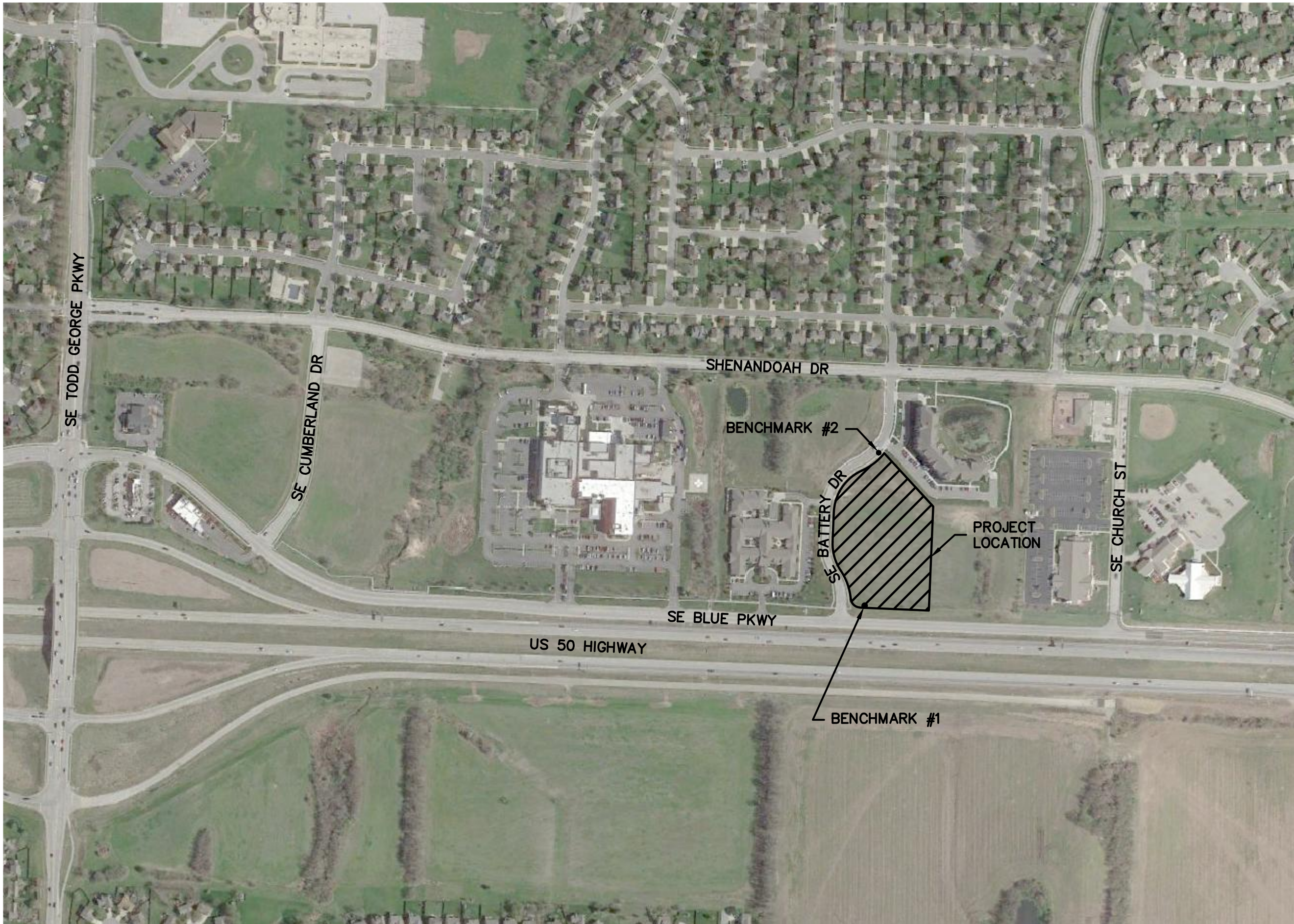


DWG: F:\2018\0001-0500\018-0330-B\40-Design\AutoCAD\Final Plans\Sheets\GNCV\WORK PACKAGE 4\LEES SUMMIT\C\_COV\_80330.dwg USER: jfrleing  
DATE: Mar 26, 2019 10:21am XREFS: C:\4LS\_XBASE\_80330 C:\4LS\_PBASE\_80330

CONSTRUCTION DOCUMENTS FOR  
MID-CONTINENT PUBLIC LIBRARY  
EAST LEE'S SUMMIT BRANCH  
NE CORNER, SE BATTERY DRIVE AND SE BLUE PARKWAY  
NE 1/4 OF SECTION 10, TOWNSHIP 47 NORTH, RANGE 31 WEST  
LEE'S SUMMIT, JACKSON COUNTY, MISSOURI

Sheet List Table	
Sheet Number	Sheet Title
C1.0	COVER SHEET
C1.1	GENERAL NOTES
C1.2	SURVEY SHEET
C1.3	FINAL DEVELOPMENT PLAN
C2.0	DEMOLITION PLAN
C3.0	GENERAL LAYOUT PLAN
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C4.1	SITE DIMENSION PLAN
C5.0	GRADING PLAN
C5.1	GRADING DETAILS
C5.2	GRADING DETAILS
C5.3	GRADING DETAILS
C5.4	GRADING DETAILS
C5.5	GRADING DETAILS
C6.0	UTILITY PLAN
C7.0	STORM SEWER PLAN & PROFILE
C7.1	STORM SEWER PLAN & PROFILE
C8.0	DRAINAGE PLAN REVISED
C9.0	EROSION CONTROL PLAN – PRE CONSTRUCTION
C9.1	EROSION CONTROL – CONSTRUCTION
C9.2	EROSION CONTROL – POST CONSTRUCTION
C9.3	EROSION CONTROL DETAILS
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C9.5	EROSION CONTROL DETAILS
C10.0	STANDARD DETAILS
C10.1	STANDARD DETAILS
C10.2	STANDARD DETAILS
C10.3	STANDARD DETAILS
C10.4	STANDARD DETAILS
C10.5	STANDARD DETAILS
C10.6	STANDARD DETAILS REVISED



NOT TO SCALE

**BENCHMARKS:**

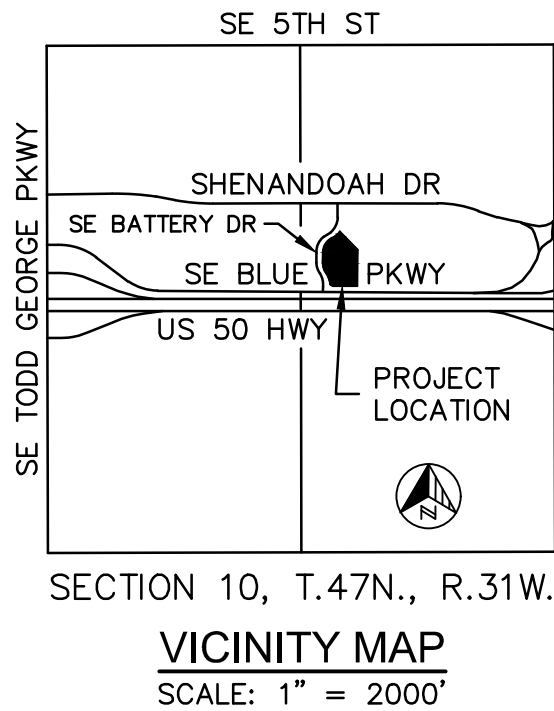
**BENCHMARK #1:**  
ELEVATION=1027.22'  
SET RAILROAD SPIKE IN THE NORTH SIDE OF A POWER POLE, 66'± NORTH OF THE CL OF SE BLUE PARKWAY, 86' EAST OF THE CL OF SE BATTERY DRIVE.

**BENCHMARK #2:**  
ELEVATION=1009.31'  
SET CHISELED "I" CUT ON THE SOUTHWESTERLY CORNER OF A CONCRETE CURB INLET #40636, 630'± NORTH OF THE CL OF SE BLUE PARKWAY, 15' EAST OF THE CL OF SE BATTERY DRIVE.

**LEGAL DESCRIPTION:**

LOT 2A, MAGNOLIA PLACE AT CHARLESTON PARK, SECOND PLAT, LOTS 2A-3, A SUBDIVISION IN LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, EXCEPT THE EAST 81.81 FEET THEREOF, CONTAINING 165,561 SQUARE FEET OR 3.8008 ACRES, MORE OR LESS .

DEVELOPMENT TEAM CONTACT INFORMATION	
OWNER/DEVELOPER	
STEVEN V. POTTER MID-CONTINENT PUBLIC LIBRARY	ADMINISTRATIVE HEADQUARTERS 15616 E. 24 HWY. INDEPENDENCE, MO 64050 816.836.5200
CIVIL ENGINEER	
TERRY PARSONS OLSSON ASSOCIATES	7301 W. 133RD STREET SUITE 200 OVERLAND PARK, KS 66213 PH: 913.381.1170 FAX: 913.381.1174 tparsons@olssonassociates.com



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Kansas City, MO 64108 816.300.0300

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Mid-Continent Public Library

CONSTRUCTION DOCUMENT PLANS FOR

EAST LEE'S SUMMIT BRANCH

2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE  
04

Engineer of Record

Terry M Parsons, Engineer MO PE-2018010505

olsson

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Olsson  
Missouri State Certificate of Authority #001592

Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19
3	DETENTION MODIFICATIONS	02.26.20

Project No. B18-0330	Date 12.07.18	Drawn RLK
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Drawing No.  
**C1.0**  
COVER SHEET

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DATE: Mar 26, 2019 10:22am XREFS: C:\S\_TBLK\_80330 C:\S\_XBASE\_80330 C:\S\_PBASE\_80330 USER: jfrleing

GENERAL NOTES:

- THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND MAY NOT INCLUDE ALL LINES PRESENT. THE CONTRACTOR SHALL BE RESPONSIBLE TO CALL "1-800-DIG-RITE", 1(800)344-7483 OR 811 AND COORDINATE FIELD LOCATION OF EXISTING UNDERGROUND UTILITIES PRIOR TO BEGINNING GRADING ACTIVITIES. !STOP!! CALL BEFORE YOU DIG!!
- THE CONTRACTOR SHALL NOT CHANGE OR DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE OWNER AND ENGINEER.
- ALL WORK AND MATERIALS SHALL BE SUBJECT TO INSPECTION AND APPROVAL BY THE OWNER OR THE OWNER'S REPRESENTATIVE.
- ALL ESTIMATES OF QUANTITIES ARE FOR INFORMATION PURPOSES ONLY. CONTRACTOR AND SUBCONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING ALL QUANTITIES AND FOR BRINGING THE PROJECT TO THE LINES AND GRADES SHOWN HEREIN. CONTRACTOR SHALL PROVIDE ALL WORK AND MATERIALS REQUIRED TO FULFILL THE PLANS IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO DETERMINE EARTHWORK QUANTITIES AND TO ACCOUNT FOR HAUL IN OR HAUL OFF OF MATERIAL AS NECESSARY TO MEET THE LINES AND GRADES OF THE PLANS EVEN IF QUANTITY ESTIMATES ARE SHOWN WITHIN THESE DOCUMENTS. NO ADDITIONAL PAYMENTS WILL BE MADE FOR IMPORT OR EXPORT OF MATERIAL OR FOR ADJUSTMENTS TO QUANTITY ESTIMATES.
- ALL CONSTRUCTION SHALL CONFORM TO THE LATEST STANDARDS AND SPECIFICATIONS OF THE AMERICAN PUBLIC WORKS ASSOCIATION - KANSAS CITY METROPOLITAN CHAPTER (APWA-KC) AND THE CITY OF LEE'S SUMMIT, MO, EXCEPT WHERE SHOWN OTHERWISE. NOTIFY ENGINEER OF DISCREPANCIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS, PAYING ALL FEES AND FOR OTHERWISE COMPLYING WITH ALL APPLICABLE REGULATIONS GOVERNING THE WORK.
- THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF MISSOURI STATE LAW WHICH REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT-OF-WAY DO SO ONLY AFTER GIVING NOTICE TO, AND OBTAINING INFORMATION FROM UTILITY COMPANIES.
- PRIOR TO COMMENCEMENT OF WORK, THE CONTRACTOR SHALL NOTIFY ALL THOSE COMPANIES WHICH HAVE FACILITIES IN THE NEAR VICINITY OF THE CONSTRUCTION TO BE PERFORMED.
- THE CONTRACTOR SHALL LIMIT THE REMOVAL OF TREES TO THE LIMITS OF DEMOLITION SHOWN ON THE DEMOLITION PLAN.
- CLEARING AND GRUBBING OPERATIONS AND DISPOSAL OF ALL DEBRIS THEREFROM SHALL BE PERFORMED BY THE CONTRACTOR IN STRICT ACCORDANCE WITH ALL LOCAL CODES AND ORDINANCES.
- ALL WASTE MATERIAL RESULTING FROM THE PROJECT SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR.
- ALL MANHOLES, CATCH BASINS, UTILITY VALVES AND METER PITS ARE TO BE ADJUSTED OR REBUILT TO GRADE AS REQUIRED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROL OF SURFACE EROSION DURING CONSTRUCTION AND UNTIL THE OWNER ACCEPTS THE WORK AS COMPLETE. EROSION CONTROL MEASURES INCLUDING, BUT NOT LIMITED TO, THE SILT FENCES AND GRAVEL FILTER BAGS SHOWN ON THE EROSION CONTROL PLAN SHALL BE IN PLACE FOR THE DURATION OF THE SITE IMPROVEMENTS.
- ALL HDPE PIPE SHALL BE ADS (N-12) OR APPROVED EQUAL, AND CONFORM TO AASHTO M294 SPECIFICATIONS. ALL PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
- IF PRECAST CONCRETE STORM SEWER STRUCTURES ARE TO BE USED ON THIS PROJECT, THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS AND HAVE THEM APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE STRUCTURES. FAILURE TO DO SO SHALL BE CAUSE FOR REJECTION.
- EXISTING TOPSOIL SHALL BE STRIPPED TO A POINT WHERE ALL VEGETATION IS REMOVED. SITE PREPARATION, GRADING AND EXCAVATION PROCEDURES SHALL CONFORM TO THE SPECIFICATIONS PROVIDED.
- THE CONTRACTOR SHALL, BY HIS OWN INVESTIGATION, AND PRIOR TO COMMENCING WORK, SATISFY HIMSELF AS TO THE SURFACE AND SUBSURFACE CONDITIONS TO BE ENCOUNTERED.
- THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL BOUNDARY CORNERS AND SECTION CORNERS. ANY BOUNDARY CORNER AND/OR SECTION CORNER DISTURBED OR DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE RESET BY A LAND SURVEYOR LICENSED IN THE STATE OF MISSOURI, AT THE CONTRACTOR'S EXPENSE.
- NO FEDERALLY OWNED MAILBOX MAY BE DISTURBED. THE CONTRACTOR SHALL GIVE AT LEAST TWENTY-FOUR (24) HOURS ADVANCE NOTICE TO THE MANAGER OF DELIVERY AND COLLECTIONS. TAMPERING WITH FEDERAL MAIL FACILITIES MAY SUBJECT THE CONTRACTOR TO PROSECUTION BY THE FEDERAL GOVERNMENT.
- THE CONTOUR LINES SHOWN ARE FOR MASS GRADING PURPOSES.
- EXISTING CONTOURS REPRESENT MASS GRADING ELEVATIONS.
- THE CONTRACTOR SHALL FINISH GRADE SLOPES AS SHOWN NO STEEPER THAN 1 FOOT VERTICAL IN 3 FEET HORIZONTAL UNLESS OTHERWISE SHOWN BY CONTOURS OR SPOT ELEVATIONS.
- THE CONTRACTOR SHALL GRADE LANDSCAPED AREAS TO PROVIDE POSITIVE DRAINAGE IN THE BORROW AREA.
- THE CONTRACTOR SHALL MAKE HIS OWN ASSUMPTIONS ON THE LOCATION AND CONSISTENCY OF ANY EXISTING ROCK LAYERS UNDERLYING THE PROJECT SITE. ALL ROCK EXCAVATION AND REMOVAL SHALL BE INCLUDED IN THE CONTRACTORS' BID.
- CONTRACTOR TO FIELD VERIFY ELEVATIONS AND LOCATIONS OF EXISTING UTILITIES AND INFRASTRUCTURE PRIOR TO CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES BETWEEN PLANS AND FIELD CONDITIONS.
- BY ACCEPTING AND UTILIZING ANY ELECTRONIC FILE OF ANY DRAWING, REPORT OR DATA TRANSMITTED BY OLSSON (OLSSON), THE RECIPIENT AGREES FOR ITSELF, ITS SUCCESSORS, ASSIGNS, INSURERS AND ALL THOSE CLAIMING UNDER OR THROUGH IT, THAT BY USING ANY OF THE INFORMATION CONTAINED IN THE ELECTRONIC FILE, ALL USERS AGREE TO BE BOUND BY THE FOLLOWING TERMS. ALL OF THE INFORMATION CONTAINED IN THIS ELECTRONIC FILE IS THE WORK PRODUCT AND INSTRUMENT OF SERVICE OF OLSSON, WHO SHALL BE DEEMED THE AUTHOR, AND SHALL RETAIN ALL COMMON LAW, STATUTORY LAW AND OTHER RIGHTS, INCLUDING COPYRIGHTS, UNLESS THE SAME HAVE PREVIOUSLY BEEN TRANSFERRED IN WRITING TO THE RECIPIENT. THE INFORMATION CONTAINED IN THE ELECTRONIC FILE IS PROVIDED FOR THE CONVENIENCE OF THE RECIPIENT AND IS PROVIDED IN "AS IS" CONDITION. THE RECIPIENT IS AWARE THAT DIFFERENCES MAY EXIST BETWEEN THE ELECTRONIC FILES AND THE PRINTED HARD-COPY ORIGINAL SIGNED AND SEALED DRAWINGS OR REPORTS. IN THE EVENT OF A CONFLICT BETWEEN THE SIGNED AND SEALED ORIGINAL DOCUMENTS PREPARED BY OLSSON AND THE ELECTRONIC FILES TRANSFERRED HEREWITH, THE SIGNED AND SEALED ORIGINAL DOCUMENTS SHALL GOVERN. OLSSON SPECIFICALLY DISCLAIMS ALL WARRANTIES, EXPRESSED OR IMPLIED, INCLUDING WITHOUT LIMITATION, ANY WARRANTY OF MERCHANTABILITY OR FITNESS FOR A PARTICULAR PURPOSE WITH RESPECT TO ELECTRONIC FILES. IT SHALL BE THE RECIPIENT'S RESPONSIBILITY TO CONFIRM THE ACCURACY OF THE INFORMATION CONTAINED IN THE ELECTRONIC FILE AND THAT IF ACCURATELY REFLECTS THE INFORMATION NEEDED BY THE RECIPIENT. THE RECIPIENT SHALL NOT RETRANSMIT THE ELECTRONIC FILE, OR ANY PORTION THEREOF, WITHOUT INCLUDING THIS DISCLAIMER AS PART OF ANY SUCH TRANSMISSION. IN ADDITION, THE RECIPIENT AGREES, TO THE FULLEST EXTENT PERMITTED BY LAW, TO INDEMNIFY AND HOLD HARMLESS OLSSON, ITS OFFICERS, DIRECTORS, EMPLOYEES AND SUBCONSULTANTS AGAINST ANY AND ALL DAMAGES, LIABILITIES, CLAIMS OR COSTS, INCLUDING REASONABLE ATTORNEY'S AND EXPERT WITNESS FEES AND DEFENSE COSTS, ARISING FROM ANY CHANGES MADE BY ANYONE OTHER THAN OLSSON OR FROM ANY REUSE OF THE ELECTRONIC FILES WITHOUT THE PRIOR WRITTEN CONSENT OF OLSSON.
- DESIGN PROFESSIONAL SHALL REVIEW SHOP DRAWINGS OR SAMPLES FOR GENERAL CONFORMANCE WITH THE DESIGN CONCEPTS ON THE PROJECT AND FOR COMPLIANCE WITH THE INFORMATION GIVEN IN THE CONTRACT DOCUMENTS, AND SHALL NOT EXTEND TO MEANS OR METHODS OF CONSTRUCTION. THE DESIGN PROFESSIONAL'S REVIEW SHALL NOT RELIEVE CONTRACTOR FROM RESPONSIBILITY FOR ANY VARIATION FROM THE REQUIREMENTS OF THE CONTRACT DOCUMENTS UNLESS CONTRACTOR HAS IN WRITING CALLED DESIGN PROFESSIONAL'S ATTENTION TO EACH SUCH VARIATION AT THE TIME OF SUBMISSION, AND DESIGN PROFESSIONAL HAS GIVEN WRITTEN APPROVAL OF EACH SUCH VARIATION BY SPECIFIC WRITTEN NOTATION THEREOF INCORPORATED INTO OR ACCOMPANYING THE SHOP DRAWING OR SAMPLE; NOR WILL ANY APPROVAL BY THE DESIGN PROFESSIONAL RELIEVE CONTRACTOR FROM RESPONSIBILITY FOR ERRORS OR OMISSIONS IN SHOP DRAWINGS WITH CONFORMANCE TO CONTRACT DOCUMENTS.

- GENERAL CONSTRUCTION NOTE REGARDING SEQUENCING OF EROSION CONTROL - ALL PERIMETER SILT FENCE, EARTH DIKES, SEDIMENT BASINS, AND ROCK CONSTRUCTION ENTRANCES WILL BE INSTALLED BEFORE GRADING OPERATIONS BEGIN, EXCEPT THAT SILT FENCE WHICH IS TO BE PLACED ALONG THE BACK OF CURB FOR PROTECTION OF THE STREET. SILT FENCE AND EARTH DIKES THAT ARE PLACED BEFORE GRADING BEGINS WILL BE MAINTAINED BY THE GRADING CONTRACTOR UNTIL ALL UTILITIES ARE IN PLACE. THE SILT FENCE THAT IS PLACED ALONG THE BACK OF THE CURB OR RIGHT -OF-WAY WILL BE INSTALLED IMMEDIATELY AFTER THE CURB IS CONSTRUCTED. EROSION AND SEDIMENTATION CONTROLS ARE TEMPORARY AND MUST BE REMOVED BY THE CONTRACTOR AFTER CONSTRUCTION IS COMPLETE AND THE DISTURBED AREA IS AT LEAST 70% PERMANENTLY VEGETATED.
- HANDICAP PARKING STALLS SHALL BE SIGNED WITH CITY/ADA APPROVED SIGNAGE AND CONSTRUCTED IN STRICT ACCORDANCE WITH THE LATEST STANDARDS AND SPECIFICATIONS OF THE APWA-KC, CITY OF LEE'S SUMMIT ADA STANDARDS, AND SHALL NOT EXCEED 2.00 PERCENT IN ANY DIRECTION. ACCESSIBLE SIDEWALKS HAVE A MAXIMUM CROSS SLOPE OF 2 PERCENT AND A MAXIMUM LONGITUDINAL SLOPE OF 5 PERCENT.
- ALL WATER LINES SHALL BE INSTALLED PER THE LATEST STANDARDS AND SPECIFICATIONS OF THE APWA-KC AND THE CITY OF LEE'S SUMMIT, MO. ALL WATER LINES SHALL BE A MINIMUM OF 48 INCHES BELOW THE FINISHED GRADE ELEVATIONS SHOWN HEREIN.
- ALL WATER LINES SHALL BE INSTALLED PER CITY STANDARDS. ALL WATER LINES SHALL BE A MINIMUM OF 48 INCHES BELOW THE FINISHED GRADE ELEVATIONS SHOWN HEREIN.
- ALL EXTERIOR CONCRETE SHALL BE KCMMB-4K AND HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 4000 PSI, SHALL MEET KCMMB STANDARDS AND SPECIFICATIONS, AND SHALL BE AIR ENTRAINED. FLYASH IS NOT A SUITABLE REPLACEMENT FOR PORTLAND CEMENT.
- ALL ON-SITE WIRING AND CABLES SHALL BE PLACED UNDERGROUND.
- CONCRETE PAVEMENT JOINTS SHALL BE CONSTRUCTED AS FOLLOWS (REFER TO HARDSCAPE PLANS FOR SPECIFIC TREATMENT OF THESE AREAS):
  - CONTROL JOINTS SPACED AT INTERVALS NOT GREATER THAN 12 FEET AND TOOLED TO 1/3 THE SLAB THICKNESS.
  - CONSTRUCTION JOINTS AT THE END OF EACH POUR AND WHEN PAVING OPERATIONS ARE SUSPENDED FOR 30 MINUTES OR MORE.
  - ISOLATION JOINTS PLACED WHERE THE PAVEMENT ABUTS THE BUILDING, DRAINAGE STRUCTURES AND OTHER FIXED STRUCTURES, CONSTRUCTED WITH A 1/2" NONEXTRUDING FILLER, CLOSED-CELL FOLSSONM RUBBER OR A BITUMEN-TREATED FIBER-BOLSSONRD, AND WITH A THICKENED EDGE, INCREASED BY 20 PERCENT, TAPERED TO THE REGULAR THICKNESS IN FEET.
  - ALL EXPANSION JOINTS SHALL BE FILLED AND SEALED WITH A PLASTIC JOINT SEALANT MATERIAL.
- TELEPHONE AND COMMUNICATION SERVICE ROUTING AND CONDUITS NOT SHOWN ON PLANS. CONTRACTOR SHALL INSTALL NECESSARY CONDUIT PRIOR TO PAVEMENT INSTALLATION. CONTRACTOR SHALL COORDINATE ROUTING AND INSTALLATION SCOPE WITH SERVICE PROVIDER.
- ANY CONTRACTOR BIDDING ANY PORTION OF THIS WORK SHALL HAVE IN HIS OR HER POSSESSION A COMPLETE SET OF CONSTRUCTION DOCUMENTS AND BE FAMILIAR WITH ALL SCOPES OF WORK AND TRADES TO UNDERSTAND THEIR INTERACTIONS.
- EXISTING TOPSOIL SHALL BE STRIPPED TO A POINT WHERE ALL VEGETATION IS REMOVED. REFER TO THE GEOTECHNICAL REPORT PROVIDED BY OLSSON DATED 01/09/2019 AND ALL ADDENDUMS.
- SITE PREPARATION, GRADING AND EXCAVATION PROCEDURES SHALL CONFORM TO THE RECOMMENDATIONS AS OUTLINED IN THE GEOTECHNICAL REPORT PREPARED BY OLSSON DATED 01/09/2019 AND ALL ADDENDUMS.

GENERAL UTILITY NOTES

- THE SIZE AND LOCATION OF SERVICES SHALL BE VERIFIED WITH THE ARCHITECTURAL AND MEP PLANS PRIOR TO CONSTRUCTION. IF DISCREPANCIES EXIST, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY.
- IRRIGATION CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING SLEEVING UNDER PAVING AREAS WHERE NECESSARY.
- INSTALL ALL PIPE LENGTHS, BENDS AND FITTINGS NECESSARY FOR UTILITY CONNECTIONS.
- CONTRACTOR SHALL VERIFY ALL CROSSING ELEVATIONS AND LOCATIONS, SIZES, AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO CONSTRUCTION OF STORM LINES AND ALL UTILITY SERVICE CONNECTIONS. ANY CONFLICTS SHALL BE MADE KNOWN TO THE ENGINEER AND RESOLVED PRIOR TO CONSTRUCTION.
- CONTRACTOR TO VERIFY FIRE SERVICE SIZE WITH SPRINKLER DESIGNER/CONTRACTOR PRIOR TO CONSTRUCTION AND INSTALLATION OF METER/BACKFLOW PREVENTER AND SERVICES. NOTIFY ENGINEER OF ALTERATIONS.
- CONTRACTOR RESPONSIBLE FOR PAYING ALL TAP AND CONNECTION FEES AND SHALL CONTRACT AND PAY FOR ANY REQUIRED SUB CONTRACTORS BY UTILITY COMPANIES.
- REFERENCE MEP PLANS FOR BUILDING CONNECTIONS.
- CONTRACTOR TO REPAIR ALL AREA DAMAGED BY CONSTRUCTION TO EXISTING CONDITIONS OR BETTER.
- BACK FLOW PREVENTION TO BE PROVIDED INSIDE BUILDING. SEE MEP AND ARCHITECTURAL PLANS FOR DETAILS.
- LOCATION FOR POWER SHOWN IS APPROXIMATE AND SUBJECT TO CHANGE. CONTRACTOR TO VERIFY FINAL LOCATION AND DESIGN WITH UTILITY COMPANY PRIOR TO CONSTRUCTION.
- CONTRACTOR TO COORDINATE LIGHT POLE LOCATIONS WITH OWNER, STORM SEWER INSTALLATION AND UTILITY COMPANIES PRIOR TO INSTALLATION TO AVOID CONFLICTS. NOTIFY ENGINEER AND ARCHITECT OF ANY CONFLICTS PRIOR TO INSTALLATION.
- WATER METER CANNOT BE INSTALLED IN THE BUILDING.
- CONTRACTOR SHALL COORDINATE CABLE/FIBER OPTIC CONDUIT AND SERVICE INSTALLATION WITH UTILITY COMPANY.
- ALL TAPS AND CONNECTIONS FOR FIRE AND DOMESTIC WATER SERVICES ARE TO BE IN ACCORDANCE WITH THE CITY OF BLUE SPRINGS STANDARDS AND SPECIFICATIONS.
- CONTRACTOR TO COORDINATE POWER ROUTING TO MONUMENT SIGNS NOT SHOWN ON PLANS.
- ALL ROOF DRAIN AND DOWNSPOUT HEADER PIPES SHALL BE 12" HDPE PIPE AND INSTALLED AT 1.00% MINIMUM SLOPE UNLESS OTHERWISE NOTED WITHIN THIS PLAN. ALL BENDS AND FITTINGS NEEDED TO BUILD ROUTING AS SHOWN SHALL BE INCLUDED IN BID.
- CONTRACTOR SHALL PROVIDE AND INSTALL ALL NECESSARY FITTINGS TO COMPLETE ROOF DRAIN AND DOWNSPOUT CONNECTIONS TO BUILDINGS. ALL ROOF DRAIN AND DOWNSPOUT CONNECTIONS / FITTINGS, INCLUDING BUT NOT LIMITED TO BENDS AND TEES, SHALL BE MADE OF HDPE PIPE UNLESS OTHERWISE NOTED WITHIN THIS PLAN.



THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF THE SENATE BILL NUMBER 583, 78TH GENERAL ASSEMBLY OF THE STATE OF MISSOURI. THE BILL REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT-OF-WAY DO SO ONLY AFTER GIVING NOTICE TO, & OBTAINING INFORMATION FROM, UTILITY COMPANIES. STATE LAW REQUIRES 48 HOURS ADVANCE NOTICE. CALL 1-800-DIG-RITE.

SAPP DESIGN ARCHITECTS

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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
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JACKSON COUNTY  
PACKAGE 04



Terry M Parsons, Engineer MO PE-2018010505

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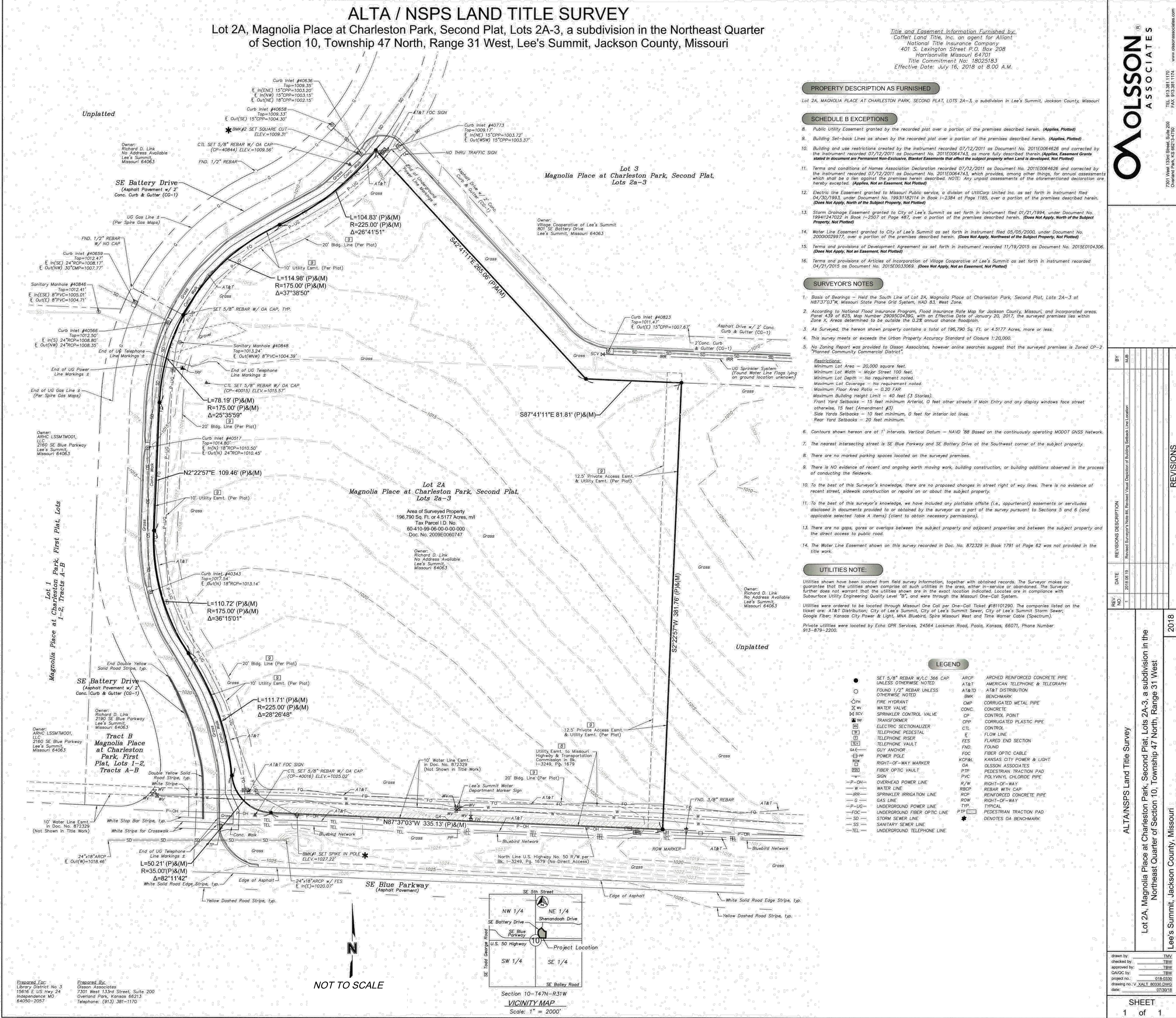
Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No.	Date	Drawn
B18-0330	12.07.18	RLK

Drawing No. C1.1  
GENERAL NOTES



DWG: F:\2018\0001-0500\018-0330-B\40-Design\GNCV\SPRINT\Sheets\2440 SE Blue Parkway EAST Lee's Summit MO - Library\XALT\_80330.dwg USER: trideline  
DATE: Jul 30, 2018 1:30pm XREFS: X-Base-80330



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## helix

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Kansas City, MO 64108 816.300.0300

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## EAST LEE'S SUMMIT BRANCH

Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

Engineer of Record  
**TERRY M. PARSONS**  
NUMBER  
PE-2018016505  
03-26-2019  
PROFESSIONAL ENGINEER

Terry M Parsons, Engineer MO PE-2018016505

# olsson

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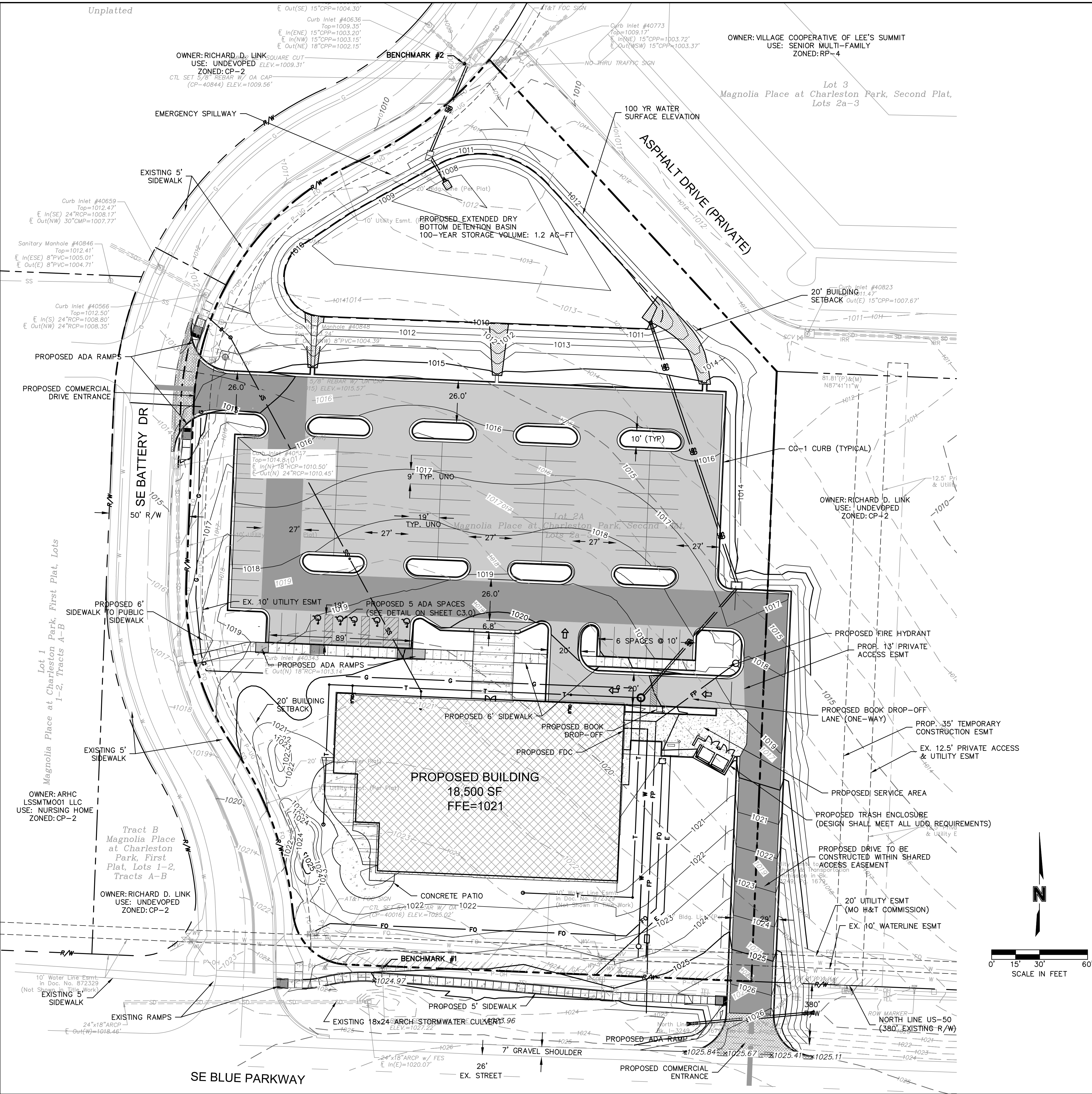
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Missouri State Certificate of Authority #001592

Revision No.	Description	Date
1	AS#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No. B18-0330 Date 12.07.18 Drawn RLK  
Drawing No. C1.2  
SURVEY SHEET

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SITE DATA		
ZONING & SITE AREA		
PROPOSED USE:	PUBLIC LIBRARY	
SITE AREA		ZONING
LOT 2 (AS DESCRIBED):	3.80 ACRES (165,528 SF)	CP-2
IMPERVIOUS:	1.71 ACRES (74,488 SF) (45%)	
PERVIOUS:	2.09 ACRES (91,040 SF) (55%)	
FAR (0.55 MAX):	0.15	
BUILDING AREA		
BUILDING TYPE	# STORIES	SQUARE FOOTAGE
BUILDING	1	18,500 SF
PARKING		
USE	REQUIRED	PROVIDED
LIBRARY	4 PER 1000 SF = 74	83
ADA	3 (PER CITY TABLE)	5
TOTAL	74	88 (INCLUDING ADA)

NOTE:  
ACCORDING TO MDNR STATE OIL & GAS COUNSEL THERE ARE NO OIL AND GAS WELLS LOCATED WITHIN OR ADJACENT TO THE PROPERTY.

#### EXISTING CONDITIONS LEGEND

- PROPERTY LINES
- RIGHT-OF-WAY LINES
- EASEMENT LINES
- BUILDING SETBACK LINES
- OVERHEAD ELECTRIC
- UNDERGROUND ELECTRIC
- UNDERGROUND TELEPHONE
- UNDERGROUND FIBER OPTIC
- GAS LINE
- WATER LINE
- STORM SEWER LINE
- SANITARY SEWER LINE
- GRADE CONTOURS

#### PROPOSED CONDITIONS LEGEND

- PROPOSED UNDERGROUND ELECTRIC
- PROPOSED FIBER OPTIC
- PROPOSED WATER LINE
- PROPOSED FIRE PROTECTION LINE
- PROPOSED STORM SEWER LINE
- PROPOSED TURF DRAIN LINE
- PROPOSED SANITARY SEWER SERVICE
- CONCRETE CURB & GUTTER
- PROPOSED BUILDING
- PROPOSED CONCRETE SIDEWALK
- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED GRADE CONTOURS
- PROPOSED FIRE ACCESS ROAD

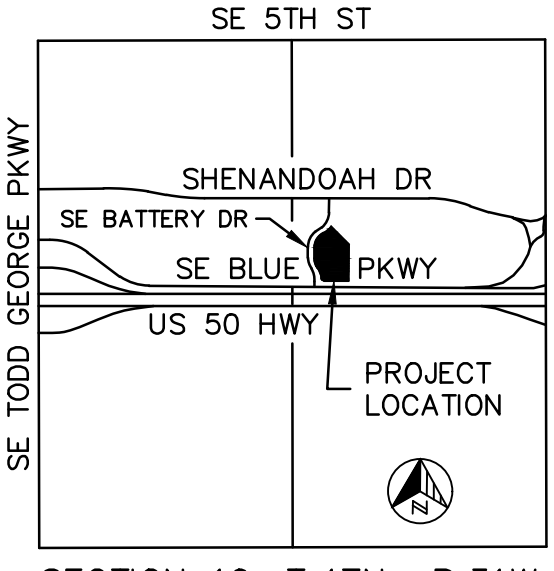
#### BENCHMARKS:

BENCHMARK #1:  
ELEVATION=1027.22'  
SET RAILROAD SPIKE IN THE NORTH SIDE OF A POWER POLE, 66'± NORTH OF THE CL OF SE BLUE PARKWAY, 86' EAST OF THE CL OF SE BATTERY DRIVE.

BENCHMARK #2:  
ELEVATION=1009.31'  
SET CHISELED "C" CUT ON THE SOUTHWESTERLY CORNER OF A CONCRETE CURB INLET #40636, 630'± NORTH OF THE CL OF SE BLUE PARKWAY, 15' EAST OF THE CL OF SE BATTERY DRIVE.

#### LEGAL DESCRIPTION:

LOT 2A, MAGNOLIA PLACE AT CHARLESTON PARK, SECOND PLAT, LOTS 2A-3, A SUBDIVISION IN LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, EXCEPT THE EAST 81.81 FEET THEREOF, CONTAINING 165,561 SQUARE FEET OR 3.8008 ACRES, MORE OR LESS.



