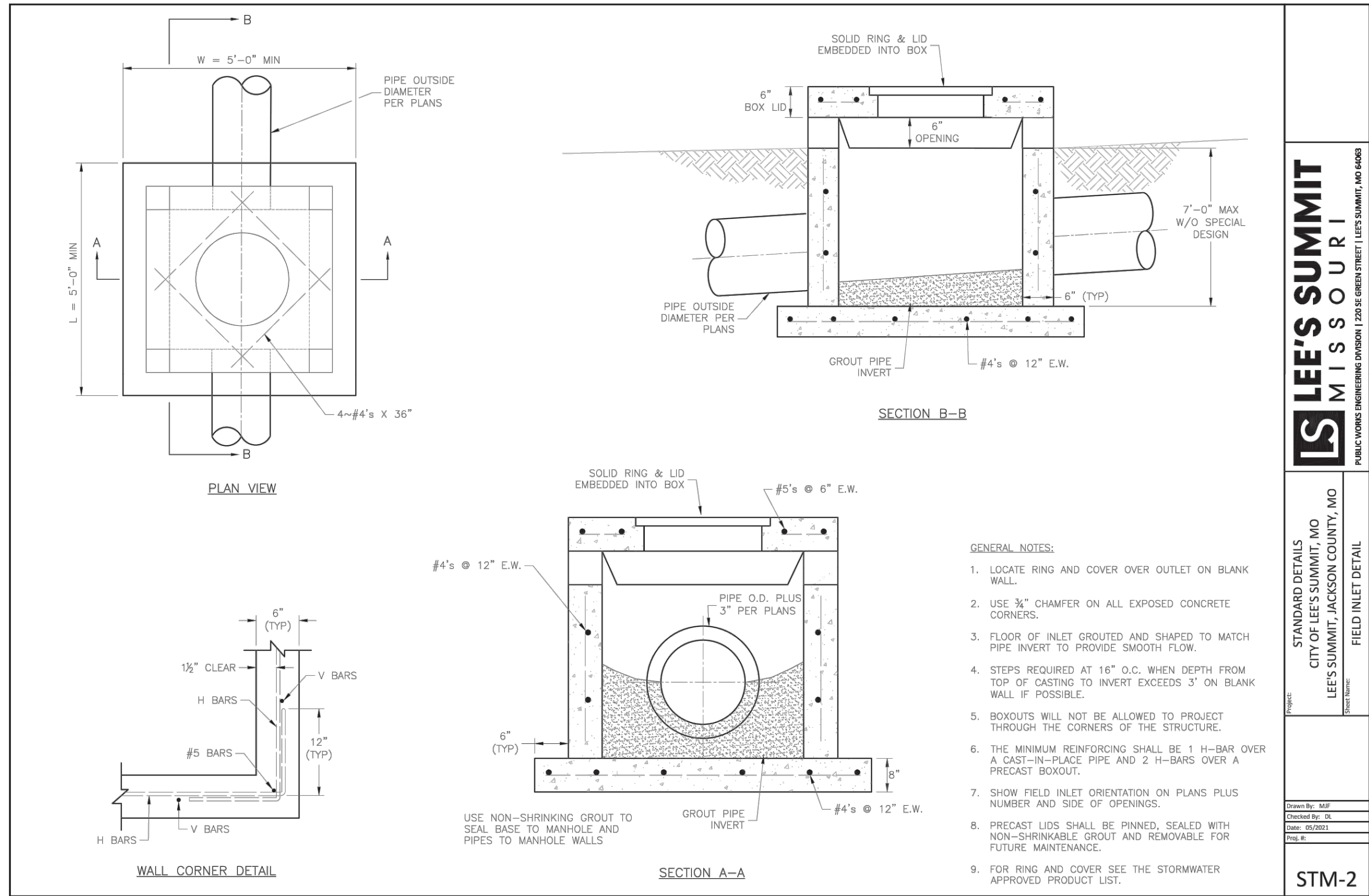
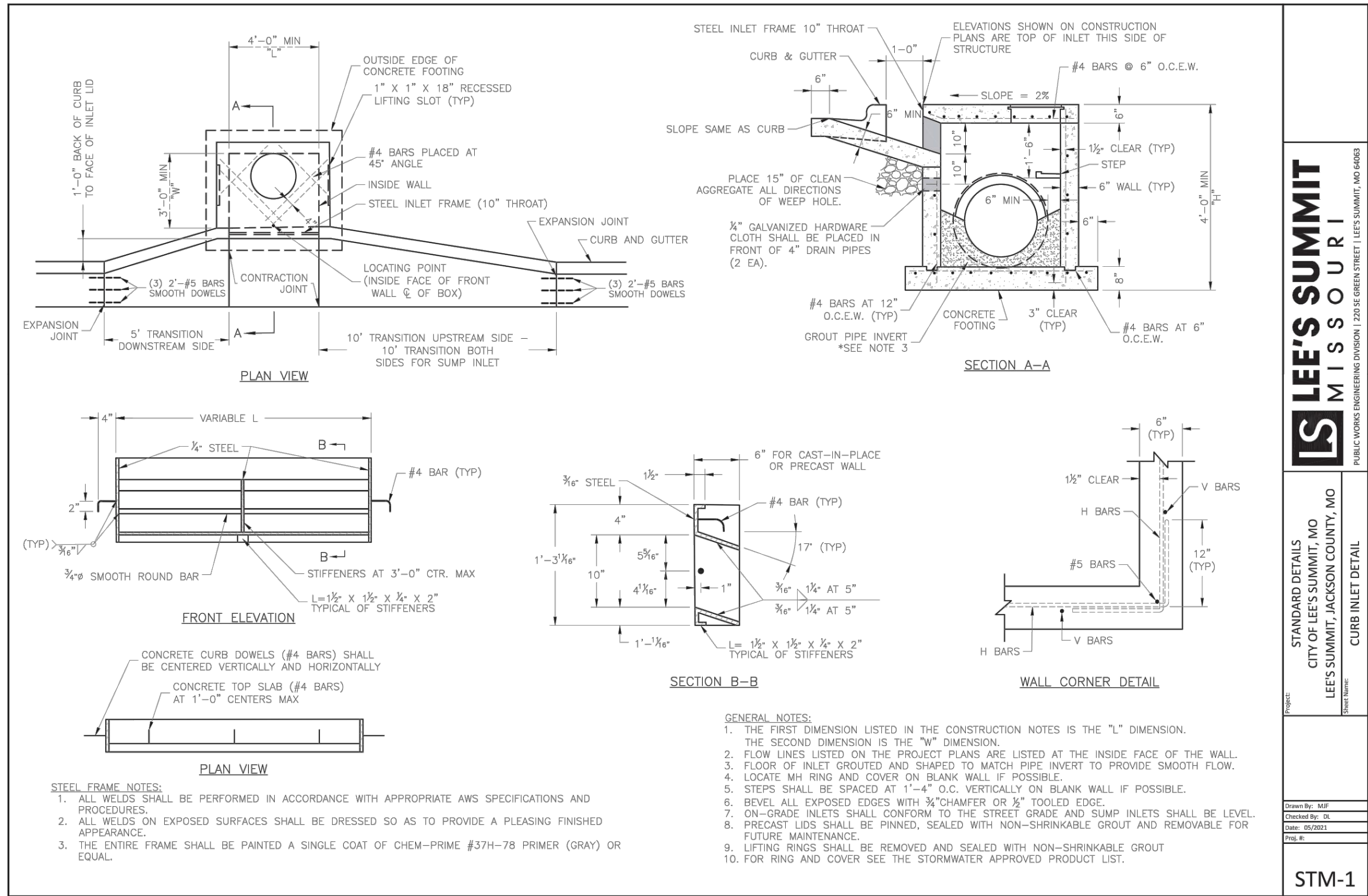
FREELAND and KAUFFMAN, INC.
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Greenville, South Carolina 29609
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CALIBER COLLISION
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LEE'S SUMMIT, MO 64063
CROSS DEVELOPMENT, CG LEE'S SUMMIT, LLC
4338 MARSH RIDGE ROAD
CARROLLTON, TX 75010
TEL (214) 614-8252

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CHECKED
TMB
DATE
11/17/2021
SCALE

DRAWING



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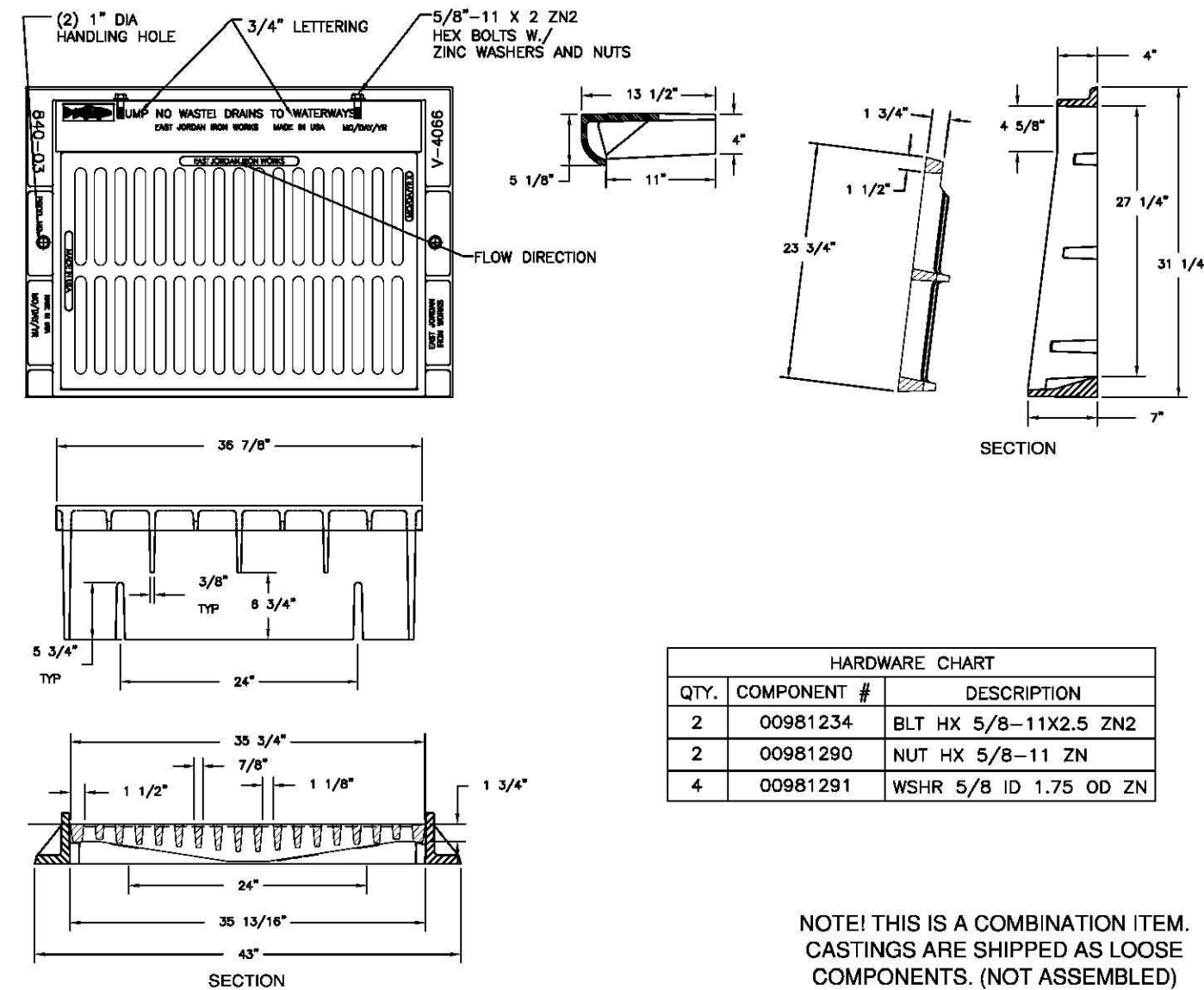
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TMB
DATE
11/7/2021
SCALE

DRAWING

16

STORM DETAILS

V4066-9 ASSEMBLY



QTY.	COMPONENT #	DESCRIPTION
2	00981234	BLT. HX 5/8-11X2.5 ZN2
2	00981290	NUT HX 5/8-11 ZN
4	00981291	WSHR 5/8 ID 1.75 OD ZN

NOTE! THIS IS A COMBINATION ITEM. CASTINGS ARE SHIPPED AS LOOSE COMPONENTS. (NOT ASSEMBLED)

PRODUCT NUMBER
44066930C31

DESIGN FEATURES

MATERIALS
GRATE-GRAY IRON
ASTM A48 CL35B
FRAME-GRAY IRON
ASTM A48 CL35B
BACK-GRAY IRON
ASTM A48 CL35B

DESIGN LOAD
HEAVY DUTY

COATING
UNDIPPED

OPEN AREA
366 SQ. IN.

✓ DESIGNATES MACHINED SURFACE

REFERENCE INFORMATION

44066010
44066930
44066030

DRAWING DETAILS

ORIGINAL DRAWING: DEF 04/26/08
REVISED BY: JJD 04/03/10

Corporate Headquarters
301 Spring Street
PO Box 439
East Jordan, MI
49727-0439
800.626.4100

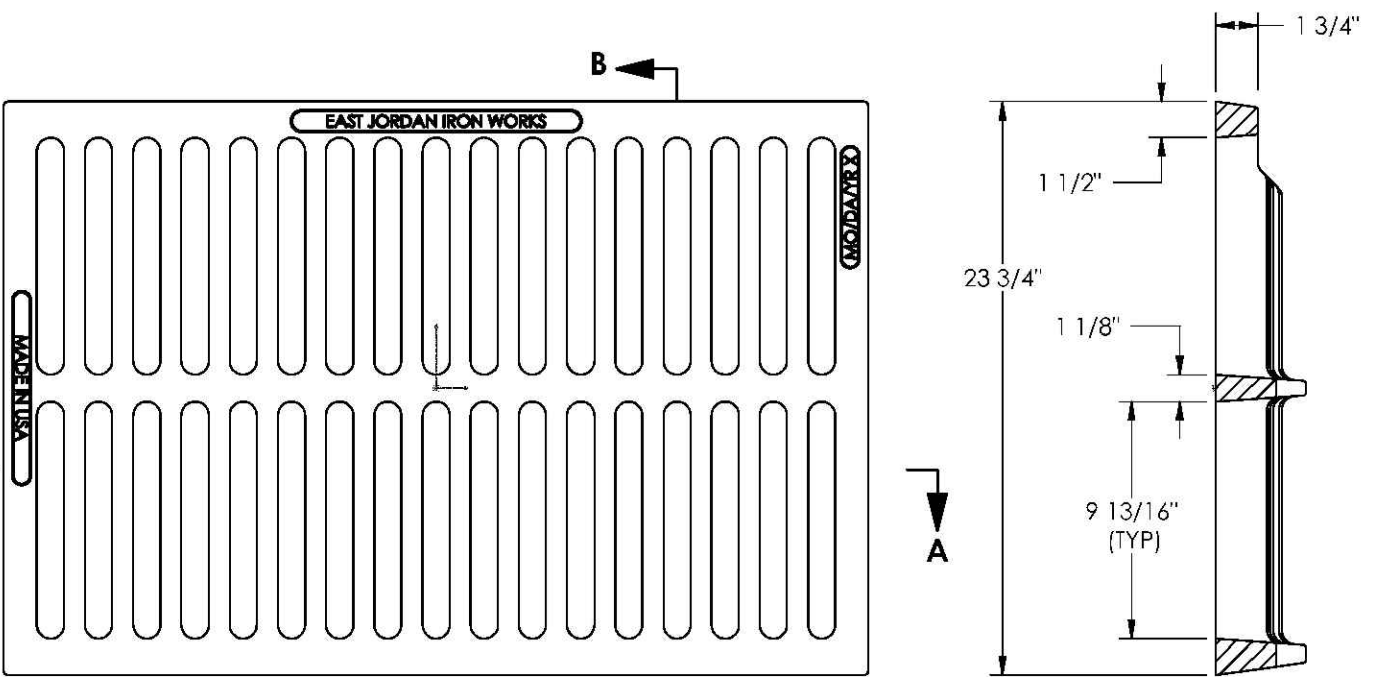
GROUP *

East Jordan
Made in the USA

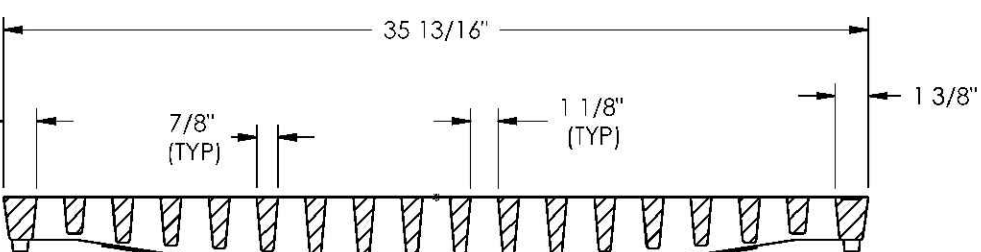
800.626.4653
EJIV EAST JORDAN
MADE IN THE USA

*Weights, dimensions, and drawings are for information only. They are not intended to be used for design purposes. They are not intended to be used for design purposes. They are not intended to be used for design purposes.

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SECTION B-B



SECTION A-A

CATCH BASIN
FRAME AND GRATE

N.T.S.

EJIV EAST JORDAN

800-626-4653
www.ejiw.com
MADE IN USA

PRODUCT NUMBER
44066930

CATALOG NUMBER
V-4066-9

GRATE

LOAD RATING
HEAVY DUTY

COATING
UNDIPPED

ESTIMATED WEIGHT

GRATE: 230 LBS

MATERIAL SPECIFICATION
**GRATE - GRAY IRON
ASTM A48 CL35B**

OPEN AREA
366 SQ. IN.

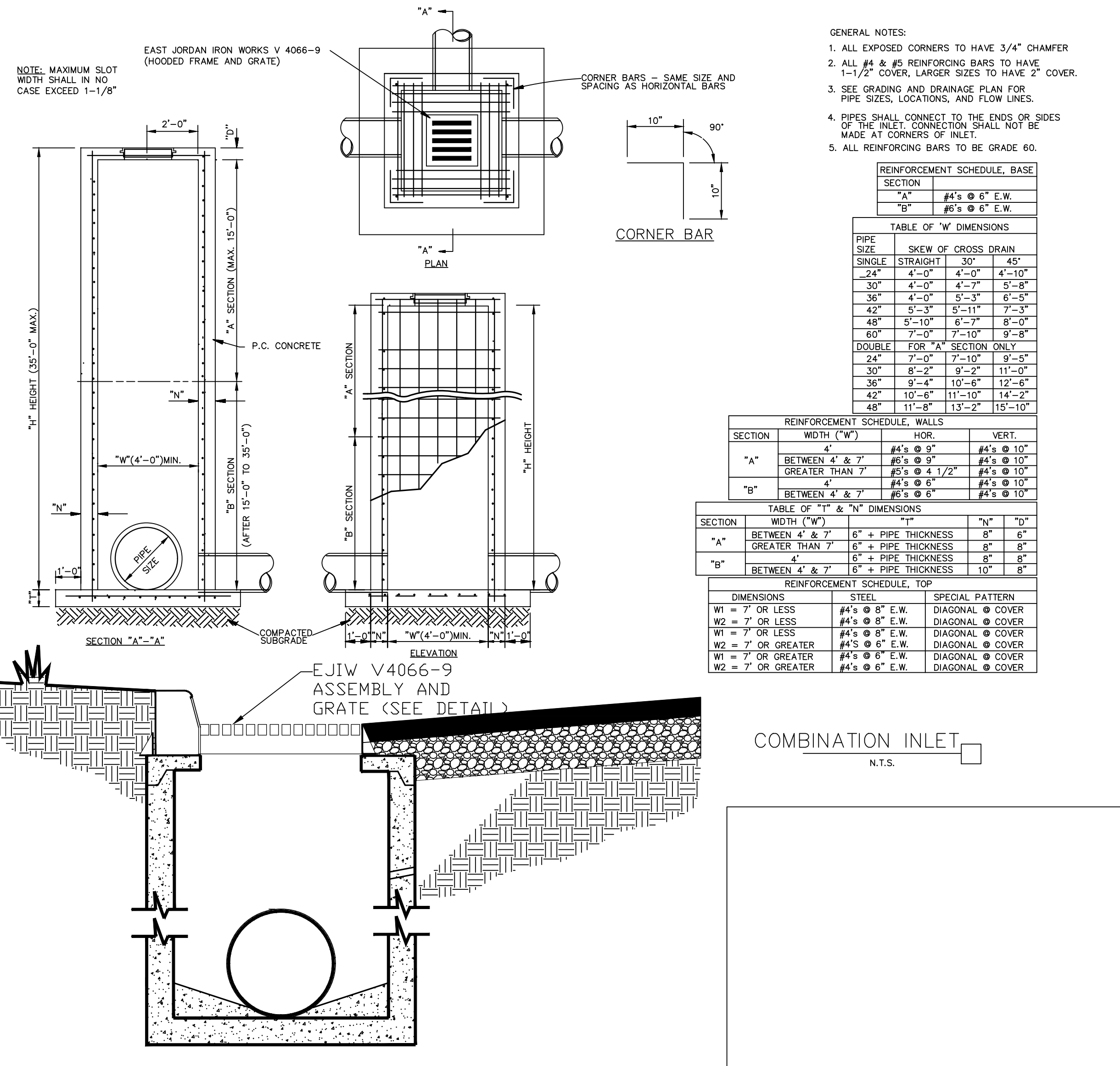
✓ DESIGNATES
MACHINED SURFACE

DRAWN
SBB

DATE
11/12/07

LAST REVISED
DATE

REFERENCE INFORMATION
44066930



- GENERAL NOTES:
1. ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER
 2. ALL #4 & #5 REINFORCING BARS TO HAVE 1-1/2" COVER, LARGER SIZES TO HAVE 2" COVER.
 3. SEE GRADING AND DRAINAGE PLAN FOR PIPE SIZES, LOCATIONS, AND FLOW LINES.
 4. PIPES SHALL CONNECT TO THE ENDS OR SIDES OF THE INLET. CONNECTION SHALL NOT BE MADE AT CORNERS OF INLET.
 5. ALL REINFORCING BARS TO BE GRADE 60.

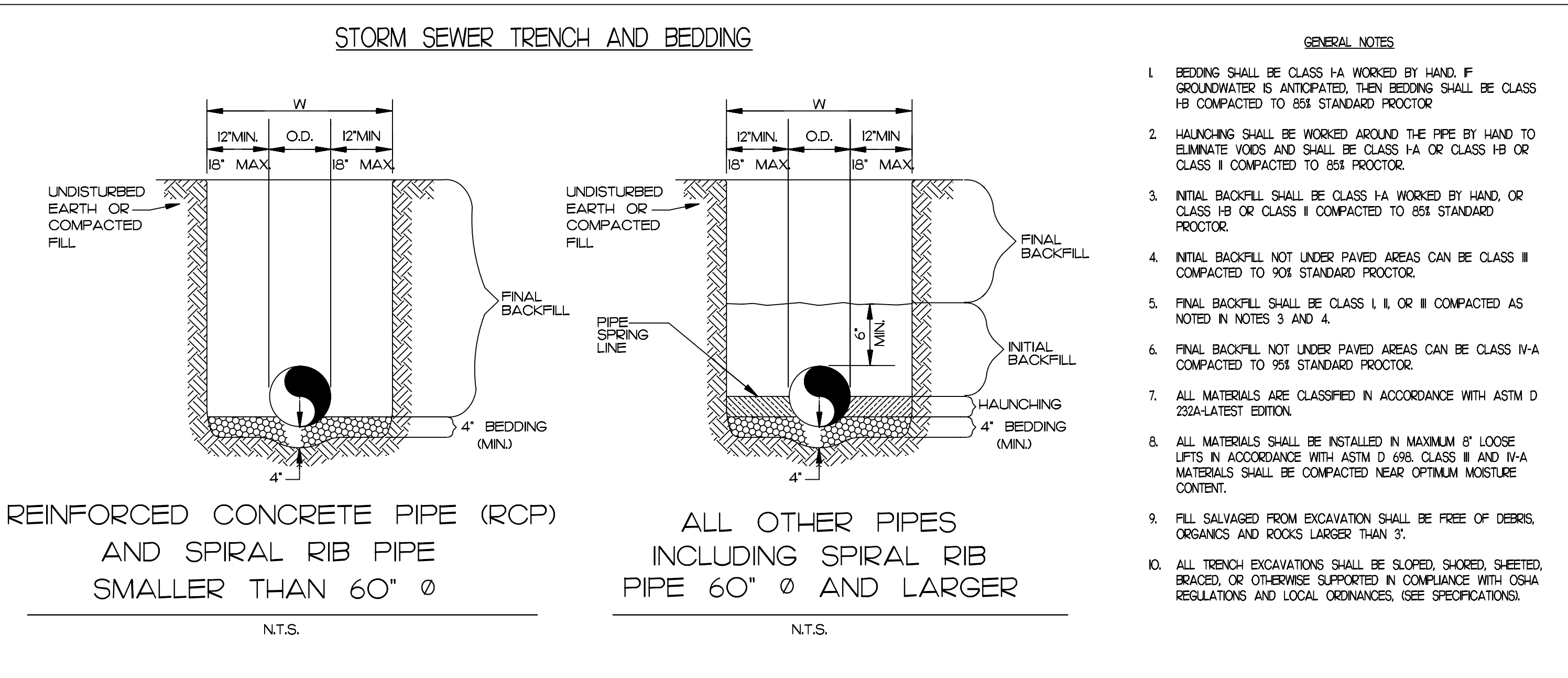
SECTION	#4's @ 6" E.W.	#6's @ 6" E.W.
"A"	#4's @ 6" E.W.	#6's @ 6" E.W.
"B"	#4's @ 6" E.W.	#6's @ 6" E.W.

