

# CONSTRUCTION PLANS FOR NW PRYOR ROAD SIGNAL IMPROVEMENTS & LANE WIDENING STREETS OF WEST PRYOR LEE'S SUMMIT, MISSOURI

JULY 2019

ELECTRIC- SERVICE  
KCP&L  
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COMMUNICATION SERVICE  
GOOGLE FIBER  
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KC-Google-UC@google.com  
rebeccadavis@google.com



CALL BEFORE YOU  
DIG - DRILL - BLAST  
1-800-344-7483  
(TOLL FREE)

MISSOURI ONE CALL SYSTEM, INC.

## UTILITY STATEMENT:

THE UNDERGROUND UTILITIES SHOWN HEREON ARE FROM FIELD SURVEY INFORMATION OF ONE-CALL LOCATED UTILITIES, FIELD SURVEY INFORMATION OF ABOVE GROUND OBSERVABLE EVIDENCE, AND/OR THE SCALING AND PLOTTING OF EXISTING UTILITY MAPS AND DRAWINGS AVAILABLE TO THE SURVEYOR AT THE TIME OF SURVEY. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. FURTHERMORE, THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES BY EXCAVATION UNLESS OTHERWISE NOTED ON THIS SURVEY.

## CAUTION - NOTICE TO CONTRACTOR

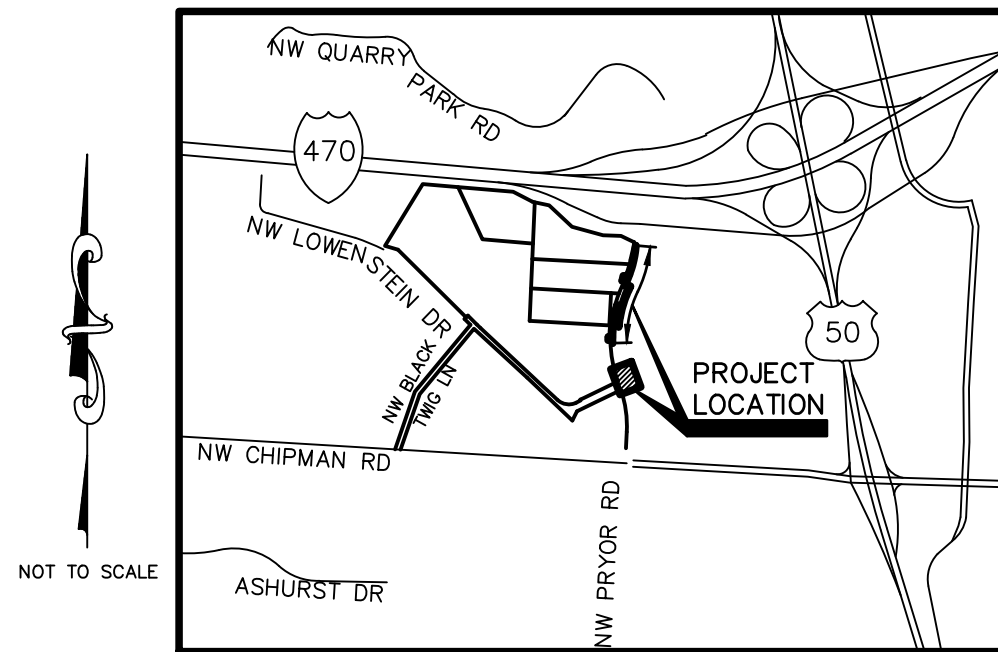
THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXPOSE EXISTING UTILITIES AT LOCATIONS OF POSSIBLE CONFLICTS PRIOR TO ANY CONSTRUCTION.

## SAFETY NOTICE TO CONTRACTOR

IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

## WARRANTY / DISCLAIMER

THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER KAW VALLEY ENGINEERING, INC NOR ITS PERSONNEL CAN OR DO WARRANTY THESE DESIGNS OR PLANS AS CONSTRUCTED, EXCEPT IN THE SPECIFIC CASES WHERE KAW VALLEY ENGINEERING PERSONNEL INSPECT AND



LOCATION MAP  
CITY OF LEE'S SUMMIT, MISSOURI

## INDEX TO SHEETS

- TITLE SHEET
- TYPICAL SECTIONS & GENERAL CONSTRUCTION NOTES
- GENERAL LAYOUT SHEET
- DEMOLITION PLAN
- DEMOLITION PLAN
- NW PRYOR DRIVE WIDENING - PLAN & PROFILE
- NW PRYOR DRIVE WIDENING - PLAN & PROFILE
- NW PRYOR DRIVE WIDENING - PLAN & PROFILE
- DRIVE AT PROPOSED PRIVATE ROAD - A - PLAN & PROFILE
- GEOMETRIC LAYOUT - NW PRYOR ROAD AT NW SUMMIT WOODS
- GEOMETRIC LAYOUT - NW PRYOR ROAD AT PROPOSED PRIVATE ROAD - C
- ADA RAMPS
- GRADING PLAN
- EROSION CONTROL PLAN
- NW PRYOR ROAD PAVEMENT MARKING PLAN
- NW PRYOR ROAD PAVEMENT MARKING PLAN
- TRAFFIC SIGNAL GENERAL NOTES, SIGNS & CONSTRUCTION SEQUENCING
- SHEET NOT USED
- TRAFFIC SIGNAL PLAN - NW PRYOR RD & NW LOWENSTEIN DR
- TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD & NW LOWENSTEIN DR
- TRAFFIC SIGNAL QUANTITIES - NW PRYOR RD & NW LOWENSTEIN DR
- TRAFFIC SIGNAL PLAN - NW PRYOR RD & NW SUMMIT WOODS
- TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD & NW SUMMIT WOODS
- TRAFFIC SIGNAL QUANTITIES - NW PRYOR RD & NW SUMMIT WOODS
- TRAFFIC INTERCONNECT PLAN, NOTES AND WIRING
- SHEET NOT USED
- PAVEMENT MARKING DETAIL SHEET
- SIGN AND TRAFFIC CONTROL DETAIL SHEET
- STREET DETAIL SHEET
- STREET DETAIL SHEET
- TRAFFIC SIGNAL STANDARD CITY DETAILS
- TRAFFIC SIGNAL STANDARD CITY DETAILS
- TRAFFIC SIGNAL STANDARD CITY DETAILS
- EROSION CONTROL DETAIL SHEET
- EROSION CONTROL DETAIL SHEET
- EROSION CONTROL DETAIL SHEET
- NW PRYOR ROAD - CROSS-SECTIONS
- NW PRYOR ROAD - CROSS-SECTIONS

**OWNER:**  
STREETS OF WEST PRYOR,  
7200 WEST 132ND STREET  
OVERLAND PARK, KS 66213  
CONTACT: MATT PENNINGTON  
email: matt@drakekc.com

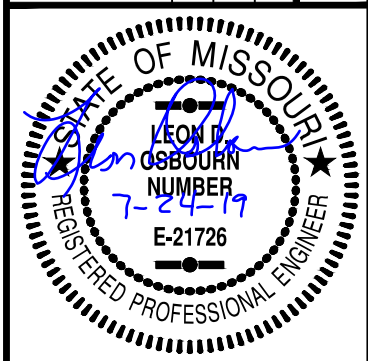
**DEVELOPER:**  
STREETS OF WEST PRYOR, LLC  
7200 WEST 132ND STREET  
OVERLAND PARK, KS 66213  
AGENT: DAVID N. OLSON  
email: daveolson@monarchprojectllc.com

**PREPARED BY:**  
KAW VALLEY ENGINEERING, INC.  
2319 N. JACKSON  
JUNCTION CITY, KS 66441  
785-762-5040  
CONTACT: LEON D OSBOURN  
EMAIL: ldo@kveng.com

**DATUM BENCHMARK:**  
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING OPUS PROJECTS ON PROJECT CONTROL.

**BENCHMARKS:**  
BM #1: CHISELED "SQUARE" ON TOP OF CURB POINT OF INTERSECTION OF WEST PARK PARKING LOT AT EAST DRIVE ENTRANCE.  
BM #2: CHISELED "SQUARE" ON NORTHWEST CORNER AREA INLET 25'± EAST OF CURB LINE AND ON-LINE WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90° BEND IN ROAD.

REV	DATE	DESCRIPTION
2	7-24-19	REVISED PER CITY COMMENTS
1	7-2-19	REVISED PER CITY COMMENTS
0	5-23-19	INITIAL ISSUE
DSN		
DWN		
CHK		



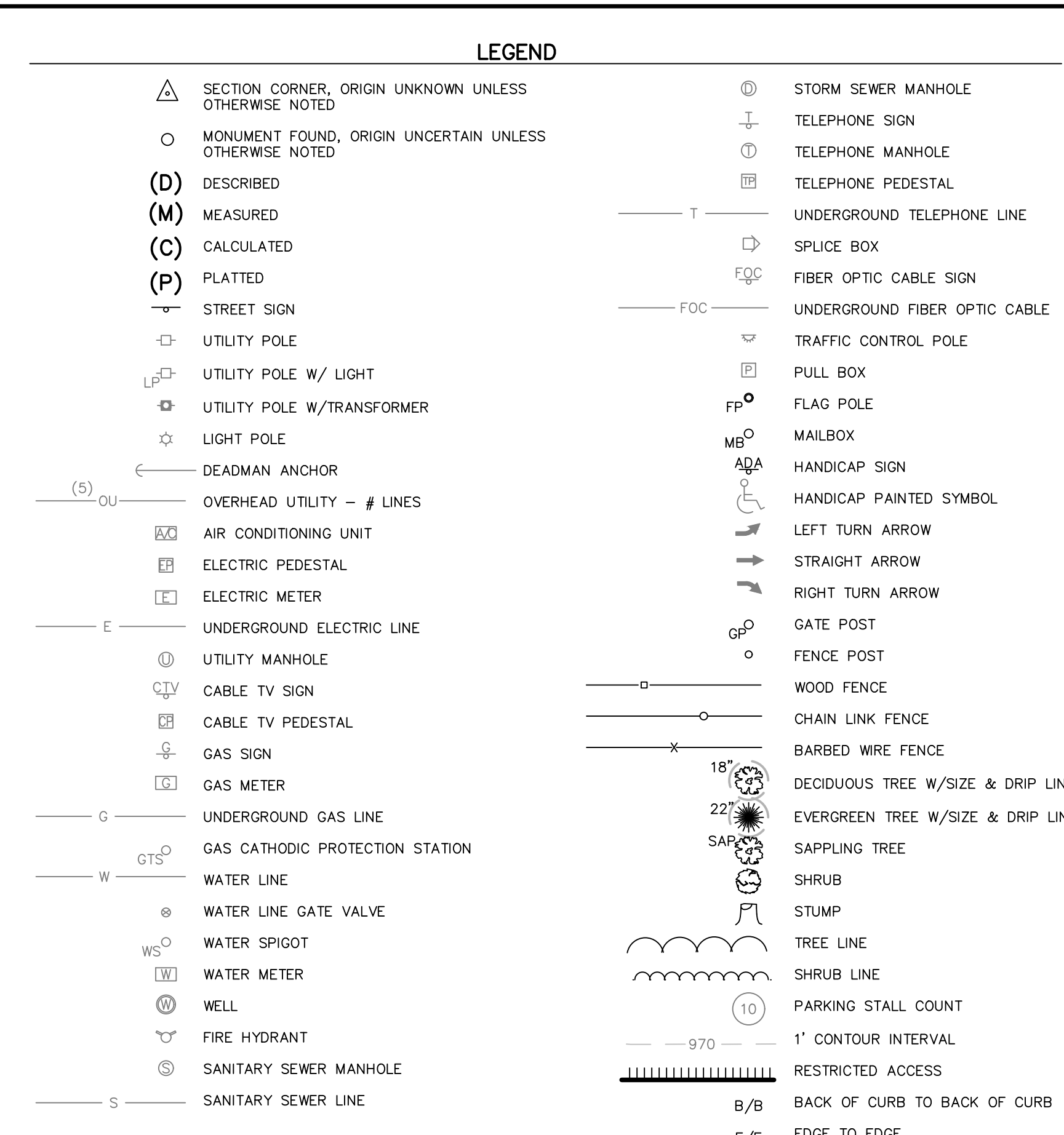
LEON D. OSBOURN  
ENGINEER  
MO # 021726

2319 N. JACKSON | P.O. BOX 1304  
JUNCTION CITY, KANSAS 66441  
PH. (785) 762-5040 | FAX (785) 762-7744  
jdo@kveng.com | www.kveng.com  
**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19.

**STREETS OF WEST PRYOR**  
**NWQ NW PRYOR RD & NW LOWENSTEIN DR**  
**LEE'S SUMMIT, MISSOURI**  
**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING**  
**TITLE SHEET**

PROJ. NO.	A14-7067-1
DESIGNER	LDO
DRAWN BY	JT/BKR
CFN	7067-1SIG-TS
SHEET	1
REV	2





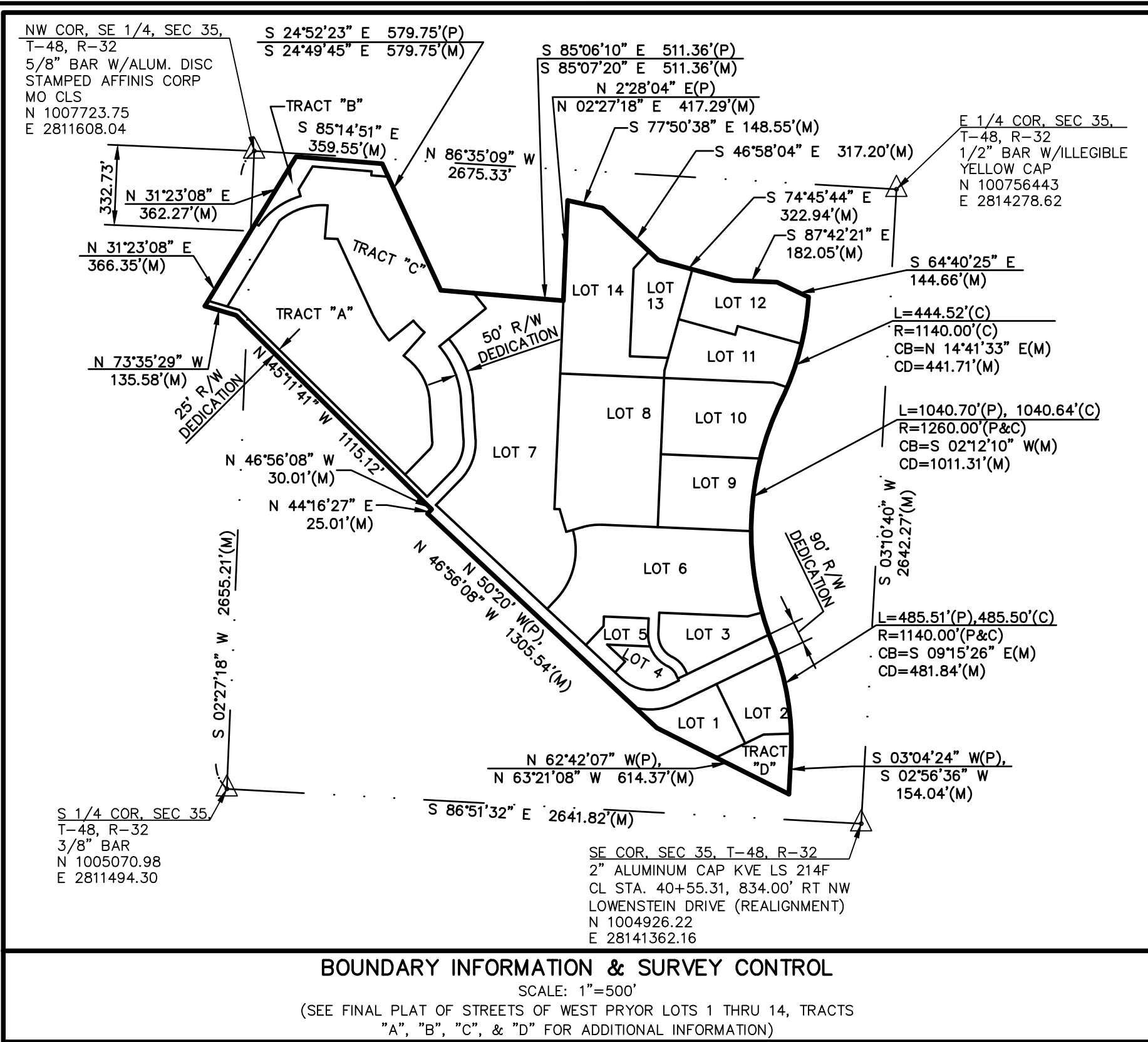
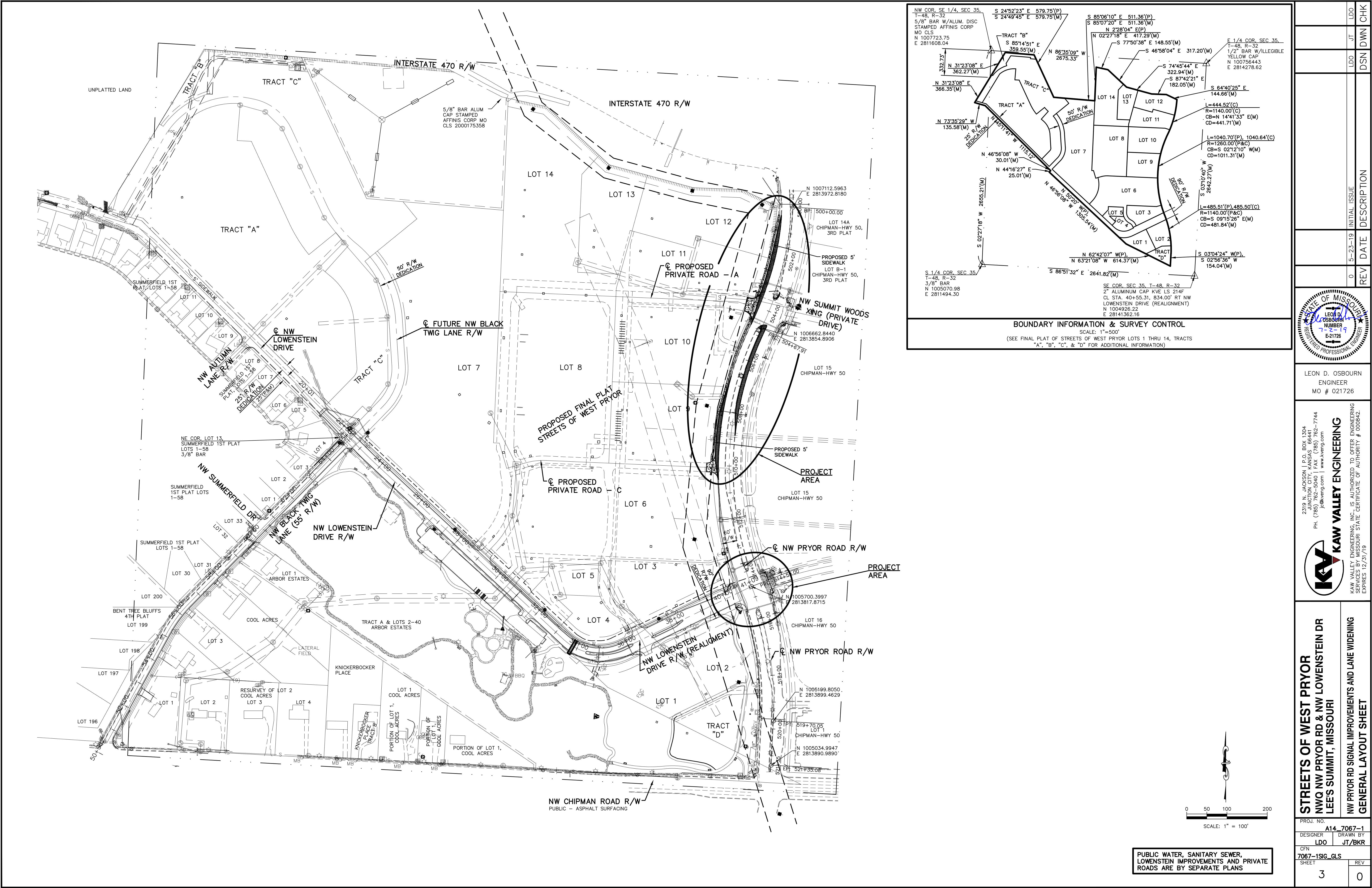
Technical drawing of a 16"X64" sign. The sign is rectangular with a 1 1/2" R corner. It features the text "PRYOR" in large, bold, black letters. To the left of "PRYOR" is the text "NW" and to the right is "RD". The sign is mounted on a 3/4" thick base. Dimensions include a total width of 64", a total height of 16", and a mounting bracket height of 5". The sign is labeled "D3-1(A) 16\"X64\"".