SECTION 10, T.47N., R.31W.  
VICINITY MAP  
SCALE: 1" = 2000'

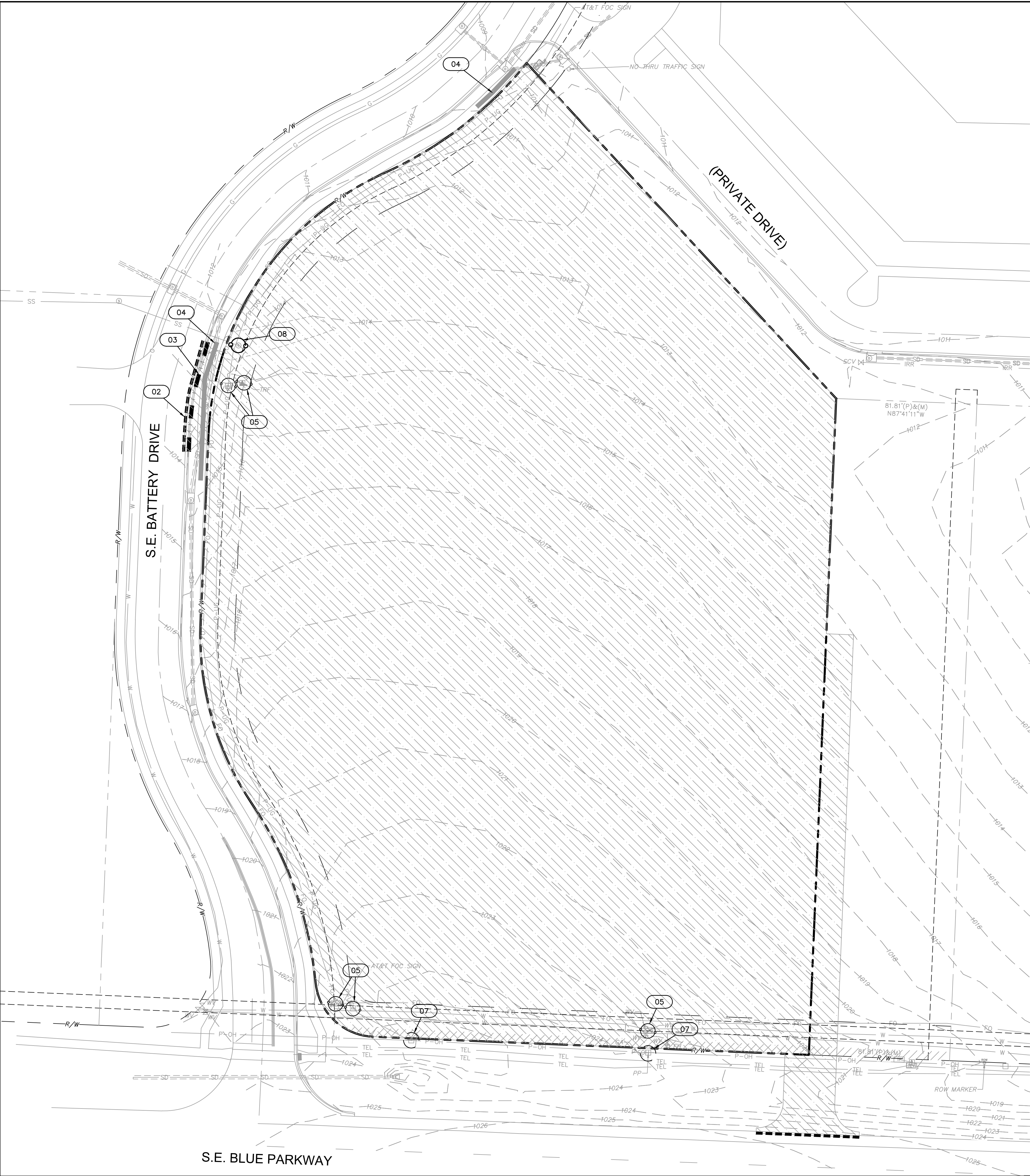
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#### DEMOLITION NOTES

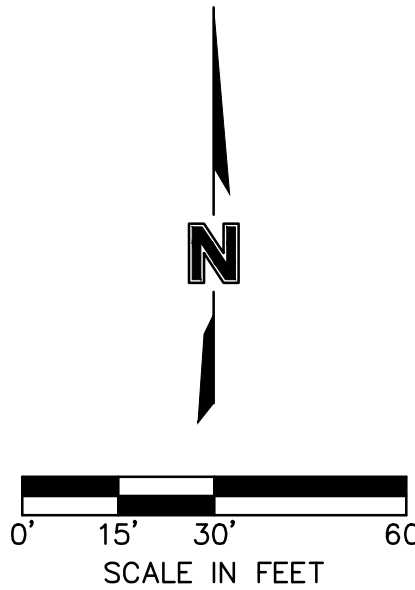
1. CONTRACTOR TO PRESERVE ALL SURVEY CONTROL.
2. CONTRACTOR TO COMPLETE DEMOLITION PER THE INTENT OF THESE PLANS.
3. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE ENGINEER MAKES NO GUARANTEES THAT THE UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE ENGINEER HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. THIS INCLUDES PRIVATE AND PUBLIC UTILITIES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT MISSOURI ONE CALL AT 1-800-344-7483 IN ADVANCE OF ANY EXCAVATION TO COORDINATE UTILITY LOCATIONS.
4. CONTRACTOR IS REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN AND ANY OTHER EXISTING LINES NOT OF RECORD OR SHOWN ON THESE PLANS.
5. CONTRACTOR TO REMOVE ALL TREES AND BRUSH FOR INSTALLATION OF IMPROVEMENTS.
6. REMOVAL AND DISPOSAL OF BUSHES AND TREES SMALLER THAN 12" IN DIAMETER SHALL BE CONSIDERED SUBSIDIARY TO THE PRICE BID FOR CLEARING AND GRUBBING.
7. ALL ITEMS REMOVED SHALL BE LEGALLY DISPOSED OF OFF SITE BY THE CONTRACTOR.
8. DO NOT DISRUPT UTILITY SERVICE TO ADJACENT BUSINESSES OR RESIDENCES WITHOUT PRIOR WRITTEN APPROVAL BY THE ENGINEER.
9. DO NOT DISRUPT TRAFFIC ON ADJACENT PUBLIC STREETS WITHOUT PRIOR WRITTEN APPROVAL BY THE CITY.
10. ALL SIDEWALK AND PAVEMENT TO REMAIN SHALL BE PROTECTED IN PLACE INCLUDING PROTECTION FROM DAMAGE CAUSED BY REMOVAL OF ABUTTING PAVEMENT. CONTRACTOR SHALL SAW CUT WHERE NECESSARY.
11. CONTRACTOR SHALL GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DISCONNECTION, DEMOLITION, AND REMOVAL OF SERVICE LINES. CAP ALL LINES BEFORE PROCEEDING WITH WORK ON THIS CONTRACT.
12. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANIES WORK FORCE AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES.
13. CONTRACTOR SHALL PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC. TO THE BEST PRACTICES AND AS APPROVED BY THE ENGINEER AND THE CITY.
14. DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
15. DEMOLITION OF BUILDINGS SHALL INCLUDE THE BUILDING STRUCTURE, PAD, FOOTINGS, FOUNDATIONS, BASEMENT WALLS, BASEMENT FLOORS, TRUCK DOCKS, STEPS, DECKS, ALL ITEMS REMAINING IN BUILDING, ALL BUILDING UTILITY SERVICES, SIDEWALKS, AND BACKFILLING AND RESTORING REMAINING EXCAVATIONS, BASEMENTS AND TRENCHES PER SPECIFICATIONS. ALL REMOVALS SHALL INCLUDE BUT NOT BE LIMITED TO ITEMS SUCH AS BUILDING FOOTINGS, SUBSURFACE STRUCTURES, SURFACE STRUCTURES, DRAINAGE STRUCTURES, PIPES, PAVEMENT OF ALL TYPES, CURB & GUTTER, SIDEWALK, ROCK, ABANDONED UTILITIES AS DIRECTED BY THE ENGINEERS, TREES, TREE ROOTS, ANY UNKNOWN MATERIALS, AND UNSUITABLE SUBGRADE MATERIALS. THE SITE SHOULD BE CLEARED FOR THE PROPOSED IMPROVEMENTS PER CITY REQUIREMENTS.
16. ALL LIGHT POLE DEMOLITION SHALL INCLUDE FIXTURES, BASES AND WIRING.
17. ALL UTILITY DEMOLITION SHALL INCLUDE METERS, MANHOLES AND OTHER STRUCTURES ASSOCIATED WITH THE UTILITY SERVICE LINE.

#### KEY NOTE LEGEND

01	REMOVE EXISTING INFRASTRUCTURE, BUILDINGS, VEGETATION AND PAVEMENT FOR THE PURPOSE OF PROJECT CONSTRUCTION.
02	SAWCUT EXISTING PAVEMENT
03	REMOVE EXISTING CURB & GUTTER
04	REMOVE EXISTING SIDEWALK
05	PROTECT EXISTING UTILITY EQUIPMENT/ MANHOLE
06	INSTALL TREE PROTECTION. SEE DETAIL ON C6.5
07	COORDINATE DEMOLITION WITH POWER COMPANY
08	PROTECT EXISTING SANITARY SEWER TO REMAIN

#### LEGEND

----- PROPERTY LINE



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DESIGN  
ARCHITECTS**

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Sapp Design Associates Architects, P.C.  
Missouri State Certificate of Authority #000607

**helix.**

1629 Walnut  
Kansas City, MO 64108 816.300.0300

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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
**EAST LEE'S SUMMIT BRANCH**  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE  
**04**

Engineer of Record



Terry M Parsons, Engineer MO PE-2018010505

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Revision No.	Description	Date
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2	RESPONSE TO CITY COMMENTS	03.26.19

Project No. B18-0330 Date 12.07.18 Drawn RLK

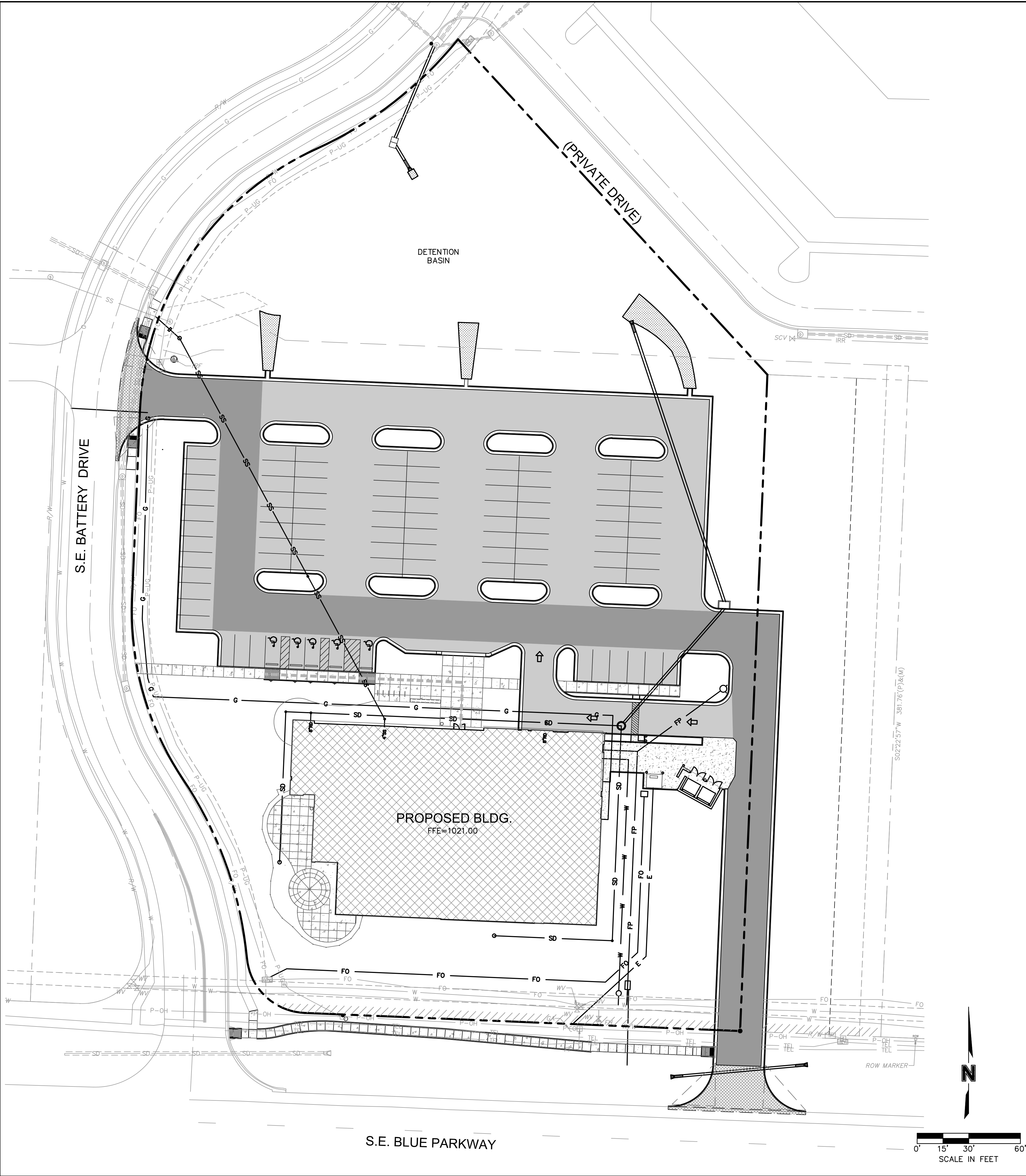
Drawing No. **C2.0**

DEMOLITION PLAN

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### LEGEND

- PROPERTY LINE
- CONCRETE CURB & GUTTER TYPE "CG-1"
- CONCRETE CURB & GUTTER TYPE "CG-1 DRY"
- SAWCUT PAVEMENT FULL DEPTH
- COMMERCIAL ENTRANCE  
8" PCC CONCRETE PAVEMENT
- 6" PCC CONCRETE PAVEMENT
- CONCRETE SIDEWALK PAVEMENT
- ASPHALT PAVEMENT – HEAVY DUTY
- ASPHALT PAVEMENT– PARKING AREAS
- ADA ACCESSIBLE ROUTE

### \*BID ALTERNATES

- BID ALTERNATE 1 –  
THE SUBSTITUTION OF LIGHT DUTY ASPHALT PAVEMENT FOR HEAVY DUTY ASPHALT PAVEMENT IN PARKING DRIVE LANES.
- BID ALTERNATE 2 –  
THE PLACEMENT OF TENSAR TRIAX TX GEOGRID BETWEEN THE SUBGRADE AND BASE ASPHALT LAYER UNDER ALL ASPHALT SURFACING INSTALLED PER MANUFACTURES RECOMMENDATIONS.
- BID ALTERNATE 3 –  
THE SUBSTITUTION OF NON-TINTED CONCRETE FOR INTEGRALLY TINTED CONCRETE IN THE PEDESTRIAN PATIO AREA.

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## Mid-Continent Public Library CONSTRUCTION DOCUMENT PLANS FOR EAST LEE'S SUMMIT BRANCH

PACKAGE  
**04**

2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

Engineer of Record



Terry M Parsons, Engineer MO PE-2018010505

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Drawing No.

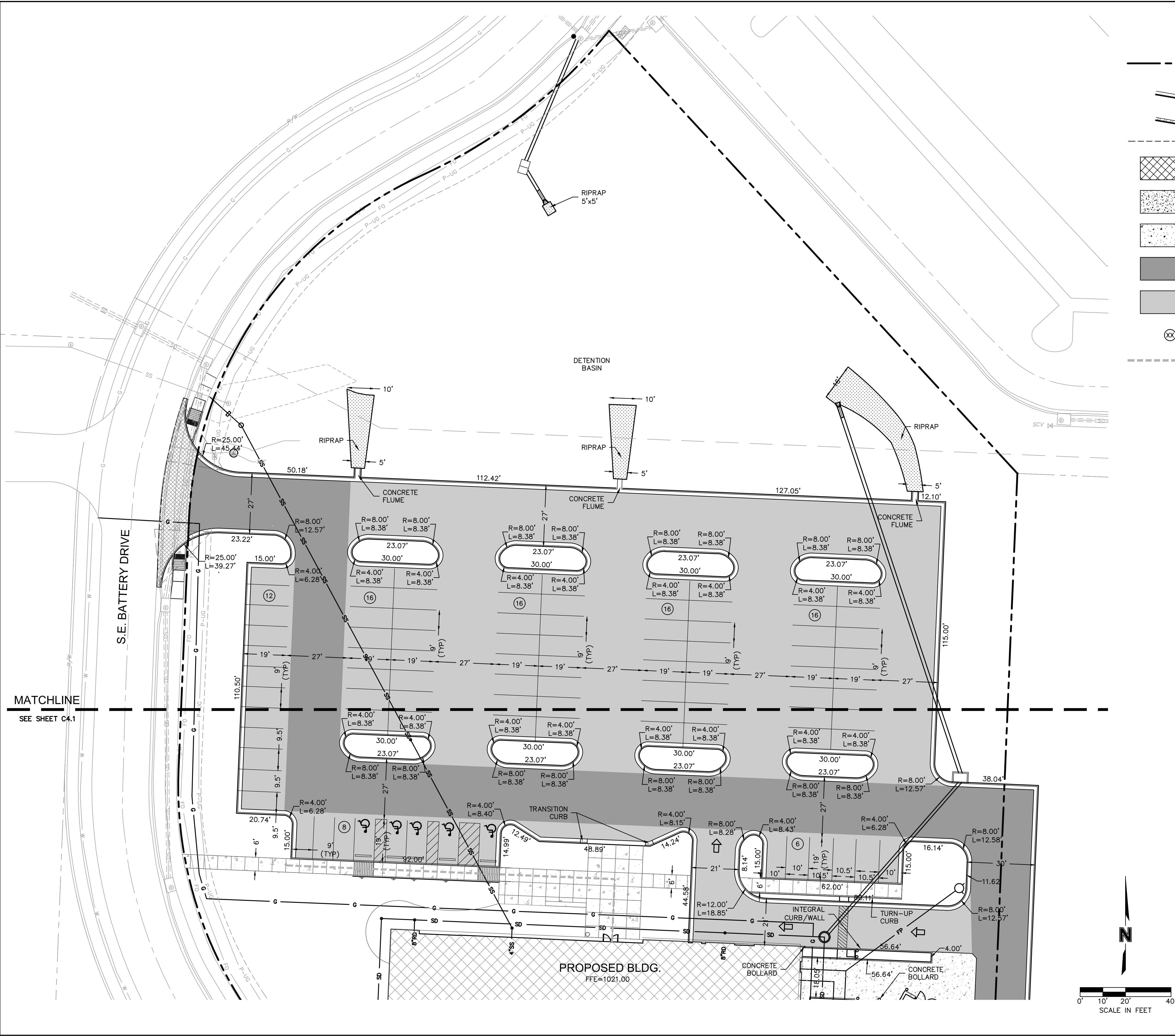
## C3.0

GENERAL LAYOUT PLAN

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### LEGEND

- PROPERTY LINE
- CONCRETE CURB & GUTTER TYPE "CG-1"
- CONCRETE CURB & GUTTER TYPE "CG-1 DRY"
- SAWCUT PAVEMENT FULL DEPTH
- COMMERCIAL ENTRANCE 8" PCC CONCRETE PAVEMENT
- 6" PCC CONCRETE PAVEMENT
- CONCRETE SIDEWALK PAVEMENT
- ASPHALT PAVEMENT - HEAVY DUTY
- ASPHALT PAVEMENT - PARKING AREAS
- PARKING STALLS
- ADA ACCESSIBLE ROUTE

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## EAST LEE'S SUMMIT BRANCH

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JACKSON COUNTY

PACKAGE  
**04**

Engineer of Record



Terry M Parsons, Engineer MO PE-2018010505

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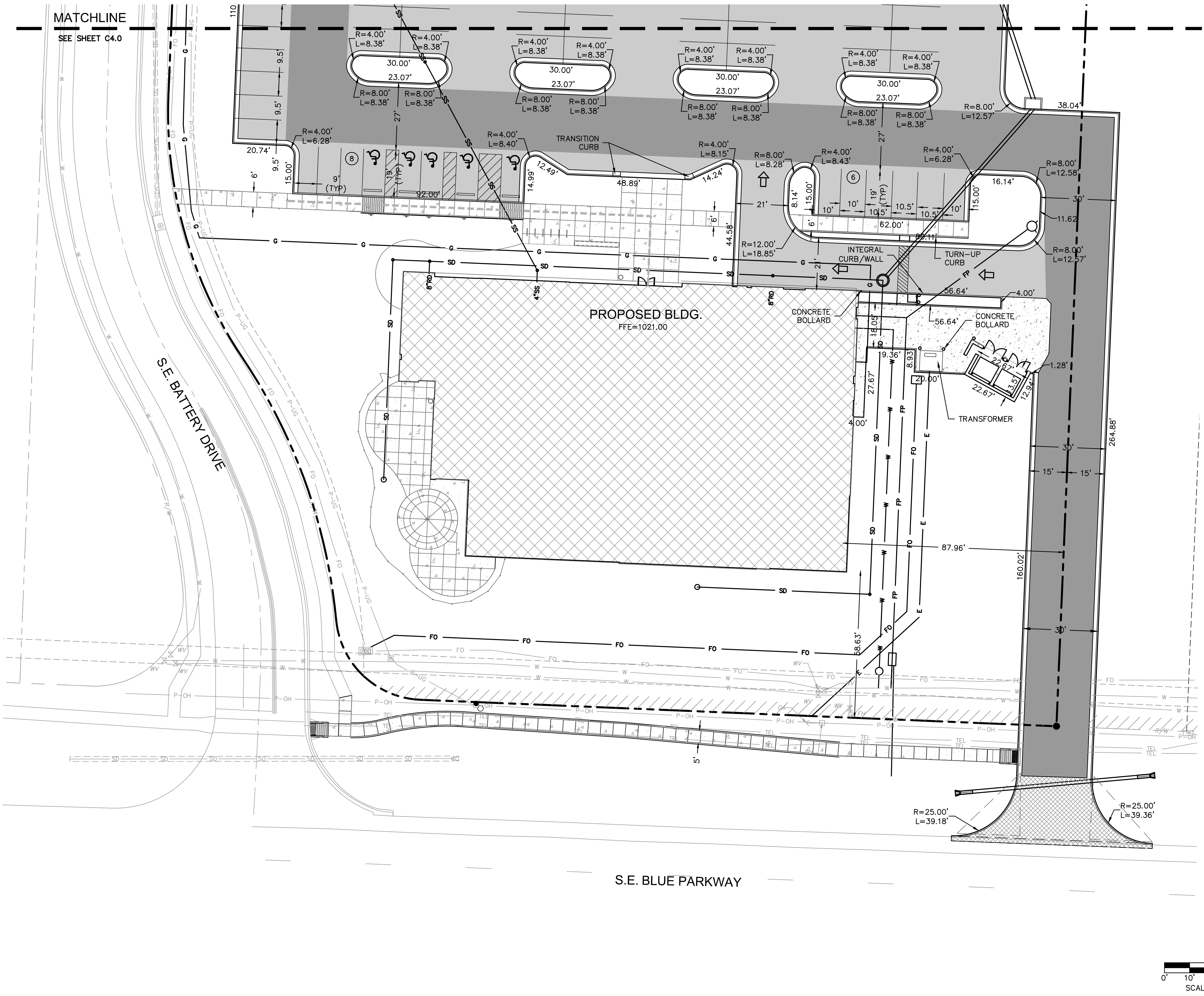
Project No. B18-0330 Date 12.07.18 Drawn RLK

Drawing No. **C4.0**

SITE DIMENSION PLAN

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**LEGEND**

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- CONCRETE CURB & GUTTER TYPE "CG-1 DRY"
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- ASPHALT PAVEMENT - PARKING AREAS
- PARKING STALLS
- ADA ACCESSIBLE ROUTE

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PACKAGE **04**

Engineer of Record

**TERRY M. PARSONS**  
NUMBER  
PE-2018010505  
03-26-2019  
PROFESSIONAL ENGINEER

Terry M Parsons, Engineer MO PE-2018010505

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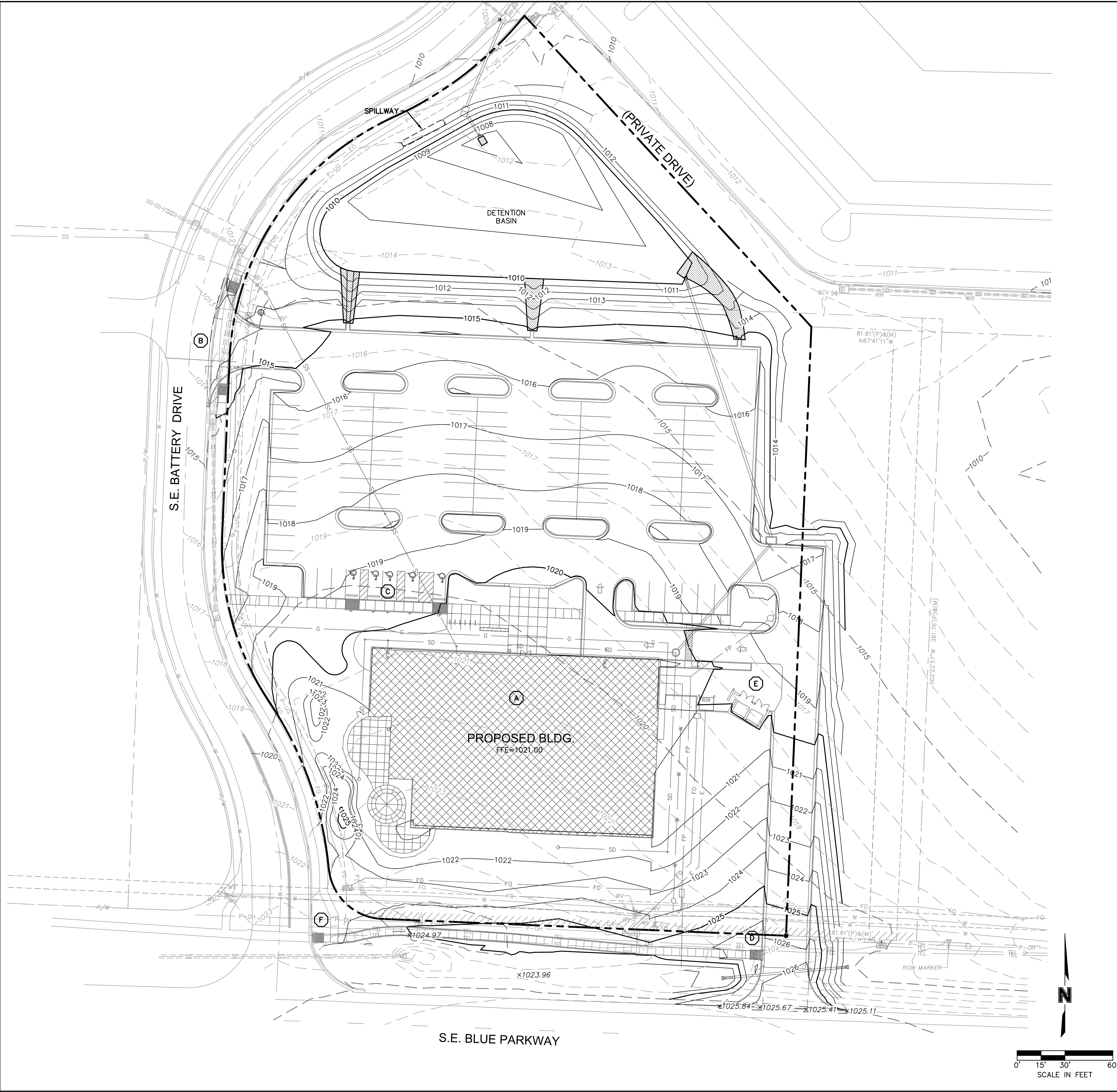
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**SITE DIMENSION PLAN**

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**LEGEND**

--- PROPERTY LINE  
--- 995 --- EXISTING MAJOR CONTOUR  
--- 995 --- EXISTING MINOR CONTOUR  
--- 998 --- PROPOSED MAJOR CONTOUR  
--- 998 --- PROPOSED MINOR CONTOUR

RAMP

CONCRETE CURB & GUTTER TYPE "CG-1"

CONCRETE CURB & GUTTER TYPE "CG-1 DRY"

CONCRETE CURB TYPE "C-1" MODIFIED

GRADING DETAIL DESIGNATION RE: SHEET C403 THRU C408

- NOTES:**
1. CONTRACTOR TO REMOVE AND REPLACE ALL SIDEWALK NECESSARY FOR CONNECTION TO EXISTING.
  2. ALL ADA ACCESSIBLE SIDEWALK CROSS SLOPES SHALL HAVE A MAXIMUM CROSS SLOPE OF 2.00% AND MAXIMUM LONGITUDINAL SLOPE OF 5.00%.
  3. ALL ADA ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2.00% IN ANY DIRECTION.

**SPOT ELEVATION LEGEND**

ALL SPOT ELEVATIONS ARE TOP OF PAVEMENT ELEVATION UNLESS NOTED OTHERWISE. RE: PLAN VIEW, LEGEND AND DETAILS FOR CURB TYPE AND TO CALCULATE TOP OF CURB ELEVATION.

TC= TOP OF CURB  
FG= FINISHED GRADE WITHIN GREENSPACE  
TS= TOP OF STRUCTURE  
TP=TC= CURB DEPRESSED TO BE FLUSH WITH ADJACENT PAVEMENT

HP= HIGH POINT  
LP= LOW POINT  
MATCH EX.= MATCH EXISTING  
FFE= FINISH FLOOR ELEVATION AT TOP OF SLAB

SAPP  
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2240 SE BLUE PARKWAY  
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JACKSON COUNTY

PACKAGE  
**04**

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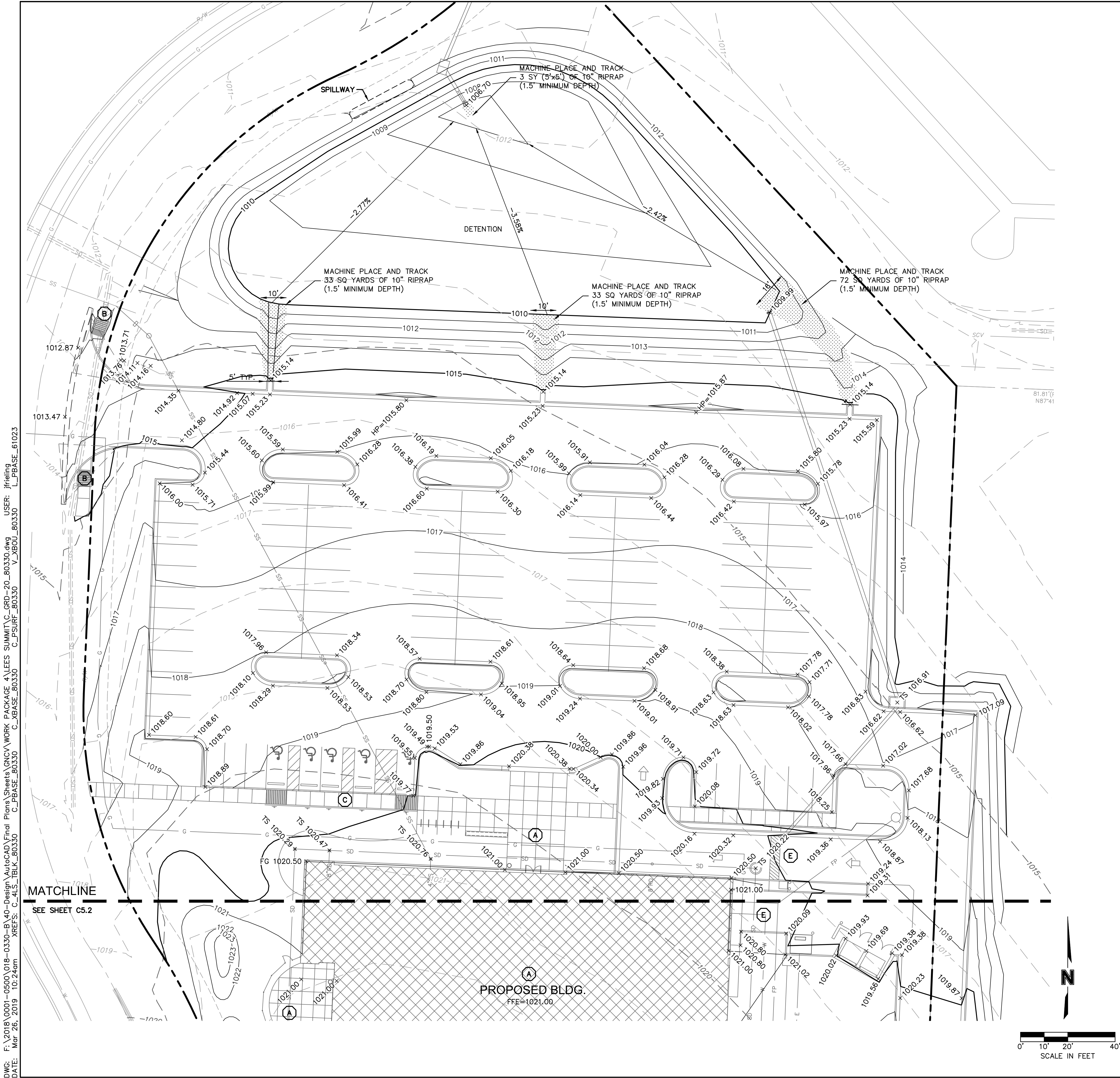
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Drawing No.  
**C5.0**  
GRADING PLAN

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**LEGEND**

--- PROPERTY LINE

-999- EXISTING GRADE CONTOUR

-999- FINISHED GRADE CONTOUR

--- RIDGE --- RIDGE LINE

--- VALLEY --- VALLEY LINE

[Hatched Box] RAMP

[Curb Section] CONCRETE CURB & GUTTER TYPE "CG-1"

[Curb Section] CONCRETE CURB & GUTTER TYPE "CG-1 DRY"

[Curb Section] CONCRETE CURB TYPE "C-1" MODIFIED

(X) GRADING DETAIL DESIGNATION RE: SHEET C5.1 THRU C5.5

- NOTES:**
1. CONTRACTOR TO REMOVE AND REPLACE ALL SIDEWALK NECESSARY FOR CONNECTION TO EXISTING.
  2. ALL ADA ACCESSIBLE SIDEWALK CROSS SLOPES SHALL HAVE A MAXIMUM CROSS SLOPE OF 2.00% AND MAXIMUM LONGITUDINAL SLOPE OF 5.00%.
  3. ALL ADA ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2.00% IN ANY DIRECTION.

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TC= TOP OF CURB

FG= FINISHED GRADE WITHIN GREENSPACE

TS= TOP OF STRUCTURE

TP=TC= CURB DEPRESSED TO BE FLUSH WITH ADJACENT PAVEMENT

HP= HIGH POINT

LP= LOW POINT

MATCH EX.= MATCH EXISTING

FFE= FINISH FLOOR ELEVATION AT TOP OF SLAB

SPOT GRADES AT OVERHEAD DOORS REPRESENT A 2" LAG/DROP FROM FINISH FLOOR TO EXTERIOR PAVEMENT. RE: C-06

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Mid-Continent Public Library

CONSTRUCTION DOCUMENT PLANS FOR

## EAST LEE'S SUMMIT BRANCH

2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE **04**

Engineer of Record

Terry M Parsons, Engineer MO PE-2018010505

## olsson

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Olsson  
Missouri State Certificate of Authority #001592

Revision No.	Description	Date
1	ASI #1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No.	Date	Drawn
B18-0330	12.07.18	RLK

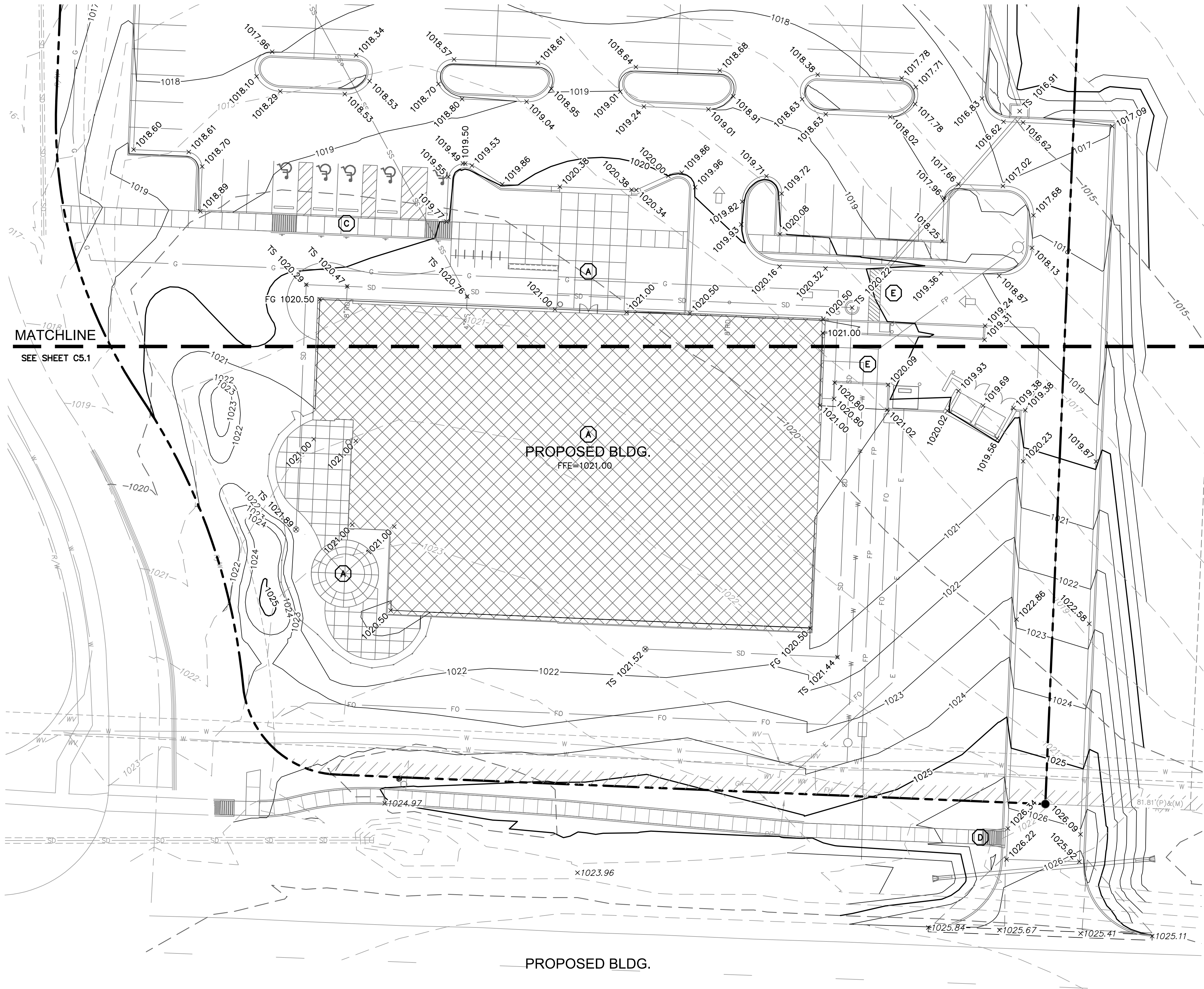
Drawing No. **C5.1**

**GRADING DETAILS**

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DATE: Mar 26, 2019 10:24am XREFS: C\_41S\_TBLK\_80330 C\_PSURF\_80330 C\_XBASE\_80330 C\_PBASE\_80330 V\_XBOU\_80330



## LEGEND

	PROPERTY LINE
	EXISTING GRADE CONTOUR
	FINISHED GRADE CONTOUR
	RIDGE LINE
	VALLEY LINE
	RAMP
	CONCRETE CURB & GUTTER TYPE "CG-1"
	CONCRETE CURB & GUTTER TYPE "CG-1 DRY"
	CONCRETE CURB TYPE "C-1" MODIFIED
	GRADING DETAIL DESIGNATION RE: SHEET C5.1 THRU C5.5

## NOTES:

- CONTRACTOR TO REMOVE AND REPLACE ALL SIDEWALK NECESSARY FOR CONNECTION TO EXISTING.
- ALL ADA ACCESSIBLE SIDEWALK CROSS SLOPES SHALL HAVE A MAXIMUM CROSS SLOPE OF 2.00% AND MAXIMUM LONGITUDINAL SLOPE OF 5.00%.
- ALL ADA ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2.00% IN ANY DIRECTION.

## SPOT ELEVATION LEGEND

ALL SPOT ELEVATIONS ARE TOP OF PAVEMENT ELEVATION UNLESS NOTED OTHERWISE. RE: PLAN VIEW, LEGEND AND DETAILS FOR CURB TYPE AND TO CALCULATE TOP OF CURB ELEVATION.

TC=	TOP OF CURB
FG=	FINISHED GRADE WITHIN GREENSPACE
TS=	TOP OF STRUCTURE
TP=TC=	CURB DEPRESSED TO BE FLUSH WITH ADJACENT PAVEMENT
HP=	HIGH POINT
LP=	LOW POINT
MATCH EX.=	MATCH EXISTING
FFE=	FINISH FLOOR ELEVATION AT TOP OF SLAB

SPOT GRADES AT OVERHEAD DOORS REPRESENT A 2" LAG/DROP FROM FINISH FLOOR TO EXTERIOR PAVEMENT. RE: C-06

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Sapp Design Associates Architects, P.C.  
Missouri State Certificate of Authority #000607

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Drawing No.

