

---

AN ORDINANCE APPROVING A PRELIMINARY DEVELOPMENT PLAN LOCATED AT 704 AND 708 SE OLDHAM COURT IN DISTRICT CS FOR THE PROPOSED ARISTOCRAT MOTORS IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 33, UNIFIED DEVELOPMENT ORDINANCE, OF THE LEE'S SUMMIT CODE OF ORDINANCES, FOR THE CITY OF LEE'S SUMMIT, MISSOURI.

WHEREAS, Application #PL2019-204 submitted by Aristocrat Motors, requesting approval of a preliminary development plan in District CS (Commercial Services) on land located at 704 and 708 SE Oldham Court was referred to the Planning Commission to hold a public hearing; and,

WHEREAS, the Unified Development Ordinance provides for the approval of a preliminary development plan by the City following public hearings by the Planning Commission and City Council; and,

WHEREAS, after due public notice in the manner prescribed by law, the Planning Commission held a public hearing for the consideration of the preliminary development plan on August 8, 2019, and rendered a report to the City Council recommending that the preliminary development plan be approved; and,

WHEREAS, after due public notice in the manner prescribed by law, the City Council held a public hearing on September 3, 2019, and rendered a decision to approve the preliminary development plan for said property.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF LEE'S SUMMIT, MISSOURI, as follows:

SECTION 1. That a preliminary development plan is hereby approved in District CS on the following described property:

*Lots 2 and 3, Oldham East Business Park, a recorded subdivision in Lee's Summit, Jackson County, Missouri.*

SECTION 2. That the following conditions of approval apply:

1. The developer shall construct the segment of sidewalk along SE Oldham Pkwy located west of the driveway as part of this development. The developer shall make payment to the City of Lee's Summit for construction cost in lieu of actual construction for the segment of sidewalk along SE Oldham Pkwy located east of the driveway.

SECTION 3. That development shall be in accordance with the preliminary development plan, date stamped July 16, 2019, appended hereto and made a part hereof.

SECTION 4. Nonseverability. All provisions of this ordinance are so essentially and inseparably connected with, and so dependent upon, each other that no such provision would be enacted without all others. If a court of competent jurisdiction enters a final judgment on the merits that is not subject to appeal and that declares any provision or part of this ordinance void, unconstitutional, or

unenforceable, then this ordinance, in its collective entirety, is invalid and shall have no legal effect as of the date of such judgment.

SECTION 5. That failure to comply with all of the provisions contained in this ordinance shall constitute violations of both this ordinance and Chapter 33, the Unified Development Ordinance, of the Code of Ordinances of the City of Lee's Summit.

SECTION 6. That this ordinance shall be in full force and effect from and after the date of its passage and adoption, and approval by the Mayor.

PASSED by the City Council of the City of Lee's Summit, Missouri, this 17<sup>th</sup> day of September, 2019.



\_\_\_\_\_  
Mayor William A. Baird

ATTEST:



\_\_\_\_\_  
City Clerk Trisha Fowler Arcuri

APPROVED by the Mayor of said city this 20<sup>th</sup> day of September, 2019.



\_\_\_\_\_  
Mayor William A. Baird

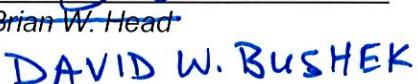
ATTEST:



\_\_\_\_\_  
City Clerk Trisha Fowler Arcuri

APPROVED AS TO FORM:



\_\_\_\_\_  
ACTING City Attorney Brian W. Head  
 DAVID W. BUSHEK



# LEE'S SUMMIT

## MISSOURI

### DEVELOPMENT REVIEW FORM

### TRANSPORTATION IMPACT

**DATE:** July 30, 2019  
**SUBMITTAL DATE:** June 14, 2019  
**APPLICATION #:** PL2019204  
**PROJECT NAME:** ARISTOCRAT MOTORS

**CONDUCTED BY:** Michael K Park, PE, PTOE  
**PHONE:** 816.969.1800  
**EMAIL:** Michael.Park@cityofls.net  
**PROJECT TYPE:** Prel Dev Plan (PDP)

#### **SURROUNDING ENVIRONMENT (Streets, Developments)**

The proposed development is located along the north side Oldham Parkway, east of Hamblen Road, across from Century Drive. The surrounding property is commercial/industrial and bordered by US 50 Highway to the north/northeast.

#### **ALLOWABLE ACCESS**

Access to the site is proposed from an existing driveway (a vacated public street, Oldham Court) along Oldham Parkway.

#### **EXISTING STREET CHARACTERISTICS (Lanes, Speed limits, Sight Distance, Medians)**

Oldham Parkway is a two/three-lane, undivided, commercial collector with a posted speed limit of 40 mph. There is adequate sight distance at the existing intersection with the private driveway.

**ACCESS MANAGEMENT CODE COMPLIANCE?** YES  No

The driveway location along Oldham Parkway is existing and complies with the Access Management Code. The driveway has a left-turn lane in compliance with code requirements.

#### **TRIP GENERATION**

Time Period	Total	In	Out
Weekday	200	100	100
A.M. Peak Hour	16	12	4
P.M. Peak Hour	28	13	15

**TRANSPORTATION IMPACT STUDY REQUIRED?** YES  No

The proposed development will not likely generate more than 100 vehicle trips during a weekday peak hour; the minimum condition for traffic impact study requirements.

**LIVABLE STREETS (Resolution 10-17)**

**COMPLIANT**

**EXCEPTIONS**

The proposed development includes all Livable Streets elements identified in the City's adopted Comprehensive Plan, associated Greenway Master Plan and Bicycle Transportation Plan attachments, and elements otherwise required by ordinances and standards in full compliance with the Livable Streets Policy adopted by Resolution 10-17. Sidewalk will be constructed west of the existing driveway in accordance with code requirements. Staff supports payment in lieu of sidewalk construction east of the existing driveway in consideration of the US 50 Highway frontage proximity to Oldham Parkway and unavailable land for development whereby sidewalk could not continue eastward nor likely have pedestrian use in the future.

**RECOMMENDATION:**

**APPROVAL**

**DENIAL**

**N/A**

**STIPULATIONS**

*Recommendations for Approval refer only to the transportation impact and do not constitute an endorsement from City Staff.*

Staff recommends approval of the proposed preliminary development plan.



ARCHITECTS  
SLAGGIE ARCHITECTS, INC. © 2019  
Kearny City, Omaha, Nebraska  
402.342.2444  
http://slaggie.com

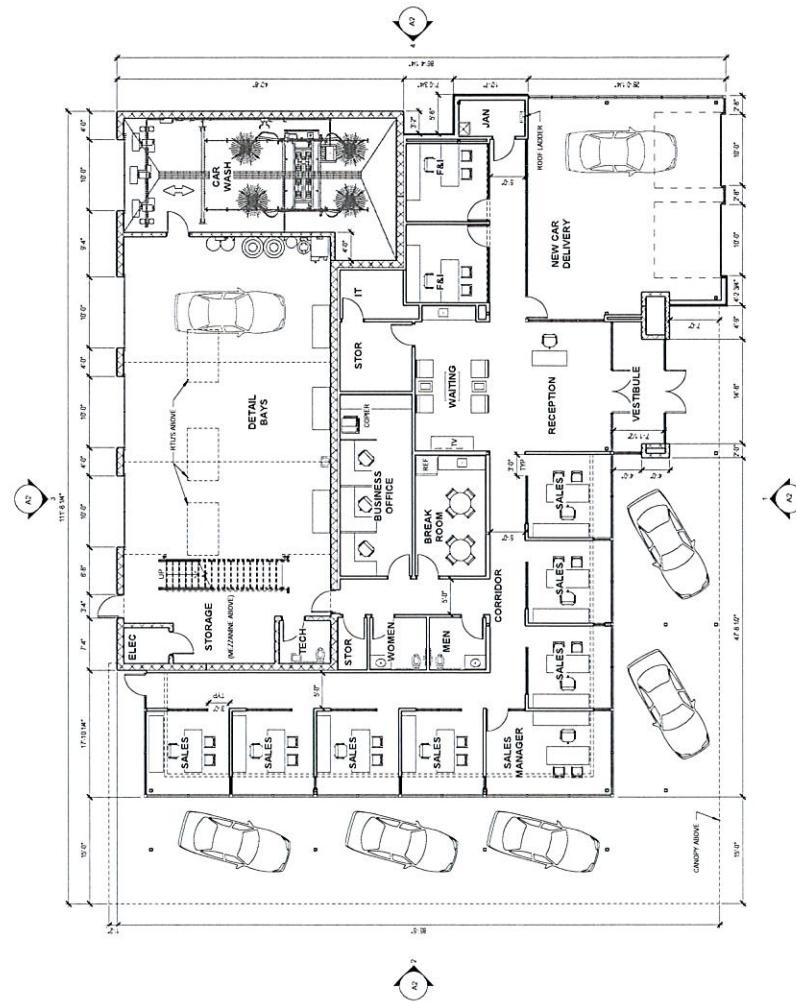
Lee's Summit Mo  
Savee Automobile Group