5" AND 10" SERIES "C", UPPER CASE (NO LOWER CASE) WITH SILVER WHITE, HIGH INTENSITY LETTERS AND 3/4" BORDER ON GREEN, HIGH INTENSITY BACKGROUND

D3-1(A)  
16"X64"

THIS DRAWING SHALL NOT BE UTILIZED BY ANY PERSON, FIRM, OR CORPORATION IN WHOLE OR IN PART WITHOUT THE SPECIFIC PERMISSION OF KAW VALLEY ENGINEERING, INC.





REV	DATE	DESCRIPTION	INITIALS	STATUS
0	5-23-19	INITIAL ISSUE	LDO	CHK
1			JT	DWN

LEON D. OSBOURN  
ENGINEER  
MO # 021726

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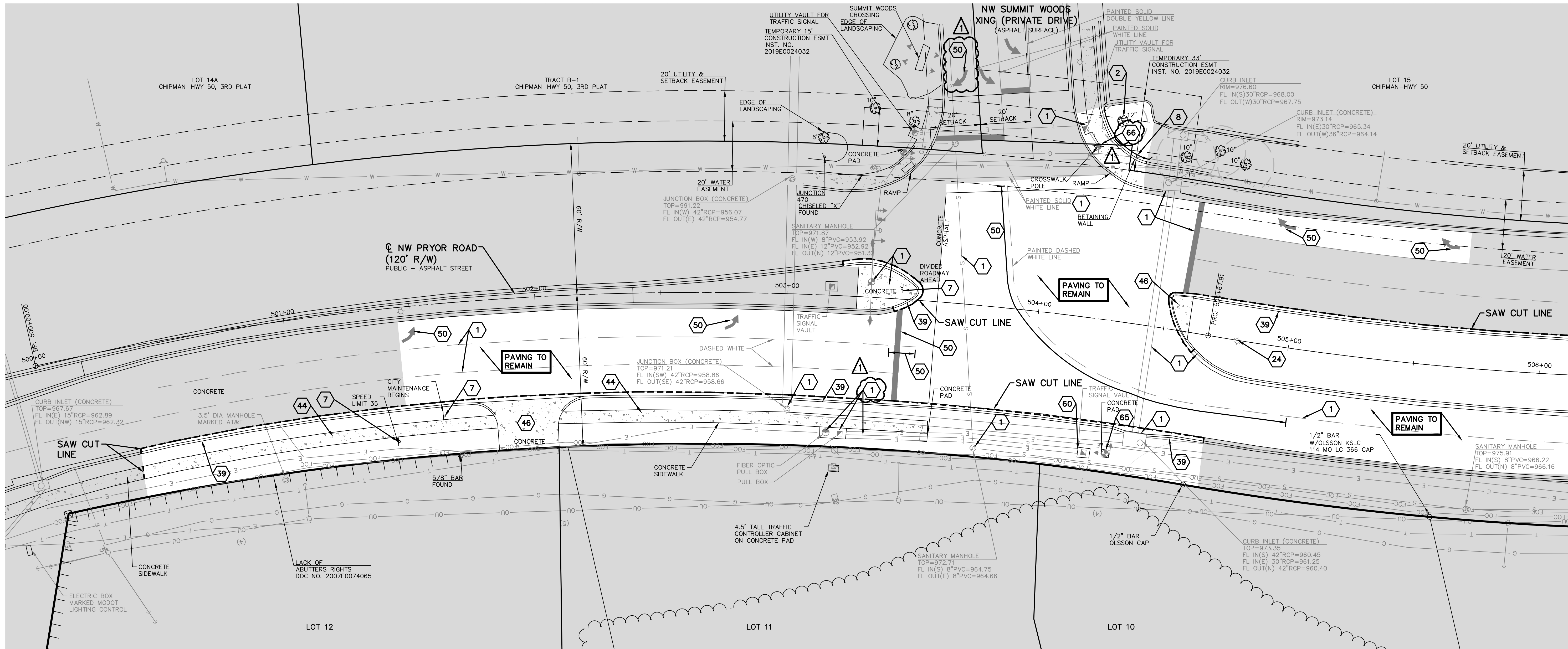
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**STREETS OF WEST PRYOR**  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING**  
GENERAL LAYOUT SHEET

PROJ. NO.	A14-7067-1
DESIGNER	LDO
DRAWN BY	JT/BKR
CFN	7067-1SIG_GLS
SHEET	3
REV	0





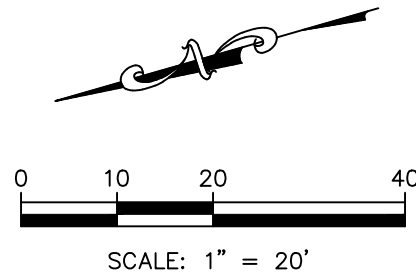
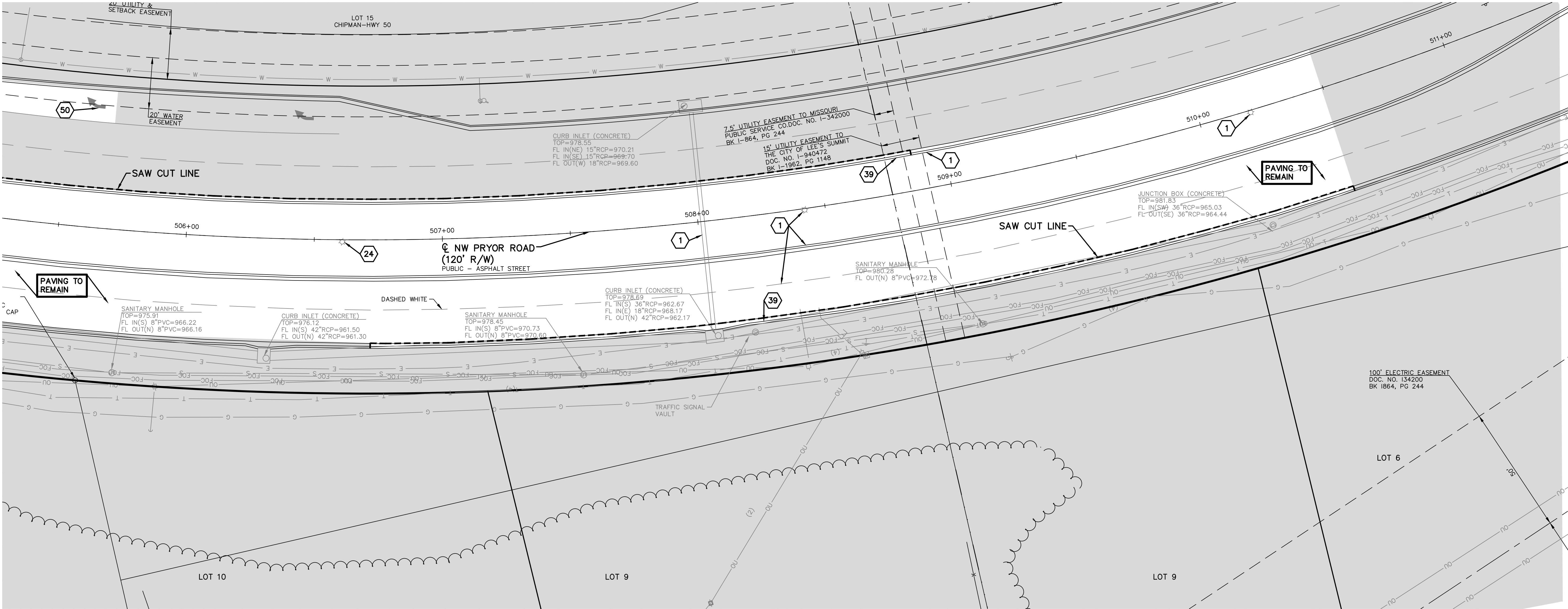
**DATUM BENCHMARK:**  
VERTICAL DATUM IS NAVD 88 ESTABLISHED USING  
OPUS PROJECTS ON PROJECT CONTROL.

**BENCHMARKS:**

BM #1: CHISELED "SQUARE" ON TOP OF CURB P  
OF INTERSECTION OF WEST PARK PARKING LOT &  
EAST DRIVE ENTRANCE. ELEV=984.97

BM #2: CHISELED "SQUARE" ON NORTHWEST COR  
AREA INLET, 25'± EAST OF CURB LINE AND ON-1  
WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90°  
BEND IN ROAD. ELEV=970.98





DEMOLITION NOTES:

- 1 TO REMAIN
- 2 REMOVE TREE (SEE PLANS)
- 3 TREES TO REMAIN
- 7 SIGN TO BE RELOCATED
- 8 PORTION OF RETAINING WALL TO BE REMOVED (SEE PLANS)
- 11 TO BE REMOVED BY ELECTRIC COMPANY
- 24 STREET LIGHT TO BE RELOCATED (SEE PLANS)
- 39 CONTRACTOR TO REMOVE (CURBS)
- 44 CONTRACTOR TO REMOVE (SIDEWALK)
- 46 CONTRACTOR TO REMOVE (PAVING)
- 50 CONTRACTOR TO SAND BLAST STRIPING (SEE PAVEMENT MARKING SHEETS)
- 65 TRAFFIC POLE TO BE RELOCATED. (SEE PLANS)
- 66 TRAFFIC POLE TO BE REMOVED AND RETURNED TO THE CITY OF LEES SUMMIT

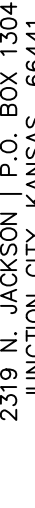
LEGEND

- [Hatched Box] NOT A PART OF DEMOLITION ACTIVITIES
- SAW CUT LIMITS

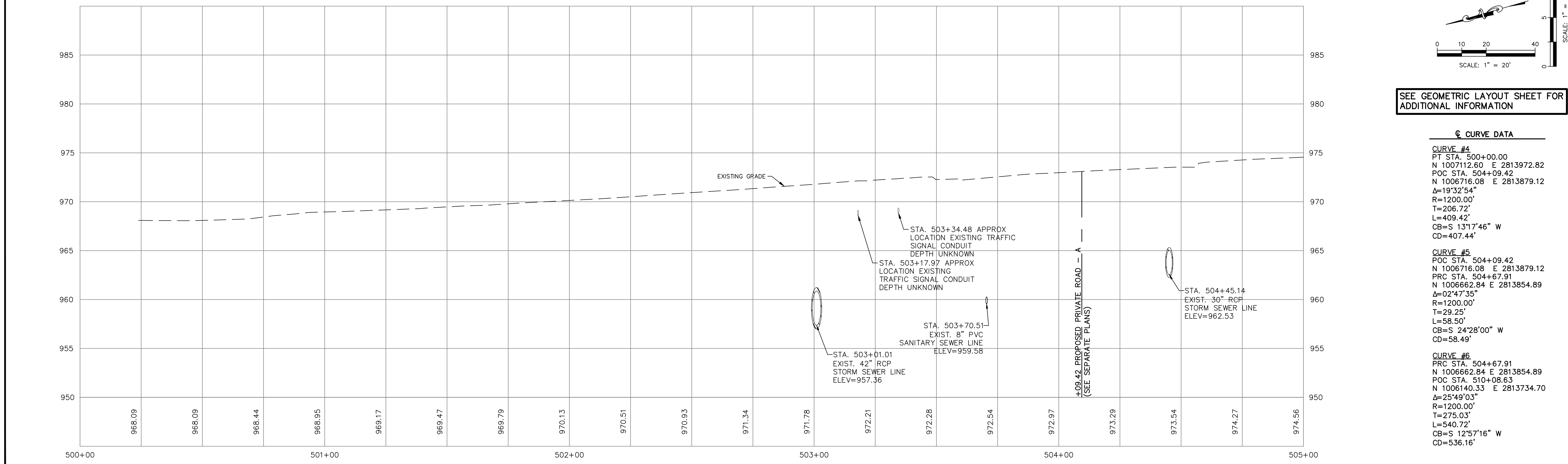
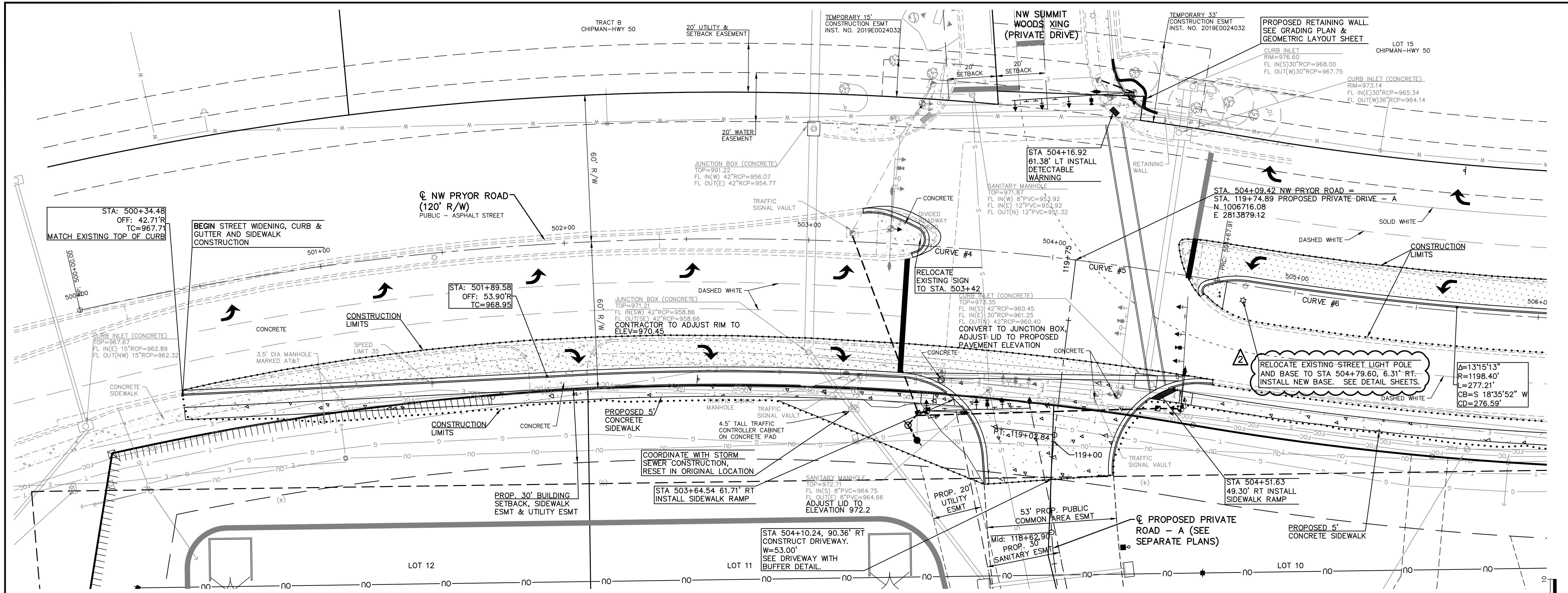
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BENCHMARKS:  
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OF INTERSECTION OF WEST PARK PARKING LOT AT  
EAST DRIVE ENTRANCE. (ELEV=984.97)

BM #2: CHISELED "SQUARE" ON NORTHWEST CORNER  
AREA INLET, 25'± EAST OF CURB LINE AND ON-LINE  
WITH SOUTH CURB OF LOWENSTEIN DRIVE A 90°  
BEND IN ROAD. (ELEV=970.98)

STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		PROJ. NO. A14-7067-1		DESIGNER LDO		DRAWN BY JT/BKR		CFN		SHEET 5		REV 1		NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING DEMOLITION PLAN		2319 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX (785) 762-7744 joe@kveeng.com   www.kveeng.com		 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19		LEON D. OSBOURN ENGINEER MO # 021726																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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REV	DATE	DESCRIPTION	CHK
2	7-24-19	REVISED PER CITY COMMENTS	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO
0	5-23-19	INITIAL ISSUE	LDO

