

AN ORDINANCE APPROVING A REZONING FROM DISTRICT AG TO DISTRICT RP-4 AND CP-2, PRELIMINARY DEVELOPMENT PLAN AND CONCEPTUAL PLAN GENERALLY LOCATED AT THE NORTHWEST CORNER OF NE DOUGLAS ST AND NW COLBERN RD, PROPOSED ARIA AND SUMMIT VILLAGE NORTH DEVELOPMENT IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 33, THE UNIFIED DEVELOPMENT ORDINANCE OF LEE'S SUMMIT CODE OF ORDINANCES, FOR THE FOR THE CITY OF LEE'S SUMMIT, MISSOURI.

WHEREAS, Application #PL2019-075 submitted by Unity Realty, LLC, requesting approval of a rezoning from District AG (Agricultural District) to RP-4 (Planned Apartment Residential District) and CP-2 (Planned Community Commercial District), preliminary development plan and conceptual plan on land generally located at the northwest corner of NE Douglas St and NW Colbern Rd was referred to the Planning Commission to hold a public hearing; and,

WHEREAS, the Unified Development Ordinance provides for the approval of a rezoning, preliminary development plan and conceptual plan by the City following public hearings by the Planning Commission and City Council; and,

WHEREAS, after due public notice in the manner prescribed by law, the Planning Commission held a public hearing for the consideration of the rezoning, preliminary development plan and conceptual plan on May 23, 2019, and rendered a report to the City Council recommending that the rezoning, preliminary development plan and conceptual plan be approved; and,

WHEREAS, after due public notice in the manner prescribed by law, the City Council held a public hearing on July 9, 2019, and rendered a decision to approve the rezoning and preliminary development plan for said property.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF LEE'S SUMMIT, MISSOURI, as follows:

SECTION 1. That a rezoning, preliminary development plan and conceptual plan is hereby approved on the following described property:

Portion to be rezoned to RP-4:

A tract of land in the Northeast Quarter of Section 30 Township 48 North, Range 31 West of the 5th Principal Meridian in Lee's Summit, Jackson County, Missouri being bounded and described as follows: Commencing at the Northwest corner of said Northeast Quarter Section; thence South 01°37'50" West, on the west line of said quarter section, 853.75 feet to a point on the southeast lot line of Lot 1, Lee's Summit Road Pump Station, a subdivision recorded in book I 51 at page 90 in the Jackson County Recorder of Deeds office; thence North 50°41'15" East, on said southeast lot line, 59.22 feet; to the Point of Beginning of the tract of land to be herein described; thence continuing North 50°41'15" East along said line, 116.89 feet; thence North 44°49'51" East, 792.38 feet to a point on the westerly right-of-way line of NW Lee's Summit Road as established by

Document number 2015E0017982 in the Jackson County Recorder of Deeds office; thence on said westerly right-of-way line, South 54°34'12" East, 95.69 feet; thence Southeasterly along a curve to the right being tangent to the last described course with a radius of 1,142.00 feet, a central angle of 56°01'43" and an arc distance of 1,116.74 feet; thence South 01°27'31" West, 322.27 feet; thence leaving said right-of-way line, North 88°32'29" West, 375.74 feet; thence North 01°27'27" East, 62.94 feet; thence South 90°00'00" West, 195.68 feet; thence North 45°00'00" West, 889.18 feet to the Point of Beginning. Containing 979,968 square feet or 22.50 acres, more or less.

Portion to be rezoned to CP-2:

A tract of land in the Northeast Quarter of Section 30 Township 48 North, Range 31 West of the 5th Principal Meridian in Lee's Summit, Jackson County, Missouri being bounded and described as follows: Commencing at the Northwest corner of said Northeast Quarter Section; thence South 01°37'50" West, on the west line of said quarter section, 853.75 feet to a point on the southeast lot line of Lot 1, Lee's Summit Road Pump Station, a subdivision recorded in book I 51 at page 90 in the Jackson County Recorder of Deeds office, and the Point of Beginning of the tract of land to be herein described; thence North 50°41'15" East, on said southeast lot line, 59.22 feet; thence leaving said lot line, South 45°00'00" East, 889.18 feet; thence North 90°00'00" East, 195.68 feet; thence South 01°27'27" West, 62.94 feet; thence South 88°32'29" East, 375.74 feet to a point on the westerly right-of-way line of NE Douglas Street as established by Document number 2015E0017982 in the Jackson County Recorder of Deeds office; thence on said westerly right-of-way line, South 01°27'31" West, 1,114.58 feet; thence South 46°40'17" West, 35.22 feet to the intersection of the northerly right-of-way line of NE Colburn Road as established by a survey provided by Hamilton Sterrett and Dooley, project number 99021; thence on said northerly right-of-way line North 88°06'56" West, 730.85 feet; thence Westerly along a curve to the left having an initial tangent bearing of North 87°33'04" West with a radius of 2,904.93 feet, a central angle of 10°06'02" and an arc distance of 512.10 feet to the Southwest corner of the said Northeast Quarter; thence North 01°37'22" East, on the west line of the northeast quarter of said section 30, a distance of 1,328.05 feet to the northwest corner of the southwest quarter of the said northeast quarter; thence continuing on the said west line, North 01°37'50" East, 473.96 feet to the Point of Beginning. Containing 1,717,197 square feet or 39.42 acres, more or less.

SECTION 2. That the following conditions of approval apply:

1. A modification shall be granted to the required high- and medium-impact landscape buffers along the southwest and northwest portions of the property, to allow an existing off-site, dense-treed area to serve as a buffer.
2. A modification shall be granted to the maximum allowable density of 12 dwelling units per acre in the RP-4 district, to allow 21.55 dwelling units per acre.
3. The Developer shall execute a mutually satisfactory development agreement with the City, which addresses, at a minimum, an off-site landscape easement, road improvements on Lee's Summit Road and possible down stream stormwater improvements. No building permits shall be issued for any structure in the development until written proof is provided to the City that the development agreement has been recorded in the Jackson County Recorder of Deeds Office.
4. Development shall be in accordance with the preliminary development plan date stamped May 7, 2019.
5. Road improvements shall be provided on both sides of the street for the length of the applicant's development.

SECTION 3. Nonseverability. All provisions of this ordinance are so essentially and inseparably connected with, and so dependent upon, each other that no such provision would be enacted without all others. If a court of competent jurisdiction enters a final judgment on the merits that is not subject to appeal and that declares any provision or part of this ordinance void, unconstitutional, or unenforceable, then this ordinance, in its collective entirety, is invalid and shall have no legal effect as of the date of such judgment.

SECTION 4. That failure to comply with all of the provisions contained in this ordinance shall constitute violations of both this ordinance and Chapter 33, the City's Unified Development Ordinance, of the Code of Ordinances for the City of Lee's Summit.

SECTION 5. That this ordinance shall be in full force and effect from and after the date of its passage and adoption, and approval by the Mayor.


PASSED by the City Council of the City of Lee's Summit, Missouri, this 16th day of July, 2019.


Mayor William A. Baird

ATTEST:


City Clerk Trisha Fowler Arcuri

APPROVED by the Mayor of said city this 18th day of July, 2019.



Mayor William A. Baird

ATTEST:



City Clerk *Trisha Fowler Arcuri*

APPROVED AS TO FORM:



City Attorney *Brian Head*



LEE'S SUMMIT MISSOURI

DEVELOPMENT REVIEW FORM TRANSPORTATION IMPACT

DATE: May 14, 2019 CONDUCTED BY: Michael K Park, PE, PTOE
SUBMITTAL DATE: May 7, 2019 PHONE: 816.969.1800
APPLICATION #: PL2019075 EMAIL: Michael.Park@cityofls.net
PROJECT NAME: ARIA/SUMMIT VILLAGE NORTH PROJECT TYPE: Rezoning

SURROUNDING ENVIRONMENT (*Streets, Developments*)

The proposed rezoning with preliminary development plan and conceptual development plan is located at the northwest corner of Douglas Street/Lee's Summit Road and Colbern Road. The surrounding area is generally undeveloped. The Lee's Summit Airport is located east of Douglas Street/Lee's Summit Road in the immediate vicinity of the project. The preliminary development plan is more specifically located near the intersection of Lee's Summit Road and Douglas Road/Douglas Street. The conceptual development plan is located between the preliminary development plan and Colbern Road along the west side of Lee's Summit Road/Douglas Street.

ALLOWABLE ACCESS

The proposed development will be accessed from Lee's Summit Road/Douglas Street (hereafter referred to as Lee's Summit Road where located north of Colbern Road). Portions of the conceptual development will also access Colbern Road. All access along Lee's Summit Road and Colbern Road and the travel ways within the development will be private streets or private driveways. All intersections proposed along Lee's Summit Road and Colbern Road are full access with exception of a driveway limited to right-in/right-out movements along Colbern Road in closest proximity to Douglas Street to comply with the Access Management Code.