New Construction

## Aristocrat Pre-Owned Sales Building

FLOOR PLAN  
**A1**

Floor Plan **1**





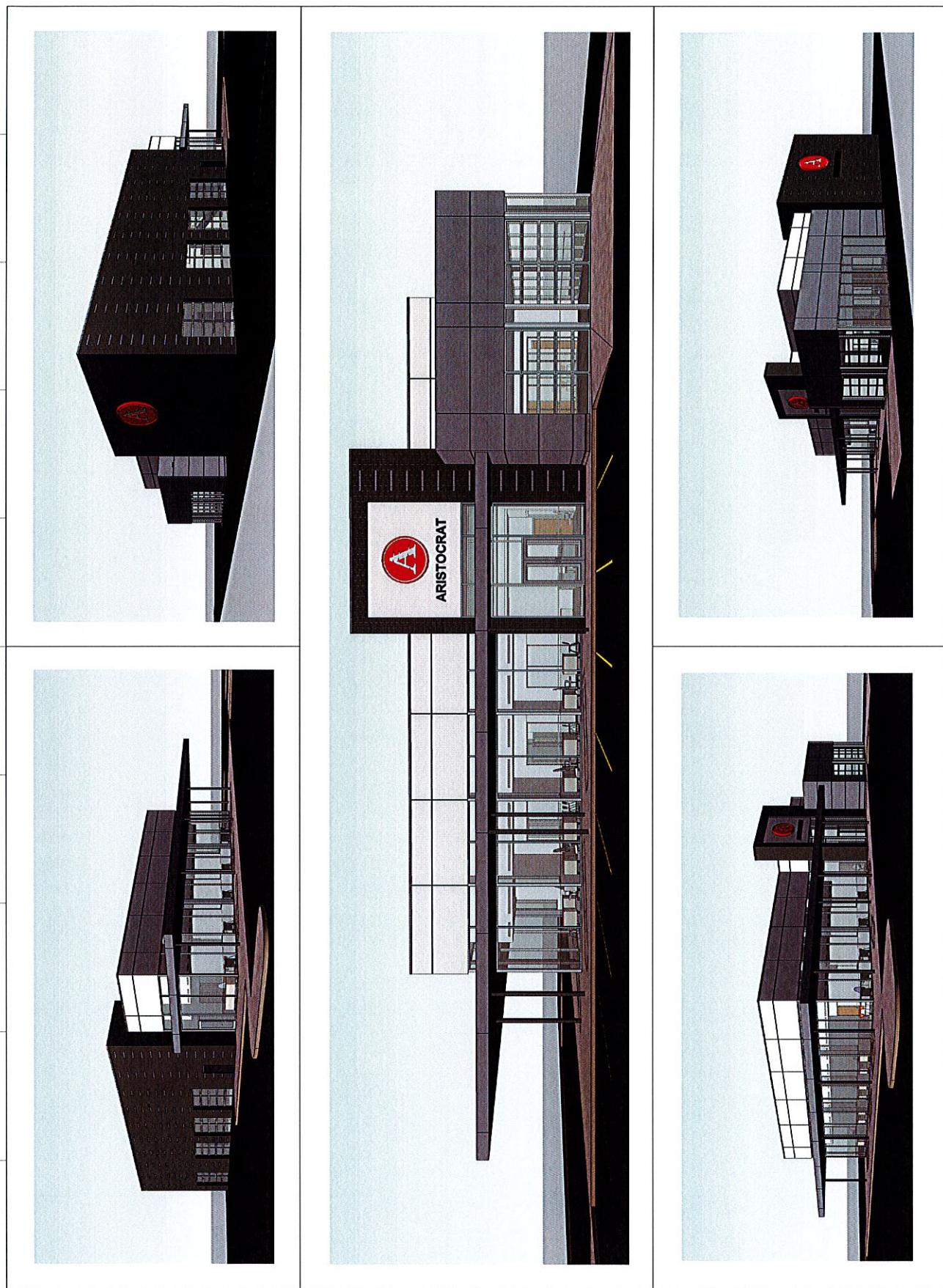


ARCHITECTS  
SLAGGIE ARCHITECTS, INC. © 2010  
Kanata City Architects Inc.  
401-1000 Bank Street, Ottawa, Ontario  
K2B 5H6 • 613.745.1111 • Fax 613.745.1112  
http://www.slaggie.com

Lee's Summit MO  
Save Automotive Group  
New Construction

## Aristocrat Pre-Owned Sales Building

EXTERIOR EXHIBITS  
**A3**



## INDEX

- 1 COOLER SHEET
- 2 DRACTION PLAN
- 3 DIMENSION PLAN
- 4 CRADING PLAN
- 5 ADA RAMP DETAIL
- 6 STORM SEWER PLAN
- 7 DRAINAGE AREA MAP
- 8 UTILITY AREA MAP
- 9 EROSION CONTROL PLAN
- 10-14 DETAIL SHEETS

APPENDIX:  
 ARCHITECTURAL FLOORPLANS AND ELEVATIONS  
 LANDSCAPING PLANS AND SPECS  
 LIGHTING PLANS AND SPECS  
 ADS STORM TECH DETENTION PLANS

# ARISTOCRAT MOTORS LOTS 2 AND 3 OLDHAM EAST BUSINESS PARK LEE'S SUMMIT - JACKSON COUNTY MO PRELIMINARY DEVELOPMENT PLAN

7/9/2019

DATE ISSUED:

7/16/19

DATE APPROVED:

7/16/19

REVIEWED:

7/16/19

SUPERVISOR:

R. J. Jones

APPROVED:

R. J. Jones

REVIEWER:

R. J. Jones

APPROVING OFFICER:

R. J. Jones

REVIEWING OFFICER:

R. J. Jones

APPROVING DATE:

7/16/19

REVIEWING DATE:

7/16/19

APPROVING SIGNATURE:

R. J. Jones

REVIEWING SIGNATURE:

R. J. Jones

APPROVING TITLE:

R. J. Jones

REVIEWING TITLE:

R. J. Jones

APPROVING COMPANY:

R. J. Jones

REVIEWING COMPANY:

R. J. Jones

APPROVING ADDRESS:

R. J. Jones

REVIEWING ADDRESS:

R. J. Jones

APPROVING PHONE:

R. J. Jones

REVIEWING PHONE:

R. J. Jones

APPROVING FAX:

R. J. Jones

REVIEWING FAX:

R. J. Jones

APPROVING E-MAIL:

R. J. Jones

REVIEWING E-MAIL:

R. J. Jones

APPROVING STATE:

R. J. Jones

REVIEWING STATE:

R. J. Jones

APPROVING CITY:

R. J. Jones

REVIEWING CITY:

R. J. Jones

APPROVING ZIP CODE:

R. J. Jones

REVIEWING ZIP CODE:

R. J. Jones

APPROVING COUNTY:

R. J. Jones

REVIEWING COUNTY:

R. J. Jones

APPROVING STATE CODE:

R. J. Jones

REVIEWING STATE CODE:

R. J. Jones

APPROVING CITY CODE:

R. J. Jones

REVIEWING CITY CODE:

R. J. Jones

APPROVING ZIP CODE CODE:

R. J. Jones

REVIEWING ZIP CODE CODE:

R. J. Jones

APPROVING COUNTY CODE:

R. J. Jones

REVIEWING COUNTY CODE:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones

APPROVING STATE CODE NUMBER:

R. J. Jones

REVIEWING STATE CODE NUMBER:

R. J. Jones

APPROVING CITY CODE NUMBER:

R. J. Jones

REVIEWING CITY CODE NUMBER:

R. J. Jones

APPROVING ZIP CODE CODE NUMBER:

R. J. Jones

REVIEWING ZIP CODE CODE NUMBER:

R. J. Jones

APPROVING COUNTY CODE NUMBER:

R. J. Jones

REVIEWING COUNTY CODE NUMBER:

R. J. Jones



### GENERAL NOTES:

1. The intended use of OGDHAM EAST Business Park is in proportion of this Final Development Plan.
2. Roads, easements or common boundaries and other property rights as shown on the "Environmental Impact Survey of Abandoned Oil and Gas Wells in Lee's Summit, Missouri" - Edward Allou Mar., Jr., P.E., dated 12/10/95, shall be honored.
3. In all developments, the developer will be responsible for all costs associated with the removal of oil and gas wells.
4. All developments shall be planned and developed in accordance with Title 8, of the UDO, current 1000.
5. Construction units shall be planned and developed in accordance with Title 8, of the UDO, current 1000.
6. Contractors to develop and construct any new buildings shall meet quality standards as necessary.
7. The developer shall meet the requirements of the Unified Development Ordinance, Design and Construction Manual, Access Management Code, Access Management Code, Access Management Code, and other ordinances for development including, but not limited to, porting services and other ordinances for development from the date of the sign and the end of the construction period.
8. Existing ADA access shall be provided to all developments.
9. All developments shall be planned and developed in accordance with Title 8, of the UDO, current 1000.
10. All developments shall be planned and developed in accordance with Title 8, of the UDO, current 1000.
11. There shall be two new traffic medians.
12. See architectural plan for building dimensions.
13. See parking plans and site plan.
14. See lighting plans and site plan.
15. All areas to be GFCI-protected for building dimensions, instruments, signs and lighting details.
16. Existing Storm Sewer System shall be replaced by the developer.
17. Existing Storm Sewer System shall be replaced by the developer.
18. Water Main East First Line Blue River to Prairie Lee Lane.
19. A minor plan will be prepared to continue the road to Prairie Lee Lane.
20. All areas to be active parking or are 5 minutes drive away.
21. All areas to be active parking or are 5 minutes drive away.

Any work done: No. 1-1200-2  
Job No.: 1-01-01522  
Contractor Name: E&L

US 50 Hwy

P.L.

E 118.06

118.06

S 29°42'29" E

29°42'29"

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

Card to be planned and  
designated NO PARKING -  
FINE LANE

P.L.

E 118.07

118.07

29°42'29"

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

Card to be planned and  
designated NO PARKING -  
FINE LANE

P.L.

E 118.08

118.08

29°42'29"

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

Card to be planned and  
designated NO PARKING -  
FINE LANE

P.L.

E 118.09

118.09

29°42'29"

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

Card to be planned and  
designated NO PARKING -  
FINE LANE

P.L.

E 118.10

118.10

29°42'29"

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

Card to be planned and  
designated NO PARKING -  
FINE LANE

P.L.

E 118.11

118.11

29°42'29"

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

29'

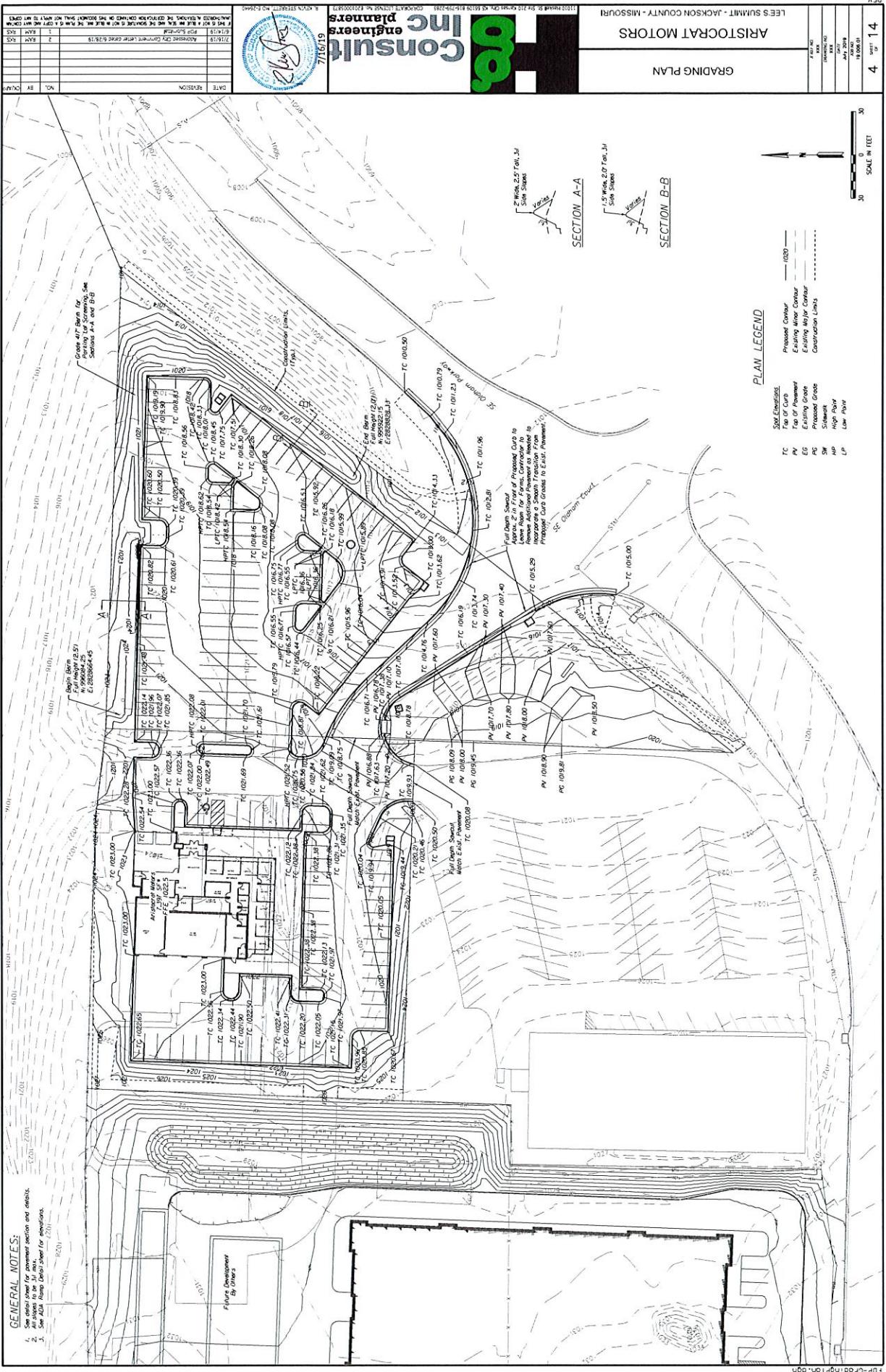
Card to be planned and  
designated NO PARKING -  
FINE LANE

P.L.