LEON D. OSBOURN  
ENGINEER  
MO # 021726

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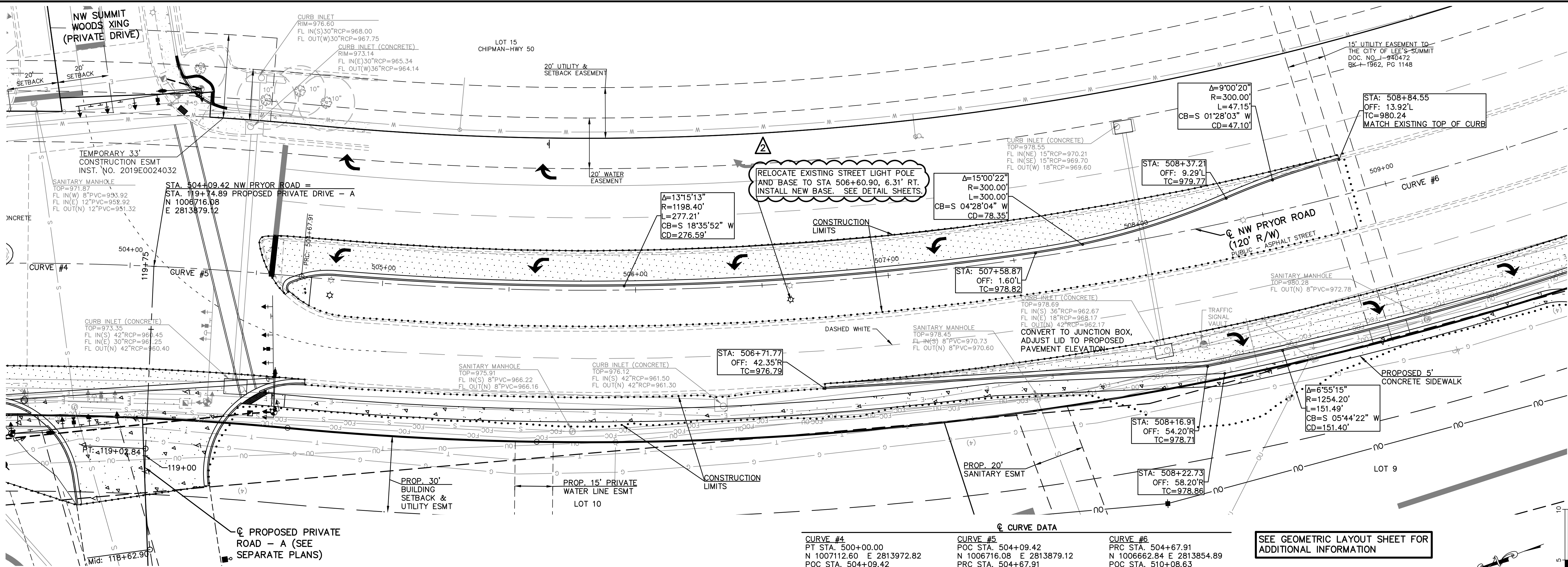
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**STREETS OF WEST PRYOR**  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
NW PRYOR DRIVE WIDENING - PLAN & PROFILE

PROJ. NO.	A14 7067-1
DESIGNER	LDO
DRAWN BY	JT/BKR
CFN	7067-1SG_STPP
SHEET	6
REV	2



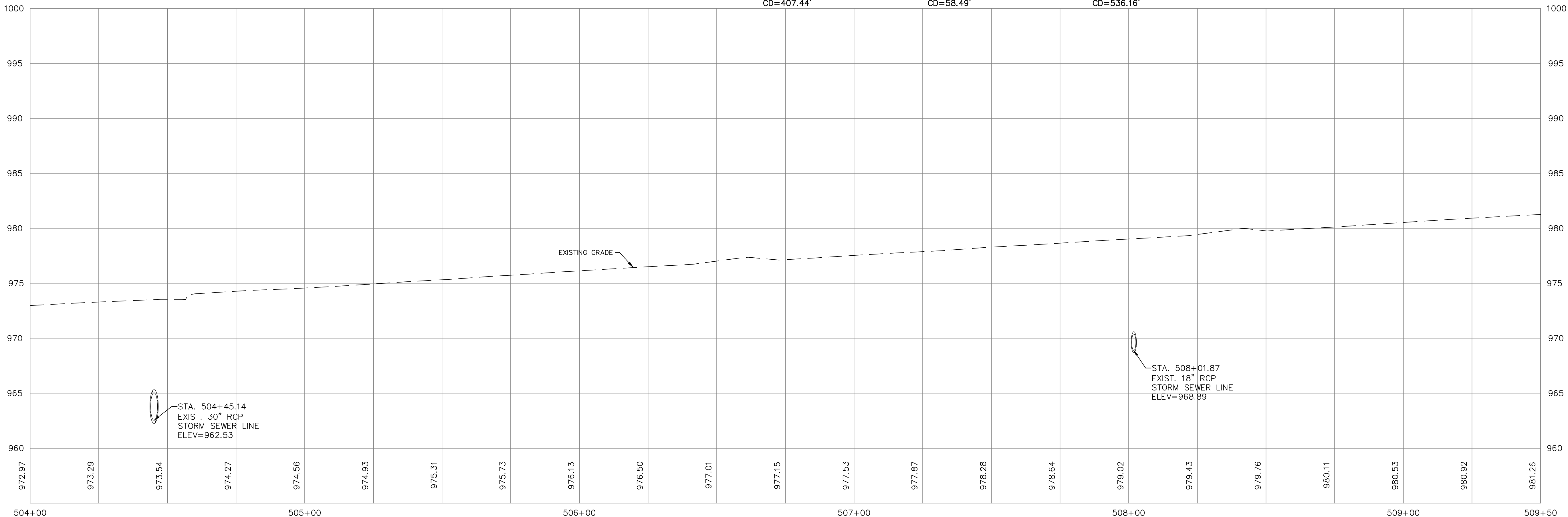
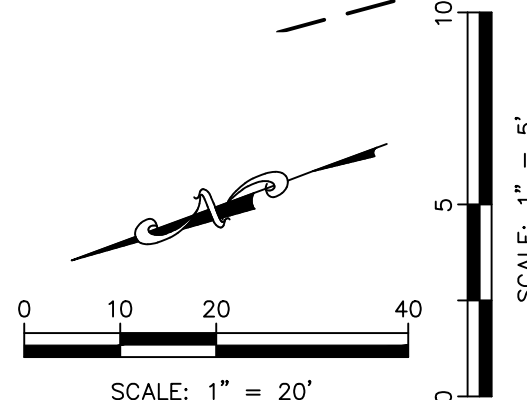


**CURVE #4**  
PT STA. 500+00.00  
N 1007112.60 E 2813972.82  
POC STA. 504+09.42  
N 1006716.08 E 2813879.12  
N 1006716.08 E 2813879.12  
 $\Delta=19'32'54''$   
R=1200.00'  
T=206.72'  
L=409.42'  
CB=S 13°17'46" W  
CD=407.44'

**CURVE #5**  
POC STA. 504+09.42  
N 1006716.08 E 2813879.12  
PRC STA. 504+67.91  
N 1006662.84 E 2813854.89  
 $\Delta=02'47'35''$   
R=1200.00'  
T=29.25'  
L=58.50'  
CB=S 24°28'00" W  
CD=58.49'

**CURVE #6**  
PRC STA. 504+67.91  
N 1006662.84 E 2813854.89  
POC STA. 510+08.63  
N 1006140.33 E 2813734.70  
 $\Delta=25'49'03''$   
R=1200.00'  
T=275.03'  
L=540.72'  
CB=S 12°57'16" W  
CD=536.16'

SEE GEOMETRIC LAYOUT SHEET FOR  
ADDITIONAL INFORMATION



REV	DATE	DESCRIPTION	DSN	CHK
2	7-24-19	REVISED PER CITY COMMENTS	LDO	JT
1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT
0	5-23-19	INITIAL ISSUE	LDO	JT

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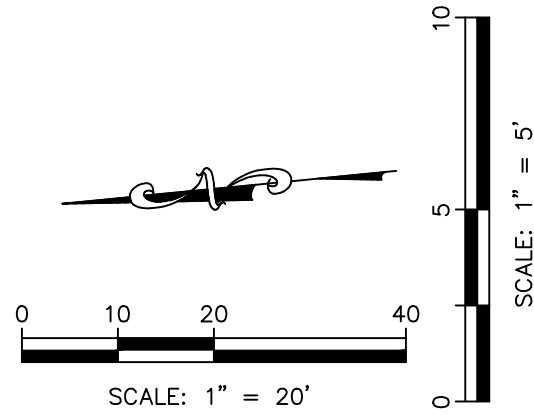
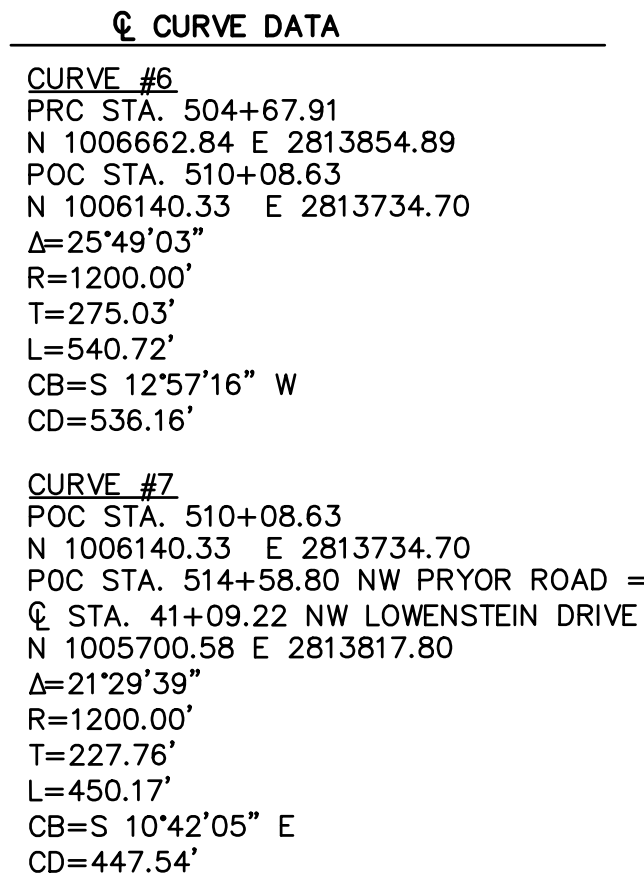
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LEE'S SUMMIT, MISSOURI

**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING**  
**NW PRYOR DRIVE WIDENING - PLAN & PROFILE**

PROJ. NO.	A14-7067-1
DESIGNER	LDO
DRAWN BY	JT/BKR
CFN	7067-1SIC-STPP
SHEET	7
REV	2





SEE GEOMETRIC LAYOUT SHEET FOR  
ADDITIONAL INFORMATION



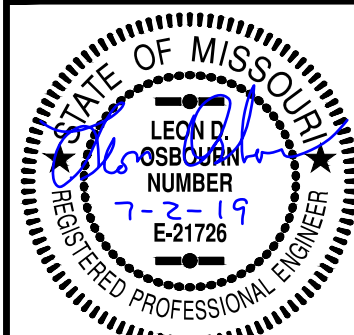
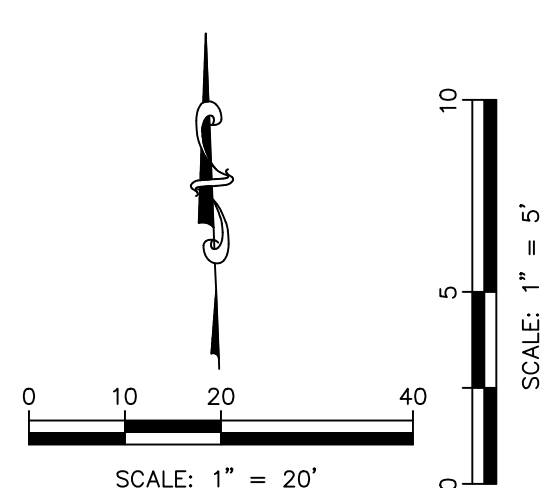
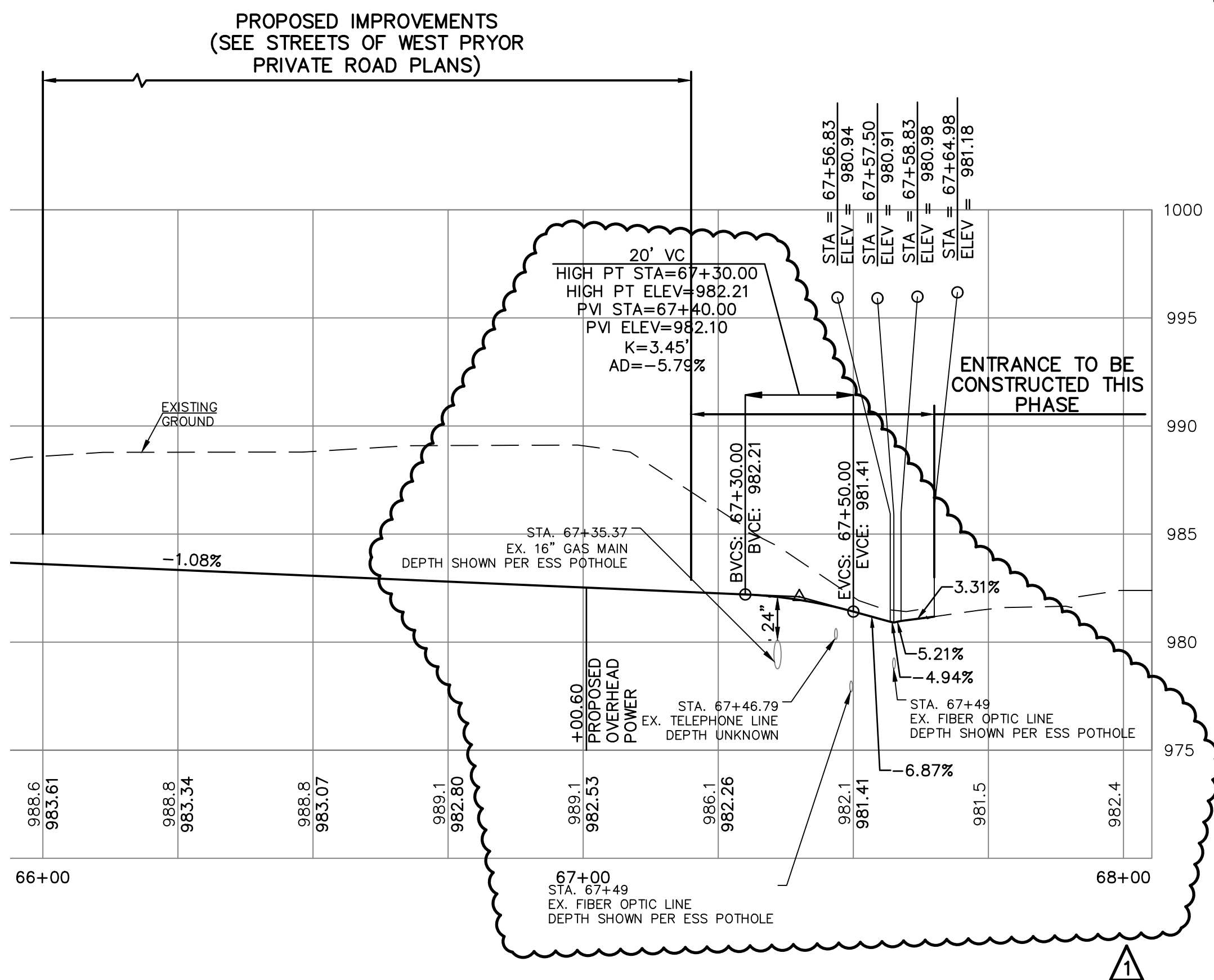
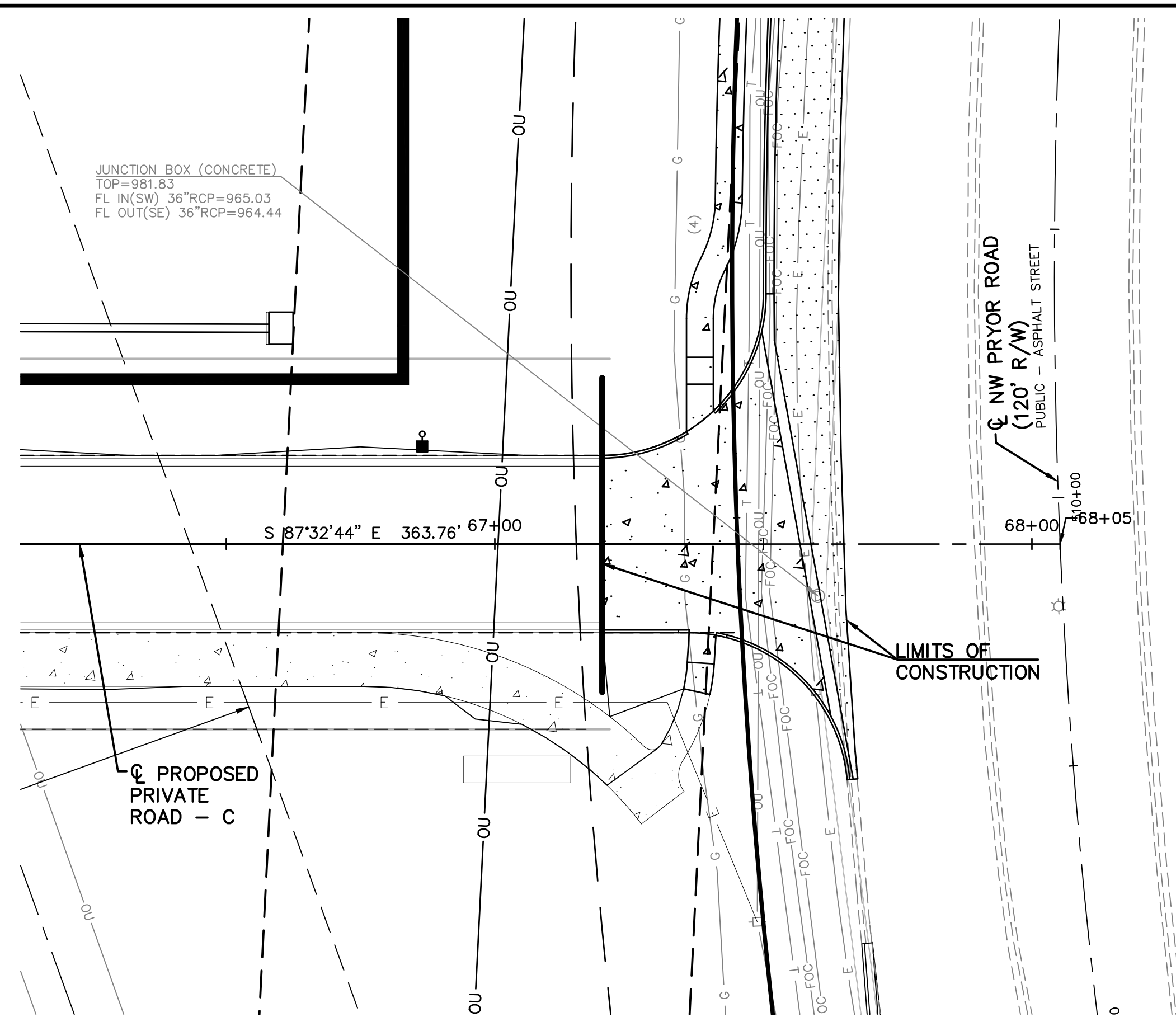
LEON D. OSBOURN  
ENGINEER  
MO # 021726