EXISTING STREET CHARACTERISTICS (*Lanes, Speed limits, Sight Distance, Medians*)

Colbern Road is a 45 mph two-lane undivided major arterial currently constructed to interim/unimproved road (e.g. rural) standard between Blue Parkway and Douglas Street. East of Douglas Street and West of Blue Parkway, Colbern Road is has an urban design with four or more lanes including medians along some divided roadway sections. The City of Lee's Summit has a planned, and funded, capital improvement project for the interim/unimproved section of Colbern Road between 350 Highway and Douglas Street. Those capital improvements, funded in part by federal aid, will widen Colbern Road to at least 4 lanes, replace the bridge over the UPRR, add curb, street lights, sidewalk, shared-use path, medians, turn lanes, etc. Those capital improvements are currently under design and scheduled for construction in 2021/22. Colbern Road extends east-west across Lee's Summit with a roundabout intersection at Blue Parkway (major arterial) and signal controlled intersections at several other thoroughfares and highway interchanges (e.g. Douglas Street, Todd George Parkway, Blackwell Parkway, Independence Ave., M-291 Highway, 350 Highway, etc.) along the corridor. Lee's Summit Road/Douglas Street (referred to as Douglas Street south of Colbern Road and Lee's Summit Road north of Colbern Road) is a 45 mph major arterial with signal control at the intersection of Colbern Road. Douglas Street south of Colbern Road is an urban multi-lane facility having curbs, lighting, medians, turn lanes, and sidewalk with an interchange at I-470 adjacent to Colbern Road. Lee's Summit Road north of Colbern Road is a three-lane, 45 mph, roadway built to interim road standards (i.e. paved shoulders), but also has lighting and shared-use path along the east side. Lee's Summit Road

includes curbing and the shoulders transition to bike lanes at Kansas City's municipal boundary (about halfway between Strother Road and Douglas Road). Blue Parkway is a four-lane median divided major arterial between I-470 and Colbern Road with a multi-lane roundabout at the intersection of Colbern Road.

Sight distances at the proposed intersections with Lee's Summit Road, especially where located along the curve in Lee's Summit Road near Douglas Road, should be verified to have adequate conditions during the engineering review. Intersections proposed along Colbern Road should be coordinated during the design of Colbern Road by the City of Lee's Summit to ensure adequate spacing and sight distance.

UNIMPROVED ROAD POLICY (*Resolution 16-22*)

CONFORMANT ☐

EXCEPTIONS ☒

Lee's Summit Road and Colbern Road are defined according to the City Council adopted Unimproved Road Policy as built to interim road and unimproved road standards, respectively. Based on the Unimproved Road Policy, development including the land uses proposed on the preliminary development plan and conceptual development plan, are not permitted on interim roads or unimproved roads. Improvement of Lee's Summit Road and Colbern Road to meet the policy defined urban standard (e.g. curbs, sidewalks, typical lane widths, etc.) are required for any non-residential (i.e. single family exclusive) development regardless of traffic volume or impact. The conceptual development will be phased to occur after the completion of City planned improvements to Colbern Road. Thus, the conceptual development, if phased after the Colbern Road improvements, will conform to the policy. The preliminary development plan does not directly access Colbern Road. However, the preliminary development plan does access Lee's Summit Road and does not propose any urban roadway improvement.

The applicant is requesting an exception to this policy be granted by City Council so as to leave Lee's Summit Road in the current interim road condition. The policy would otherwise require the construction of curb along Lee's Summit Road from the City Limit (where Lee's Summit Road is currently improved to urban standards) to Colbern Road if conformance with the policy is desired (the sidewalk and lighting roadway elements already exist). Staff recommends conformance with the adopted policy, but would support a partial improvement approach or phased implementation discussed with the applicant along the corridor in consideration of the conceptual development plan opportunity to complete the road improvements at a future date. A partial or phased approach would include urban road improvements from the south property line of the preliminary development plan to the north City Limit along Lee's Summit Road, both sides; leaving the remainder of improvement to Lee's Summit Road from the preliminary development plan to Colbern Road for future responsibility of adjacent development when the conceptual development plan proceeds.

ACCESS MANAGEMENT CODE

COMPLIANT ☒

WAIVERS ☐

The preliminary development plan and conceptual development plan includes all required turn lanes in compliance with the Access Management Code. The turn lanes would be constructed prior to each associated phase of development.

All driveways and intersections associated with these development plans are properly separated. The internal site circulation, driveway throat depths, sight distance, connectivity and queue storages required by the Access Management Code have also been satisfied.

Access management and required improvements along Douglas Street, Lee's Summit Road and Colbern Road in association with the impact of this development extend beyond the jurisdictional controls of Lee's Summit. Consequently, this proposed development has been coordinated between the developer, Kansas City, MoDOT and Lee's Summit. MoDOT and the City of Kansas City, Missouri have reviewed the traffic impact of this development and its recommendations, roadway improvements, etc. Both agencies have accepted the traffic study, its recommendations and support the related transportation improvements as written in the staff conditions of approval to meet their respective requirements.

TRIP GENERATION

Time Period	Total	In	Out
Weekday	2,616	1,308	1,308
A.M. Peak Hour	160	42	118
P.M. Peak Hour	200	122	78

The trip generation shown above represents the Preliminary Development Plan for the Weekday A.M. and P.M. Peak Hours. Trip generation shown in the subsequent table represents the total traffic anticipated during the Weekday A.M. and P.M. Peak Hours for the Preliminary Development Plan and Conceptual Development Plan combined.

Time Period	Total	In	Out
Weekday	15,422	7,711	7,711
A.M. Peak Hour	1,242	630	614
P.M. Peak Hour	1,258	676	582

TRANSPORTATION IMPACT ANALYSIS

STUDY REQUIRED ☒

STUDY NOT REQUIRED ☐

The proposed preliminary development plan will likely generate more than 100 vehicle trips to the surrounding street system during any given peak hour. The proposed conceptual development plan will generate significant amounts of traffic in addition to the preliminary development plan.

A traffic impact study for the proposed development was completed by Olsson, dated April 24, 2019. The traffic study characterized the preliminary development plan as Phase 1 and conceptual development plan as Phase 2. The various traffic study scenarios included existing conditions, existing plus Phase 1 conditions, and existing plus Full Build (Phase 1 and Phase 2 combined) conditions during the A.M. and P.M. Peak Hours. The study included analysis of existing and proposed intersections along Lee's Summit Road/Douglas Street from Gregory Boulevard/Little Blue Road through the I-470 interchange, including Strother Road and St. Michaels High School/85th Street intersections. The study also included analysis of existing and proposed intersections along Colbern Road from Blue Parkway to Douglas Street.

The study analysis and recommendations for mitigation of the development impact to achieve adequate operations at each of the study intersections for each scenario and peak hour were based on level of service (vehicle delay) and vehicle queuing. Level of service is characterized according to a performance rating from A to F, similar to a school grade card with A representing free flow conditions and F highly congested gridlock or failed operation. It is a standardized measure of acceptable traffic operations. The City Council has adopted a level of service goal C,

but level of service D and below may be acceptable for some stop controlled movements. MoDOT (LOS E) and Kansas City (LOS D) may have different standards of acceptance for level of service than Lee's Summit.

The study results for existing conditions and existing plus Phase 1 (the preliminary development plan) show adequate traffic operations and managed queues for all intersections during both peak hours with exception of the Colbern Road and Douglas Street traffic signal during the P.M. Peak Hour and a few stop controlled movements along Lee's Summit Road . The stop controlled movements that exhibit a level of service (LOS) D or E during the peak hours have an associated vehicle queue less than 50 feet and cannot be mitigated by turn lane improvements (and traffic signals are not warranted for these stop controlled conditions). The intersection of Colbern Road and Douglas Street has an existing LOS D during the P.M. Peak Hour and continues to have a projected LOS D upon impact of Phase 1 during the same time period. Vehicle queues may extend beyond the adjacent interchange ramp intersection, but MoDOT concurs with the study recommendations as existing conditions that do not significantly change as a result of the preliminary development plan. Staff also concurs with the study analysis, reported performance results and improvement recommendations.

The study results for Full Build conditions that include trip generation from the conceptual development plan area exhibit various required left-turn and right-turn lanes at proposed driveways/intersections along Lee's Summit Road and Colbern Road, as well as mitigations at the intersection of Colbern Road and Douglas Street for adequate level of service. The study assumed Colbern Road would be improved as planned by the City between Blue Parkway and Douglas Street prior to Full Build. Those improvements were assumed to include a raised median along Colbern Road to restrict access at Drive 7 to right-in/right-out only. In consideration of these improvements and other mitigations recommended in the traffic study, the intersection level of service at Douglas Street and Colbern Road would meet the City's minimum desired goal (LOS C) for both peak hours.