E 118.12

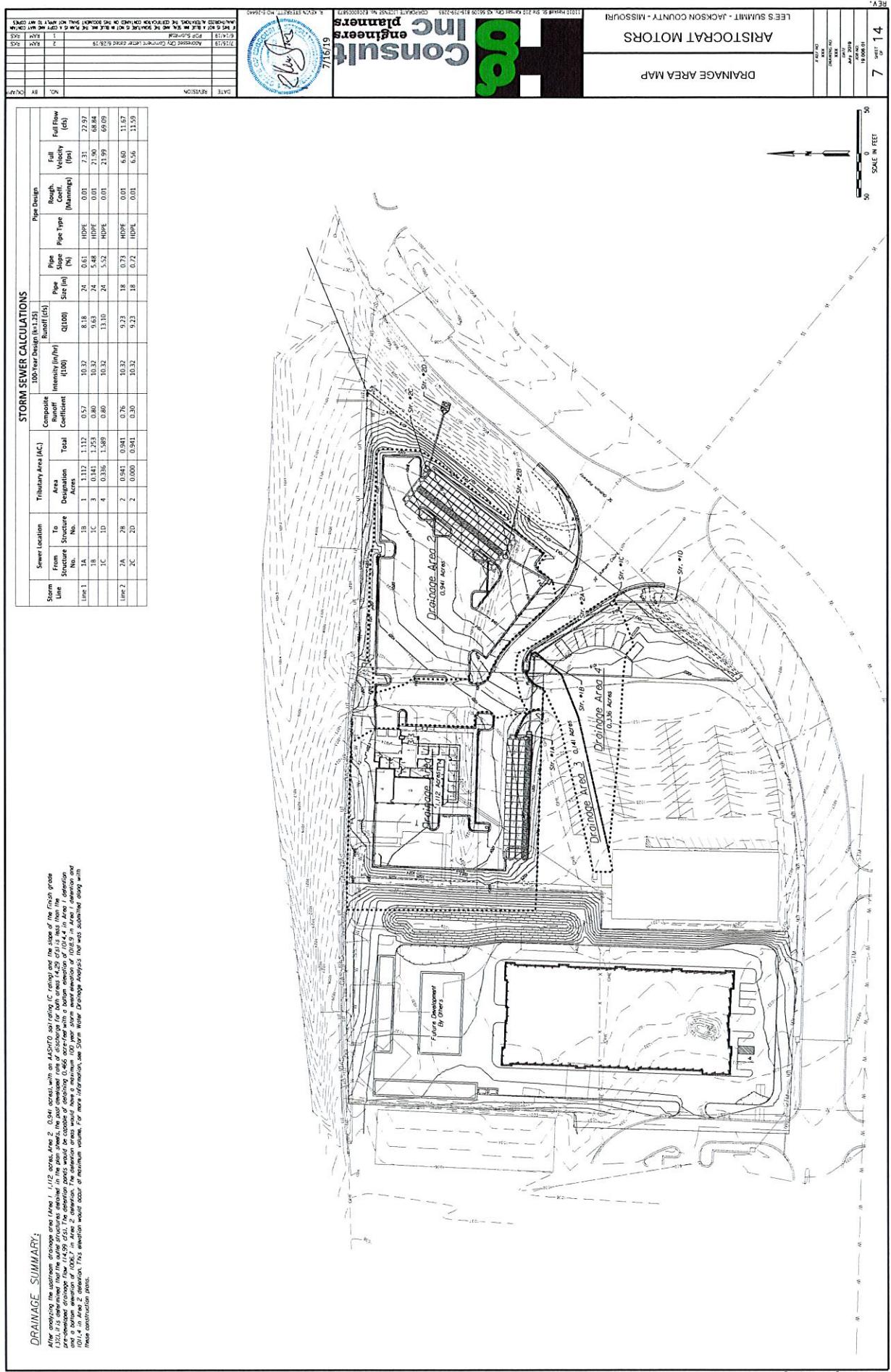
118.12

29°42'29"

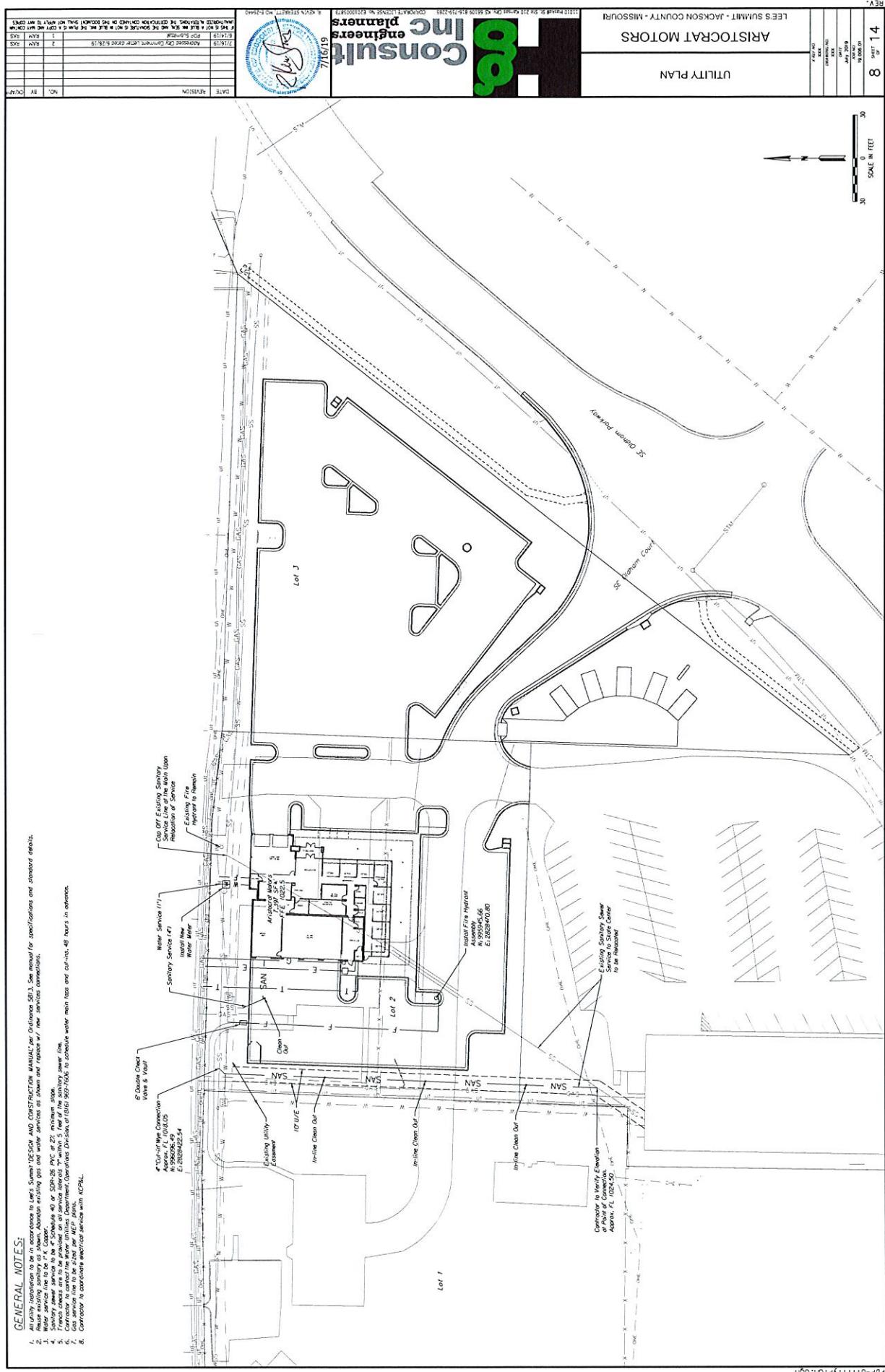




GENERAL NOTES: All storm sewer pipe to be HDPE w/ smooth interior.