**STREETS OF WEST PRYOR**  
NW/4 NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
NW PRYOR DRIVE WIDENING - PLAN & PROFILE

PROJ. NO.	
A14_7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
7067-1SIG_STPP	
SHEET	REV
8	1





LEON D. OSBOURN  
ENGINEER  
MO # 021726

2319 N. JACKSON | P.O. BOX 1304  
JUNCTION CITY, KANSAS 66441  
PH. (785) 762-5040 | FAX (785) 762-7744  
jc@kveng.com | www.kveng.com



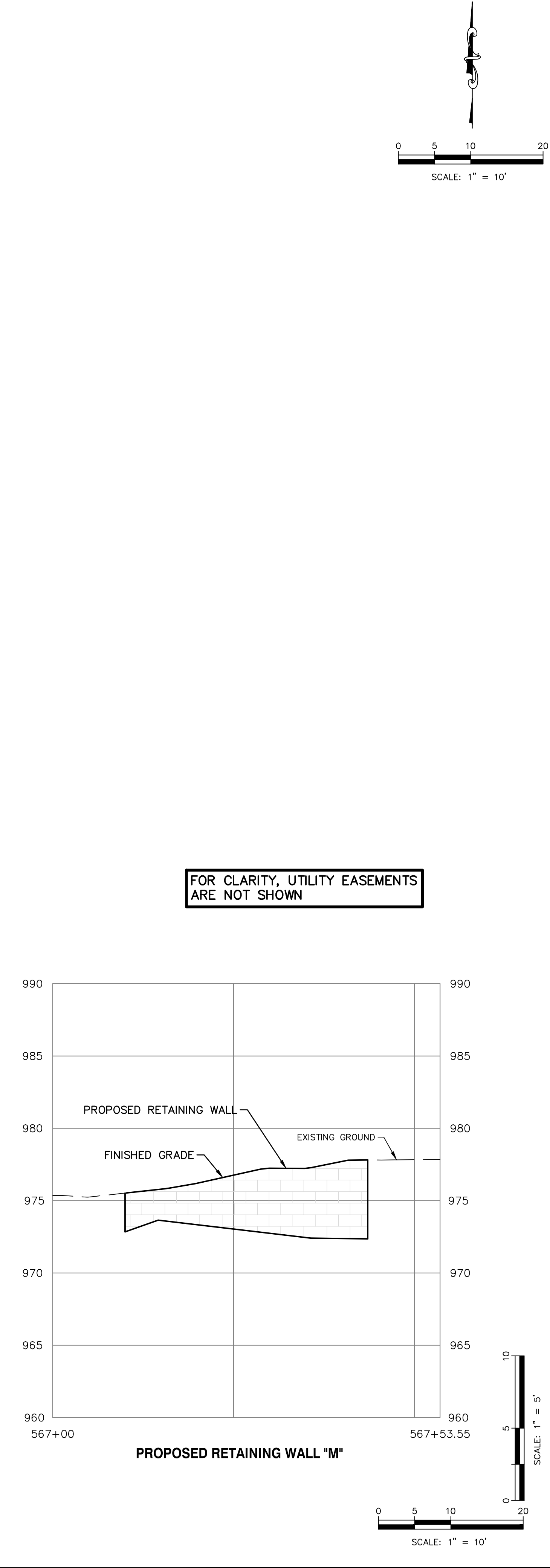
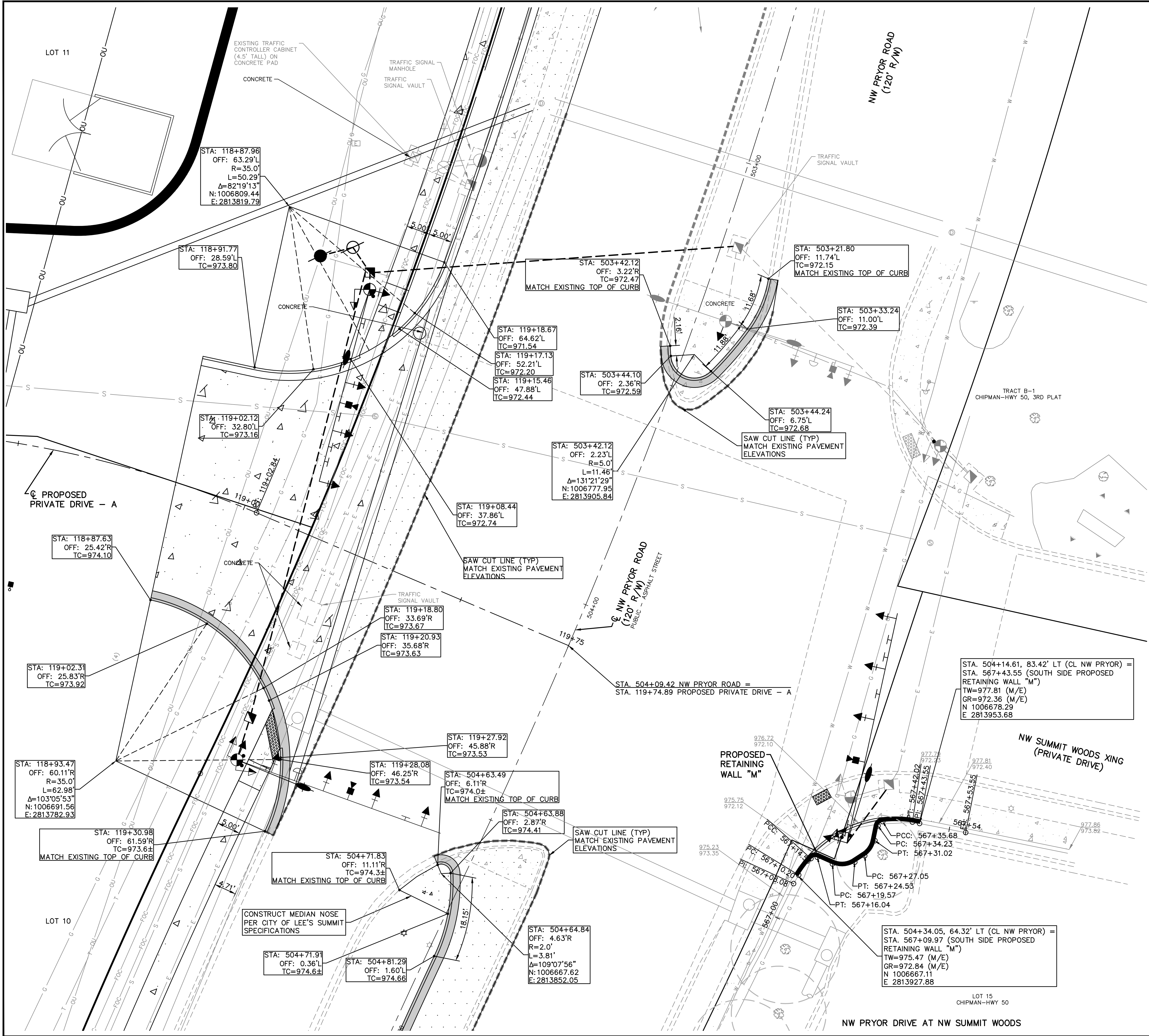
KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842.

**JUST PRYOR**  
**& NW LOWENSTEIN DR**  
**BOUR**

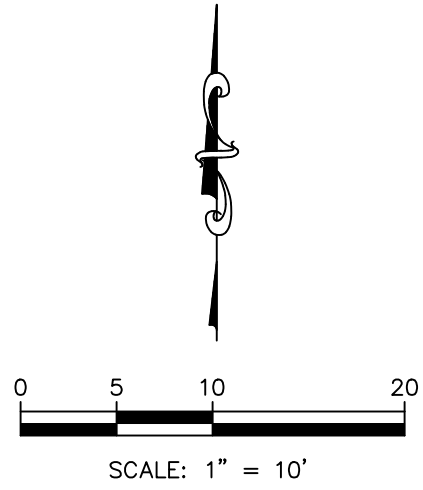
**WNW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
DRIVE AT PROPOSED PRIVATE ROAD - A - PLAN & PROFILE**

PROJ. NO.		A14_7067-	
DESIGNER	LDO	DRAWN BY	JT/BK
CFN			
7067-1SIG_DRIVEPP			
SHEET		REV	
9		1	





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FOR CLARITY, UTILITY EASEMENTS  
ARE NOT SHOWN

PROJ. NO. A14-7067-1	
DESIGNER LDO	DRAWN BY JT/BKR
CFN	REV
7067-1SIG GEOM	
SHEET	10
2	

REV	DATE	DESCRIPTION
2	7-24-19	REVISED PER CITY COMMENTS
1	7-2-19	REVISED PER CITY COMMENTS
0	5-23-19	INITIAL ISSUE

DSN	DWN	CHK
LDO	JT	LDO
LDO	JT	LDO
LDO	JT	LDO

STATE OF MISSOURI  
LEON D. OSBOURN  
REGISTERED PROFESSIONAL ENGINEER  
E-21726

LEON D. OSBOURN  
ENGINEER  
MO # 021726

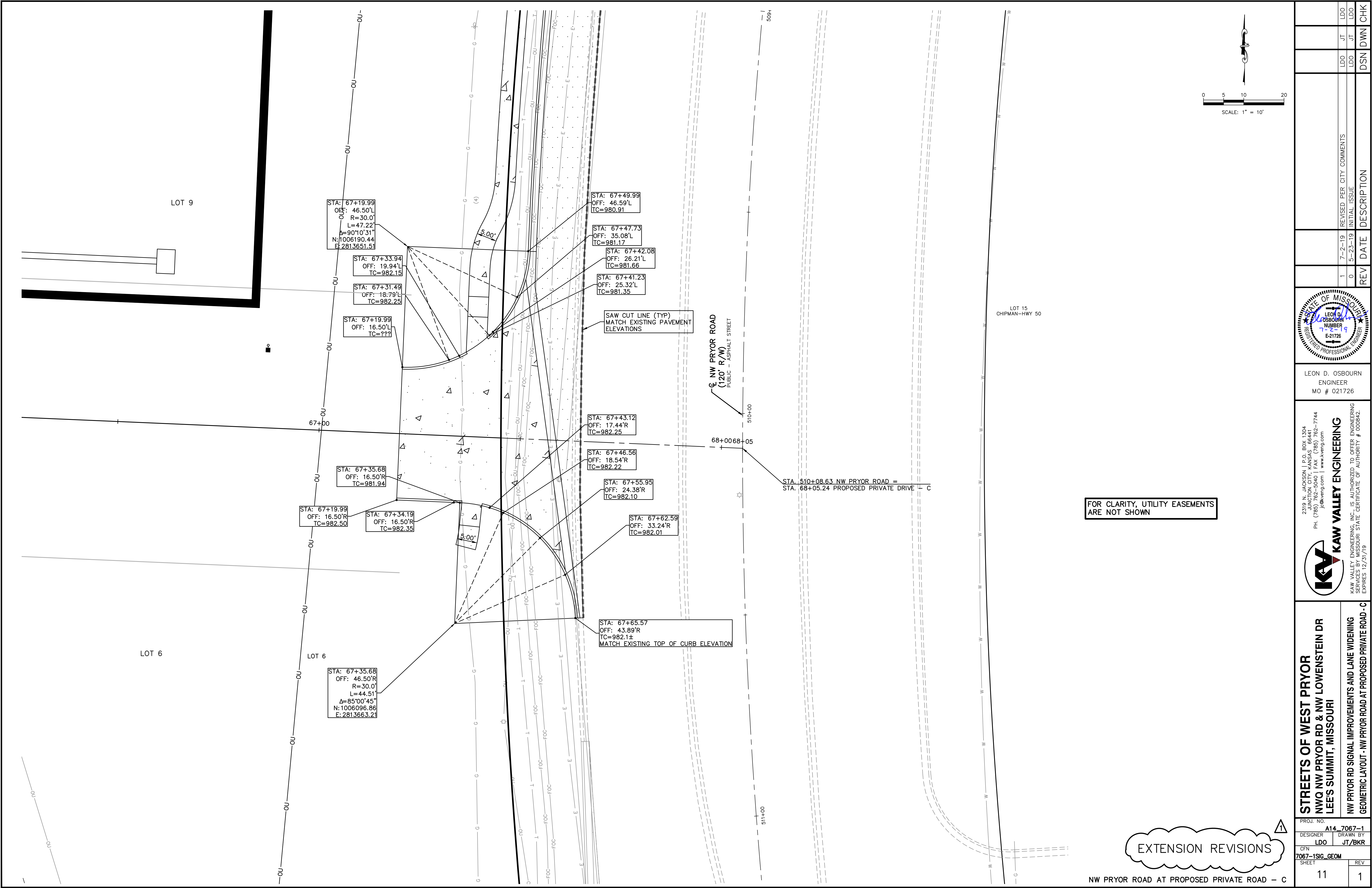
2319 N. JACKSON | P.O. BOX 1304  
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joe@kve.com | www.kve.com

**KAW VALLEY ENGINEERING**  
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EXPIRES 12/31/19

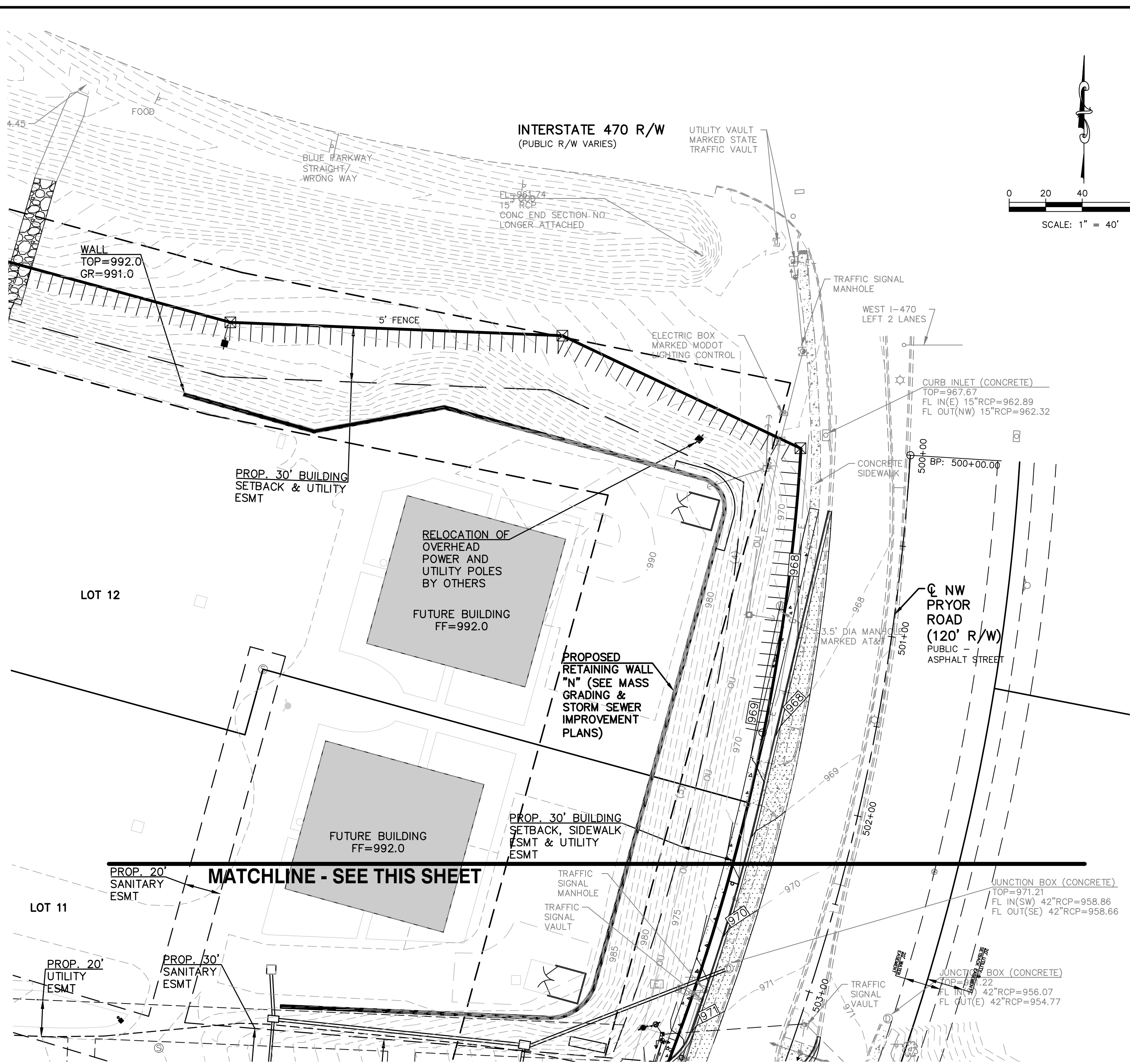
**STREETS OF WEST PRYOR**  
NW Q NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
GEOMETRIC LAYOUT - NW PRYOR ROAD AT NW SUMMIT WOODS