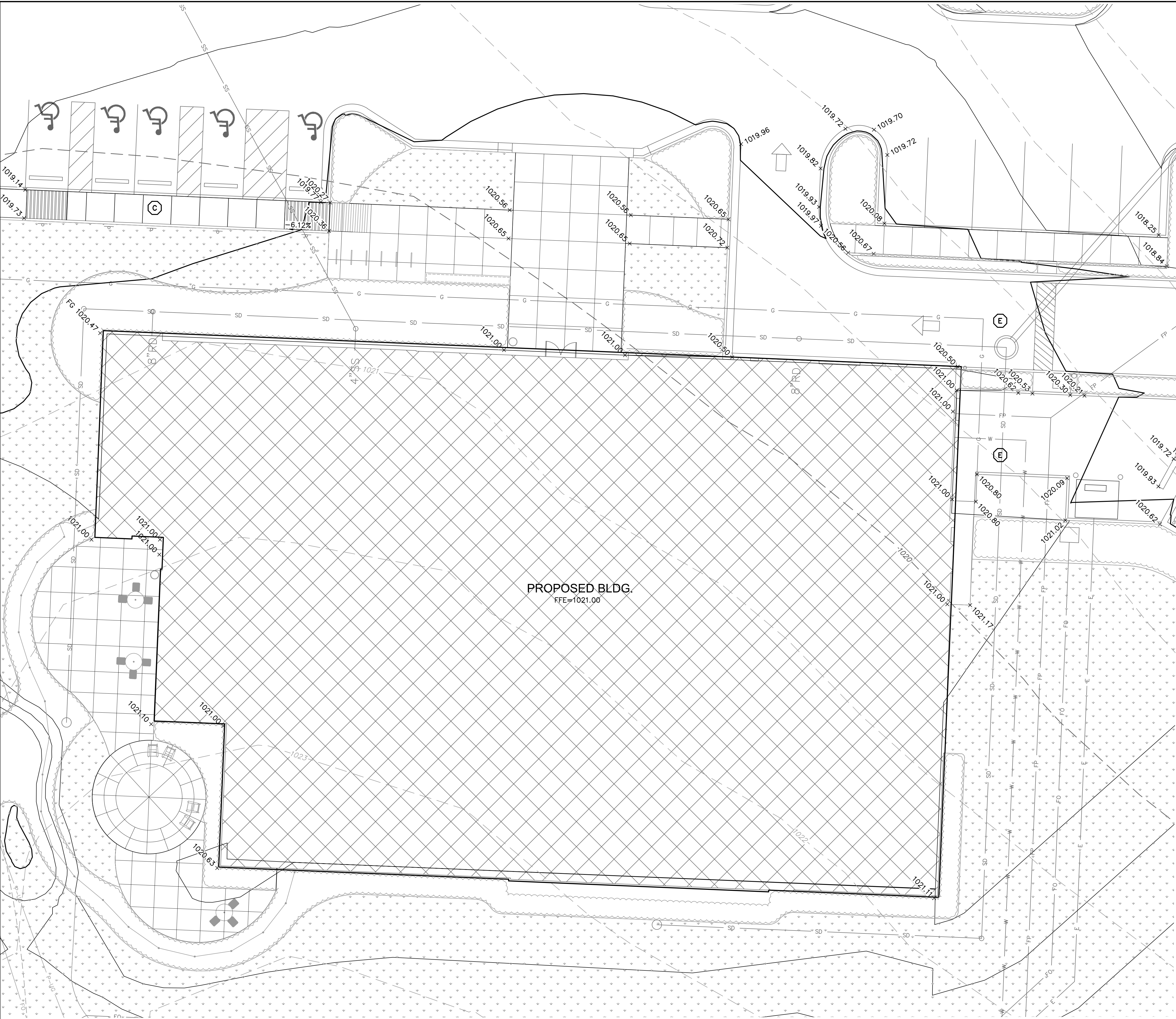
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DETAIL A

LEGEND

- PROPERTY LINE
- EXISTING GRADE CONTOUR
- FINISHED GRADE CONTOUR
- RIDGE LINE
- VALLEY LINE
- RAMP
- CONCRETE CURB & GUTTER TYPE "CG-1"
- CONCRETE CURB & GUTTER TYPE "CG-1 DRY"
- CONCRETE CURB TYPE "C-1" MODIFIED
- GRADING DETAIL DESIGNATION RE: SHEET C5.1 THRU C5.5

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SPOT GRADES AT OVERHEAD DOORS REPRESENT A 2" LAG/DROP FROM FINISH FLOOR TO EXTERIOR PAVEMENT. RE: C-06



0' 5' 10' 20'  
SCALE IN FEET

SAPP  
DESIGN  
ARCHITECTS

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Springfield, MO 65804 417.877.9600

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Kansas City, MO 64108 816.300.0300

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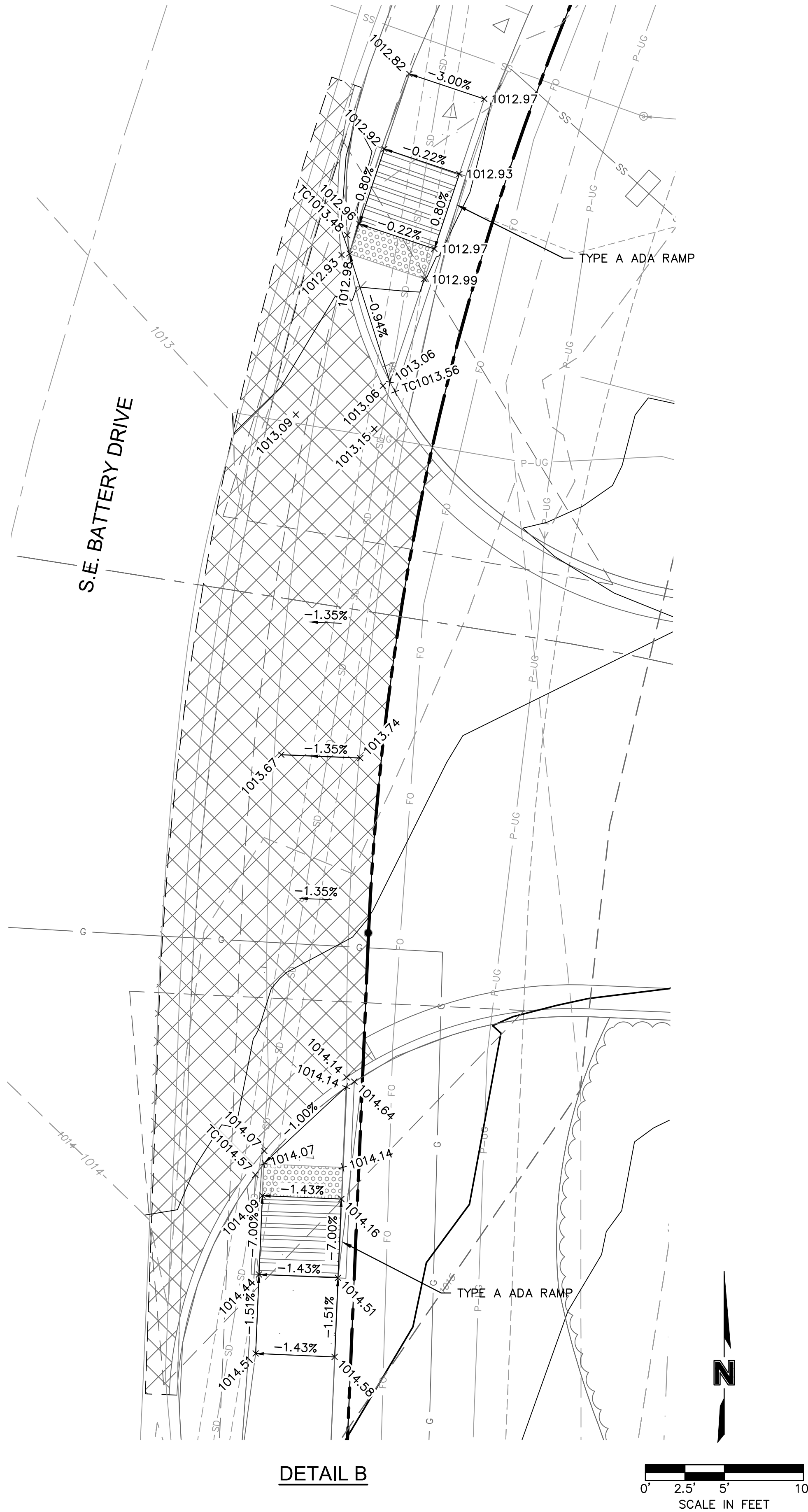
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B18-0330	12.07.18	RLK

Drawing No.  
**C5.3**  
GRADING DETAILS

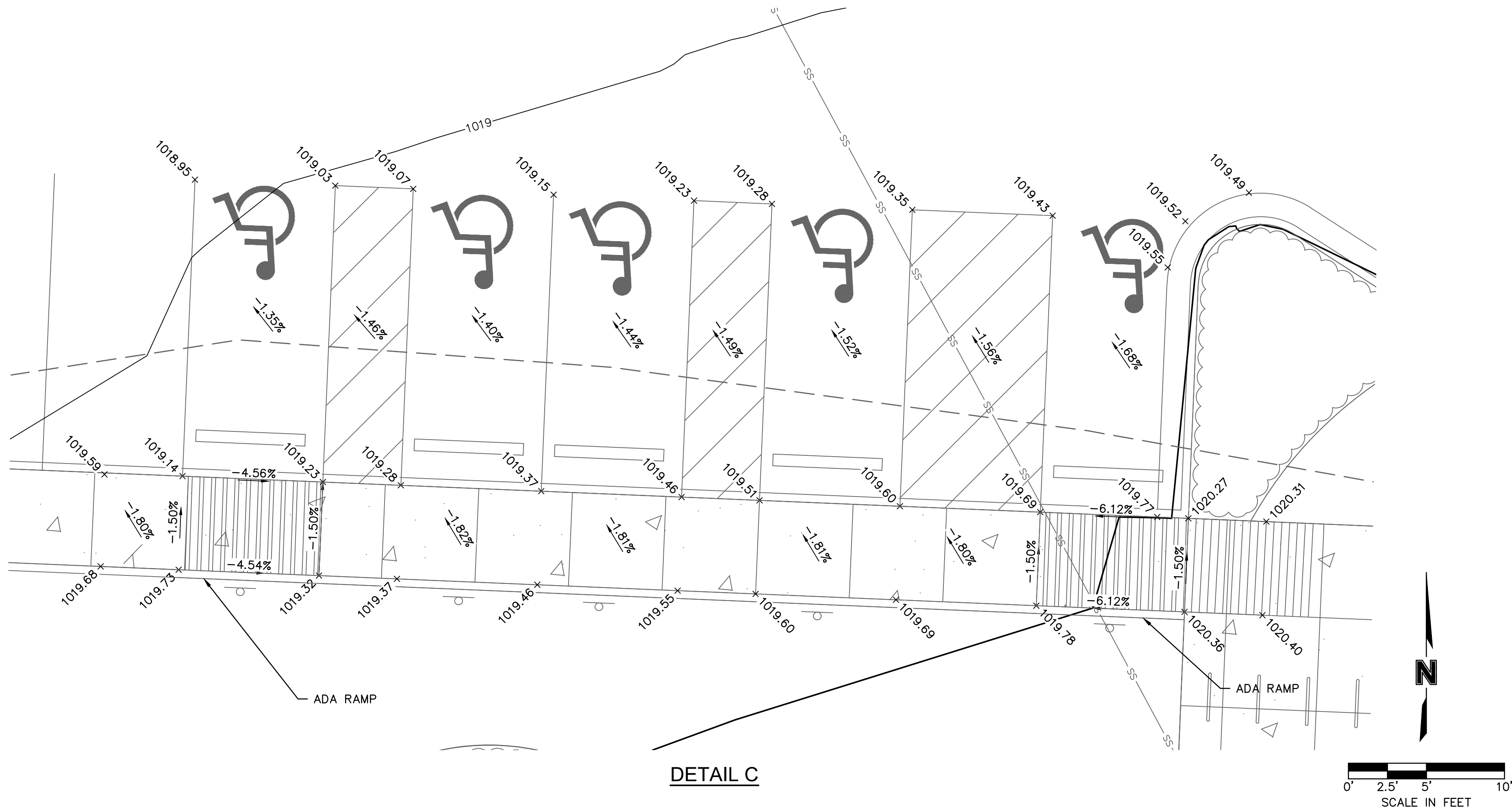
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DETAIL B



DETAIL C

LEGEND

	PROPERTY LINE
	EXISTING GRADE CONTOUR
	FINISHED GRADE CONTOUR
	RIDGE LINE
	VALLEY LINE
	RAMP
	CONCRETE CURB & GUTTER TYPE "CG-1"
	CONCRETE CURB & GUTTER TYPE "CG-1 DRY"
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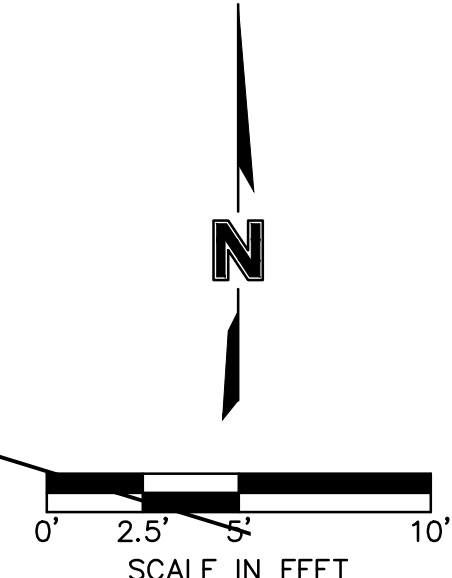
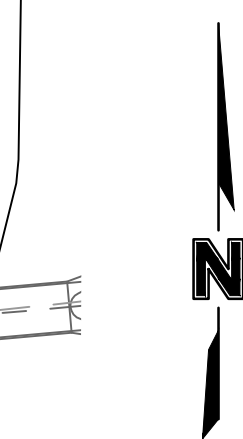
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Project No.	Date	Drawn
B18-0330	12.07.18	RLK

Drawing No.  
**C5.4**  
GRADING DETAILS

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SPOT GRADES AT OVERHEAD DOORS REPRESENT A 2"  
LAG/DROP FROM FINISH FLOOR TO EXTERIOR  
PAVEMENT. RE: C-06

PROPERTY LINE

EXISTING GRADE CONTOUR

FINISHED GRADE CONTOUR

RIDGE LINE

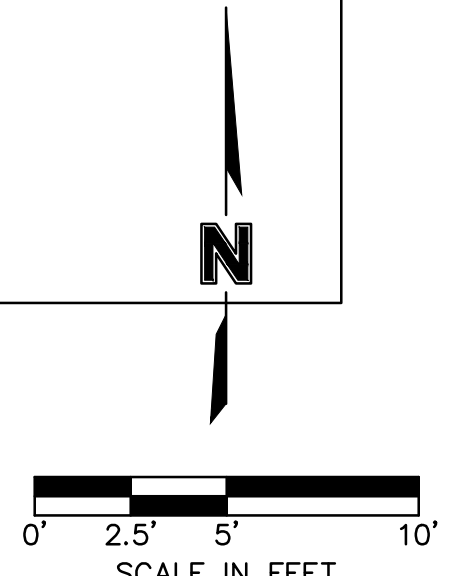
VALLEY LINE

RAMP

CONCRETE CURB & GUTTER TYPE "CG-1"

CONCRETE CURB & GUTTER TYPE "CG-1" DRY"

CONCRETE CURB TYPE "C-1" MODIFIED



0' 2.5' 5' 10'

SCALE IN FEET

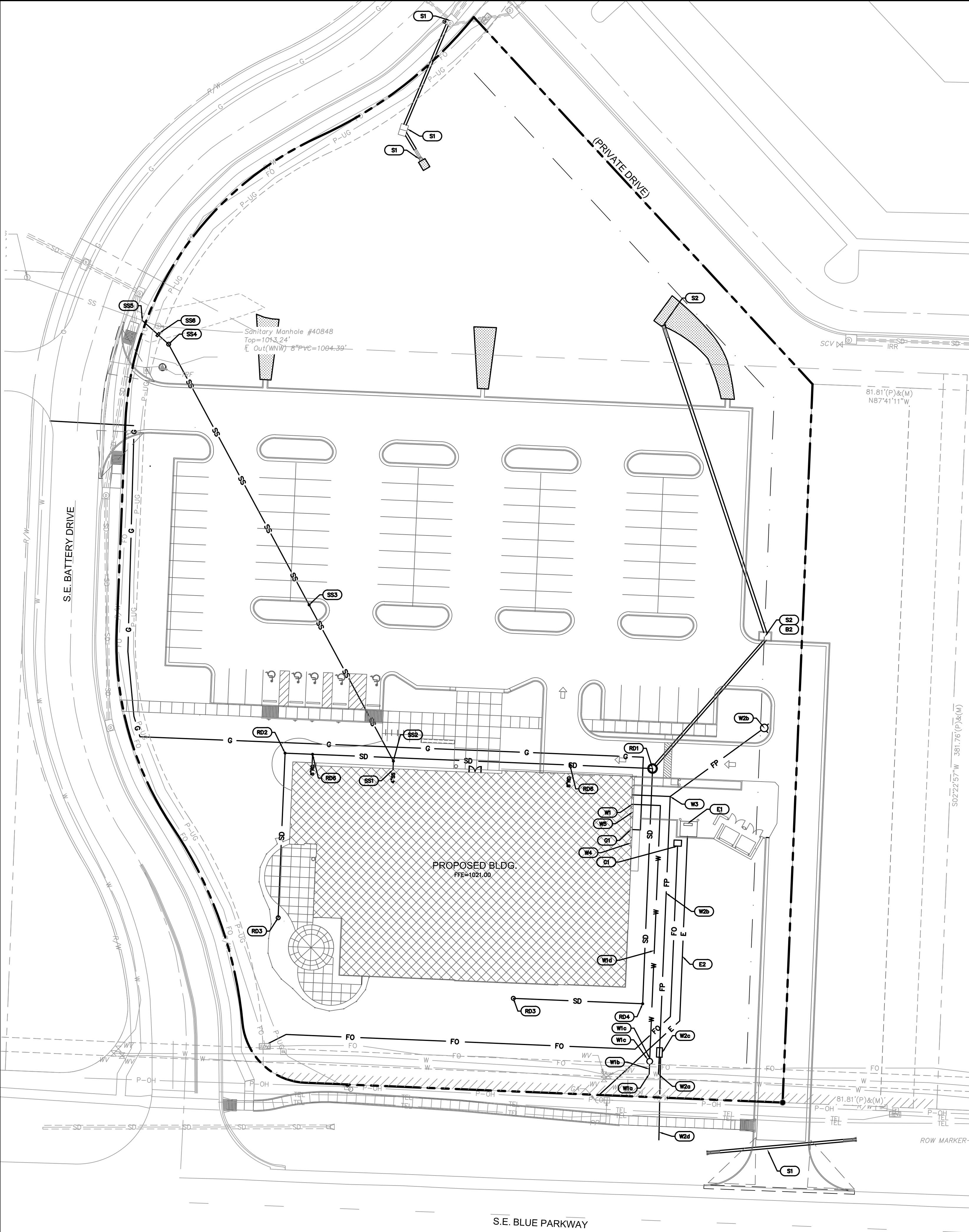
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DATE: Mar 26, 2019 10:27am XREFS: C-41S-TBLK\_80330 C-PBASE\_80330 V-XBOL\_80330 C-XBASE\_80330 L-PBASE\_61023



UTILITY NOTES:

1. THE SIZE AND LOCATION OF SERVICES SHALL BE VERIFIED WITH THE ARCHITECTURAL AND MEP PLANS PRIOR TO CONSTRUCTION. IF DISCREPANCIES EXIST, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY.
2. IRRIGATION CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING SLEEVING UNDER PAVING AREAS WHERE NECESSARY.
3. INSTALL ALL PIPE LENGTHS, BENDS AND FITTINGS NECESSARY FOR UTILITY CONNECTIONS.
4. CONTRACTOR SHALL VERIFY ALL CROSSING ELEVATIONS AND LOCATIONS, SIZES, AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO CONSTRUCTION OF STORM LINES AND ALL UTILITY SERVICE CONNECTIONS. ANY CONFLICTS SHALL BE MADE KNOWN TO THE ENGINEER AND RESOLVED PRIOR TO CONSTRUCTION.
5. CONTRACTOR TO VERIFY FIRE SERVICE SIZE WITH SPRINKLER DESIGNER/CONTRACTOR PRIOR TO CONSTRUCTION AND INSTALLATION OF METER/BACKFLOW PREVENTOR AND SERVICES. NOTIFY ENGINEER OF ALTERATIONS.
6. CONTRACTOR RESPONSIBLE FOR PAYING ALL TAP AND CONNECTION FEES AND SHALL CONTRACT AND PAY FOR ANY REQUIRED SUB CONTRACTORS BY UTILITY COMPANIES.
7. CONTRACTOR TO REPAIR ALL AREA DAMAGED BY CONSTRUCTION TO EXISTING CONDITIONS OR BETTER.
8. BACKFLOW PREVENTION TO BE PROVIDED INSIDE BUILDING. SEE MEP AND ARCHITECTURAL PLANS FOR DETAILS.
9. LOCATION FOR POWER SHOWN IS APPROXIMATE AND SUBJECT TO CHANGE. CONTRACTOR TO VERIFY FINAL LOCATION AND DESIGN WITH UTILITY COMPANY PRIOR TO CONSTRUCTION.
10. WATER METER CANNOT BE INSTALLED IN THE BUILDING.
11. CONTRACTOR SHALL COORDINATE CABLE/FIBER OPTIC CONDUIT AND SERVICE INSTALLATION WITH UTILITY COMPANY.
12. ALL TAPS AND CONNECTIONS FOR FIRE AND DOMESTIC WATER SERVICES ARE TO BE IN ACCORDANCE WITH CITY OF LEES SUMMIT STANDARDS AND SPECIFICATIONS.

LEGEND

---	PROPERTY LINE
---	CONSTRUCT STORM SEWER
SS	SS
SS	SS
UGE	UGE
UGE	UGE
W	W
W	W
FP	FP
FP	FP
T	T
T	T
SD	SD
SD	SD

SUGGESTED UTILITY DEPTHS:

COORDINATE WITH UTILITY PROVIDERS AND FOLLOW UTILITY PROVIDERS STANDARDS IF CONFLICT EXISTS.

PRIMARY ELECTRIC 24"

SECONDARY ELECTRIC 18"

DOMESTIC WATER 48"

FIRE SERVICE 48"

COMMUNICATION LINES 18"

KEY NOTES:

- STORM SEWER:**
- S1. SEE SHEETS C-10 THRU C-13 FOR STORM SEWER PLAN & PROFILE.
- S2. CONNECT TO EXISTING STORM SEWER. RE: SHEET C-10
- ROOF / LANDSCAPE DRAIN SYSTEM:**
- B2. INSTALL 89 LF OF 12" PVC @ 2.84% (SW) TO RD1 -  $\bar{E}$ =1011.86
- RD1. INSTALL 24" ADS BASIN WITH STEEL COVER, 193 LF OF 10" PVC @ 1.00% (E) AND 125 LF 8" PVC @ 1.00 (S).
- TS=1020.20  $\bar{E}$  OUT (NE)=1013.39,  $\bar{E}$  IN (W)=1013.89,  $\bar{E}$  IN (S)=1014.76
- TS=1020.20,  $\bar{E}$ =1015.82
- RD2. INSTALL 15" ADS BASIN WITH DOMED LID AND 88 LF OF 8" PVC (S) @ 1.00% TS=1020.20,  $\bar{E}$ =1015.82
- RD3. INSTALL 10" ADS BASIN WITH DOMED LID. TS=1020.20,  $\bar{E}$ =1016.70
- RD4. INSTALL 12" ADS BASIN WITH DOMED LID AND 69 LF OF 8" PVC (S) @ 1.00% TS=1020.20,  $\bar{E}$ =1016.01
- RD5. INSTALL 10" ADS BASIN WITH DOMED LID. TS=1020.20,  $\bar{E}$ =1016.70
- RD6. INSTALL ONE-WAY CLEANOUT AND 5 LF OF 8" PVC (S.) @ 1.00% MIN. RE: MEP FOR CONNECTION AT BLDG.

- ELECTRICAL:**
- E1. PROPOSED TRANSFORMER LOCATION. COORDINATED INSTALLATION WITH UTILITY COMPANY AND MEP PLANS. CONTRACTOR TO INSTALL TRANSFORMER PAD PER UTILITY COMPANY STANDARDS AND SPECIFICATIONS.
- E2. INSTALL 156+/-LF OF PVC CONDUIT FOR PRIMARY ELECTRICAL SERVICE PER UTILITY COMPANY STANDARDS AND SPECIFICATIONS. VERIFY CONDUIT SIZE AND ROUTING WITH KCP&L. ELECTRICAL DESIGN IS BY KCP&L. THE ALIGNMENT SHOWN ON THE PLANS IS APPROXIMATE. THE CONTRACTOR SHALL COORDINATE ELECTRICAL SERVICE ROUTE DIRECTLY WITH KCP&L AND PROVIDE NECESSARY CONDUIT TO TO SERVE THE PROJECT. SEE MEP PLANS FOR LIGHTING AND LIGHTING CONDUIT.

- WATER:**
- !!!CONTRACTOR NOTE: ALL CONNECTIONS WILL BE MADE TO THE CITY'S 12" MAIN. THE 20" MAIN IS NOT TO BE DISTURBED.!!!
- W1. DOMESTIC WATER:
- a. TAP EXISTING 12" CITY WATER LINE PER STANDARD CITY DETAIL. COORDINATE TAP WITH LEE'S SUMMIT WATER DISTRICT.
  - b. INSTALL 6 LF OF 2" TYPE COPPER PIPE FROM TAP TO METER.
  - c. INSTALL 1-1/2" METER AND PIT PER STANDARD CITY DETAIL.
  - d. INSTALL A MINIMUM 10 LF OF 2" TYPE SPLICE-FREE COPPER PIPE FROM TAP TO METER TOWARDS BUILDING PER CITY DETAIL.
  - e. CONTINUE BUILDING SERVICE CONNECTION WITH 140 LF OF 2" POLYETHYLENE PIPE (AWWA C901) RE: MEP FOR CONNECTION AT BLDG.
- W2. FIRE LINE TO HYDRANT:
- a. CUT 12x12x6 TEE INTO EXISTING 12" CITY MAIN
  - b. INSTALL 209 LF OF 6" AWWA C900 FIRE LINE, HYDRANT AND GATE VALVE.(SEE SHEET C10.4 FOR HYDRANT DETAIL)
  - c. INSTALL 6" GATE VALVE, BACK FLOW PREVENTOR, AND VAULT PER STANDARD CITY DETAIL ON SHEET C10.2.
  - d. INSTALL 50 LF OF 2" PVC PIPE @ 1% MINIMUM SLOPE TO DRAIN BACK FLOW PREVENTOR TO EXISTING DITCH. PLACE CONCRETE COLLAR FLUSH WITH GRADE ON DAYLIGHTED PIPE TO PREVENT MOWER DAMAGE.
- W3. FIRE LINE TO BUILDING - INSTALL 20 LF OF 6" AWWA C900 FIRE LINE. RE: MEP FOR CONNECTION AT BLDG.
- W4. IRRIGATION PLANS AND CONNECTION (BY OTHERS).
- W5. FDC LOCATION (REF: MEP PLANS)

- GAS:**
- G1 521± LF GAS SERVICE LINE AND METER. COORDINATE CONNECTION AND LOCATION WITH GAS SERVICE COMPANY.

- COMMUNICATIONS:**
- C1 INSTALL 312 LF OF 3-4" PVC CONDUIT W/PULL STRING. COORDINATE CONNECTION AND LOCATION WITH COMMUNICATIONS COMPANY.

- SANITARY SEWER:**
- SS1 275 LF 4" PVC SERVICE LINE @ 4.26% WITH TRACER WIRE. RE:MEP  $\bar{E}$ =1017.00
- SS2 INSTALL ONE-WAY CLEANOUT. TS=1020.76  $\bar{E}$ =1016.80
- SS3 INSTALL ONE-WAY CLEANOUT. TS=1018.81  $\bar{E}$ =1012.80
- SS4 INSTALL ONE-WAY CLEANOUT TS=1013.29  $\bar{E}$ =1006.11
- SS5 INSTALL WYE CONNECTION ON EXISTING 8" MAIN.  $\bar{E}$ =1005.31
- SS6 INSTALL TRENCH CHECK PER CITY DETAIL

**SAPP  
DESIGN  
ARCHITECTS**

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Engineer of Record  
**TERRY M. PARSONS**  
NUMBER  
PE-2018010505  
03-26-2019  
PROFESSIONAL ENGINEER

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Project No. B18-0330 Date 12.07.18 Drawn RLK

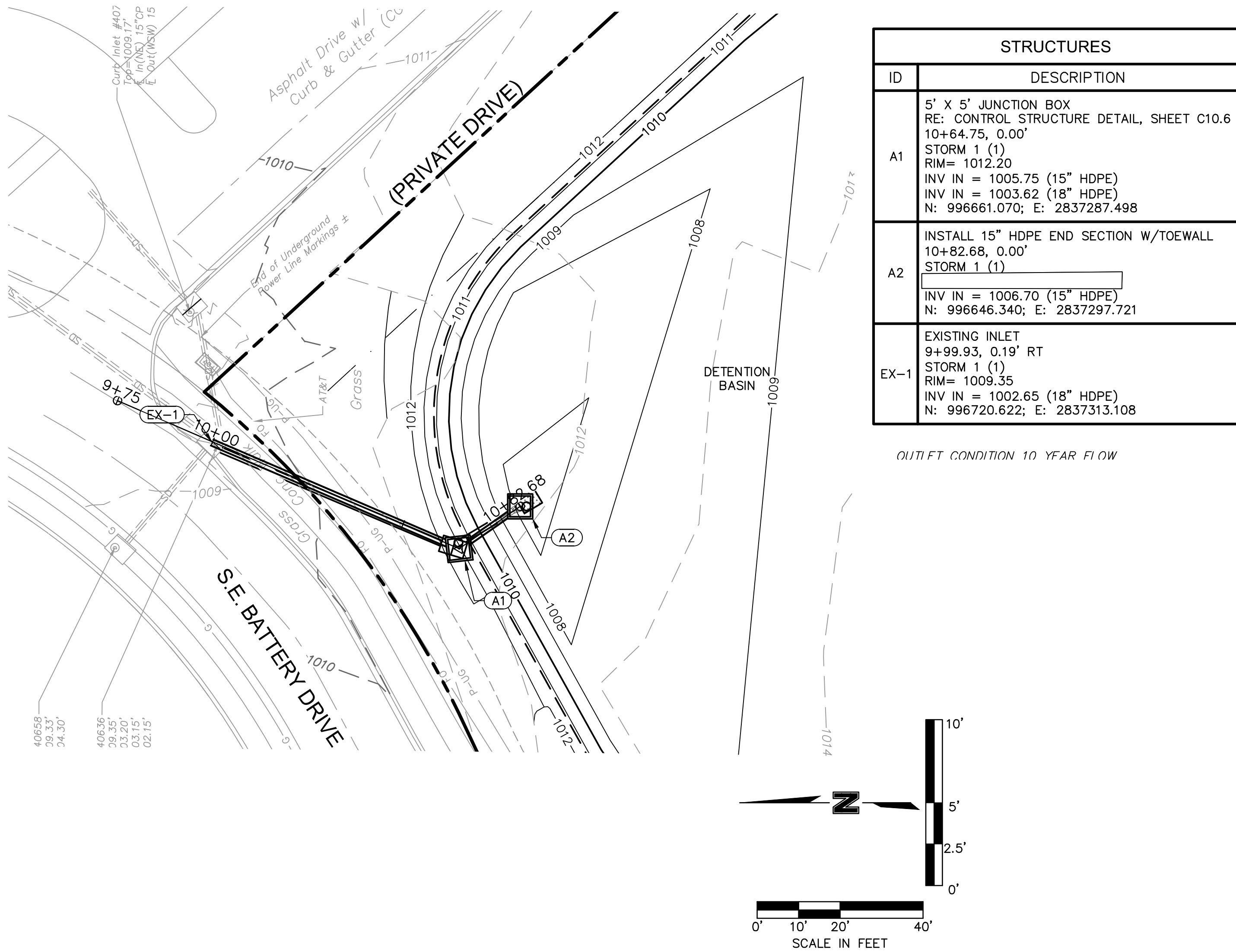
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**C6.0**

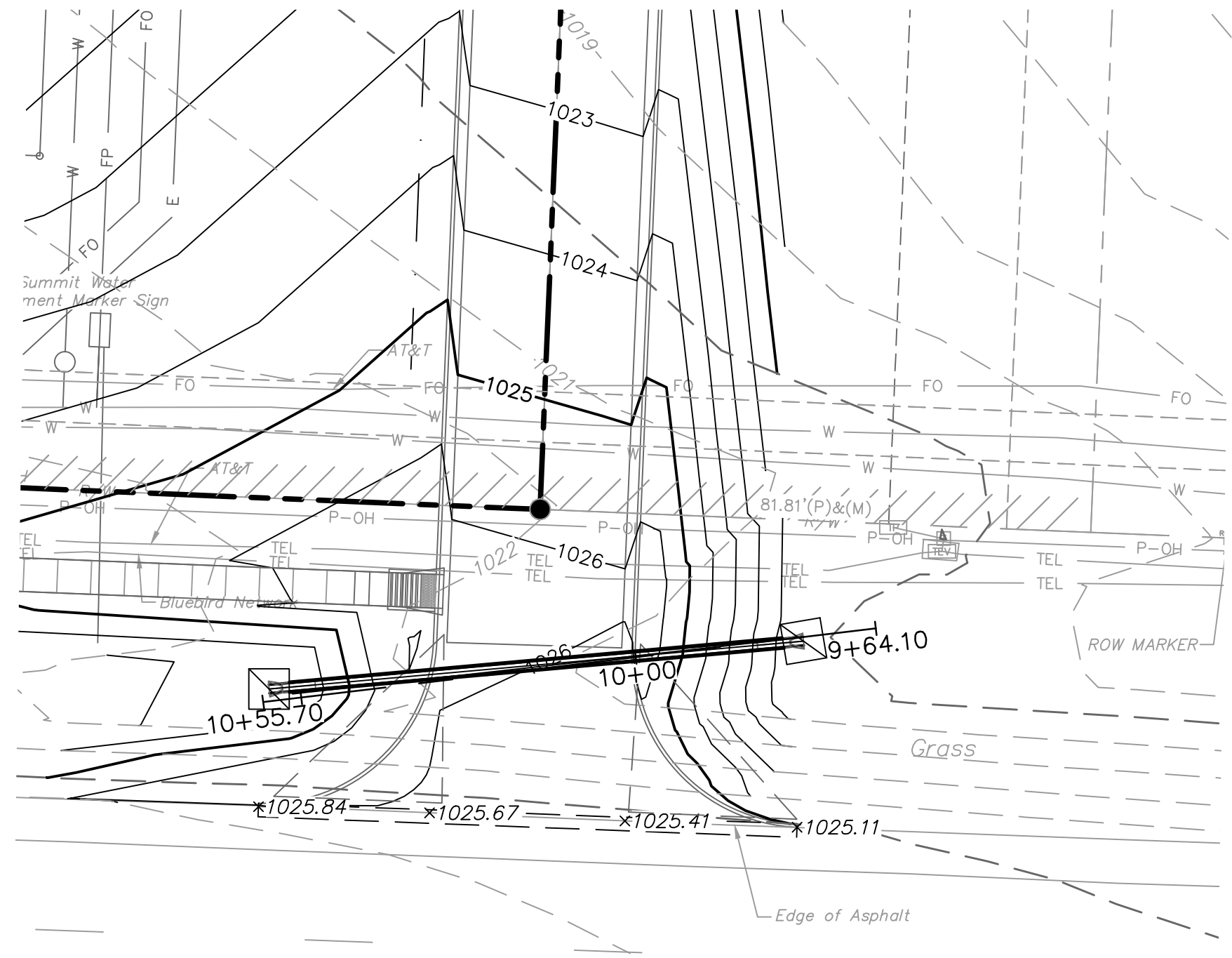
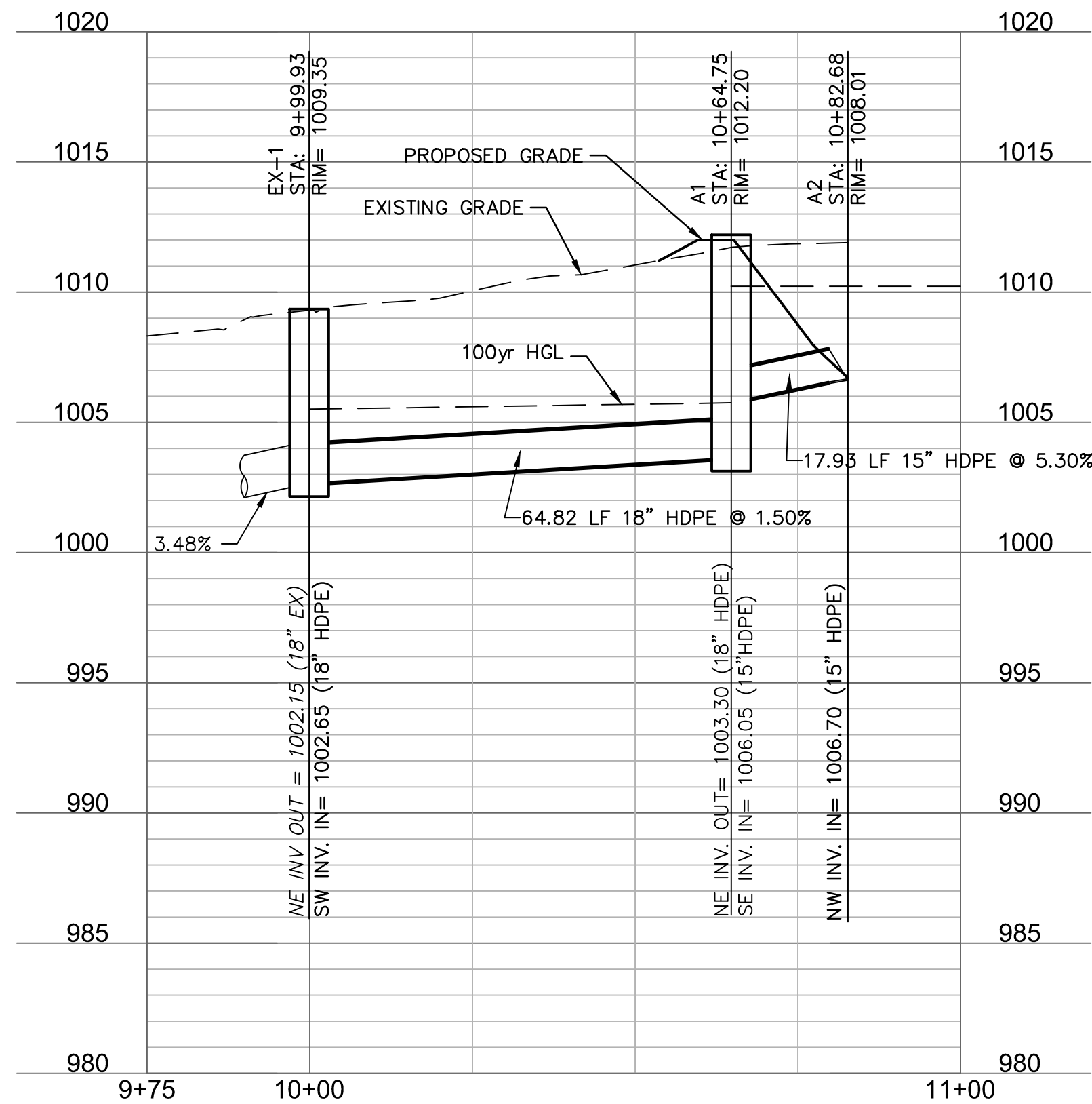
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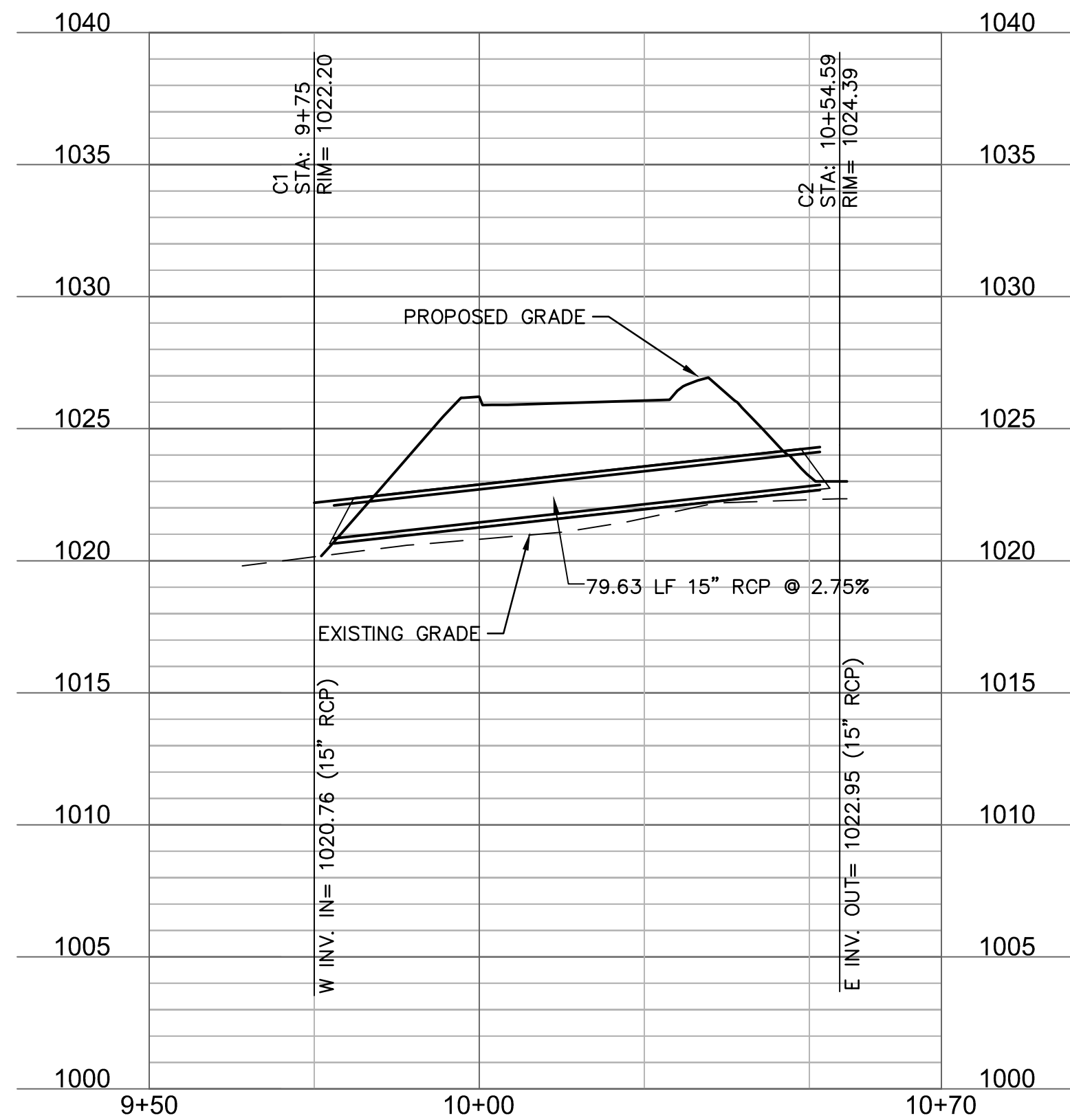
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STORM 1 (1) (9+75 - 11+00)



STORM 4 (9+50 - 10+70)



STRUCTURES	
ID	DESCRIPTION
C1	INSTALL 15" RCP END SECTION WITH CONCRETE TOEWALL AND 1.9 CUBIC YARDS CLASS 2 RIPRAP 9+75, -0.61' LT STORM 4 RIM= 1022.20 INV IN = 1020.76 (15" RCP) N: 996126.759; E: 2837528.253
C2	INSTALL 15" RCP END SECTION WITH CONCRETE TOEWALL AND 1.9 CUBIC YARDS CLASS 2 RIPRAP 10+54.59, 1.82' RT STORM 4 RIM= 1024.39 INV OUT = 1022.95 (15" RCP) N: 996119.661; E: 2837448.943

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Springfield, MO 65804 417.877.9600

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1629 Walnut  
Kansas City, MO 64108 816.300.0300

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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
**EAST LEE'S SUMMIT BRANCH**  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE **04**

Engineer of Record  
**TERRY M. PARSONS**  
NUMBER  
PE-2018016805  
03-26-2019  
PROFESSIONAL ENGINEER

Terry M Parsons, Engineer MO PE-2018010605

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Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No. B18-0330 Date 12.07.18 Drawn RLK

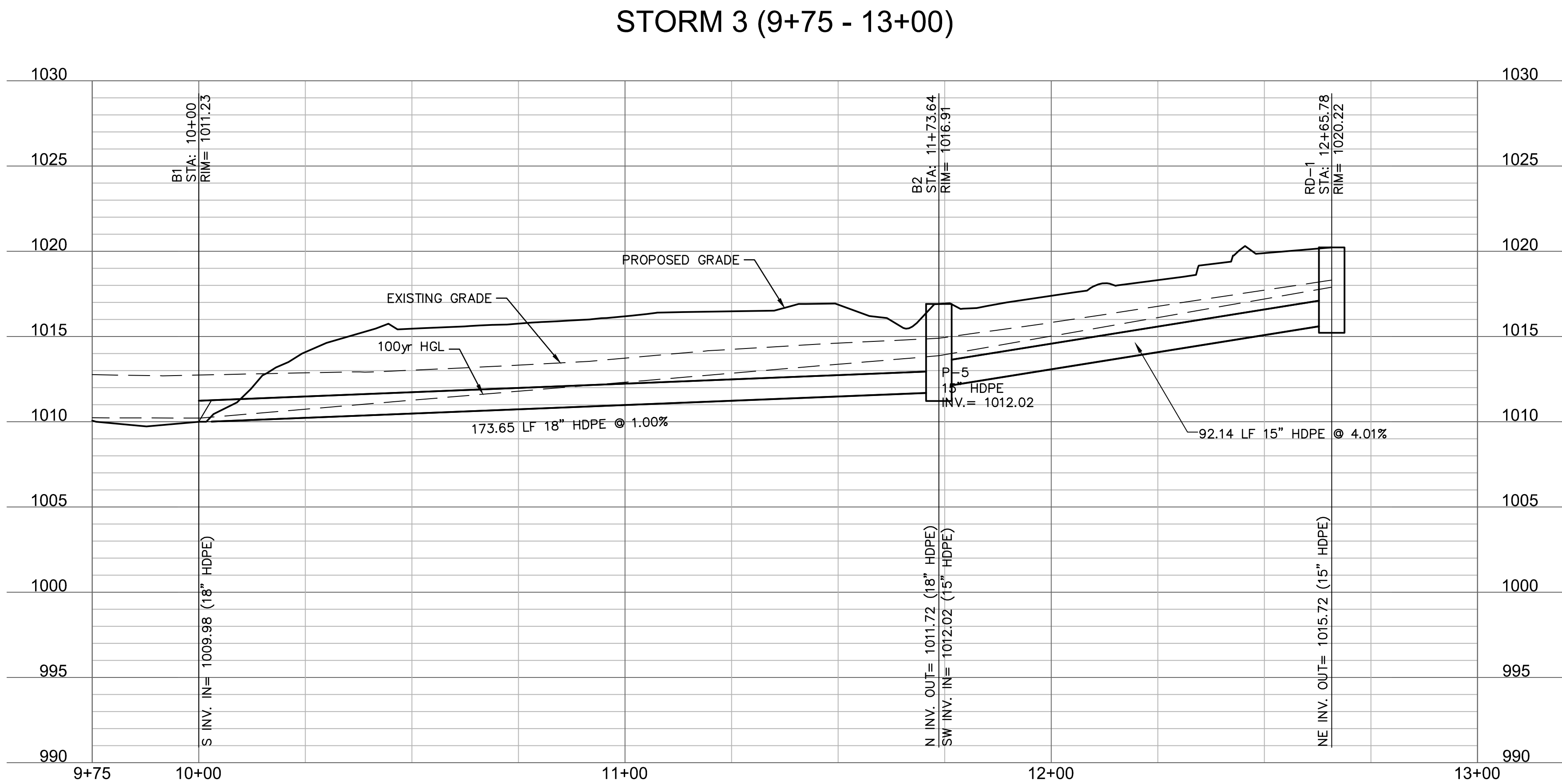
Drawing No. **C7.0**

**STORM SEWER PLAN & PROFILE**

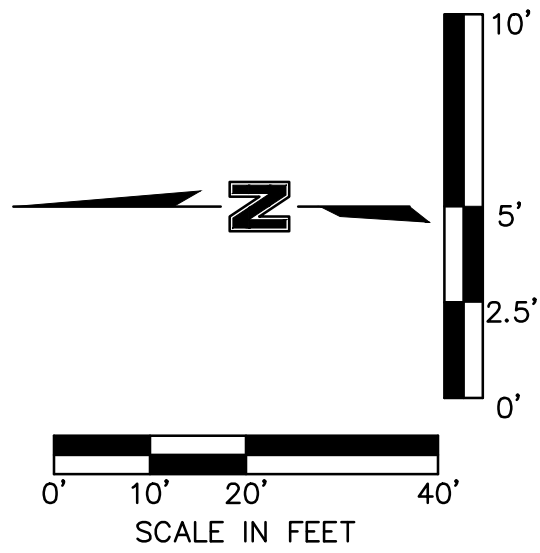
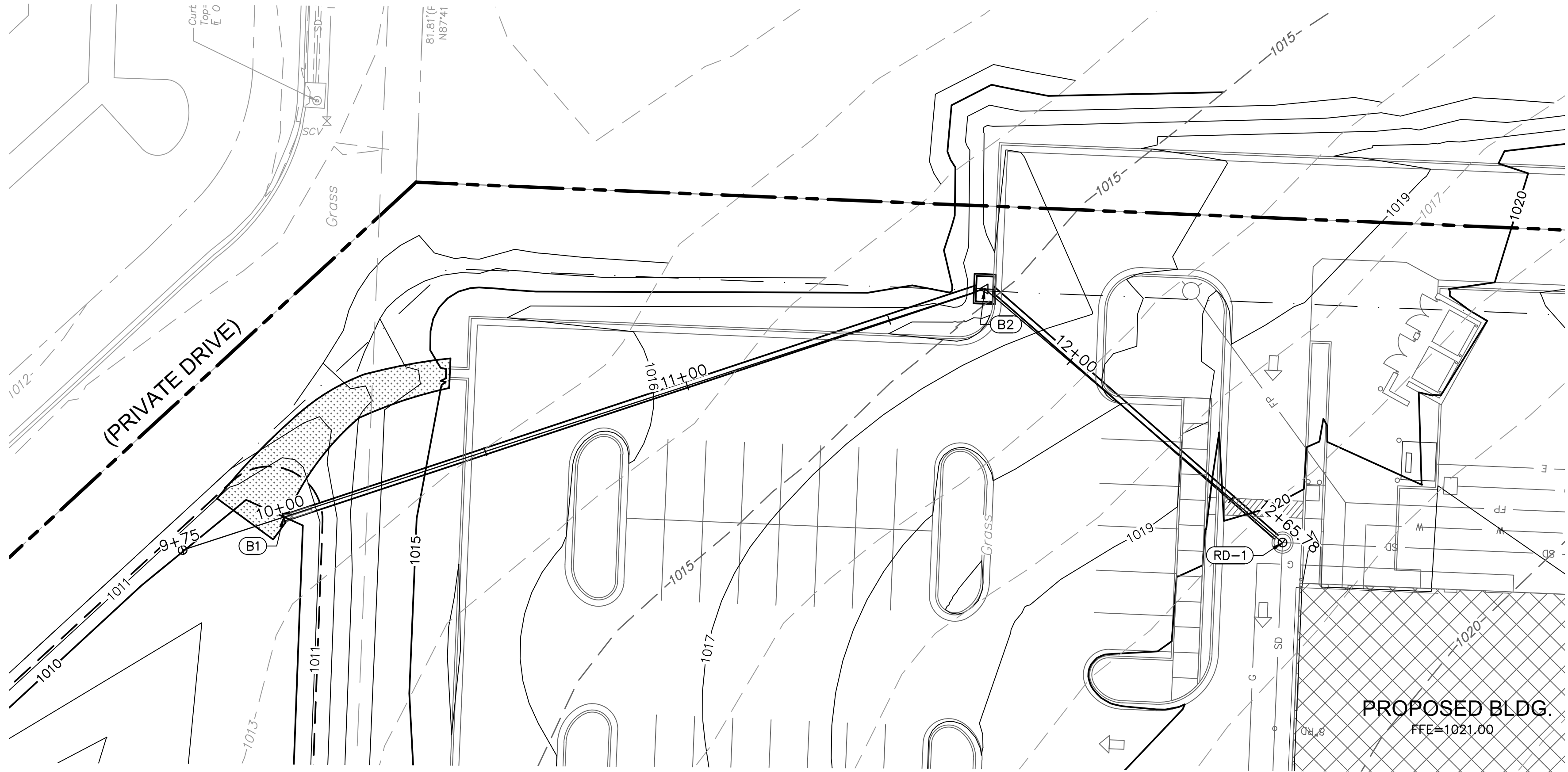
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STRUCTURES	
ID	DESCRIPTION
B1	INSTALL 18" HDPE END SECTION W/TOEWALL AND XX LF OF 150# RIPRAP 10+00, 0.00' STORM 3 RIM= 1011.23 INV IN = 1009.98 (18" HDPE) N: 996558.877; E: 2837426.099
B2	INSTALL STD. 6"x4' NON-SETBACK CURB INLET 11+73.64, 0.00' STORM 3 RIM= 1016.91 INV IN = 1012.02 (15" HDPE) INV OUT = 1011.72 (18" HDPE) N: 996393.767; E: 2837479.875
RD-1	INSTALL STD. 4' DIA. MH W/HEAVY DUTY RING & COVER 12+65.78, 0.00' STORM 3 RIM= 1020.22 INV OUT = 1015.72 (15" HDPE) N: 996323.625; E: 2837420.130