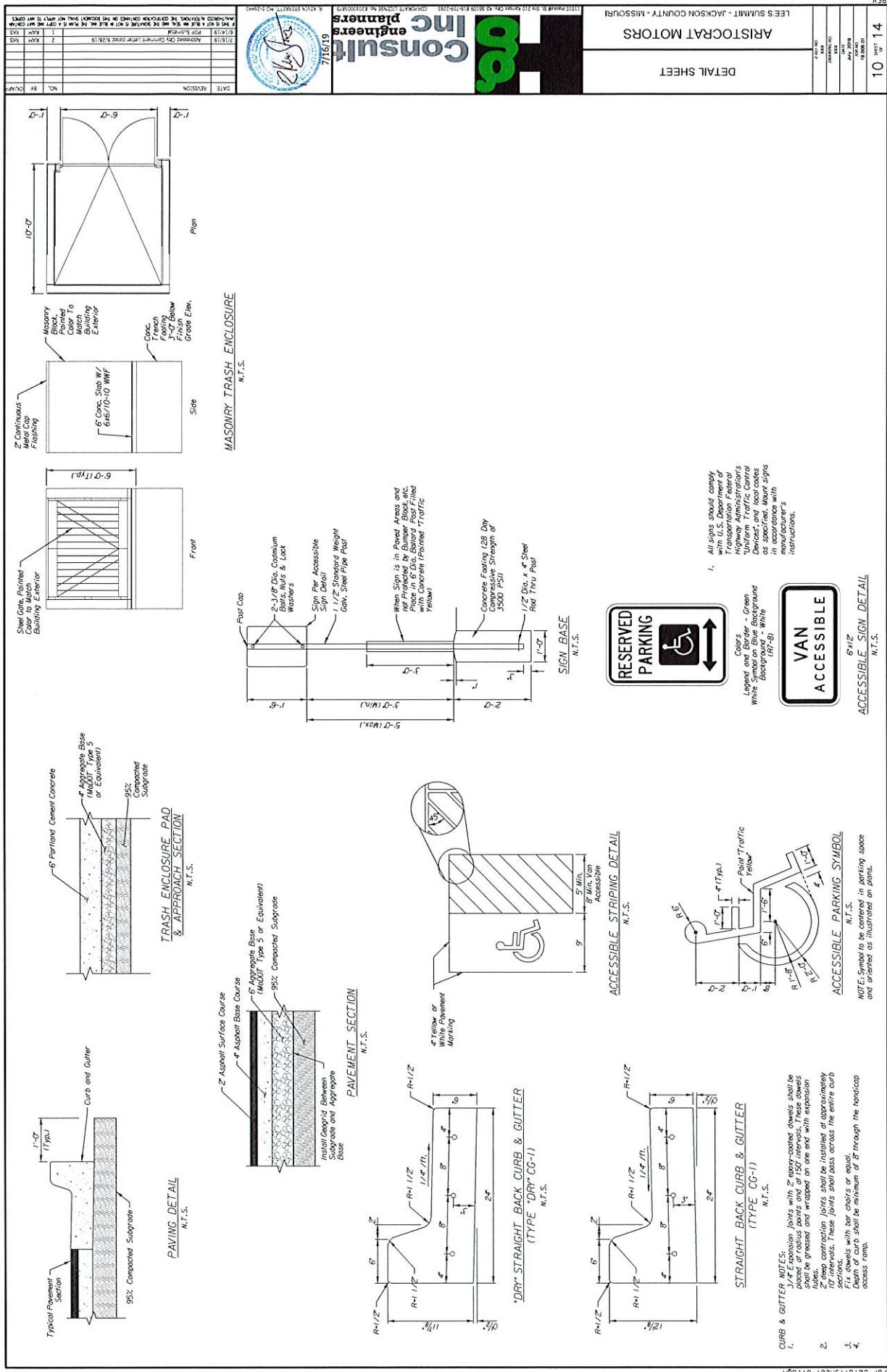


#### DRAINAGE SUMMARY:



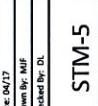
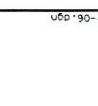
GENERAL NOTES.

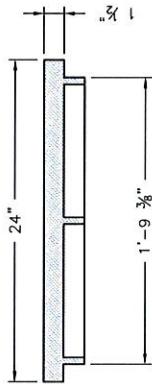
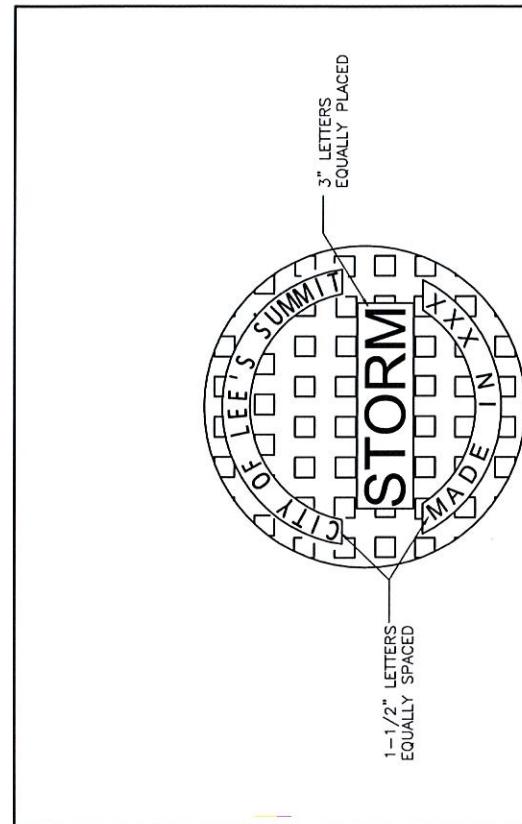






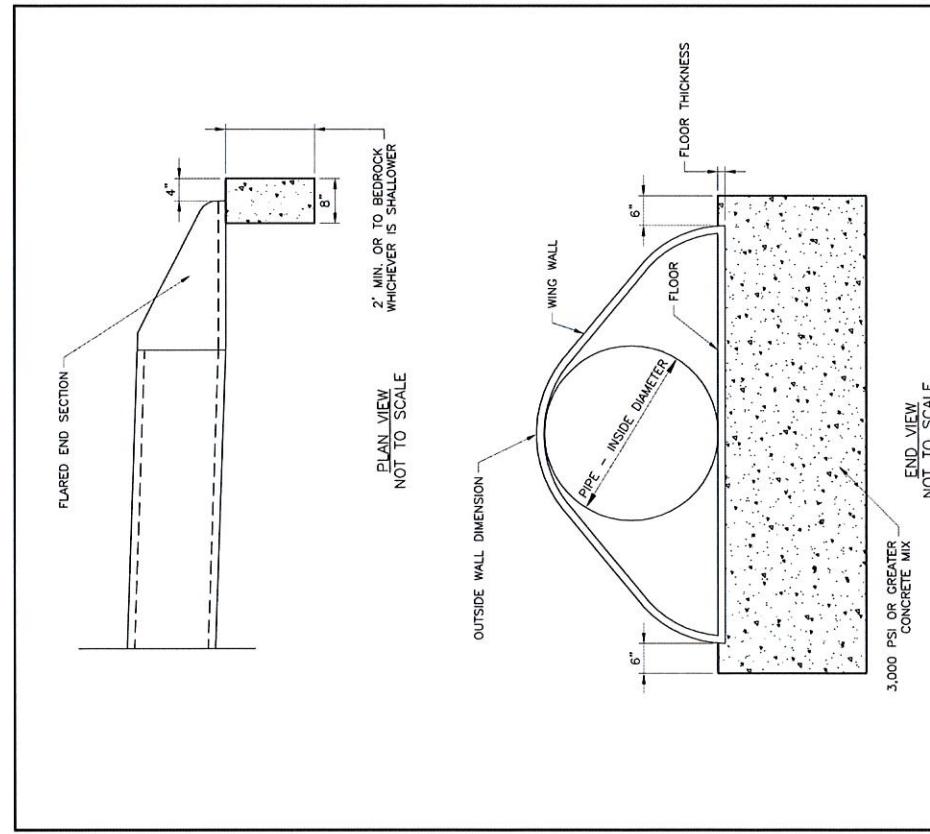


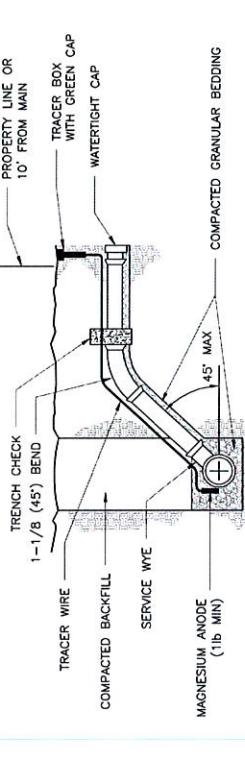
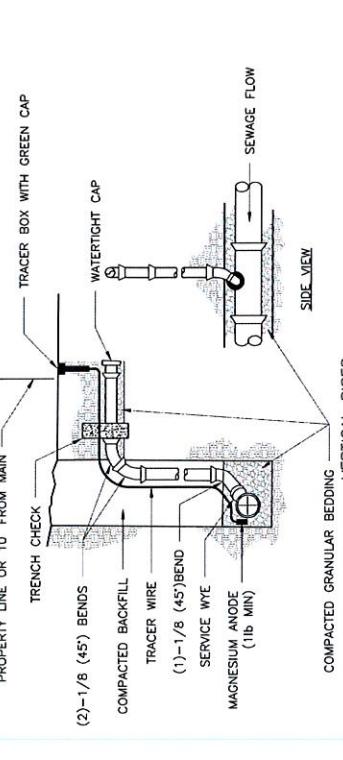
 <p><b>H2Consult</b> Inc. engineers</p> <p>LEES SUMMIT - JACKSON COUNTY - MISSOURI</p> <p>11150 Research Dr. Ste. 200, Lees Summit, MO 64063 Phone: 816-392-5925 Fax: 816-392-5926</p> <p>7/16/19</p> <p>DATE ISSUED 7/16/19</p> <p>NO. AB ACQUA</p>	<p><b>ARISTOCRAT MOTORS</b></p> <p>DETAIL SHEET</p> <p>REV. 13</p>	<p>LEES SUMMIT - JACKSON COUNTY - MISSOURI</p> <p>11150 Research Dr. Ste. 200, Lees Summit, MO 64063 Phone: 816-392-5925 Fax: 816-392-5926</p> <p>7/16/19</p> <p>DATE ISSUED 7/16/19</p> <p>NO. AB ACQUA</p>
 <p><b>LS</b></p> <p><b>LEE'S SUMMIT</b> MISSOURI</p> <p>PUBLIC WORKS ENGINEERING DIVISION   220 S GREEN STREET   LEE'S SUMMIT, MO 64063</p> <p><b>FLARED END SECTION SUPPORT DETAIL</b></p>		
 <p><b>LS</b></p> <p><b>LEE'S SUMMIT</b> MISSOURI</p> <p>PUBLIC WORKS ENGINEERING DIVISION   220 S GREEN STREET   LEE'S SUMMIT, MO 64063</p> <p><b>STM-5</b></p>		
 <p><b>LS</b></p> <p><b>LEE'S SUMMIT</b> MISSOURI</p> <p>PUBLIC WORKS ENGINEERING DIVISION   220 S GREEN STREET   LEE'S SUMMIT, MO 64063</p> <p><b>STM-6</b></p>		
 <p><b>LS</b></p> <p><b>LEE'S SUMMIT</b> MISSOURI</p> <p>PUBLIC WORKS ENGINEERING DIVISION   220 S GREEN STREET   LEE'S SUMMIT, MO 64063</p>		



STANDARD 24" MANHOLE COVER  
MINIMUM WEIGHT = 160 LB  
NOTE: PICK HOLES NOT SHOWN

\*COVER AND FRAME MODEL INFORMATION REFER TO  
THE STORMWATER APPROVED PRODUCT LIST.