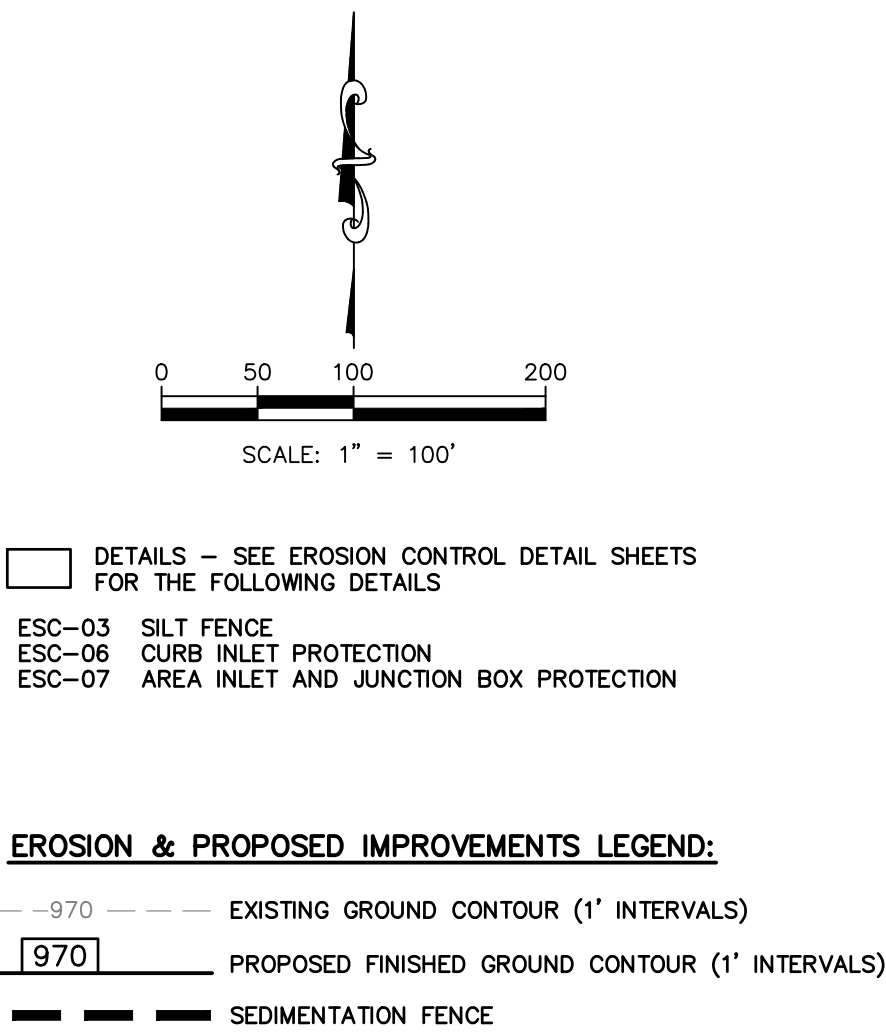
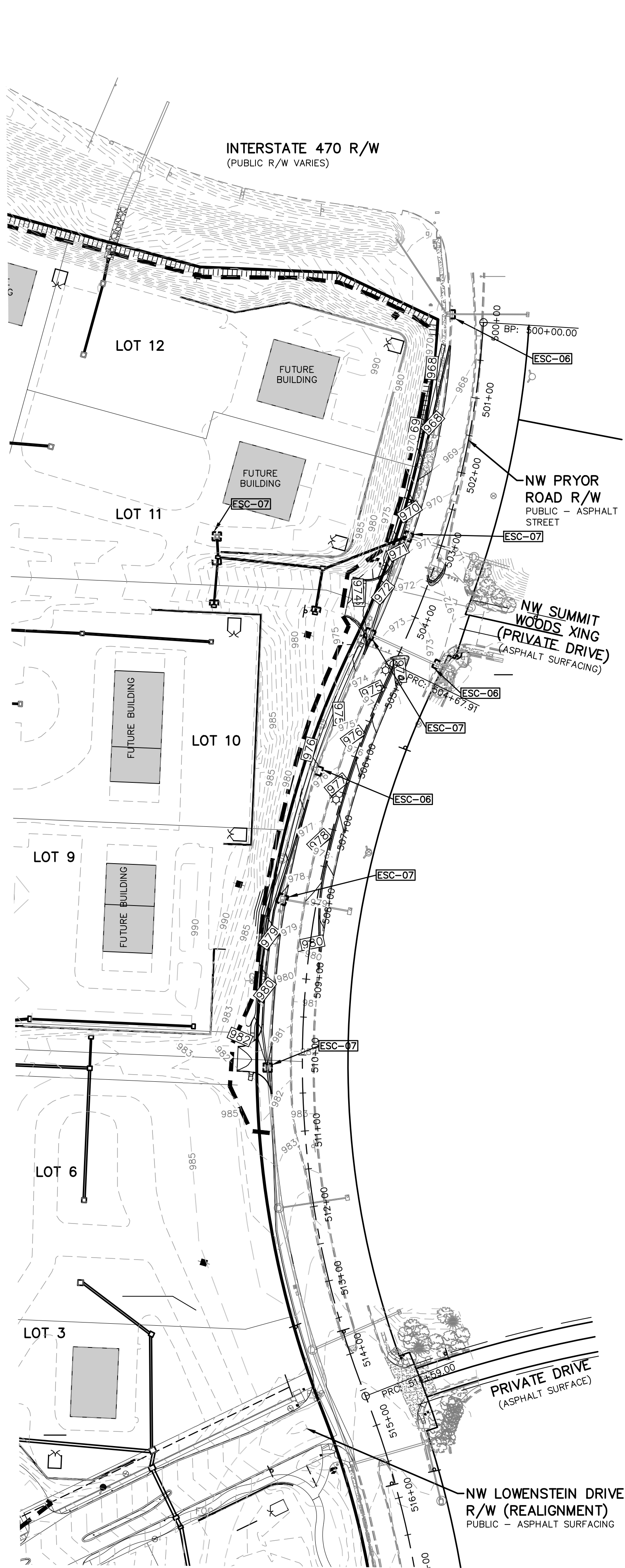






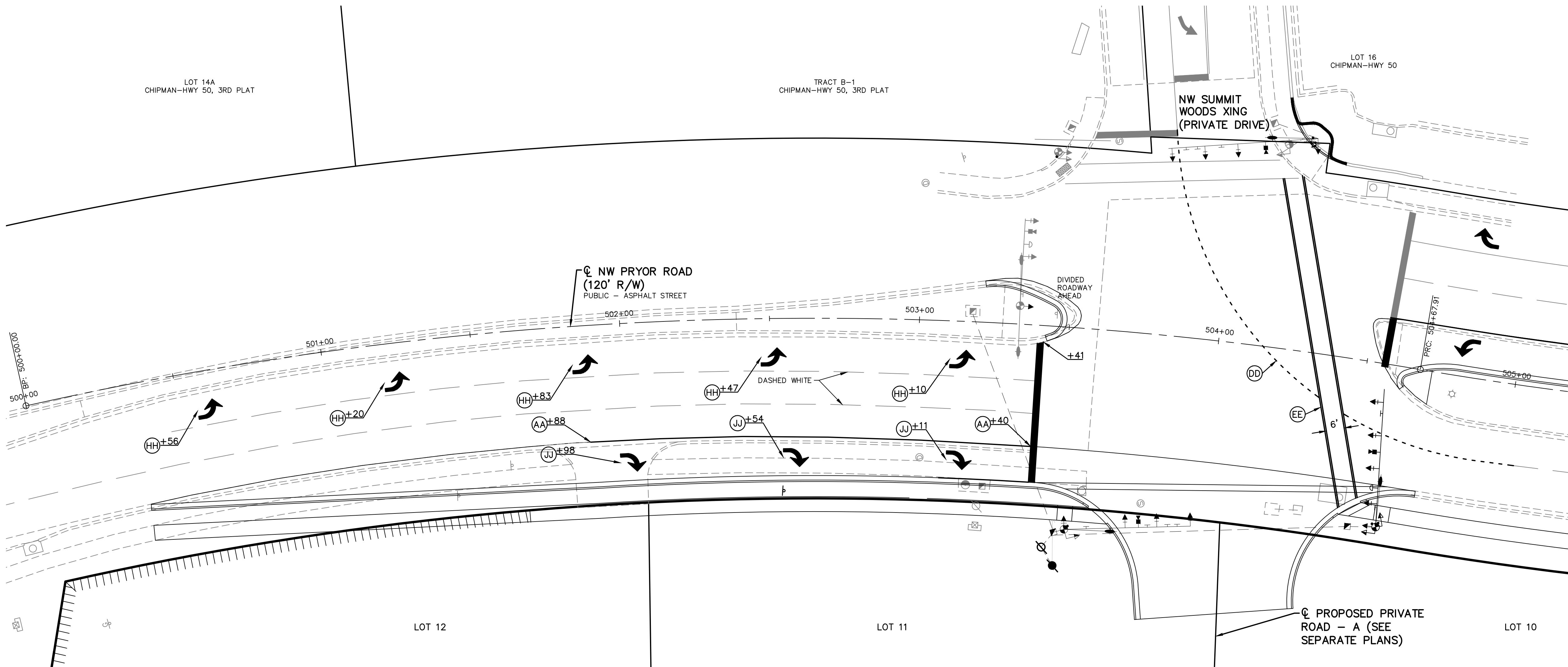
<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		PROJ. NO. <b>A14 7067-1</b>	
		DESIGNER <b>LDO</b>	DRAWN BY <b>JT/BKR</b>
CFN <b>7067-1SIG_GP</b>	SHEET <div> <div>12</div> <div>2</div> </div>		





- GENERAL NOTES:
1. PROPERTY LINE IS LIMITS OF CONSTRUCTION EXCEPT AS SHOWN.
  2. THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES AS SHOWN ON THE DRAWINGS PRIOR TO BEGINNING EARTHWORK OPERATIONS.
  3. THE CONTRACTOR SHALL MAINTAIN ALL SILT CONTROL MEASURES DURING CONSTRUCTION.
  4. ALL SILT SHALL REMAIN ON SITE AND SURROUNDING STREETS SHALL BE KEPT CLEAR OF ALL MUD AND DEBRIS.
  5. A SEDIMENTATION BARRIER IS TO BE INSTALLED AS SHOWN.
  6. ACCUMULATED SEDIMENT SHALL BE REMOVED AND THE SEDIMENTATION BARRIERS MAINTAINED AS NEEDED TO PREVENT SEDIMENTATION BYPASS OF THE BARRIER.
  7. SLOPES ARE TO BE LEFT IN A ROUGH CONDITION DURING GRADING.
  8. CURB INLET SEDIMENTATION BARRIERS ARE TO BE INSTALLED AROUND INLETS AND WEIRS WHERE SEDIMENTATION IS A CONCERN. INLET BARRIERS SHALL BE EITHER BLOCK AND GRAVEL, OR SECURED STRAW BALES, OR SILT FENCE.
  9. SEDIMENT IS TO BE REMOVED FROM STORM WATER DRAINAGE SYSTEMS.
  10. RIPRAP IS TO BE INSTALLED AT AREAS OF CONCENTRATED FLOW (I.E. CULVERT OUTLETS).
  11. CONTRACTOR IS RESPONSIBLE FOR INSTALLING ANY ADDITIONAL EROSION CONTROL AS HE/SHE DEEMS NECESSARY.
  12. THE CONTRACTOR SHALL PROVIDE ALL MATERIALS, TOOLS, EQUIPMENT AND LABOR AS NECESSARY TO INSTALL AND MAINTAIN ADEQUATE EROSION AND SILTATION CONTROLS REQUIRED TO PREVENT SOIL EROSION FROM LEAVING THE PROJECT SITE. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO ENSURE THAT METHODS UTILIZED ARE ADEQUATE AND COMPLY WITH REQUIREMENTS OF THE SPECIFICATIONS AND GOVERNMENTAL AGENCIES HAVING JURISDICTION OVER THE WORK.
  13. TEMPORARY SEDIMENT FENCE TO REMAIN UNTIL ADEQUATE VEGETATION IS ESTABLISHED.
  14. MUD AND DEBRIS SHALL BE CLEANED UP AT THE CONCLUSION OF EACH WORKING DAY, OR AFTER EACH RAINFALL IF SILT IS PRESENT.
  15. INSPECTION, MAINTENANCE AND REPAIR OF EROSION CONTROL DEVICES SHALL BE ON GOING THROUGHOUT THE LIFE OF BUILDING CONSTRUCTION TO KEEP THE DEVICES IN OPERABLE CONDITION AT ALL TIMES. ADDITIONAL MEASURES SHALL BE INSTALLED AS REQUIRED BY ACTUAL FIELD CONDITIONS AND/OR GOVERNING INSPECTION AGENCIES.
  16. INSTALL CONSTRUCTION ENTRANCE AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING THE SITE AND AS SHOWN ON PLANS.
  17. AT COMPLETION OF SITE GRADING AND OTHER RELATED CONSTRUCTION ACTIVITIES, ALL DISTURBED AREAS WITHIN THE PROJECT SITE SHALL BE SEEDED, SODDED, OR LANDSCAPED AS SHOWN ON THE LANDSCAPE PLAN WITHIN 14 DAYS.
  18. TOPSOIL IS TO BE PLACED IN AREAS UNSUITABLE FOR VEGETATIVE GROWTH.
  19. STRIP TOPSOIL PRIOR TO EXCAVATION, STOCKPILE AND SPREAD ONTO DISKED SUBGRADE (4" MIN) A THICKNESS OF 4 INCHES.
  20. ROCK LINING (RIPRAP) SHALL BE DURABLE STONE CONTAINING A COMBINED TOTAL OF NOT MORE THAN 10 PERCENT OF EARTH, SAND, SHALE AND NON-DURABLE ROCK. AT LEAST 60 PERCENT OF THE MASS SHALL BE OF PIECES HAVING A MINIMUM WEIGHT OF 150 POUNDS OR MORE PER CUBIC FOOT.
  21. THE CONTRACTOR SHALL HAVE THE RESPONSIBILITY FOR RESOLVING COMPLAINTS IN THE EVENT THAT COMPLAINTS OR DAMAGE CLAIMS ARE FILED DUE TO DAMAGES OCCURRING ADJACENT TO OR DOWNSTREAM FROM PROPERTY BY SEDIMENT RESULTING FROM EROSION ON THE PROJECT SITE.
  22. GOOD HOUSEKEEPING PRACTICES SHALL BE MAINTAINED ON SITE TO KEEP SOLID WASTE FROM ENTRY INTO WATERS.
  23. ALL FUELING FACILITIES PRESENT ON SITE SHALL ADHERE TO APPLICABLE FEDERAL AND STATE REQUIREMENTS CONCERNING UNDERGROUND STORAGE, ABOVE GROUND STORAGE AND DISPENSERS, INCLUDING SPILL PREVENTION, CONTROL AND COUNTER MEASURES.
  24. RIGHT OF WAY TO BE STABILIZED AS REQUIRED BY APWA SECTION 2400.
  25. EROSION CONTROL IS TO BE PLACED IN PHASING AS CONSTRUCTION PROGRESSES.
  26. MINIMAL WASHING OF CONCRETE EQUIPMENT ALLOWED, CHUTE ETC. CONCRETE WASHOUT OF THE DRUM IS NOT ALLOWED. ANY PIT/WASHOUT AREA NEEDS TO BE MAINTAINED IN A NON-DISCHARGING MANNER AND ANY WASTE RESIDUE WILL NEED TO BE CLEANED OUT AND REMOVED AT THE END OF PROJECT.
  27. EROSION CONTROL SEDIMENT FENCE TO BE INSTALLED 1'-0" BEHIND CURB & GUTTER UPON COMPLETION OF BACKFILL OF CURB IN ALL AREAS WHERE SLOPES FROM LOT DRAIN TOWARDS CURB. UPON COMPLETION OF FINAL GRADING THE TOES OF ALL EMBANKMENTS IN EXCESS OF TWO FEET IN HEIGHT WILL HAVE EROSION CONTROL SEDIMENT FENCE INSTALLED.





**NOTE:**  
1. PAVEMENT MARKING SHALL BE PROVIDED AND INSTALLED IN ACCORDANCE WITH THE CURRENT CITY OF LEE'S SUMMIT PAVEMENT MARKING STANDARDS.  
2. ALL DIMENSIONS REFERENCING DOUBLE YELLOW LINES REFERS TO THE CENTER OF THE SPACE BETWEEN THE TWO DOUBLE YELLOW LINES.  
3. CONTRACTOR TO GRIND OFF CONFLICTING PAVEMENT MARKING.

LIST OF PAVEMENT MARKING	
MARK	ITEM
AA	4" SOLID WHITE LINE (HIGH BUILD PAINT)
DD	6" DASHED WHITE EXTENSION LINE (THERMOPLASTIC)
EE	6" SOLID WHITE CROSS-WALK LINE (THERMOPLASTIC)
GG	24" STOP BAR (THERMOPLASTIC)
HH	TURN ARROW WHITE (LEFT) (THERMOPLASTIC)
JJ	TURN ARROW WHITE (RIGHT) (THERMOPLASTIC)

PROJ. NO. <b>A14_7067-1</b>	
DESIGNER <b>LDO</b>	DRAWN BY <b>JT/BKR</b>
CFN <b>7067-1SIG_PMP</b>	
SHEET <b>14</b>	REV <b>2</b>

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
NW PRYOR ROAD PAVEMENT MARKING PLAN

2319 N. JACKSON | P.O. BOX 1304  
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jke@kveg.com | www.kveg.com

**KAW VALLEY ENGINEERING**  
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EXPIRES 12/31/19

LEON D. OSBOURN  
ENGINEER  
MO # 021726

STATE OF MISSOURI  
LEON D. OSBOURN  
NUMBER  
7-24-19  
E-21726  
REGISTERED PROFESSIONAL ENGINEER

2  
7-24-19  
REVISED PER CITY COMMENTS

1  
7-2-19  
REVISED PER CITY COMMENTS

0  
5-23-19  
INITIAL ISSUE

REV  
DATE  
DESCRIPTION

LOT 14A  
CHIPMAN-HWY 50, 3RD PLAT

TRACT B-1  
CHIPMAN-HWY 50, 3RD PLAT

LOT 16  
CHIPMAN-HWY 50

NW SUMMIT  
WOODS XING  
(PRIVATE DRIVE)

LOT 12

LOT 11

LOT 10

CL NW PRYOR ROAD  
(120' R/W)  
PUBLIC - ASPHALT STREET

CL PROPOSED PRIVATE  
ROAD - A (SEE  
SEPARATE PLANS)

500+00

501+00

502+00

503+00

504+00

505+00

HH+56

HH+20

HH+83

AA+88

JJ+98

HH+47

JJ+54

HH+10

JJ+11

AA+40

DD

EE

6'

PRC: 50+67.91

DIVIDED  
ROADWAY  
AHEAD

DASHED WHITE

0

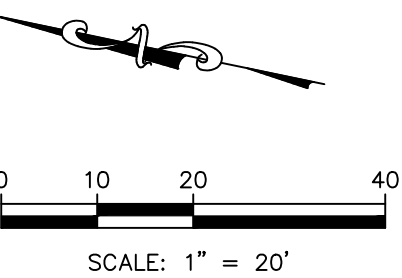
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20

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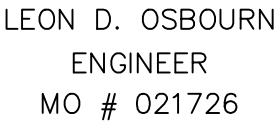
SCALE: 1" = 20'





**NOTE:**

1. PAVEMENT MARKING SHALL BE PROVIDED AND INSTALLED IN ACCORDANCE WITH THE CURRENT CITY OF LEE'S SUMMIT PAVEMENT MARKING STANDARDS.
2. ALL DIMENSIONS REFERENCING DOUBLE YELLOW LINES REFERS TO THE CENTER OF THE SPACE BETWEEN THE TWO DOUBLE YELLOW LINES.
3. CONTRACTOR TO GRIND OFF CONFLICTING PAVEMENT MARKING.