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CONSTRUCTION DOCUMENT PLANS FOR  
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LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE  
**04**

Engineer of Record



Terry M Parsons, Engineer MO PE-2018010505

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1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No.	Date	Drawn
B18-0330	12.07.18	RLK

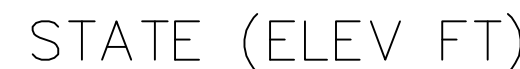
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**C7.1**

**STORM SEWER  
PLAN & PROFILE**

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04  
PACKAGE

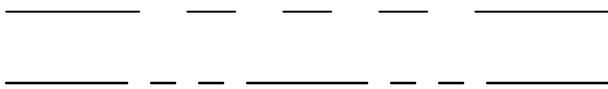


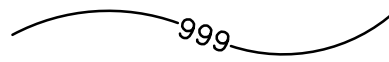




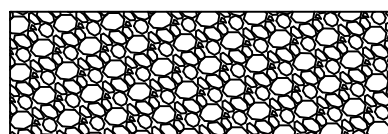

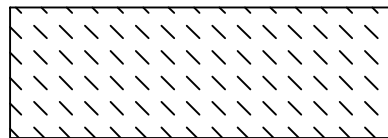


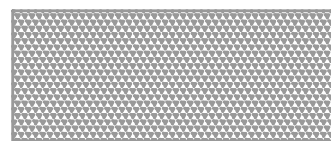
## DRAINAGE PLAN

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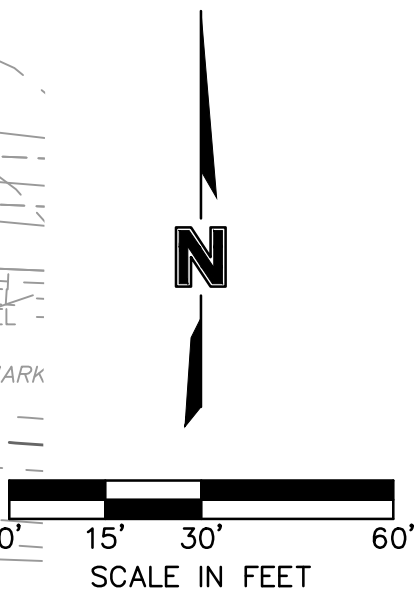




SEDIMENT TRAP DESIGN DATA			
TITLE: 46 Summit			
JOB #: 017-0865			
Design Item	Trap #1	Units	Notes:
<i>Pond Data</i>			
Drainage Area	1.02	acres	
Required Volume (1800 cft/acre)	69.0	cy	
Provided Volume	70.0	cy	
Excavated Volume	111.0	cy	
Sediment Cleanout Volume	14.0	cy	
Sediment Cleanout Elevation	917.1		

<u>LEGEND</u>	
	RIGHT OF WAY LINE
	LOT LINE
	EXISTING GRADE CONTOUR
	FINISHED GRADE CONTOUR
	GRAVEL FILTER BAGS
	SEDIMENT FENCE
	TEMPORARY 18" DIVERSION BERM (SEE DETAILS)
	LIMITS OF DISTURBANCE (XX ACRES)
	TEMPORARY CONSTRUCTION ENTRANCE
	CONCRETE WASHOUT
	STAGING/STOCKPILE AREA
	EROSION CONTROL REFERENCE NUMBER
	STRAW WATTLES
	STABILIZATION. "LANDLOK (S2)" SHALL BE USED OR APPROVED EQUAL. CONTACTOR TO SUBMIT SHOP DRAWINGS TO THE ENGINEER FOR APPROVAL. SEED WITH PERENNIAL SEEDING TO MEET SWPPP REQUIREMENTS. INSTALL PER DETAIL AND MANUFACTURER RECOMMENDATIONS.

EROSION CONTROL STAGING CHART				
PROJECT STAGE	EROSION CONTROL BMP REFERENCE NO.	BMP DESCRIPTION	REMOVE AFTER STAGE	NOTES:
A – PRE-CONSTRUCTION	A1	TEMPORARY CONSTRUCTION ENTRANCE	C	INSTALL AS INDICATED ON PLANS
	A2	CONCRETE WASHOUT	C	INSTALL AS INDICATED ON PLANS
	A3	TEMPORARY SLOPE BARRIER (SEDIMENT FENCE)	D	INSTALL AS INDICATED ON PLANS
	A4	STAGING / STOCKPILE AREA	C	INSTALL AS INDICATED ON PLANS
	A5	EXISTING CURB INLET PROTECTION – TEMPORARY SEDIMENT BARRIER – CONDITION A	C	INSTALL AS INDICATED ON PLANS
	A6	TEMPORARY SEDIMENT TRAP	C	INSTALL AS INDICATED ON PLANS
	A7	TEMPORARY DIVERSION BERM	C	INSTALL AS INDICATED ON PLANS
B – STORM SEWER & UTILITY CONSTRUCTION (STABILIZE ANY DISTURBED AREAS OUTSIDE OF BUILDING PADS AND PARKING LOTS)	B1	TEMPORARY SEDIMENT BARRIER – CURB INLET – CONDITION A	C	INSTALL AS INDICATED ON PLANS
	B2	TEMPORARY SEDIMENT BARRIER – JUNCTION BOX – CONDITION A	C	INSTALL AS INDICATED ON PLANS
	B3	TEMPORARY EROSION CONTROL BLANKET (LANDLOCK S2)	C	INSTALL AS INDICATED ON PLANS
C – BUILDING AND PAVEMENT CONSTRUCTION	C1	TEMPORARY SEDIMENT BARRIER – CURB INLET – CONDITION B	D	INSTALL AS INDICATED ON PLANS
	C2	TEMPORARY SEDIMENT BARRIER – JUNCTION BOX – CONDITION B	D	INSTALL AS INDICATED ON PLANS
D – FINAL STABILIZATION	D1	REPLACE TOP SOIL, SEED MULCH, SOD, LANDSCAPE	N/A	ESTABLISH PERENNIAL VEGETATION WITH A 70% DENSITY OVER 100% OF DISTURBED AREA.



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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
**T LEE'S SUMMIT BRANCH**  
2240 SE BLUE PARKWAY  
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JACKSON COUNTY

# 04 PACKAGE

Engineer of Record



Terry M Parsons, Engineer MO PE-201801050

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Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No.	Date	Drawn
B18-0330	12.07.18	BLK

Drawing No. 000

## C9.0

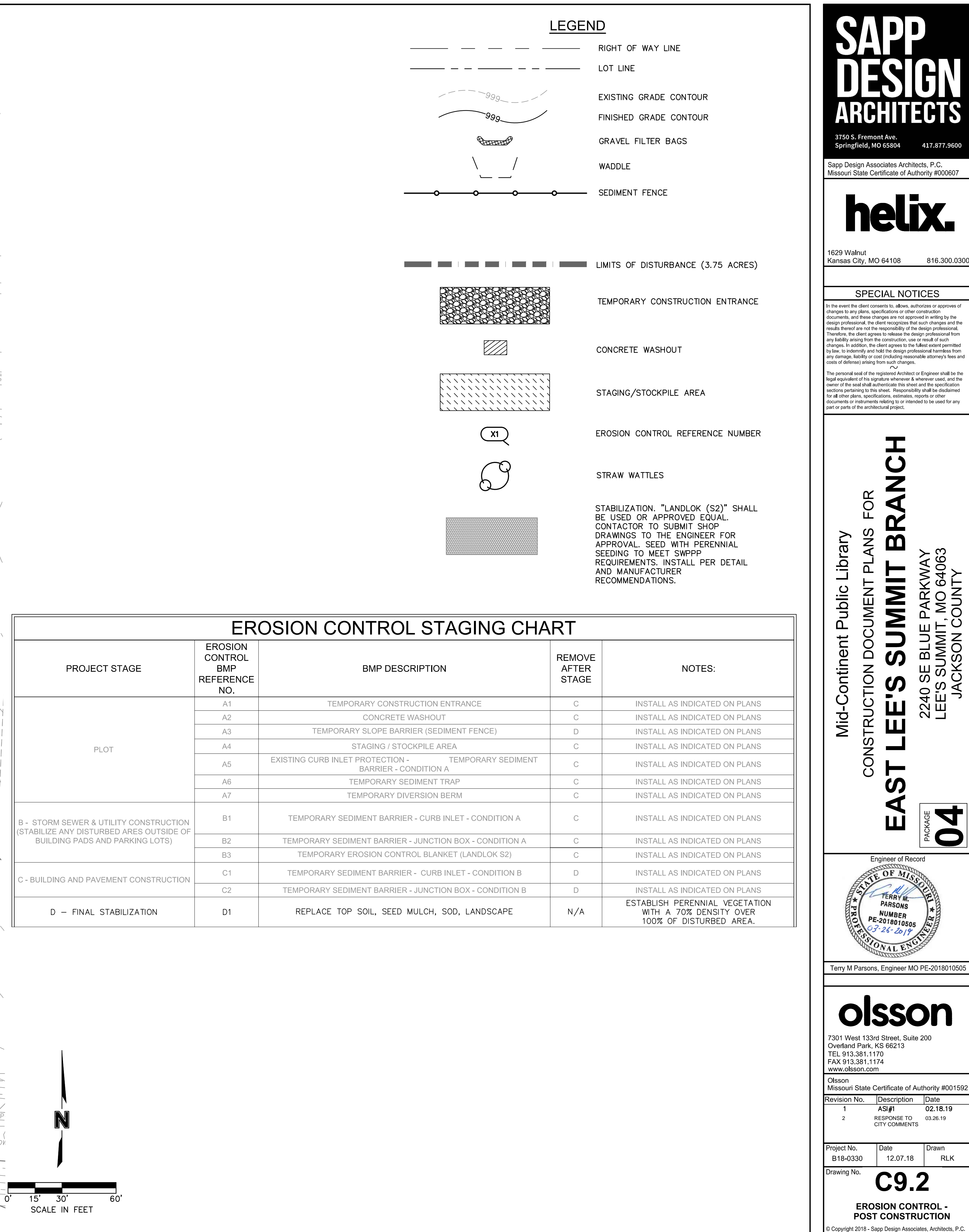
### EROSION CONTROL PLAN - PRE CONSTRUCTION

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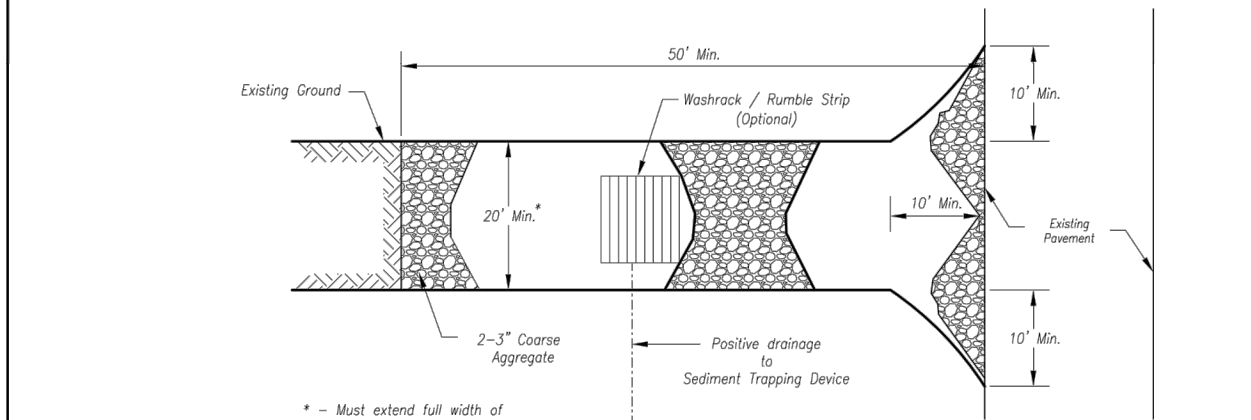




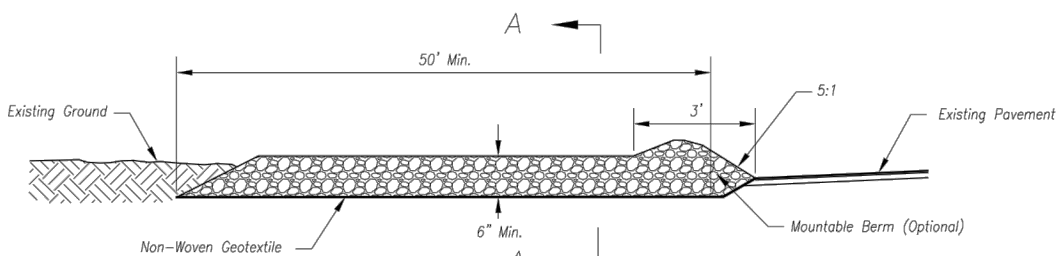


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1629 Walnut Kansas City, MO 64108      816.300.0300		
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Mid-Continent Public Library  CONSTRUCTION DOCUMENT PLANS FOR	<h1 style="margin: 0;">EAST LEE'S SUMMIT BRANCH</h1>	2240 SE BLUE PARKWAY LEE'S SUMMIT, MO 64063 JACKSON COUNTY
<div style="display: flex; justify-content: space-between; align-items: center;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-size: 0.8em;">PACKAGE</div> <div style="font-size: 2em; font-weight: bold; border: 1px solid black; padding: 5px;">04</div> </div>		
<div style="display: flex; justify-content: space-between;"> <div style="width: 30%;"> <p>Engineer of Record</p>  </div> <div style="width: 65%; text-align: right;"> <p><b>TERRY M. PARSONS</b>  <b>NUMBER</b>  <b>PE-20180-0565</b>  <b>03-26-2019</b></p> </div> </div>		
Terry M Parsons, Engineer MO PE-20180-05055		
<h1 style="margin: 0;">olsson</h1>		
7301 West 133rd Street, Suite 200 Overland Park, KS 66213 TEL 913.381.1170 FAX 913.381.1174 <a href="http://www.olsson.com">www.olsson.com</a>		
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Revision No.	Description	Date
1	AS-B#	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19
Project No. <b>B18-0330</b>	Date 12.07.18	Drawn RLK
Drawing No. <span style="font-size: 2em; font-weight: bold;">C9.2</span>		
<b>EROSION CONTROL - POST CONSTRUCTION</b>		
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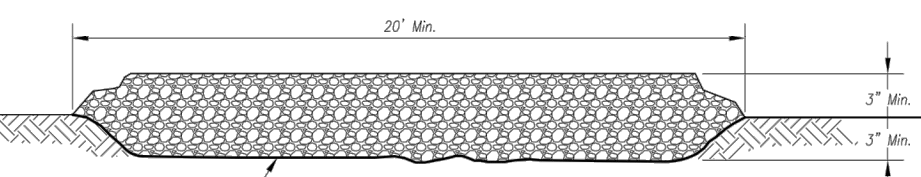




Plan View  
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Side Elevation  
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Section A-A  
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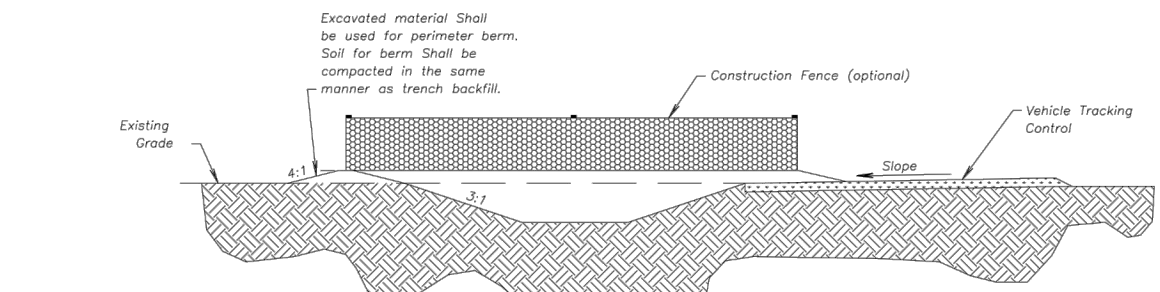
Notes for Construction Entrance:

1. Avoid locating on steep slopes, at curves on public roads, or overland of elevated areas.
2. Remove all vegetation and other unsuitable material from the foundation area, grade, and crown for positive drainage.
3. If slope towards the public road exceeds 2%, construct a 6- to 8-inch high ridge with 20:1 side slopes across the foundation approximately 15 feet from the edge of the public road to direct runoff from it.
4. Install pipe under the entrance if needed to maintain drainage ditches along public roads.
5. Place stone to dimensions and grade as shown on plans. Lane surface sloped for drainage.
6. Divert all surface runoff and drainage from the entrance to a sediment control device.
7. If conditions warrant, place geotextile fabric on the graded foundation to improve stability.

Maintenance for Construction Entrance:

1. Reshape entrance as needed to maintain function and integrity of installation. Top areas with clean aggregate as needed.

CONSTRUCTION ENTRANCE



CONCRETE WASHOUT

AMERICAN PUBLIC WORKS ASSOCIATION	
KANSAS CITY METRO CHAPTER	
CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT	STANDARD DRAWING NUMBER ESC-01 ADOPTED: 10/24/2016

Construction Entrance modified from 2015 Overland Park Standard Details for Erosion and Sediment Control; Concrete Washout modified from 2009 City of Great Bend Standard Drawings.

Notes for Concrete Washout:

1. Concrete washout areas shall be installed prior to any concrete placement on site.
2. Concrete washout areas shall include a flat subsurface pit sized relative to the amount of concrete to be placed on site. The slope leading out of the subsurface pit shall be 3:1. The vehicle tracking post shall be placed towards the concrete washout area.
3. Vehicle tracking control is required at the access point to all concrete washout areas.
4. Signs shall be placed at the construction site entrance, washout area and elsewhere as necessary to clearly indicate the location(s) of the concrete washout area(s) to operators of concrete truck and dump rigs.
5. A one-piece impervious liner may be required along the bottom and sides of the subsurface pit in sandy or gravelly soils.

Maintenance for Concrete Washout:

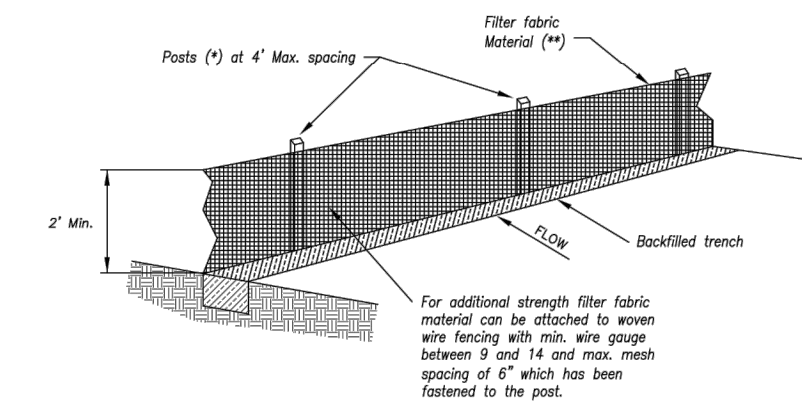
1. Concrete washout materials shall be removed once the materials have filled the washout to approximately 75% full.
2. Concrete washout areas shall be enlarged as necessary to maintain capacity for washed concrete.
3. Concrete washout water, washed pieces of concrete and all other debris in the subsurface pit shall be transported from the job site in a well-sealed container and disposed off property.
4. Concrete washout areas shall remain in place until all concrete for the project is placed.
5. When concrete washout areas are removed, excavations shall be filled with suitable compacted backfill and topped, any disturbed areas associated with the installation, maintenance, and/or removal of the concrete washout areas shall be stabilized.

Excavated material shall be used for perimeter berm. Soil for berm shall be compacted in the same manner as trench backfill.

CONCRETE WASHOUT

AMERICAN PUBLIC WORKS ASSOCIATION	
KANSAS CITY METRO CHAPTER	
CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT	STANDARD DRAWING NUMBER ESC-01 ADOPTED: 10/24/2016

Construction Entrance modified from 2015 Overland Park Standard Details for Erosion and Sediment Control; Concrete Washout modified from 2009 City of Great Bend Standard Drawings.



- (\*) POSTS
- MIN. LENGTH 4'
  - HARDWOOD 1 3/4" x 1 3/4"
  - NO.2 SOUTHERN PINE 2 3/4" x 2 3/4"
  - STEEL 1.33 LB/FT

(\*\*) - Geotextile Fabric shall meet the requirements of ASTM D 2875

SILT FENCE DETAILS

Not to Scale

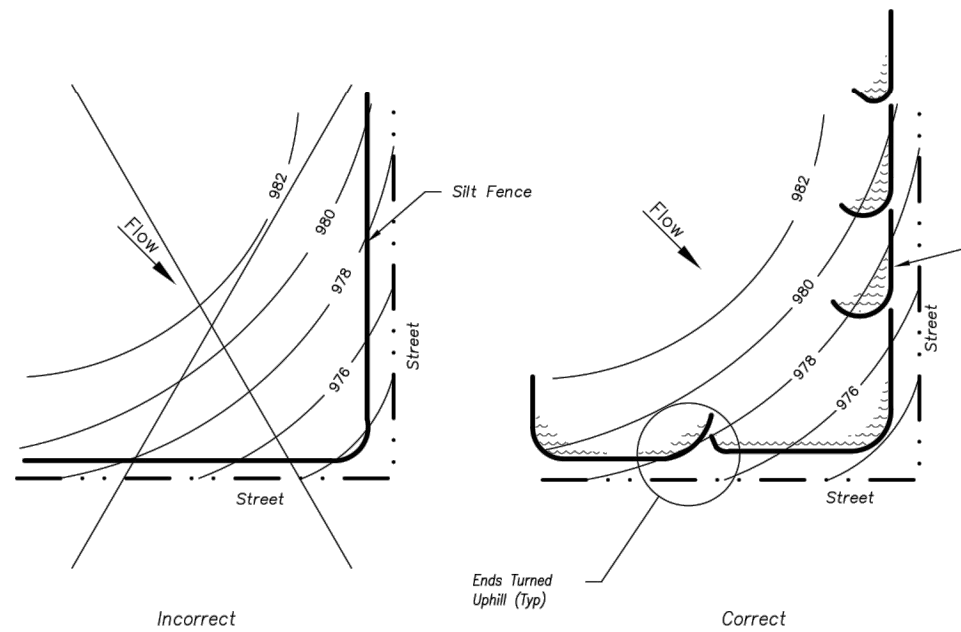
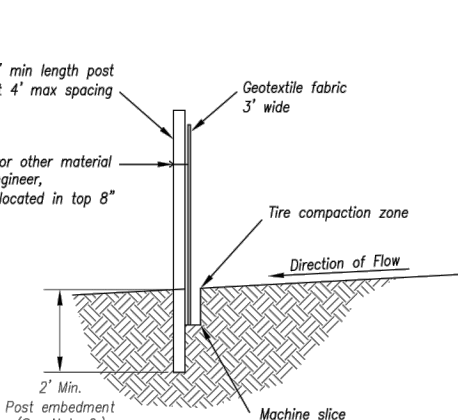


Figure A

SILT FENCE LAYOUT

Not to Scale

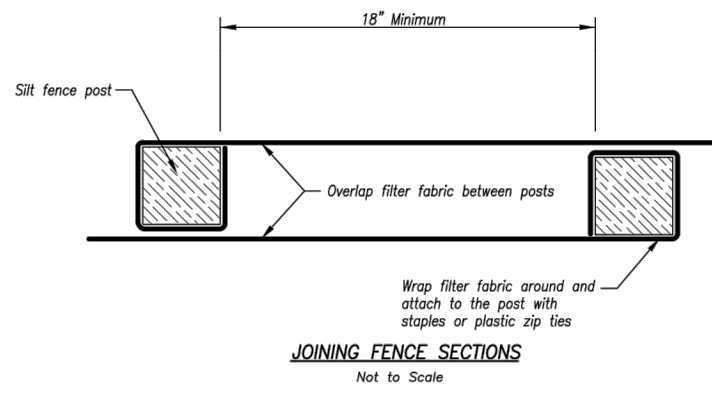


Notes:

1. In order to contain water, the ends of the silt fence must be turned uphill (Figure A).
2. Long perimeter runs of silt fence must be limited to 100'. Runs should be broken up into several smaller segments to minimize water concentrations (Figure A).
3. Long slopes should be broken up with intermediate rows of silt fence to slow runoff velocities.
4. Attach fabric to upstream side of post.
5. Install posts a minimum of 2' into the ground.
6. Tranching will only be allowed for small or difficult installation, where sloping machine cannot be reasonably used.

Maintenance:

1. Remove and dispose of sediment deposits when the deposit approaches 1/2 the height of silt fence.
2. Repair as necessary to maintain function and structure.

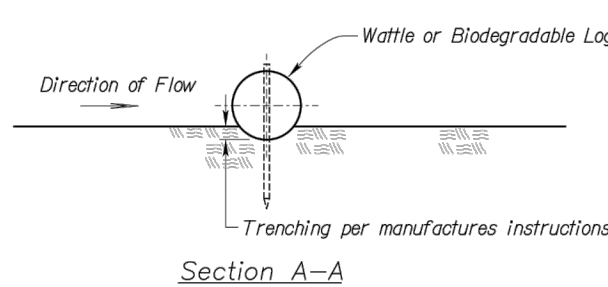


JOINING FENCE SECTIONS

Not to Scale

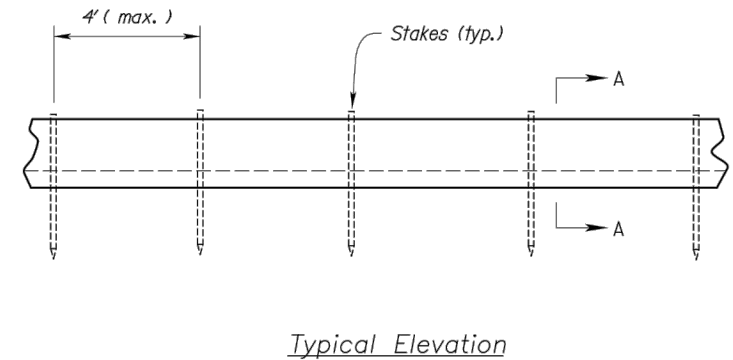
AMERICAN PUBLIC WORKS ASSOCIATION	
KANSAS CITY METRO CHAPTER	
SILT FENCE	STANDARD DRAWING NUMBER ESC-03 ADOPTED: 10/24/2016

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.



Section A-A

WATTLES AND BIODEGRADABLE LOG



Typical Elevation

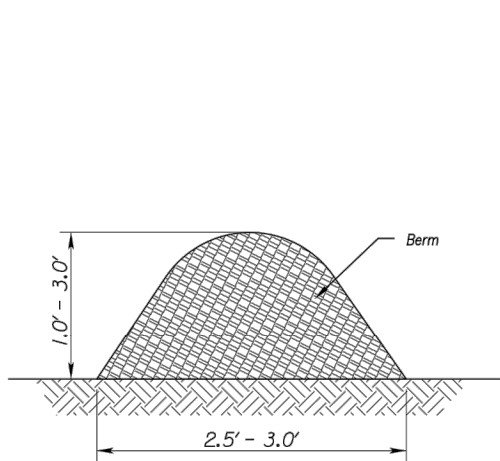


Figure 1  
(Perimeter Control)

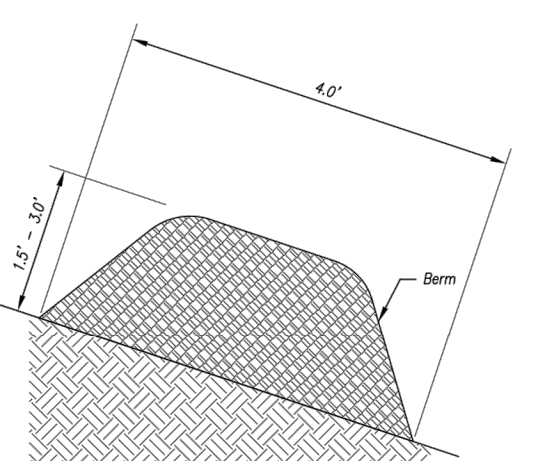


Figure 2  
(Steep Slopes)

MULCH OR COMPOST FILTER BERMS

Notes for Wattles and Biodegradable Log Slope Protection:

1. The Slope barriers shall be placed along contour lines, with a short section turned uphill at each end of the barrier. The maximum length of the slope barrier shall not exceed 250 feet, and the barrier ends need to be staggered.
2. Install wattles and biodegradable logs per manufacturer's instructions.
3. Spacing of stakes per manufacturer's instructions with 4' max. spacing. Length of stakes shall be a minimum of 2 times the diameter of the log with minimum of 24".

Notes for Mulch and Compost Filter Berms:

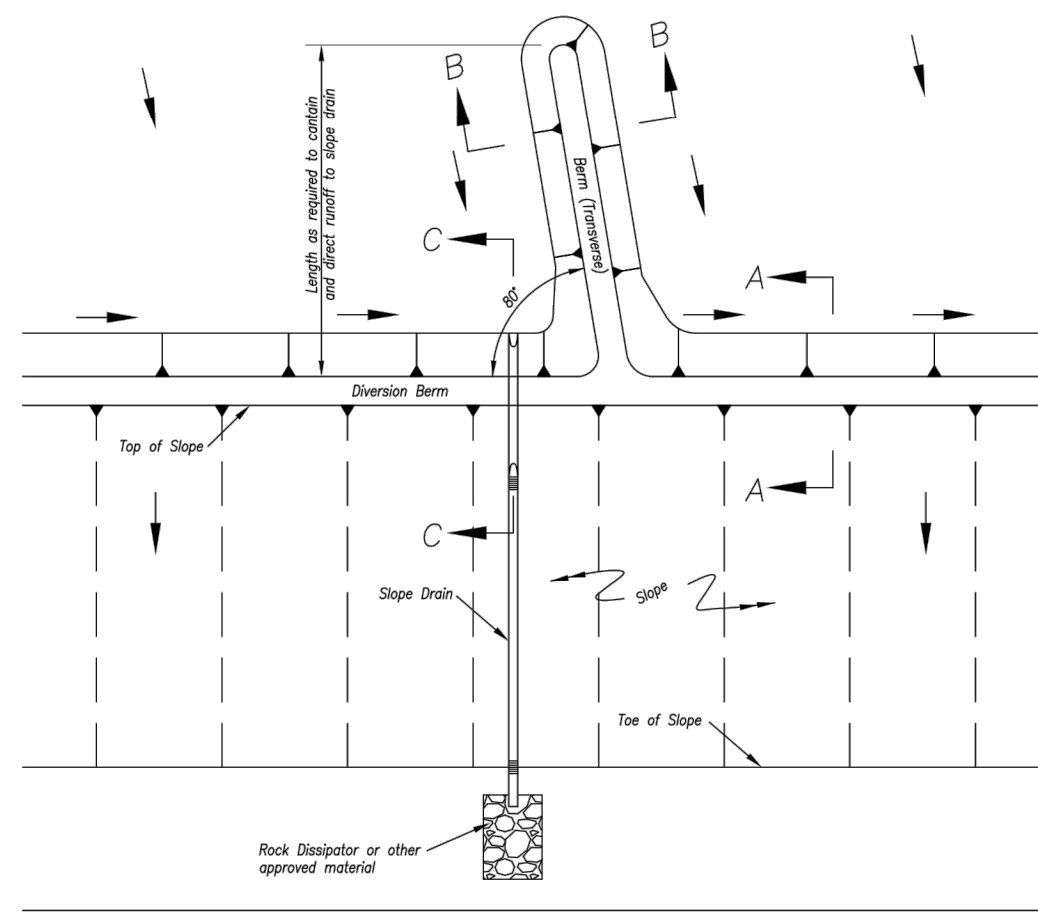
1. The sediment control berm shall be placed uncompact in a windrow of locations shown on the plans or as directed by the engineer.
2. Parallel to the base of the slope, or around the perimeter of other affected areas, construct a 1 to 3 foot high by 2.5 to 3 foot wide berm (see Figure 1). For maximum water treatment ability or for steep slopes, construct a 1.5 to 3 foot high trapezoidal berm that is a minimum of 4 feet wide at the base (see Figure 2). In extreme conditions, or where specified by the engineer, a second berm shall be constructed at the top of the slope. Engineer will specify berm requirements.
3. If berm is to be left as permanent or part of the natural landscape, the compost berm may be seeded during application for permanent vegetation.
4. Do not use compost or wood mulch berms in any runoff channels or concentrated flow areas.
5. Wood mulch shall consist of tree and shrub debris resulting from clearing and grubbing and shall be ground by the mechanical means such as a chipper, hammermill, tub grinder or other approved method. Mulch sizing varies with a maximum width of 2" and a maximum length of 10".

Maintenance for Mulch and Compost Filter Berms:

1. Berms shall be reshaped and material added as necessary to maintain function and dimensions.
2. Breaches in the berm shall be repaired promptly.

AMERICAN PUBLIC WORKS ASSOCIATION	
KANSAS CITY METRO CHAPTER	
WATTLES/Biodegradable LOG AND MULCH/COMPOST FILTER BERM	STANDARD DRAWING NUMBER ESC-04 ADOPTED: 10/24/2016

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.



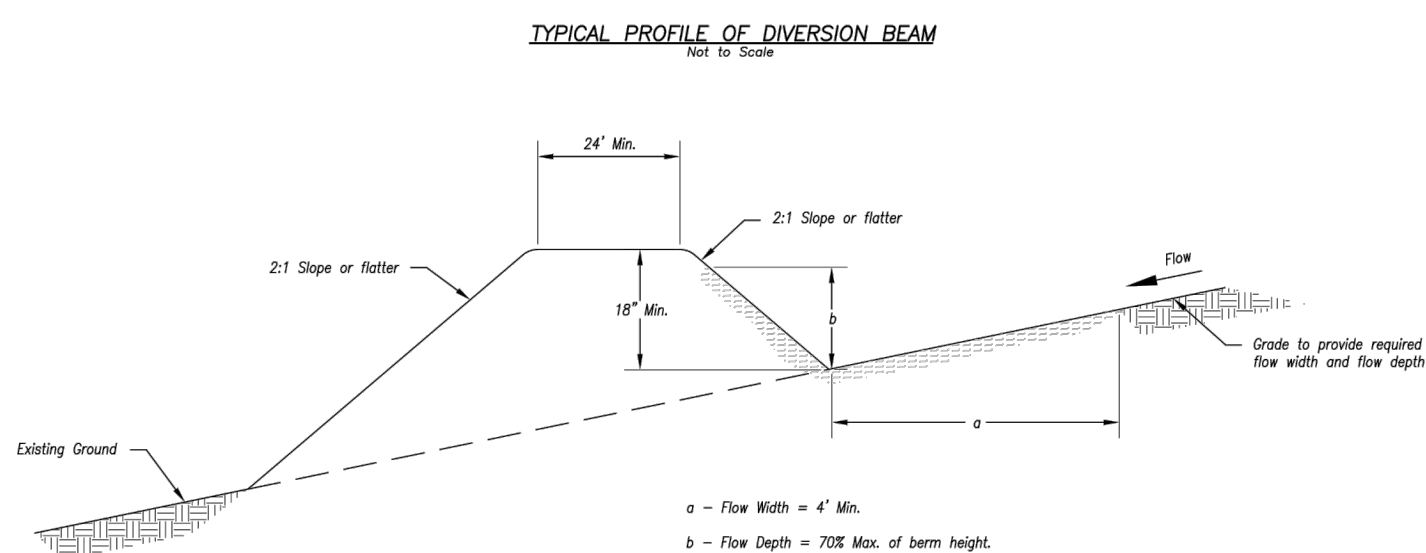
Typical Plan View of Diversion Berm and Slope Drain

Notes for Diversion Berm:

1. Slope drains are optional, but may be required by the engineer if the berm is at the top of a steep slope.
2. Diversion berms must be installed as a first step in the land-disturbing activity and must be functional prior to upstate land disturbance.
3. The berm should be adequately compacted to prevent failure.
4. Temporary or permanent seeding and mulch shall be applied to the berm immediately following its construction.
5. Place the berm so to minimize damages by construction operations and traffic.
6. The berm must discharge to a temporary sediment trap or stabilized area.
7. All trees, brush, stumps, obstructions and other objectionable material shall be removed and disposed of so as not to interfere with the proper functioning of diversion.
8. The diversion shall be excavated or shaped to line, grade and cross-section as required to meet the criteria specified herein, free of irregularities which will impede flow.
9. Fills shall be compacted as needed to prevent unequal settlement that would cause damage in the completed diversion. Fill shall be composed of soil which is free from excessive organic debris, rocks or other objectionable materials.

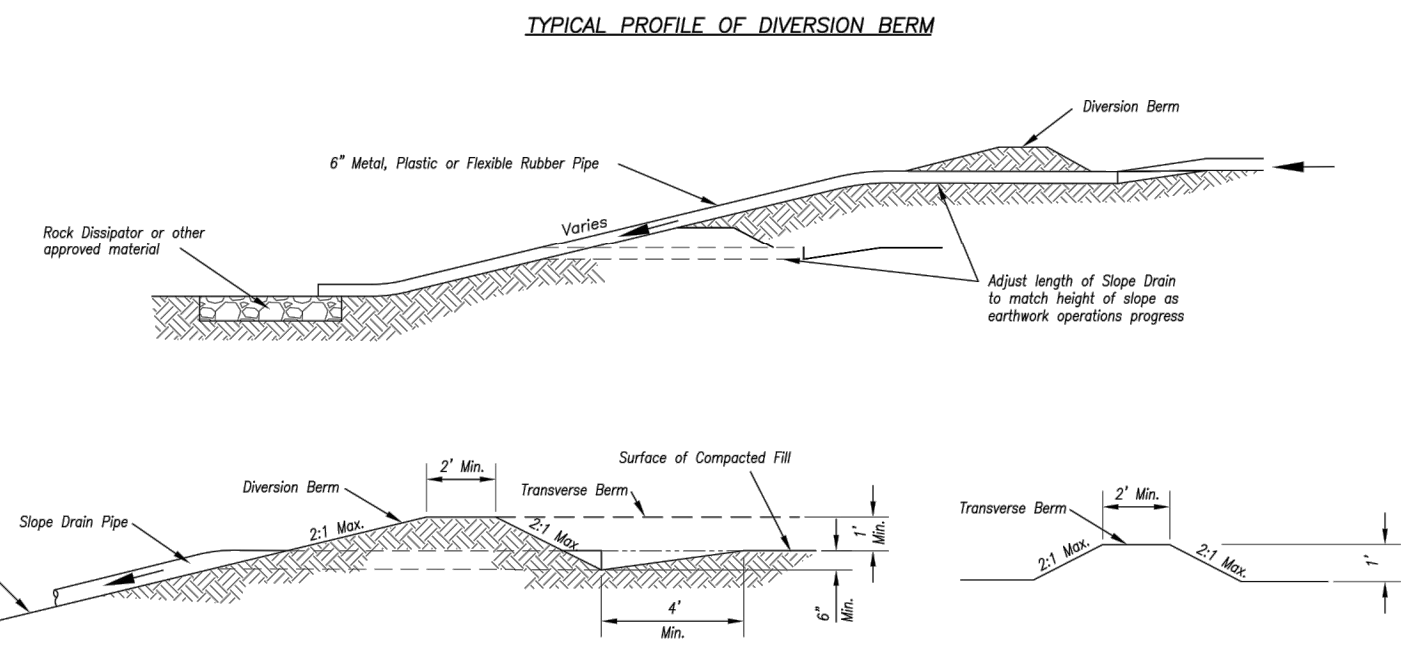
Maintenance:

1. Berms shall be reshaped, compacted, and stabilized as necessary to maintain its function.
2. Breaches in the berm shall be repaired immediately.



Typical Profile of Diversion Berm

Not to Scale



Typical Profile of Diversion Berm with Slope Drain

Not to Scale

Notes for Slope Drain:

1. Slope Drain and Diversion Berm may be used on either project foreslopes or project backlopes.
2. Discharge of Slope Drains shall be into stabilized ditch or area, or into Sediment Basin.
3. Pipe shall be secured in place as approved by Engineer.

Maintenance:

1. Accumulation of any visible sediment at the inlet and outlet shall be removed promptly.
2. Outlet conditions shall be repaired if scour is observed. Leaking or damaged section of pipe shall be repaired immediately.
3. Barriers directing water to the inlet shall be monitored for continuity and effectiveness.

AMERICAN PUBLIC WORKS ASSOCIATION	
KANSAS CITY METRO CHAPTER	
DIVERSION BERMS AND SLOPE DRAINS	STANDARD DRAWING NUMBER ESC-05 ADOPTED: 10/24/2016

Modified from 2015 Overland Park Standard Details for Erosion and Sediment Control.