PIPE SIZE	SKW OF CROSS DRAIN	30"	45"
24"	4'-0"	4'-0"	4'-10"
30"	4'-0"	4'-7"	5'-8"
36"	4'-0"	5'-3"	6'-5"
42"	5'-3"	5'-11"	7'-3"
48"	5'-10"	6'-7"	8'-0"
60"	7'-0"	7'-10"	9'-8"
72"	7'-0"	7'-10"	9'-5"
30"	8'-2"	9'-2"	11'-0"
36"	9'-4"	10'-6"	12'-6"
42"	10'-6"	11'-10"	14'-2"
48"	11'-8"	13'-2"	15'-10"

SECTION	WIDTH ("W")	HOR.	VERT.
"A"	4'	#4's @ 3"	#4's @ 10"
"B"	BETWEEN 4' & 7'	#4's @ 3"	#4's @ 10"
"C"	GREATER THAN 7'	#4's @ 4 1/2"	#4's @ 10"
"D"	BETWEEN 4' & 7'	#4's @ 3"	#4's @ 10"

SECTION	WIDTH ("W")	"T"	"N"	"D"
"A"	BETWEEN 4' & 7'	6" + PIPE THICKNESS	8"	6"
"B"	GREATER THAN 7'	6" + PIPE THICKNESS	8"	8"
"C"	4'	6" + PIPE THICKNESS	8"	8"
"D"	BETWEEN 4' & 7'	6" + PIPE THICKNESS	10"	8"

DIMENSIONS	STEEL	SPECIAL PATTERN
W1 = 7' OR LESS	#4's @ 8" E.W.	DIAGONAL @ COVER
W2 = 7' OR LESS	#4's @ 8" E.W.	DIAGONAL @ COVER
W1 = 7' OR LESS	#4's @ 8" E.W.	DIAGONAL @ COVER
W2 = 7' OR GREATER	#4's @ 6" E.W.	DIAGONAL @ COVER
W1 = 7' OR GREATER	#4's @ 6" E.W.	DIAGONAL @ COVER
W2 = 7' OR GREATER	#4's @ 6" E.W.	DIAGONAL @ COVER



GENERAL NOTES

1. BEDDING SHALL BE CLASS I-A WORKED BY HAND. IF GROUNDWATER IS ANTICIPATED, THEN BEDDING SHALL BE CLASS I-B COMPACTED TO 85% STANDARD PROCTOR.
2. HAUNCHING SHALL BE WORKED AROUND THE PIPE BY HAND TO ELIMINATE VOIDS AND SHALL BE CLASS I-A OR CLASS I-B OR CLASS II COMPACTED TO 85% PROCTOR.
3. INITIAL BACKFILL SHALL BE CLASS I-A WORKED BY HAND, OR CLASS I-B OR CLASS II COMPACTED TO 85% STANDARD PROCTOR.
4. INITIAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS II COMPACTED TO 90% STANDARD PROCTOR.
5. FINAL BACKFILL SHALL BE CLASS I, II, OR III COMPACTED AS NOTED IN NOTES 3 AND 4.
6. FINAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS I-A COMPACTED TO 95% STANDARD PROCTOR.
7. ALL MATERIALS ARE CLASSIFIED IN ACCORDANCE WITH ASTM D 2324-LATEST EDITION.
8. ALL MATERIALS SHALL BE INSTALLED IN MAXIMUM 6" LOOSE LIFTS IN ACCORDANCE WITH ASTM D 698, CLASS II, AND I-A MATERIALS SHALL BE COMPACTED NEAR OPTIMUM MOISTURE CONTENT.
9. FILL SALVAGED FROM EXCAVATION SHALL BE FREE OF DEBRIS, ORGANICS AND ROCKS LARGER THAN 3".
10. ALL TRENCH EXCAVATIONS SHALL BE SLOPED, SHORED, SHEETED, BRACED, OR OTHERWISE SUPPORTED IN COMPLIANCE WITH OSHA REGULATIONS AND LOCAL ORDINANCES. (SEE SPECIFICATIONS).

REVISIONS BY

FREELAND and KAUFFMAN, INC.
Engineers * Landscape Architects
209 West Stone Avenue
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864-233-5497
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CROSS DEVELOPMENT, CC LEE'S SUMMIT, LLC
4338 MARSH RIDGE ROAD
CARROLLTON, TX 75010
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17


STORM DETAILS



//ADS	4640 TREEMAN BLVD MILWAUKEE, WI 53208	StormTech® Chamber System 666-982-2964 WWW.STORMTECH.COM	DATE: 7/27/21	AGE: 6MO	REASON FOR CONCERN:	DESCRIPTION:
3	SHEET		DATE: 7/27/21	AGE: 6MO	REASON FOR CONCERN:	DESCRIPTION:
OF			DATE: 7/27/21	AGE: 6MO	REASON FOR CONCERN:	DESCRIPTION:
5			DATE: 7/27/21	AGE: 6MO	REASON FOR CONCERN:	DESCRIPTION:

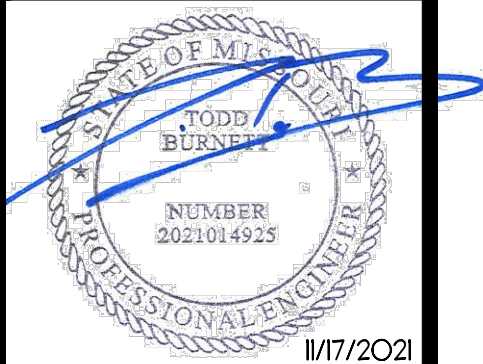
95	STORMTECH MC-3500 CHAMBERS
22	STORMTECH MC-3500 END CAPS
12	STONE ABOVE (in)
9	STONE BELOW (in)
30	% STONE VOID
18,021	INSTALLED SYSTEM VOLUME (CF) (PERIMETER STONE INCLUDED)
6351	SYSTEM AREA (ft ²)
749	SYSTEM PERIMETER (ft)

1021.00	MAXIMUM ALLOWABLE GRADE (TOP OF PAVEMENT/UNPAVED)
1015.00	MINIMUM ALLOWABLE GRADE (UNPAVED WITH TRAFFIC)
1014.50	MINIMUM ALLOWABLE GRADE (UNPAVED NO TRAFFIC)
1014.50	MINIMUM ALLOWABLE GRADE (BASE OF FLEXIBLE PAVEMENT)
1014.50	MINIMUM ALLOWABLE GRADE (TOP OF RIGID PAVEMENT)
1014.00	TOP OF STONE
1013.00	TOP OF MC-3500 CHAMBER
1011.37	15" TOP MANIFOLD INVERT
1009.42	24" ISOLATOR ROW PLUS CONNECTION INVERT
1009.38	15" BOTTOM CONNECTION INVERT
1009.25	BOTTOM OF MC-3500 CHAMBER
1008.50	UNDERDRAIN INVERT
1008.50	BOTTOM OF STONE

		StormTech® Chamber System		DATE: 07-14-21 DRAWN: RSG PROJECT #: S24074 CHECKED: JAC		CALIBER COLLISION LEES SUMMIT, MO	
4440 TREHMAN BLVD HILLARIO, OK 73056		888-852-2684 WWW.STORMTECH.COM		DATE: 07-14-21 RSG: JAC DESIGNED BY: RSG: JAC DRAWN: RSG: JAC REVISIONS: NONE		THE DESIGN NUMBER SHALL INDICATE THE YEAR, MONTH AND DAY OF THE DATE THE DESIGN NUMBER WAS ISSUED. THE YEAR SHALL BE THE LAST TWO DIGITS OF THE YEAR, THE MONTH SHALL BE THE LAST TWO DIGITS OF THE MONTH, AND THE DAY SHALL BE THE LAST TWO DIGITS OF THE DAY. THE DESIGN NUMBER SHALL BE USED TO IDENTIFY THE DESIGN AND SHALL BE USED TO IDENTIFY THE DESIGN. THE DESIGN NUMBER SHALL BE USED TO IDENTIFY THE DESIGN AND SHALL BE USED TO IDENTIFY THE DESIGN.	
2 SHEET OF 5		0 30' 60'					

[illegible][illegible]

FREELAND and KAUFFMAN, INC.
*Engineers * Landscape Architects*
209 West Stone Avenue
Greenville, South Carolina 29609
864-233-5497
fax 864-233-8915



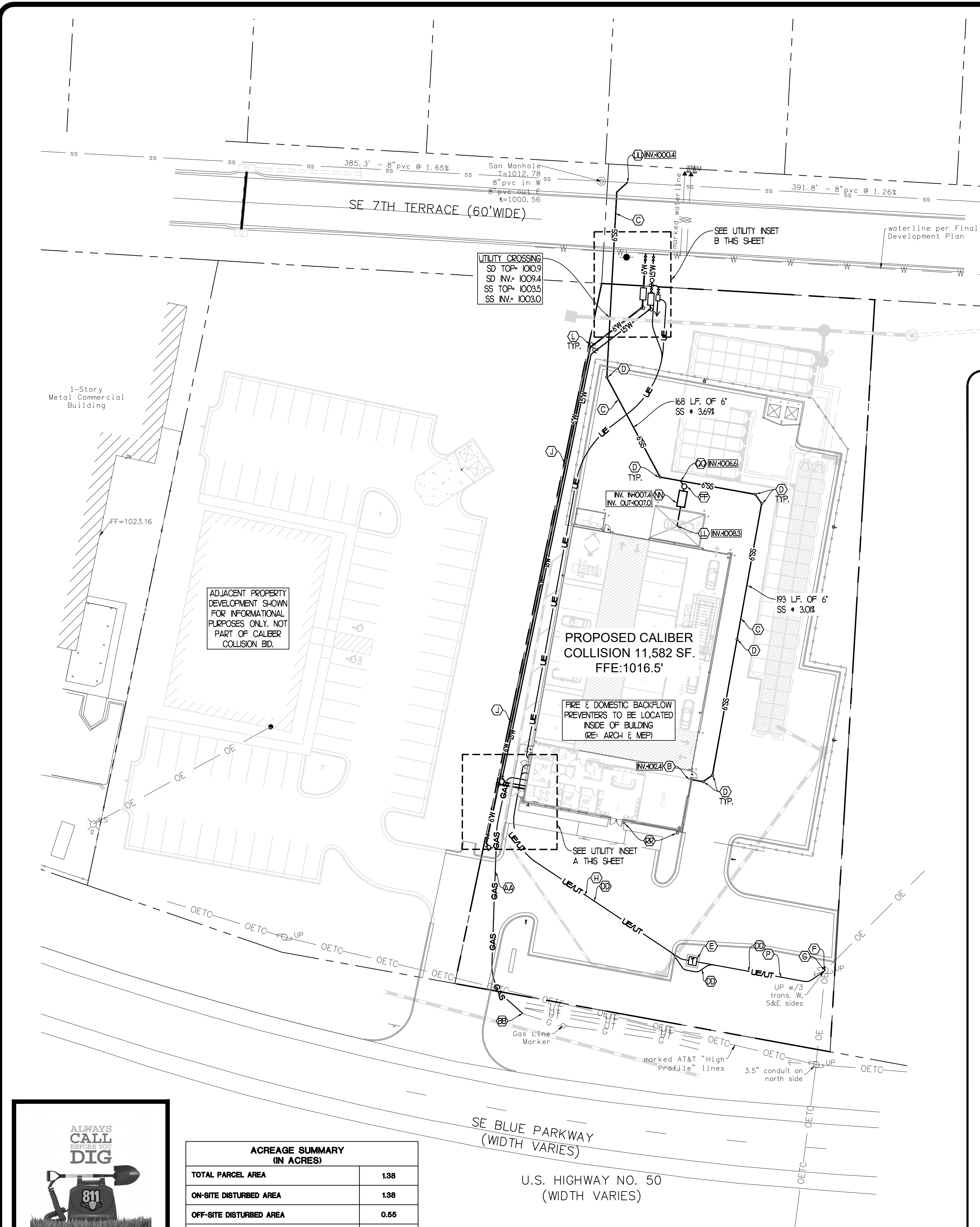
CALIBER COLLISION
710 SE BLUE PARKWAY
LEE'S SUMMIT, MO 64063

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CHECKED
TMB
DATE
11/17/2021
SCALE
DRAWING

DRAWING

UGD DETAILS

18



UTILITY CONTACTS

WATER:
LEE'S SUMMIT WATER UTILITIES
PHONE: (816) 969-1900

SEWER:
LEE'S SUMMIT WATER UTILITIES
PHONE: (816) 969-1900

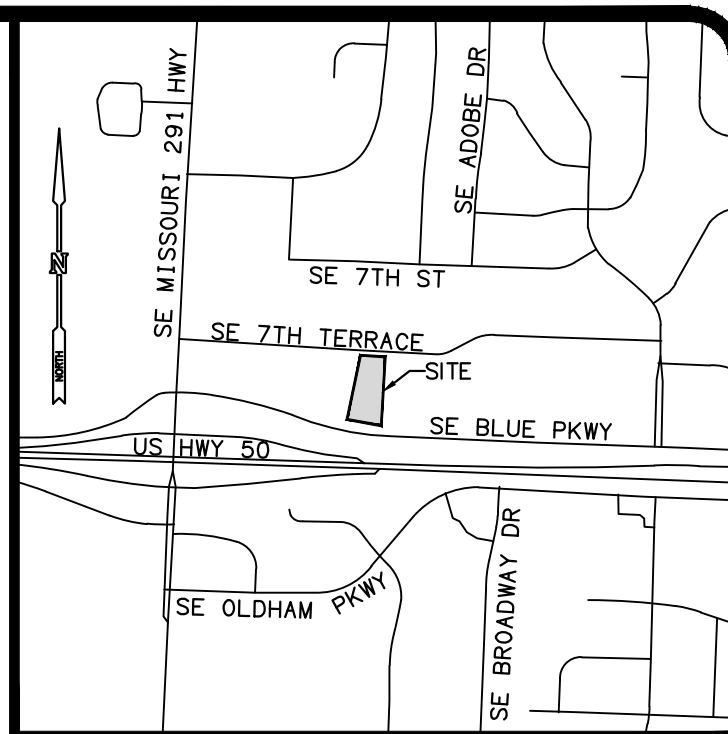
GAS:
SPIKE, INC.
CONTACT: BOBBIE GAULSBERRY
PHONE: (816) 969-2266

ELECTRIC:
EVERGY LEE'S SUMMIT SERVICE
CONTACT: DOUG DAVIN
PHONE: (816) 347-4320

TELEPHONE/CABLE:
AT&T
CONTACT: DARRIN SHEPARD
PHONE: (816) 772-0336

LEGEND

EXISTING	PROPOSED	DESCRIPTION
15" W	15" W	15" DOMESTIC WATER LINE
6" F	6" F	6" FIRE LINE
1" R	1" R	1" IRRIGATION WATER LINE
—	—	FIRE HYDRANT
—	—	UNDERGROUND ELECTRIC LINE
—	—	UNDERGROUND TELEPHONE LINE
—	—	STORM DRAIN
—	—	SANITARY SEWER LINE
—	—	GAS LINE
—	—	SS MAN-HOLE
—	—	SS CLEAN-OUT



LOCATION MAP
NOT TO SCALE

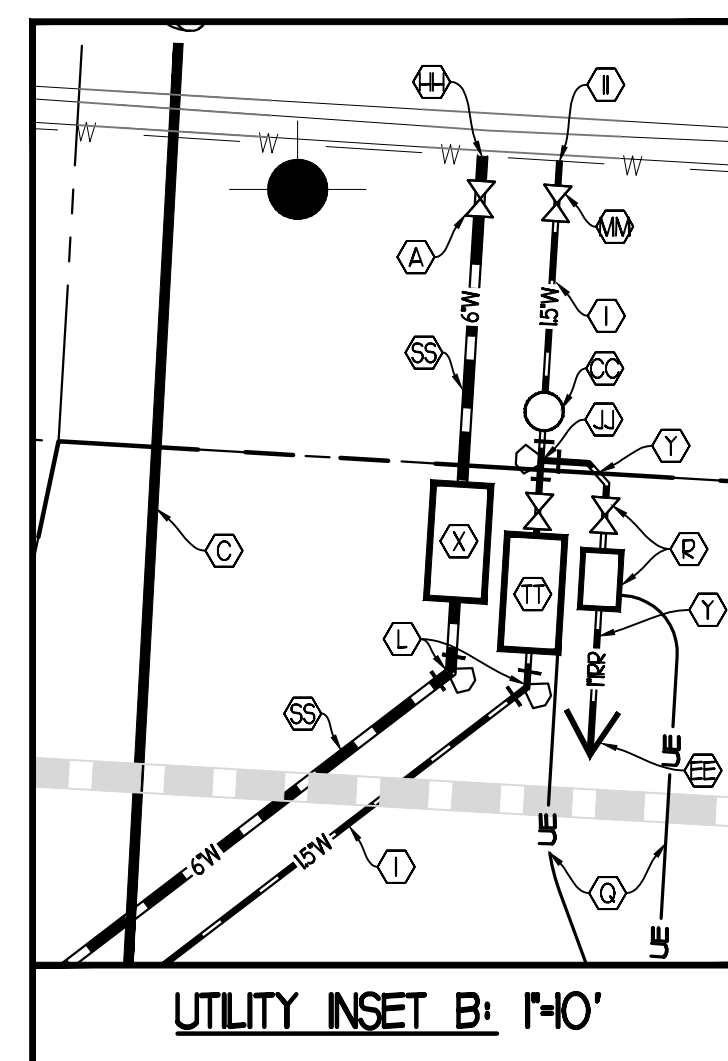
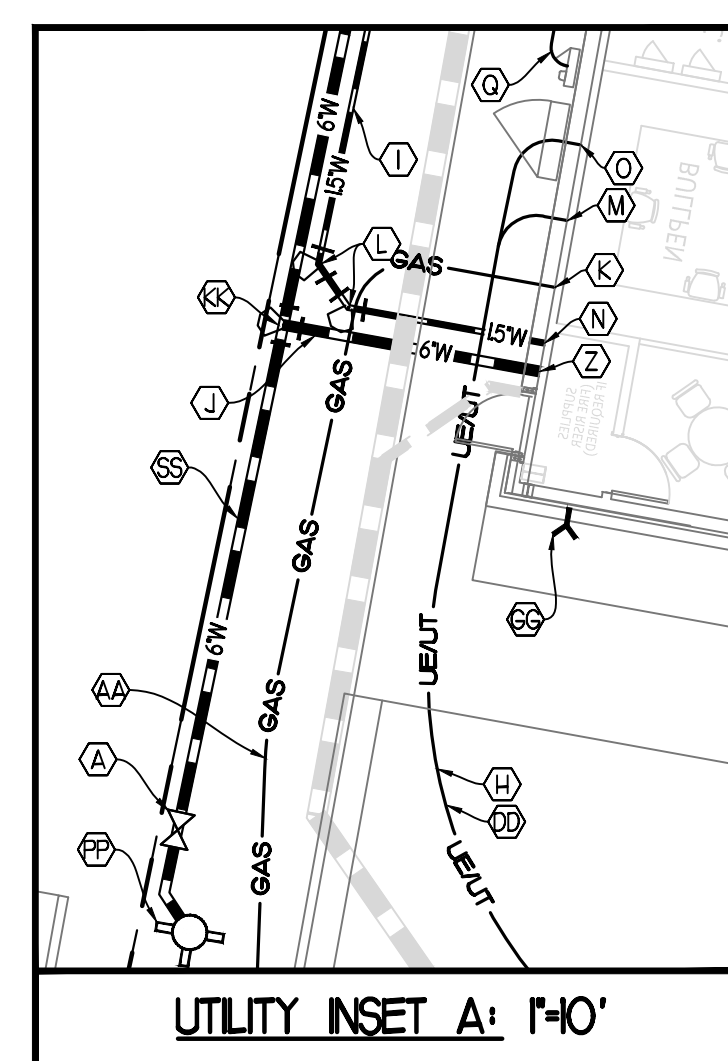
- CONTRACTOR SHALL USE EXTREME CAUTION WHILE WORKING UNDERNEATH OVERHEAD POWER LINES. ANY DAMAGE TO THE EXISTING UTILITY POLES AND UTILITY LINES SHALL BE REPAIRED/REPLACED AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR IS RESPONSIBLE FOR RELOCATING ANY APPLICABLE CONFLICTING UTILITIES NECESSARY TO COMPLETE SERVICE INSTALLATIONS.
- CONTRACTOR IS RESPONSIBLE FOR STAKING SITE AS NECESSARY TO DETERMINE SERVICE LOCATIONS AND ELEVATIONS PRIOR TO MOBILIZING TO INSTALL SERVICES.
- CONTRACTOR SHALL ENSURE THAT PROPOSED WATER LINE EXTENSION HAS A MINIMUM OF 18" VERTICAL CLEARANCE WHEN CROSSING STORM & SANITARY SEWER (PROPOSED OR EXISTING) BENDS AND FITTINGS SHALL BE UTILIZED TO MEET THIS REQUIREMENT.
- CONTRACTOR TO INCLUDE ALL METER INSTALLATION AND CAPACITY FEES FOR UTILITY SERVICES IN THE BASE BID TO PROVIDE UTILITY SERVICES AS SHOWN.
- HYDRANTS SHALL BE PROTECTED FROM MECHANICAL DAMAGE. THE MEANS OF PROTECTION SHALL BE ARRANGED SO THAT IT WILL NOT INTERFERE WITH THE OPERATION OF THE HYDRANT (3 FEET CLEARANCE).