**S. SHEETS OF WEST PRYOR  
NW/4 NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI**

PROJ. NO.	
A14_7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
7067-1SIG_PMP	
SHEET	REV
15	2



GENERAL NOTES:

1. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS (APPROVED BY THE CITY OF LEE'S SUMMIT) AND ONE (1) COPY OF THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES.

2. CONSTRUCTION OF THE IMPROVEMENTS SHOWN OR IMPLIED BY THIS SET OF DRAWINGS SHALL NOT BE INITIATED OR ANY PART THEREOF UNDERTAKEN UNTIL THE DIRECTOR OF PUBLIC WORKS OR HIS AGENT IS NOTIFIED OF SUCH INTENT, AND ALL REQUIRED AND PROPERLY EXECUTED BONDS AND CONTRACT AGREEMENTS ARE RECEIVED AND APPROVED BY THE CITY.

3. THE CONSTRUCTION COVERED BY THESE PLANS SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI, IN CURRENT USE. SPECIFICALLY, BUT NOT EXCLUSIVE TO: TRAFFIC SIGNAL SPECIFICATION: SECTION 2900. TRAFFIC SIGNAL STANDARD DRAWINGS: TS-1 THROUGH TS-10.

4. ALL WORKMANSHIP AND MATERIALS SHALL BE SUBJECT TO THE INSPECTION AND APPROVAL OF THE PUBLIC WORKS DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI.

5. RIGHT-OF-WAY LIMITS SHOULD BE CROSS CHECKED BY THE CONTRACTOR AND APPROVED BY THE FIELD INSPECTOR BEFORE UNDERTAKING ANY EXCAVATIONS AT THE SITE.

6. THE CONTRACTOR SHALL STAKE THE LOCATION OF ALL POLE BASES, PULL BOXES, AND CONTROLLER CABINET BASE, THEN PROVIDE THE CITY ONE WEEK NOTICE PRIOR TO THE START OF CONSTRUCTION, AND SUBSEQUENT CONSTRUCTION ACTIVITIES, FOR INSPECTION AND APPROVAL. THE CONTRACTOR SHALL PROVIDE A WORK SCHEDULE, CONTACT NAMES, AND PHONE NUMBERS.

7. ALL LOCATIONS INDICATED IN DRAWINGS, INCLUDING CONDUIT RUNS ARE SUBJECT TO ADJUSTMENT TO CLEAR OBSTRUCTIONS AND TO MEET SITE CONDITIONS, IF ANY BY THE CITY.

8. EXISTENCE AND LOCATION OF ANY UNDERGROUND OR OVERHEAD FACILITIES SHOWN ON THESE DRAWINGS OR REFERENCE TO ANY SOIL CONDITIONS, IF MADE, ARE APPROXIMATE ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL SITE CONDITIONS AND TO LOCATE ALL UTILITIES, INCLUDING DEPTH, BEFORE STARTING CONSTRUCTION SO THAT ANY ADJUSTMENTS TO DESIGN CAN BE MADE PRIOR TO POLE ORDERING OR FABRICATION. IN ADDITION, THE CONTRACTOR SHALL AVOID DISRUPTION OF SERVICES PROVIDED BY THE UTILITIES AND SHALL INSURE THAT PROPER CLEARANCES (OVERHEAD AND UNDERGROUND) ARE MAINTAINED FOR THE DURATION OF CONSTRUCTION. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UTILITIES.

9. THE CONTRACTOR SHALL COORDINATE WITH THE CITY TRAFFIC ENGINEER FOR ANY NECESSARY CHANGES TO THE TRAFFIC SIGNALS RESULTING FROM EXISTING UTILITIES OR OTHER CONSTRUCTION ISSUES.

10. ANY EQUIPMENT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

11. SIGNAL EQUIPMENT SHALL NOT FORM AN OBSTRUCTION TO THE MOVEMENT OF PEDESTRIAN AND WHEELCHAIR TRAFFIC AND SHALL BE ADA ACCESSIBLE. WHERE SIDEWALKS ARE PRESENT, A MINIMUM CLEAR WIDTH OF 48 INCHES SHALL BE AVAILABLE FOR PEDESTRIAN AND WHEELCHAIR MOVEMENT. PULL BOXES SHALL NOT BE INSTALLED ON WHEELCHAIR RAMPS.

12. CONDUITS TO BE PLACED OUTSIDE OF PAVED AREAS SHALL BE TRENCHED IN PLACE. IF THE PROJECT INCLUDES ROADWAY IMPROVEMENTS, THE CONDUIT SHALL BE TRENCHED AFTER THE ROADWAY ROUGH GRADE IS ESTABLISHED AND PRIOR TO ANY FINAL ROADWAY PAVING, CURB & GUTTER, MEDIAN OR SIDEWALK SECTIONS ARE PLACED. ALL COMPACTION AND BACKFILL SHALL MEET CITY OF LEE'S SUMMIT REQUIREMENTS. AT THE OPTION OF THE CONTRACTOR, CONDUITS MAY BE BORED OUTSIDE PAVED AREAS, BUT THERE WILL BE NO ADJUSTMENT TO THE UNIT PRICES FOR CONDUIT INSTALLATION AND ANY CHANGE IN COST WOULD BE THE CONTRACTOR'S RESPONSIBILITY. ANY CONDUIT BORE OUTSIDE THE PAVED AREAS SHALL BE DONE AFTER ROADWAY IMPROVEMENTS ARE COMPLETE. CONDUITS TO BE PLACED WITHIN THE LIMITS OF PAVEMENT SHALL BE BORED UNLESS OTHERWISE AUTHORIZED BY THE CITY TRAFFIC ENGINEER. IF THE PROJECT INCLUDES ROADWAY IMPROVEMENTS, THE CONDUIT SHALL BE BORED PRIOR TO ANY FINAL ROADWAY PAVING. POTHOLING FOR UTILITIES ON ROAD BORES AFTER FINAL PAVING WILL NOT BE ALLOWED.

13. THE TRAFFIC SIGNAL CONTROLLER, CABINET AND RELATED EQUIPMENT, AS SPECIFIED FOR THIS PROJECT, SHALL BE DELIVERED TO THE CITY FOR TESTING PRIOR TO INSTALLATION. ALL SIGNAL TIMINGS WILL BE PROVIDED BY THE CITY TRAFFIC ENGINEER. THE CONTRACTOR SHALL COORDINATE MATERIAL DELIVERY AND PICK-UP WITH THE PUBLIC WORKS OPERATIONS DEPARTMENT (969-1870) AT LEAST 48 HOURS PRIOR TO TRANSPORTATION. THE CONTRACTOR ASSUMES ALL DAMAGE LIABILITY AND SHOULD INSPECT ALL MATERIALS BEFORE AND AFTER TRANSPORTATION OF EQUIPMENT.

14. THE CONTRACTOR SHALL COORDINATE ALL ELECTRICAL POWER REQUIREMENTS AND CONNECTION ACTIVITIES WITH THE UTILITY COMPANY, INCLUDING LOCATION OF THE METER, CIRCUITRY AND CONNECTION REQUIREMENTS, AND POWER UP THE COMPLETE SYSTEM.

15. ALL DISTURBED SURFACES SHALL BE MADE GOOD TO MATCH EXISTING AT THE CONTRACTOR'S EXPENSE.

16. CONTRACTOR SHALL MAINTAIN AT ALL TIMES ACCESS FOR EMERGENCY VEHICLES AND RESIDENTS ALONG THE ENTIRE PROJECT.

17. SUBSTANTIAL COMPLETION OF THE TRAFFIC SIGNALS SHALL BE DEFINED AS ALL COMPONENTS OF THE TRAFFIC SIGNAL OPERATED FULLY AND SATISFACTORILY WITH RED, YELLOW, AND GREEN CYCLES. SUBSTANTIAL COMPLETION SHALL ALLOW FOR TESTING OF THE SIGNALS, INCLUDING A FLASH PERIOD, PRIOR TO SIGNALS OPERATING WITH CYCLES.

18. FINAL ACCEPTANCE OF TRAFFIC SIGNALS SHALL BE DEFINED AS FINAL WRITTEN APPROVAL AND ACCEPTANCE BY THE CITY, INCLUDING COMPLETION OR CORRECTIONS OF ALL PUNCH LIST ITEMS AND THE TRAFFIC SIGNALS FULLY OPERATIONAL FOR A TIME PERIOD OF FIFTEEN (15) DAYS, WITHOUT ANY PROBLEM, AS NOTED IN THE SPECIFICATIONS. AS-BUILT PLANS SHALL BE SUBMITTED PRIOR TO FINAL ACCEPTANCE BY THE CITY.

19. THE CONTRACTOR SHALL VERIFY AND/OR COORDINATE WITH THE VARIOUS SERVICE PROVIDERS ON THE EXACT LOCATIONS OF METERS, CONNECTION POINTS AND OTHER SPECIFIC PROJECT REQUIREMENTS. THE PROPOSED SERVICE LINE LOCATIONS THAT ARE SHOWN ARE CONCEPTUAL AND ARE NOT WARRANTED.

PROJECT SPECIFIC NOTES:

1. THE CONTRACTOR SHALL SUPPLY THE TRAFFIC SIGNAL CONTROLLER, CABINET, SIGNAL POLES WITH MAST ARMS AND POWER SUPPLY ASSEMBLY, AND THE CONTRACTOR SHALL INSTALL THE EQUIPMENT. CONTRACTOR SHALL COORDINATE WITH THE PUBLIC WORKS OPERATIONS DEPARTMENT (816-969-1870) FOR PICK-UP OF EQUIPMENT AT LEAST 48 HOURS PRIOR TO TRANSPORTATION. THE CONTRACTOR ASSUMES ALL DAMAGE LIABILITY AND SHOULD INSPECT ALL MATERIALS BEFORE AND AFTER TRANSPORTATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR SUPPLYING THE SIGNAL PEDESTAL POLES.

2. THE VIDEO DETECTION SYSTEM SHALL BE AN IN SYNC ADAPTIVE TRAFFIC CONTROL SYSTEM SUPPLIED BY RHYTHM ENGINEERING, 12351 W 96TH TERRACE, SUITE 107, LENEXA, KANSAS 66215, PHONE (913)227-0603. MATERIALS SUPPLIED BY RHYTHM ENGINEERING SHALL CONSIST OF AN IN SYNC PROCESSOR, VIDEO CAMERAS, TRANSFORMER, RG CABLE, DETECTOR CARDS AND CABLES, SITE EQUIPMENT PANELS, MONITOR, KEYBOARD AND CAMERA MOUNTING HARDWARE.

3. CABLES FOR THE VIDEO DETECTION SYSTEM SHALL BE ONE CONTINUOUS 3c--#14 CABLE AND ONE CONTINUOUS CATEGORY 5E+ CABLE (SHIELDED OUTDOOR RATED) BETWEEN THE CONTROLLER AND EACH CAMERA. THE 3c--#14 CABLE SHALL MEET REQUIREMENTS OF IMSA 20-1. CABLES ARE INCLUDED IN THE LUMP SUM PRICE FOR THE VIDEO DETECTION SYSTEM. NO ADDITIONAL PAYMENTS WILL BE MADE.

CONSTRUCTION SEQUENCING:

1. CONTRACTOR TO OBTAIN AND FOLLOW TEMPORARY TRAFFIC CONTROL PERMIT (TTCP) CONDITION REQUIREMENTS.

2. CONTRACTOR TO SUBMIT FOR APPROVAL BY CITY OF LEE'S SUMMIT TRAFFIC ENGINEER A DETAILED TRAFFIC SEQUENCING PHASING PLAN.

3. CONTRACTOR TO MAINTAIN EXISTING SIGNAL OPERATIONS UNTIL NEW SYSTEM, HAS BEEN TESTED AND OPERATIONAL.

4. PER TTCP, CONTRACTOR TO INSTALL TRAFFIC CONTROL, CHANNELIZATION DEVICES AND TEMPORARY PAVEMENT MARKING AROUND WORK ZONE.

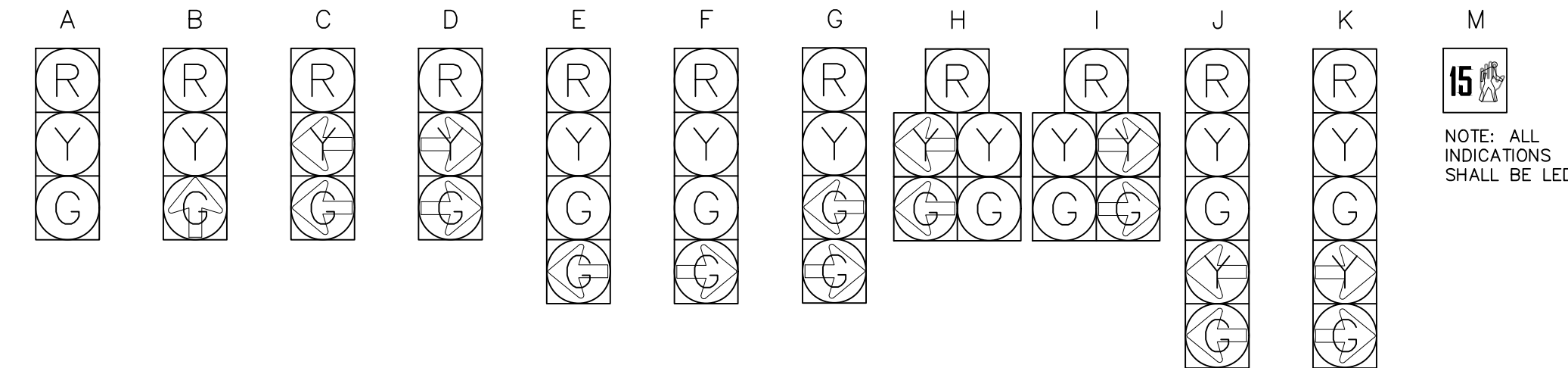
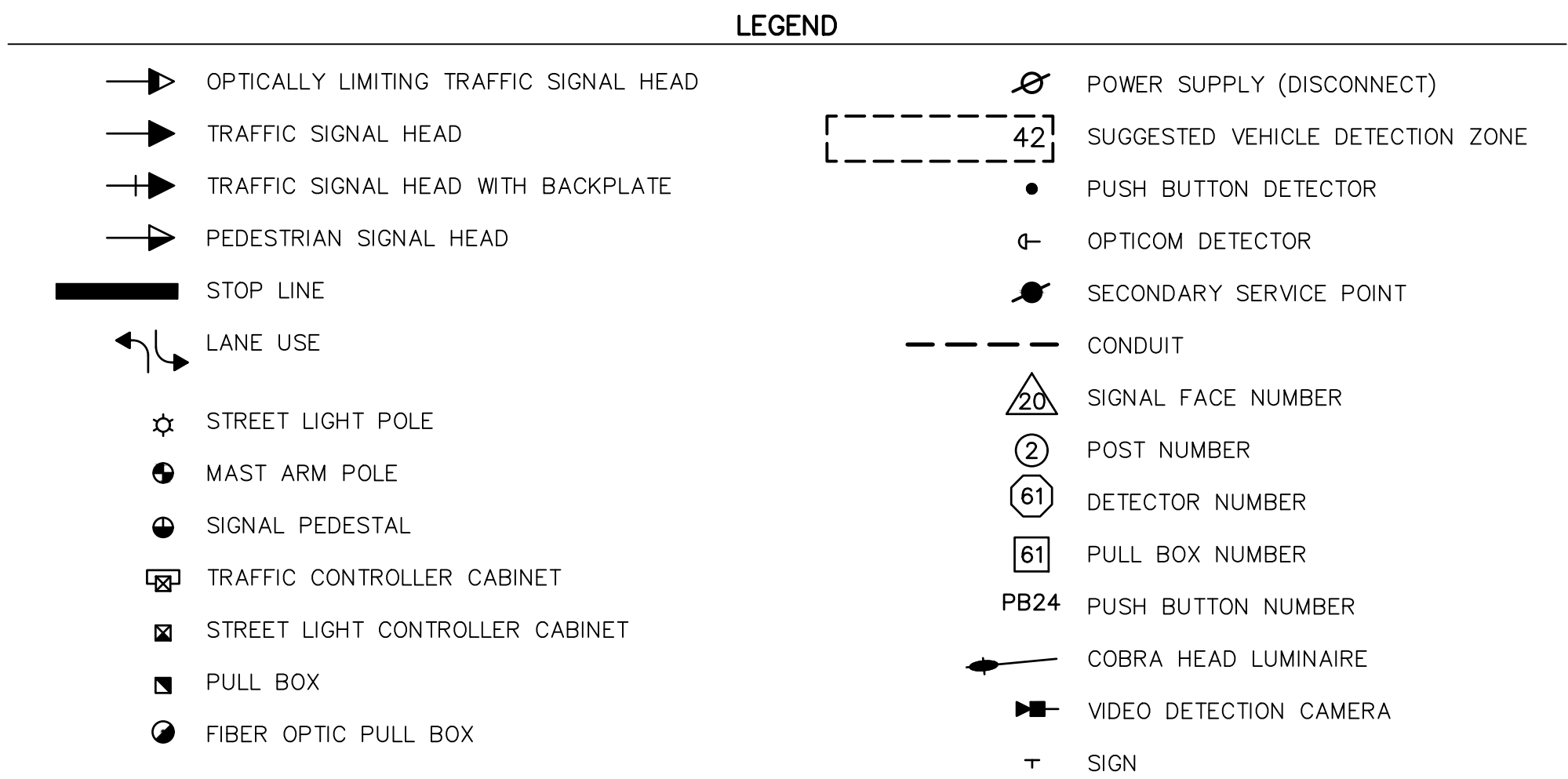
5. CONTRACTOR TO PROVIDE PEDESTRIAN DETOUR PLAN ALONG WITH TRAFFIC SEQUENCING PLAN.

6. CONTRACTOR TO INSTALL CONSTRUCTION FENCING AROUND AREAS WHERE PEDESTRIAN MOVEMENTS MAY CONFLICT WITH CONSTRUCTION ACTIVITIES.