# SAPP DESIGN ARCHITECTS

3750 S. Fremont Ave.  
Springfield, MO 65804 417.877.9600

Sapp Design Associates Architects, P.C.  
Missouri State Certificate of Authority #000607

# helix.

1629 Walnut  
Kansas City, MO 64108 816.300.0300

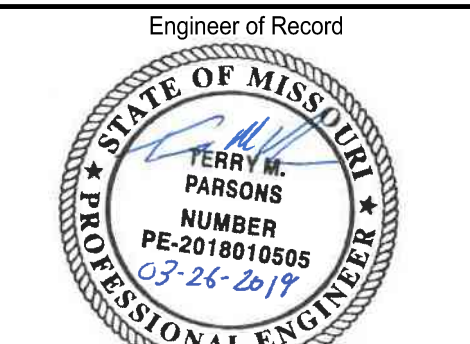
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# EAST LEE'S SUMMIT BRANCH

Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY



Terry M Parsons, Engineer MO PE-2018010505

# olsson

7301 West 133rd Street, Suite 200  
Overland Park, KS 66213  
TEL 913.381.1170  
FAX 913.381.1174  
www.olson.com

Olsson  
Missouri State Certificate of Authority #001592

Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

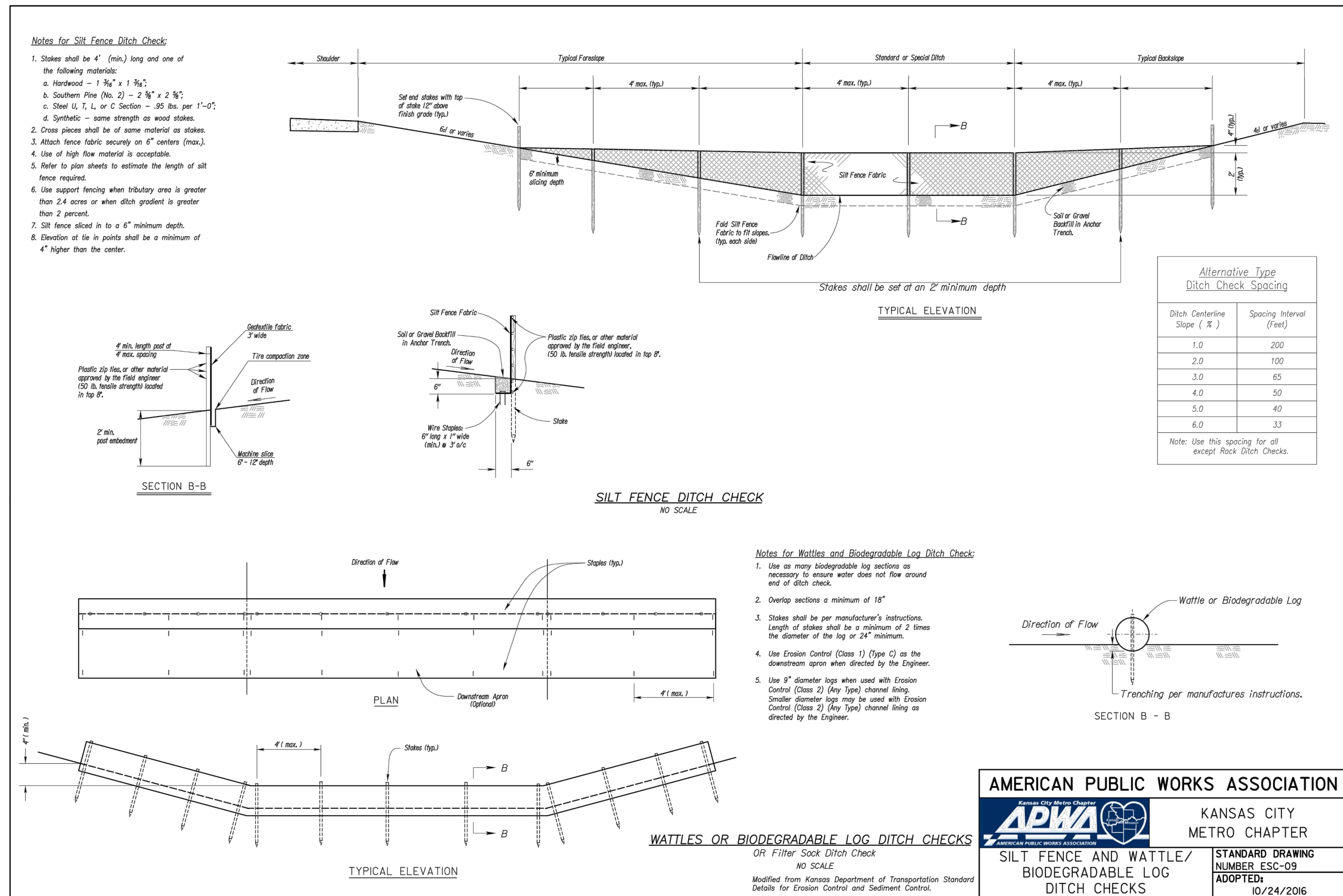
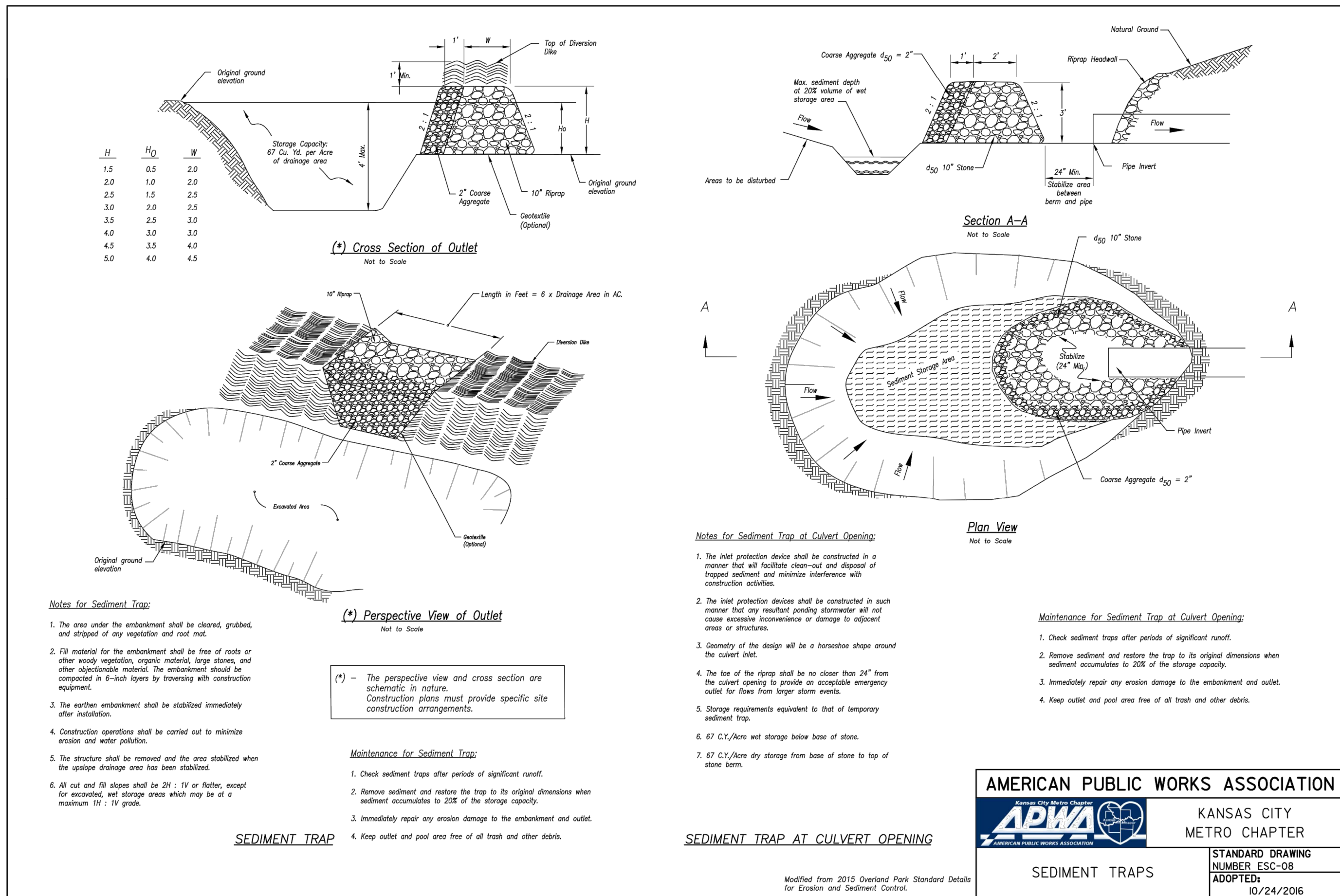
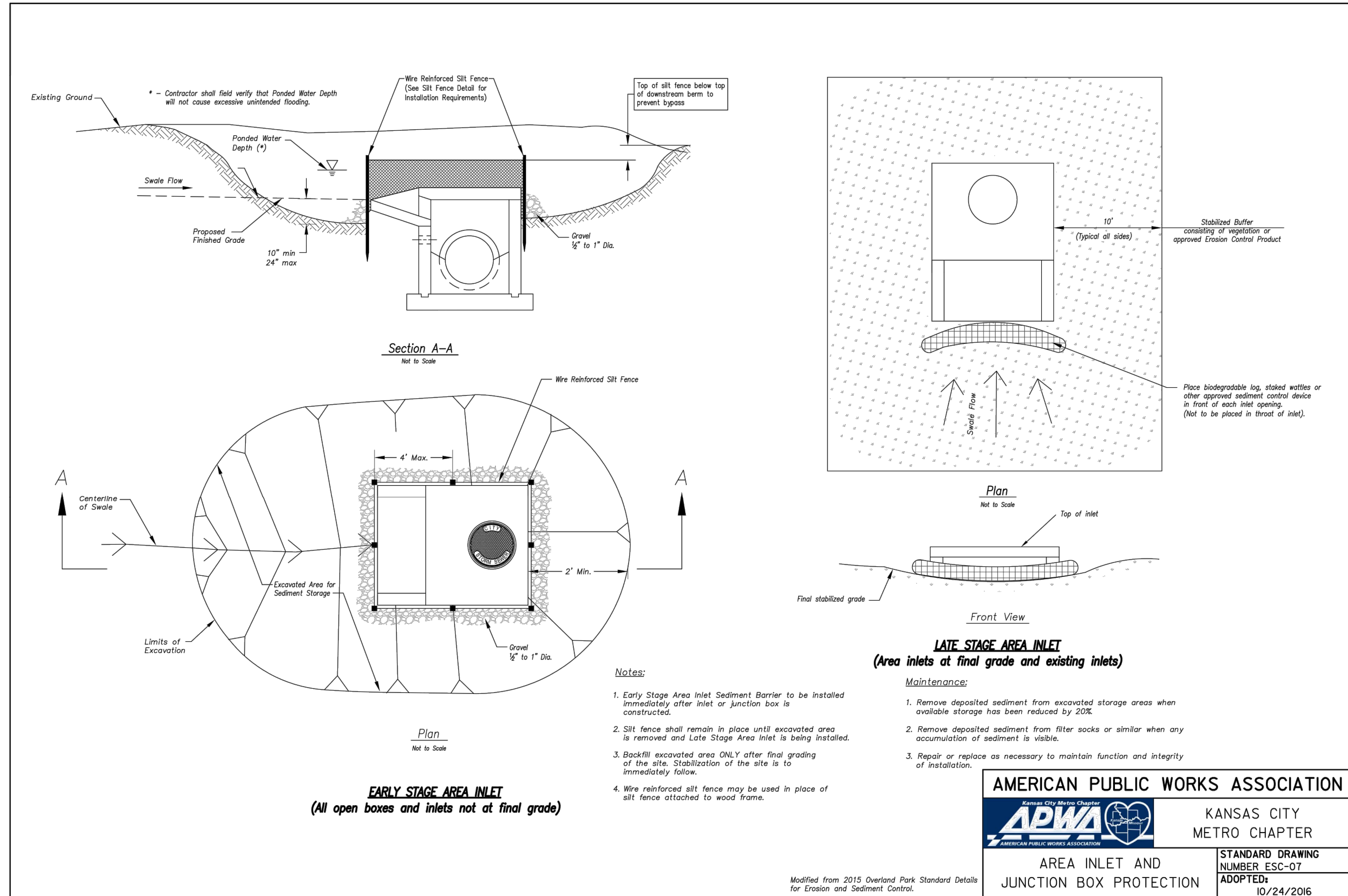
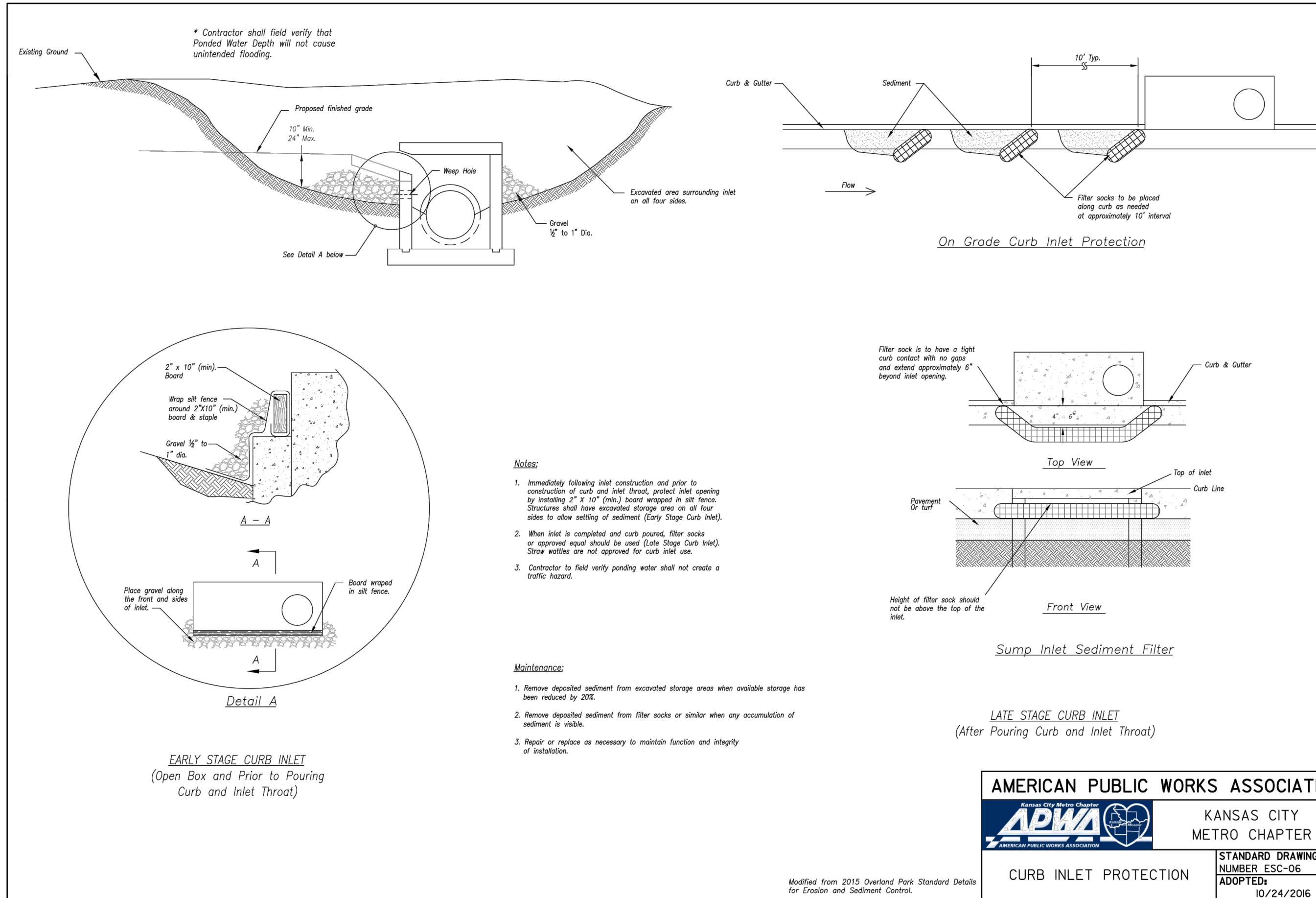
Project No. B18-0330 Date 12.07.18 Drawn RLK

# C9.3

## EROSION CONTROL DETAILS

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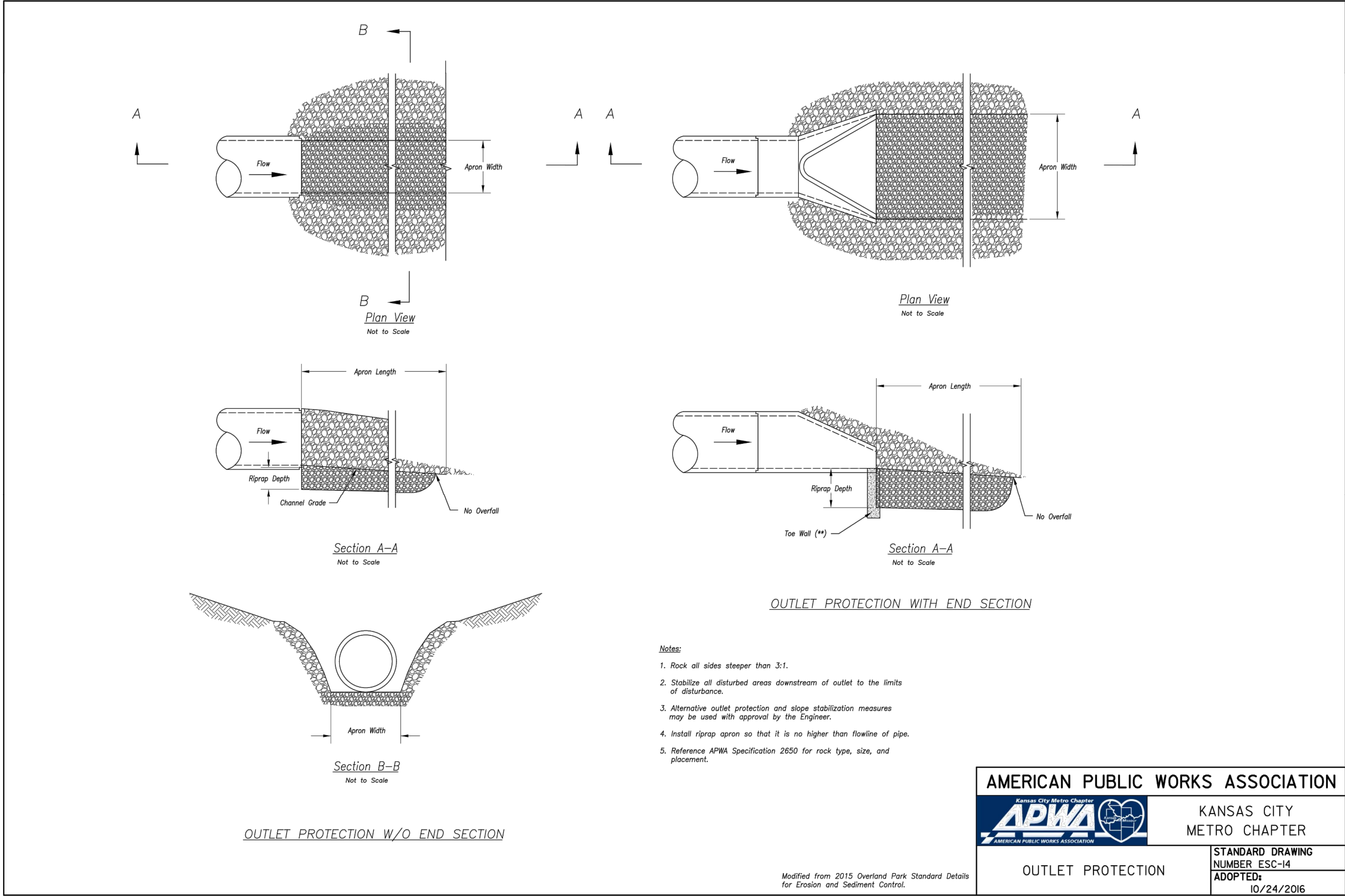
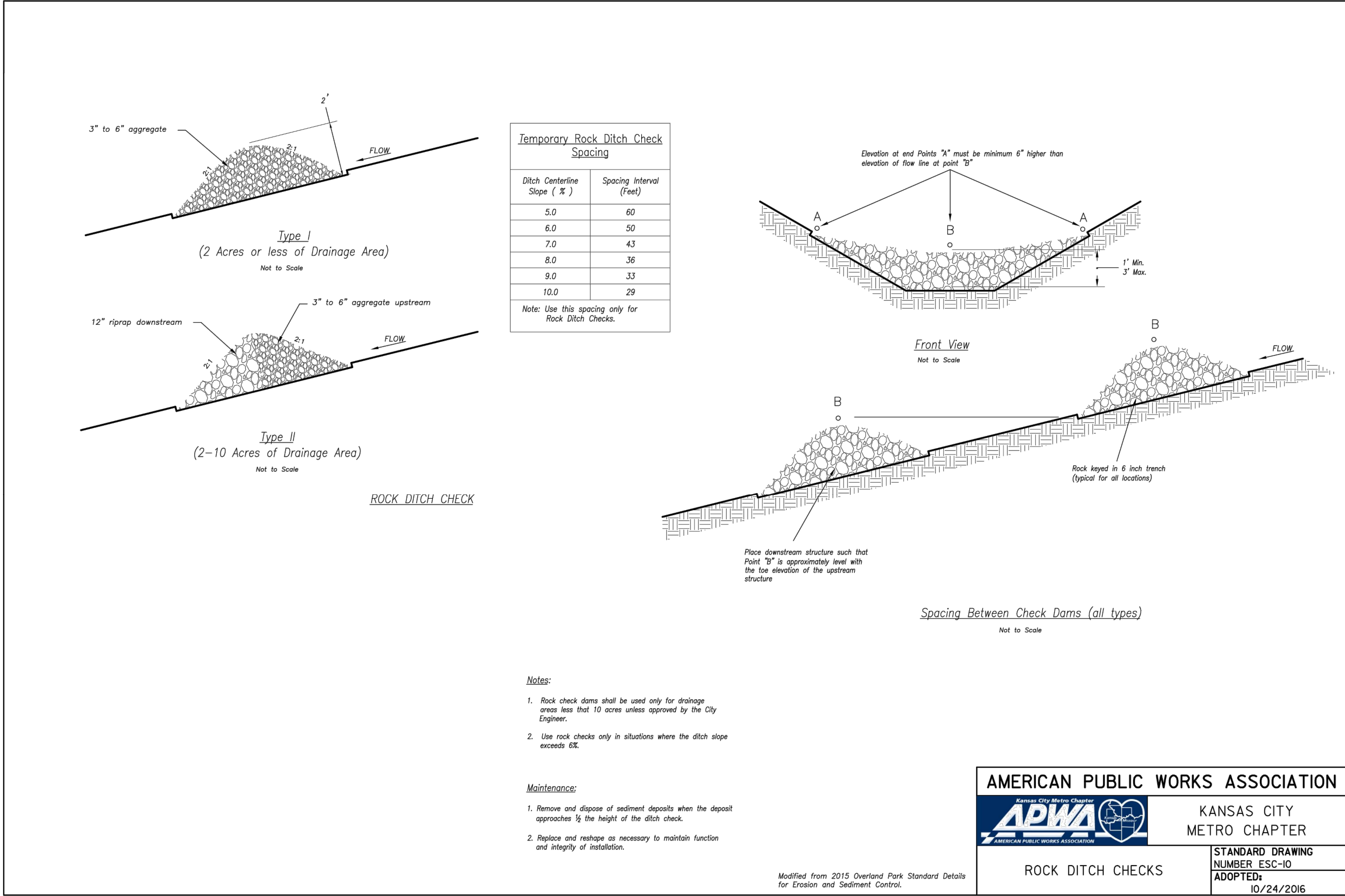


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Terry M Parsons, Engineer MO PE-2018010505

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TEL 913.381.1170  
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Olsson  
Missouri State Certificate of Authority #001592

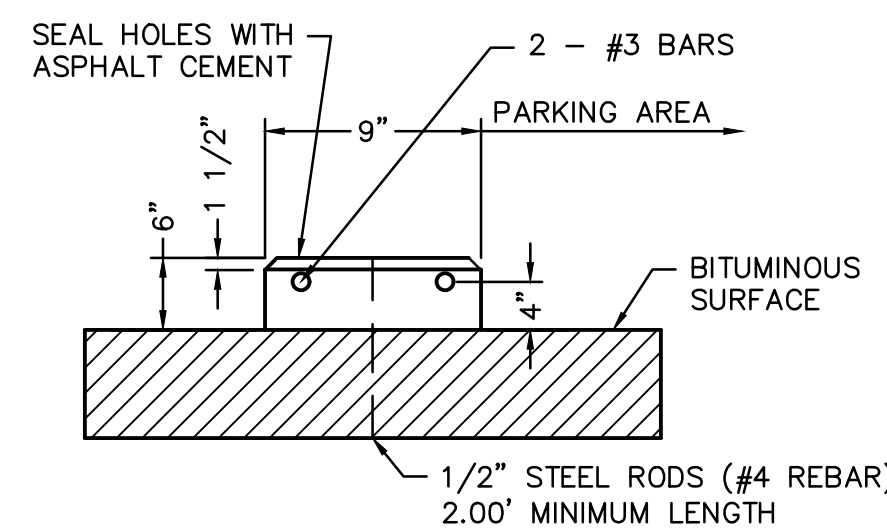
Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No.	Date	Drawn
B18-0330	12.07.18	RLK





**NOTE:**  
ANCHOR RODS TO BE SLEEVED OF  
GREASED THRU CONCRETE CURBS TO  
PERMIT RELOCATION OF CURBS.

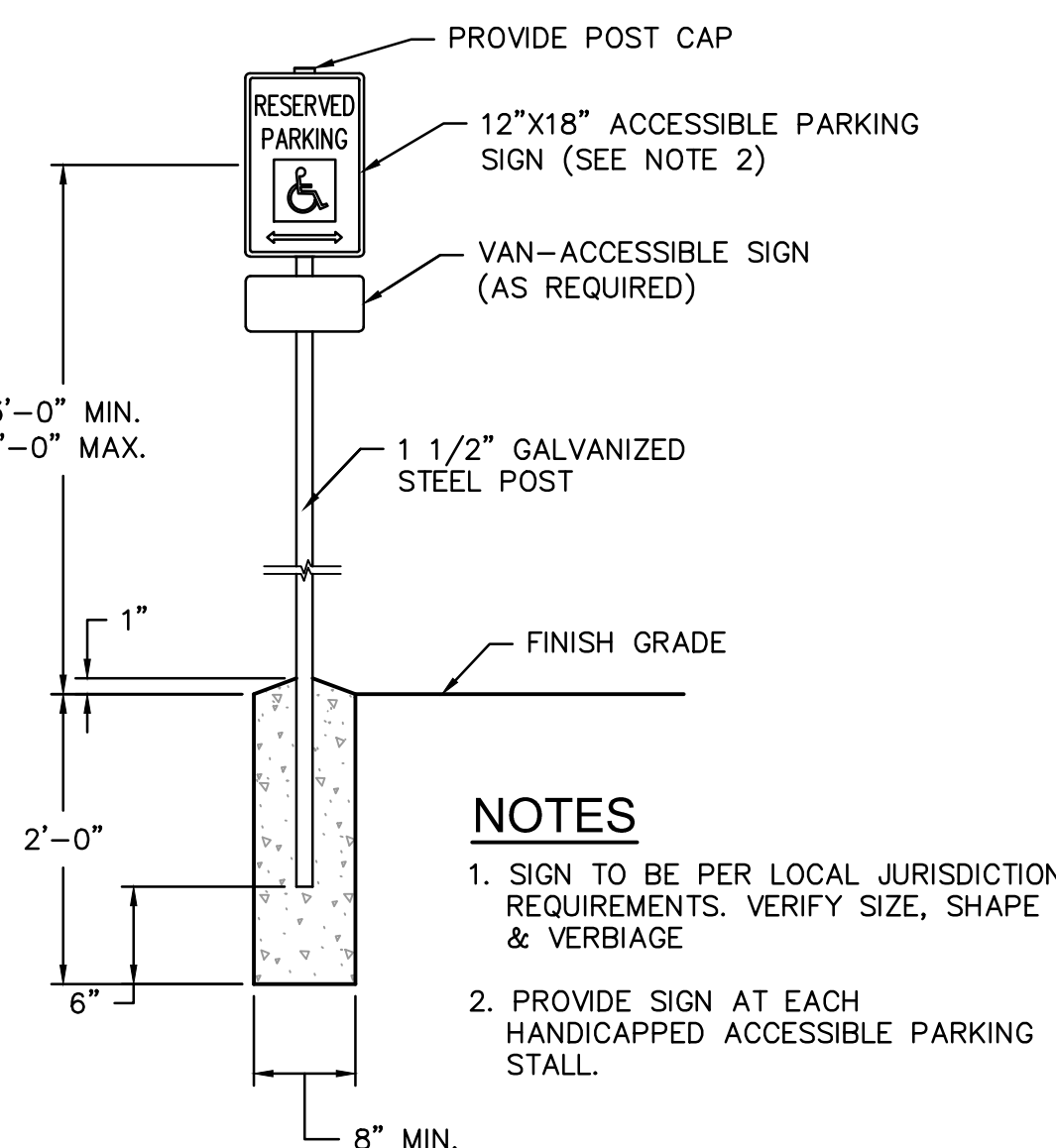


Technical drawing of a bicycle seat post. The drawing shows a side view of the seat post with various dimensions and labels. The dimensions are as follows:

- Top horizontal dimension: 3"
- Top vertical dimension: 5"
- Top horizontal dimension: 10" DIAMETER
- Top vertical dimension: 2'-0"
- Top horizontal dimension: 1'-4"
- Top vertical dimension: 2'-0"
- Top horizontal dimension: 6"
- Top vertical dimension: 1'-0"
- Top horizontal dimension: 12"
- Top vertical dimension: 2'-3"
- Top horizontal dimension: 67.5"
- Top vertical dimension: 5" WIDE (TYP. ALL LINES)



1. SUBGRADE MUST BE OF STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
2. 1.5% CROSS SLOPE MUST BE MAINTAINED THROUGH DRIVEWAYS.
3. KCMMB 4K CONCRETE MIX SHALL BE REQUIRED FOR ALL SIDEWALKS OR AS APPROVED BY THE CITY INSPECTOR.
4. ALL SIDEWALKS SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG)
5. AN EXPANSION JOINT SHALL BE PLACED AT A MAXIMUM OF 150 FT. CONSTRUCTION JOINTS SHALL BE PLACED THE SAME WIDTH OF SIDEWALK, BUT NO GREATER THAN 10 FT.
6. SIDEWALK FINISHING (NO PICTURE FRAMING) AS DIRECTED BY CITY INSPECTOR.
7. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.



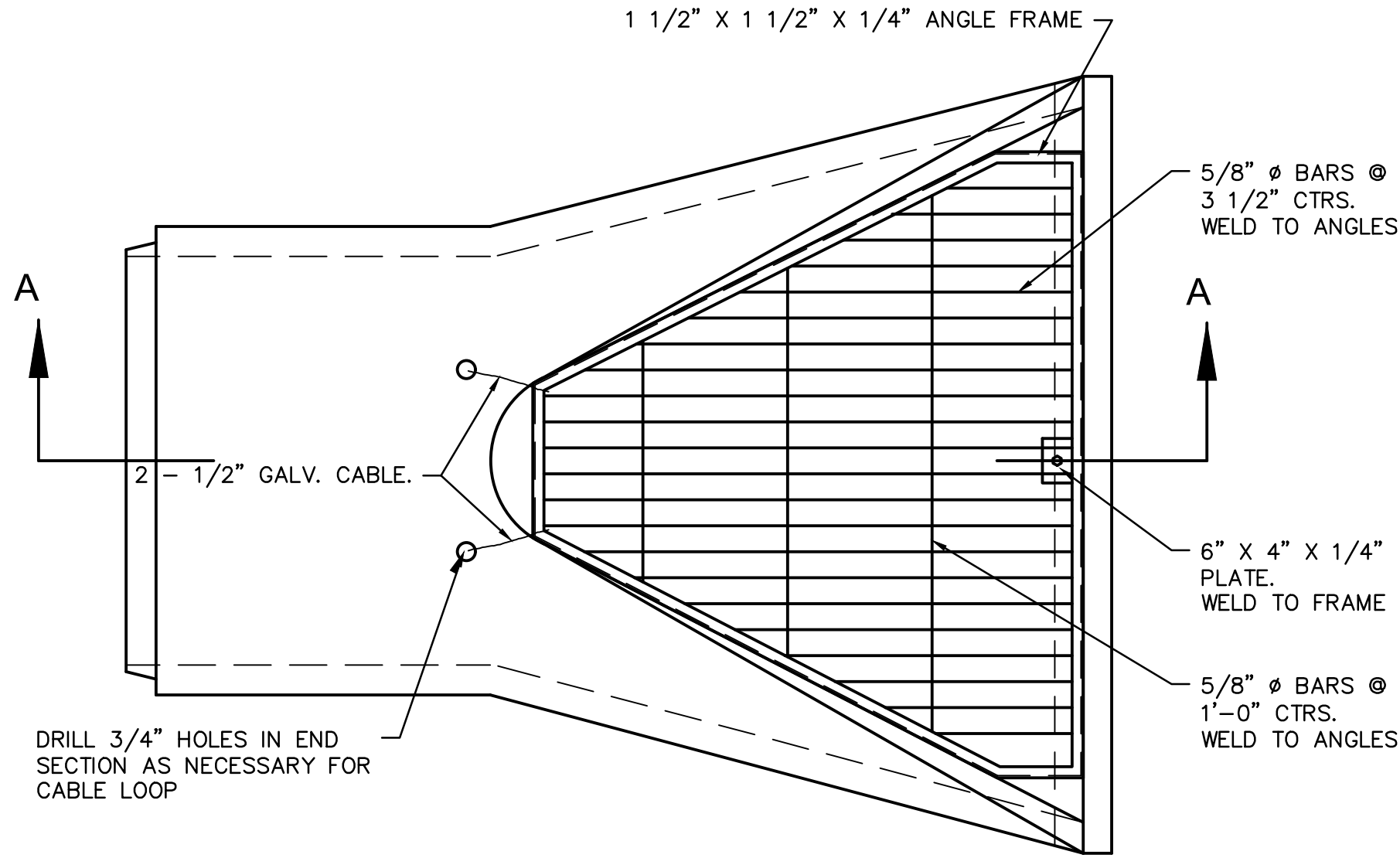
## NOTES

1. SIGN TO BE PER LOCAL JURISDICTION REQUIREMENTS. VERIFY SIZE, SHAPE & VERBIAGE
2. PROVIDE SIGN AT EACH HANDICAPPED ACCESSIBLE PARKING STALL.

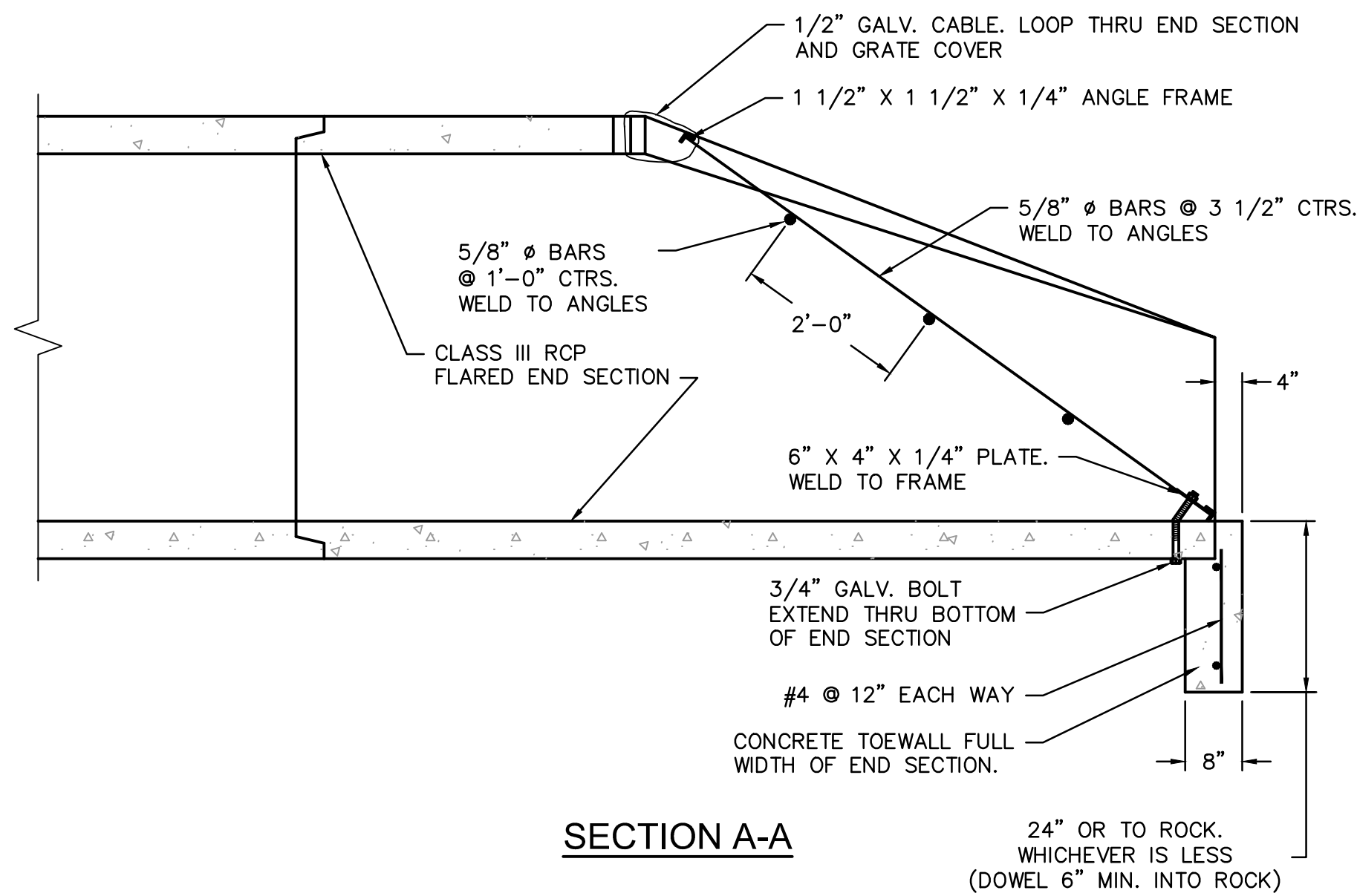
Project No.	Date	Drawn
R18-0330	12-07-18	RLK



DWG: F:\2018\0001-0500\018-0330-B\40-Design\AutoCAD\Final Plans\Streets\GNCV\WORK PACKAGE 4\LEES SUMMIT\C\_DTL\_80330.dwg  
DATE: Mar 26, 2019 10:30am  
XREFS: C\_4LS\_TBLK\_80330 C\_4LS\_XBASE\_80330 C\_4LS\_PBASE\_80330 C\_4LS\_PSURF\_80330  
USER: jfrieling  
L\_PBASE\_61023  
TE DETAIL  
C\_PBASE\_80330



PLANVIEW

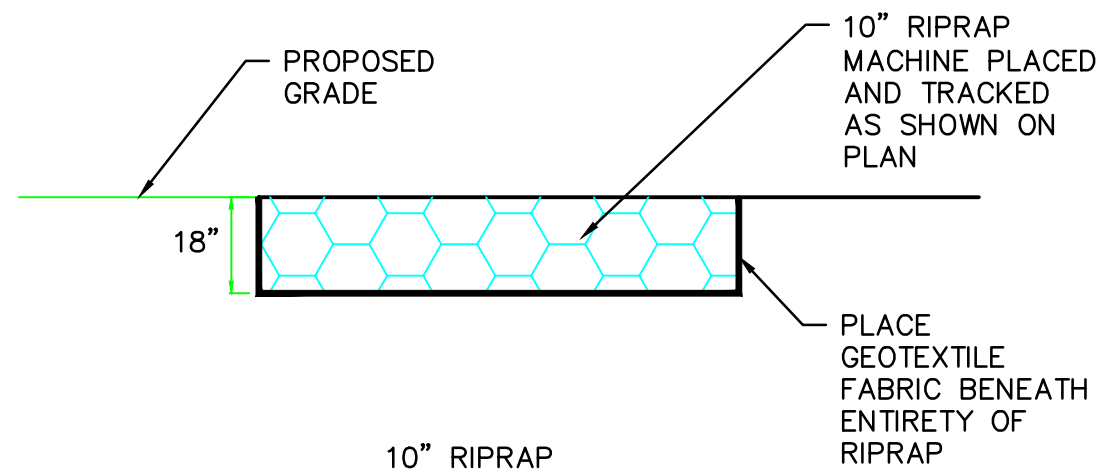


SECTION A-A

### END SECTION NOTES

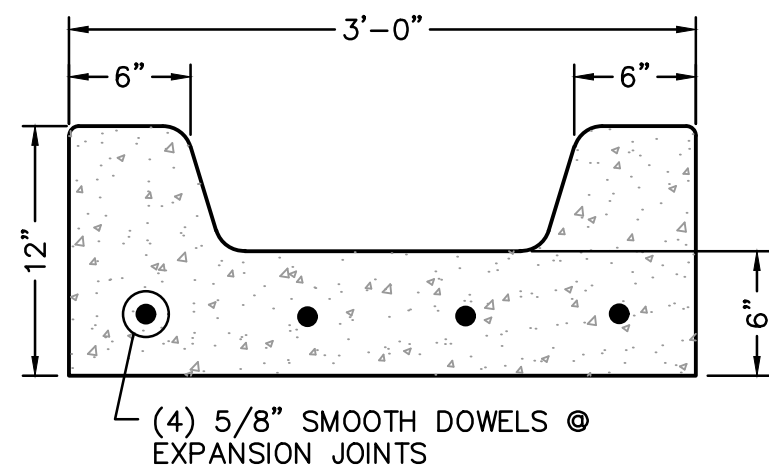
1. GRATE COVER DETAIL SHALL BE ADJUSTED AS NECESSARY TO FIT END SECTION PROVIDED.
2. MAXIMUM OPENING THRU END SECTION SHALL BE NO GREATER THAN 6". ADJUST DETAIL AS NECESSARY.
3. ALL METAL SURFACES SHALL BE HOT DIP ZINC COATED IN ACCORDANCE WITH ASTM A-123.
4. USE CITY APPROVED CONCRETE THROUGHOUT.
5. ALL CONCRETE AND MATERIALS USED IN THIS WORK SHALL MEET THE REQUIREMENTS OF THE GOVERNING BODY.
6. REINFORCING STEEL SHALL BE NEW BILLET, MINIMUM GRADE 40 AS PER ASTM A615, AND SHALL BE BENT COLD.
7. ALL DIMENSIONS RELATIVE TO REINFORCING STEEL ARE TO CENTERLINE OF BARS. 2" CLEARANCE SHALL BE PROVIDED THROUGHOUT UNLESS NOTED OTHERWISE. TOLERANCE OF  $\pm 1/8"$  SHALL BE PERMITTED.
8. ALL LAP SPLICES NOT SHOWN SHALL BE A MINIMUM OF 40 BAR DIAMETERS IN LENGTH.
9. ALL DOWELS SHALL BE ACCURATELY PLACED AND SECURELY TIED IN PLACE PRIOR TO PLACEMENT OF BOTTOM SLAB CONCRETE. STICKING OF DOWELS INTO FRESH OR PARTIALLY HARDENED CONCRETE WILL NOT BE ACCEPTABLE.
10. ALL REINFORCING STEEL SHALL BE SUPPORTED ON FABRICATED STEEL BAR SUPPORTS @ 3'-0" MAXIMUM SPACING.
11. DO NOT SCALE THESE DRAWINGS FOR DIMENSIONS OR CLEARANCES. ANY QUESTIONS REGARDING DIMENSIONS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO CONSTRUCTION.