UTILITY PLAN NOTES

- ALL SITE WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE PLANS PREPARED BY FREELAND & KAUFFMAN, CURRENT REQUIREMENTS OF LEE'S SUMMIT WATER UTILITIES, JACKSON COUNTY, AND ALL OTHER PERTINENT FEDERAL AND STATE LAWS.
- THE CONTRACTOR SHALL COMPLY AT ALL TIMES WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, PROVISIONS, AND POLICES GOVERNING THE MEANS OF PROTECTION SHALL BE ARRANGED SO THAT IT WILL NOT INTERFERE WITH THE OPERATION OF THE HYDRANT (3 FEET CLEARANCE).
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXAMINING THE AREAS AND CONDITIONS UNDER WHICH THE PROJECT IS TO BE CONSTRUCTED PRIOR TO THE SUBMISSION OF A BID. SUBMISSION OF A BID SHALL BE CONSIDERED TO MEAN THE CONTRACTOR HAS REVIEWED THE SITE AND IS FAMILIAR WITH CONDITIONS AND CONSTRAINTS OF THE SITE.
- BEFORE EXCAVATION, ALL UNDERGROUND UTILITIES SHALL BE LOCATED IN THE FIELD BY THE PROPER AUTHORITIES. THE CONTRACTOR SHALL NOTIFY "CALL BEFORE YOU DIG" AT 811. THE LOCATION OF ALL UTILITIES AND UNDERGROUND STRUCTURES ARE APPROXIMATE AND MAY NOT ALL BE SHOWN. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXISTENCE AND EXACT LOCATION OF ALL EXISTING UTILITIES.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO BID AND PERFORM ALL UTILITY WORK IN COMPLIANCE TO ALL APPLICABLE LOCAL AND STATE CODES AND REGULATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL FEES ASSOCIATED WITH THE INSTALLATION, INSPECTION, TESTING AND FINAL ACCEPTANCE OF ALL PROPOSED UTILITIES. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY COMPANY ON THE ADDITION, REMOVAL AND/OR RELOCATION OF UTILITIES AND UTILITY POLES AND THE EXTENSION OF ALL PROPOSED UTILITIES TO THE BUILDING.
- ALL UTILITIES SHALL BE INSTALLED IN ACCORDANCE WITH THE SPECIFICATIONS OF THE RESPECTIVE UTILITY COMPANY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE ALL UTILITIES ARE INSTALLED CORRECTLY TO MEET PROJECT REQUIREMENTS WHETHER PERFORMED BY THE CONTRACTOR OR NOT.
- A SEALED AS-BUILT DRAWING OF NEW UTILITY SERVICES SHALL BE PREPARED BY THE ENGINEER/SURVEYOR AND SUBMITTED TO LEE'S SUMMIT WATER UTILITIES UPON COMPLETION & PRIOR TO ACTIVATION OF THE PROJECT.
- REFER TO THE BUILDING PLANS FOR ALL CONDUIT SIZES, NUMBER AND LOCATIONS FOR SERVICE TO THE SIGN AND SITE LIGHT POLES. COORDINATE FINAL CONDUIT LOCATIONS WITH THE OWNER.
- CONTRACTOR SHALL COORDINATE WITH THE OWNER ON CONDUIT ROUTE TO THE STORE FROM THE TRANSFORMER AND/OR SERVICE UTILITY POLE FOR TELEPHONE AND ELECTRICAL SERVICES.
- ALL FILL MATERIAL IS TO BE IN PLACE AND COMPACTED PRIOR TO INSTALLATION OF PROPOSED UTILITIES.
- CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY'S INSPECTORS 72 HOURS BEFORE CONNECTION OR DEMOLITION OF ANY EXISTING LINE.
- MATERIALS AND WORKMANSHIP SHALL CONFORM WITH THE LEE'S SUMMIT WATER UTILITIES WATER STANDARDS AND SPECIFICATIONS AND LEE'S SUMMIT WATER UTILITIES SEWER STANDARDS AND SPECIFICATIONS.
- MINIMUM TRENCH WIDTH SHALL BE 2 FEET.
- RESTRAINED JOINTS BY AN APPROVED PIPE MANUFACTURER ARE TO BE USED FOR ALL PUBLIC WATERLINES WHERE THRUST RESTRAINING IS REQUIRED, HOWEVER, THRUST BLOCKS ARE PERMITTED WHERE CONNECTIONS ARE MADE TO EXISTING WATERLINES OR WHERE THE USE OF MECHANICAL RESTRAINT IS NOT FEASIBLE.
- ALL UTILITIES SHOULD BE KEPT TEN (10') APART (PARALLEL) OR WHEN CROSSING 18" VERTICAL CLEARANCE (OUTSIDE EDGE OF PIPE TO OUTSIDE EDGE OF PIPE), UTILITIES OF THE SAME SERVICE ARE NOT REQUIRED TO PROVIDE 18" OF VERTICAL SEPARATION.
- MAINTAIN A MINIMUM OF 3'-0" COVER ON ALL WATERLINES.
- IN THE EVENT OF A VERTICAL CROSSING LESS THAN 18" BETWEEN WATERLINES AND SANITARY LINES (EXISTING AND PROPOSED), THE SANITARY LINE SHALL BE DUCTILE IRON PIPE WITH MECHANICAL JOINTS AT LEAST 10 FEET ON BOTH SIDES OF CROSSING. THE WATERLINE SHALL HAVE RESTRAINED JOINTS WITH APPROPRIATE THRUST BLOCKING AS REQUIRED MEETING ALL ANSI, AWWA, AND CITY OF LEE'S SUMMIT WATER UTILITIES REQUIREMENTS.
- LINES UNDERGROUND SHALL BE INSTALLED, INSPECTED AND APPROVED BEFORE BACKFILLING.
- ALL CONCRETE FOR ENCASUREMENTS SHALL HAVE A MINIMUM 28 DAY COMPRESSION STRENGTH AT 3000 PSI.
- REFER TO INTERIOR PLUMBING DRAWINGS FOR TIE-IN OF ALL UTILITIES.
- 1" REDUCED PRESSURE PRINCIPLE BACKFLOW PREVENTION ASSEMBLY INSTALLED ABOVE-GROUND WITHIN HEATED INSULATED ENCLOSURE PER LEE'S SUMMIT WATER UTILITIES REQUIREMENTS. ENCLOSURE TO INCLUDE DRAIN PORT(S) FOR DISCHARGE WATER PER LEE'S SUMMIT WATER UTILITIES REQUIREMENTS. SEE DETAIL ON UTILITY DETAILS SHEET.

ELECTRIC SERVICE NOTES:

- CONTRACTOR RESPONSIBLE FOR COORDINATING TIMING AND EXACT CONDUIT ROUTING WITH EVERGY.
- CONTRACTOR SHALL INSTALL TRANSFORMER PAD PER EVERGY STANDARDS AND SPECIFICATIONS.
- EVERGY SHALL INSTALL TRANSFORMER AND PRIMARY CONDUIT & CABLE FROM THE EXISTING UTILITY TO THE PROPOSED TRANSFORMER LOCATION.
- CONTRACTOR SHALL INSTALL CT ENCLOSURE, CT, METER BASE, AND METER AT ONCE EVERGY HAS INSTALLED THE TRANSFORMER.
- CONTRACTOR SHALL PROVIDE AND INSTALL SECONDARY CONDUIT & CABLE FOR SERVICE (SEE KEYNOTES FOR SIZE AND QUANTITY).
- CONTRACTOR IS RESPONSIBLE FOR INSPECTION OF PROPER ELECTRICAL SERVICE INSTALLATION UPON COMPLETION.



KEYNOTES

- (A) 6" GATE VALVE PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (B) SANITARY SEWER ENTRY (RE: ARCH FOR EXACT LOCATION)
- (C) 6" PVC SANITARY SEWER LATERAL (SEE PLAN FOR SLOPE (RE: ARCH FOR BUILDING ENTRY))
- (D) SEWER CLEAN-OUT (SEE DETAIL)
- (E) TRANSFORMER PAD AND METER (SEE ELECTRIC SERVICE NOTES THIS SHEET FOR LIST OF RESPONSIBILITIES)
- (F) UNDERGROUND TELEPHONE POINT OF CONNECTION AT POLE BY AT&T
- (G) UNDERGROUND ELECTRIC POINT OF CONNECTION AT POLE BY EVERGY LEE'S SUMMIT SERVICE CENTER
- (H) CONTRACTOR TO PROVIDE (4) 4" CONDUIT AND SERVICE FOR SECONDARY UNDERGROUND ELECTRIC UTILITY (RE: ARCH FOR BUILDING ENTRY)
- (I) 15" DOMESTIC WATERLINE PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (J) 6" WATERLINE FOR FIRE SERVICE PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (K) GAS SERVICE ENTRY AND METER (RE: ARCH)
- (L) WATER LINE BEND WITH THRUST BLOCK
- (M) TELEPHONE SERVICE ENTRY (RE: ARCH)
- (N) DOMESTIC WATERLINE ENTRY (RE: ARCH)
- (O) ELECTRIC SERVICE ENTRY (RE: ARCH FOR EXACT LOCATION)
- (P) (2) 6" PRIMARY ELECTRICAL CONDUIT & PULLWIRE (SEE ELECTRIC SERVICE NOTES THIS SHEET FOR LIST OF RESPONSIBILITIES)
- (Q) 1" LOW VOLTAGE ELECTRICAL CONDUIT FOR HEATED BFP ENCLOSURE
- (R) 1" RPZ BFP DEVICE IN ABOVE GROUND HEATED ENCLOSURE ADD 1" PRESSURE REDUCING VALVE BEFORE RPZ BY CONTRACTOR (SEE DETAIL)
- (X) 6" DOUBLE DETECTOR CHECK VALVE BACKFLOW PREVENTER FOR FIRE SERVICE LOCATED IN BELOW GROUND VAULT
- (Y) 1" IRRIGATION WATERLINE PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (Z) FIRE LINE ENTRY (RE: ARCH)
- (AA) 1" GAS SERVICE LINE
- (BB) GAS SERVICE POINT OF CONNECTION DOMINION ENERGY TO EXTEND SERVICE FROM MAIN
- (CC) 15" DOMESTIC WATERLINE METER AND VAULT PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (DD) 1" TELEPHONE CONDUIT & PULLWIRE BY CONTRACTOR - SERVICE BY AT&T
- (EE) 1" WATER LINE TO SITE IRRIGATION (BY OTHERS)
- (FF) SAMPLING WELL (RE: ARCH)
- (GG) BUILDING MOUNTED FDC PER FIRE MARSHALL STANDARDS TO BE LOCATED WITHIN 100 FEET OF THE PROPOSED FIRE HYDRANT. SEE DETAIL
- (HH) 6" X 6" OUT IN TEE, VALVE AND THRUST BLOCK TO EXISTING MAIN PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (II) 6" X 15" TAP WITH A CORPORATION STOP TO EXISTING MAIN PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (JJ) 15" X 1" TEE PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (KK) 6" X 6" TEE PER LEE'S SUMMIT WATER UTILITIES STANDARDS
- (LL) SANITARY SEWER EXT FROM DETAIL BAY (RE: ARCH FOR EXACT LOCATION)
- (MM) DOMESTIC WATER GATE VALVE
- (NN) SAND/CL SEPARATOR (RE: ARCH)
- (OO) SITE LIGHTING, TYPICAL (RE: PHOTOMETRIC)
- (PP) FIRE HYDRANT PER LEE'S SUMMIT WATER UTILITIES AND CITY OF LEE'S SUMMIT FIRE MARSHALL STANDARDS
- (QQ) SANITARY SEWER WYE CONNECTION SEE PLAN FOR INVERT
- (RR) KNOX BOX PER LEE'S SUMMIT WATER UTILITIES AND CITY OF LEE'S SUMMIT FIRE MARSHALL STANDARDS
- (SS) 6" C900 PVC WATER LINE PER CITY OF LEE'S SUMMIT WATER UTILITIES STANDARDS & SPECIFICATIONS
- (TT) 15" RPZ BFP DEVICE IN ABOVE GROUND HEATED ENCLOSURE ADD 1" PRESSURE REDUCING VALVE BEFORE RPZ
- (UU) 6" SANITARY SEWER SERVICE LINE WYE CONNECTION TO EXISTING 8" SANITARY SEWER MAIN SEE PLAN FOR INVERT

ACREAGE SUMMARY (IN ACRES)	
TOTAL PARCEL AREA	1.38
ON-SITE DISTURBED AREA	1.38
OFF-SITE DISTURBED AREA	0.55
TOTAL DISTURBED AREA	1.93
IMPERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY & INCLUDES GRAVEL)	0.48
PERVIOUS AREA PRE-CONSTRUCTION (ON-SITE ONLY)	0.90
IMPERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.90
PERVIOUS AREA POST-CONSTRUCTION (ON-SITE ONLY)	0.48
IMPERVIOUS AREA INCREASE (PRE VS POST) (ON-SITE ONLY)	0.42
IMPERVIOUS PERCENTAGE (AT POST-CONSTRUCTION STATUS)	65.2%

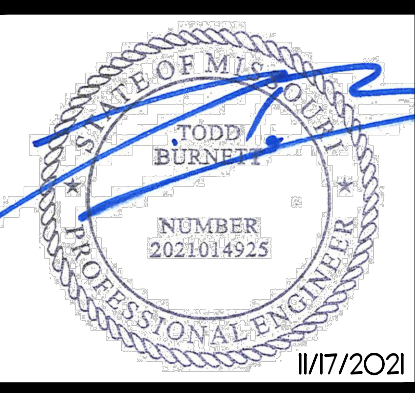
ALWAYS CALL 811 BEFORE YOU DIG

EVERY DIGGING JOB REQUIRES A CALL - EVEN SMALL PROJECTS LIKE PLANTING TREES OR SHRUBS. IF YOU HIT AN UNDERGROUND UTILITY LINE WHILE DIGGING, YOU CAN HARM YOURSELF OR THOSE AROUND YOU, DISRUPT SERVICE TO AN ENTIRE NEIGHBORHOOD AND POTENTIALLY BE RESPONSIBLE FOR FINES AND REPAIR COSTS.

SMART DIGGING MEANS CALLING 811 BEFORE EACH JOB. WHETHER YOU ARE A HOMEOWNER OR A PROFESSIONAL EXCAVATOR, ONE CALL TO 811 GETS YOUR UNDERGROUND UTILITY LINES MARKED FOR FREE.