As previously mentioned, trips generated by the proposed development will directly impact Kansas City and MoDOT roadways and highways. As such, the traffic study was submitted to Kansas City and MoDOT for review. Kansas City and MoDOT have accepted the traffic study, its conclusions and recommendations. There are no additional improvements recommended by Kansas City or MoDOT conveyed to the City for inclusion as conditions of approval consideration.

LIVABLE STREETS (*Resolution 10-17*)

CONFORMANT ☒

EXCEPTIONS ☐

The proposed preliminary development plan and conceptual development plan includes all Livable Streets elements identified in the City's adopted Comprehensive Plan, associated Greenway Master Plan and Bicycle Transportation Plan attachments, and elements otherwise required by ordinances and standards, including but not limited to sidewalk, trails and accessibility. No exceptions to the Livable Streets Policy adopted by Resolution 10-17 have been proposed.

RECOMMENDATION: **APPROVAL** ☒

DENIAL ☐

STIPULATIONS/CONDITIONS ☐

Recommendations for Approval herein refer only to the transportation impact and do not constitute an endorsement of the development from City Staff.

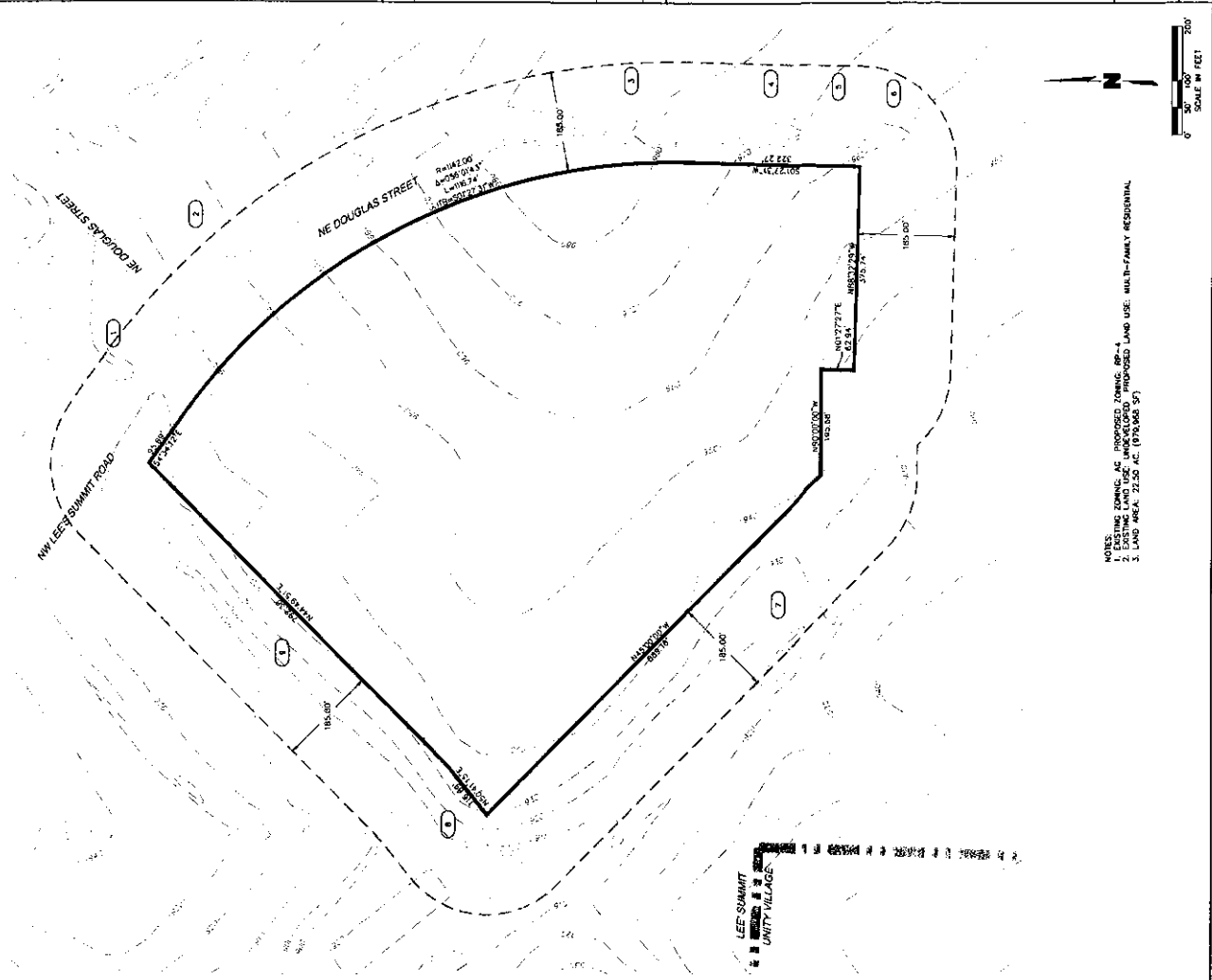
Staff recommends approval of the proposed rezoning and preliminary development plan subject to the following conditions:

1. Re-stripe the existing two-way left-turn lane along Lee's Summit Road for a dedicated northbound left-turn lane with at least 200 feet of storage at the intersection of Drive 1.
2. Re-stripe the existing two-way left-turn lane along Lee's Summit Road for a dedicated northbound left-turn lane with at least 200 feet of storage at the intersection of Drive 2.
3. Construct curb and gutter along Lee's Summit Road from Colbern Road to the north city limits, both sides, in conformance with the Unimproved Road Policy.

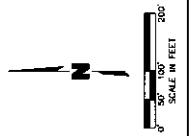
With regards to the Conceptual Development Plan, the Traffic Impact Study shall be updated (or replaced), as necessary, at such time the Conceptual Development Plan is submitted for Preliminary Development Plan consideration. The recommendations in the traffic impact study related to the Conceptual Development Plan (a.k.a. Phase 2/Full Build), including the Colbern Road Improvements planned by the City prior to development of the Conceptual Development Plan area and various turn lanes and warranted traffic controls required by code or to provide adequate traffic operations and safety, will be reviewed at that time and similarly conditioned, as appropriate, upon any related Preliminary Development Plan application approved.

KEY	ADDRESS	OWNERS' & MAILING ADDRESS
1	** NO ADDRESS **	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
2	** NO ADDRESS **	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
3	** NO ADDRESS **	DANMER BROTHERS INVESTMENT LLC LENSA, KS 66214
4	** NO ADDRESS ** (PUBLIC OWNED)	THE CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
5	1915 W DOUGLAS ST LEE'S SUMMIT, MO 64064	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
6	1901 W DOUGLAS ST LEE'S SUMMIT, MO 64064	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
7	** NO ADDRESS **	UNITY SCHOOL OF CHRISTIANITY UNITY WILGAS, MO 64065
8	2300 W LEE'S SUMMIT RD. LEE'S SUMMIT, MO 64064	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
9	** NO ADDRESS **	UNITY SCHOOL OF CHRISTIANITY UNITY WILGAS, MO 64065