END SECTION TOE WALL WITH GRATE  
NOT TO SCALE

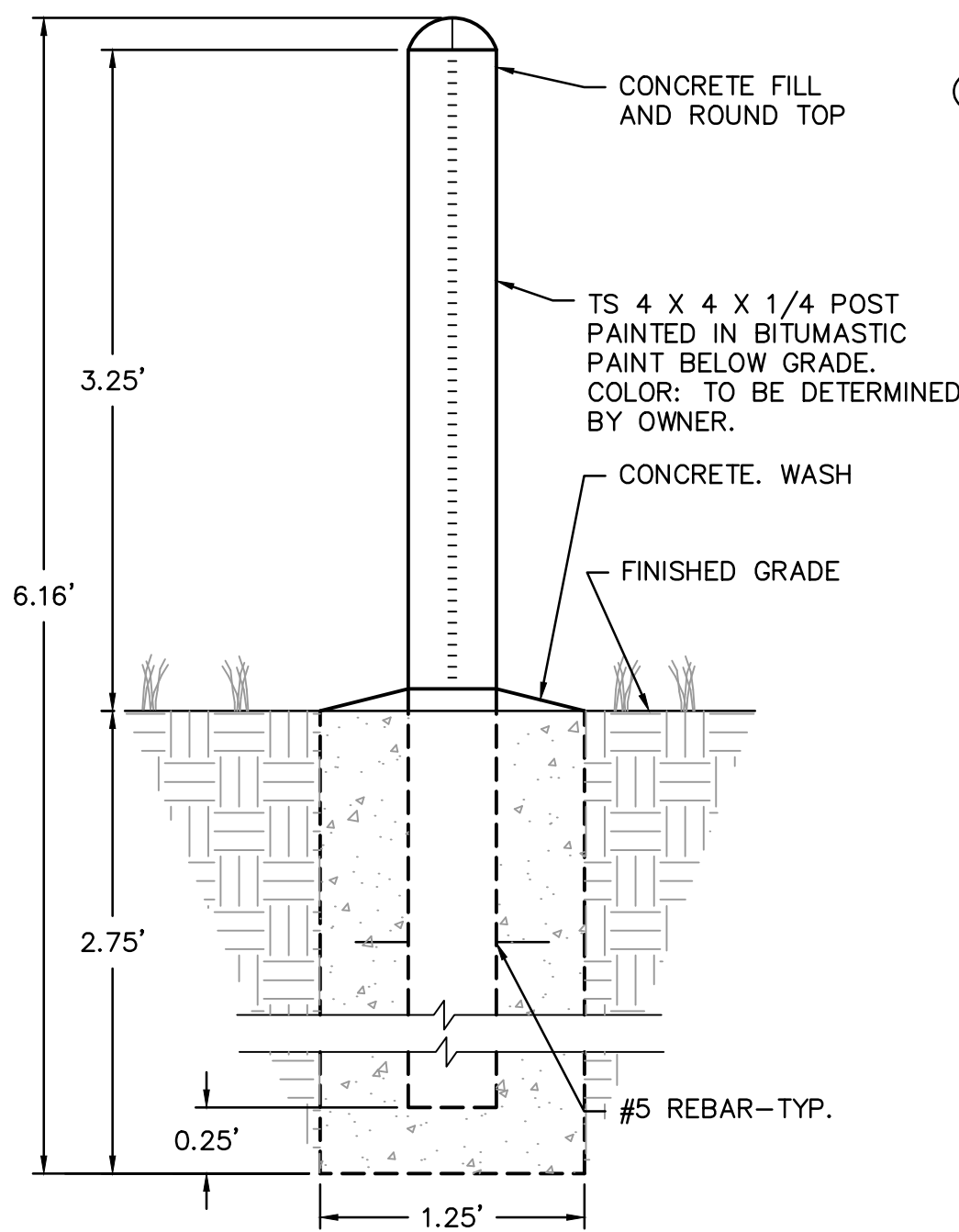


10" RIPRAP	
WEIGHT OF INDIVIDUAL PIECES	MINIMUM PERCENT LARGER THAN
500 lbs.	0%
250 lbs.	50%
125 lbs.	70%
10 lbs.	90%

STONE RIPRAP DETAIL  
NOT TO SCALE

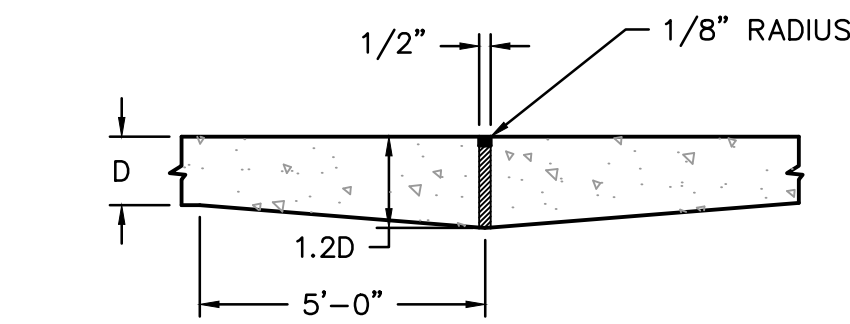


CONCRETE FLUME DETAIL  
NOT TO SCALE



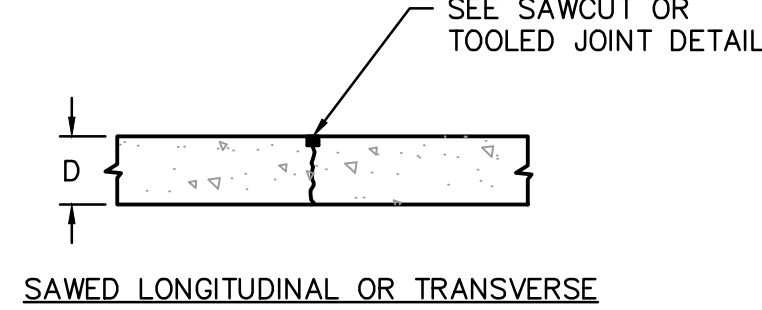
NOTE: WHEN PLACED IN PAVEMENT, TOP OF CONCRETE FOUNDATION SHALL BE PLACED BELOW NEW PAVEMENT WITH 1/2 ISOLATION JOINT & SEAL.

BOLLARD DETAIL  
NOT TO SCALE

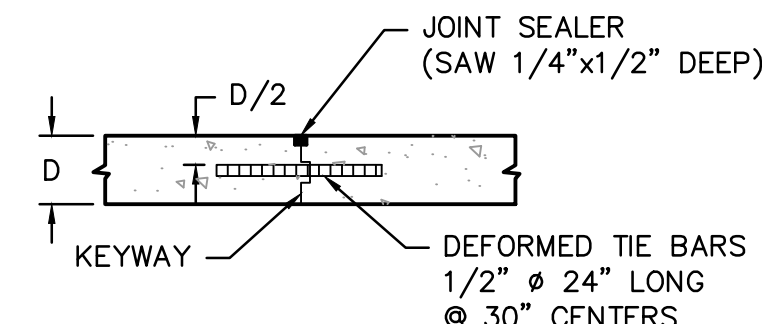


EXPANSION JOINT AT CONNECTION TO EXISTING PAVEMENT

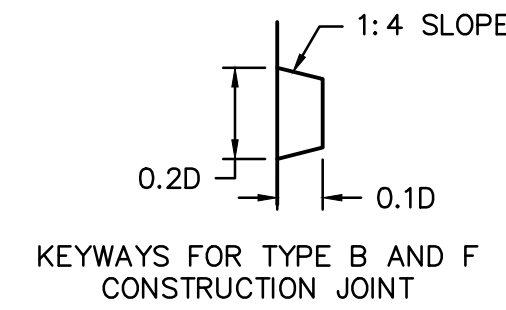
- NOTE:
1. EXPANSION JOINTS TO BE USED IN ALL AREAS THAT CONCRETE PAVEMENT ABUTS BUILDING
  2. 6X6X6 WELDED WIRE MESH IS TO BE USED IN ALL CONCRETE WALKS AROUND BUILDINGS.



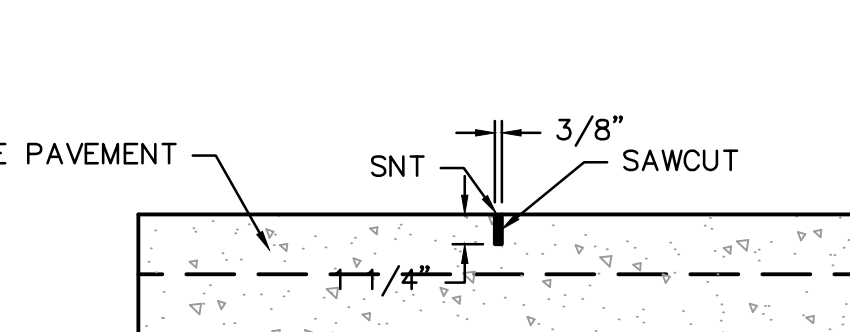
SAWED LONGITUDINAL OR TRANSVERSE



EMERGENCY TIED TRANSVERSE CONSTRUCTION JOINT  
(USED AT MIDDLE THIRD NORMAL JOINT SPACING)



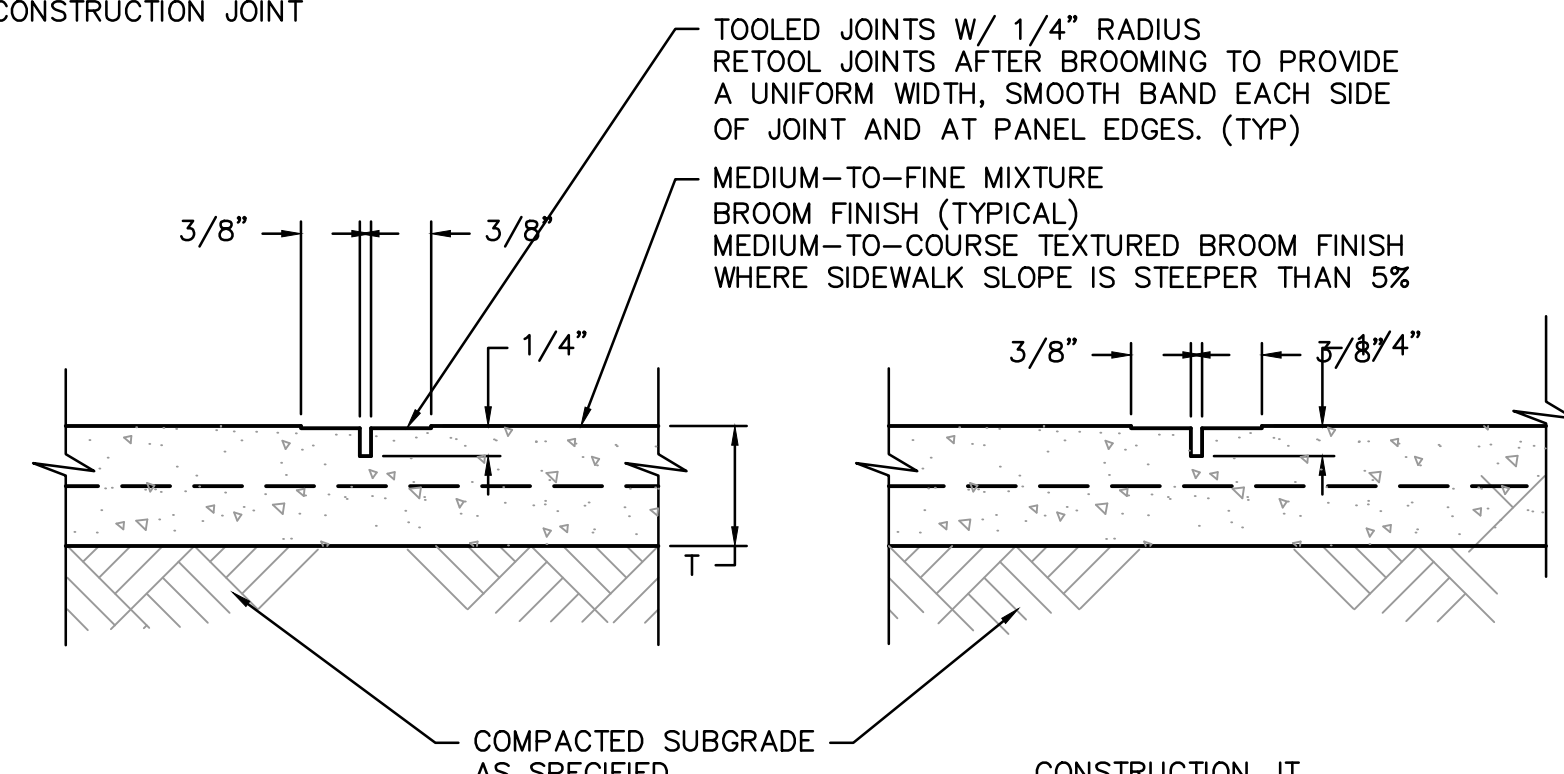
KEYWAYS FOR TYPE B AND F  
CONSTRUCTION JOINT



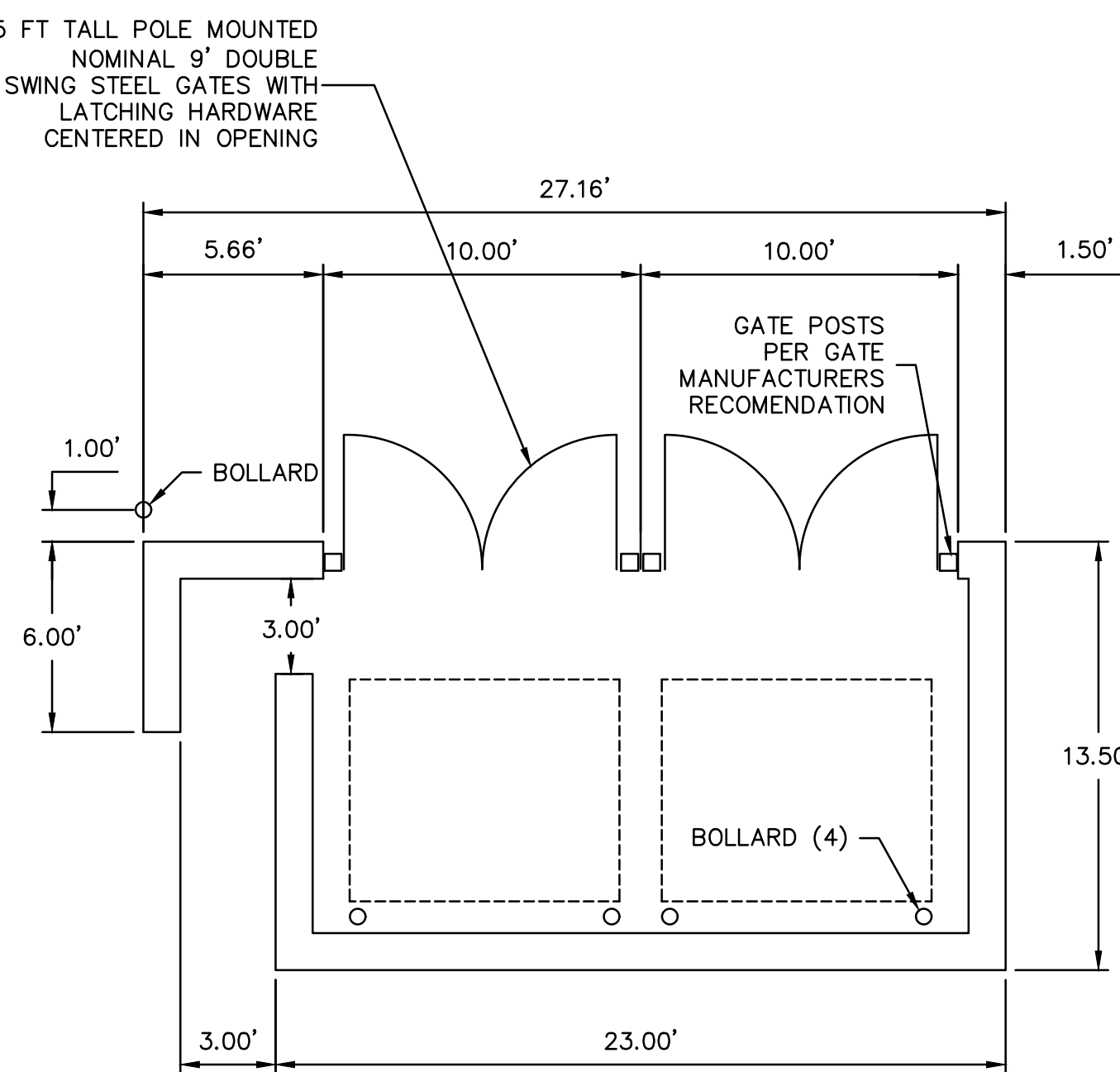
CONCRETE PAVEMENT CONTRACTION (SAWCUT) JOINT SECTION

SAWCUT JOINT

CONCRETE PAVING JOINT DETAILS  
NOT TO SCALE

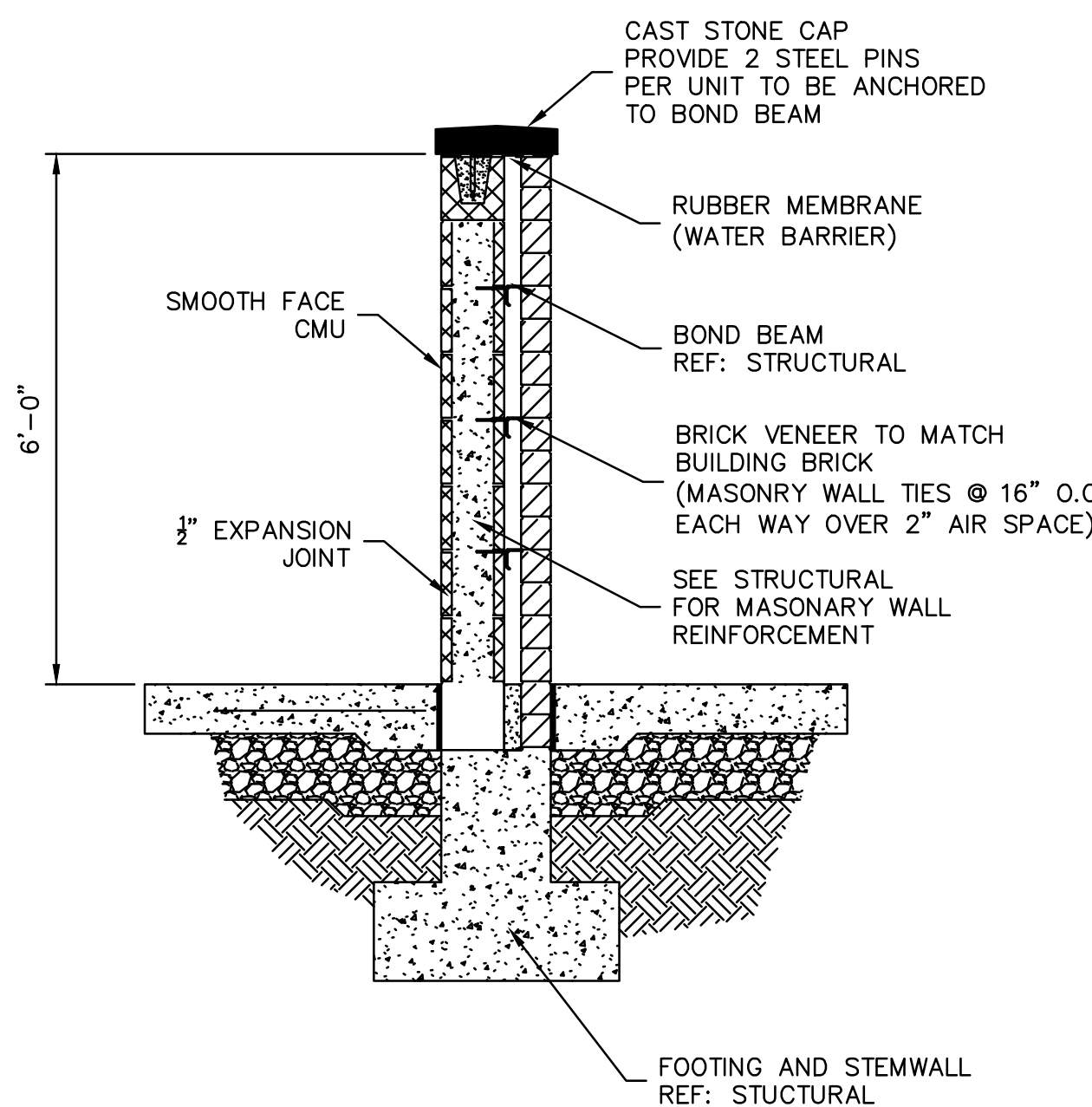


TOOLED JOINT



NOTE: ALL EXPOSED STEEL SHALL BE GALVANIZED AND EPOXY PAINTED. COLOR TO BE DETERMINED BY THE OWNER.

PLAN



TYPICAL WALL  
SECTION

**SAPP  
DESIGN  
ARCHITECTS**

3750 S. Fremont Ave.  
Springfield, MO 65804 417.877.9600

Sapp Design Associates Architects, P.C.  
Missouri State Certificate of Authority #000607

**helix.**

1629 Walnut  
Kansas City, MO 64108 816.300.0300

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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
**EAST LEE'S SUMMIT BRANCH**  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE  
**04**

Engineer of Record



Terry M Parsons, Engineer MO PE-2018010505

**olsson**

7301 West 133rd Street, Suite 200  
Overland Park, KS 66213  
TEL 913.381.1170  
FAX 913.381.1174  
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Olsson  
Missouri State Certificate of Authority #001592

Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

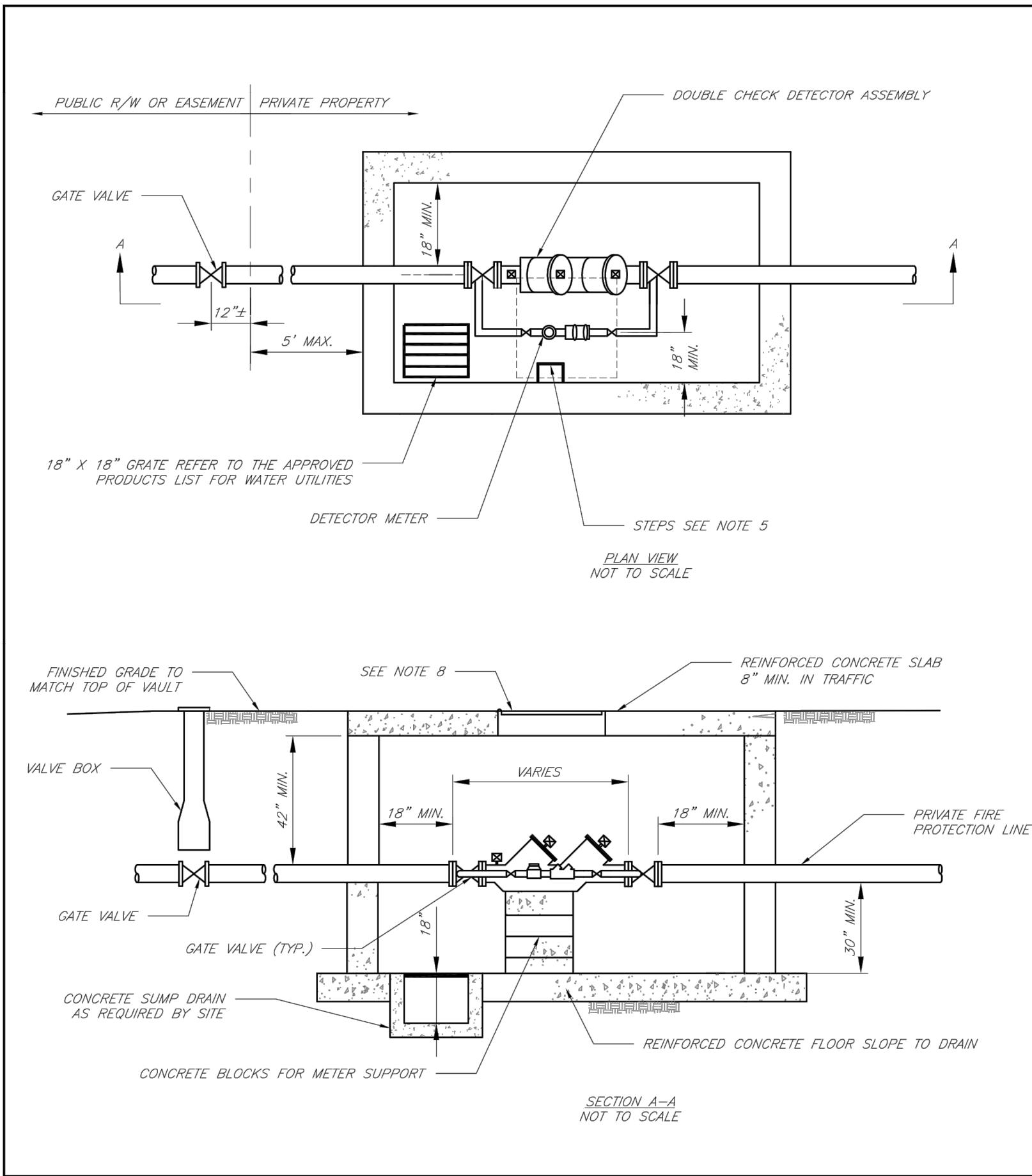
Project No. B18-0330 Date 12.07.18 Drawn RLK

Drawing No. **C10.1**

STANDARD DETAILS

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GENERAL NOTES:

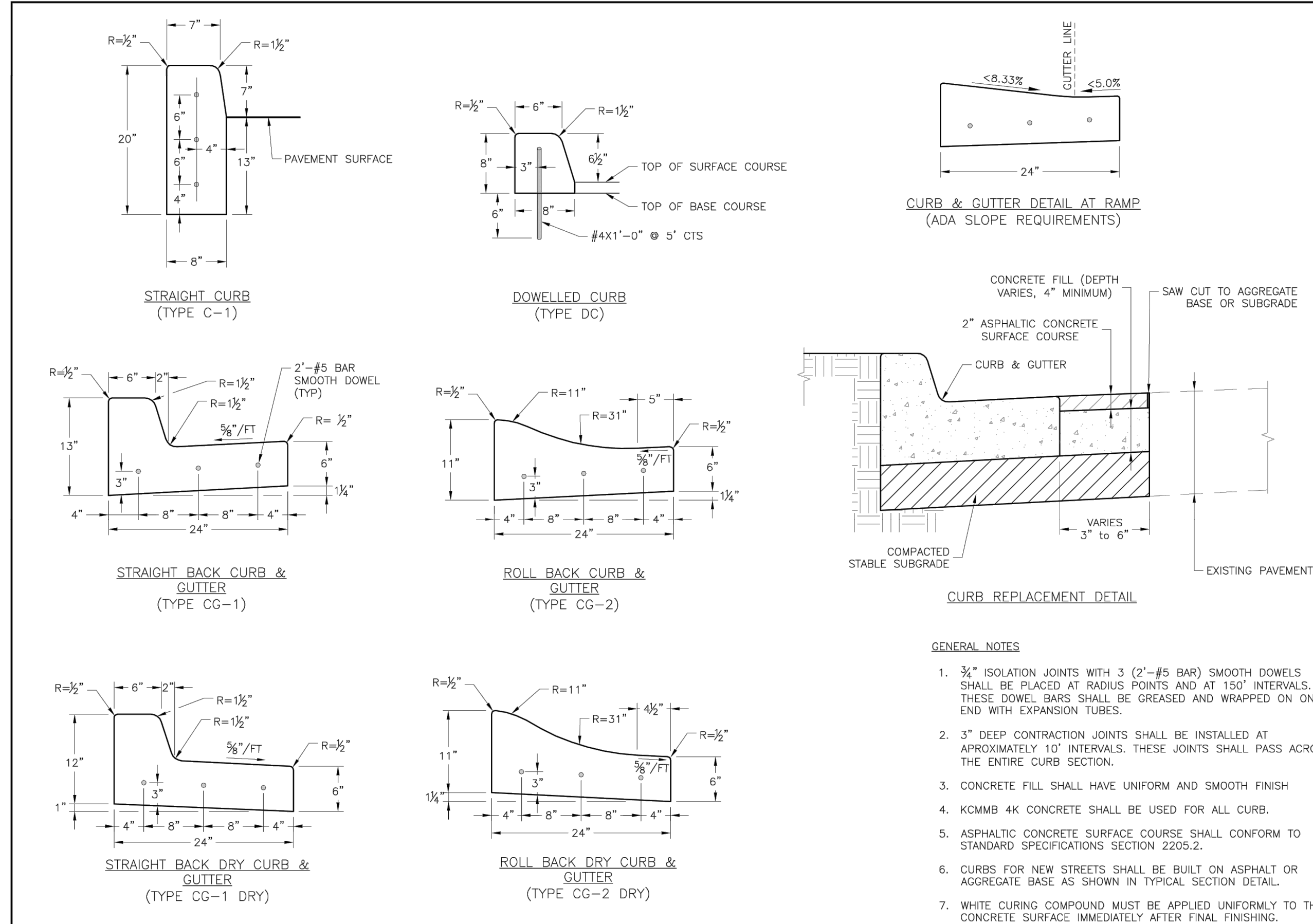
1. METER VAULT WALLS TO BE POURED OR PRECAST CONCRETE. METER VAULT ROOF TO BE REINFORCED CONCRETE WITH OPENING CENTERED OVER DETECTOR METER. REINFORCED WALLS AND SLABS ARE TO BE DESIGNED BY THE OWNER'S ENGINEER OR PRECAST ENGINEER.
2. METER VAULT TO BE LOCATED, WHEN POSSIBLE, OUTSIDE TRAFFIC AREA AND WHERE SURFACE WATER WILL NOT DRAIN INTO IT. PROVIDE CONCRETE SUMP TO DRAIN TO AN ABOVE GROUND DISCHARGE POINT.
3. ALL PIPE AND FITTINGS FROM THE CITY WATER MAIN THROUGH THE VAULT SHALL BE PROVIDED WITH RESTRAINED JOINT FITTINGS.
4. ALL FITTINGS FOR THE DETECTOR METER TO BE BRASS.
5. STEPS SHALL BE IN ACCORDANCE WITH THE APPROVED PRODUCTS LIST FOR WATER UTILITIES AND SHALL BE ON 16" CENTERS.
6. A DEPARTMENT OF NATURAL RESOURCES APPROVED DOUBLE CHECK DETECTOR ASSEMBLY BACKFLOW PREVENTER MUST BE USED. FOR A COPY OF THE MISSOURI DEPARTMENT OF NATURAL RESOURCES APPROVED BACKFLOW PREVENTION ASSEMBLIES, CONTACT WATER UTILITIES AT 916-969-1900.
7. ALL VALVES SHALL HAVE RISING STEMS.
8. MANHOLE COVER SHALL BE A BILCO K-1 MODEL UNLESS IN A VEHICLE TRAFFIC AREA. SEE THE APPROVED PRODUCTS LIST FOR WATER UTILITIES FOR TRAFFIC CONDITIONS. THE COVER SHALL HAVE A 1-3/4" Ø HOLE DRILLED FOR A TOUCH/READ DEVICE.
9. A MINIMUM OF 18" CLEARANCE SHALL BE PROVIDED AROUND ALL PIPING, VALVES, APPURTENANCES, ETC.
10. CONTACT PUBLIC WORKS ENGINEERING FOR VAULTS THAT INCLUDE A FIRE DEPARTMENT CONNECTION OR A 3" OR LARGER METER.



CITY OF LEE'S SUMMIT, MO  
PUBLIC WORKS DEPARTMENT  
ENGINEERING DIVISION  
LEES SUMMIT, MISSOURI

STANDARD DRAWINGS  
VAULT FOR DOUBLE CHECK DETECTOR CHECK

Drawn By: AS  
Checked By: JS  
Date: 7/14  
Rev: 1/14  
or  
WAT-12



GENERAL NOTES

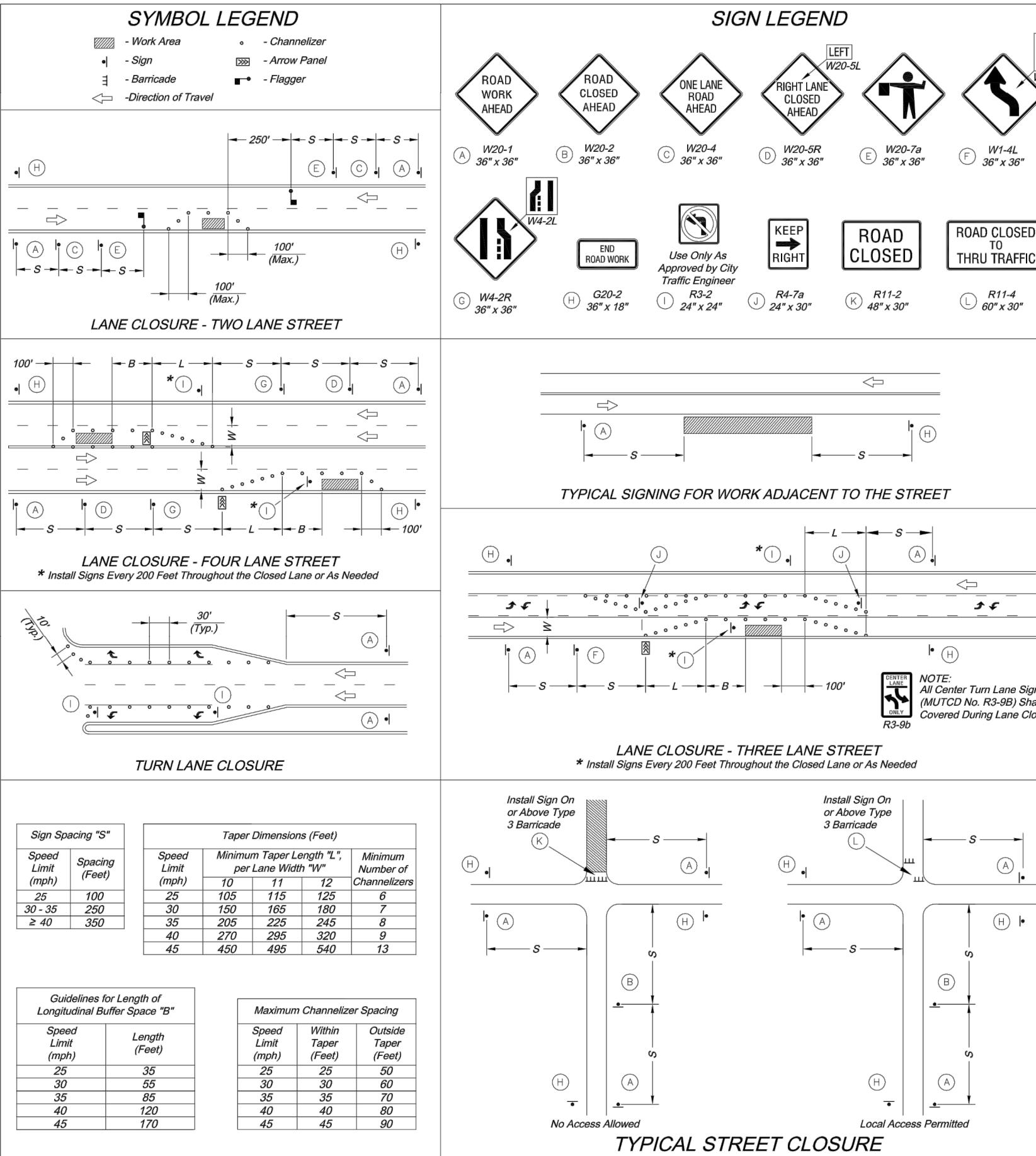
1. 3/4" ISOLATION JOINTS WITH 3 (2"-#5 BAR) SMOOTH DOWELS SHALL BE PLACED AT RADIUS POINTS AND AT 150' INTERVALS. THESE DOWEL BARS SHALL BE GREASED AND WRAPPED ON ONE END WITH EXPANSION TUBES.
2. 3" DEEP CONTRACTION JOINTS SHALL BE INSTALLED AT APPROXIMATELY 10' INTERVALS. THESE JOINTS SHALL PASS ACROSS THE ENTIRE CURB SECTION.
3. CONCRETE FILL SHALL HAVE UNIFORM AND SMOOTH FINISH.
4. KCMMB 4K CONCRETE SHALL BE USED FOR ALL CURBS.
5. ASPHALTIC CONCRETE SURFACE COURSE SHALL CONFORM TO STANDARD SPECIFICATIONS SECTION 2205.2.
6. CURBS FOR NEW STREETS SHALL BE BUILT ON ASPHALT OR AGGREGATE BASE AS SHOWN IN TYPICAL SECTION DETAIL.
7. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.

GEN-4

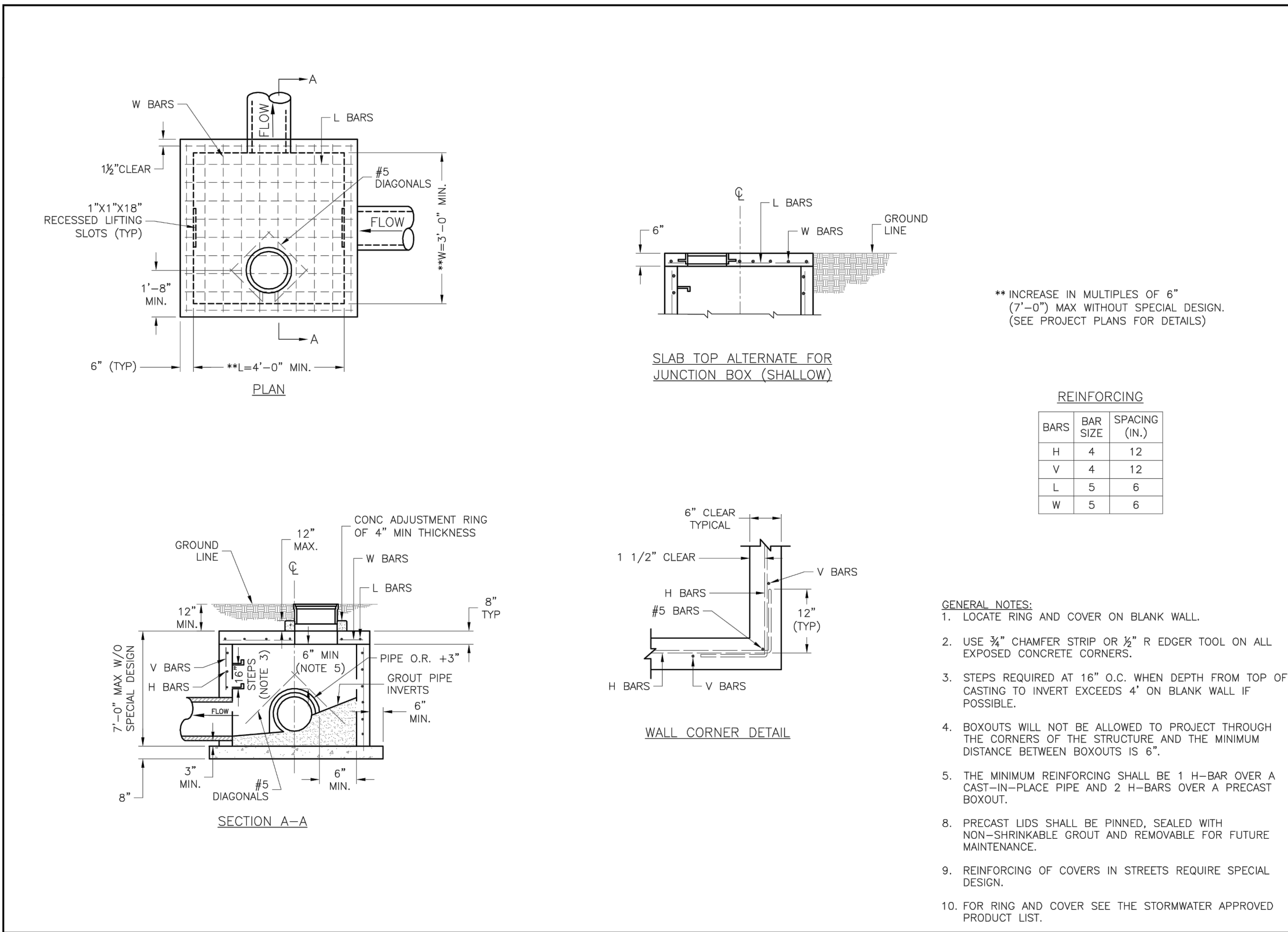
LEE'S SUMMIT  
MISSOURI

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEES SUMMIT, JACKSON COUNTY, MO  
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEES SUMMIT, MO 64663

CURB & GUTTER DETAIL





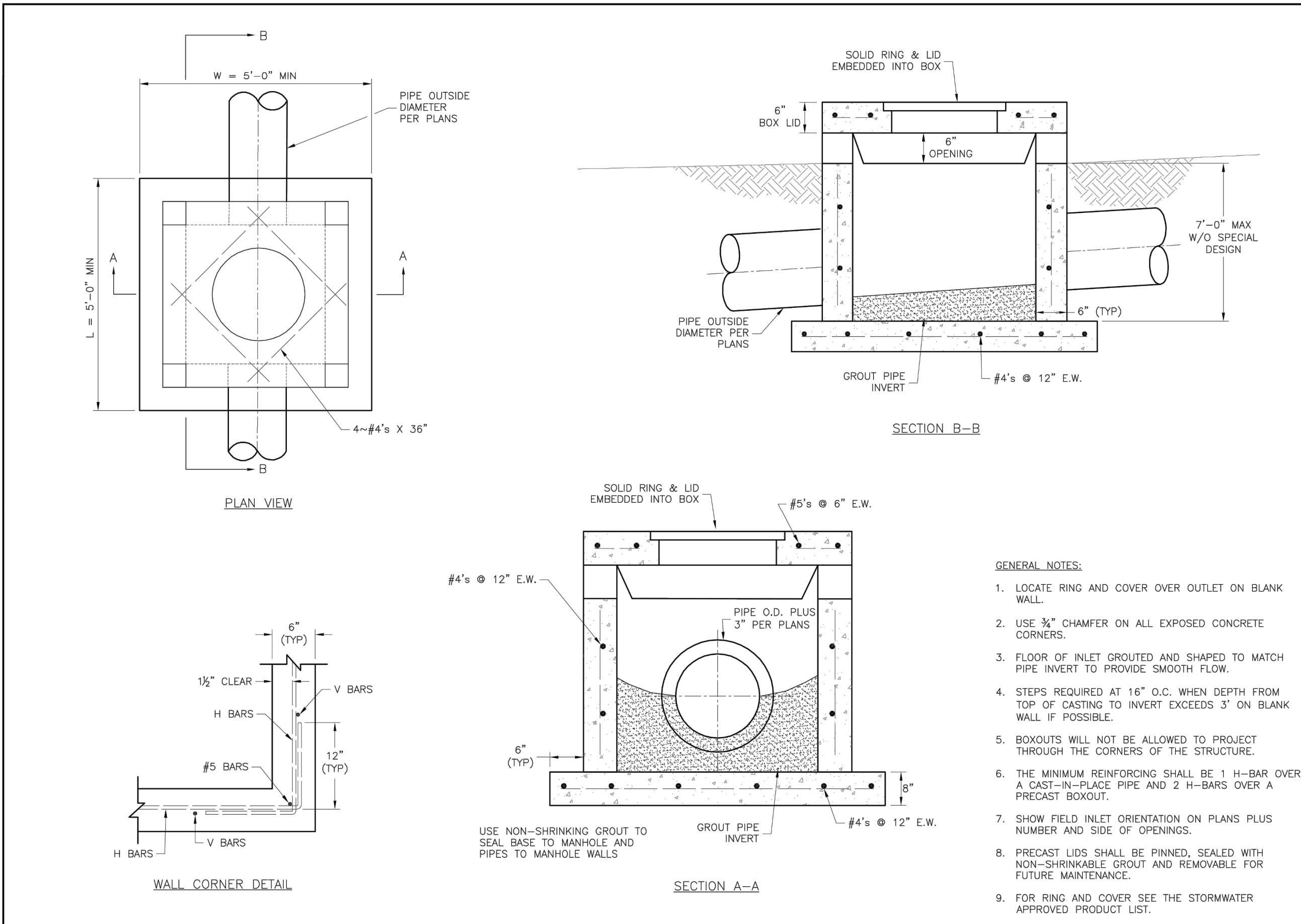


**LEE'S SUMMIT MISSOURI**  
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO

JUNCTION BOX DETAIL

STM-3

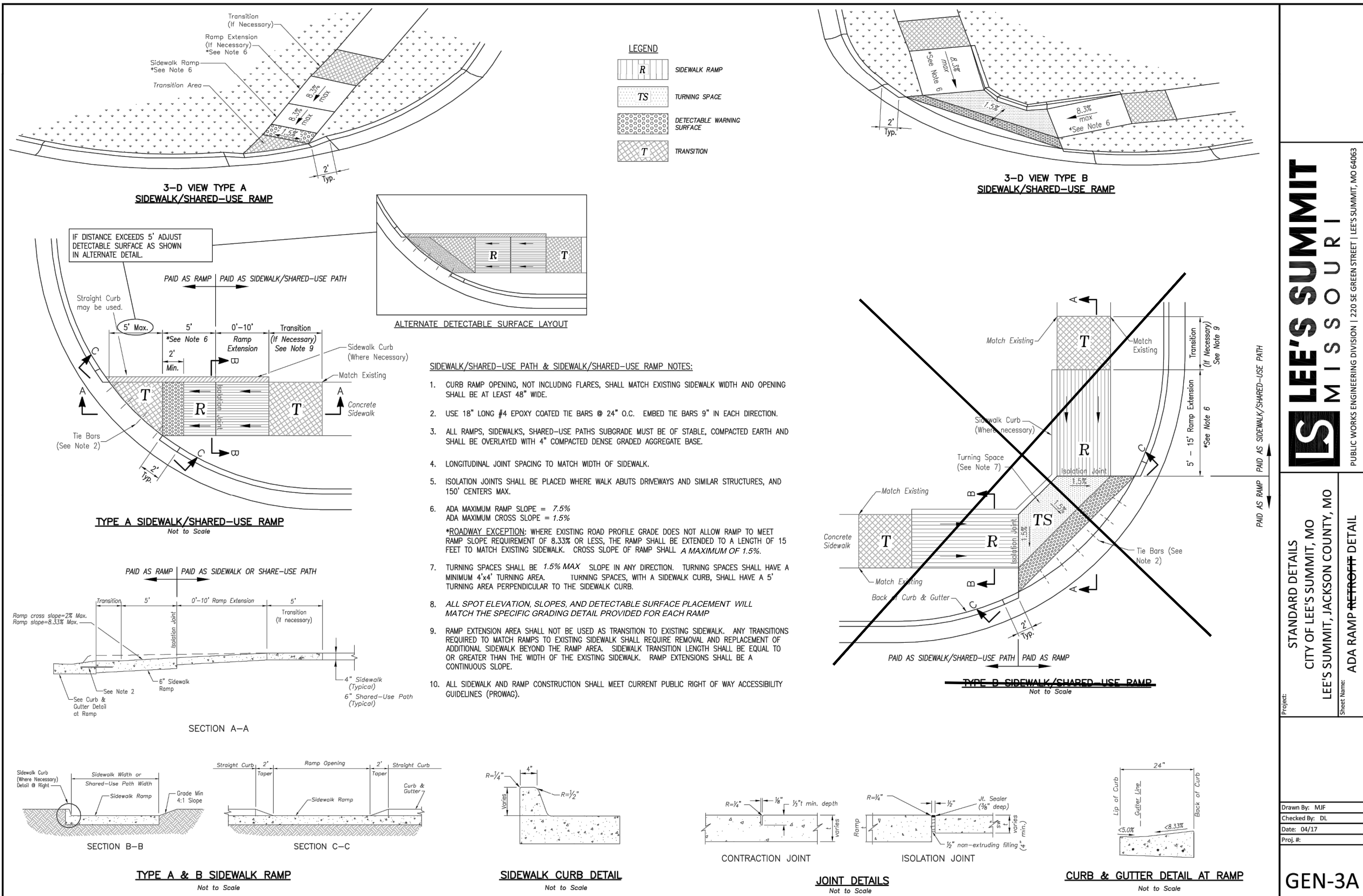


**LEE'S SUMMIT MISSOURI**  
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO

FIELD INLET DETAIL

STM-2

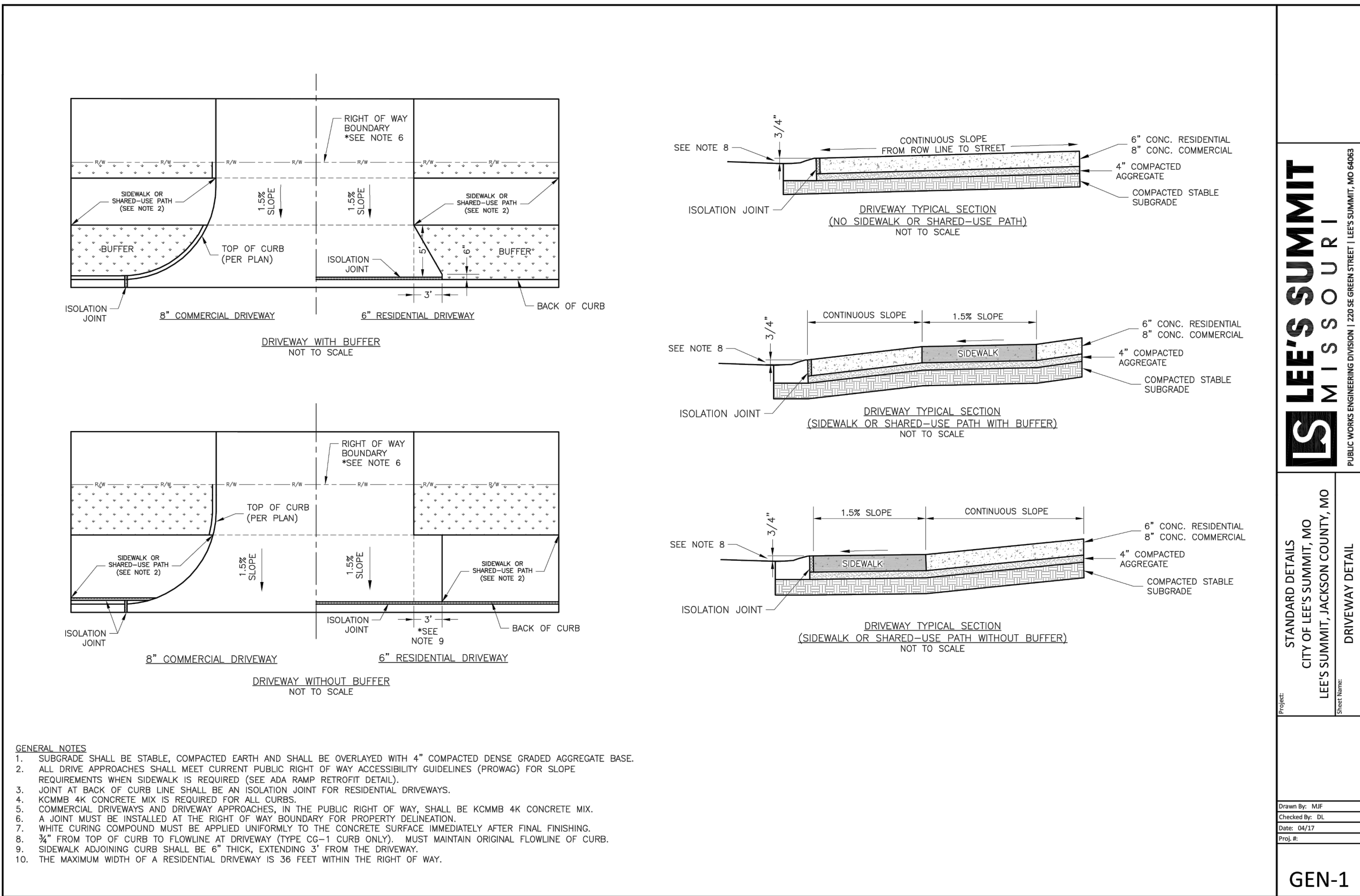


**LEE'S SUMMIT MISSOURI**  
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO

ADA RAMP DETAIL

GEN-3A



**LEE'S SUMMIT MISSOURI**  
PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

STANDARD DETAILS  
CITY OF LEE'S SUMMIT, MO  
LEE'S SUMMIT, JACKSON COUNTY, MO

DRIVEWAY DETAIL

GEN-1

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1629 Walnut  
Kansas City, MO 64108 816.300.0300