 <p><b>LEE'S SUMMIT - JACKSON COUNTY - MISSOURI</b></p> <p>7/9/2019</p>	<p><b>ARISTOCART MOTORS</b></p> <p>7/16/19</p>	<p><b>DETAIL SHEET</b></p>	<p>REV. 14</p>								
 <p><b>H2 Consult Engineers</b></p>											
<div style="display: flex; justify-content: space-between;"> <div style="width: 45%;">  <p><b>STANDARD INSTALLATION</b> NOT TO SCALE</p> </div> <div style="width: 45%;">  <p><b>SIDE VIEW</b> NOT TO SCALE</p> </div> </div> <p><b>NOTES:</b></p> <ol style="list-style-type: none"> <li>ALL SEWER STUBS SHALL BE CONSTRUCTED TO PROPERTY LINE OR 10' MINIMUM FROM THE MAIN, WHERE SIDEWALKS ARE PRESENT, CONTRACTOR SHALL EXTEND SERVICE LINE UNDER EXISTING SIDEWALK TO TWO FEET BEYOND.</li> <li>ALL NEW CONSTRUCTION OFF SEWER STUBS SHALL BE TEMPORARILY MARKED WITH A MARKING STAKE, 36" ABOVE GROUND AND PAINTED GREEN, AND PAINTED GREEN.</li> <li>IMPERVIOUS TRENCH CHECKS SHALL BE PLACED ON BUILDING SEWER STUDS (AT LEAST 5' AWAY FROM THE SANITARY SEWER MAIN).</li> <li>TRENCH CHECKS ON THE BUILDING SEWER STUBS SHALL EXTEND 6" BELOW THE BOTTOM OF THE PIPE, LENGTH SHALL BE A MINIMUM OF 12". THE HEIGHT OF THE TRENCH CHECK SHALL EXTEND 12" ABOVE THE TOP OF THE PIPE.</li> <li>THE WIDTH OF THE TRENCH CHECK SECTION 2100 FOR SEWER MAIN BEDDING AND BACKFILL.</li> <li>INSTALLED DIRECTLY ABOVE THE SEWER SERVICE LINE, SHALLOW FROM THE TRENCH FLOOR TO THE TRENCH TOP.</li> <li>FOR SERVICES, THE TRENCH CHECK SHALL BE RATED AT 5' DIA. AND 10' DIA. FOR SEWER, AND 10' DIA. FOR STORM.</li> <li>THE TRACER WIRE SHALL BE REINFORCED WITH A COATING THIN AS POSSIBLE, TO THE GREATEST EXTENT POSSIBLE, MADE WITH SPILT BOLT CONNECTORS. NUTS SHALL NOT BE USED. A WATER-TIGHT CONNECTION IS NECESSARY TO PREVENT CORROSION.</li> </ol>											
 <p><b>LEE'S SUMMIT</b> <b>M I S S O U R I</b> PUBLIC WORKS ENGINEERING DIVISION   220 SE GREEN STREET   LEE'S SUMMIT, MO 64063</p> <p><b>BUILDING SEWER STUB AND RISER</b></p> <p><b>STM-7</b></p>											
<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 10%;">Date: 04/17</td> <td style="width: 10%;">Drawn By: Mif</td> <td style="width: 10%;">Checked By: DL</td> <td style="width: 10%;">Approved By: DR</td> </tr> <tr> <td colspan="4">F-P-D641015Sheet-07.GG</td> </tr> </table>				Date: 04/17	Drawn By: Mif	Checked By: DL	Approved By: DR	F-P-D641015Sheet-07.GG			
Date: 04/17	Drawn By: Mif	Checked By: DL	Approved By: DR								
F-P-D641015Sheet-07.GG											
<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 10%;">Date: 04/17</td> <td style="width: 10%;">Drawn By: Mif</td> <td style="width: 10%;">Checked By: DL</td> <td style="width: 10%;">Approved By: DR</td> </tr> <tr> <td colspan="4">F-P-D641015Sheet-07.GG</td> </tr> </table>				Date: 04/17	Drawn By: Mif	Checked By: DL	Approved By: DR	F-P-D641015Sheet-07.GG			
Date: 04/17	Drawn By: Mif	Checked By: DL	Approved By: DR								
F-P-D641015Sheet-07.GG											

PROJECT INFORMATION	
ENGINEERED PRODUCT MANAGER	GARRETT KUNGLER 816-401-7559 GARRETT.KUNGLER@ADS-PIPE.COM
ADS SALES REP:	JOHN WHITWOOD 516-405-5570 JOHN.WHITWOOD@ADS-PIPE.COM
PROJECT NO:	181081



ADVANCED DRAINAGE SYSTEMS, INC.

# KANSAS CITY MOTORS

## LEE'S SUMMIT, MO

### STORMTECH CHAMBER SPECIFICATIONS

1. CHAMBERS SHALL BE STORMTECH MC-3500.
2. CHAMBERS SHALL BE MADE FROM VIRGIN, IMPACT-MODIFIED POLYPROPYLENE COPOLYMERS.
3. CHAMBER ROWS SHALL PROVIDE CONTINUOUS, UNOBSTRUCTED INTERNAL SPACE WITH NO INTERNAL SUPPORT PANELS THAT WOULD IMPEDE FLOW OR LIMIT ACCESS FOR INSPECTION.
4. THE STRUCTURAL DESIGN OF THE CHAMBERS, THE STRUCTURAL BACKFILL, AND THE INSTALLATION REQUIREMENTS SHALL ENSURE THAT THE LOAD FACTORS SPECIFIED IN THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, SECTION 12.12, ARE MET FOR: 1) LONG-DURATION DEAD LOADS AND 2) SHORT-DURATION LIVE LOADS, BASED ON THE AASHTO DESIGN TRUCK WITH CONSIDERATION FOR IMPACT AND MULTIPLE VEHICLE PRESENCE.
5. CHAMBERS SHALL MEET THE REQUIREMENTS OF ASTM F2418-16, "STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
6. CHAMBERS SHALL BE DESIGNED AND ALLOWABLE LOADS DETERMINED IN ACCORDANCE WITH ASTM F2787, "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
7. ONLY CHAMBERS THAT ARE APPROVED BY THE SITE DESIGN ENGINEER WILL BE ALLOWED. THE CHAMBER MANUFACTURER SHALL SUBMIT THE FOLLOWING UPON REQUEST TO THE SITE DESIGN ENGINEER FOR APPROVAL BEFORE DELIVERING CHAMBERS TO THE PROJECT SITE:
  - a. A STRUCTURAL EVALUATION SEALED BY A REGISTERED PROFESSIONAL ENGINEER THAT DEMONSTRATES THAT THE SAFETY FACTORS ARE GREATER THAN OR EQUAL TO .95 FOR DEAD LOAD AND 1.75 FOR LIVE LOAD. THE MINIMUM REQUIRED BY ASTM F2787 AND BY AASHTO OR THERMOPLASTIC PIPE.
  - b. A STRUCTURAL EVALUATION SEALED BY A REGISTERED PROFESSIONAL ENGINEER THAT DEMONSTRATES THAT THE LOAD MODULUS DATA SPECIFIED IN THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, SECTION 12.12, ARE MET; THE 50 YEAR CREEP LONG-TERM PERFORMANCE.
  - c. STRUCTURAL CROSS SECTION DETAIL ON WHICH THE STRUCTURAL EVALUATION IS BASED.
8. CHAMBERS AND END CAPS SHALL BE PRODUCED AT AN ISO 9001 CERTIFIED MANUFACTURING FACILITY.

### IMPORTANT - NOTES FOR THE BIDDING AND INSTALLATION OF MC-3500 CHAMBER SYSTEM

1. STORMTECH MC-3500 CHAMBERS SHALL NOT BE INSTALLED UNTIL THE MANUFACTURER'S REPRESENTATIVE HAS COMPLETED A PRE-CONSTRUCTION MEETING WITH THE INSTALLERS.
2. STORMTECH MC-3500 CHAMBERS SHALL BE INSTALLED IN ACCORDANCE WITH THE "STORMTECH MC-3500/MC-4500 CONSTRUCTION GUIDE".
3. STORMTECH RECOMMENDS A BACKFILLED METHODS:
  - STONE SHOTCREW LOCATED OFF THE CHAMBER BED.
  - BACKFILL ASSEMBLIES ARE BUILT USING AN EXCAVATOR ON THE FOUNDATION STONE OR SUBGRADE.
  - BACKFILL FROM OUTSIDE THE EXCAVATION USING LONG BOOM HEE OR EXCAVATOR.
4. THE FOUNDATION STONE SHALL BE LEVELLED AND COMPACTED PRIOR TO PLACING CHAMBERS.
5. JOINTS BETWEEN CHAMBERS SHALL BE PROPERLY SEALED PRIOR TO PLACING STONE.
6. MAINTAIN MINIMUM 9' (230 mm) SPACING BETWEEN THE CHAMBER ROWS.
7. INLET AND OUTLET MANIFOLDS MUST BE INSERTED A MINIMUM OF 12" (300 mm) INTO CHAMBER END CAPS.
8. EMBEIDMENT STONE SURROUNDING CHAMBERS MUST BE A CLEAN, CRUSHED, ANGULAR STONE 3/4"-2" (20-50 mm) MEETING THE AASHTO M43 DESIGNATION OF #3 OR #4.
9. STONE MUST BE PLACED ON THE TOP CENTER OF THE CHAMBER TO ANCHOR THE CHAMBERS IN PLACE AND PRESERVE ROW SPACING.
10. THE CONTRACTOR MUST REPORT ANY DISCREPANCIES WITH CHAMBER FOUNDATION MATERIALS BEARING CAPACITIES TO THE SITE DESIGN ENGINEER.
11. ADS RECOMMENDS THE USE OF "FLEXISTORM CATCH IT" INSERTS DURING CONSTRUCTION FOR ALL INLETS TO PROTECT THE SUBSURFACE STORMWATER MANAGEMENT SYSTEM FROM CONSTRUCTION SITE RUNOFF.