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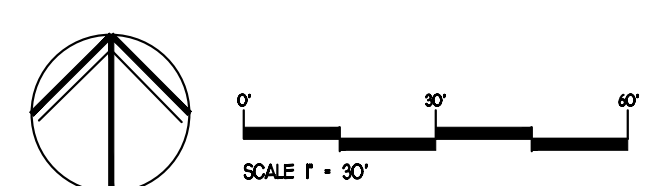
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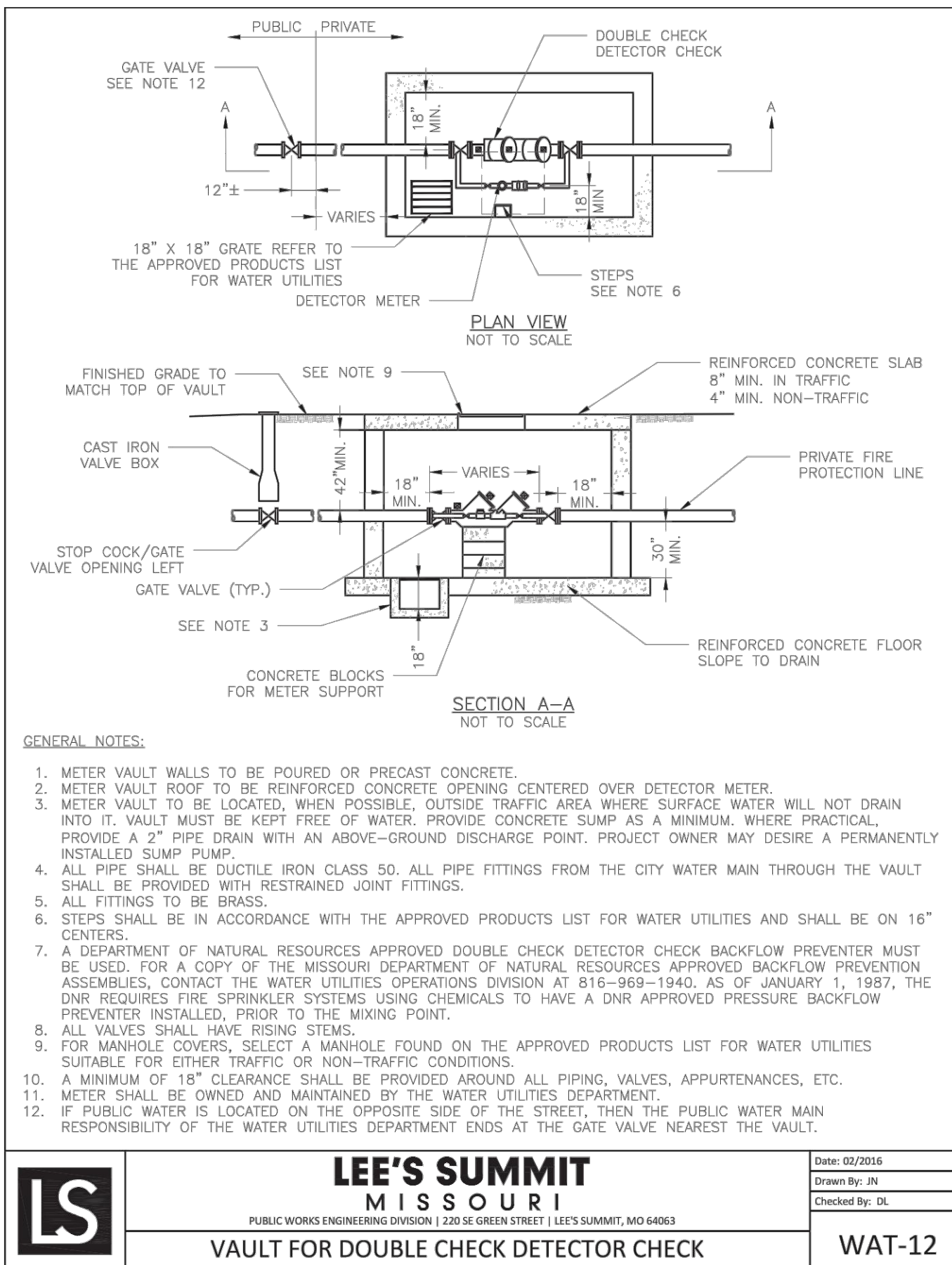
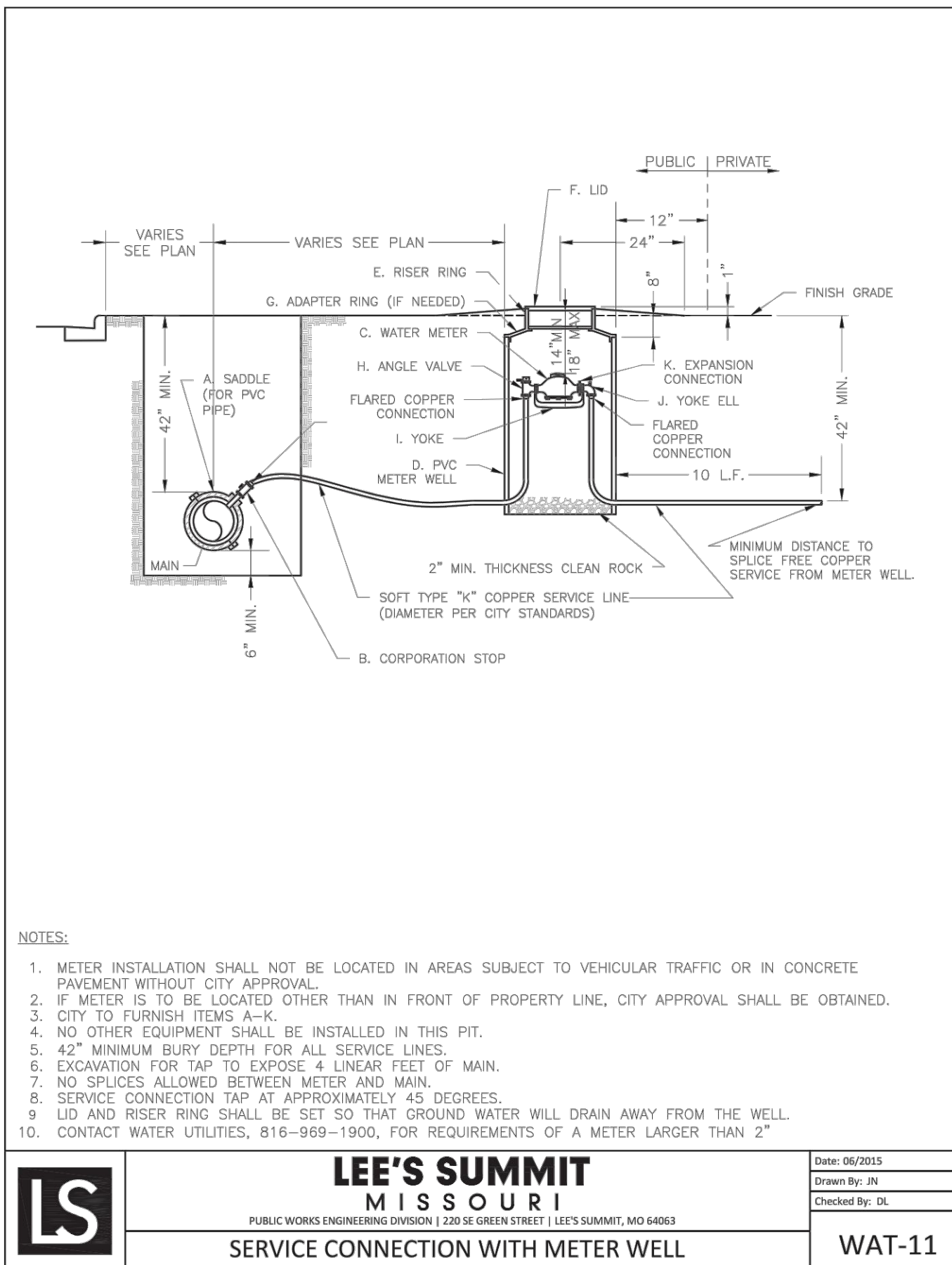
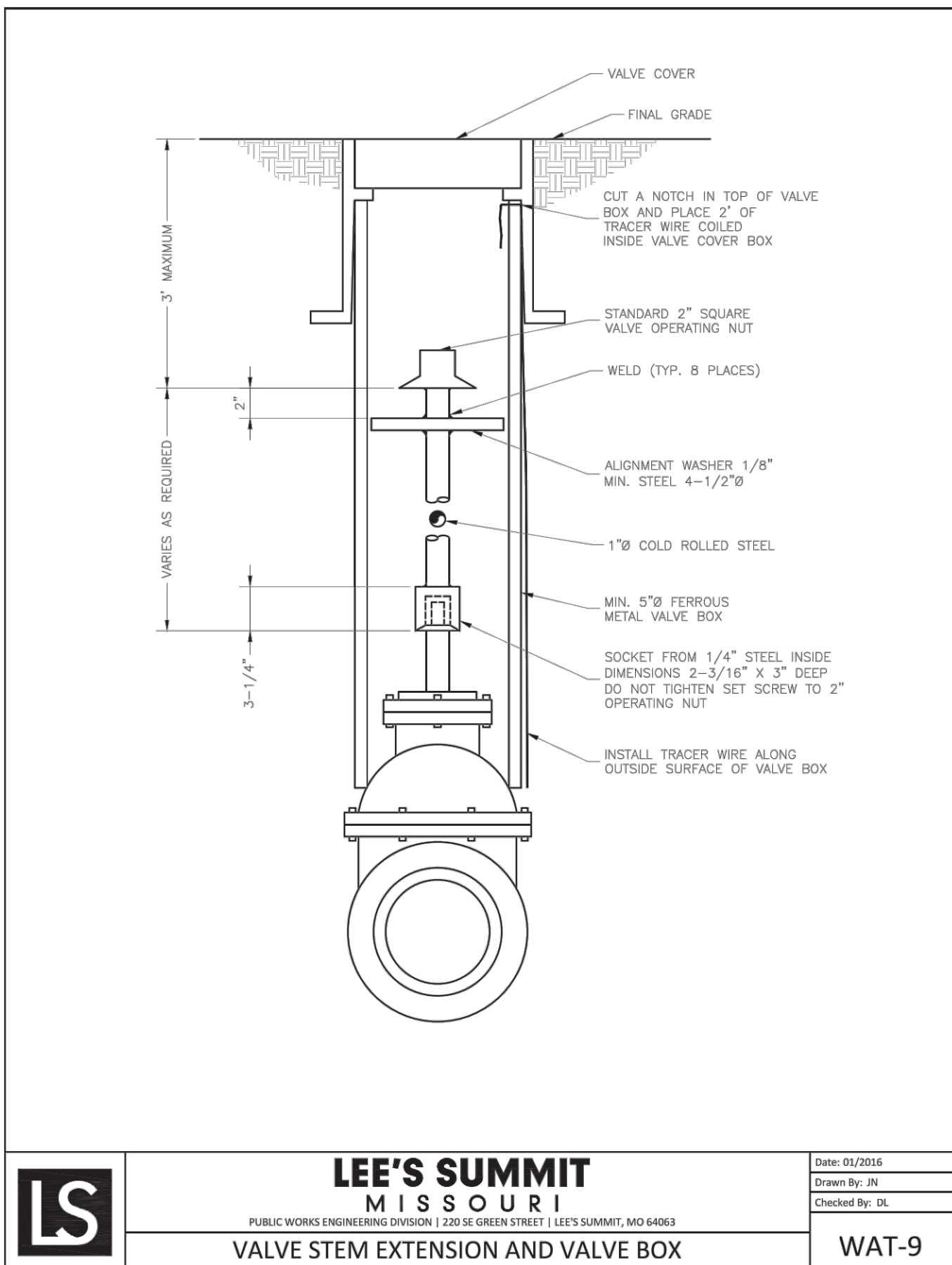
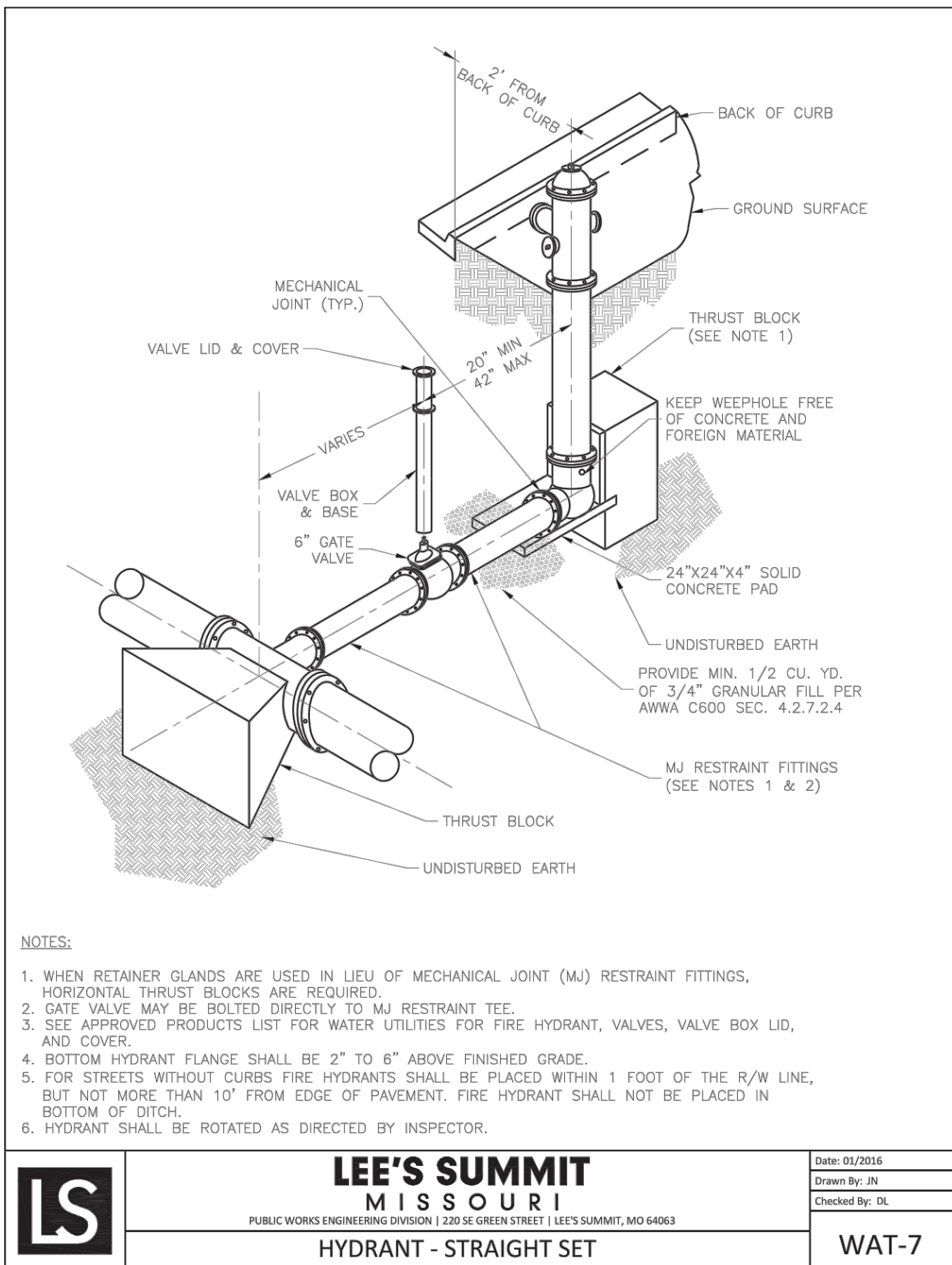
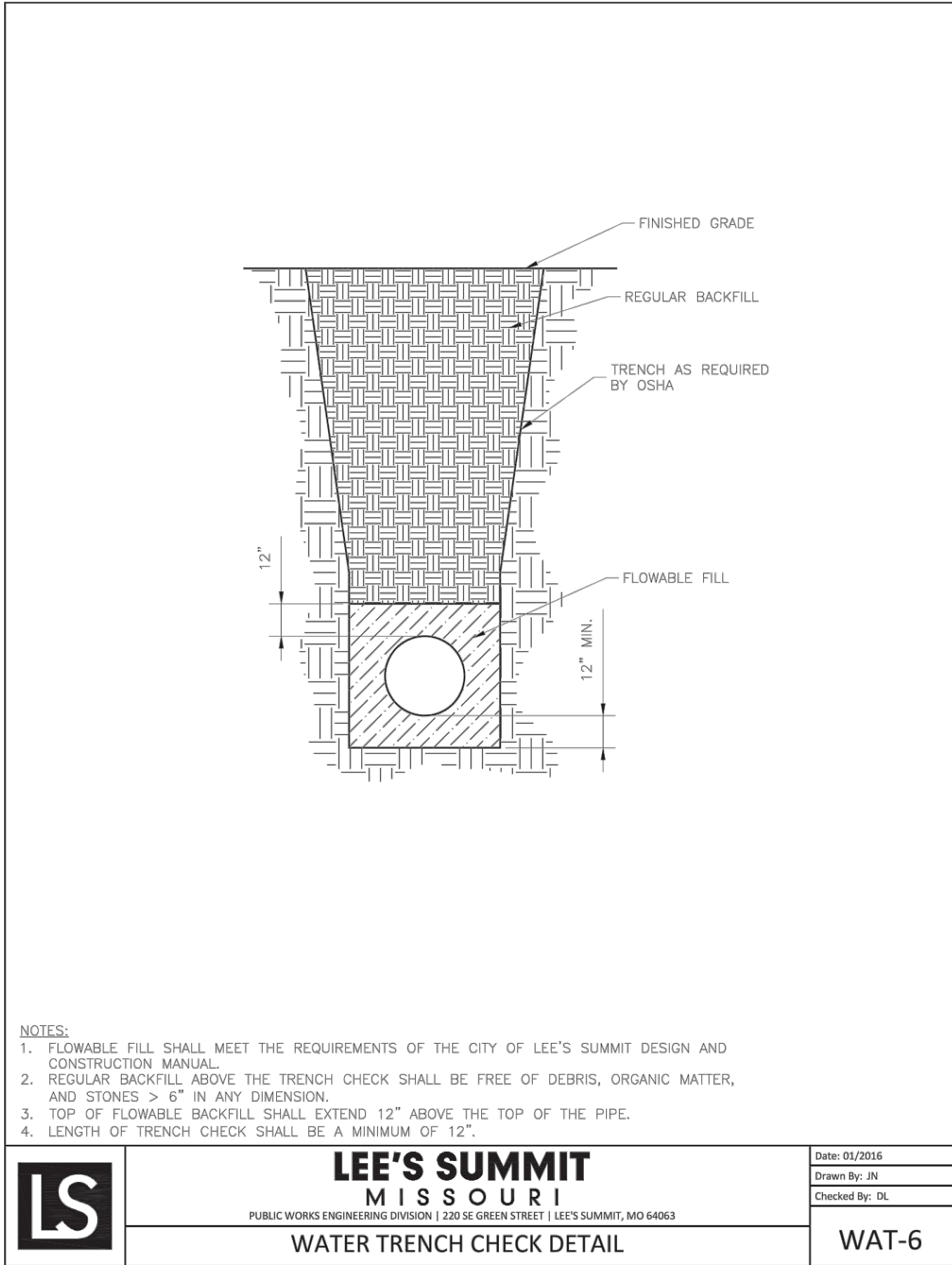
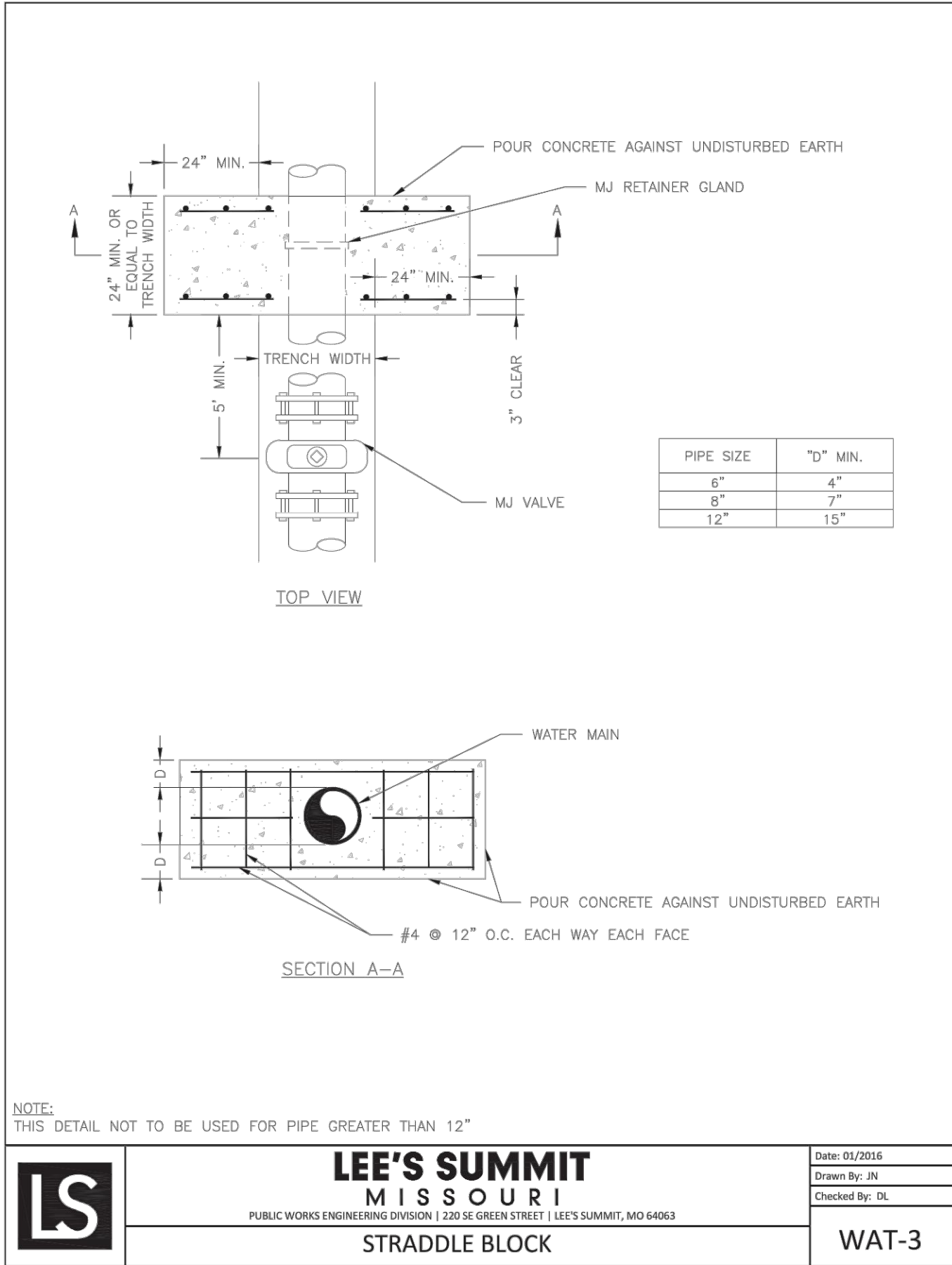
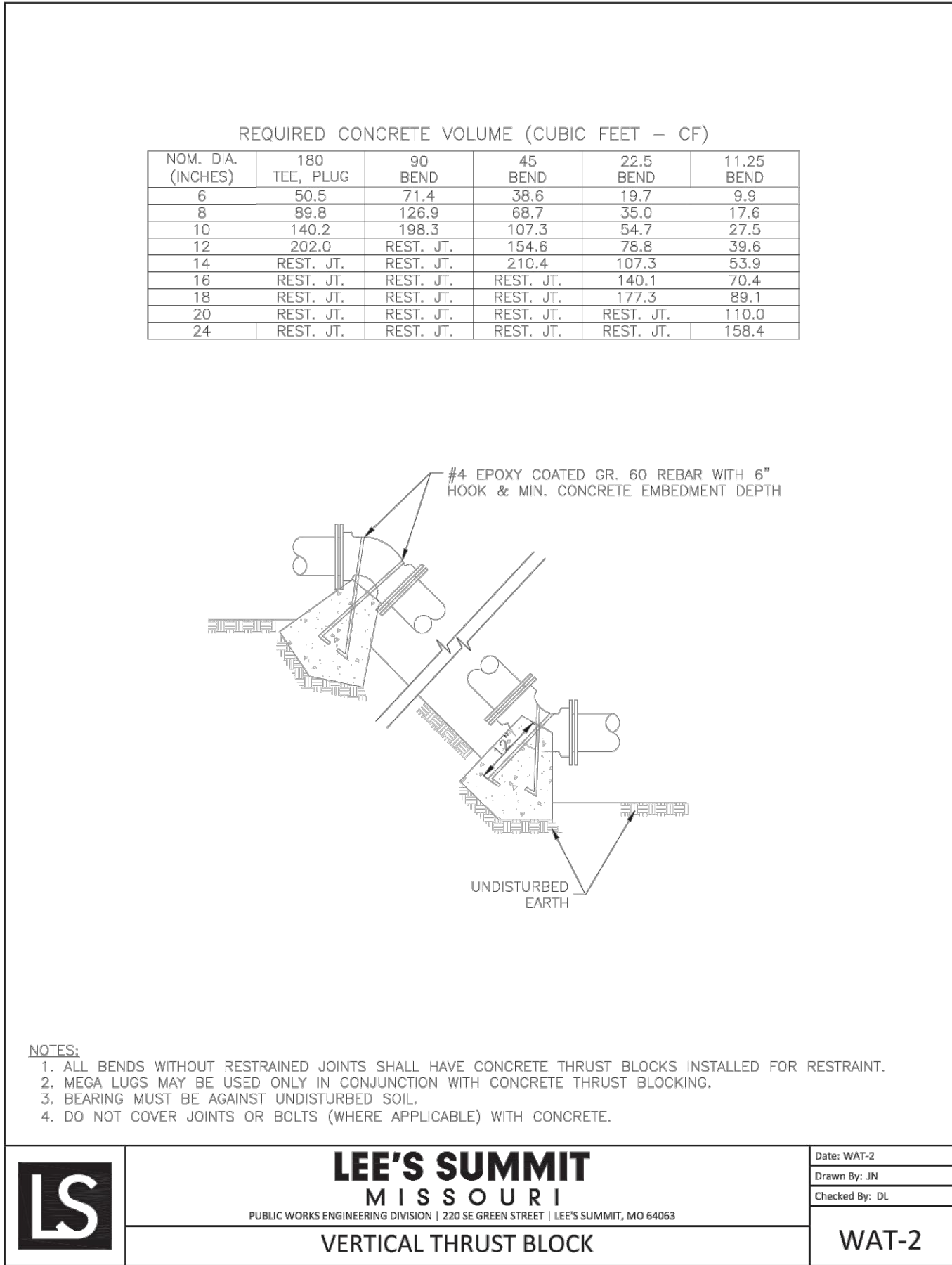
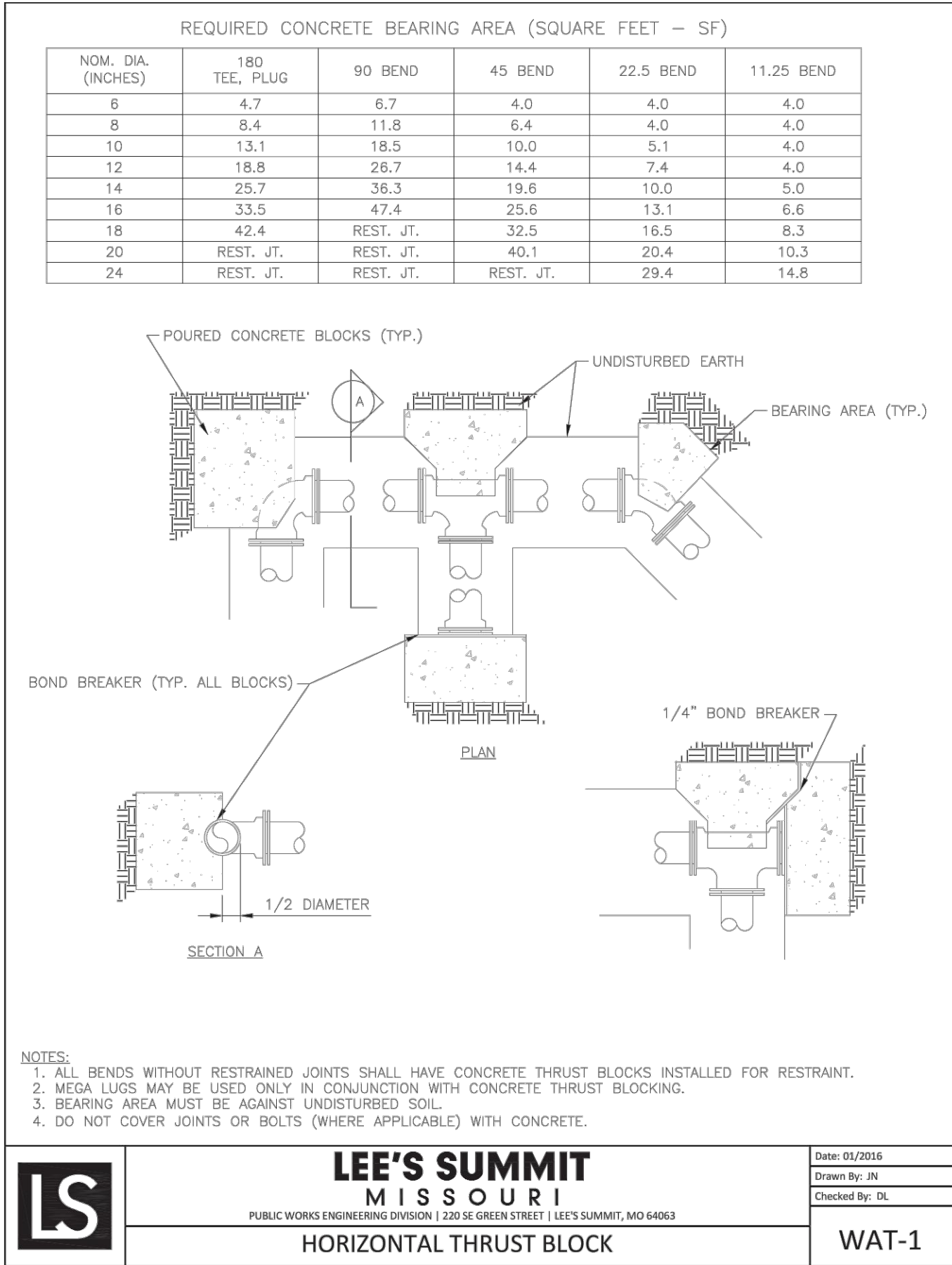


CALIBER COLLISION
710 SE BLUE PARKWAY
LEE'S SUMMIT, MO 64063
CROSS DEVELOPMENT, CG LEE'S SUMMIT, LLC
4388 MARSH RIDGE ROAD
CARROLLTON, TX 75010
TEL (214) 614-8252