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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
**EAST LEE'S SUMMIT BRANCH**  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY

PACKAGE 04

Engineer of Record  
**TERRY M. PARSONS**  
NUMBER  
PE-2018010605  
03-26-2019  
PROFESSIONAL ENGINEER

Terry M Parsons, Engineer MO PE-2018010605

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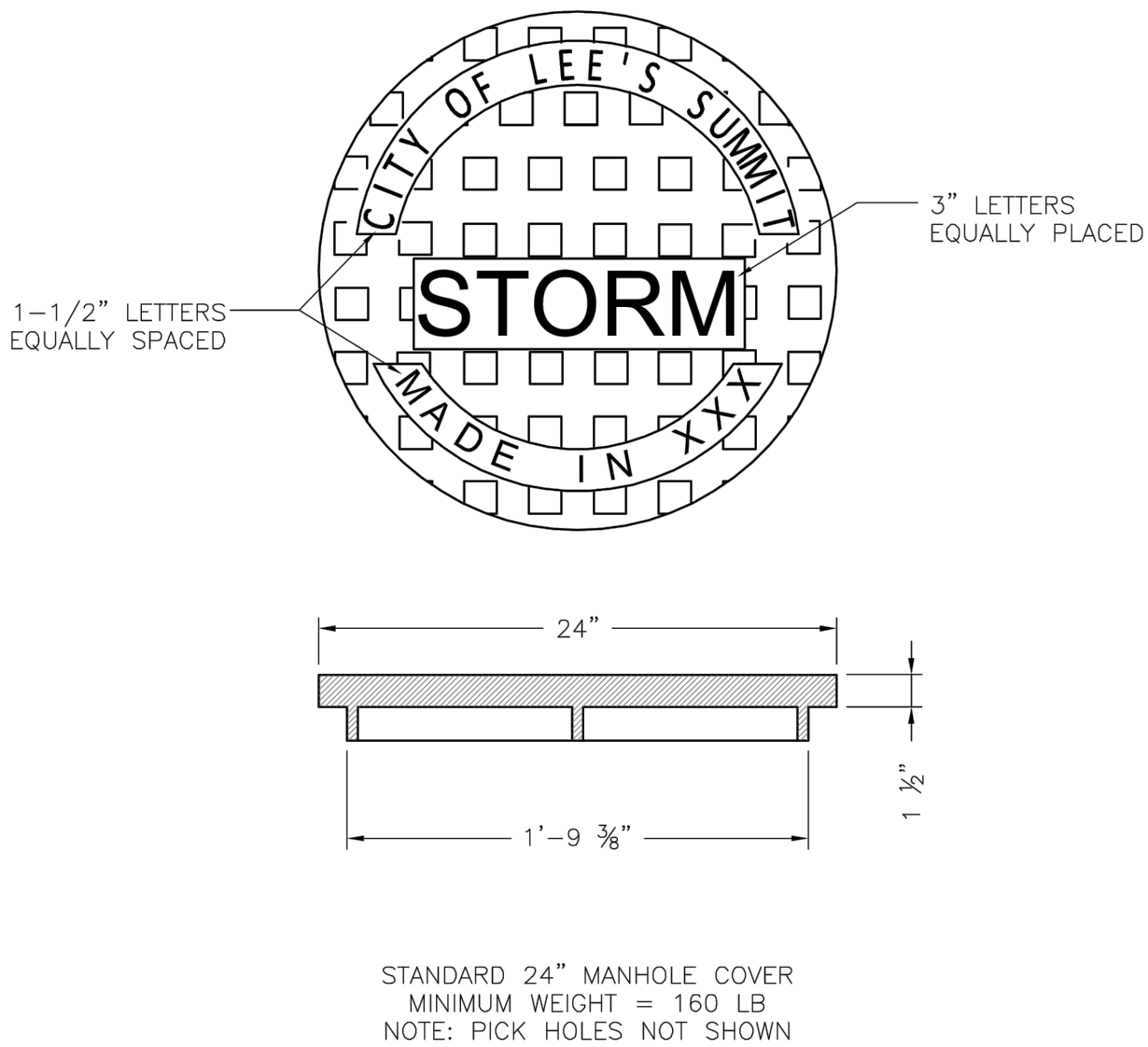
Revision No.	Description	Date
1	ASI#1	02.18.19
2	RESPONSE TO CITY COMMENTS	03.26.19

Project No. B18-0330 Date 12.07.18 Drawn RLK

Drawing No. **C10.3**  
STANDARD DETAILS

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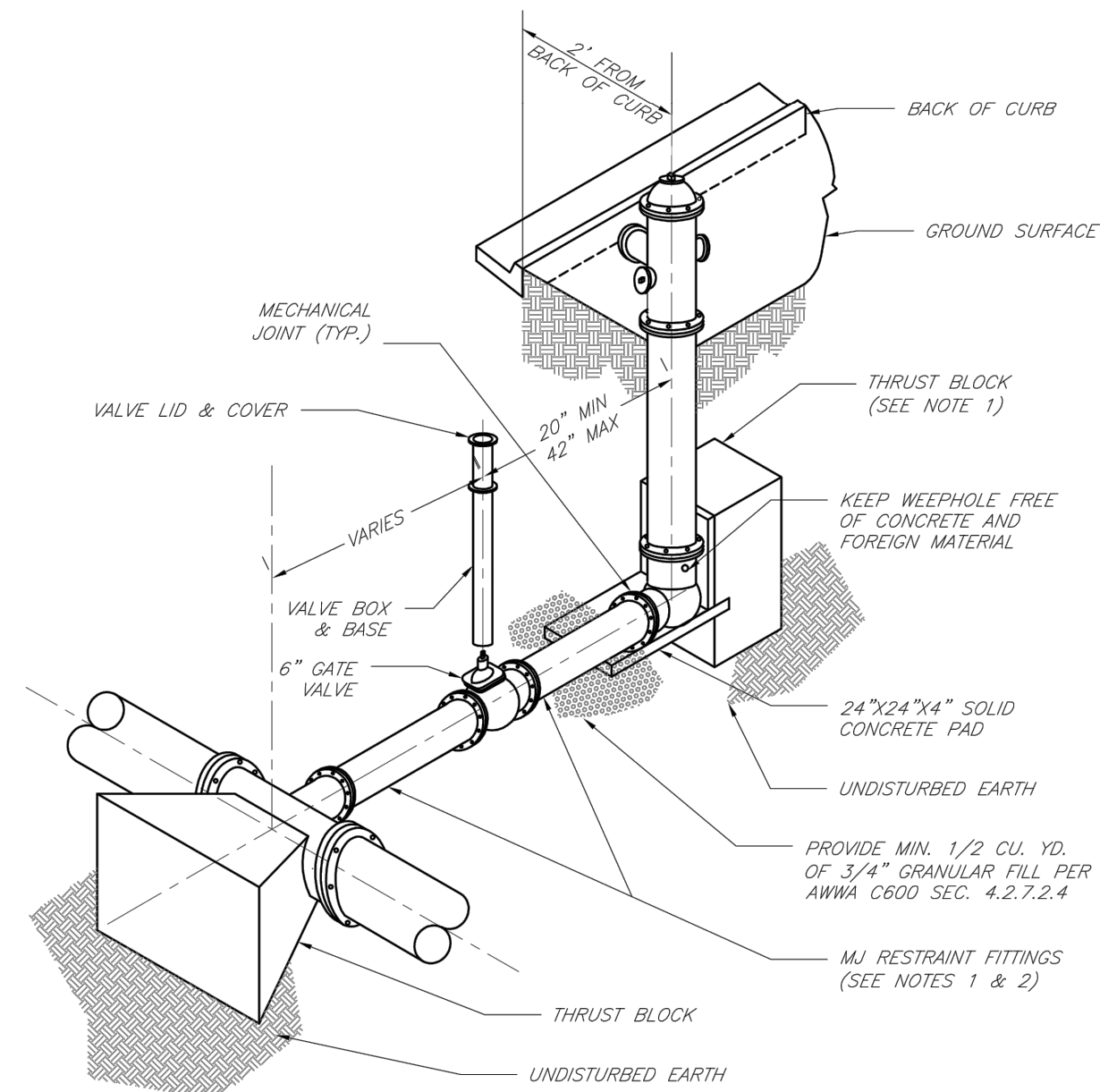




STANDARD 24" MANHOLE COVER  
MINIMUM WEIGHT = 160 LB  
NOTE: PICK HOLES NOT SHOWN

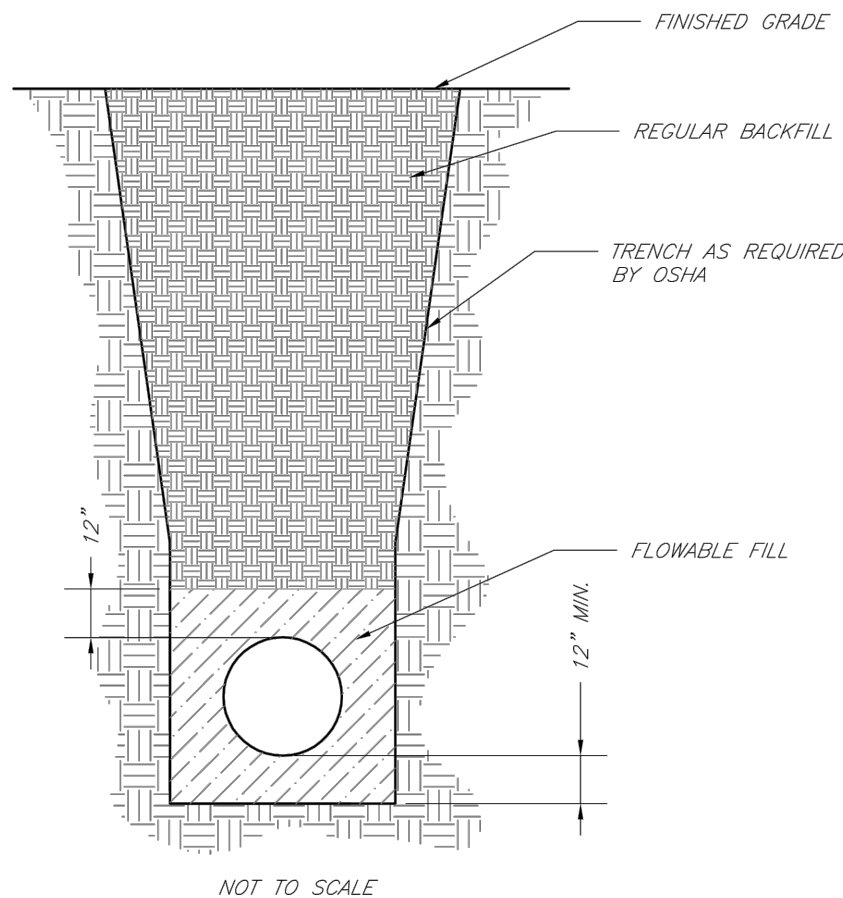
\*COVER AND FRAME MODEL INFORMATION REFER TO  
THE STORMWATER APPROVED PRODUCT LIST.

	<div>LEE'S SUMMIT MISSOURI</div> <div>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</div>	Date: 04/17
		Drawn By: MJF
		Checked By: DL
STORM MANHOLE COVER DETAIL		STM-6



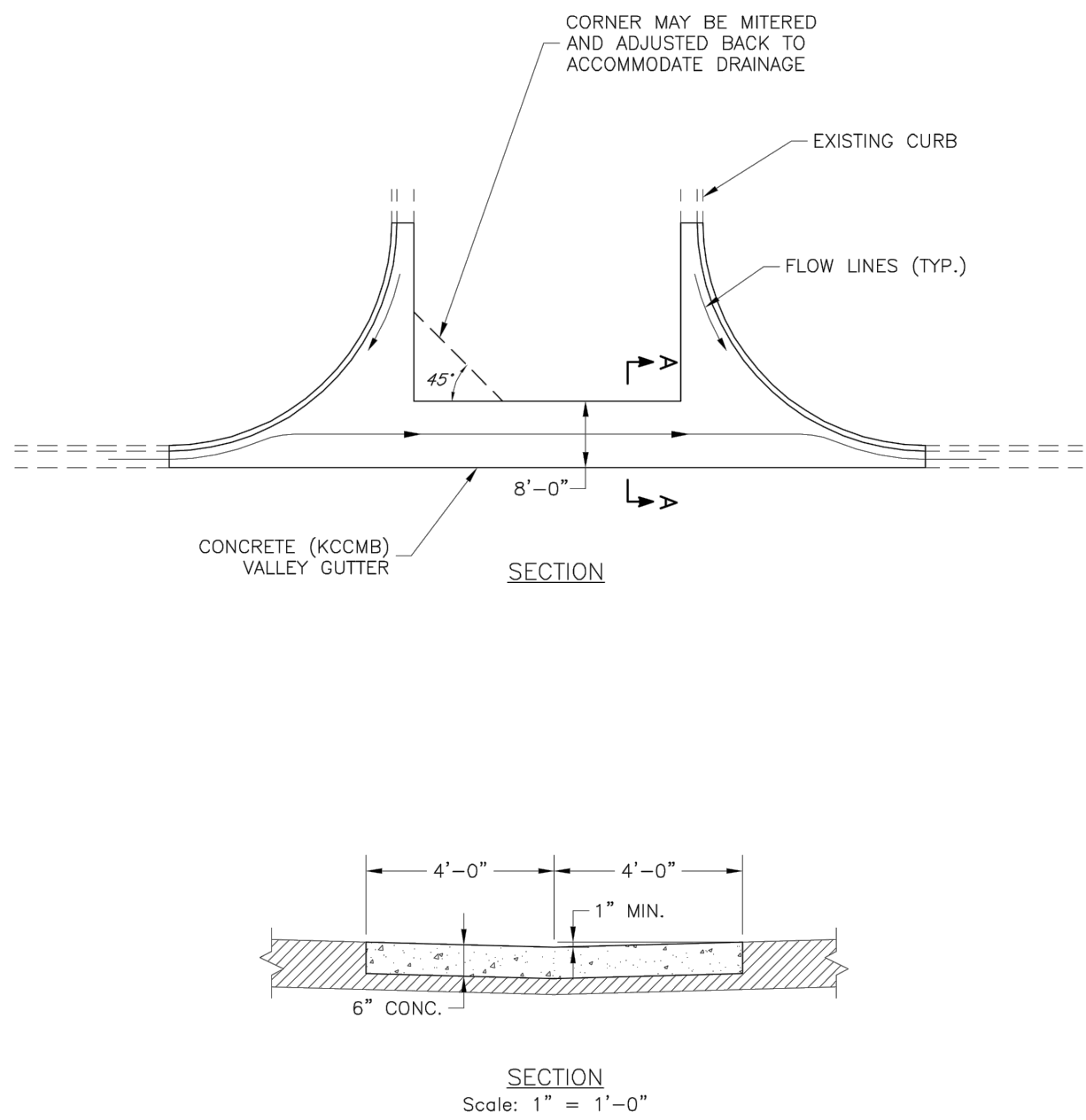
- NOTES:
1. WHEN RETAINER CLANDS ARE USED IN LIEU OF MECHANICAL JOINT (MJ) RESTRAINT FITTINGS, HORIZONTAL THRUST BLOCKS ARE REQUIRED.
  2. GATE VALVE MAY BE BOLTED DIRECTLY TO MJ RESTRAINT TEE.
  3. SEE APPROVED PRODUCTS LIST FOR WATER UTILITIES FOR FIRE HYDRANT, VALVES, VALVE BOX LID, AND COVER.
  4. BOTTOM HYDRANT FLANGE SHALL BE 2" TO 6" ABOVE FINISHED GRADE.
  5. FOR STREETS WITHOUT CURBS FIRE HYDRANTS SHALL BE PLACED WITHIN 1 FOOT OF THE R/W LINE, BUT NOT MORE THAN 10' FROM EDGE OF PAVEMENT. FIRE HYDRANT SHALL NOT BE PLACED IN BOTTOM OF DITCH.
  6. HYDRANT SHALL BE ROTATED AS DIRECTED BY INSPECTOR.

	<b>LEE'S SUMMIT</b> <b>MISSOURI</b> <small>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</small>	Date: 02/13
		Drawn By: JN
		Checked By: DL
		FILE: WAT-7
	<b>HYDRANT INSTALLATION — STRAIGHT SET</b>	
		Rev:



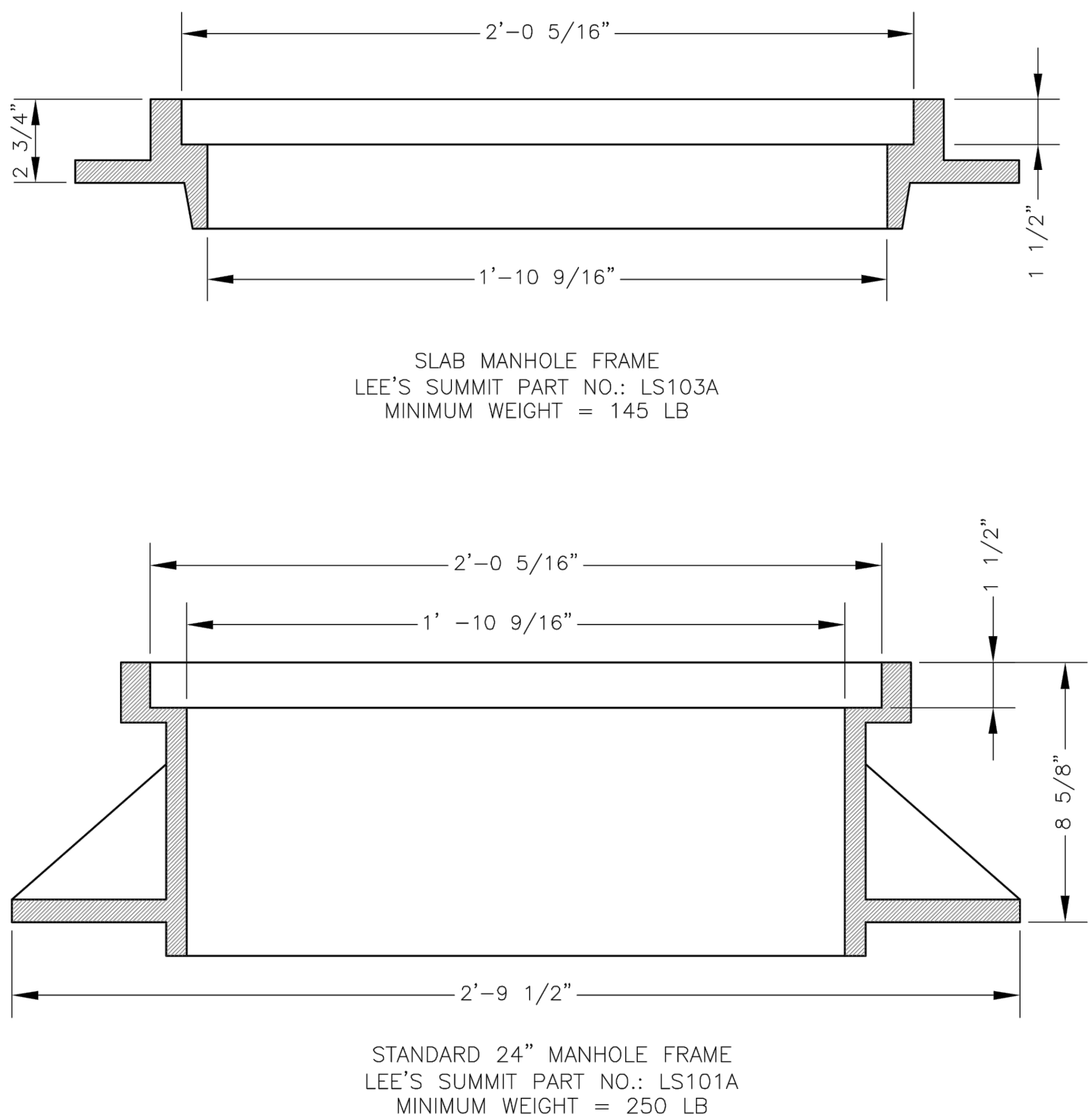
- NOTES:
1. FLOWABLE FILL SHALL MEET THE REQUIREMENTS OF THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL.
  2. REGULAR BACKFILL ABOVE THE TRENCH CHECK SHALL BE FREE OF DEBRIS, ORGANIC MATTER, AND STONES > 6" IN ANY DIMENSION.
  3. TOP OF FLOWABLE BACKFILL SHALL EXTEND 12" ABOVE THE TOP OF THE PIPE.
  4. LENGTH OF TRENCH CHECK SHALL BE A MINIMUM OF 12'.

	<div>LEE'S SUMMIT</div> <div>MISSOURI</div> <div>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</div>	Date: 02/13
		Drawn By: JN
		Checked By: DL
		FILE: WAT-6
		Rev: 1/14
TRENCH CHECK		Rev:



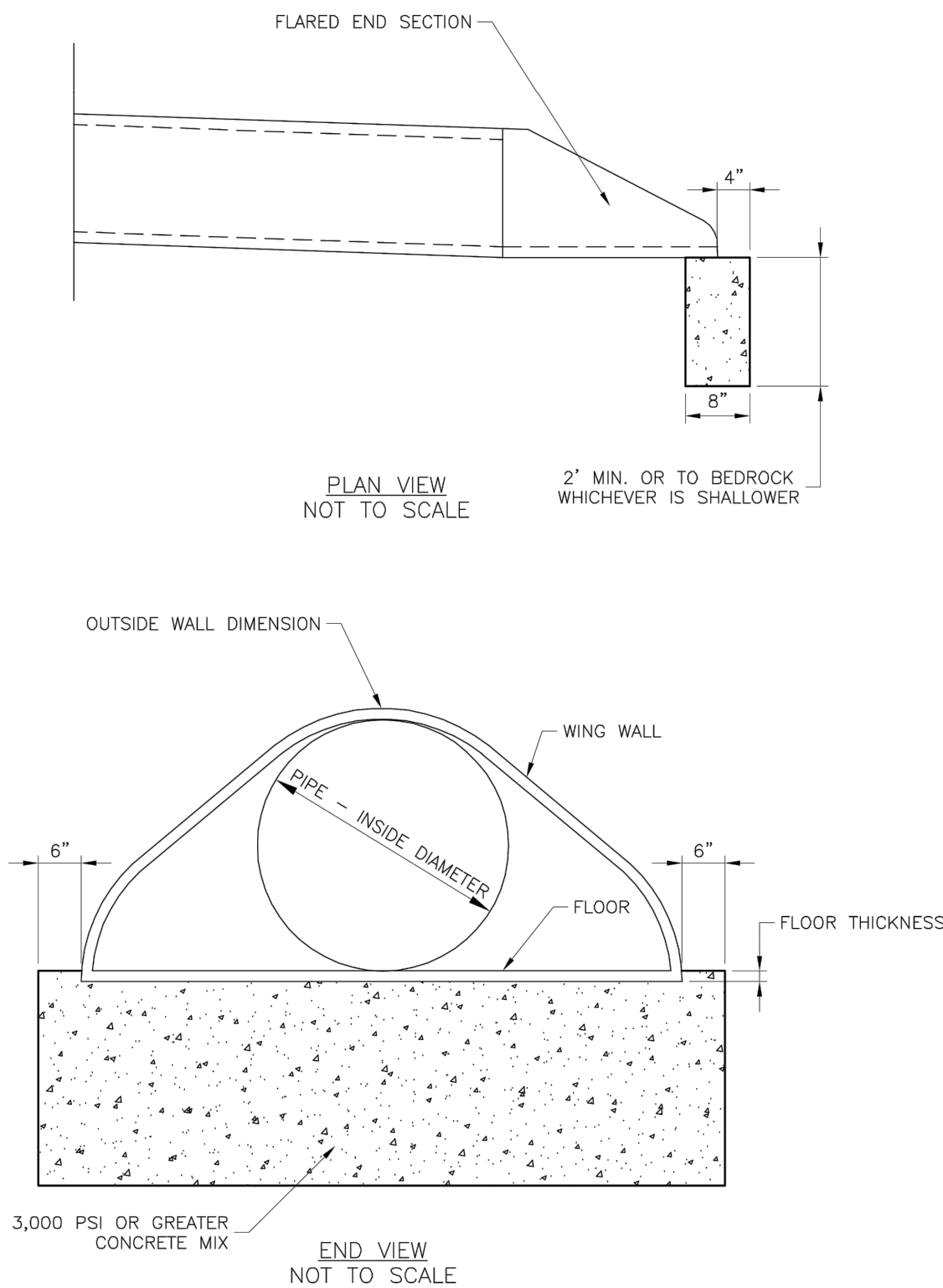
VALLEY GUTTER DETAIL  
Intersection of Two Public Streets

	<div>LEE'S SUMMIT MISSOURI</div> <div>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</div>	Date: 04/17
		Drawn By: MJF
		Checked By: DL
	VALLEY GUTTER DETAIL	

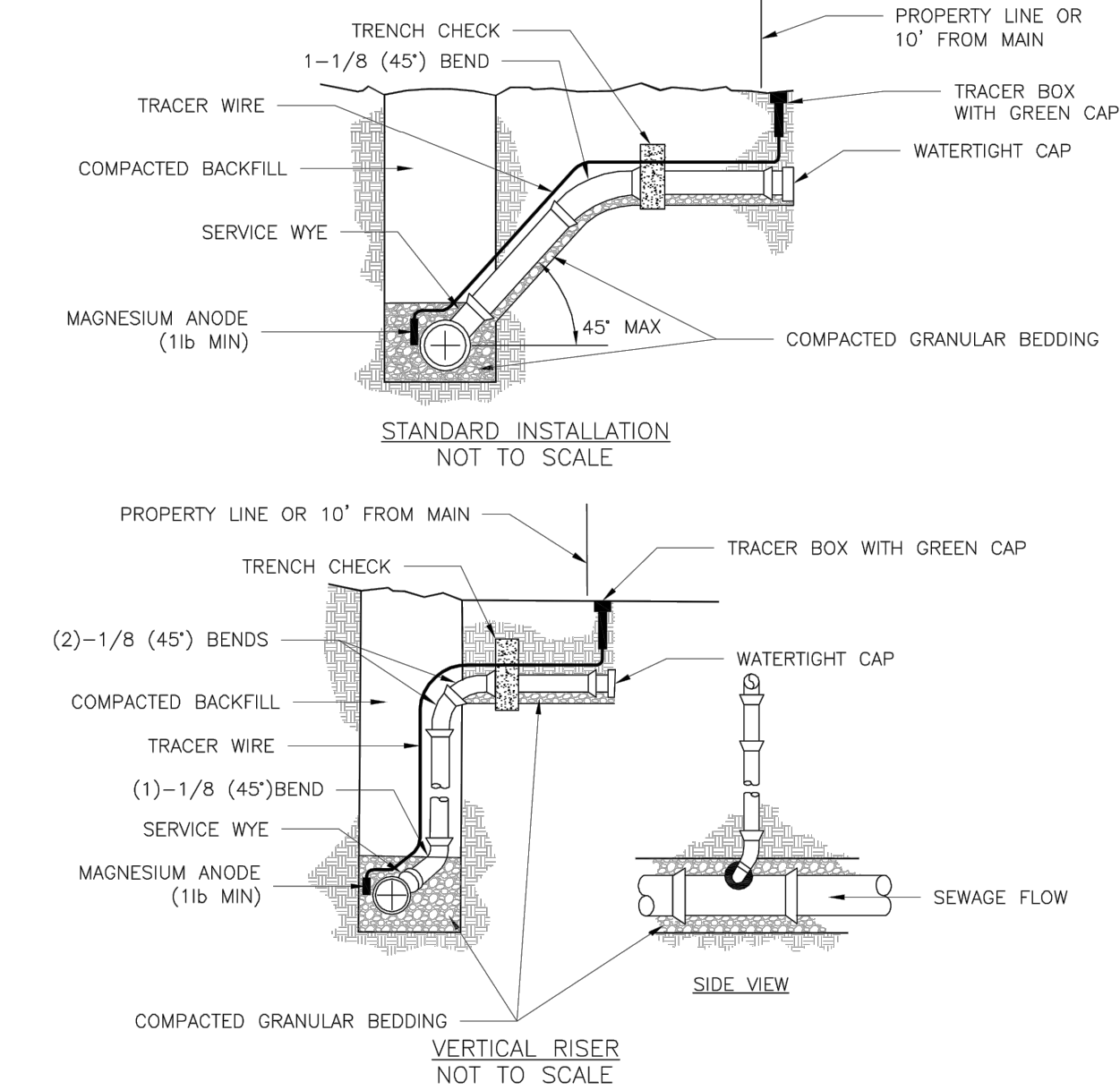


\*COVER AND FRAME MODEL INFORMATION REFER TO  
THE STORMWATER APPROVED PRODUCTS LIST.

	<div>LEE'S SUMMIT MISSOURI</div> <div>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</div>	Date: 04/17
		Drawn By: MIF
		Checked By: DL
STORM MANHOLE FRAME DETAIL		STM-7



	<b>LEE'S SUMMIT</b> <b>MISSOURI</b>		Date: 04/17
	PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063		Drawn By: MIF
	<b>FLARED END SECTION SUPPORT DETAIL</b>		Checked By: DL
			<b>STM-5</b>

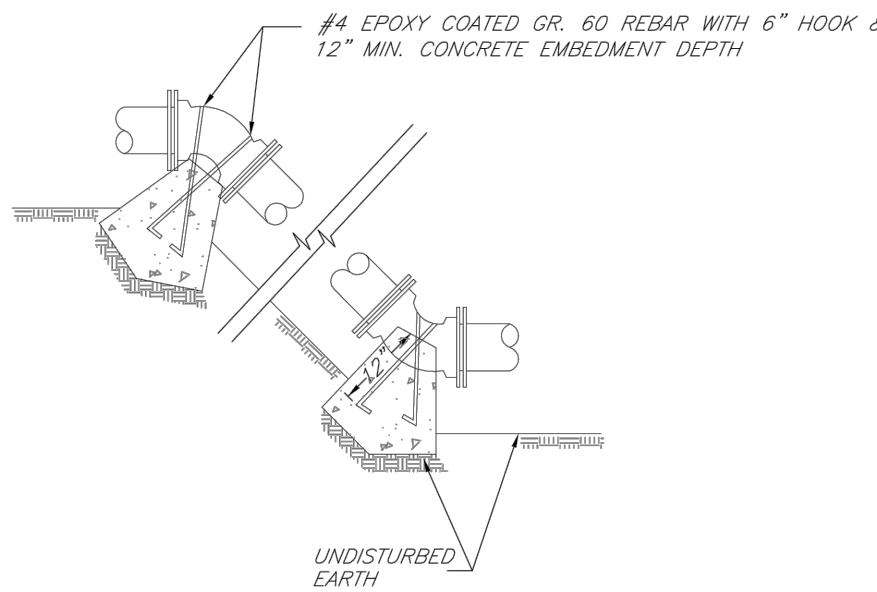



- NOTES:
1. ALL SEWER STUBS SHALL BE CONSTRUCTED TO PROPERTY LINE OR 10' MINIMUM FROM THE MAIN, WHERE SIDEWALKS ARE PRESENT, CONTRACTOR SHALL EXTEND SERVICE LINE UNDER EXISTING SIDEWALK TO TWO FEET BEYOND.
  2. ALL NEW CONSTRUCTION OFF SEWER STUBS SHALL BE TEMPORARILY MARKED WITH A MARKING STAKE, 36" ABOVE GROUND AND PAINTED GREEN.
  3. IMPERVIOUS TRENCH CHECKS SHALL BE PLACED ON BUILDING SEWER STUBS (AT LEAST 5' AWAY FROM THE SANITARY SEWER MAIN).
  4. TRENCH CHECKS ON THE BUILDING SEWER STUBS SHALL EXTEND 6" BELOW THE BOTTOM OF THE PIPE. LENGTH SHALL BE A MINIMUM OF 12'. THE HEIGHT OF THE TRENCH CHECK SHALL EXTEND 12" ABOVE THE TOP OF THE PIPE. THE WIDTH OF THE TRENCH CHECK SHALL BE THE WIDTH OF THE TRENCH.
  5. SEE SPECIFICATION SECTION 2100 FOR SEWER MAIN BEDDING AND BACKFILL.
  6. #12 GAUGE GREEN INSULATED COPPER TRACER WIRE SHALL BE INSTALLED. TRACER WIRE TERMINAL BOXES SHALL BE INSTALLED DIRECTLY ABOVE THE SEWER SERVICE OR AS DETERMINED BY THE ENGINEER.
  7. FOR SERVICES, TRACER WIRE SHALL RUN FROM THE WYE AND TERMINATE IN A FLUSH MOUNTED TRACER BOX WITH A GREEN CAST IRON LOOKALIKE TOP. WIRE SHALL BE TAPED OR TIED TO THE PIPE AT 5' INTERVALS.
  8. TRACER WIRE BOX SHALL BE INSTALLED WITHIN 1.0' OF PROPERTY LINE.
  9. THE TRACER WIRE SHALL REMAIN CONTINUOUS TO THE GREATEST EXTENT POSSIBLE. SPLICES IN THE TRACER WIRE SHOULD BE MADE WITH SPLIT BOLT CONNECTORS. WIRE NUTS SHALL NOT BE USED. A WATER-PROOF CONNECTION IS NECESSARY TO PREVENT CORROSION.

	<div>LEE'S SUMMIT</div> <div>MISSOURI</div> <div>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</div>	Date: 04/17
		Drawn By: MJF
		Checked By: DL
	BUILDING SEWER STUB AND RISER	

REQUIRED CONCRETE VOLUME (CUBIC FEET - CF)				
NOM. DIA. (INCHES)	TEE, PLUG	90 BEND	45 BEND	22.5 BEND
6	50.5	71.4	38.6	19.7
8	69.9	126.9	68.7	35.0
10	140.2	198.3	107.3	54.7
12	232.0	325.7	154.6	78.8
14	348.1	507.1	231.4	107.3
16	494.1	731.1	331.1	140.1
18	671.1	991.1	451.1	181.1
20	881.1	1291.1	591.1	231.1
24	1381.1	2091.1	1091.1	551.1

- NOTES:
1. ALL BENDS WITHOUT RESTRAINED JOINTS SHALL HAVE CONCRETE THRUST BLOCKS INSTALLED FOR RESTRAINT.
  2. MEGA LUGS MAY BE USED ONLY IN CONJUNCTION WITH CONCRETE THRUST BLOCKING.
  3. BEARING MUST BE AGAINST UNDISTURBED SOIL.
  4. DO NOT COVER JOINTS OR BOLTS (WHERE APPLICABLE) WITH CONCRETE.