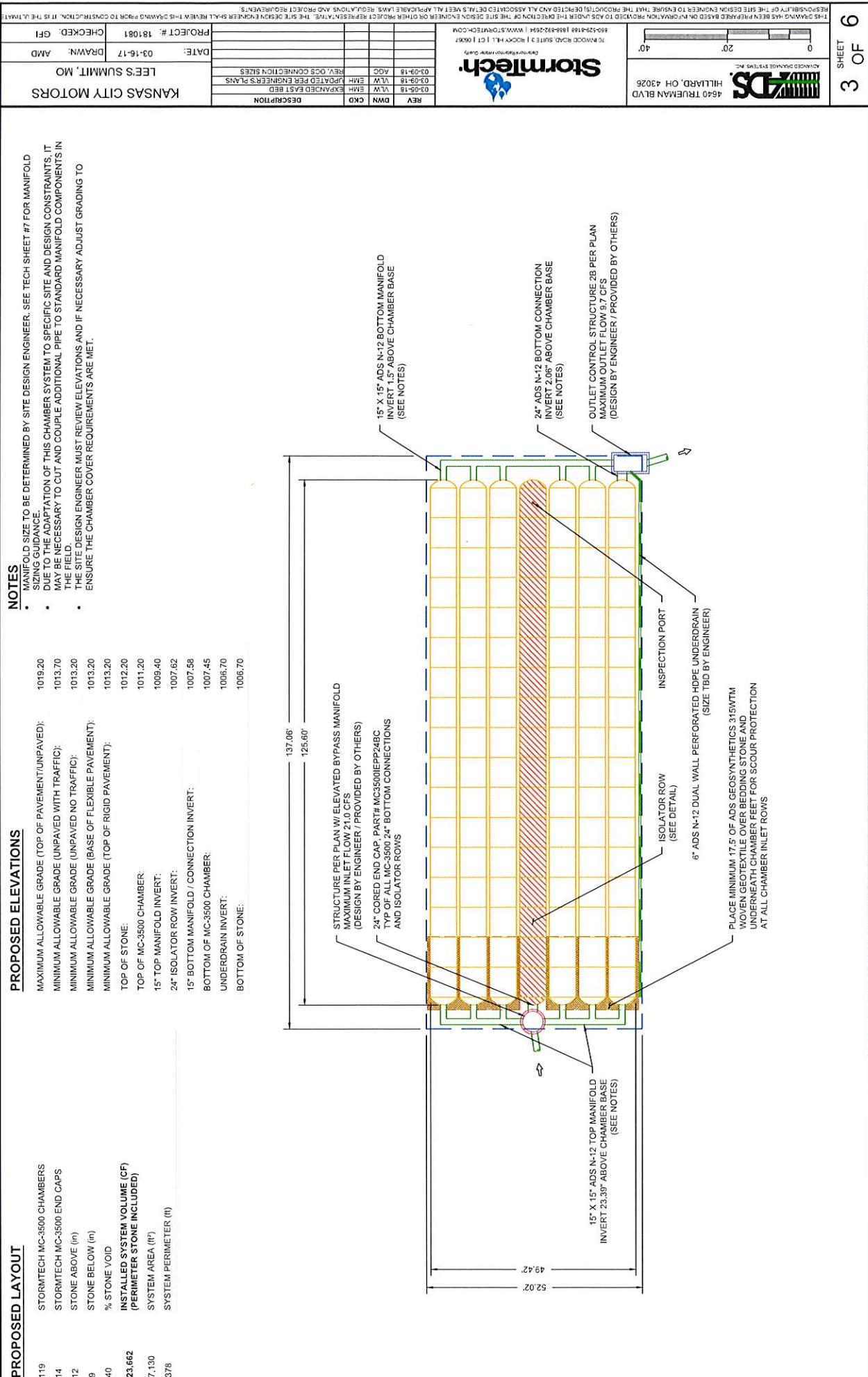
### NOTES FOR CONSTRUCTION EQUIPMENT

1. STORMTECH MC-3500 CHAMBERS SHALL BE INSTALLED IN ACCORDANCE WITH THE "STORMTECH MC-3500/MC-4500 CONSTRUCTION GUIDE".
2. THE USE OF EQUIPMENT OVER MC-3500 CHAMBERS IS LIMITED:
  - NO EQUIPMENT IS ALLOWED ON BARE CHAMBERS.
  - NO RUBBER TIRED LOADER, DUMP TRUCK OR EXCAVATORS ARE ALLOWED UNTIL PROPER FILL DEPTHS ARE REACHED IN ACCORDANCE WITH THE "STORMTECH MC-3500/MC-4500 CONSTRUCTION GUIDE".
  - WEIGHT LIMITS FOR CONSTRUCTION EQUIPMENT CAN BE FOUND IN THE "STORMTECH MC-3500/MC-4500 CONSTRUCTION GUIDE".
3. FULL 36" (900 mm) OF STABILIZED COVER MATERIALS OVER THE CHAMBERS IS REQUIRED FOR DUMP TRUCK TRAVEL OR DUMPING.
 

USE OF A DOZER TO PUSH EMBEDMENT STONE BETWEEN THE ROWS OF CHAMBERS MAY CAUSE DAMAGE TO CHAMBERS AND IS NOT AN ACCEPTABLE BACKFILL METHOD. ANY CHAMBERS DAMAGED BY USING THE "DUMP AND PUSH" METHOD ARE NOT COVERED UNDER THE STORMTECH STANDARD WARRANTY.

CONTACT STORMTECH AT 1-888-892-2694 WITH ANY QUESTIONS ON INSTALLATION REQUIREMENTS OR WEIGHT LIMITS FOR CONSTRUCTION EQUIPMENT.





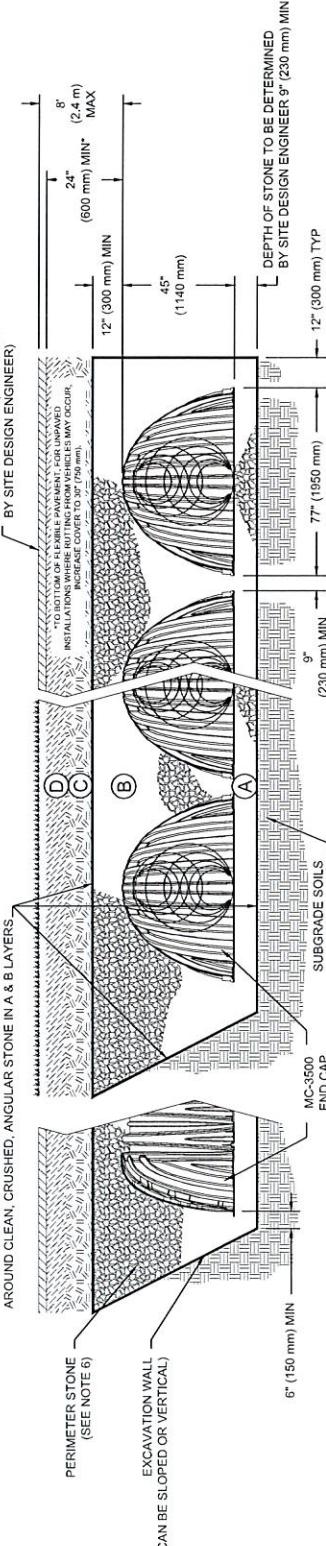
## ACCEPTABLE FILL MATERIALS: STORMTECH MC-3500 CHAMBER SYSTEMS

MATERIAL LOCATION	DESCRIPTION	AASHTO MATERIAL CLASSIFICATIONS	COMPACTION / DENSITY REQUIREMENT
D FINAL FILL: FILL MATERIAL FOR LAYER 'D' STARTS FROM THE TOP OF THE 'C' LAYER TO THE BOTTOM OF FLEXIBLE PAVEMENT OR UNPAVED FINISHED GRADE ABOVE. NOTE THAT PAVEMENT SUBGRADE MAY BE PART OF THE 'D' LAYER.	ANY SOIL/ROCK MATERIALS, NATIVE SOILS, OR PER ENGINEER'S PLANS. CHECK PLANS FOR PAVEMENT SUBGRADE REQUIREMENTS.	N/A	PREPARE PER SITE DESIGN ENGINEER'S PLANS. PAVED INSTALLATIONS MAY HAVE STRINGENT MATERIAL AND PREPARATION REQUIREMENTS.
C INITIAL FILL: FILL MATERIAL FOR LAYER 'C'. STARTS FROM THE TOP OF THE EMBEDMENT STONE ('B' LAYER) TO 24" (600 mm) ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBBASE MAY BE A PART OF THE 'C' LAYER.	GRANULAR WELL-GRADED SOIL/AGGREGATE MIXTURES, <35% FINES OR PROCESSED AGGREGATE. MOST PAVEMENT SUBBASE MATERIALS CAN BE USED IN LIEU OF THIS LAYER.	AASHTO M145 <sup>1</sup> A-1, A-2-A, A-3 OR AASHTO M43 <sup>1</sup> 3, 357, 4, 467, 5, 56, 57, 6, 67, 68, 7, 78, 8, 89, 9, 10	BEGIN COMPACTATIONS AFTER 24" (600 mm) OF MATERIAL OVER THE CHAMBERS IS REACHED. COMPACT ADDITIONAL LAYERS IN 12" (300 mm) MAX LIFTS TO A MIN. 35% PROCTOR DENSITY FOR WELL GRADED MATERIAL AND 95% RELATIVE DENSITY FOR PROCESSED AGGREGATE MATERIALS.
B EMBEDMENT STONE: FILL SURROUNDING THE CHAMBERS FROM THE FOUNDATION STONE ('A' LAYER) TO THE 'C' LAYER ABOVE.	CLEAN, CRUSHED, ANGULAR STONE	AASHTO M43 <sup>1</sup> 3, 4	NO COMPACTION REQUIRED.
A FOUNDATION STONE: FILL BELOW CHAMBERS FROM THE SUBGRADE UP TO THE FOOT (BOTTOM) OF THE CHAMBER.	CLEAN, CRUSHED, ANGULAR STONE	AASHTO M43 <sup>1</sup> 3, 4	PLATE COMPACT OR ROLL TO ACHIEVE A FLAT SURFACE. <sup>2</sup>

PLEASE NOTE:

1. THE LISTED AASHTO DESIGNATIONS ARE FOR GRADATIONS ONLY. THE STONE MUST ALSO BE CLEAN, CRUSHED, ANGULAR, FOR EXAMPLE, A SPECIFICATION FOR #4 STONE WOULD STATE: "CLEAN, CRUSHED, ANGULAR NO. 4 (AASHTO M43) STONE".
2. STORMTECH COMPACTION REQUIREMENTS ARE MET FOR 'A' LOCATION MATERIALS WHEN PLACED AND COMPACTED IN 9" (220 mm) MAX LIFTS USING TWO FULL COVERS WITH A VIBRATORY COMPACTOR. WHERE INFILTRATION SURFACES MAY BE COMPROMISED BY COMPACTION, FOR STANDARD DESIGN LOAD CONDITIONS, A FLAT SURFACE MAY BE ACHIEVED BY RAKING OR DRAGGING WITHOUT COMPACTION EQUIPMENT. FOR SPECIAL LOAD DESIGNS, CONTACT STORMTECH FOR COMPACTION REQUIREMENTS.

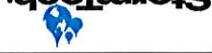
ADS GEOSYNTHETICS: 60# TON NONWOVEN GEOTEXTILE ALL AROUND CLEAN, CRUSHED, ANGULAR STONE IN 'A' & LAYERS