LEGAL DESCRIPTION FOR REZONING

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EXISTING CONDITIONS	AREA	REZONING & PRELIMINARY DEVELOPMENT PLAN	J. MO	2019
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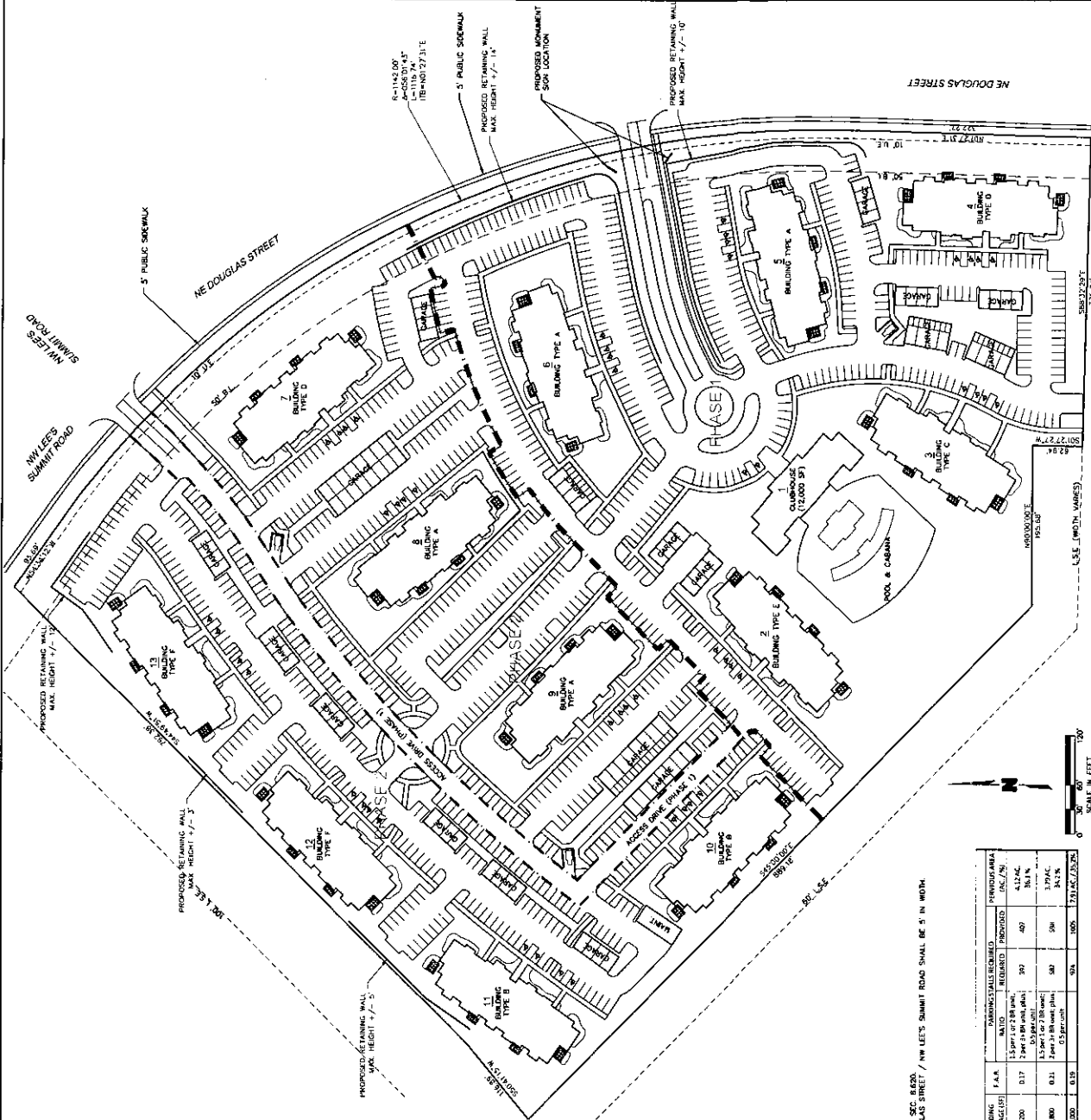
NOTES:

NO.	ADDRESS	PROPERTY OWNERS WITHIN 185'
1	" NO ADDRESS "	DWIGHT D. & MARJORIE ADAMS CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
2	" NO ADDRESS "	CITY OF LEE'S SUMMIT CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
3	" NO ADDRESS "	DANIEL BROTHERS INVESTMENT LLC 1000 N. STATE ST. LEWISVILLE, MO 64071
4	" NO ADDRESS " (PUBLIC CORRIDOR)	THE CITY OF LEE'S SUMMIT 207 N. STATE ST. LEE'S SUMMIT, MO 64063
5	1915 NE DOLGUTH ST	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
6	1001 NE DOLGUTH ST	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063
7	" NO ADDRESS "	UNITY SCHOOL OF CHRISTIANITY UNITY WILDADE, MO 64065
8	2290 NE LEE'S SUMMIT RD	CITY OF LEE'S SUMMIT LEE'S SUMMIT, MO 64063-7460
9	" NO ADDRESS "	UNITY SCHOOL OF CHRISTIANITY UNITY WILDADE, MO 64065

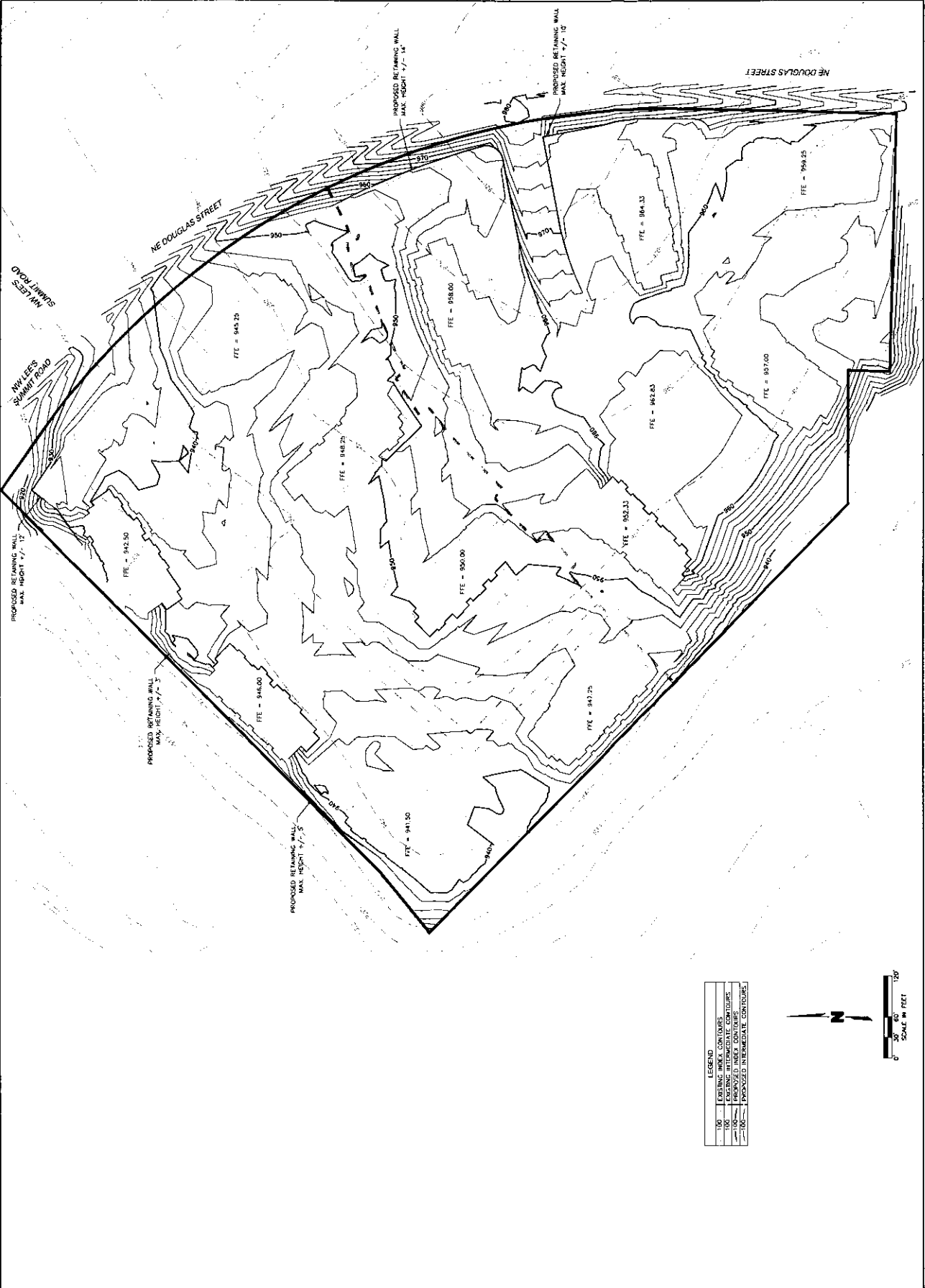
NO.	DATE	REVISIONS
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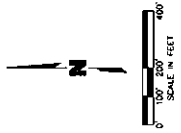
PRELIMINARY PLAT
REZONING & PRELIMINARY DEVELOPMENT PLAN
AREA
LEES SUMMIT, MO
2019