	<div>LEE'S SUMMIT MISSOURI</div> <div>PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</div>	Date: 02/13
		Drawn By: JN
		Checked By: DL
		FILE: WAT-2
		<div>VERTICAL THRUST BLOCKS</div>
		Rev:

**SAPP  
DESIGN  
ARCHITECTS**

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Missouri State Certificate of Authority #000607

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Kansas City, MO 64108 816.300.0300

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Mid-Continent Public Library  
CONSTRUCTION DOCUMENT PLANS FOR  
**EAST LEE'S SUMMIT BRANCH**  
2240 SE BLUE PARKWAY  
LEE'S SUMMIT, MO 64063  
JACKSON COUNTY  
PACKAGE **04**

Engineer of Record  
**TERRY M. PARSONS**  
NUMBER  
PE-2018010505  
03-26-2019  
PROFESSIONAL ENGINEER

Terry M Parsons, Engineer MO PE-2018010505

**olsson**

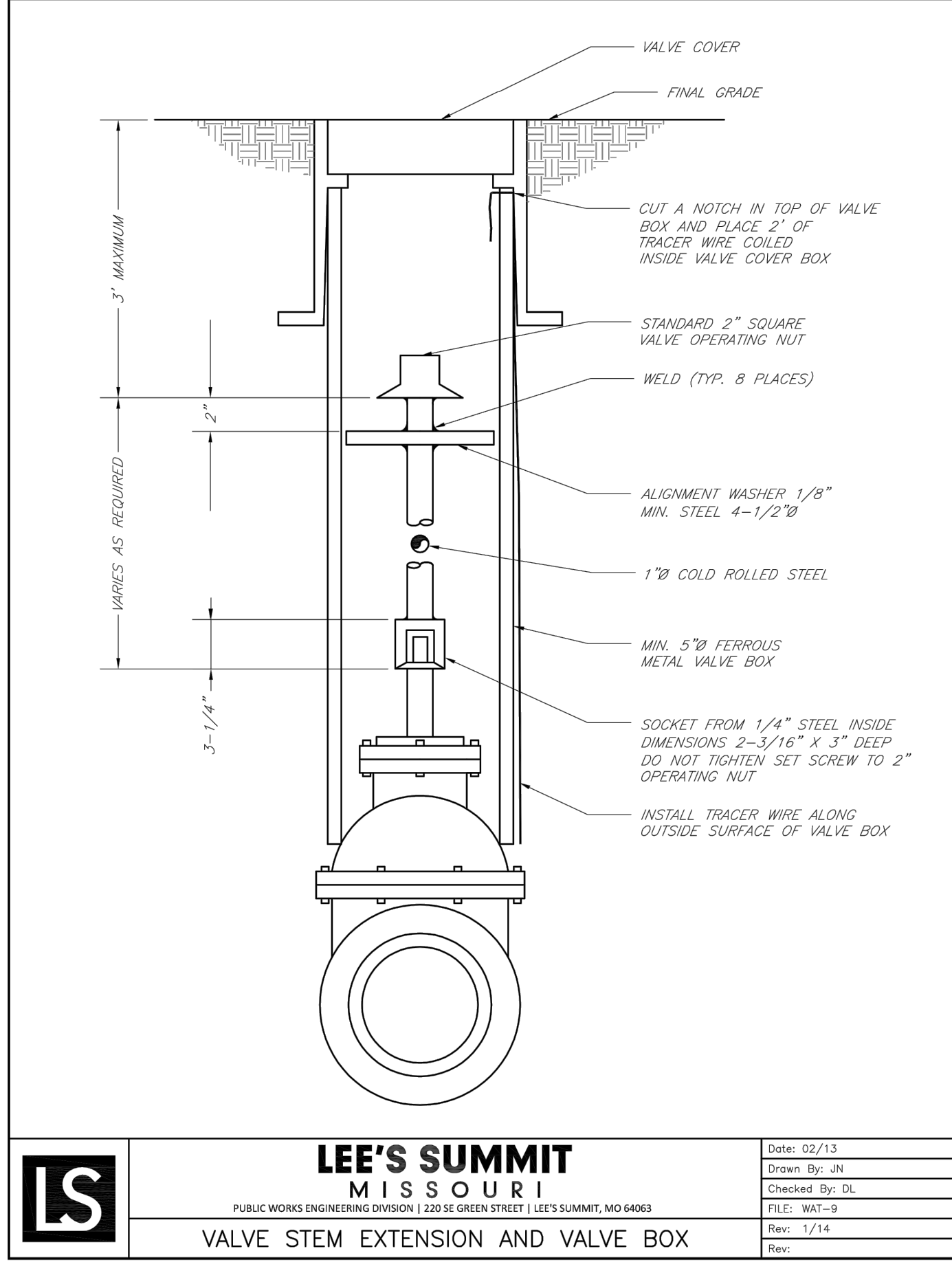
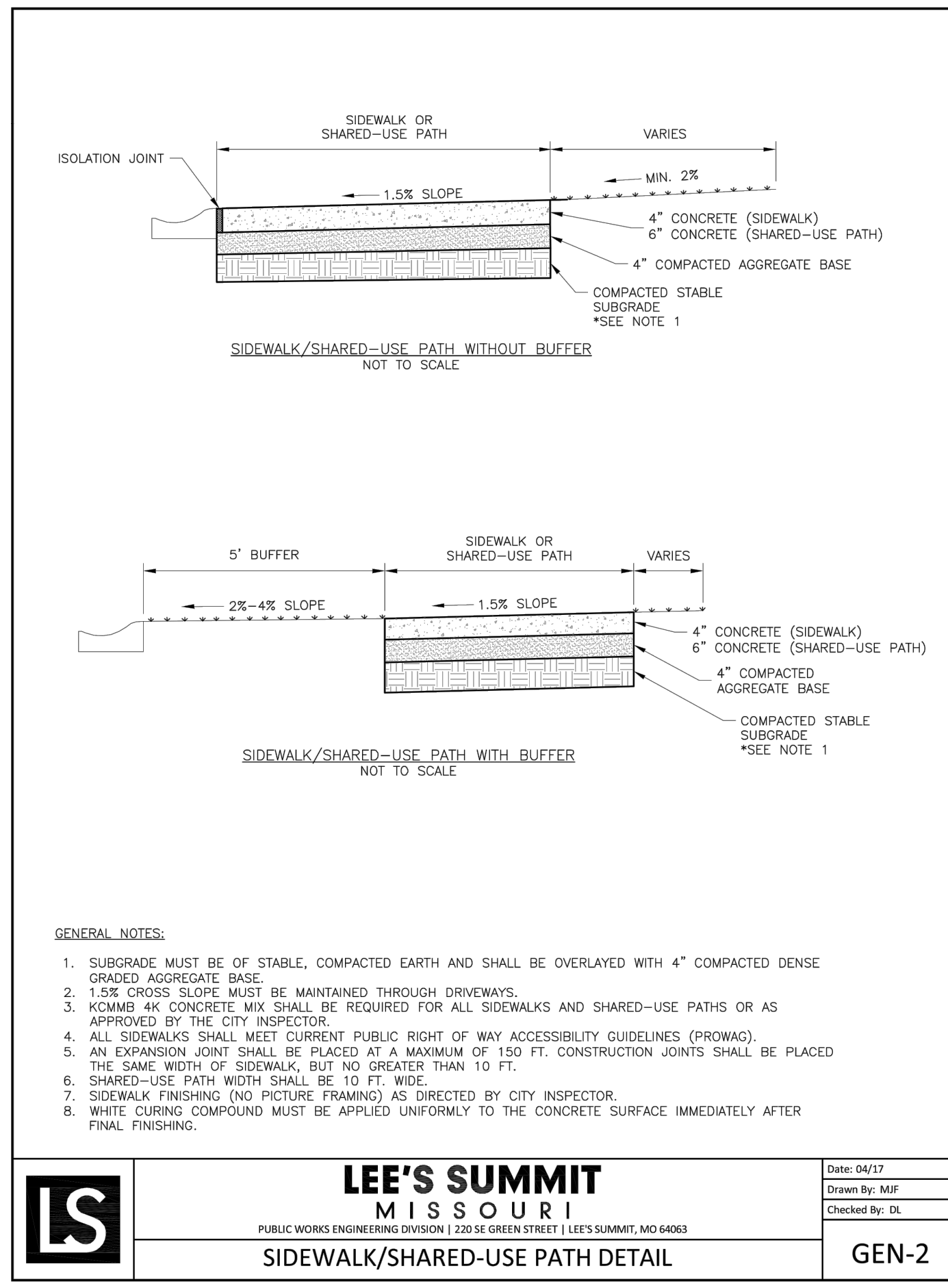
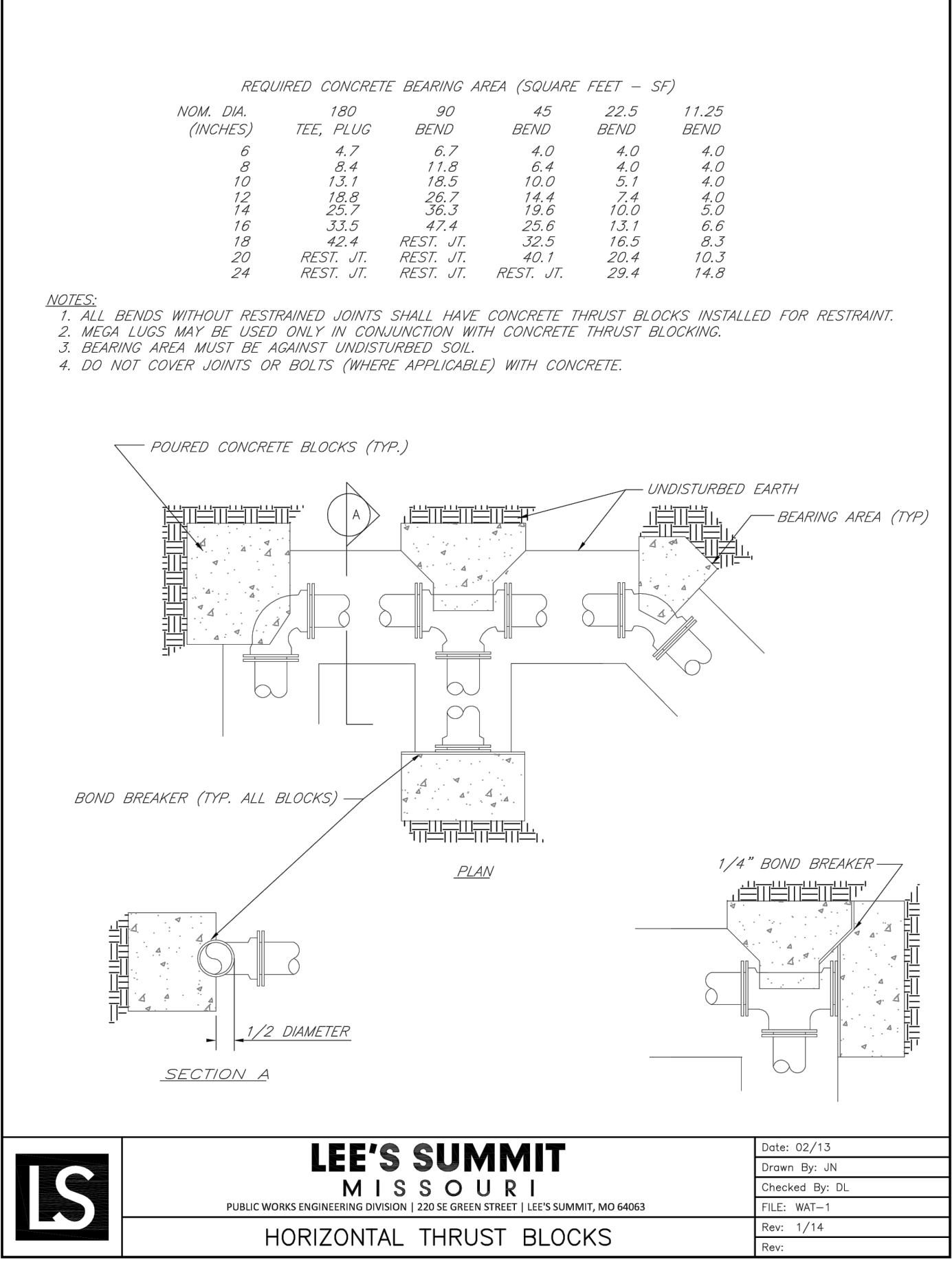
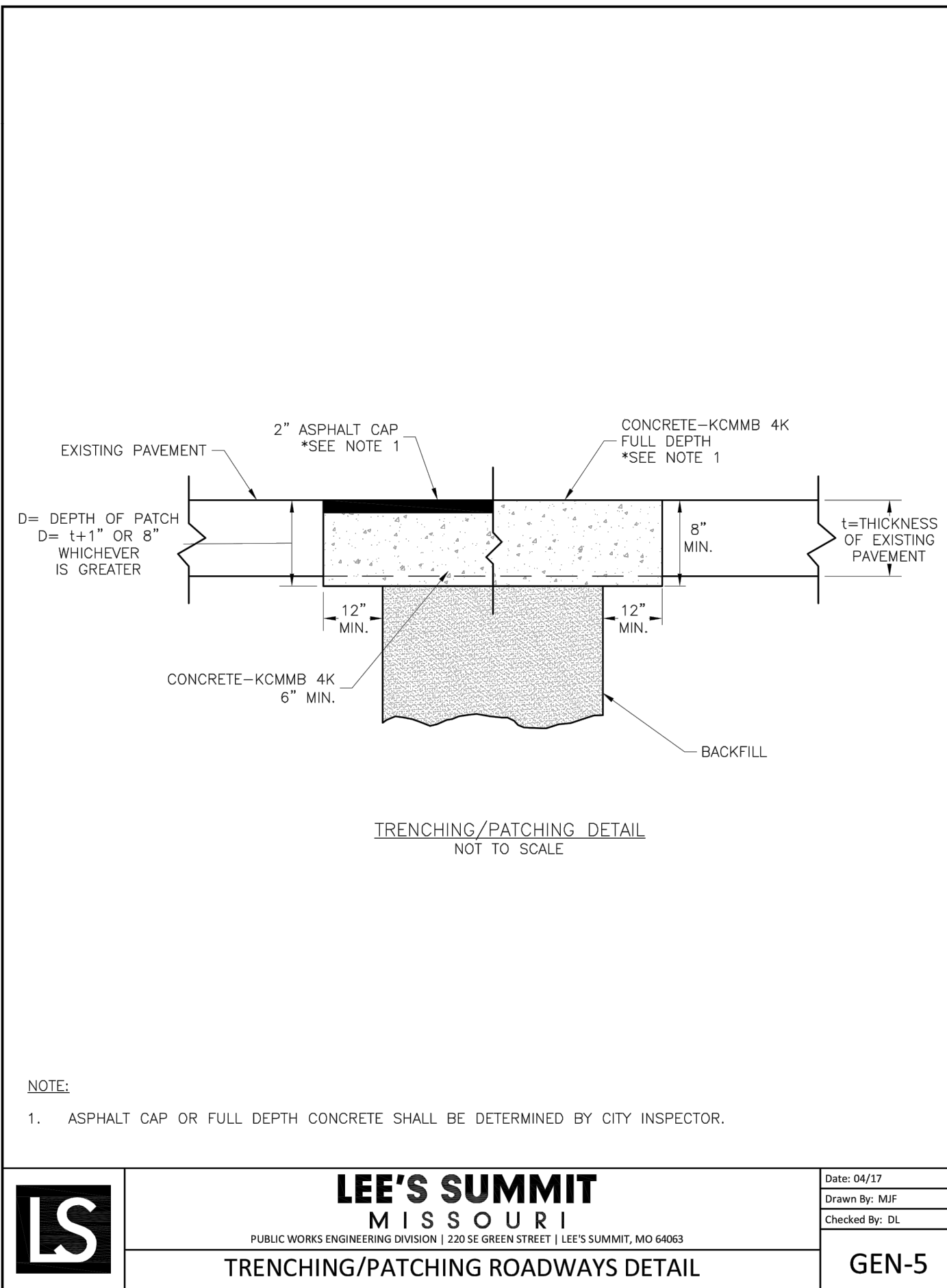
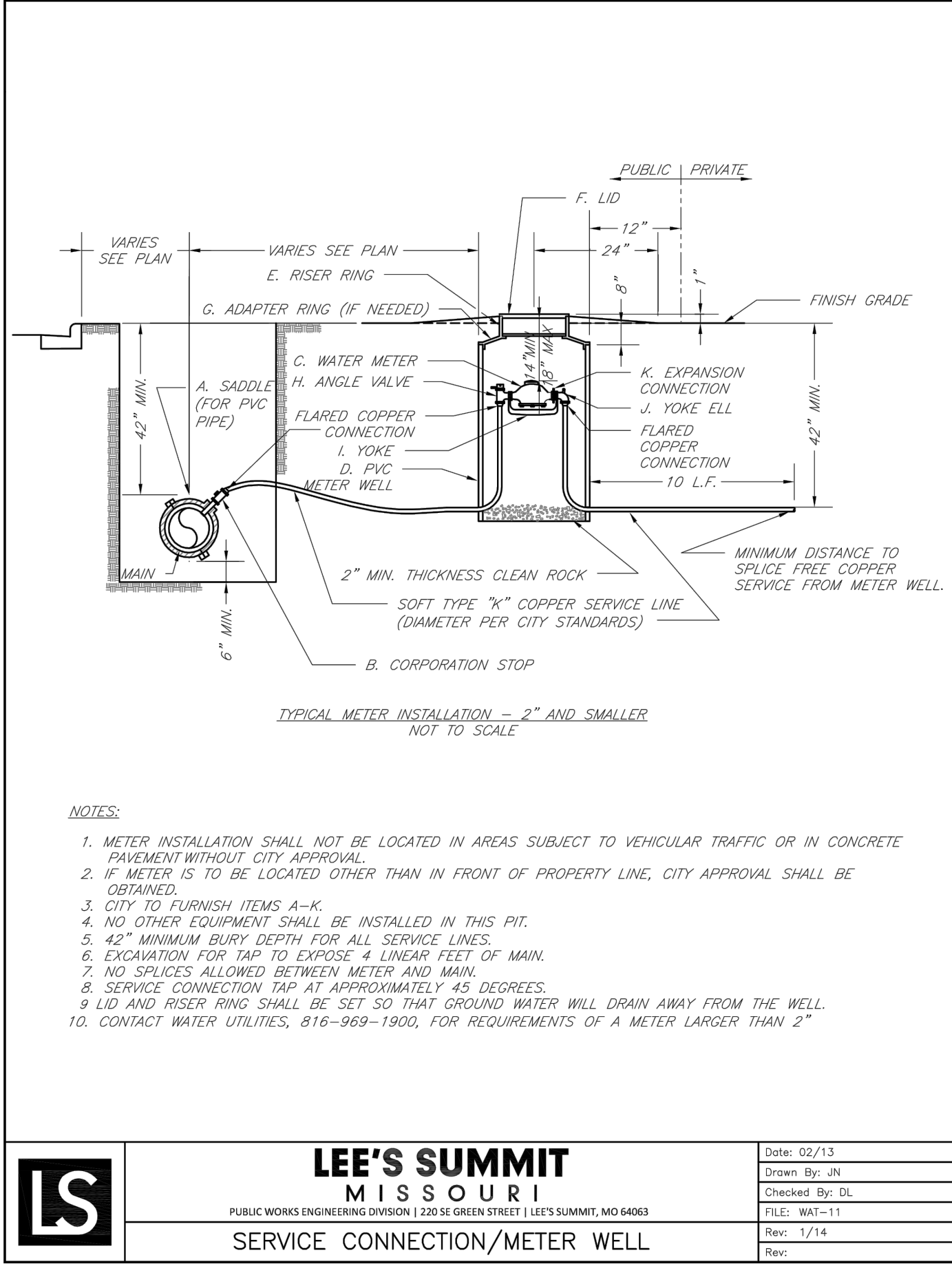
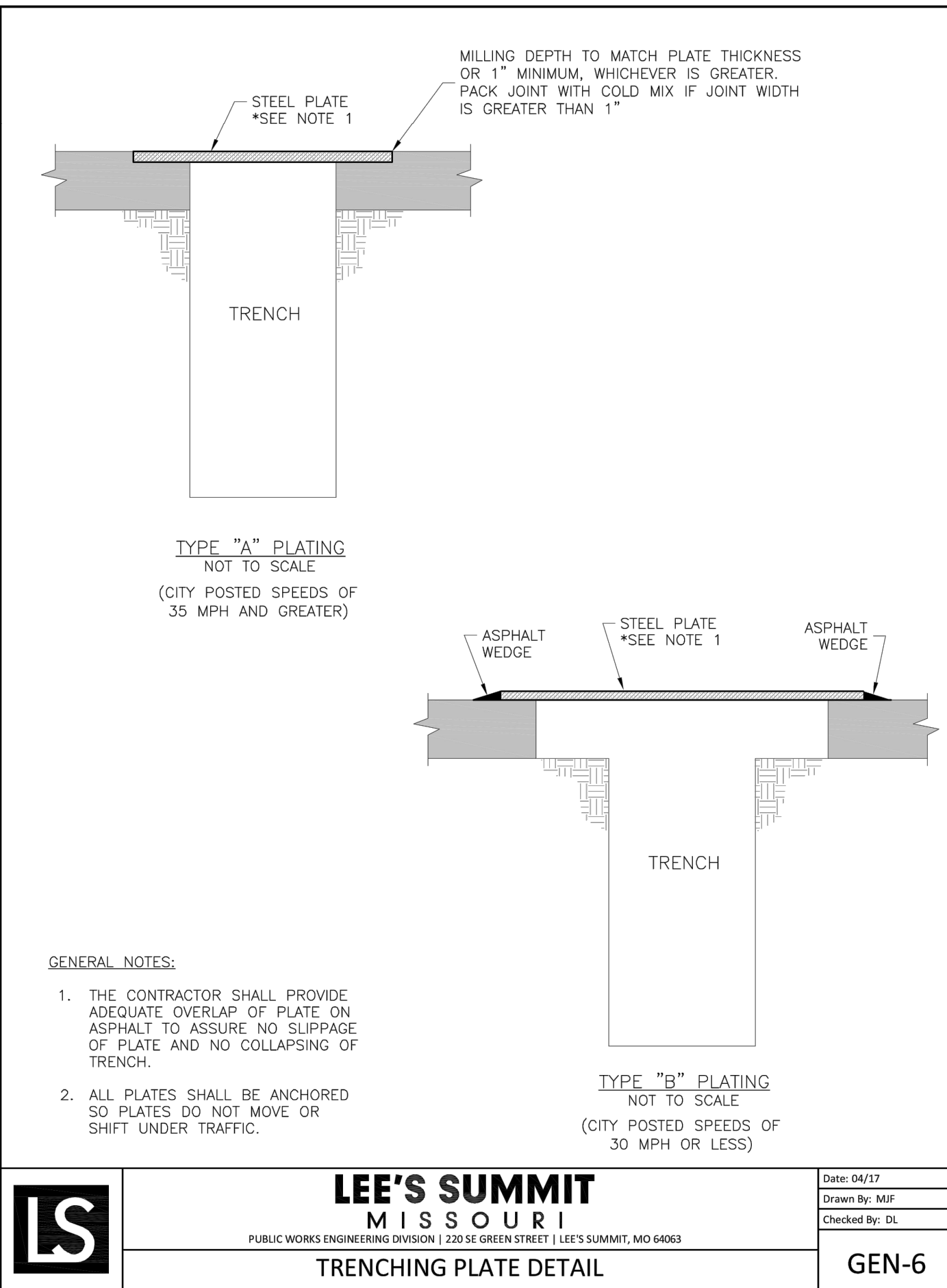
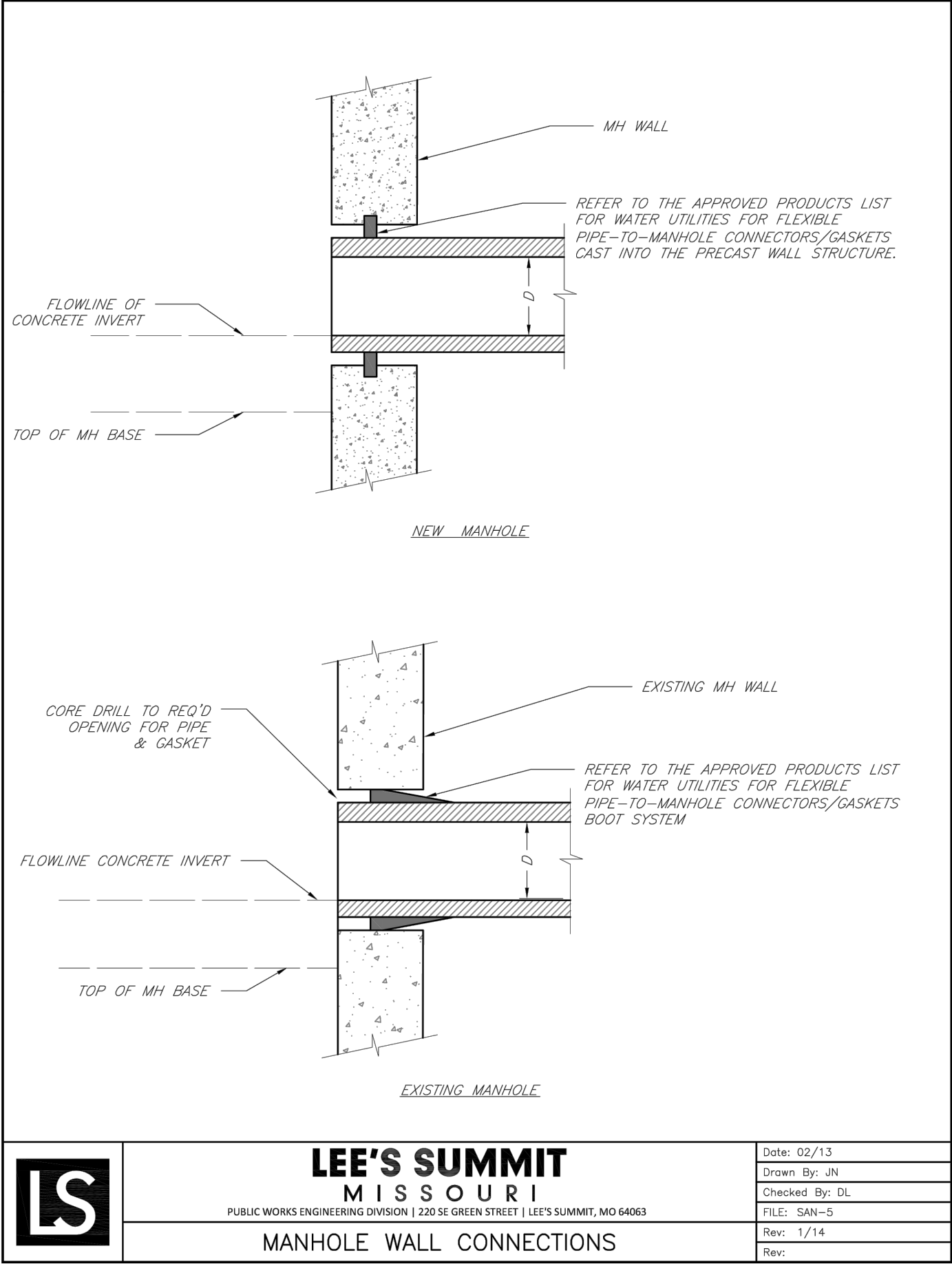
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Missouri State Certificate of Authority #001592  
Revision No. Description Date  
1 ASI#1 02.18.19  
2 RESPONSE TO CITY COMMENTS 03.26.19

Project No. Date Drawn  
B18-0330 12.07.18 RLK

Drawing No. **C10.4**  
STANDARD DETAILS





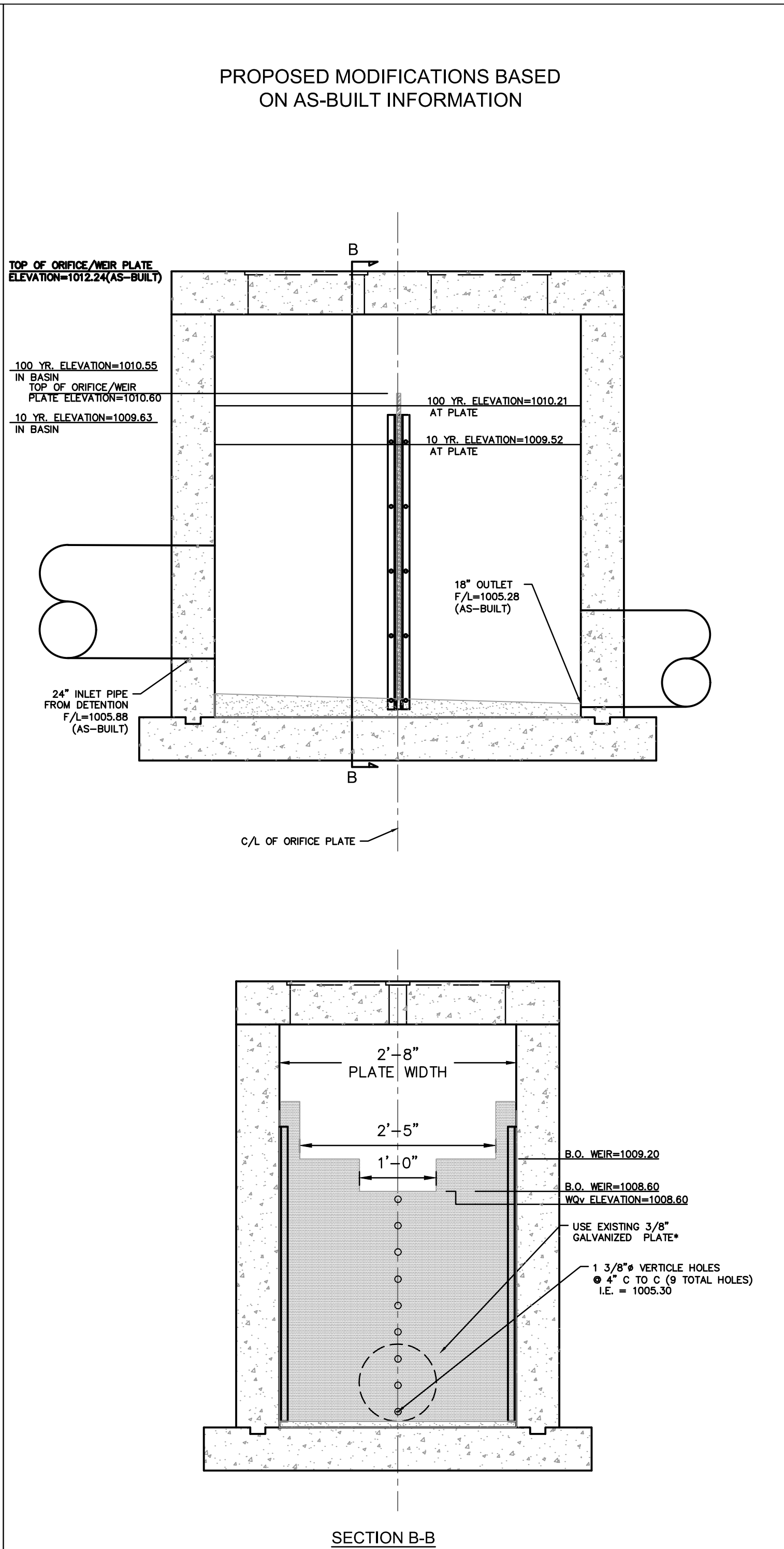
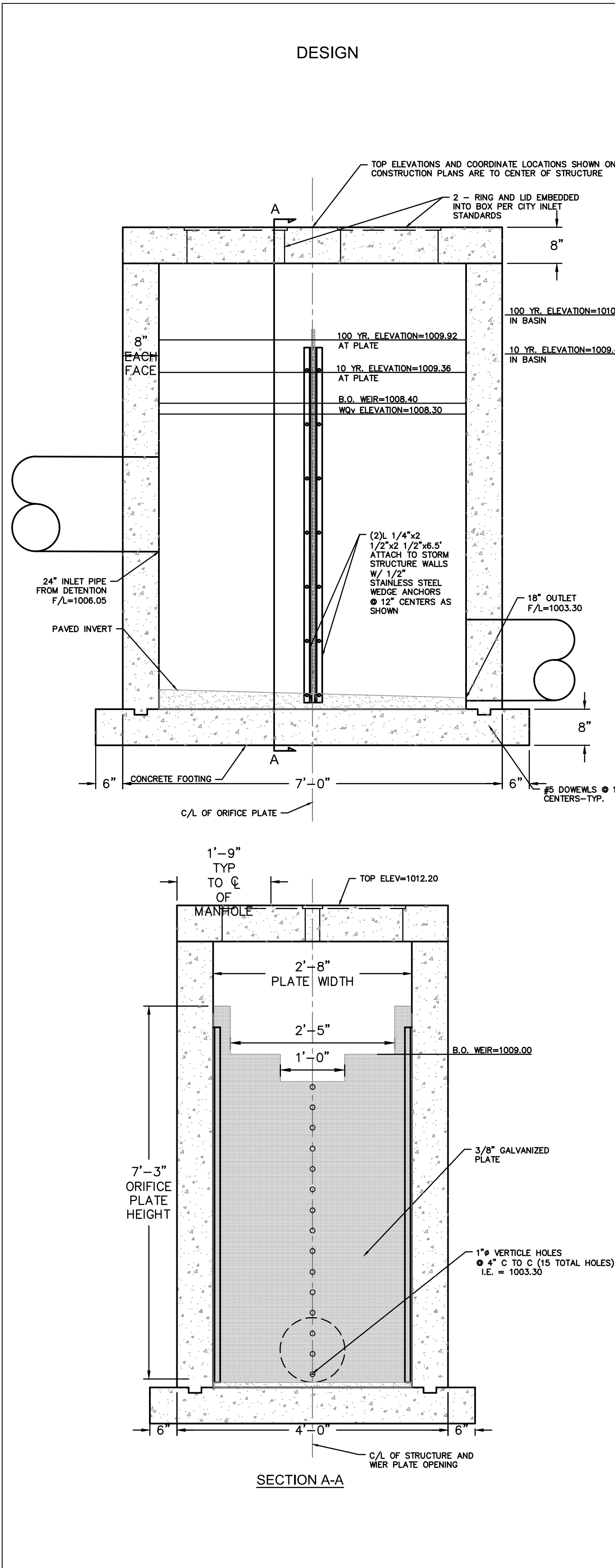
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DWG: F:\2018\0001-0500\018-0330-B\40-Design\AutoCAD\Final Plans\Sheets\GNCV\WORK PACKAGE 4\LEES SUMMIT\1.C\_DTL\_80330.dwg USER: tp030ns  
DATE: Feb 27, 2020 9:06am REF: C:\MS0\B\80330\80330.dwg C:\MS0\B\80330\80330.dwg L\_PBASE\_81023 TE DETAIL C\_PBASE\_80330



#### AS BUILT STRUCTURE INFORMATION

- THE BOTTOM OF THE STRUCTURE WAS BUILT AT AN ELEVATION APPROXIMATELY 2 FEET HIGHER THAN DESIGN. THE STRUCTURE IS APPROXIMATELY 2 FEET SHORTER THAN THE DESIGN, SO THE TOP OF THE STRUCTURE IS ONLY 0.05 FEET HIGHER THAN DESIGNED.

#### WEIR PLATE MODIFICATION NOTE:

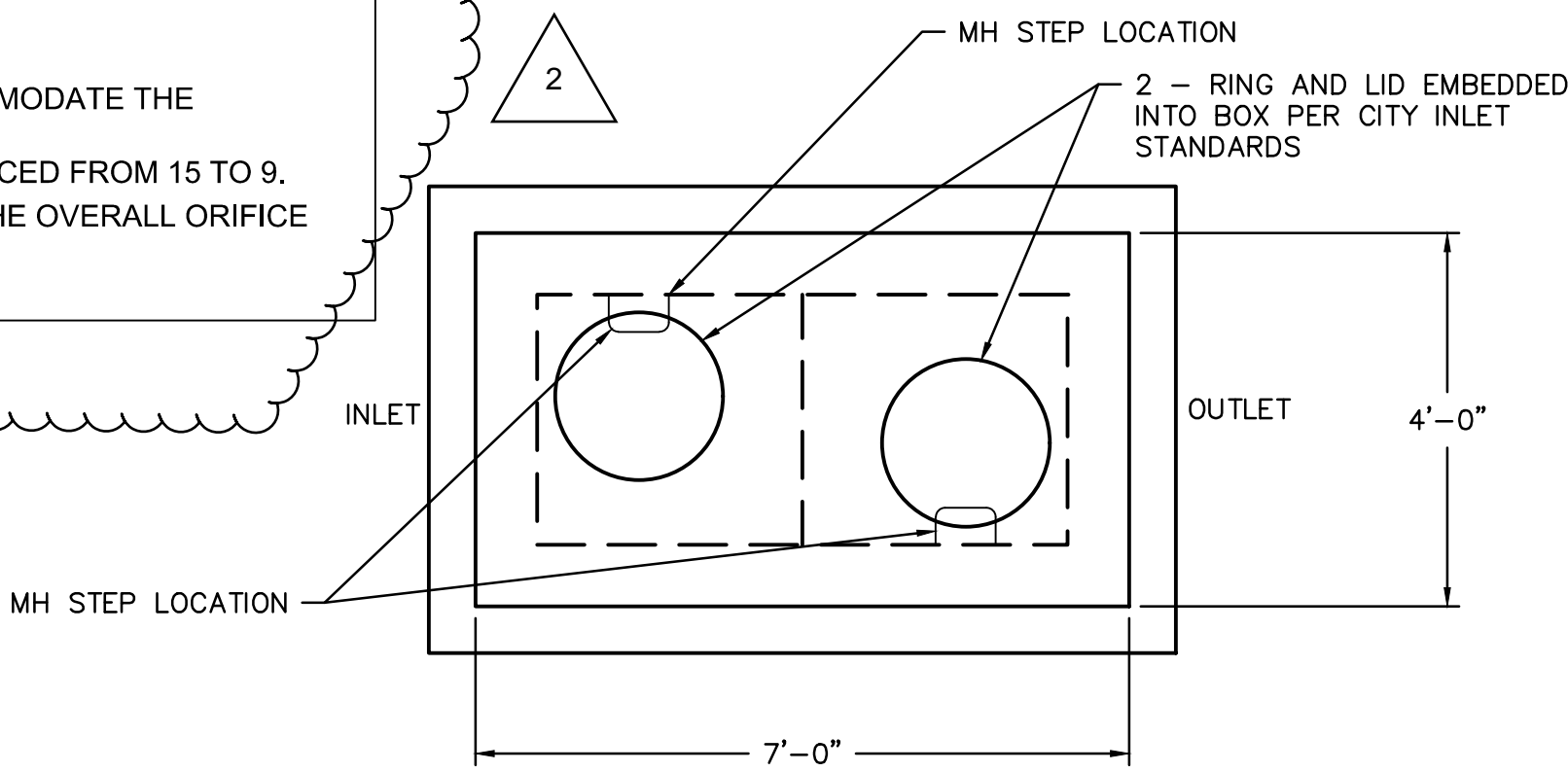
- THE PLATE WILL BE 1.8 FEET SHORTER THAN THE DESIGN
- THE BOTTOM OF THE WEIRS WILL BE RAISED 0.2 FEET TO ACCOMODATE THE AS-BUILT ADJUSTED VOLUME IN THE DAM.
- THE NUMBER OF HOLES IN THE VERTICAL HOLES WILL BE REDUCED FROM 15 TO 9. THE HOLE SIZE WILL INCREASE FROM 1" TO 1 3/8" TO MAINTAIN THE OVERALL ORIFICE AREA

#### CONTROL STRUCTURE DETAILS

NOT TO SCALE

#### NOTES

- 5,000 PSI KCMMB CONCRETE SHALL BE USED FOR STRUCTURES WITH ORIFICE PLATE
- STRUCTURES MAY BE PRE-CAST OR CAST IN PLACE AT THE CONTRACTOR'S OPTION. FOR PRE-CAST CONSTRUCTION CONTRACTOR SHALL SUBMIT SHOP DRAWINGS TO THE DESIGN ENGINEER, AND PROVIDE A COPY OF THE APPROVED SHOP DRAWINGS TO THE CITY.

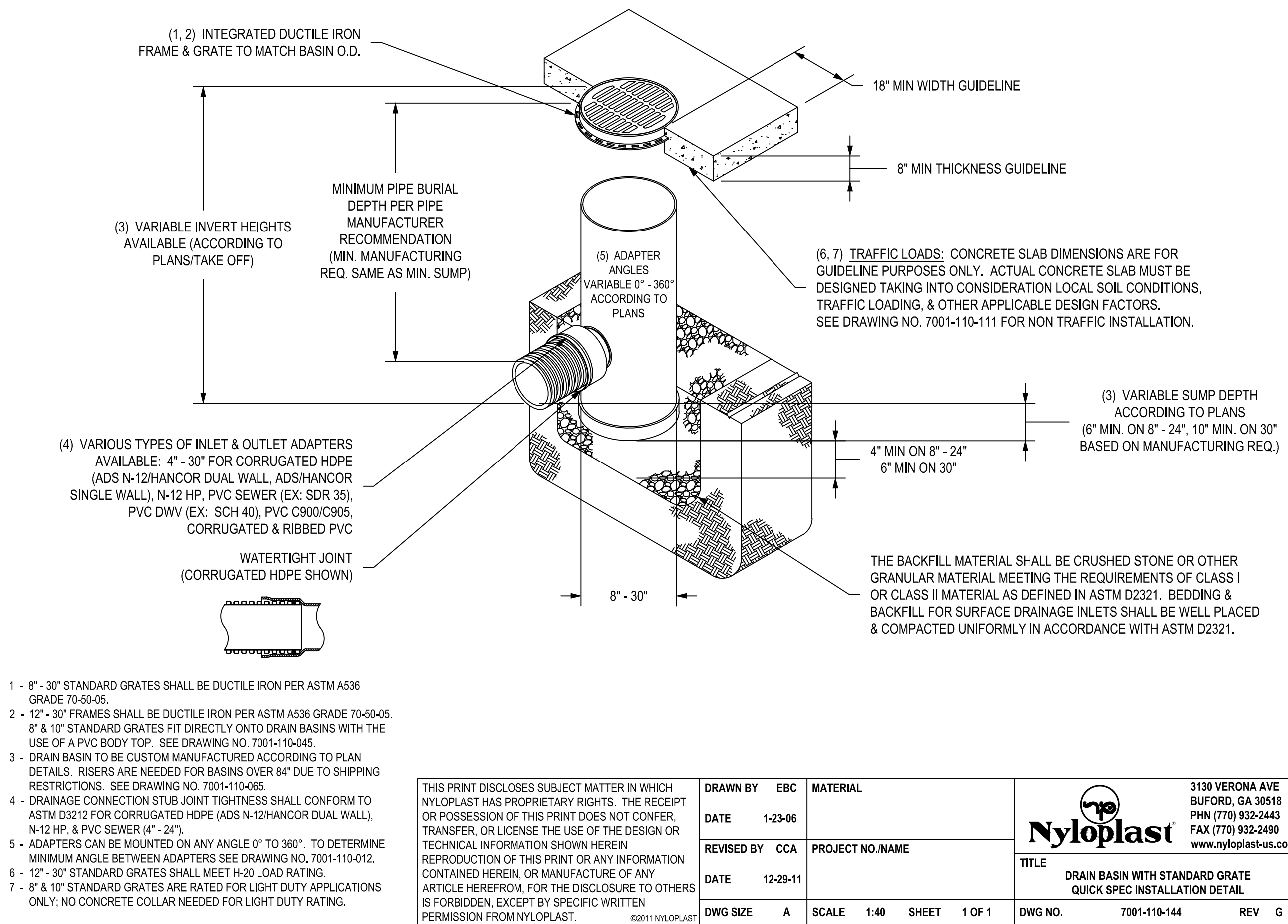


#### CONTROL STRUCTURE PLAN VIEW

#### ONE-WAY WYE CLEANOUT DETAIL

NOT TO SCALE

#### NYLOPLAST DRAIN BASIN WITH STANDARD GRATE



#### Section 2721

#### Engineered Surface Drainage Products

#### GENERAL

PVC surface drainage inlets shall include the drain basin type as indicated on the contract drawing and referenced within the contract specifications. The ductile iron grates for each of these fittings are to be considered an integral part of the surface drainage inlet and shall be furnished by the same manufacturer. The surface drainage inlets shall be as manufactured by Nyloplast a division of Advanced Drainage Systems, Inc., or prior approved equal.

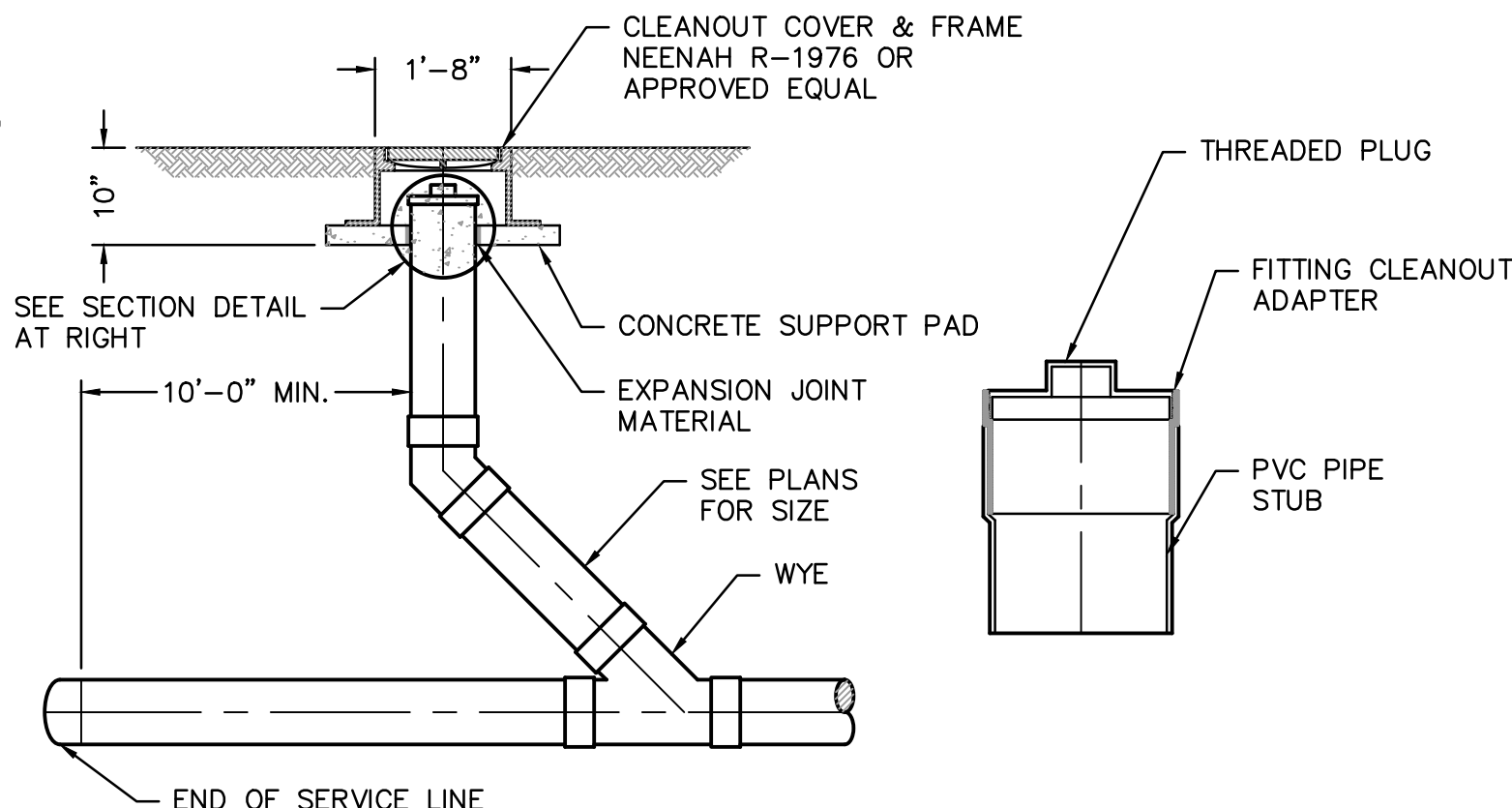
#### MATERIALS

The drain basins required for this contract shall be manufactured from PVC pipe stock, utilizing a thermoforming process to reform the pipe stock to the specified configuration. The drainage pipe connection stubs shall be manufactured from PVC pipe stock and formed to provide a watertight connection with the specified pipe system. This joint tightness shall conform to ASTM D3212 for joints for drain and sewer plastic pipe using flexible elastomeric seals. The flexible elastomeric seals shall conform to ASTM F477. The pipe bell spigot shall be joined to the main body of the drain basin or catch basin. The raw material used to manufacture the pipe stock that is used to manufacture the main body and pipe stubs of the surface drainage inlets shall conform to ASTM D1784 cell class 12454.

The grates and frames furnished for all surface drainage inlets shall be ductile iron for sizes 8", 10", 12", 15", 18", 24" and 30" and shall be made specifically for each basin so as to provide a round bottom flange that closely matches the diameter of the surface drainage inlet. Grates for drain basins shall be capable of supporting various wheel loads as specified by Nyloplast. 12" and 15" square grates will be hinged to the frame using pins. Ductile iron used in the manufacture of the castings shall conform to ASTM A536 grade 70-50-05. Grates and covers shall be provided painted black.

#### INSTALLATION

The specified PVC surface drainage inlet shall be installed using conventional flexible pipe backfill materials and procedures. The backfill material shall be crushed stone or other granular material meeting the requirements of class 1 or class 2 material as defined in ASTM D2321. Bedding and backfill for surface drainage inlets shall be well placed and compacted uniformly in accordance with ASTM D2321. The drain basin body will be cut at the time of the final grade. No brick, stone or concrete block will be required to set the grate to the final grade height. For load rated installations, a concrete slab shall be poured under and around the grate and frame. The concrete slab must be designed taking into consideration local soil conditions, traffic loading, and other applicable design factors. For other installation considerations such as migration of fines, ground water, and soft foundations refer to ASTM D2321 guidelines.



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JACKSON COUNTY

PACKAGE  
**04**

Engineer of Record



Terry M Parsons, Engineer MO PE-2018010595

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Missouri State Certificate of Authority #001592

Revision No.	Description	Date
1	AS-BUILT	02.18.19
2	DETENTION MODIFICATIONS	02.26.20

Project No. B18-0330 Date 12.07.18 Drawn RLK

Drawing No. **C10.6**

STANDARD DETAILS

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