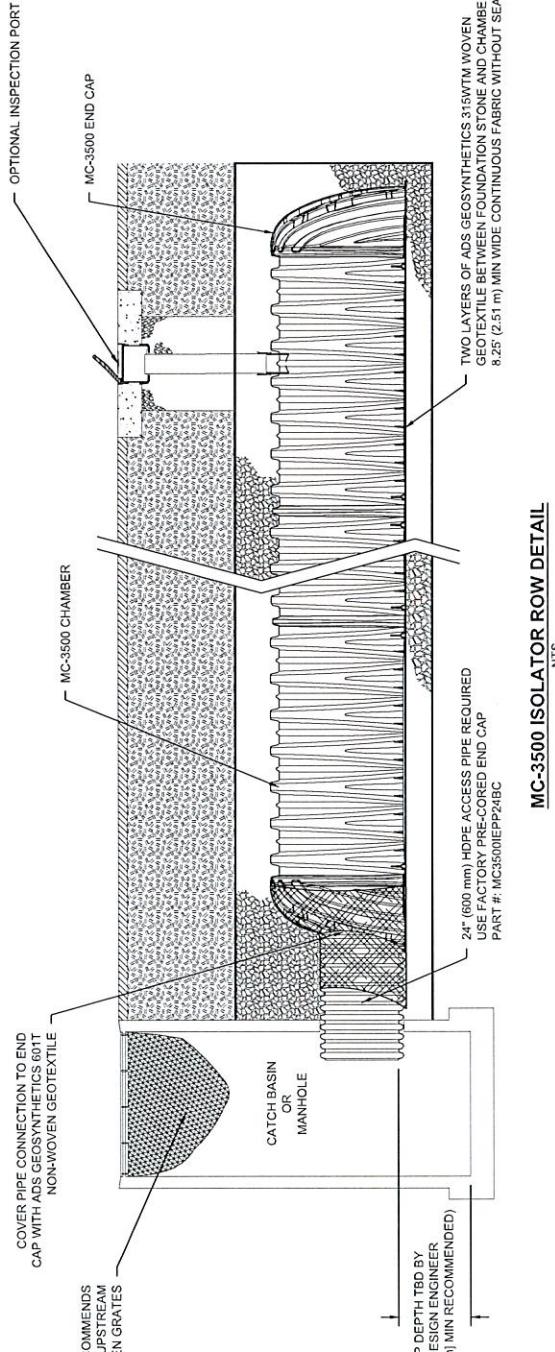


### NOTES:

1. MC-3500 CHAMBERS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F2418 "STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
2. MC-3500 CHAMBERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM F2787 "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
3. "ACCEPTABLE FILL MATERIALS" TABLE ABOVE PROVIDES MATERIAL LOCATIONS, DESCRIPTIONS, GRADATIONS, AND COMPACTION REQUIREMENTS FOR FOUNDATION, EMBODIMENT, AND FILL MATERIALS.
4. THE SITE DESIGN ENGINEER IS RESPONSIBLE FOR ASSESSING THE BEARING RESISTANCE (ALLOWABLE BEARING CAPACITY) OF THE SUBGRADE SOILS AND THE DEPTH OF FOUNDATION STONE WITH CONSIDERATION FOR THE RANGE OF EXPECTED SOIL MOISTURE CONDITIONS.
5. ONCE LAYER 'C' IS PLACED, ANY SOIL/MATERIAL CAN BE PLACED IN LAYER 'D' UP TO THE FINISHED GRADE. MOST PAVEMENT SUBBASE SOILS CAN BE USED TO REPLACE THE MATERIAL REQUIREMENTS OF LAYER 'C' OR 'D' AT THE SITE DESIGN ENGINEER'S DISCRETION.
6. PERIMETER STONE MUST BE EXTENDED HORIZONTALLY TO THE EXCAVATION WALL FOR BOTH VERTICAL AND SLOPED EXCAVATION WALLS.

<span style="font-size: 10pt; vertical-align: middle;">4640 TRUEMAN BLVD</span> <span style="font-size: 10pt; vertical-align: middle;">HILLARD, OH 43026</span>		<span style="font-size: 10pt; vertical-align: middle;">ADVANCED CHANNEL SYSTEMS INC.</span>	
<span style="font-size: 10pt;">REV: DWN</span> <span style="font-size: 10pt;">CDR:</span> <span style="font-size: 10pt;">DESCRPTION:</span>		<span style="font-size: 10pt;">03-09-18</span> <span style="font-size: 10pt;">V/W</span> <span style="font-size: 10pt;">EMH</span> <span style="font-size: 10pt;">EXPANDED EASY-BREND® PLANS</span>	
<span style="font-size: 10pt;">LETS SUMMIT, MO</span>		<span style="font-size: 10pt;">03-16-17</span> <span style="font-size: 10pt;">DRAWN:</span> <span style="font-size: 10pt;">AMD</span>	
<span style="font-size: 10pt;">DATE:</span>		<span style="font-size: 10pt;">PROJECT #: 181081</span> <span style="font-size: 10pt;">CHECKED:</span> <span style="font-size: 10pt;">GF</span>	
<small>THIS DRAWING HAS BEEN PREPARED BASED ON INFORMATION PROVIDED TO ACS NURC THE PROJECT(S) OF THE SITE DESIGN ENGINEER. ALL ASSOCIATE DESIGNERS, CONTRACTORS, AND PROFESSIONALS INVOLVED IN THE DESIGN AND CONSTRUCTION OF THIS DRAWING SHALL REVIEW THIS DRAWING PRIOR TO CONSTRUCTION. IT IS THE ULTIMATE RESPONSIBILITY OF THE SITE DESIGNER TO ENSURE THAT THE PROJECT(S) OF THE SITE DESIGN ENGINEER IS PROTECTED. THIS DRAWING IS THE PROPERTY OF THE SITE DESIGN ENGINEER. OTHER PROJECTS OWNED BY THIS COMPANY MAY NOT BE RELATED TO THIS DRAWING.</small>			

	
<small>4640 TUREMAN BLVD, HILLIARD, OH 43026 www.stormtech.com</small>	
<small>660-522-1186   666-829-1111   www.stormtech.com info@stormtech.com</small>	
<small>DESIGNER OF THE STORMTECH® SYSTEM STORMTECH® IS A REGISTERED TRADEMARK OF STORMTECH INC.</small>	
<small>THIS DESIGN IS PREPARED BASED ON PROJECT SCOPE AND THE PROJECT DESIGNER IS RESPONSIBLE FOR THE DESIGN OF THE PROJECT. THIS DESIGN IS NOT A CONTRACTUAL AGREEMENT. THE SITE DESIGN ENGINEER SHALL REVIEW THIS DESIGN AND PROPOSE CHANGES AS NECESSARY. ALL APPLICABLE LAWS, REGULATIONS AND CODES SHALL BE FOLLOWED IN THE DESIGN OF THIS SYSTEM. IT IS THE DUTY OF THE DESIGNER TO CONSTRUCT THIS SYSTEM IN ACCORDANCE WITH THE PROJECT REQUIREMENTS.</small>	
<small>REV. DWN CDG DESCRIPTION 03-05-18 L/W E/M IMPERMEABLE END CAPS 03-09-18 L/W E/M IMPERMEABLE END CAPS LEES SUMMIT, MO</small>	
<small>DATE: 03-16-17 DRAWN: AMD PROJECT #: 181081 CHECKED: GF</small>	



**MC-3500 ISOLATOR ROW DETAIL**  
NTS

**INSPECTION & MAINTENANCE**

**STEP 1)** INSPECT ISOLATOR ROW FOR SEDIMENT  
 A. INSPECTION PORTS (IF PRESENT)  
 i. REMOVE/OPEN LID ON NYLOPLAST INLINE DRAIN  
 ii. REMOVE AND CLEAN FLEXSTORM FILTER IF INSTALLED  
 iii. REMOVE AND CLEAN FLEXSTORM FILTER IF INSTALLED  
 iv. LOWER A CAMERA INTO ISOLATOR ROW FOR VISUAL INSPECTION OF SEDIMENT LEVELS ON MAINTENANCE LOG  
 v. IF SEDIMENT IS AT-, OR ABOVE, 3" (80 mm) PROCEED TO STEP 2. IF NOT, PROCEED TO STEP 3.  
 B. ALL ISOLATOR ROWS  
 i. REMOVE COVER FROM STRUCTURE AT UPSTREAM END OF ISOLATOR ROW  
 ii. USING A FLASHLIGHT, INSPECT DOWN THE ISOLATOR ROW THROUGH OUTLET PIPE  
 iii. MIRRORS ON POLES OR CAMERS MAY BE USED TO AVOID A CONFINED SPACE ENTRY  
 iv. FOLLOW OSHA REGULATIONS FOR CONFINED SPACE ENTRY IF ENTERING MANHOLE  
 v. IF SEDIMENT IS AT-, OR ABOVE, 3" (80 mm) PROCEED TO STEP 2. IF NOT, PROCEED TO STEP 3.

**STEP 2)** CLEAN OUT ISOLATOR ROW USING THE JETVAC PROCESS  
 A. A FIXED CULVERT CLEANING NOZZLE WITH TEAR FACING SPREAD OF 45° (1.1 m) OR MORE IS PREFERRED  
 B. APPLY MULTIPLE PASSES OF JETVAC UNTIL BACKFLUSH WATER IS CLEAN  
 C. VACUUM STRUCTURE SUMP AS REQUIRED

**STEP 3)** REPLACE ALL COVERS, GRATES, FILTERS, AND LIDS; RECORD OBSERVATIONS AND ACTIONS.

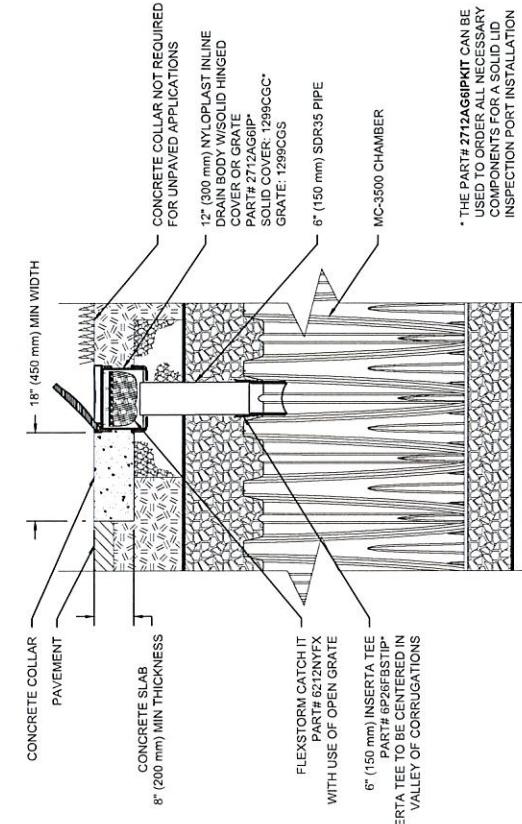
**STEP 4)** INSPECT AND CLEAN BASINS AND MANHOLES UPSTREAM OF THE STORMTECH SYSTEM.

**NOTES**

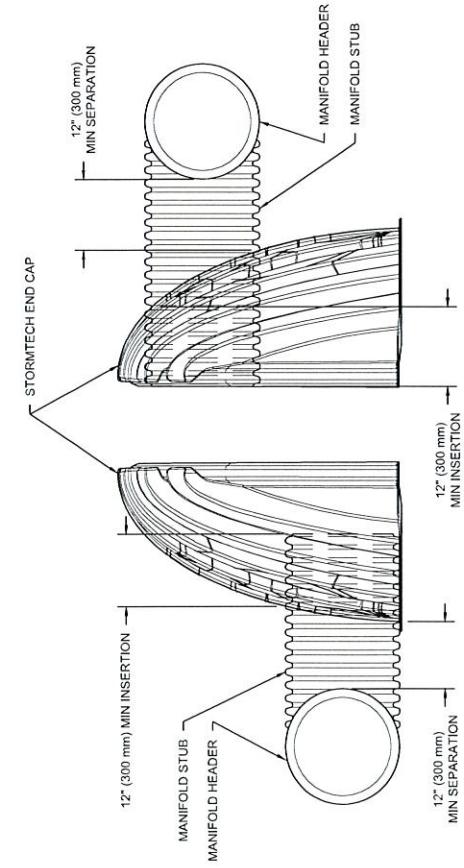
1. INSPECT EVERY 6 MONTHS DURING THE FIRST 3 YEARS OF OPERATION. ADJUST THE INSPECTION INTERVAL BASED ON PREVIOUS OBSERVATIONS OF SEDIMENT ACCUMULATION AND HIGH WATER ELEVATIONS.
2. CONDUCT JETTING AND VACTORING ANNUALLY OR WHEN INSPECTION SHOWS THAT MAINTENANCE IS NECESSARY.