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UTILITY PLAN





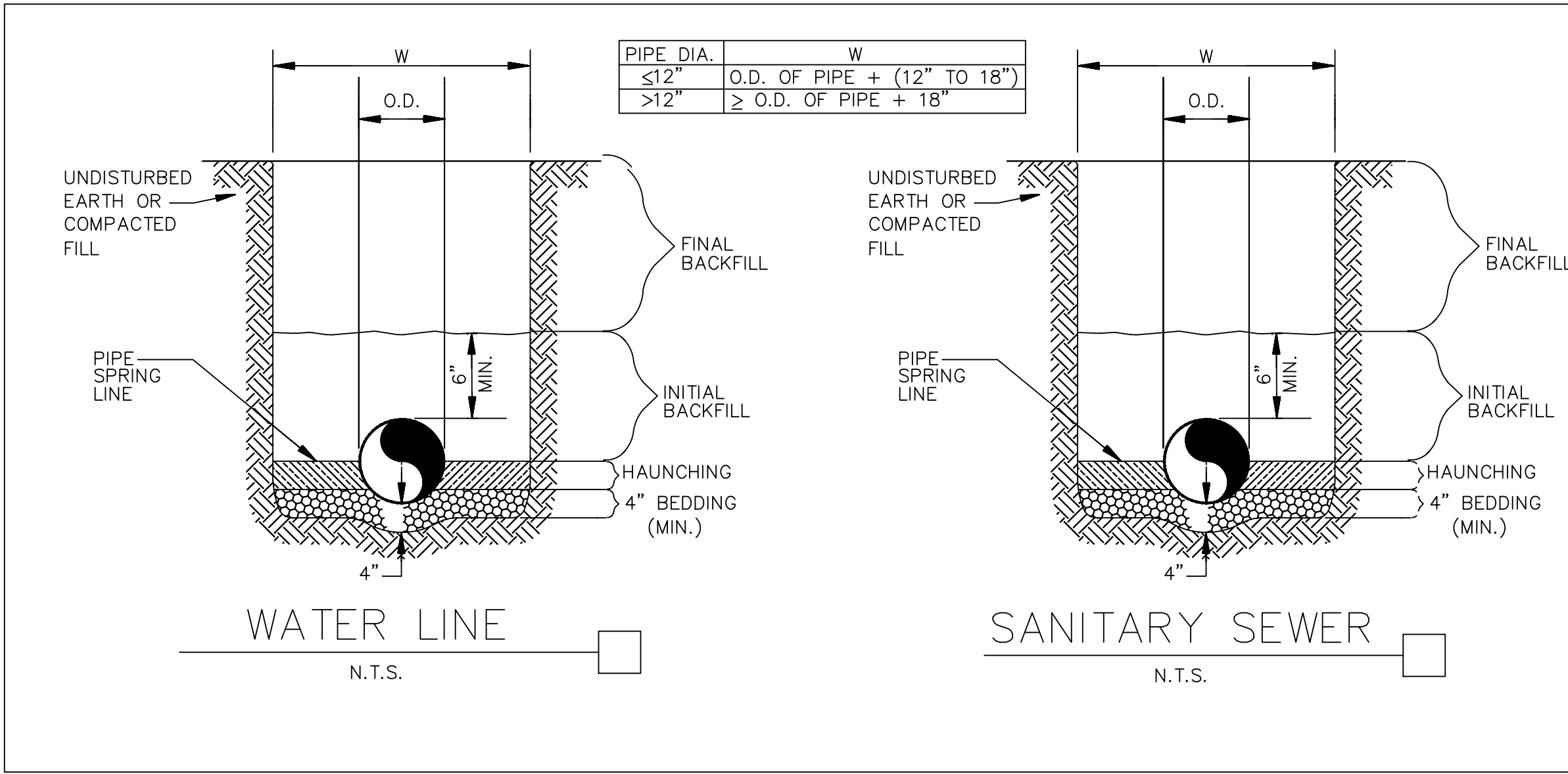
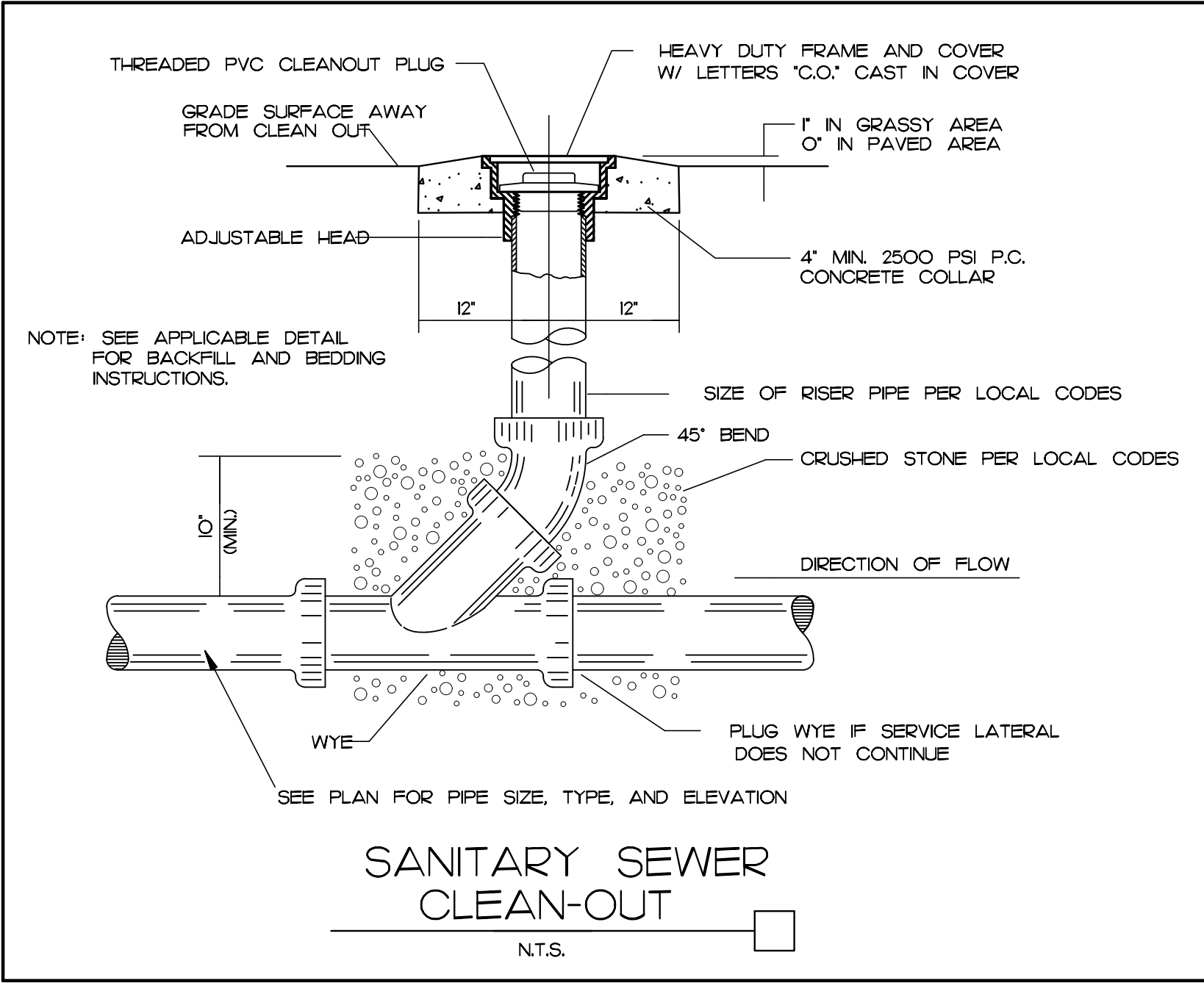
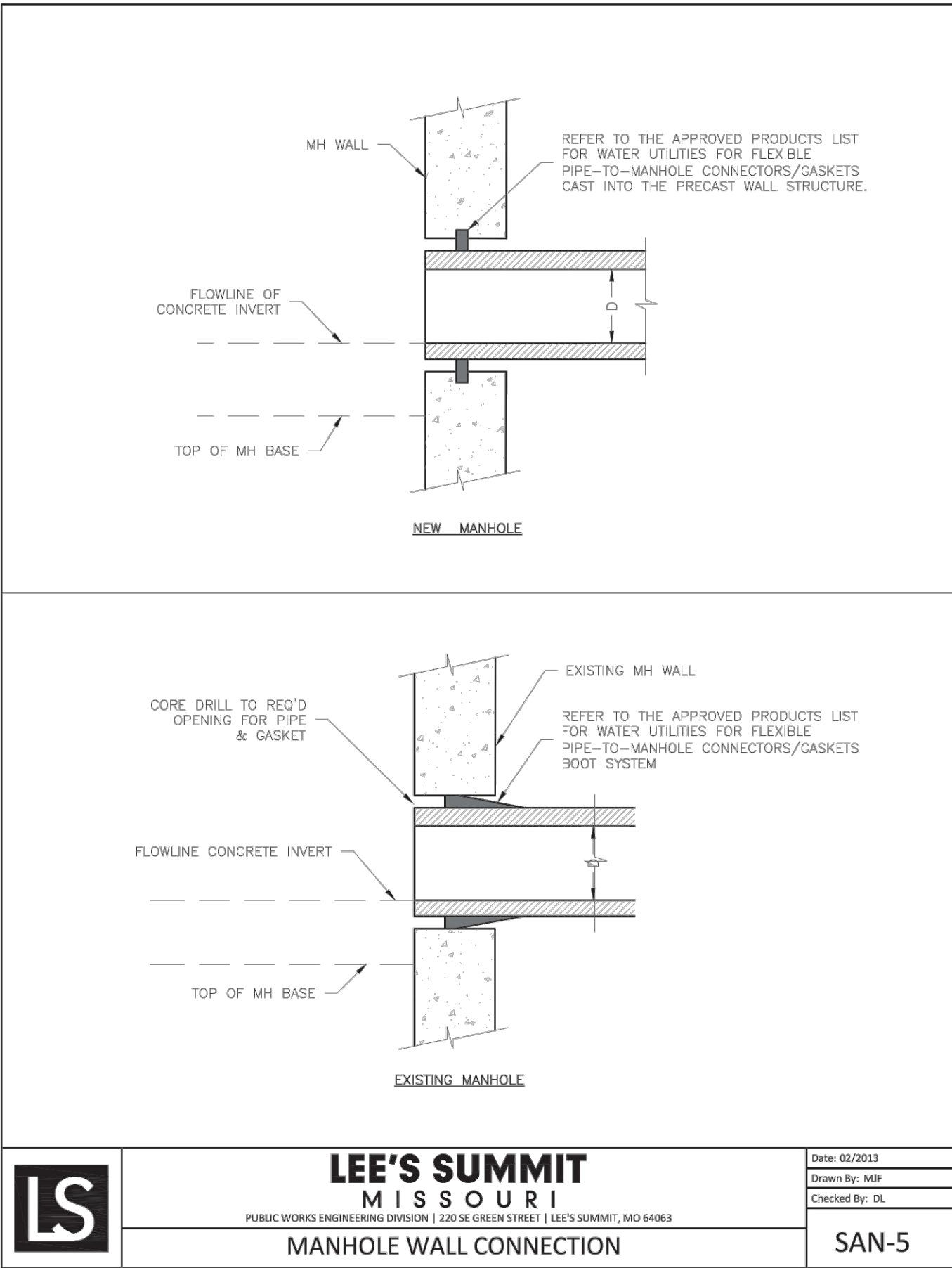
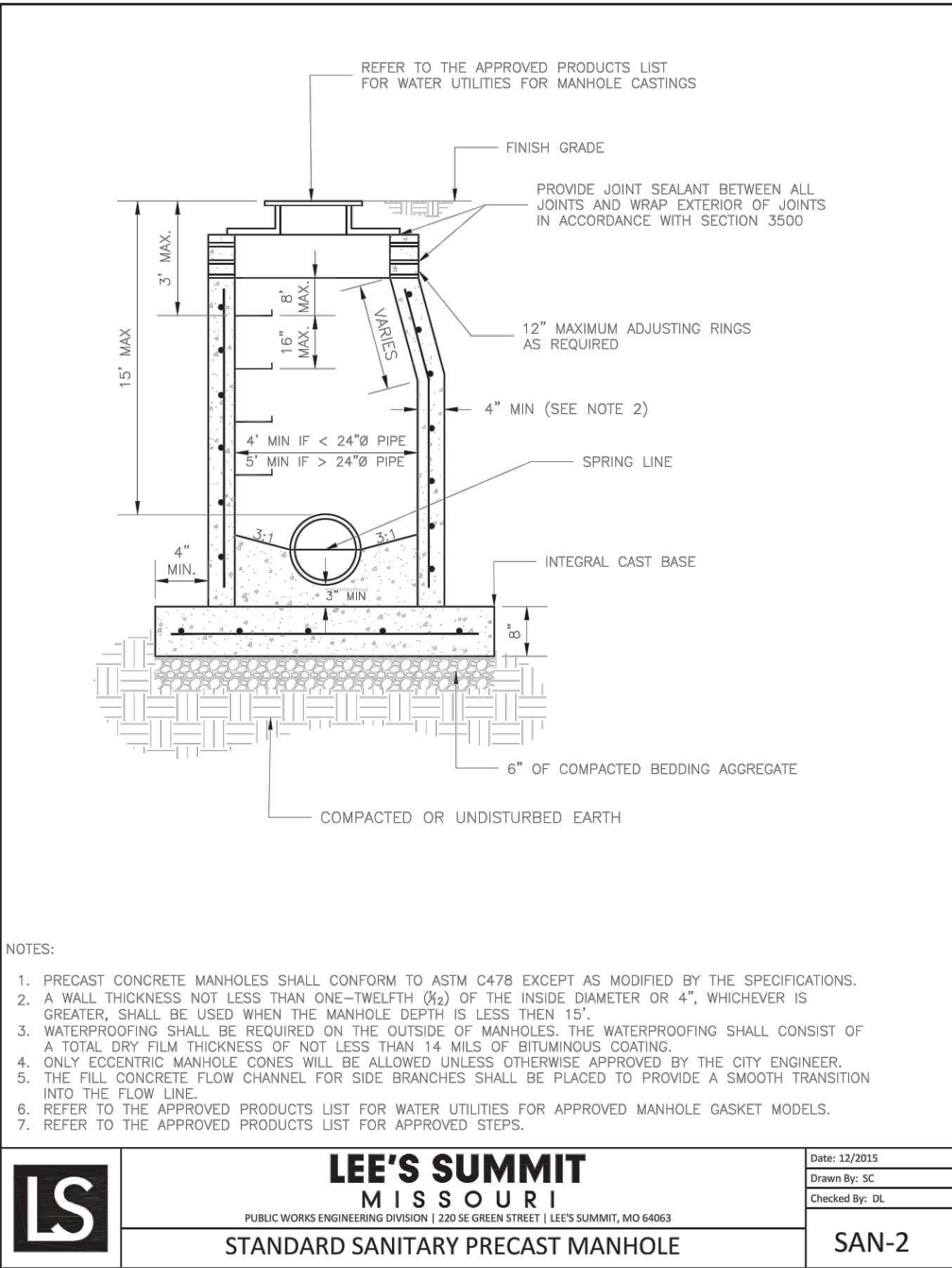
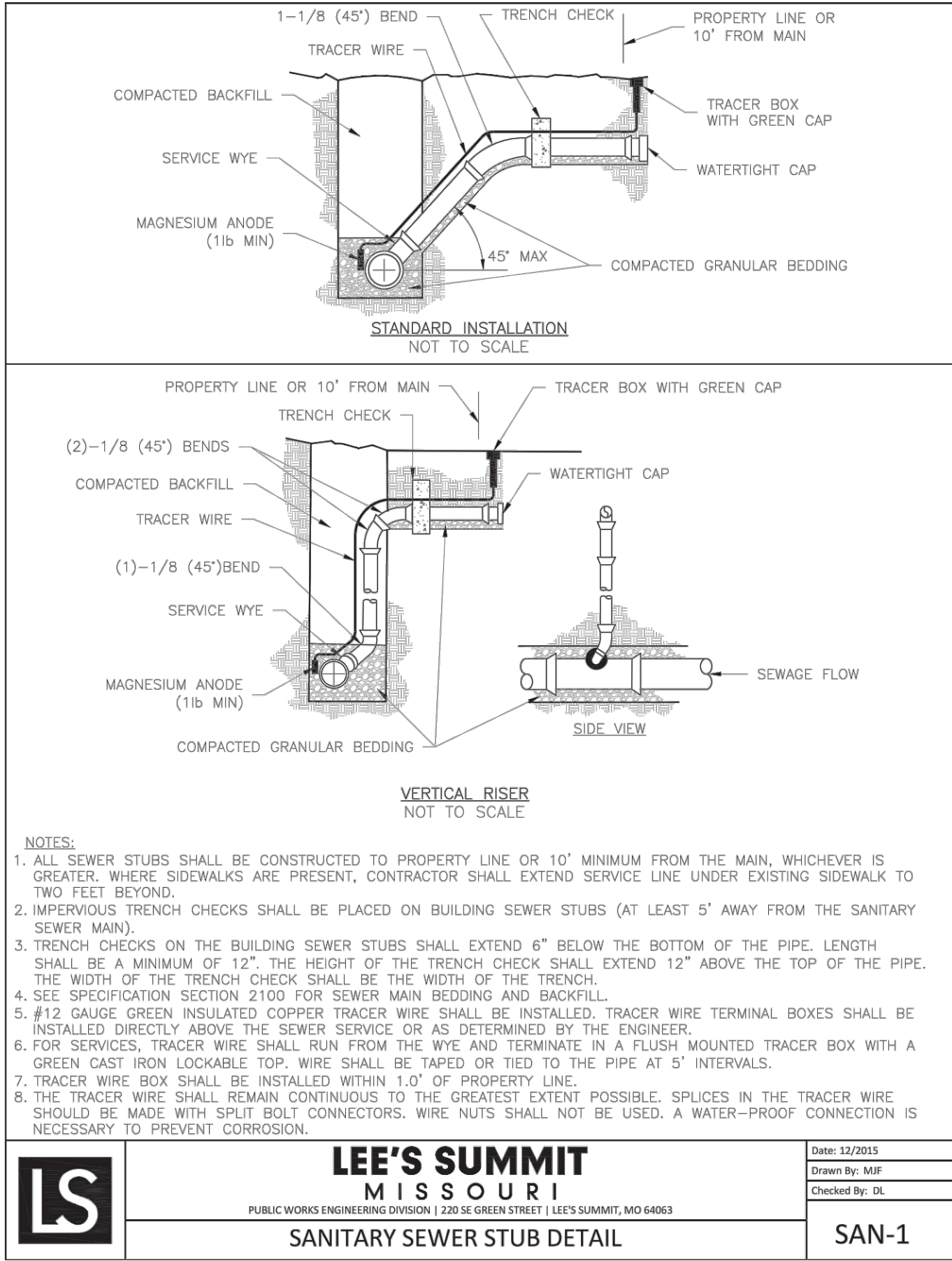
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STATE OF MISSOURI
JODD BURNETT
NUMBER 2021014925
PROFESSIONAL ENGINEER
1/17/2021

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GENERAL NOTES

1. BEDDING SHALL BE CLASS I-A WORKED BY HAND. IF GROUNDWATER IS ANTICIPATED, THEN BEDDING SHALL BE CLASS I-B COMPACTED TO 85% STANDARD PROCTOR.
2. HAUNCHING SHALL BE WORKED AROUND THE PIPE BY HAND TO ELIMINATE VOIDS AND SHALL BE CLASS I-A OR CLASS I-B OR CLASS II COMPACTED TO 95% PROCTOR.
3. INITIAL BACKFILL SHALL BE CLASS I-A WORKED BY HAND, OR CLASS I-B OR CLASS II COMPACTED TO 95% STANDARD PROCTOR.
4. INITIAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS III COMPACTED TO 90% STANDARD PROCTOR.
5. FINAL BACKFILL SHALL BE PER PROJECT SPECIFICATIONS AND PER THE PROJECT'S GEOTECHNICAL REPORT OF RECORD.
6. ALL MATERIALS ARE CLASSIFIED IN ACCORDANCE WITH ASTM D 2321-89.
7. ALL MATERIALS SHALL BE INSTALLED IN MAXIMUM 8" LOOSE LIFTS IN ACCORDANCE WITH ASTM D 698. CLASS III AND IV-A MATERIALS SHALL BE COMPACTED NEAR OPTIMUM MOISTURE CONTENT.
8. FILL SALVAGED FROM EXCAVATION SHALL BE FREE OF DEBRIS, ORGANICS AND ROCKS LARGER THAN 3".
9. ALL TRENCH EXCAVATIONS SHALL BE SLOPED, SHORED, SHEETED, BRACED, OR OTHERWISE SUPPORTED IN COMPLIANCE WITH OSHA REGULATIONS AND LOCAL ORDINANCES. (SEE SPECIFICATIONS)

UTILITY TRENCH AND BEDDING
N.T.S.