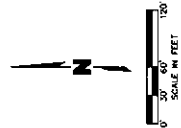
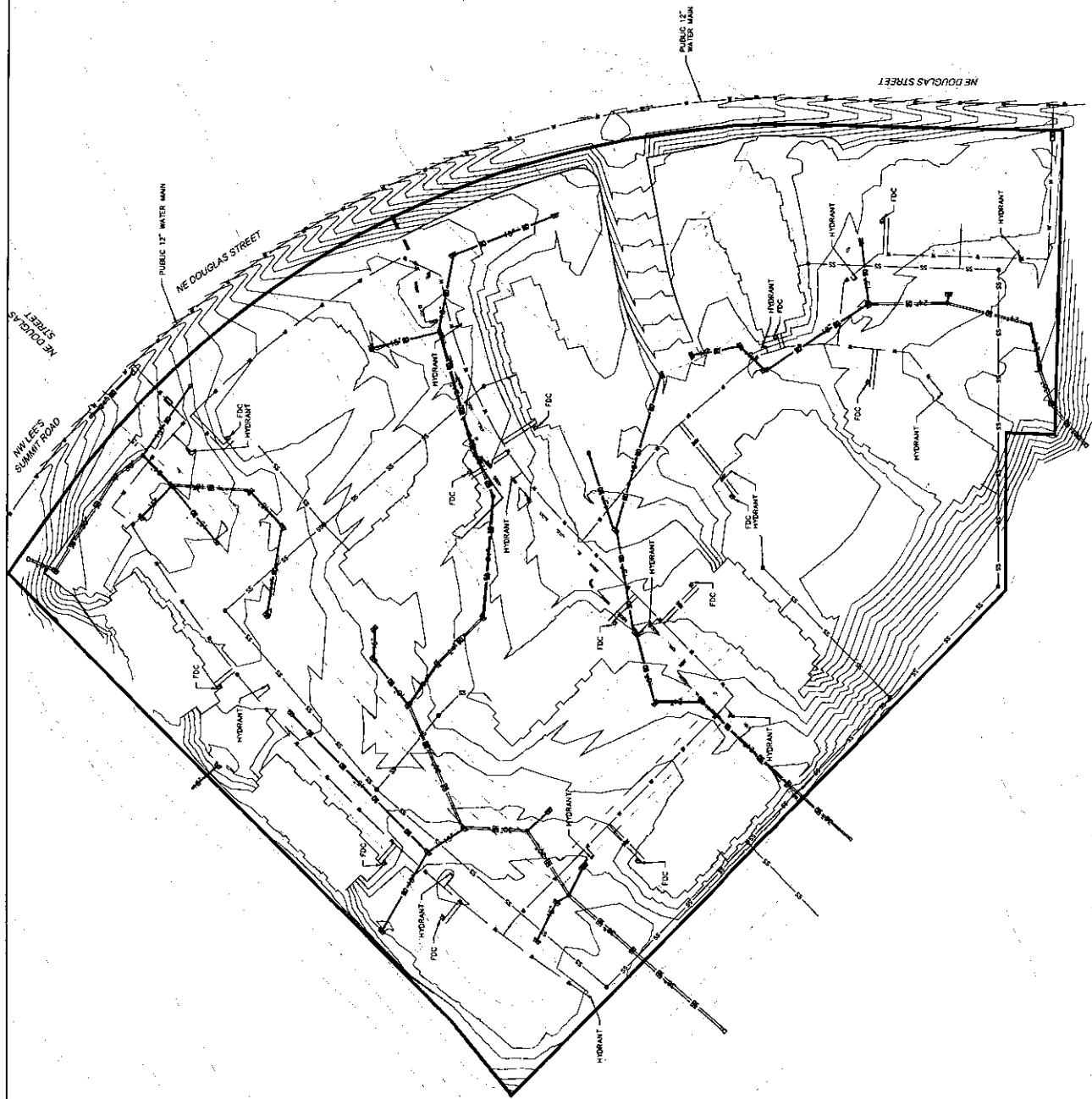
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NET DEVELOPMENT DATA												
PHASE	EXISTING ZONING	PROPOSED ZONING	GROSS AREA (AC)	STREET FRONTAGE (FT)	NET AREA (AC)	LAND USE	GROSS FLOOR AREA (SF)	MAXIMUM COVERAGE (%)	F.A.R.	PERMANENTLY TAKEN		PERMANENTLY TAKEN (%)
										REQUIRED	PROPOSED	
1	AG	RM-4	11.41	0	11.41	MULTI-FAMILY RESIDENTIAL	240,000	84.200	0.17	2.00	1.00	43.24%
2	4C	RM-4	11.37	0	11.01	MULTI-FAMILY RESIDENTIAL	84,500	122.800	0.31	5.00	0.00	37.94%
TOTAL			22.78	0	22.42		324,500	197.000	0.19	7.00	1.00	73.44%
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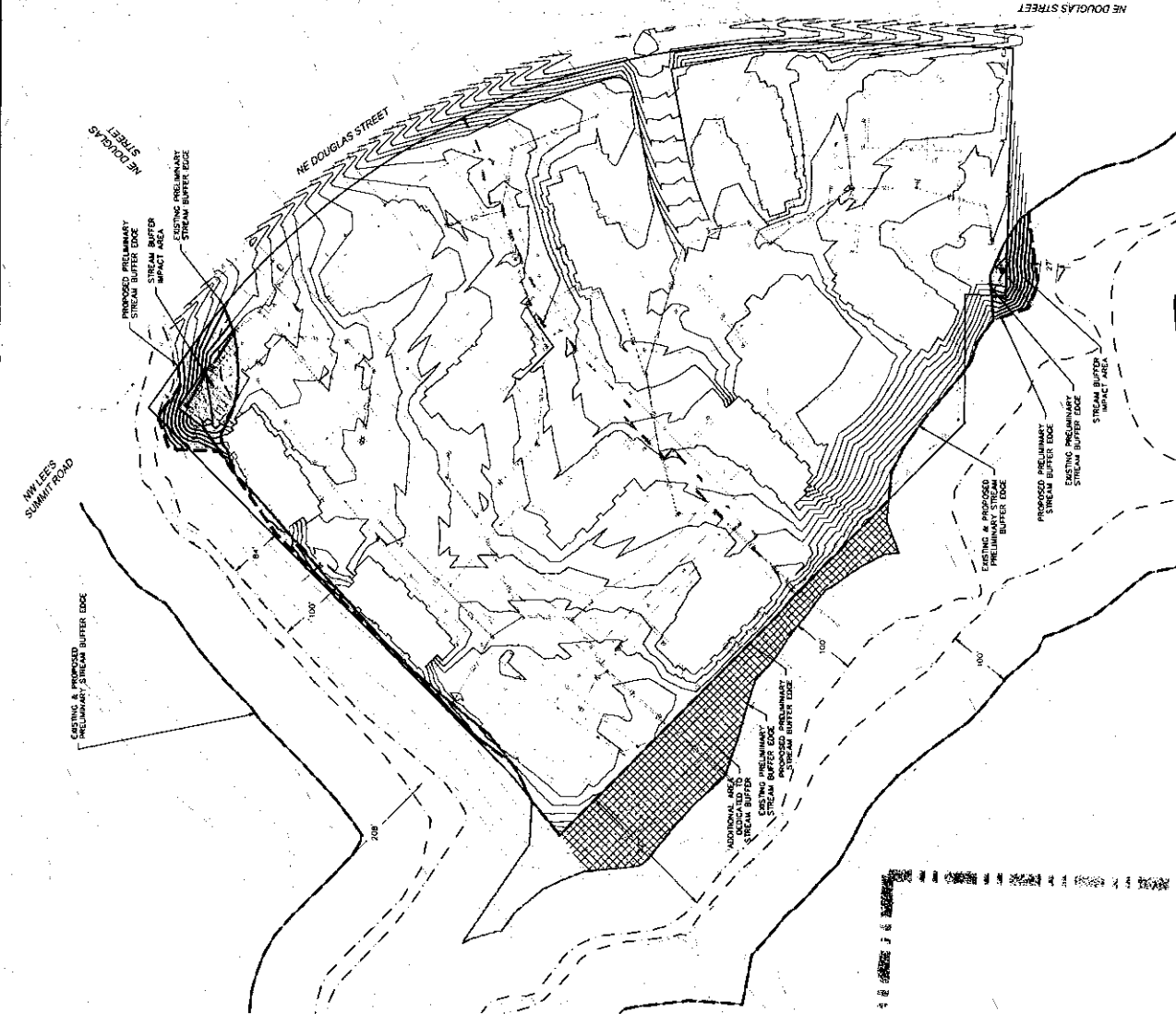
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100	PROPOSED INTERMEDIATE CONTOURS

LEGEND	
EXISTING SANITARY SEWER	
PROPOSED SANITARY SEWER	
EXISTING STORM SEWER	
PROPOSED STORM SEWER	
EXISTING WATER LINE	

PROPOSED PRIVATE UTILITY SIZES:
WATER: 10"
SEWER: 8"
SANITARY: 8"
STORM: SEE PLAN
ALL PROPOSED UTILITIES SHALL BE PRIVATE EXCEPT FOR WATER MAIN WITHIN PUBLIC RIGHT-OF-WAY