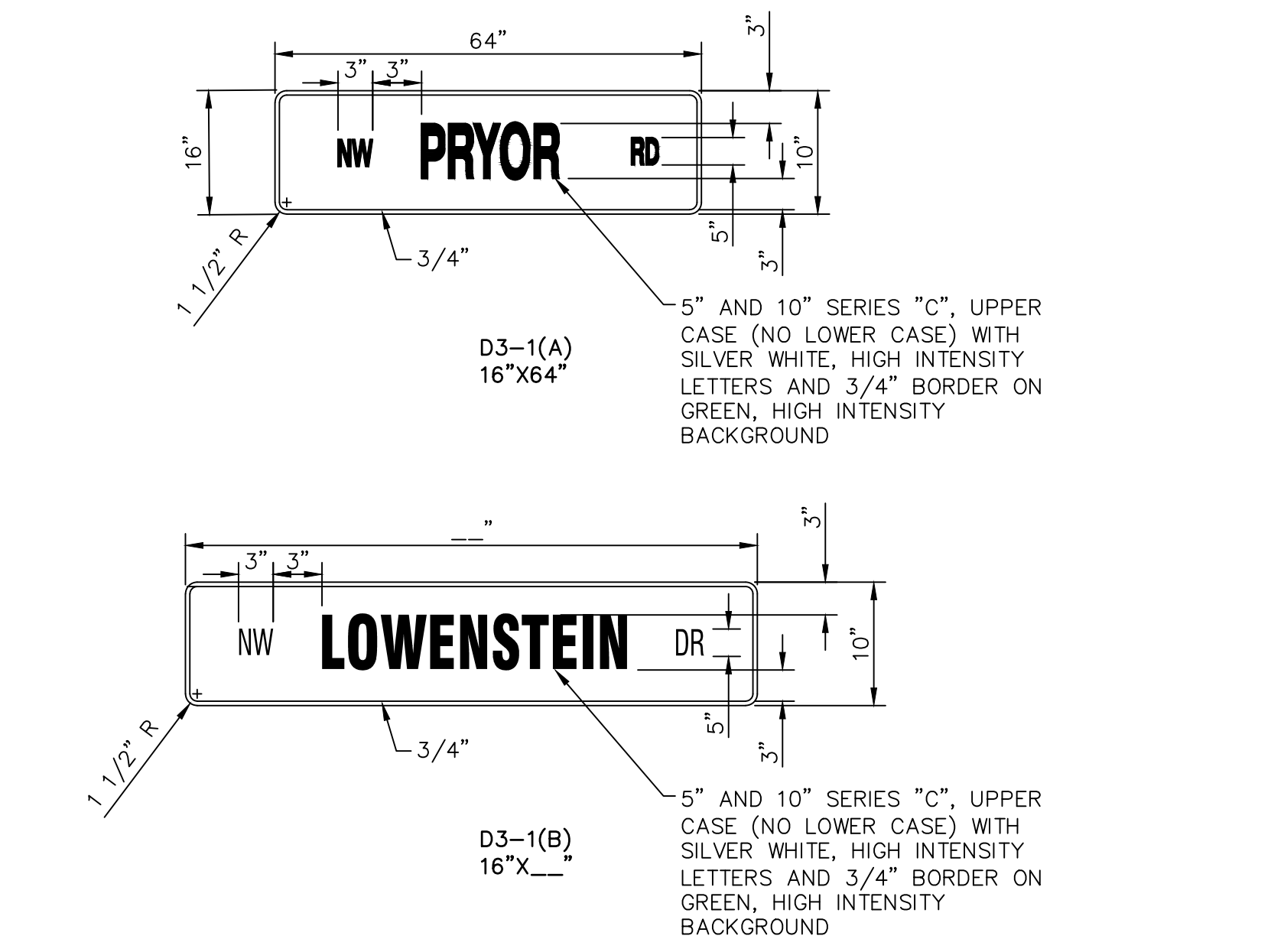
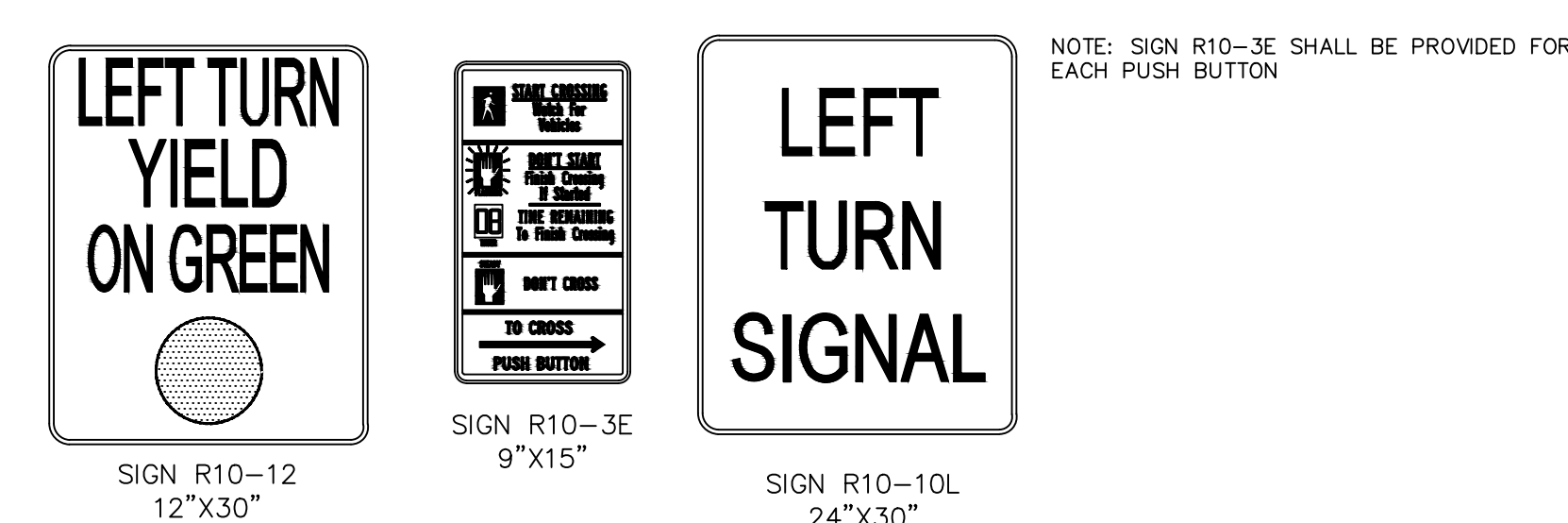
7. NO COLLECTOR OR ARTERIAL STREETS SHALL HAVE LANE(S) REDUCED OR CLOSED BEFORE 9 A.M. AND AFTER 4 P.M. MONDAY THRU FRIDAY. NO WORK ON SATURDAY, SUNDAYS OR HOLIDAYS OBSERVED BY THE CITY.

8. ALL TRAVEL LANES SHALL BE A MINIMUM OF 11.00 FEET WIDE EXCLUDING CURB AND GUTTER. ALL SIDEWALKS SHALL HAVE A MINIMUM CLEAR PATH OF 3.5 FEET. ALL MULTI-USE PATHS SHALL HAVE A MINIMUM CLEAR PATH OF 8.00 FEET.

9. CONTRACTOR SHOULD REVIEW TTCP AND MEET REQUIREMENTS PER CITY OF LEE'S SUMMIT REGULATIONS.



TYPICAL REGULATORY SIGN DETAIL

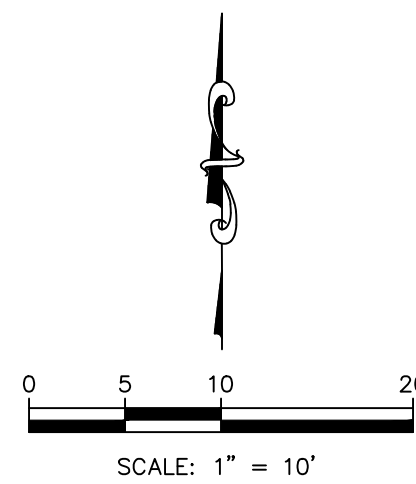






<b>STREETS OF WEST PRYOR</b> <b>NWQ NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		PROJ. NO. <b>A14-7067-1</b>	
		DESIGNER <b>LDO</b>	DRAWN BY <b>JT/BKR</b>
CWN	SHEET <b>18</b>	REV	2
<b>7067-1SIG_TSP</b>		<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b> <b>TRAFFIC SIGNAL PLAN - NW PRYOR RD &amp; NW LOWENSTEIN DR</b>	





2319 N. JACKSON | P.O. BOX 1304  
JUNCTION CITY, KANSAS 66441  
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js@k-veg.com | www.k-veg.com



PROJ. NO.	
A14_7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
7067-1SIG_WIRE	
SHEET	REV
19	2

FR1	$\phi 1$	$\phi 2$	PED $\phi 2$	$\phi 3$	$\phi 4$	PED $\phi 4$	MONITOR
FR2							
FR1	$\phi 5$	$\phi 6$	PED $\phi 6$	$\phi 7$	$\phi 8$	PED $\phi 8$	
FR2							

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[illegible]

\* SIGNAL HEAD SPACING (FEET) MAY BE ADJUSTED TO SITE CONDITIONS AS APPROVED BY THE PROJECT INSPECTOR.

[illegible]

P - POLE MOUNTED SIGNAL HEAD

M – MAST ARM MOUNTED SIGNAL HEAD

S - SPAN WIRE MOUNTED SIGNAL HEAD

NOTE:

1. QUANTITIES SHOWN HEREON ARE FOR REFERENCE ONLY.

2. ALIGN FACES OF PEDESTRIAN PUSHBUTTONS PARALLEL TO CROSS-WALK.

3. ALL PULL BOXES LOCATED IN TRAVEL WAY SHALL BE TRAFFIC RATED.

4. THE TRAFFIC SIGNAL SYSTEM SHALL BE COMPLETE AND THE CONTRACTOR SHALL FURNISH AND INSTALL ALL EQUIPMENT AND MATERIALS NECESSARY FOR THE SATISFACTORY OPERATION OF ELECTRICAL APPARATUS AND FOR COMPLETE OPERATION OF THE TRAFFIC SIGNAL SYSTEM WHETHER SPECIFICALLY MENTIONED OR NOT.

[illegible]

CONTROLLER AND EQUIPMENT	TOTALS
CABINET AND ACCESSORIES: NEMA TYPE P TS1 CABINET	1
CONTROLLER: TYPE 3608 M 52 EAGLE EPAC COMPLETE PER PLANS, INCLUDING SOFTWARE	1
EMERGENCY VEHICLE DETECTION SYSTEM (COMPLETE): 3M	2
VIDEO DETECTION SYSTEM (COMPLETE): INSYNC ADAPTIVE TRAFFIC CONTROL SYSTEM	1
FIBER OPTIC DATA LINK SWITCH: KYLAND SICOM 3170	1
1-CIRCUIT POWER SUPPLY (UTILITY ENCLOSURE PEDESTAL	1
GROUND RODS	9
PUSHBUTTON DETECTORS	6

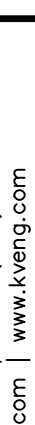

CABLE														
FROM	TO	GROUND	POWER/LIGHTING				CONTROL TYPE 20-1			LEAD-IN CABLE	EMERGENCY PRE-EMPTION CABLE *	VIDEO CABLE*		REMARKS
		#6	3c-8	2c-4			2c-14	5c-14	7c-14	2c-14	m-138	3c#14	CAT5e+	
<input checked="" type="checkbox"/>	71								243					
<input checked="" type="checkbox"/>	C1											255	255	
<input checked="" type="checkbox"/>	41								229					
<input checked="" type="checkbox"/>	42								212					
<input checked="" type="checkbox"/>	26							212						
<input checked="" type="checkbox"/>	11								417					
<input checked="" type="checkbox"/>	61								404					
61	62							12						JUMPER
<input checked="" type="checkbox"/>	C2											406	406	
<input checked="" type="checkbox"/>	EV2										380			
<input checked="" type="checkbox"/>	62								390					
<input checked="" type="checkbox"/>	63								376					
<input checked="" type="checkbox"/>	64								354					
<input checked="" type="checkbox"/>	65						344	344						
<input checked="" type="checkbox"/>	31								498					
31	32							12						JUMPER
<input checked="" type="checkbox"/>	32								484					
<input checked="" type="checkbox"/>	C3											497	497	
<input checked="" type="checkbox"/>	81								470					
<input checked="" type="checkbox"/>	EV3										453			
<input checked="" type="checkbox"/>	82								445					
<input checked="" type="checkbox"/>	83						467	467						
<input checked="" type="checkbox"/>	66						463	463						
<input checked="" type="checkbox"/>	51								163					
<input checked="" type="checkbox"/>	21								147					
21	22							12						JUMPER
22	23							8						JUMPER
<input checked="" type="checkbox"/>	C4											147	147	
<input checked="" type="checkbox"/>	22								133					
<input checked="" type="checkbox"/>	EV4										81			
<input checked="" type="checkbox"/>	23								124					
<input checked="" type="checkbox"/>	24								100					
<input checked="" type="checkbox"/>	84						79	79						
<input checked="" type="checkbox"/>	25						79	79						
<input checked="" type="checkbox"/>	(1)			202										
<input checked="" type="checkbox"/>	(2)			344										
<input checked="" type="checkbox"/>	(3)			445										
<input checked="" type="checkbox"/>	(4)			100										
<input checked="" type="checkbox"/>	(5)													
<input checked="" type="checkbox"/>	(6)						79	180						
<input checked="" type="checkbox"/>	4		34											
<input checked="" type="checkbox"/>	X		35											
<input checked="" type="checkbox"/>	X		35											

\* CABLE INCLUDED IN LUMP SUM PRICE FOR THE SYSTEM.

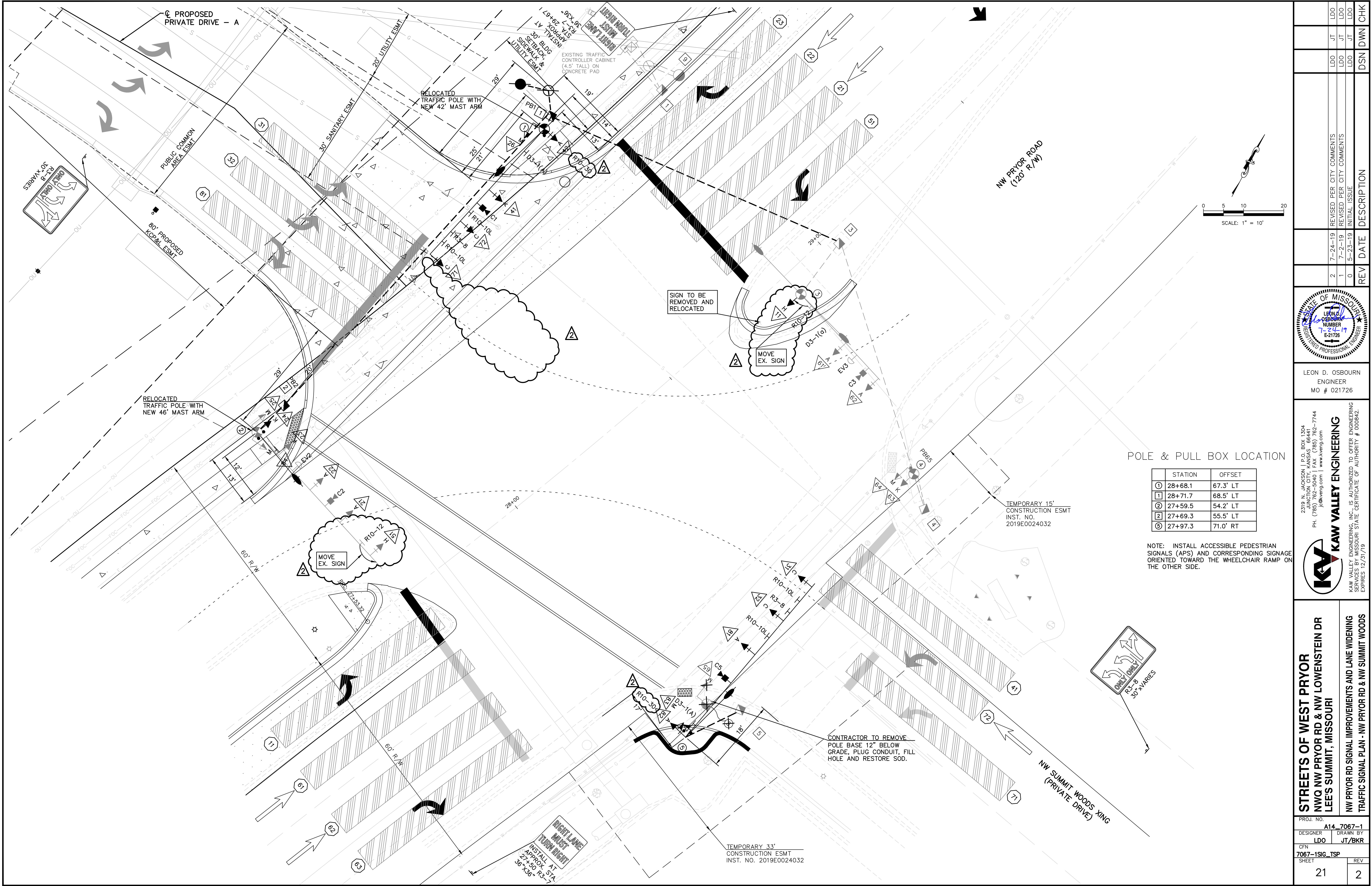
SIGNAL CONDUIT				
FROM	TO	3" PVC	2" HDPE (GREY)	4" HDPE
		5		
			5	
	[4]		24	
[X]	[1]			114
[1]	(1)			22
[1]	[2]			276
[1]	[4]			89
[1]	(6)		18	
[2]	(2)			5
[2]	[3]			81
[3]	(3)			5
[4]	(4)			23
[4]	(5)		15	

SIGNS				
SIGN	LEGEND	NO.	SF.	TOTAL S.F.
R10-10L	LEFT TURN SIGNAL	2	5.0	10.0
R10-10R				
R10-12	LT TURN YIELD ON GREEN	3	5.0	15.0
R10-3E				
<del>R10-30</del>	<del>RT OVRMY TUT</del>	<del>1</del>	<del>7.5</del>	<del>7.5</del>
R3-7L	LT LANE MUST TURN LT	3	5.0	15.0
R3-7R	RT LANE MUST TURN RT	2	5.0	10.0
R3-8	VARIES	3	10.0	30.0
D3-1(A)	NW PRYOR RD	2		
D3-1(B)	LOWENSTEIN DR.	2		
TOTAL				87.5

## EXTENSIVE REVISIONS

STREETS OF WEST PRYOR NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI		PROJ. NO.  A14-7067-1	DESIGNER LDO		DRAWN BY JT/BKR		CFN SHEET	20	2	REV	7067-1SIG_QTY												
					KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19		2319 N. JACKSON   P.O. BOX 1304 JACKSON, MO 64501   FAX: (785) 765-7744 PH: (785) 702-5040   FAX: (785) 762-7744 jcd@kveg.com   www.kveg.com		LEON D. OSBORN ENGINEER MO # 021726														
												2		7-24-19		REVISED PER CITY COMMENTS		LDO		JTD		LDO	
												1		7-2-19		REVISED PER CITY COMMENTS		LDO		JTD		LDO	
												0		5-23-19		INITIAL ISSUE		LDO		JTD		LDO	
												REV		DATE		DESCRIPTION		DSN		DWN		CHK	





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REV	DATE	DESCRIPTION	DSN	DWN	CHK
2	7-24-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
1	7-2-19	REVISED PER CITY COMMENTS	LDO	JT	LDO
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO

LEON D. OSBOURN  
ENGINEER  
MO # 021726

2319 N. JACKSON | P.O. BOX 1304  
JUNCTION CITY, KANSAS 66441  
PH. (785) 762-5040 | FAX (785) 762-7744  
joe@kve.com | www.kve.com

**KAW VALLEY ENGINEERING**

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19.