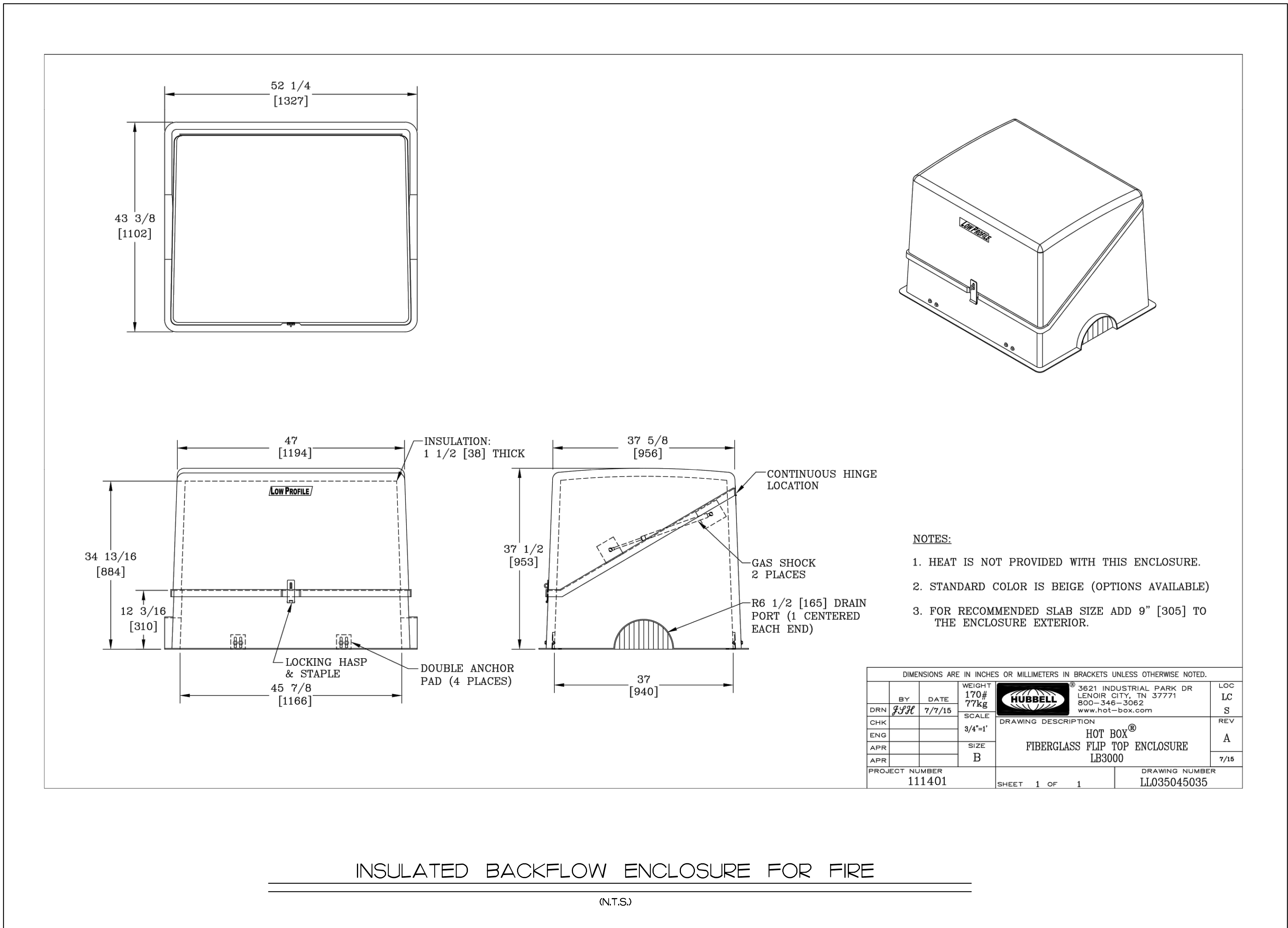
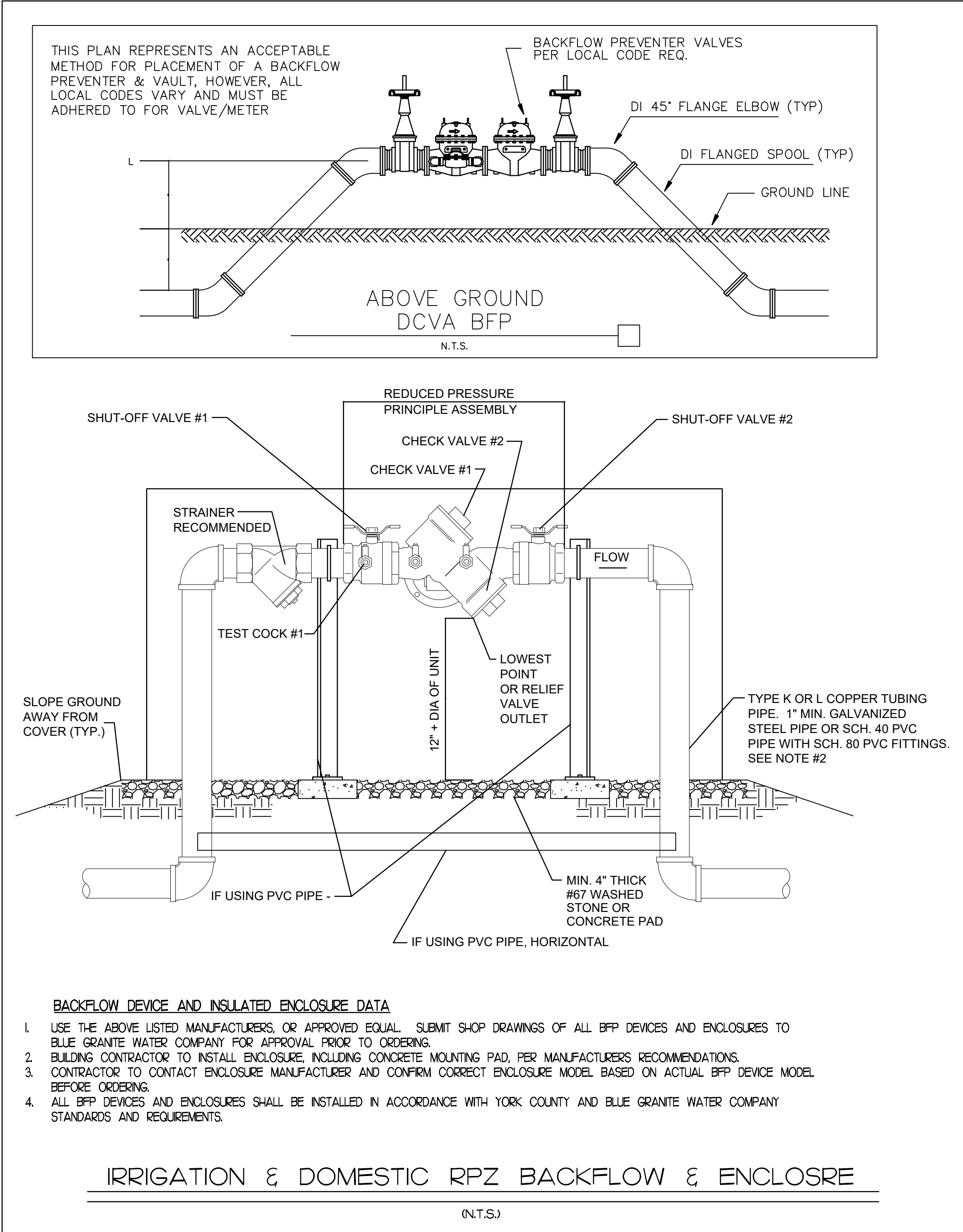
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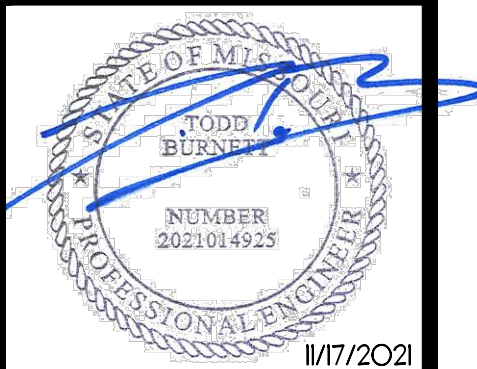
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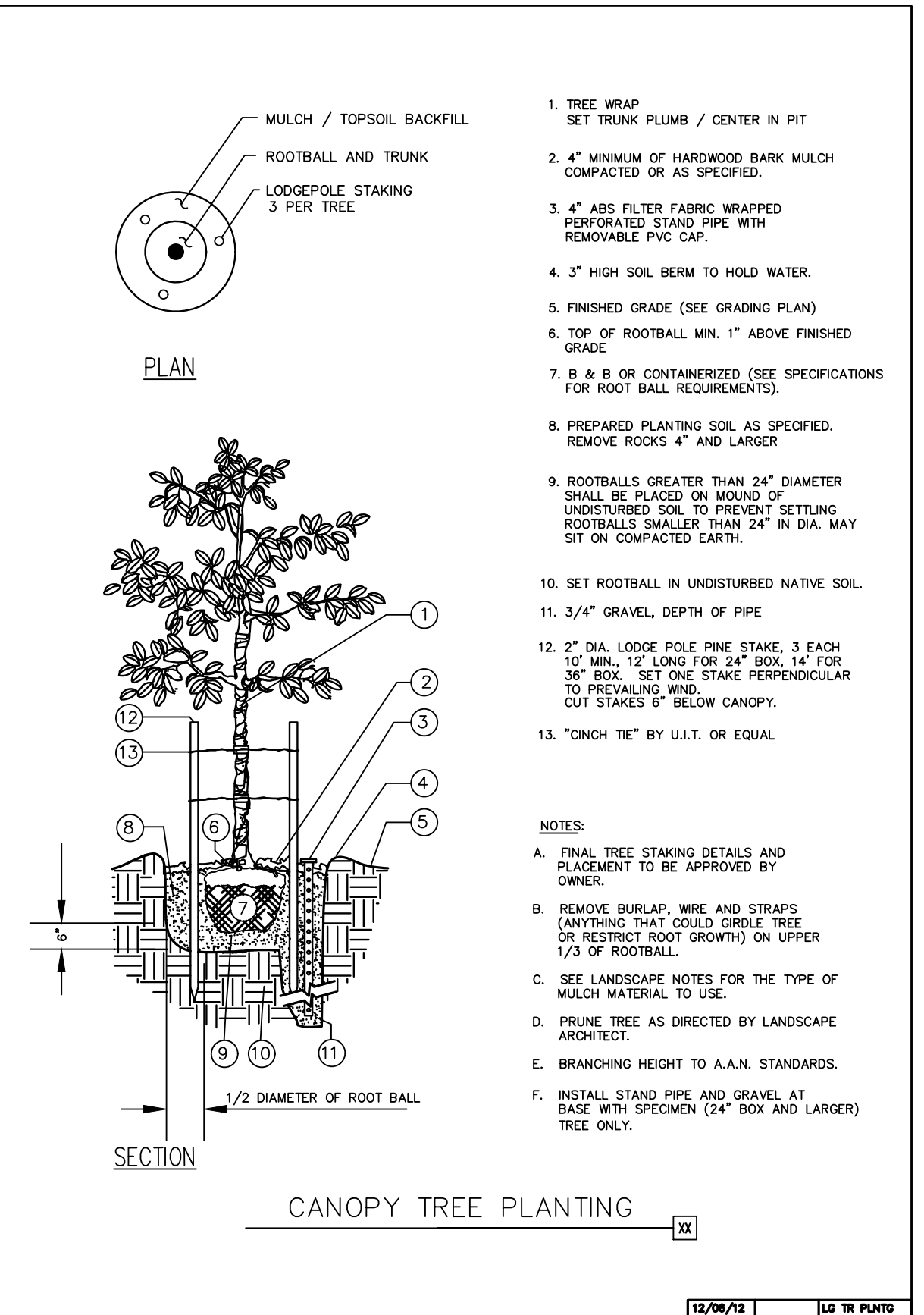
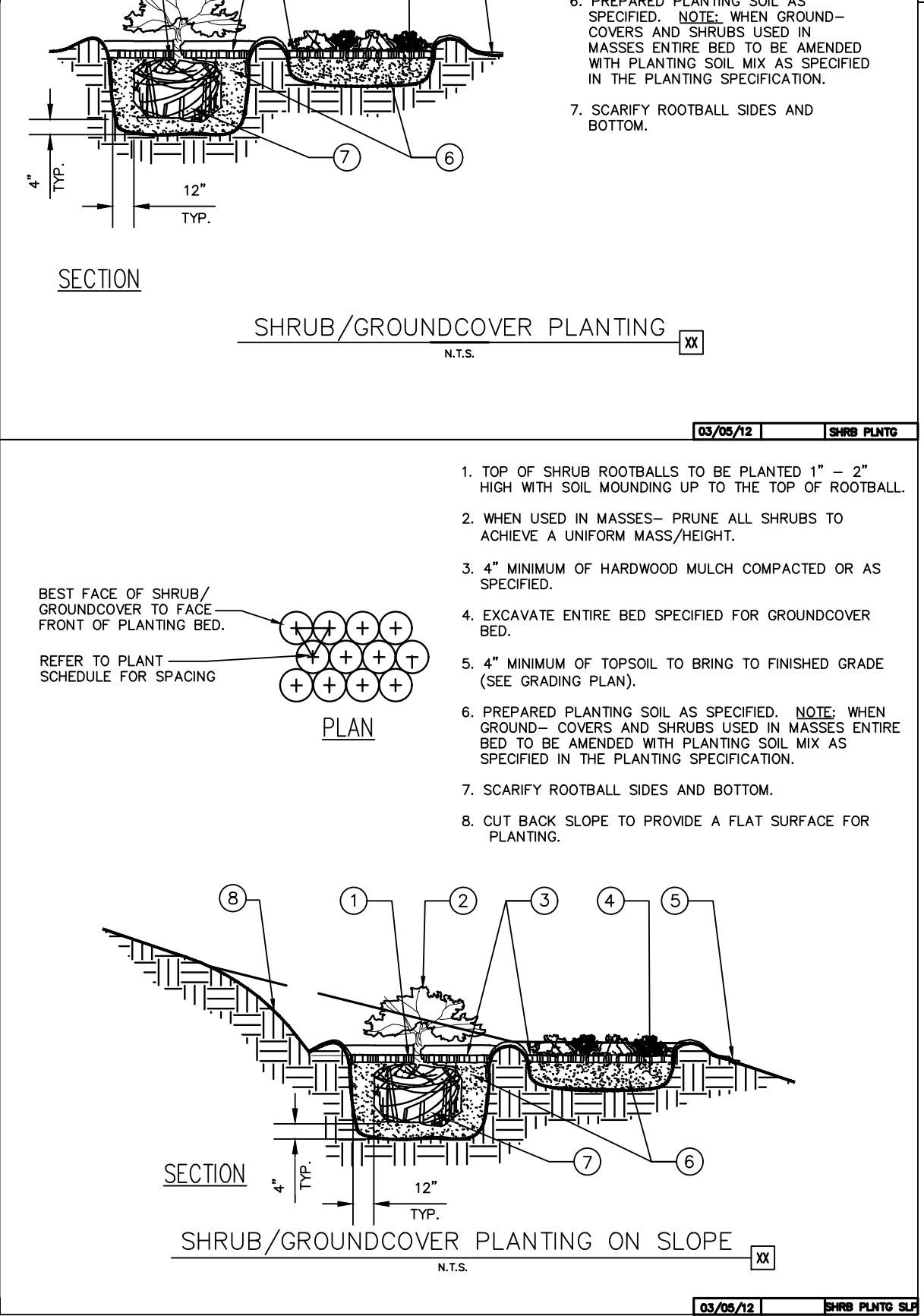
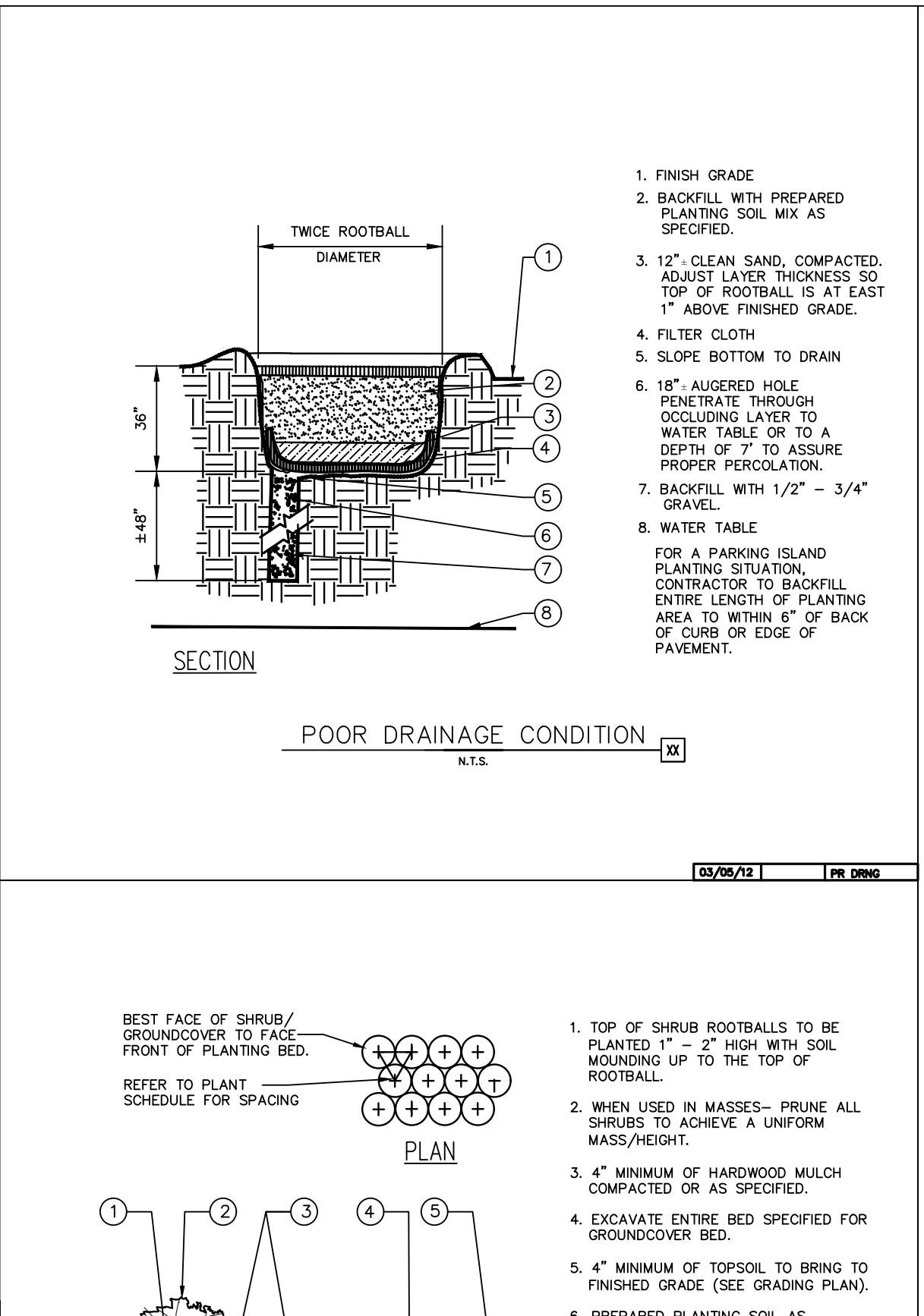
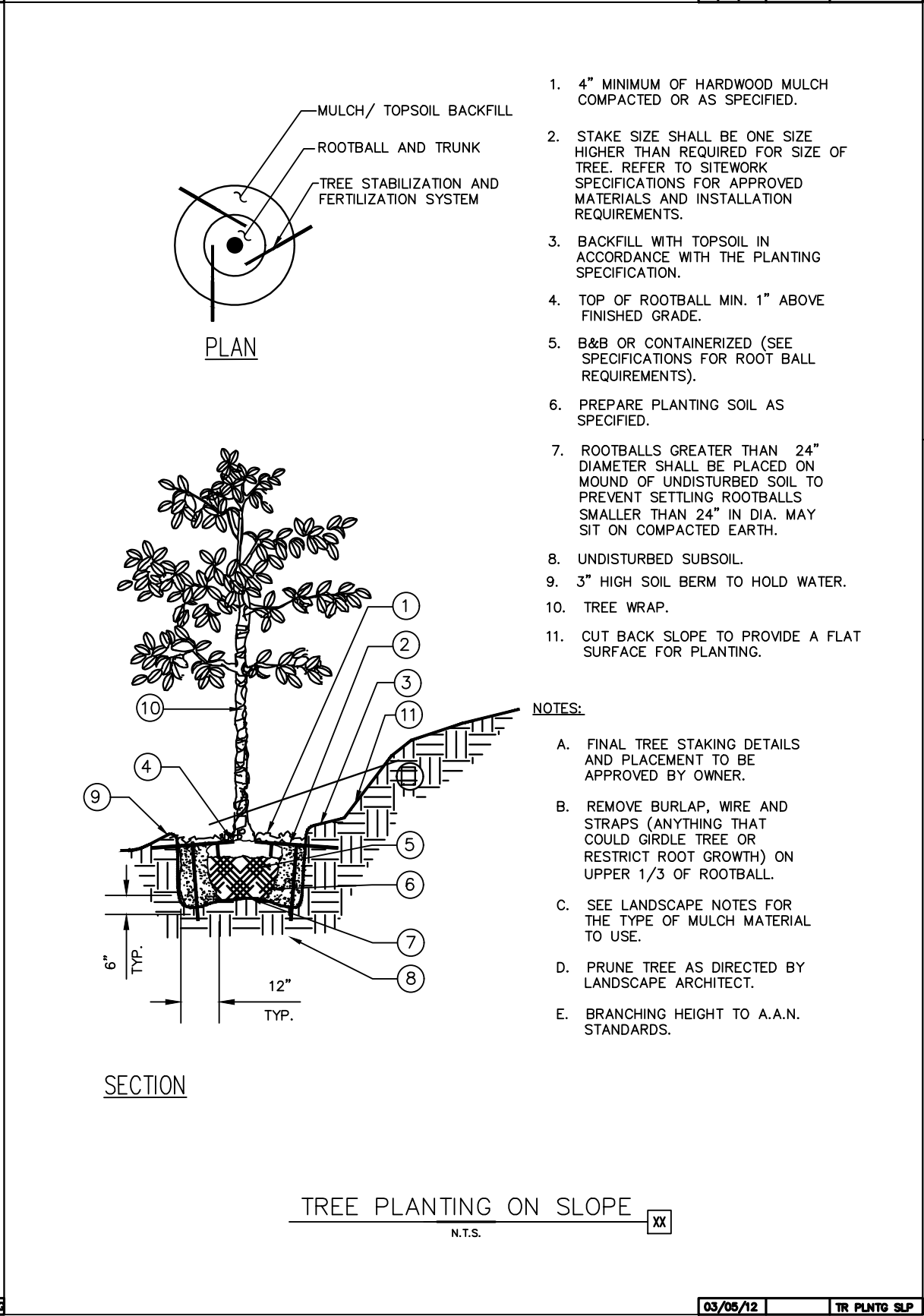
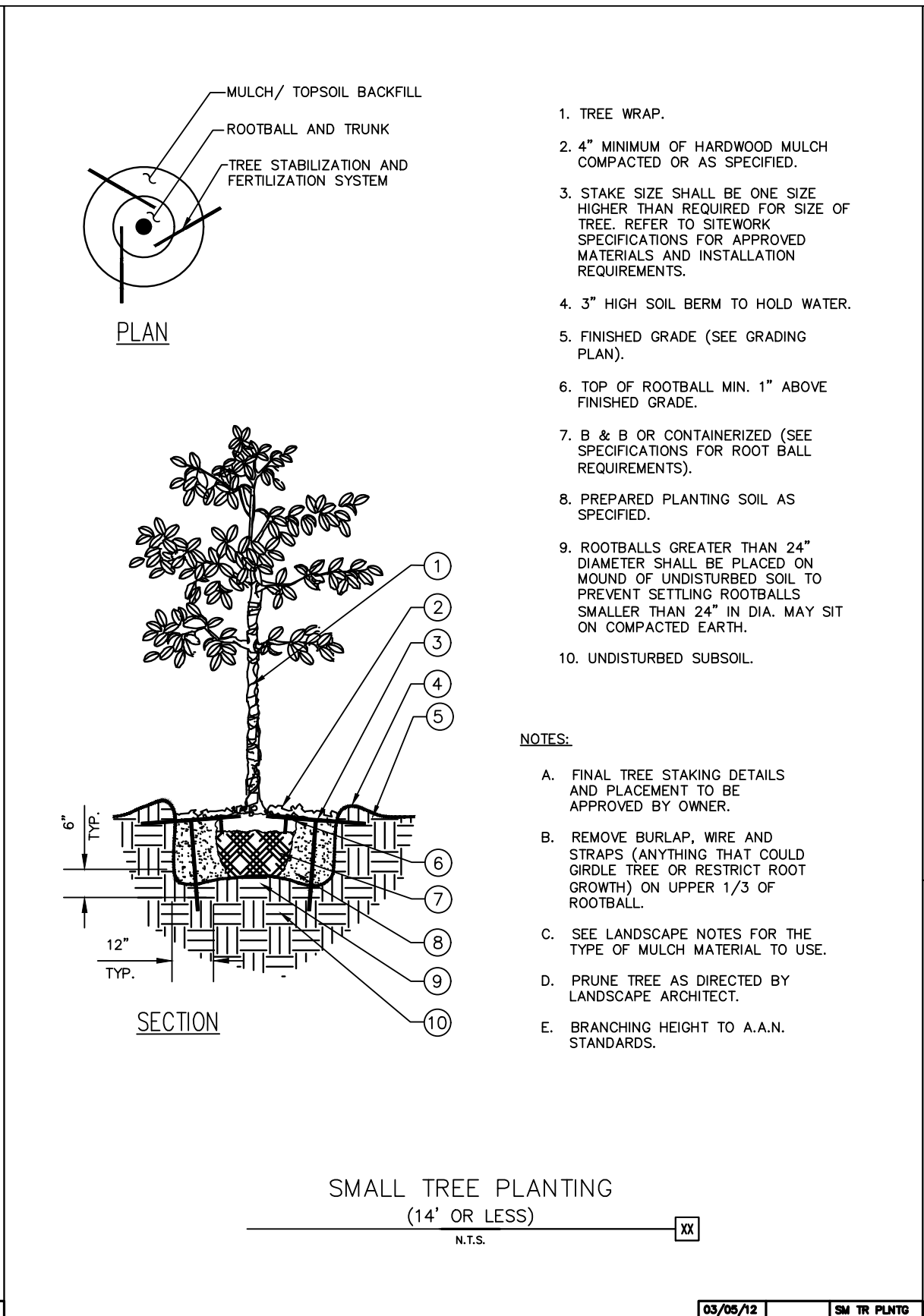
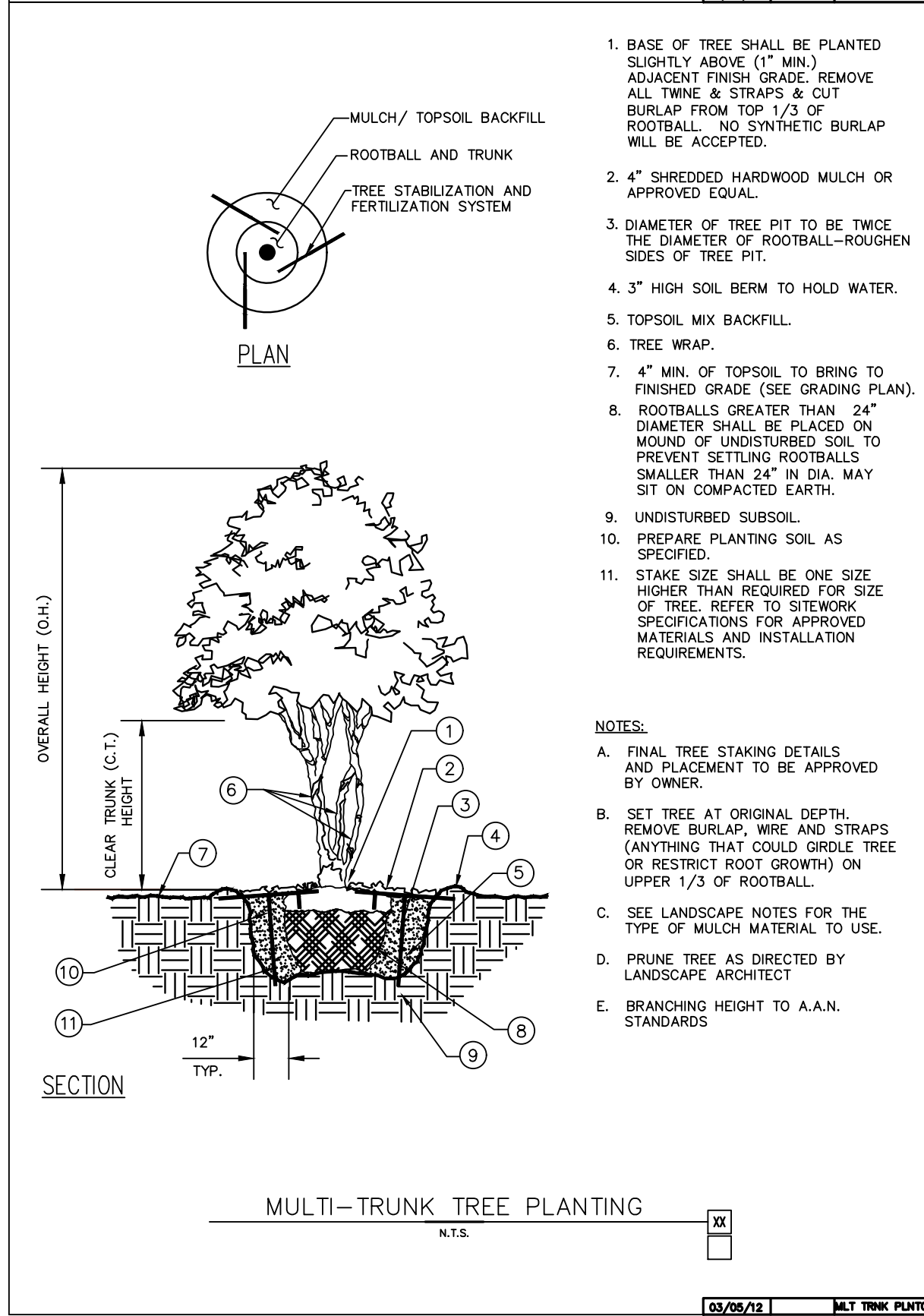
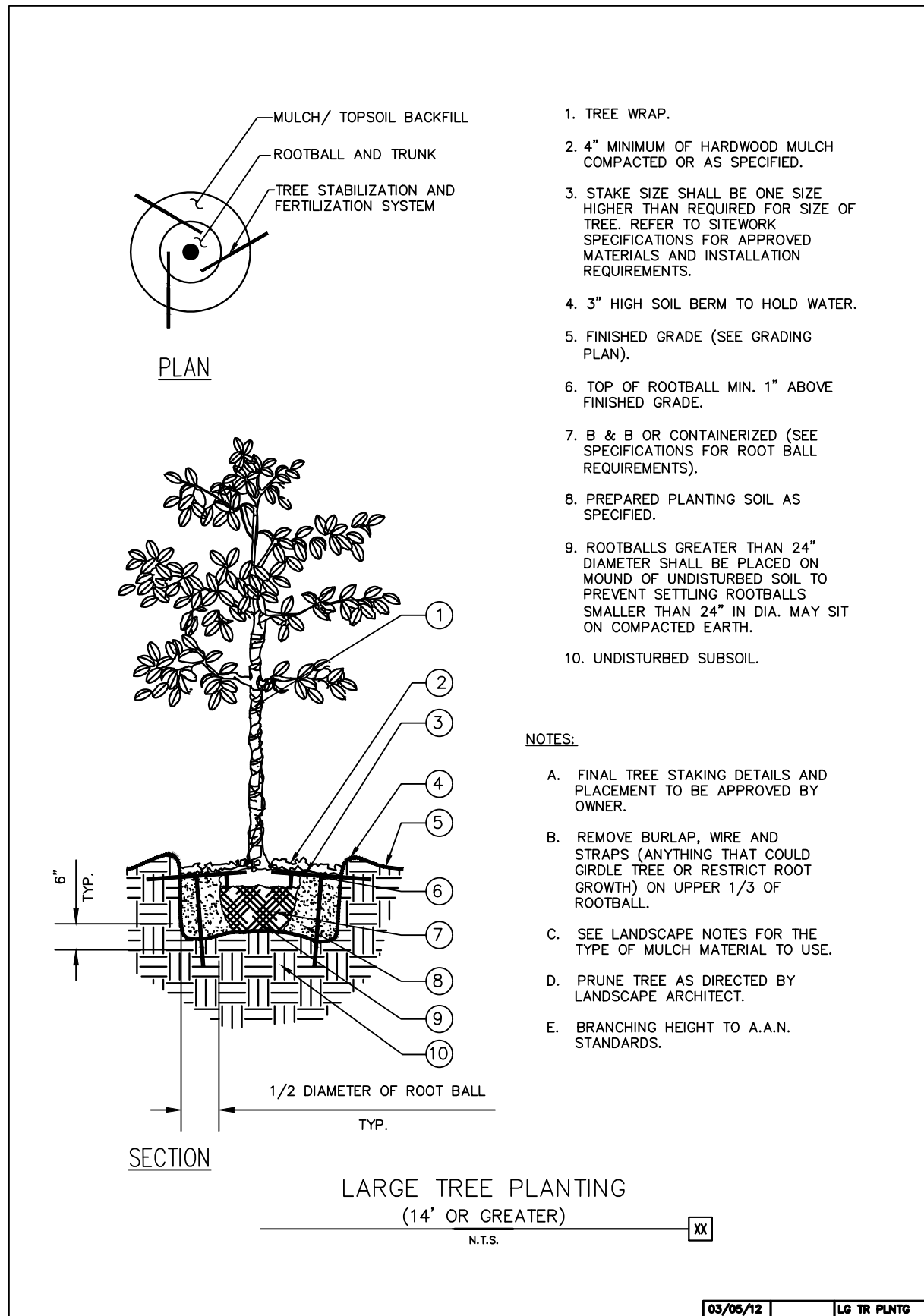
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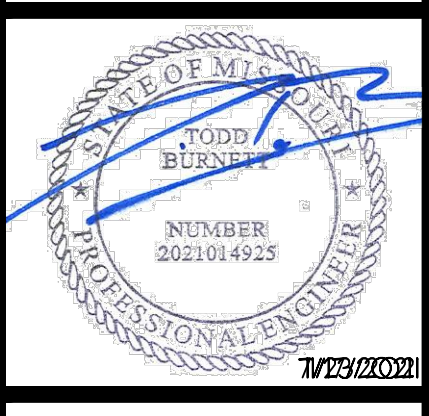
LANDSCAPE ARCHITECT INSPECTION REQUIREMENTS & NOTES:

ALL PLANT MATERIAL MUST BE INSPECTED AND ACCEPTED BY THE LANDSCAPE ARCHITECT FOR THIS PROJECT (FREELAND AND KAUFFMAN, INC.) IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS. THIS INCLUDES THE FOLLOWING PROCEDURE:

- THE GENERAL CONTRACTOR SHALL ARRANGE AN INSPECTION OF ALL TREES AT THE APPROPRIATE NURSERY (IES) WHICH WILL SUPPLY THEM. ALL TREES FOR USE ON THIS PROJECT SHALL BE IDENTIFIED AND FLAGGED AT THE NURSERY. THE LANDSCAPE CONTRACTOR SHALL BE PRESENT.
- AT THE SOLE DISCRETION OF THE LANDSCAPE ARCHITECT, PHOTOS OF SELECT TREES MAY BE ACCEPTED IN PLACE OF A PHYSICAL INSPECTION. IF PHOTOS ARE ACCEPTED, A SEPARATE PHOTO OF EACH PROPOSED TREE MUST BE PROVIDED.
- THE LANDSCAPE ARCHITECT SHALL BE CONTACTED TO INSPECT THE PLACEMENT OF TOPSOIL WITHIN ALL PARKING LOT ISLANDS AND PLANTING AREAS, PRIOR TO PLANTING OF ANY LANDSCAPE MATERIAL.
- THE LANDSCAPE ARCHITECT SHALL INSPECT PLANT MATERIAL UPON DELIVERY TO THE SITE, AND PRIOR TO PLANTING, FOR ACCEPTANCE. CONTRACTOR SHALL PROVIDE A SPECIFIC DELIVERY DATE OF ALL MATERIAL, AND SHALL MAKE EFFORTS TO INSURE THAT MATERIAL FROM SEPARATE NURSERIES IS DELIVERED AT THE SAME TIME.
- THE LANDSCAPE ARCHITECT SHALL PERFORM A SUBSTANTIAL COMPLETION INSPECTIONS. CONTRACTOR SHALL PROVIDE A SCHEDULE OF PLANT INSTALLATION.
- THE LANDSCAPE ARCHITECT SHALL PERFORM A FINAL LANDSCAPE INSPECTION WITHIN 30 DAYS OF THE CONTRACTOR'S SUBSTANTIAL COMPLETION DATE. CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT OF THE SUBSTANTIAL COMPLETION DATE.
- ALL REQUIRED LANDSCAPING SHALL MEET THE MINIMUM PLANTING STANDARDS AND BE MAINTAINED IN ACCORDANCE WITH SECTION 7-II-3 (F) OF THE CITY OF ASHEVILLE'S UNIFIED DEVELOPMENT ORDINANCES. A DIVERSE PLANT LIST SHALL BE PROVIDED (AS PER THIS PLAN) SO THAT NO ONE SPECIES IS OVER PLANTED AND TO ELIMINATE WIDESPREAD DISEASE BETWEEN THE LIKE SPECIES.

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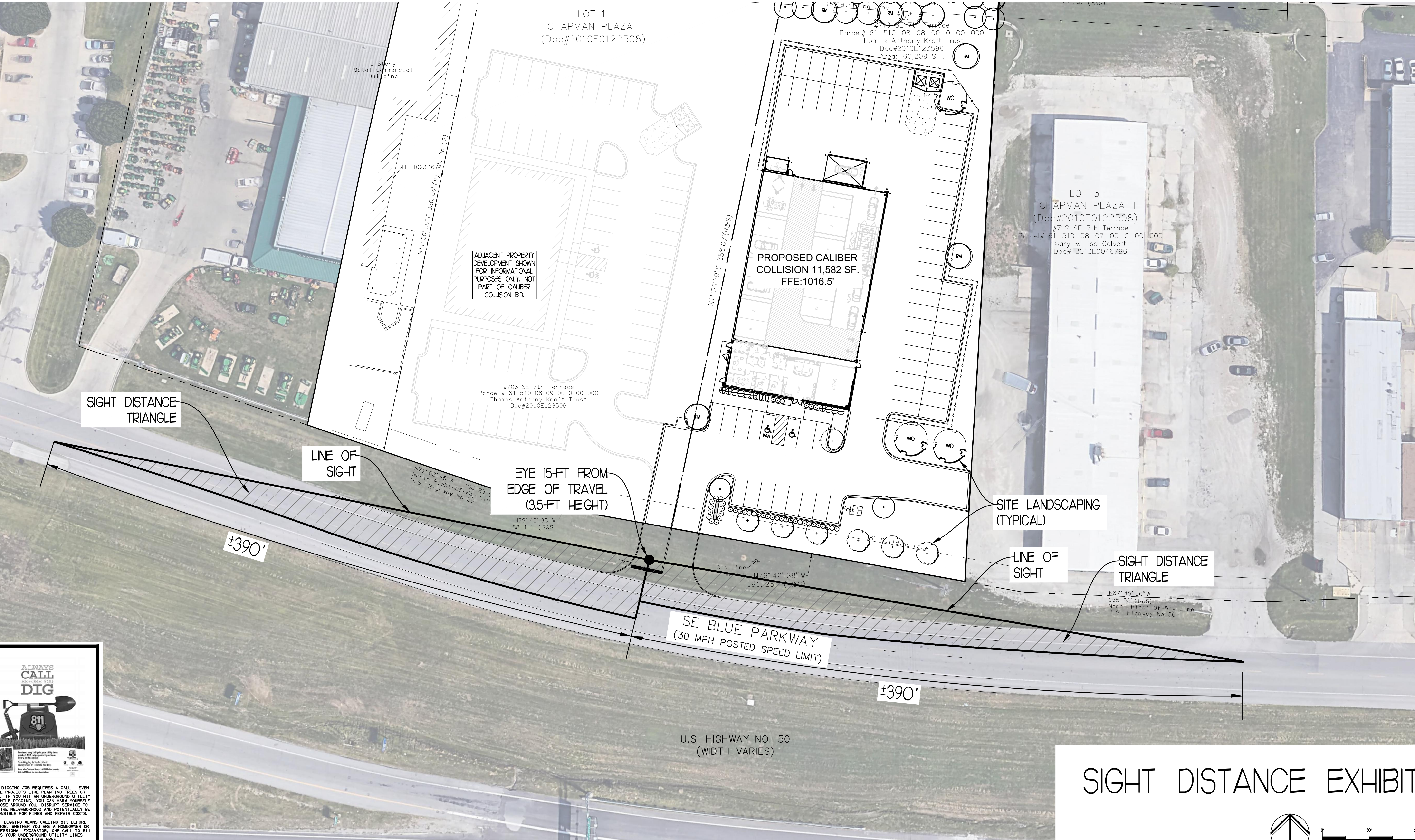
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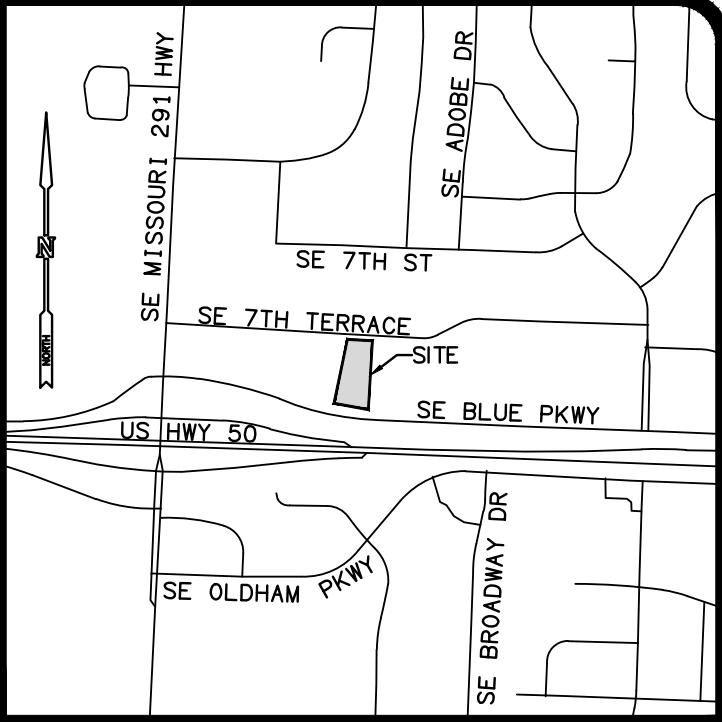
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NOTES:
POSTED SPEED LIMIT OF SE BLUE PARKWAY AT DEVELOPMENT IS 35-MPH.

LEGEND

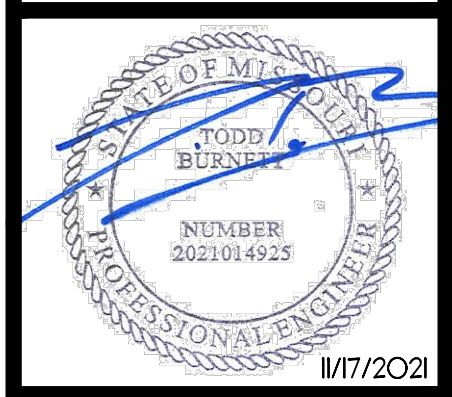
SYMBOL	DESCRIPTION
	PROPOSED SIGHT DISTANCE TRIANGLE
	PROPOSED TREES



LOCATION MAP
NOT TO SCALE

REVISIONS	BY

FREELAND and KAUFFMAN, INC.
*Engineers * Landscape Architects*
209 West Stone Avenue
Greenville, South Carolina 29609
864-233-5497
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CALIBER COLLISION
710 SE BLUE PARKWAY
LEE'S SUMMIT, MO 64063
CROSS DEVELOPMENT, CC LEE'S SUMMIT, LLC
4338 MARSH RIDGE ROAD
CARROLLTON, TX 75010
TEL (214) 614-8252

ALWAYS CALL 811 BEFORE YOU DIG

The City of Lee's Summit has a dedicated utility line marking service. Call 811 to request a utility line marking service. This service is provided at no cost to the caller. The City of Lee's Summit is not responsible for any damage to property or injury to persons resulting from the use of this service. The City of Lee's Summit is not responsible for any damage to property or injury to persons resulting from the use of this service.

EVERY DIGGING JOB REQUIRES A CALL - EVEN SMALL PROJECTS LIKE PLANTING TREES OR SINKS. IF YOU HIT AN UNDERGROUND UTILITY LINE WHILE DIGGING, YOU CAN HARM YOURSELF OR THOSE AROUND YOU. DISRUPT SERVICE TO AN ENTIRE NEIGHBORHOOD AND POTENTIALLY BE RESPONSIBLE FOR FINES AND REPAIR COSTS.

SMART DIGGING MEANS CALLING 811 BEFORE EACH JOB. WHETHER YOU ARE A HOMEOWNER OR A PROFESSIONAL EXCAVATOR, ONE CALL TO 811 GETS YOUR UNDERGROUND UTILITY LINES MARKED FOR FREE.

DRAWN
BAC
CHECKED
TMB
DATE
11/17/2021
SCALE

DRAWING

26



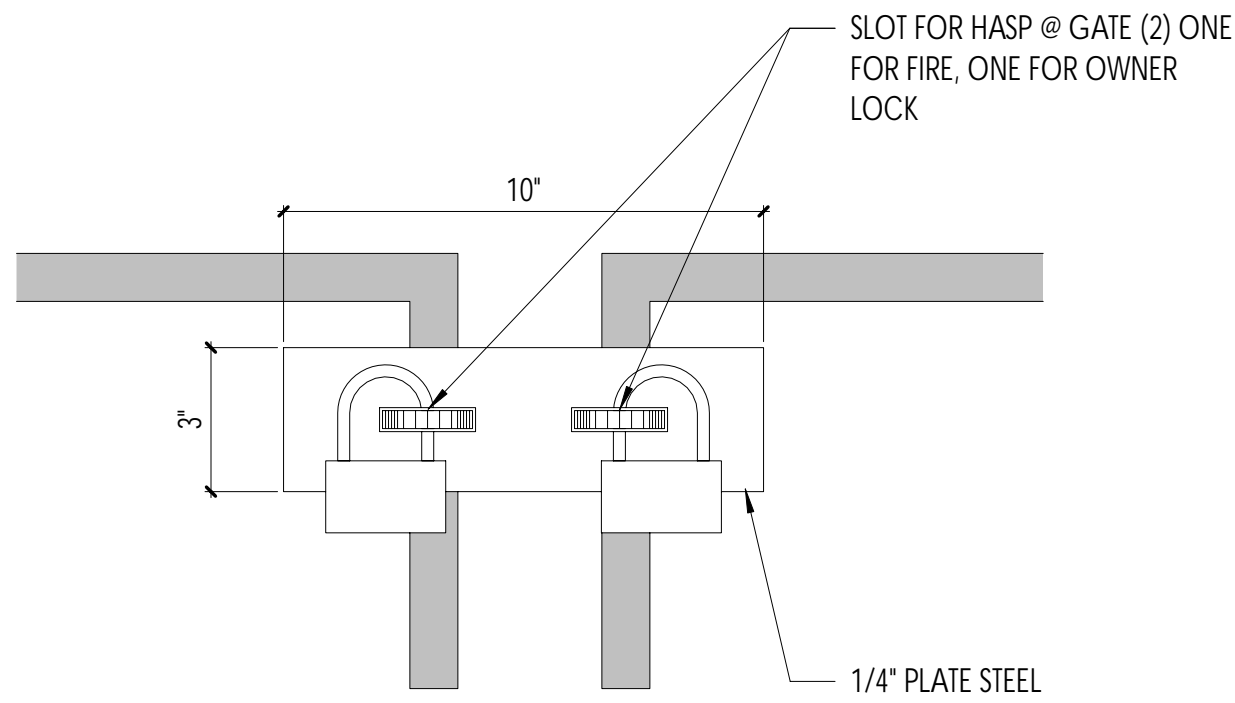
OXFORD
ARCHITECTURE

2934 Sidco Drive
Suite 120
Nashville, TN 37204

Architecture
Planning
Interior Architecture

CALIBER
COLLISION

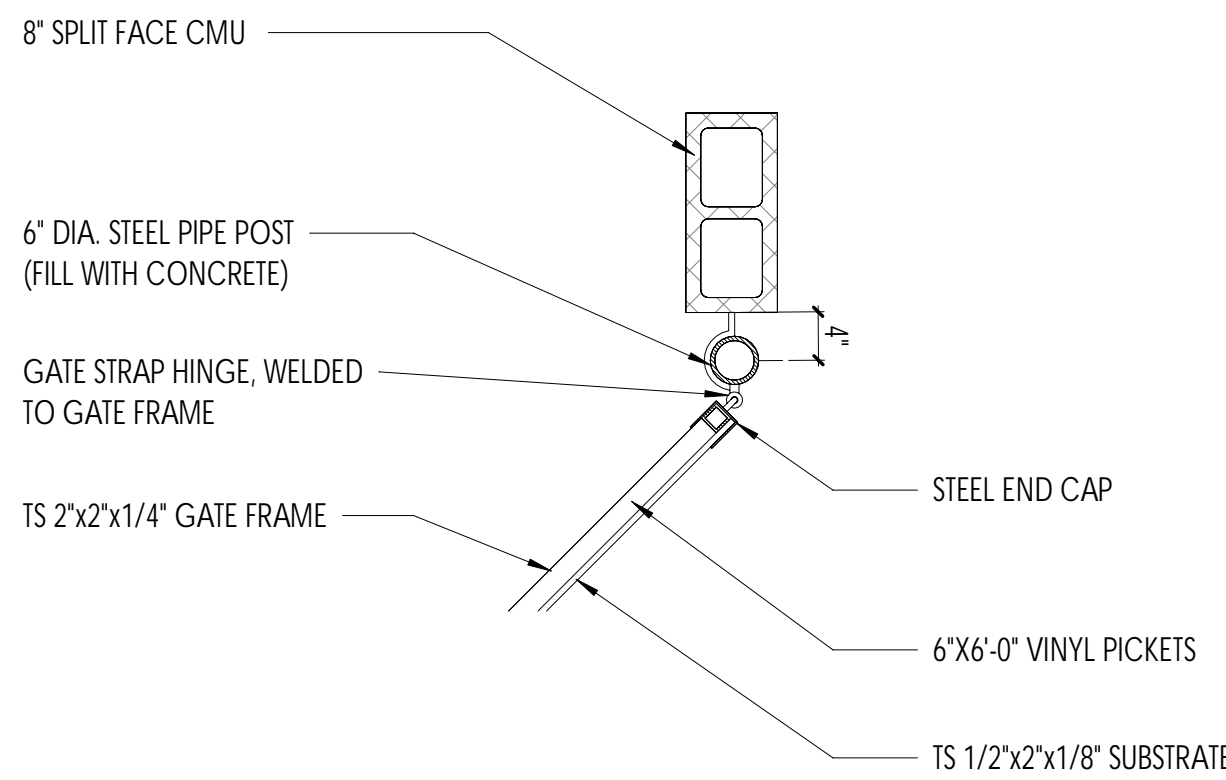
LEE'S SUMMIT,
MISSOURI



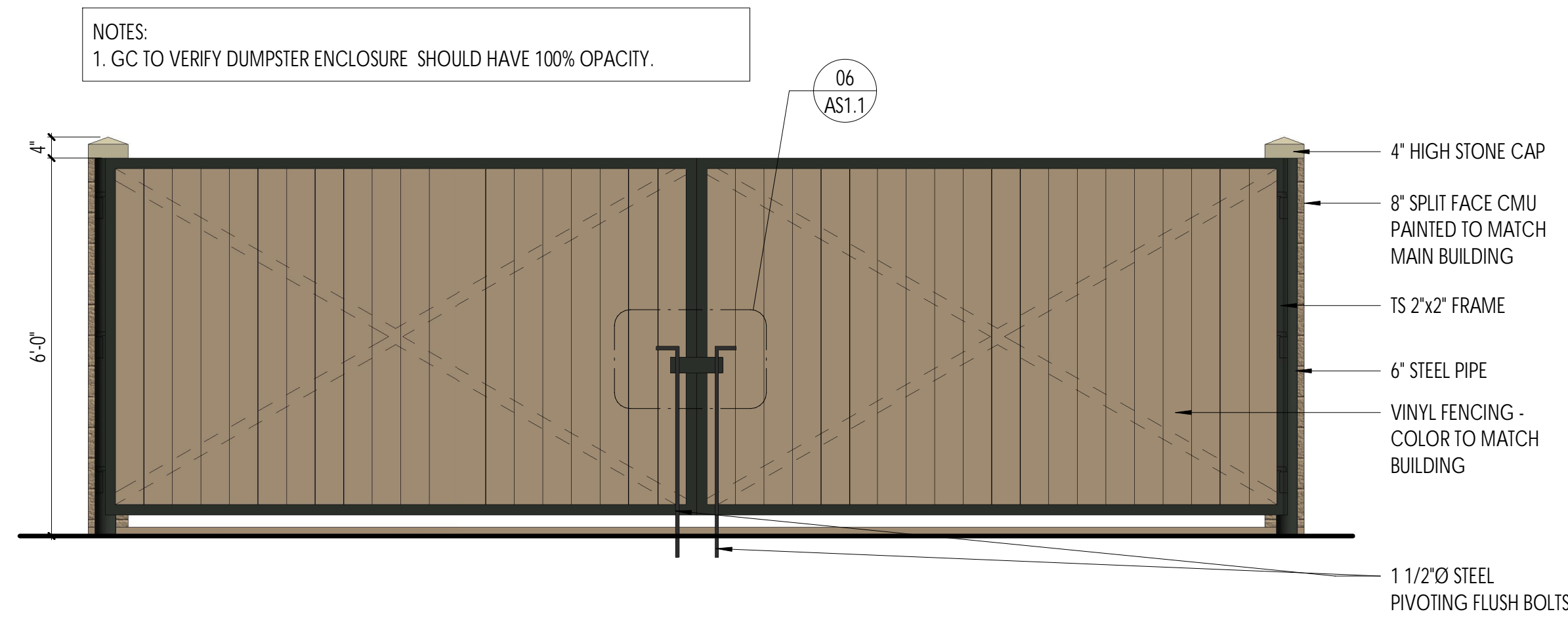
06 GATE CLASP
SCALE: 3" = 1'-0"



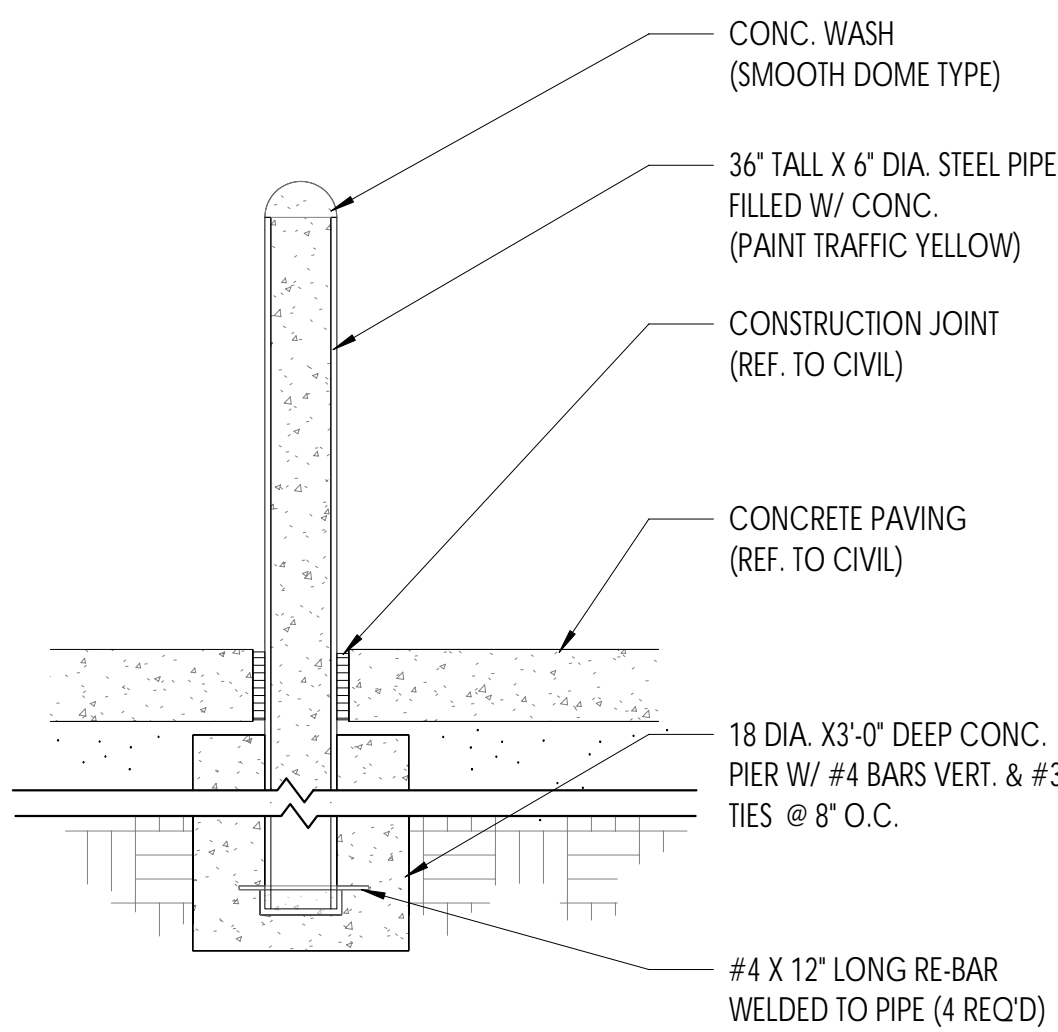
03 DUMPSTER ENCLOSURE SIDE ELEVATION
SCALE: 1/2" = 1'-0"