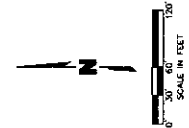
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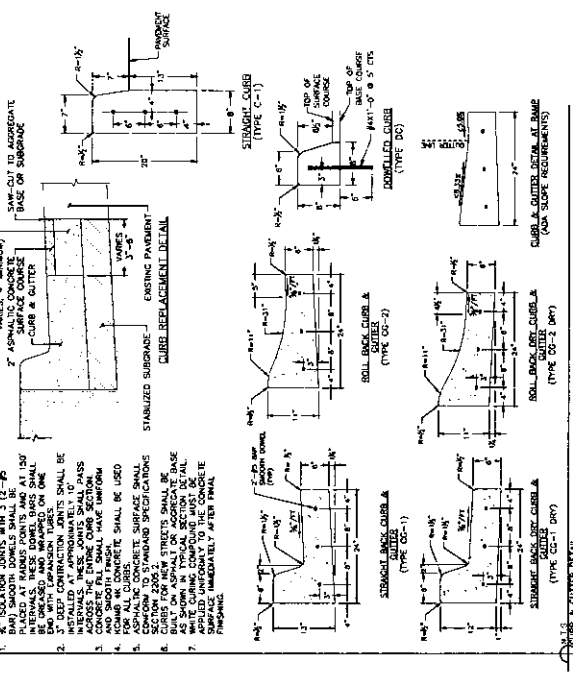
LEES SUMMIT, MO	2019
PRELIMINARY STREAM BUFFER PLAN	AREA
REZONING & PRELIMINARY DEVELOPMENT PLAN	
SHEET	C7.0



STREAM BUFFER AREAS	
STREAM BUFFER IMPACT AREA (0.70 AC)	1.19 AC
ADDITIONAL AREA DEBATED TO STREAM BUFFER	0.43 AC
NET AREA	0.43 AC
STREAM BUFFER DISTANCES DETERMINED VIA AC METRO APWA STANDARDS.	

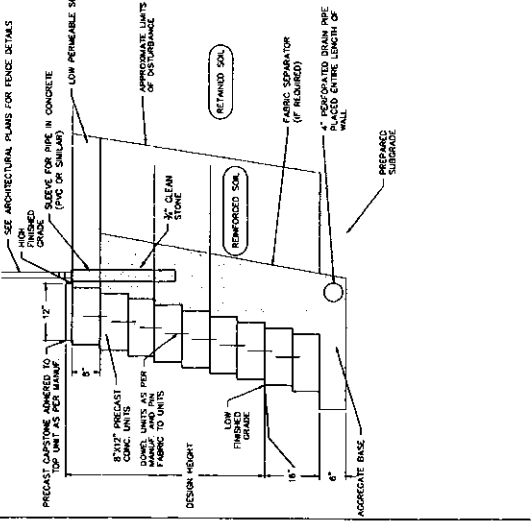
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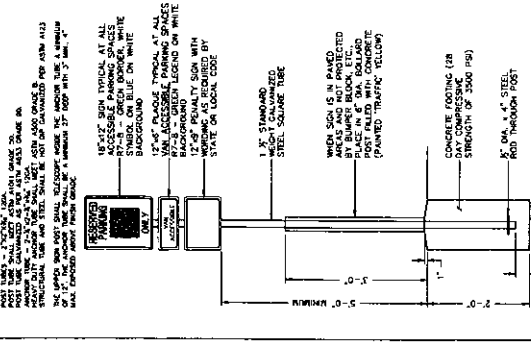


RETAINING WALL NOTES

1. THE DETAILS PROVIDED HEREIN ARE FOR GENERAL GUIDANCE ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COMPLETE MAKE-UP OF THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL SUBMIT PRODUCT DATA FOR THE CONTRACTOR'S CHOICE OF MATERIALS FOR APPROVAL BY OWNER AND ARCHITECT.
2. THE RETAINING WALL SHALL BE A VESSELO-LOC MEDIUM SANDSTONE BLIND.
3. THE WALL SHALL BE DESIGNED BY THE INSTALLER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN. CURRENTLY, THE DESIGN SHALL BE SUBMITTED TO THE ARCHITECT FOR REVIEW AND APPROVAL. THE DESIGN SHALL INCLUDE DESIGN CALCULATIONS AND DESIGN ORDERS (ANGLE OF REPOSE, SOIL WEIGHT, ETC.) SHALL BE SUBMITTED TO THE ARCHITECT FOR REVIEW AND APPROVAL. THE DESIGN SHALL BE QUALIFIED PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF MISSOURI.
4. FACTORS OF SAFETY SHALL BE AS FOLLOWS:
 - 1.5 AGAINST OVERSIGHT OR PULLOUT ON HORIZONTAL SURFACES
 - 2.0 AGAINST OVERSIGHT OR PULLOUT ON VERTICAL SURFACES
 - 2.0 AGAINST OVERTURNING
5. THE DESIGN DIMENSIONS AND MATERIALS SHOWN IN THE DETAILS ARE GENERAL IN NATURE. THE AGGREGATE MATERIALS SHALL BE AS SHOWN IN THE DETAILS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY OF THE MATERIALS AND THE QUALITY OF THE CONSTRUCTION.
6. SEE SPECIFICATIONS FOR MATERIAL SELECTION AND OTHER REQUIREMENTS.
7. RETAINING WALL SHALL INCLUDE GLOBAL STABILITY.
8. RETAINING WALL SHALL PROVIDE POSITIVE INTERLOCKING BETWEEN BLOCKS AND GROUND.
9. WALL TO MEET ALL DESIGN AND INSPECTION REQUIREMENTS OF LATE'S STANDARD, MISSOURI.
10. GLOBAL STABILITY ANALYSIS SHOULD BE PERFORMED BY THE CONTRACTOR. THE ANALYSIS SHALL BE SUBMITTED TO THE ARCHITECT FOR REVIEW AND APPROVAL. THE ANALYSIS SHALL INCLUDE ALL EXTERNAL LOADS, INCLUDING THE WEIGHT OF THE WALL AND HYDROSTATIC PRESSURE, WHEN APPLICABLE.
11. RETAINING WALLS 4' AND GREATER IN HEIGHT SHALL BE DESIGNED BY A LICENSED PROFESSIONAL ENGINEER.
12. HANDRAIL OR FENCING IS REQUIRED AT THE TOP OF WALL FOR ANY WALL IN HEIGHT OF 30" OR MORE.