\* THE PART# 2712AGRIPKIT CAN BE USED TO ORDER ALL NECESSARY COMPONENTS FOR A SOLID LID INSPECTION PORT INSTALLATION

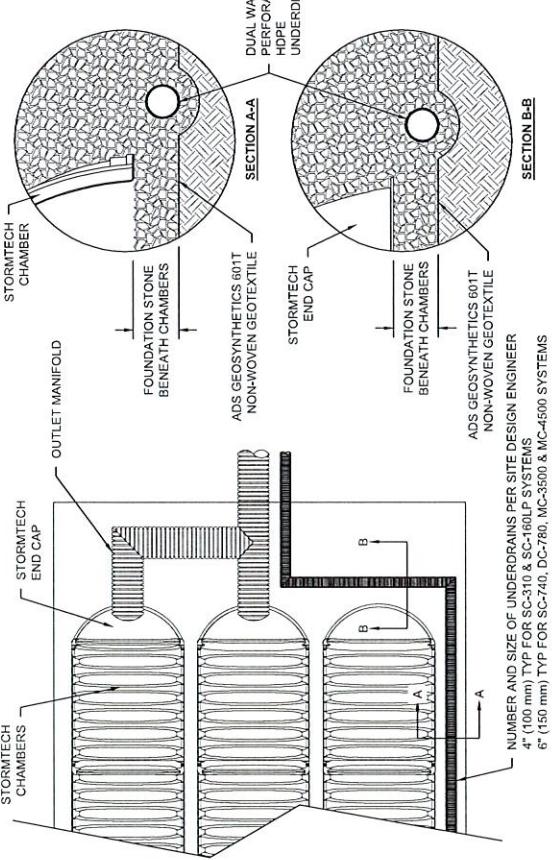
**MC-3500 6" INSPECTION PORT DETAIL**  
NTS



### MC-SERIES END CAP INSERTION DETAIL

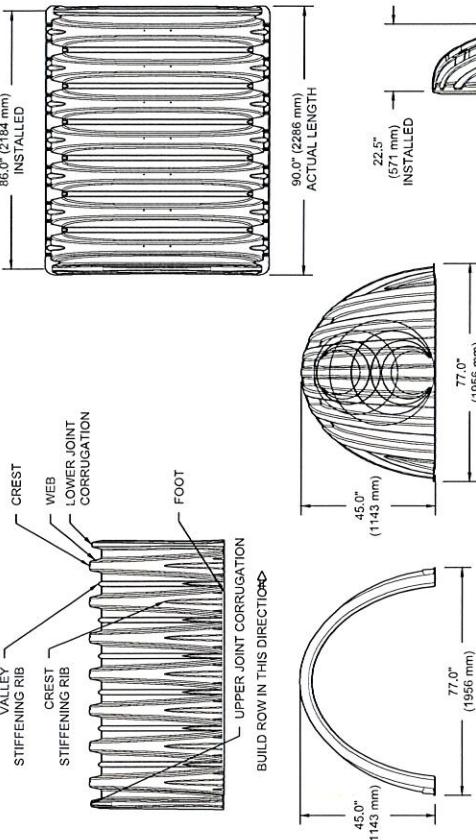


### UNDERDRAIN DETAIL



### MC-3500 TECHNICAL SPECIFICATION

N.T.S.



#### NOMINAL CHAMBER SPECIFICATIONS

SIZE (W X H X INSTALLED LENGTH)  
CHAMBER STORAGE  
MINIMUM INSTALLED STORAGE -

WEIGHT

#### NOMINAL END CAP SPECIFICATIONS

SIZE (W X H X INSTALLED LENGTH)  
END CAP STORAGE  
MINIMUM INSTALLED STORAGE -

WEIGHT

\*ASSUMES 12" (305 mm) STONE ABOVE, 9" (229 mm) STONE FOUNDATION AND BETWEEN CHAMBERS,  
12" (305 mm) STONE PERIMETER IN FRONT OF END CAPS AND 40% STONE POROSITY

STUBS AT BOTTOM OF END CAP FOR PART NUMBERS ENDING WITH "B"

PART #	STUB	B	C
MC3500IEPP016T	6" (150 mm)	33.21" (844 mm)	—
MC3500IEPP02B	8" (200 mm)	31.16" (791 mm)	0.68" (17 mm)
MC3500IEPP08T	10" (250 mm)	29.04" (738 mm)	—
MC3500IEPP10B	—	—	0.81" (21 mm)
MC3500IEPP12T	12" (300 mm)	26.36" (670 mm)	—
MC3500IEPP12B	—	—	1.35" (34 mm)
MC3500IEPP15T	15" (375 mm)	23.39" (594 mm)	—
MC3500IEPP15B	—	—	1.50" (38 mm)
MC3500IEPP18C	18" (450 mm)	20.03" (509 mm)	—
MC3500IEPP18BC	—	—	1.77" (45 mm)
MC3500IEPP24TC	24" (600 mm)	14.48" (368 mm)	—
MC3500IEPP30BC	30" (750 mm)	—	2.06" (52 mm)

NOTE: ALL DIMENSIONS ARE NOMINAL.

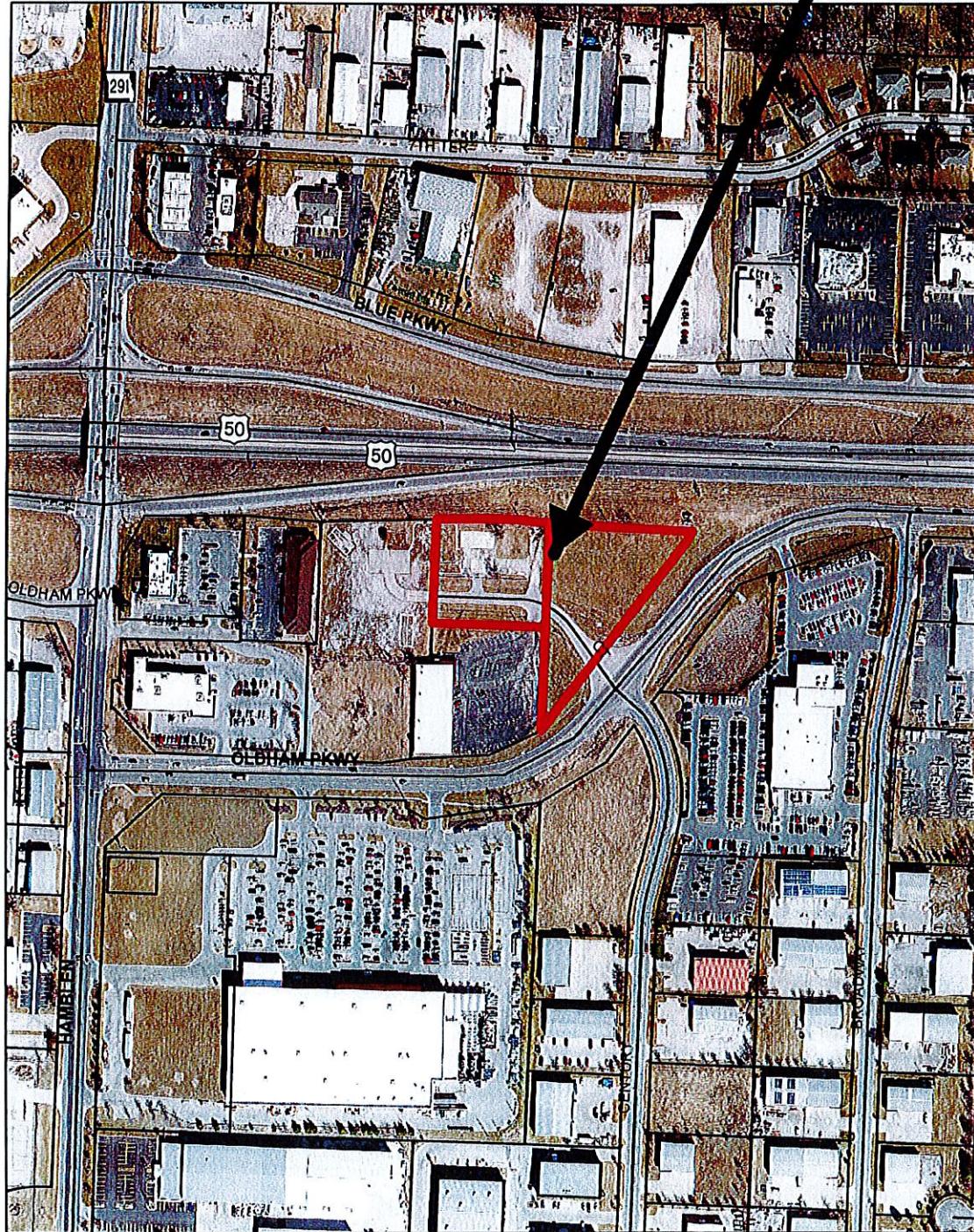
CUSTOM PRECURED INVERTS ARE AVAILABLE UPON REQUEST. INVENTORIED MANIFOLDS INCLUDE  
12"-24" (300-600 mm) SIZE ON SIZE AND 15-48" (375-1200 mm) ECCENTRIC MANIFOLDS.  
CUSTOM INVERT LOCATIONS ON THE MC-3500 END CAP CUT IN THE FIELD ARE NOT RECOMMENDED  
FOR PIPE SIZES GREATER THAN 10" (250 mm).  
THE INVERT LOCATION IN COLUMN B ARE THE HIGHEST POSSIBLE FOR THE PIPE SIZE.

6605234188 1664694294   WWW.STORMTECH.COM	DESIGNER AND APPROVED BY: SCOTT MCGOWAN   DATE: 03-16-17	REV: DWN CRD DECRPTN	03-09-18 AGC REV'D OCS CONNECTIONS PLANS	03-05-18 VLM EXPANDED EASY CONNECT	LEES SUMMIT, MO
STORMTECH.	DATE: 03-16-17 DRAWN: AMD	PROJECCT #: 181081	CHECKED: GF	REVIEWED: LMS	APPROVED: LMS
THIS DRAWING HAS BEEN PREPARED BASED ON INFORMATION PROVIDED TO ENGINEER THAT THE PRODUCTS(S) DESIGNED AND PRODUCED TO DESIGN OF THE SITE DESIGN ENGINEER SHALL REVIEW THIS DRAWING PRIOR TO CONSTRUCTION. IT IS THE ULTIMATE RESPONSIBILITY OF THE SITE DESIGNER TO ENGINEER TO INSURE THAT THE PRODUCTS(S) DESIGNED AND PRODUCED TO DESIGN OF THE SITE DESIGN ENGINEER MEET ALL ASSOCIATED REQUIREMENTS.					





**PL2019-204 PRELIM DEV PLAN and  
PL2019-203 SUP  
Artistocrat Motors**



N