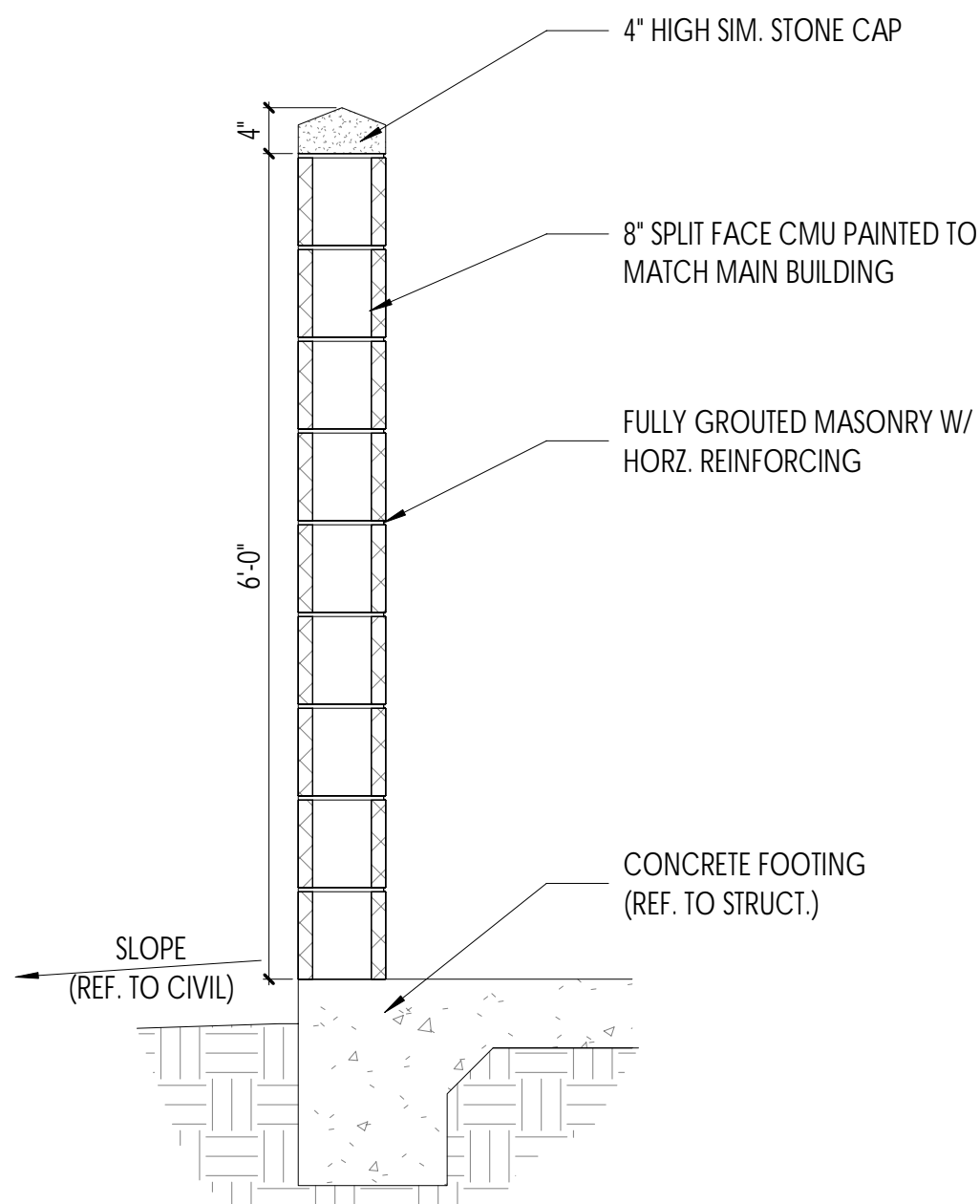
05 GATE DETAILS
SCALE: 3/4" = 1'-0"



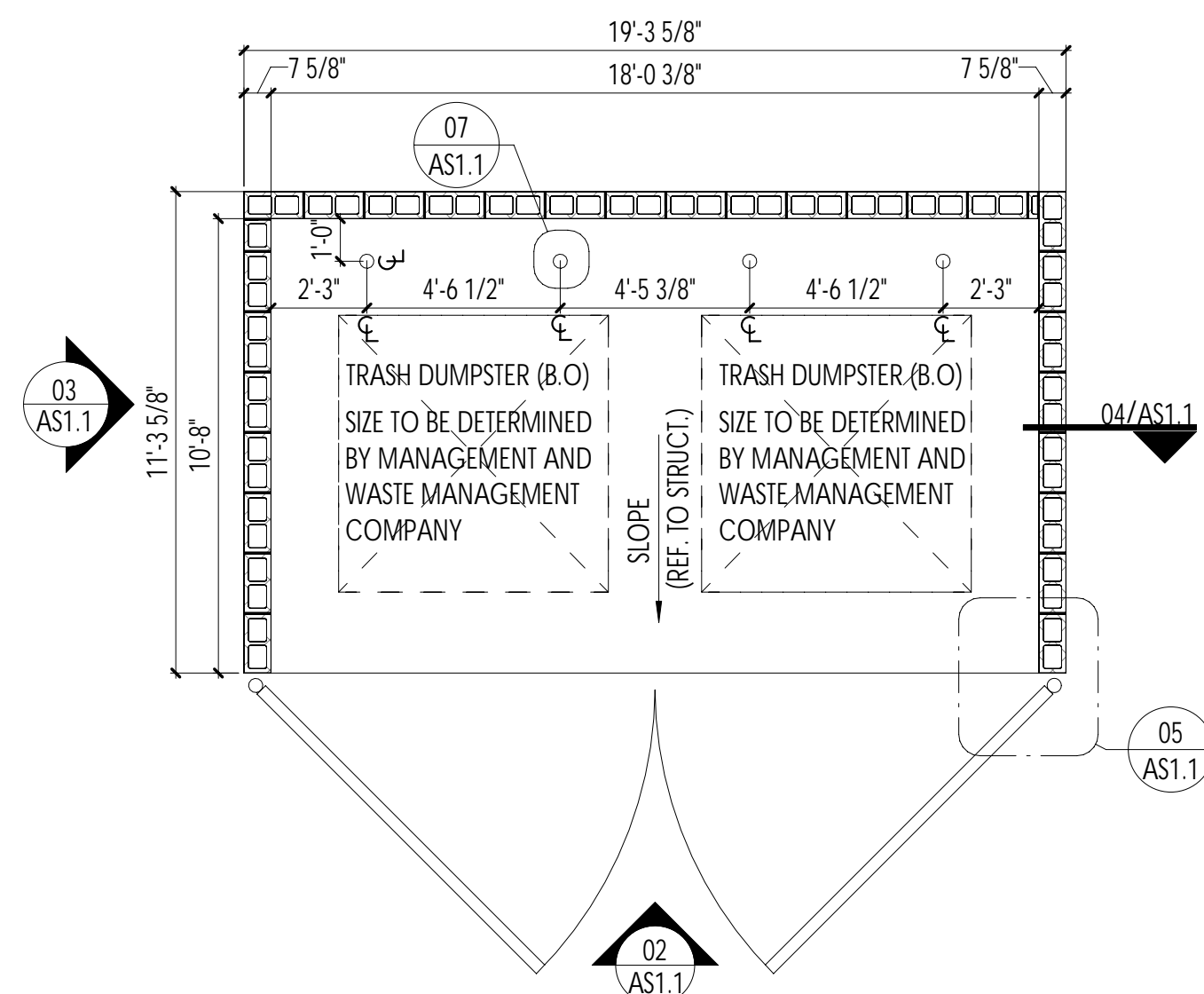
02 DUMPSTER ENCLOSURE FRONT ELEVATION
SCALE: 1/2" = 1'-0"



07 BOLLARD DETAIL
SCALE: 3/4" = 1'-0"



04 WALL SECTION
SCALE: 3/4" = 1'-0"



01 DUMPSTER ENCLOSURE PLAN
SCALE: 1/4" = 1'-0"

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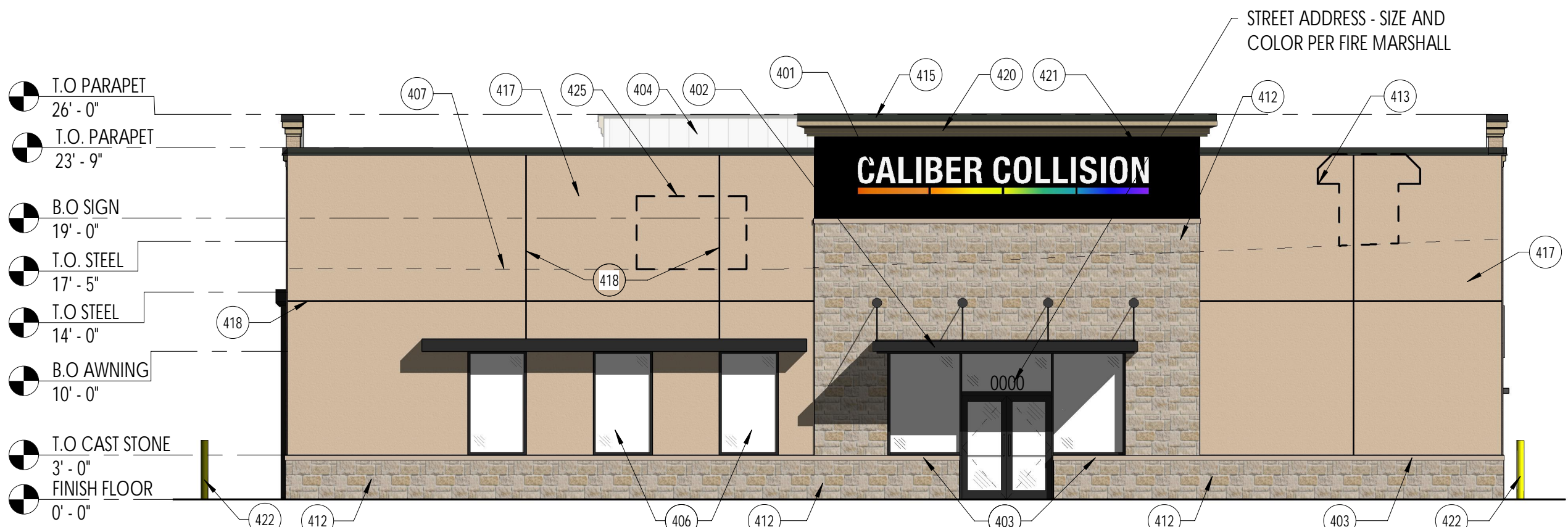
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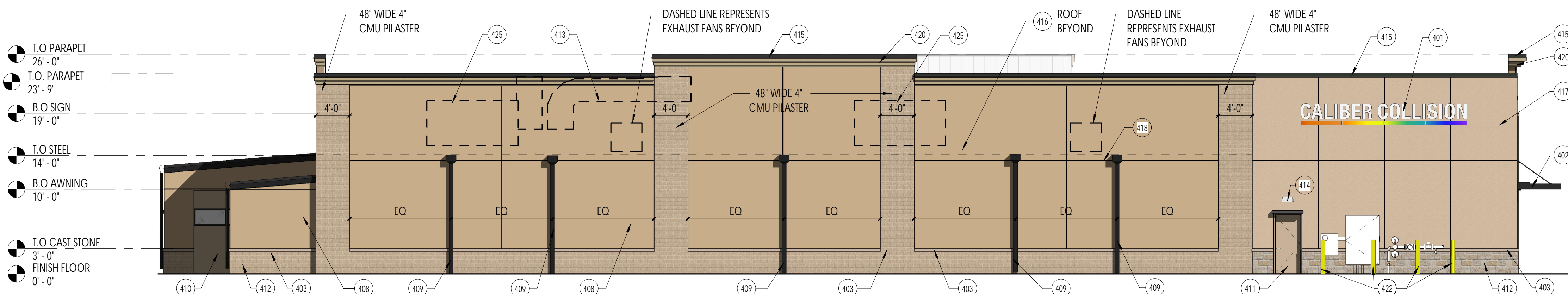
Revisions:
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Site and Trash Enclosure Details

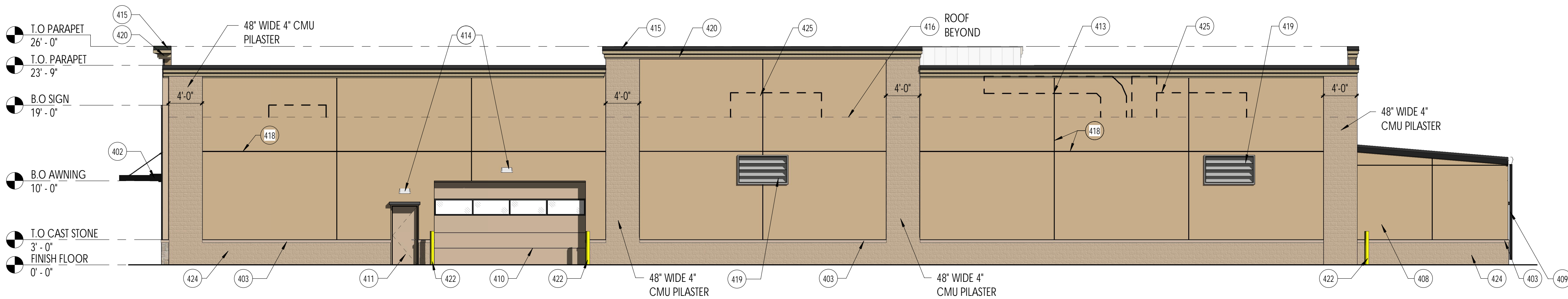
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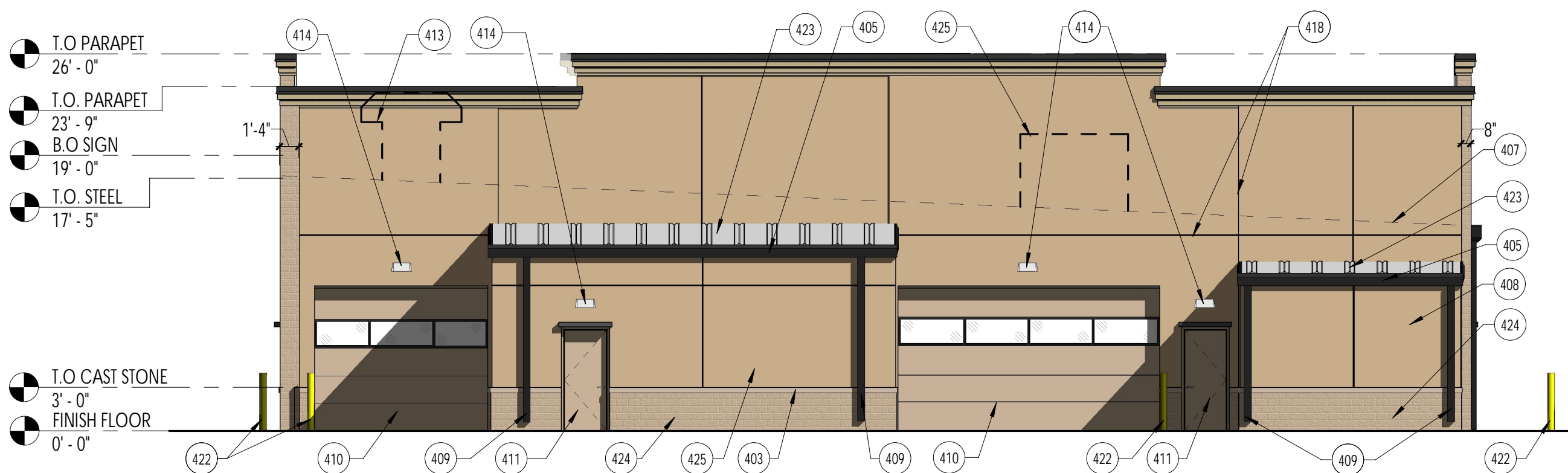
01 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



02 EAST ELEVATION
SCALE: 1/8" = 1'-0"



03 WEST ELEVATION
SCALE: 1/8" = 1'-0"



04 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

FAÇADE DIRECTION	EAST DIRECTION	WEST DIRECTION	NORTH DIRECTION	SOUTH DIRECTION
METAL (PANEL, COPING, FRAMES, AND DOORS) SQ. FT.	36.39	248.69	93.27	497.7
PERCENTAGE (%)	2.80%	7.22%	4.64%	18.00%
STONE SQ. FT.	74.12	2.31	482.7	0
PERCENTAGE (%)	2.15%	0.07%	24.00%	0%
CAST STONE SQ. FT.	50.6	44.14	37.28	32.55
PERCENTAGE (%)	1.47%	1.28%	1.85%	1.18%
EIFS SQ. FT.	109.19	139.54	172.35	80.5
PERCENTAGE (%)	3.17%	4.03%	8.57%	2.91%
GLAZING SQ. FT.	0	28.61	177	65.82
PERCENTAGE (%)	0%	0.83%	8.80%	2.38%
3 STEP STUCCO SQ. FT.	618.92	13.05	1048.3	0
PERCENTAGE (%)	17.98%	0.38%	52.13%	0%
EIFS (OVER R-PANEL) SQ. FT.	1874.16	2298.15	0	1811.91
PERCENTAGE (%)	54.46%	66.43%	0%	65.54%
CMU SQ. FT.	618.22	642.13	0	276.21
PERCENTAGE (%)	17.96%	18.56%	0%	9.99%
LOUVERS SQ. FT.	0	42	0	0
PERCENTAGE (%)	0%	1.21%	0%	0%
TOTAL SQ. FT.	3441.61	3459.62	2010.9	2764.6
TOTAL PERCENTAGE	100%	100%	100%	100%

KEYNOTES:

- 401 SIGNAGE (BY OTHERS)-PROVIDE 3/4" PLYWD. BACKING (SEPARATE PERMIT).
- 402 PREMANUFACTURED MTL. AWNING W/ TIE RODS BY G.C.
- 403 SYNTHETIC STONE SILL/WATER TABLE. REFER TO SPECS.
- 404 PARAPET BACK PANEL (SOLAR WHITE COLOR).
- 405 MTL. GUTTER-PROVIDED BY MTL. BUILDING PROVIDER AND INSTALLED BY GC.
- 406 STOREFRONT / GLAZING (AL-2).
- 407 ROOFLINE BEYOND.
- 408 STRUKTURCO STUCCO TEXTURED ARCHITECTURAL METAL PANEL (PNL-1).
- 409 PRE-FINISHED 6"x4" D (SQUARE) METAL DOWNSPOUT (BURNISHED SLATE)-PROVIDED BY MTL. BUILDING SUPPLIER AND INSTALLED BY GC.
- 410 FACTORY FINISHED SECTIONAL OH DOOR (PT-3).
- 411 HOLLOW MTL. DOOR (PT-3) AND FRAME (SADDLE TAN). PROVIDE DRIP CAP OVER DOORS WITH NO OVERHEAD COVER.
- 412 SYNTHETIC STONE VENEER (M-1).
- 413 EXHAUST AND AIR INTAKE. REF. PAINT BOOTH SHEETS.
- 414 WALL MOUNTED LIGHT FIXTURE. REF. MEP.
- 415 PRE-FINISHED MTL. COPING (MTL-2) - PROVIDED BY MTL. BUILDING SUPPLIER AND INSTALLED BY GC.
- 416 METAL BUILDING STANDING SEAM ROOFING (MTL-1).
- 417 3 STEP STUCCO INTEGRATED COLOR (PT-3).
- 418 CONTROL JOINT.
- 419 WALL LOUVER. (CLEAR ANODIZED FINISH) REF. MEP.
- 420 EIFS CORNICE. (PT-9).
- 421 EIFS ACCENT BANDING-PAINT TO MATCH SW6258 TRICORN BLACK (EF-4).
- 422 BOLLARDS.
- 423 PRE-FINISHED METAL BUILDING "R" PANEL ROOFING.
- 424 SPLIT FACE C.M.U. - COLOR TO MATCH (PT-7) LIGHT STONE.
- 425 DASH LINE REPRESENTS ROOF TOP UNIT BEYOND.

EXTERIOR FINISH LEGEND

FINISH KEY	DESCRIPTION	LOCATION	MANUFACTURER	PRODUCT COLOR	IDENTIFICATION/ FINISH/ STYLE	CONTACT
AL-2	STOREFRONT SYSTEM	OFFICE EXTERIOR	KAWNEER OR APPROVED EQUAL	DARK ANODIZED BRONZE	2" X 4-1/2" ALUMINUM FRAME	N/A
CS-1	CAST STONE (REF. ELEV)	OFFICE EXTERIOR	CORONADO STONE/ EL DORADO STONE	CORONADO- OFF WHITE EL DORADO- BUCKSKIN	REFER TO SPECS-047300	N/A-SAMPLE TO BE PROVIDED BY GC
CP-1	CUSTOM CANOPY	LOBBY ENTRANCE	ARCHITECTURAL FABRICATORS	DARK BRONZE	HELIOS 399 X 446	ARCH. FABRICATORS 1-800-962-8027
EF-4	EXTERIOR BANDING	-	SHERWIN WILLIAMS	TRICORN BLACK	-	BRETT C. HUCKLEBURY 214-728-6696
EF-5	E.I.F.S	SHOP EXTERIOR	-	COLOR TO MATCH SADDLE TAN	1-1/2" E.I.F.S OVER 1-1/2" R-PANEL	-
GR-2	EXTERIOR MASONRY MORTAR	-	SGS-SOLOMON COLORS INC.	10X BUFF	TYPE S MORTAR	BART SNOWDEN 214-794-9159
M-1	EXTERIOR MASONRY	-	GC TO PROVIDE LOCAL ALTERNATE OR APPROVAL	CORONADO- TEXAS CREAM EL DORADO- AUSTIN CREAM	CORONADO STONE/ EL DORADO STONE REFER TO SPECS-047300	N/A
PT-3	EXTERIOR DOOR PAINT	-	SHERWIN WILLIAMS	NOMADIC DESERT	SW 6107	BRETT C. HUCKLEBURY 214-728-6696
PT-7	EXTERIOR FINISH	-	SHERWIN WILLIAMS	NOMADIC DESERT	SW 6107	BRETT C. HUCKLEBURY 214-728-6696
PT-9	EXTERIOR CORNICE	-	DRYVIT	BAVARIAN WOOD #448	SAND PEBBLE	N/A
MTL-1	MTL. ROOFING	SHOP ROOF	METL-SPAN	SOLAR WHITE	PREM. WEATHER XL E0.85 SRI-76	METL-SPAN 877-585-9969
MTL-2	RAKE, GUTTER, TRIM	EXTERIOR	METL-SPAN	BURNISHED SLATE	PREM. WEATHER XL E0.86 SRI-25	METL-SPAN 877-585-9969

GENERAL NOTE

- DOWNSPOUTS ARE INDICATED FOR SIZE, QUANTITY, AND APPROXIMATE LOCATION ONLY.
- HOLLOW METAL STEEL DOORS, FRAMES, EXPOSED METAL FLASHING AND EXPOSED MISCELLANEOUS STEEL SHALL MATCH ADJACENT BUILDING COLOR UNLESS NOTED OTHERWISE. IF THERE ARE TWO ADJACENT COLORS AT THE DOOR LOCATIONS CONTRACTOR TO USE THE BASE OF THE BOTTOM FINISH FOR THE ENTIRE DOOR.
- SIGNAGE SHALL BE PERMITTED SEPARATELY.
- REFER TO THIS SHEET FOR FINISHES AND MATERIALS.
- PAINT EXPOSED STEEL BEAMS IN STORAGE AREA.
- THE LIGHT SOURCE FOR EXTERIOR FIXTURES SHALL NOT PROJECT BELOW THE OPAQUE HOUSING SELECTED. NO FIXTURE SHALL DIRECTLY PROJECT LIGHT HORIZONTALLY.

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Revisions:

Exterior Elevations (Colored)

Sheet Number:

A2.01

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