CD-15
S/RETAINING WALL DETAILS



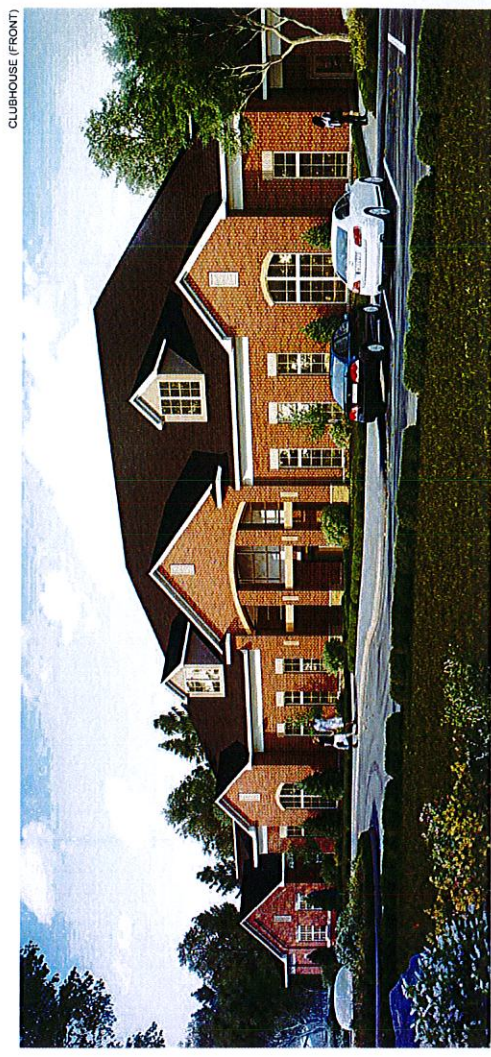
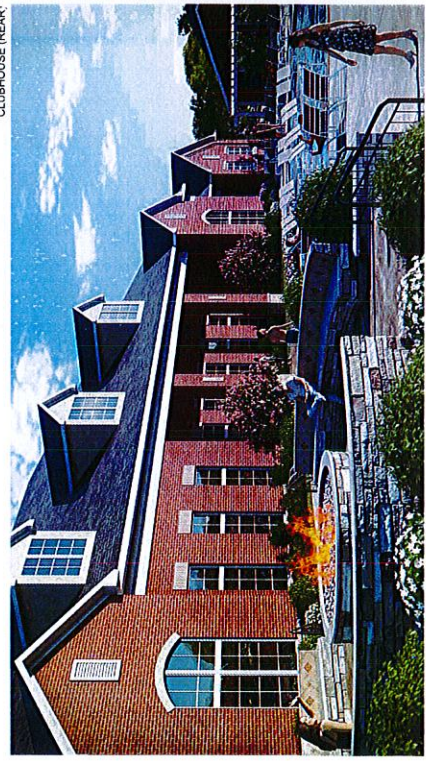
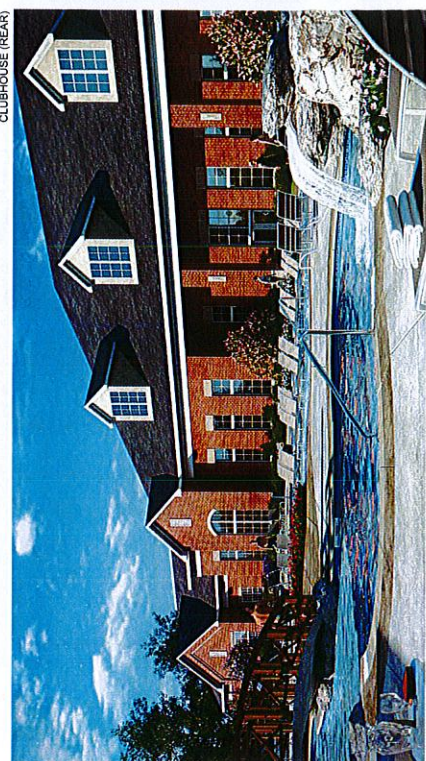
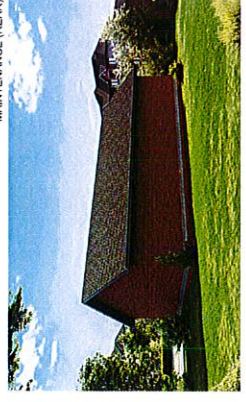
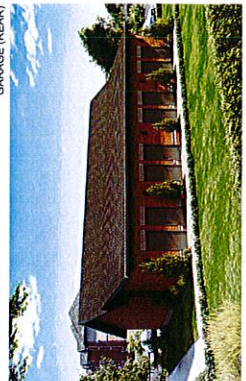
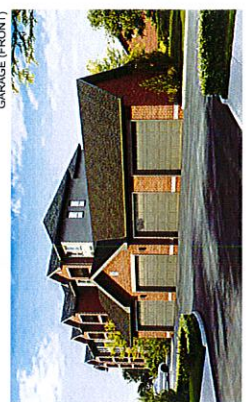
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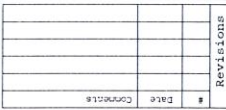
CONCEPT PLANT SCHEDULE

(21)	BUFFET TREE Deciduous. Bldg. 3' Cu Ht. 10' Sp. 10' Col. Evergreen. INB. 8' Ht.
(174)	OPEN YARD AREA TREE Deciduous. Bldg. 3' Cu Ht. 10' Sp. 10' Col. Evergreen. Bldg. 6' Ht.
(49)	STREET TREE Deciduous. Bldg. 3' Cu

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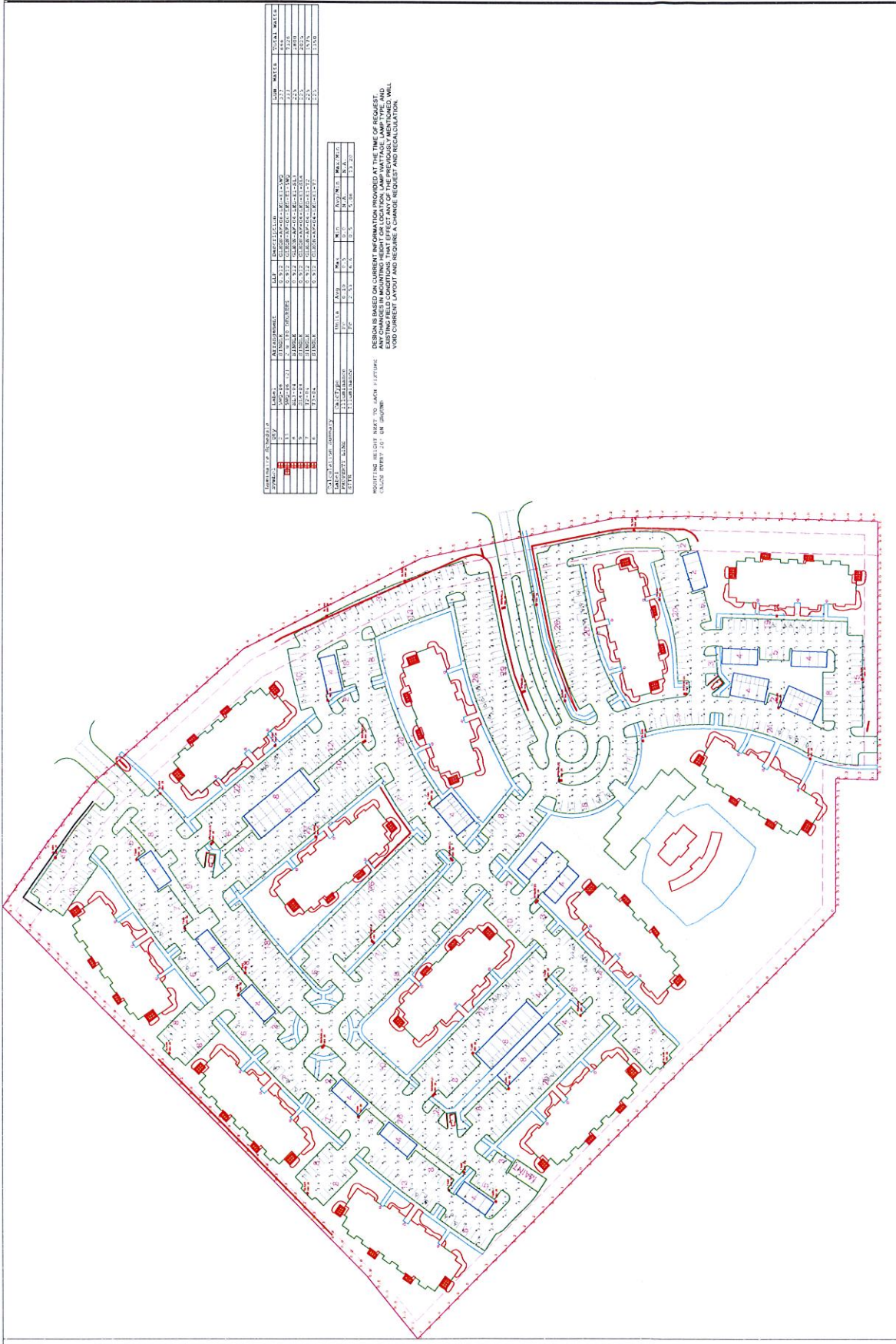
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Drawn By: BW	Checked By:	Date: 2/14/2019	Scale: None

Lighting Study

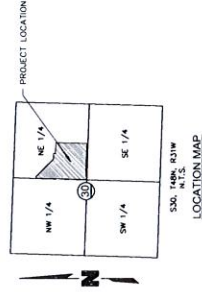


uoss|o

SHEET INDEX	
NUMBER	TITLE
C1.0	TITLE SHEET
C2.0	REZONING PLAN

NOTES:

1. PLAN LAYOUT & DESIGN SHOWN IS FOR CONCEPTUAL USE ONLY. PRELIMINARY AND FINAL DEVELOPMENT PLANS SHALL BE EXECUTED AT A FUTURE DATE.
2. EXISTING ZONING: AC
3. PROPOSED ZONING: CP-2
4. LAND AREA: 39.42 AC (1,717,197 SF)

[illegible][illegible]

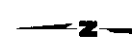
2019		

SUMMIT VILLAGE NORTH
REZONING WITH CONCEPTUAL PLAN

TELEPHONE NO.

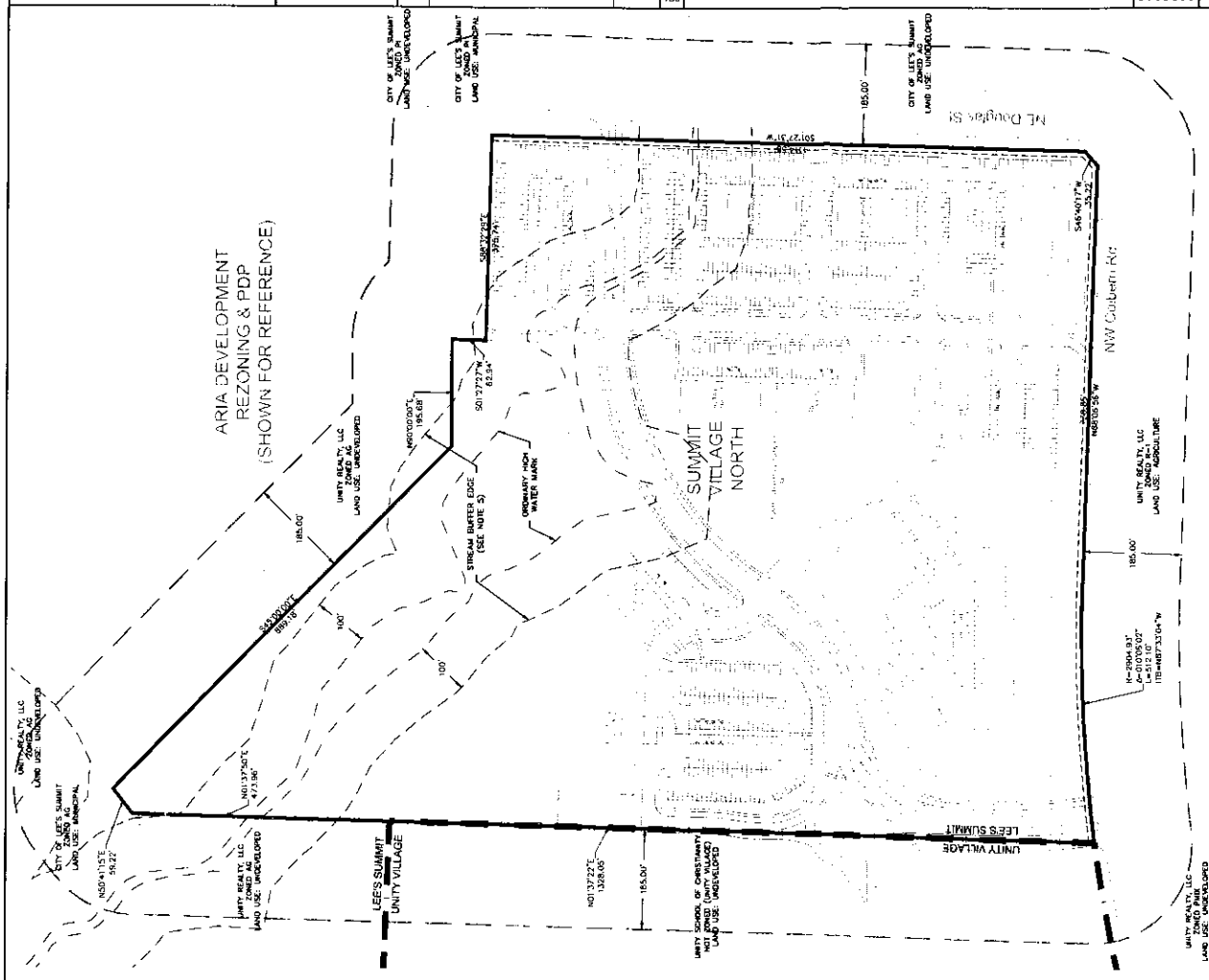
SHEET
C1.0

A list of 100 of the most important species of the Sacramento-San Joaquin River watershed is given in Table 1. The Sacramento-San Joaquin River watershed is a large, complex, and diverse system. It is the largest river system in California, and it is one of the most important in the United States. The watershed covers an area of over 100,000 square miles, and it is home to a wide variety of plants and animals. The Sacramento-San Joaquin River watershed is a vital part of California's economy and environment. It provides water for agriculture, industry, and domestic use. It also provides a habitat for many species of fish and wildlife. The Sacramento-San Joaquin River watershed is a complex and diverse system, and it is one of the most important in the United States.

[illegible]

50' 100'
SCALE IN FEET

ARIA DEVELOPMENT
REZONING & PDP
(SHOWN FOR REFERENCE)



REVISIONS	
NO.	DATE
1	2019.03.06
1	Revised per final comments
BY	REVISIONS DESCRIPTION

SUMMIT VILLAGE NORTH
REZONING WITH CONCEPTUAL PLAN

1770-1782 1783-1795 1796-1808 1809-1821 1822-1834 1835-1847 1848-1860 1861-1873 1874-1886 1887-1899 1900-1912 1913-1925 1926-1938 1939-1951 1952-1964 1965-1977 1978-1990 1991-2003 2004-2016 2017-2029 2030-2042 2043-2055 2056-2068 2069-2081 2082-2094 2095-2107 2108-2120 2121-2133 2134-2146 2147-2159 2160-2172 2173-2185 2186-2198 2199-2211 2212-2224 2225-2237 2238-2250 2251-2263 2264-2276 2277-2289 2290-2302 2303-2315 2316-2328 2329-2341 2342-2354 2355-2367 2368-2380 2381-2393 2394-2406 2407-2419 2420-2432 2433-2445 2446-2458 2459-2471 2472-2484 2485-2497 2498-2510 2511-2523 2524-2536 2537-2549 2550-2562 2563-2575 2576-2588 2589-2601 2602-2614 2615-2627 2628-2640 2641-2653 2654-2666 2667-2679 2680-2692 2693-2705 2706-2718 2719-2731 2732-2744 2745-2757 2758-2770 2771-2783 2784-2796 2797-2809 2810-2822 2823-2835 2836-2848 2849-2861 2862-2874 2875-2887 2888-2900 2901-2913 2914-2926 2927-2939 2940-2952 2953-2965 2966-2978 2979-2991 2992-3004 3005-3017 3018-3030 3031-3043 3044-3056 3057-3069 3070-3082 3083-3095 3096-3108 3109-3121 3122-3134 3135-3147 3148-3160 3161-3173 3174-3186 3187-3199 3200-3212 3213-3225 3226-3238 3239-3251 3252-3264 3265-3277 3278-3290 3291-3303 3304-3316 3317-3329 3330-3342 3343-3355 3356-3368 3369-3381 3382-3394 3395-3407 3408-3420 3421-3433 3434-3446 3447-3459 3460-3472 3473-3485 3486-3498 3499-3511 3512-3524 3525-3537 3538-3550 3551-3563 3564-3576 3577-3589 3590-3602 3603-3615 3616-3628 3629-3641 3642-3654 3655-3667 3668-3680 3681-3693 3694-3706 3707-3719 3720-3732 3733-3745 3746-3758 3759-3771 3772-3784 3785-3797 3798-3810 3811-3823 3824-3836 3837-3849 3850-3862 3863-3875 3876-3888 3889-3901 3902-3914 3915-3927 3928-3940 3941-3953 3954-3966 3967-3979 3980-3992 3993-4005 4006-4018 4019-4031 4032-4044 4045-4057 4058-4070 4071-4083 4084-4096 4097-4109 4110-4122 4123-4135 4136-4148 4149-4161 4162-4174 4175-4187 4188-4200 4201-4213 4214-4226 4227-4239 4240-4252 4253-4265 4266-4278 4279-4291 4292-4304 4305-4317 4318-4330 4331-4343 4344-4356 4357-4369 4370-4382 4383-4395 4396-4408 4409-4421 4422-4434 4435-4447 4448-4460 4461-4473 4474-4486 4487-4499 4500-4512 4513-4525 4526-4538 4539-4551 4552-4564 4565-4577 4578-4590 4591-4603 4604-4616 4617-4629 4630-4642 4643-4655 4656-4668 4669-4681 4682-4694 4695-4707 4708-4720 4721-4733 4734-4746 4747-4759 4760-4772 4773-4785 4786-4798 4799-4811 4812-4824 4825-4837 4838-4850 4851-4863 4864-4876 4877-4889 4890-4902 4903-4915 4916-4927 4928-4940 4941-4953 4954-4966 4967-4979 4980-4992 4993-5005 5006-5018 5019-5031 5032-5044 5045-5057 5058-5070 5071-5083 5084-5096 5097-5109 5110-5122 5123-5135 5136-5148 5149-5161 5162-5174 5175-5187 5188-5200 5201-5213 5214-5226 5227-5239 5240-5252 5253-5265 5266-5278 5279-5291 5292-5304 5305-5317 5318-5330 5331-5343 5344-5356 5357-5369 5370-5382 5383-5395 5396-5408 5409-5421 5422-5434 5435-5447 5448-5460 5461-5473 5474-5486 5487-5499 5500-5512 5513-5525 5526-5538 5539-5551 5552-556

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**PL2019-075 PRELIM DEV PLAN & CONCEPTUAL PLAN
ARIA & SUMMIT VILLAGE NORTH
NW CORNER OF NE DOUGLAS ST AND NW COLBERN RD**