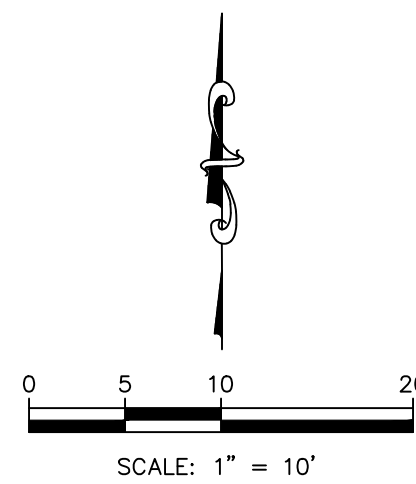
**STREETS OF WEST PRYOR**  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI



NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
TRAFFIC SIGNAL PLAN - NW PRYOR RD & NW SUMMIT WOODS

PROJ. NO.	DESIGNER	DRAWN BY	CFN	SHEET
A14_7067-1	LDO	JT/BKR	7067-1SIG_TSP	21

REV	DESCRIPTION
2	



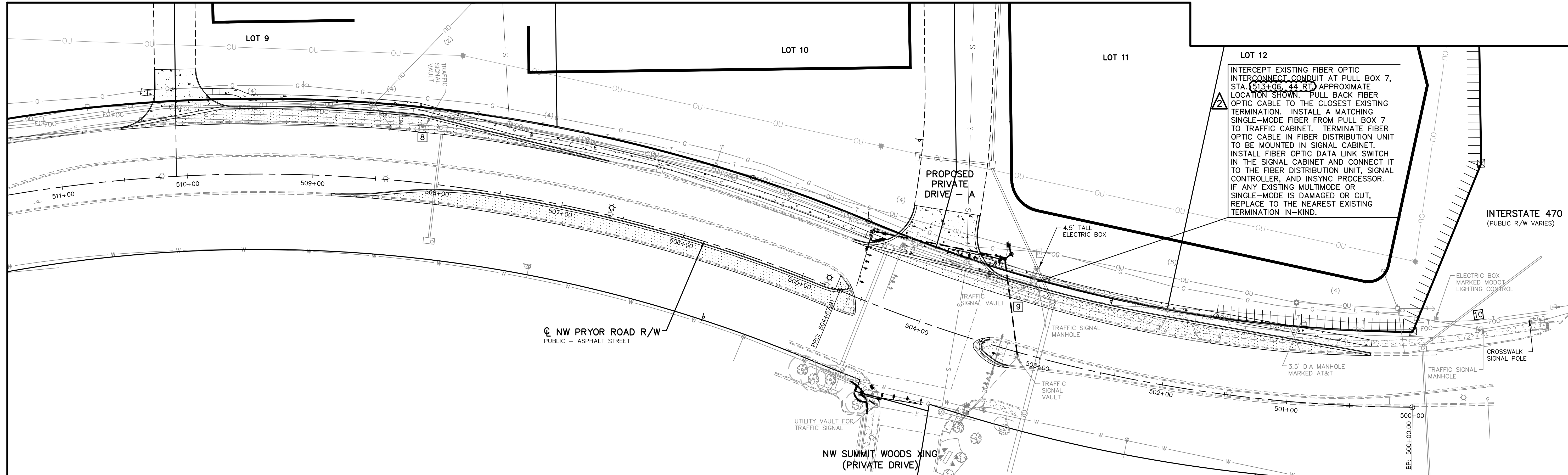
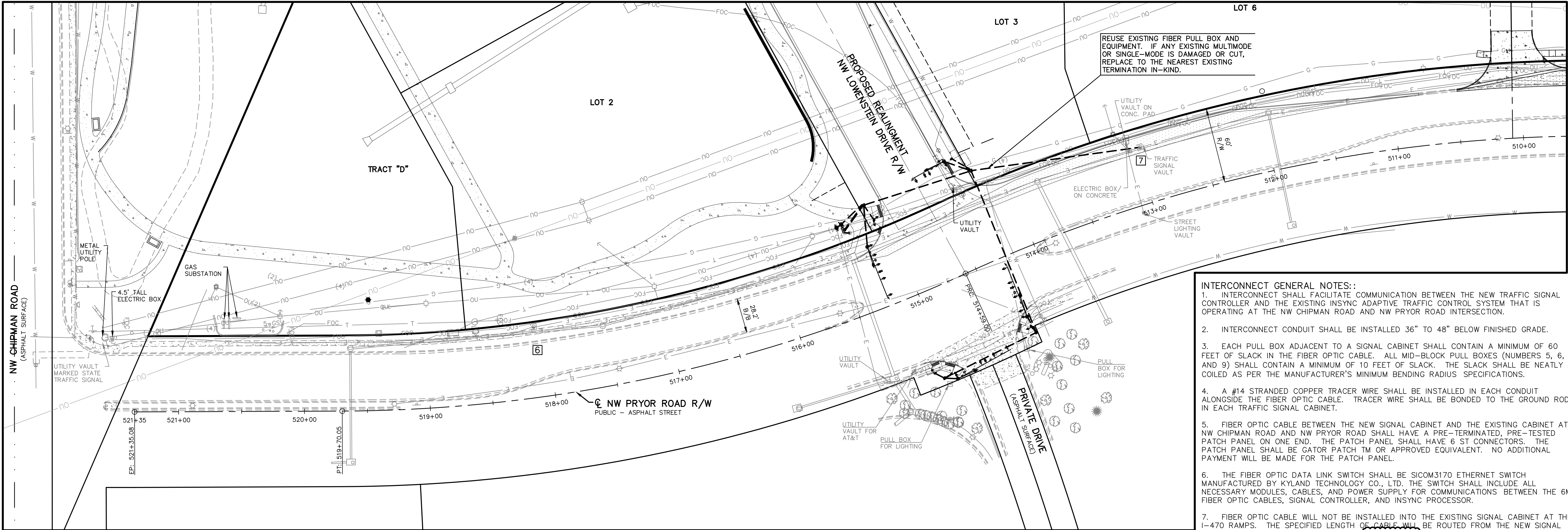


<b>STREETS OF WEST PRYOR</b> <b>NW/4 NW PRYOR RD &amp; NW LOWENSTEIN DR</b> <b>LEE'S SUMMIT, MISSOURI</b>		 <b>KAW VALLEY ENGINEERING</b> KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19		2319 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX. (785) 762-7744 <a href="http://jcekveeng.com">jcekveeng.com</a>   <a href="http://www.kveeng.com">www.kveeng.com</a>							
<b>NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING</b> <b>TRAFFIC SIGNAL WIRE DIAGRAM - NW PRYOR RD &amp; NW SUMMIT WOODS</b>											
PROJ. NO. <b>A14-7067-1</b>		DESIGNER <b>LDO</b>		DRAWN BY <b>JT/BKR</b>							
CFN											
SHEET <b>22</b>		<b>7067-1SIG_WJ</b>		REV							
				<b>2</b>							









- INTERCONNECT GENERAL NOTES:**
- INTERCONNECT SHALL FACILITATE COMMUNICATION BETWEEN THE NEW TRAFFIC SIGNAL CONTROLLER AND THE EXISTING INSYNC ADAPTIVE TRAFFIC CONTROL SYSTEM THAT IS OPERATING AT THE NW CHIPMAN ROAD AND NW PRYOR ROAD INTERSECTION.
  - INTERCONNECT CONDUIT SHALL BE INSTALLED 36" TO 48" BELOW FINISHED GRADE.
  - EACH PULL BOX ADJACENT TO A SIGNAL CABINET SHALL CONTAIN A MINIMUM OF 60 FEET OF SLACK IN THE FIBER OPTIC CABLE. ALL MID-BLOCK PULL BOXES (NUMBERS 5, 6, 7, AND 9) SHALL CONTAIN A MINIMUM OF 10 FEET OF SLACK. THE SLACK SHALL BE NEATLY COILED AS PER THE MANUFACTURER'S MINIMUM BENDING RADIUS SPECIFICATIONS.
  - A #14 STRANDED COPPER TRACER WIRE SHALL BE INSTALLED IN EACH CONDUIT ALONGSIDE THE FIBER OPTIC CABLE. TRACER WIRE SHALL BE BONDED TO THE GROUND ROD IN EACH TRAFFIC SIGNAL CABINET.
  - FIBER OPTIC CABLE BETWEEN THE NEW SIGNAL CABINET AND THE EXISTING CABINET AT NW CHIPMAN ROAD AND NW PRYOR ROAD SHALL HAVE A PRE-TERMINATED, PRE-TESTED PATCH PANEL ON ONE END. THE PATCH PANEL SHALL HAVE 6 ST CONNECTORS. THE PATCH PANEL SHALL BE GATOR PATCH TM OR APPROVED EQUIVALENT. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE PATCH PANEL.
  - THE FIBER OPTIC DATA LINK SWITCH SHALL BE SICOM3170 ETHERNET SWITCH MANUFACTURED BY KYLAND TECHNOLOGY CO., LTD. THE SWITCH SHALL INCLUDE ALL NECESSARY MODULES, CABLES, AND POWER SUPPLY FOR COMMUNICATIONS BETWEEN THE 6MM FIBER OPTIC CABLES, SIGNAL CONTROLLER, AND INSYNC PROCESSOR.
  - FIBER OPTIC CABLE WILL NOT BE INSTALLED INTO THE EXISTING SIGNAL CABINET AT THE I-470 RAMPS. THE SPECIFIED LENGTH OF CABLE WILL BE ROUTED FROM THE NEW SIGNAL CABINET TO THE EXISTING PULL BOX AT STA. 499+44. THE CABLE WILL BE NEATLY COILED AND END CAPPED. MODOT WILL INSTALL THE FIBER INTO THE SIGNAL CABINET AT A LATER DATE.
  - A MINIMUM OF NINE HOOKS, INSTALLED IN THREE LEVELS, SHALL BE INCLUDED IN EACH FIBER OPTIC PULL BOX.

2	7-24-19	REVISED PER CITY COMMENTS	LD0	JT	LD0
1	7-2-19	REVISED PER CITY COMMENTS	LD0	JT	LD0
0	5-23-19	INITIAL ISSUE	LD0	JT	LD0
REV	DATE	DESCRIPTION	DSN	DWN	CHK

LEON D. OSBOURN  
ENGINEER  
MO # 021726

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**KAW VALLEY ENGINEERING**  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19.

**STREETS OF WEST PRYOR**  
NW Q NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

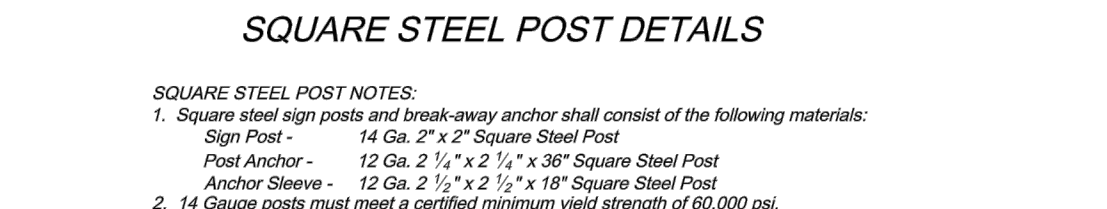
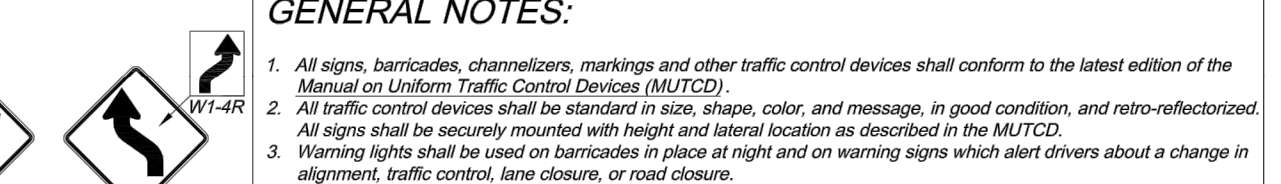
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
TRAFFIC INTERCONNECT PLAN, NOTES AND WIRING

PROJ. NO.	A14-7067-1
DESIGNER	LD0
DRAWN BY	JT/BKR
CFN	7067-1SIC_CONN
SHEET	24
REV	2







[illegible]

**GENERAL NOTES:**

- All signs, barricades, channelizers, markings and other traffic control devices shall conform to the latest edition of the **Manual on Uniform Traffic Control Devices (MUTCD)**.
- All traffic control devices shall be standard in size, shape, color, and message, in good condition, and retro-reflectized. All signs shall be securely mounted on hinged & lateral location as described in the MUTCD.
- Warning flags shall be used on barricades in place at night and on warning signs which alert drivers about a change in alignment, traffic control, lane closure, or road closure.
- Flippers that be used where indicated on the plans, where construction vehicles interact with normal traffic, or where construction activities impose a restriction on traffic, as directed by the City Traffic Engineer. Where flippers are used, advance signing shall be erected as shown in the details or as specified in the MUTCD. Flippers shall meet the requirements in the MUTCD as regard to character, training, alert, and behavior.
- Trucks are the City's preferred construction vehicles. Cones may not be used at nighttime.
- Traffic control devices not in use or not applicable shall be either covered or removed from the work area.
- The Contractor shall use barricades, street plates, or oranges as needed to effectively shield pedestrian and vehicular traffic from exposed objects, excavations, and construction activities.
- Access shall be maintained to all driveways and side streets unless otherwise noted or otherwise on the plans.
- No street shall be closed without the approval of the City Traffic Engineer. The Contractor shall notify the City Traffic Engineer at least 7 days in advance of any street closure. If a detour route around the closure is to be provided, all detour signing shall be in place on or a plan approved by the City Traffic Engineer.
- Construction vehicles parked along streets shall be located within the work area (traffic control) or where otherwise normally permitted. Construction materials, including traffic control devices and vehicles shall not restrict sight distance for vehicles exiting at streets or drives.
- Construction materials shall be kept off of sidewalks, consolidated in one location within City right-of-way, and removed daily unless otherwise approved by the Inspector. Dirt, mud, and other construction debris on streets and sidewalks shall be removed immediately.
- The Contractor shall perform any work that will restrict vehicular travel in any way between the hours of 7:00 a.m. and 9:00 a.m. on 4/00 p.m. and 6:00 p.m. Monday through Friday unless otherwise indicated in the specifications.
- All travel lanes shall be at least 11 feet wide unless otherwise authorized by the City Traffic Engineer. A "Narrow Lane" sign shall be installed in advance of a lane width reduction to less than 11 feet.
- All edge drop-offs or more than 2 inches and less than 4 inches should be protected by a wedge or barrier and all edge drop-offs greater than 4 inches shall have edge protection (see City Traffic Control Specifications for edge treatment requirements).
- The "Workers" symbolic sign (MUTCD No. W252-1a) may be used instead of the "Road Work Ahead" sign for work with a duration of 2 hours or less. The "End Road Work" sign is not required to be installed after the "Workers" sign. No traffic sign shall be erected or modified in any way without a plan approved by the City Traffic Engineer.
- The Contractor shall be responsible for maintaining all traffic control devices on the work basic, whether or not work is actively being pursued and any deficiencies noted shall be corrected immediately.
- The traffic control requirements shown on these plans are minimum requirements only and do not attempt to address all instances that may arise once construction has started. In no way do the requirements shown on these plans relieve the Contractor of his responsibility for selecting the proper traffic control devices and the methods that will be used to safely guide vehicles, pedestrians and workers at all times.
- Should the contractor fail to enforce the traffic control plan to fall in place, clean, replace or otherwise maintain the traffic control devices when directed to do so by the City Traffic Engineer or representative, the City may take one or more of the following actions:

- Employ another agency to correct deficiencies in traffic control devices and deduct the cost from the estimate.
- Stop the job until deficiencies are corrected.
- Suspend all pay estimates until deficiencies are corrected, or
- Place the Contractor in default.

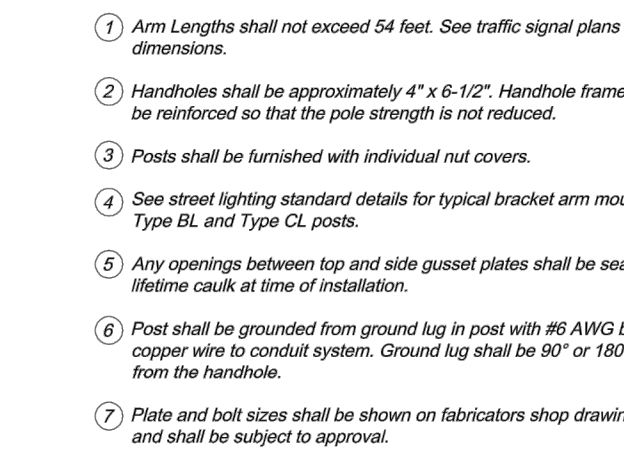
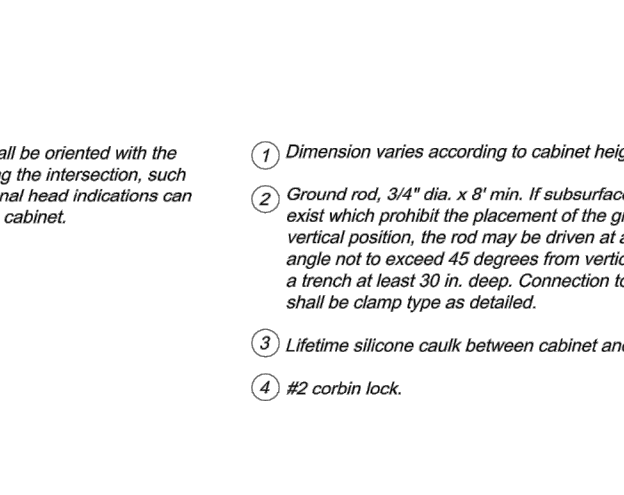
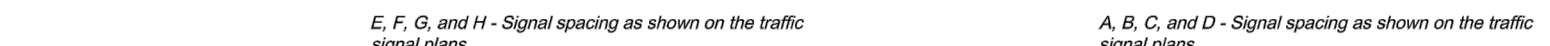
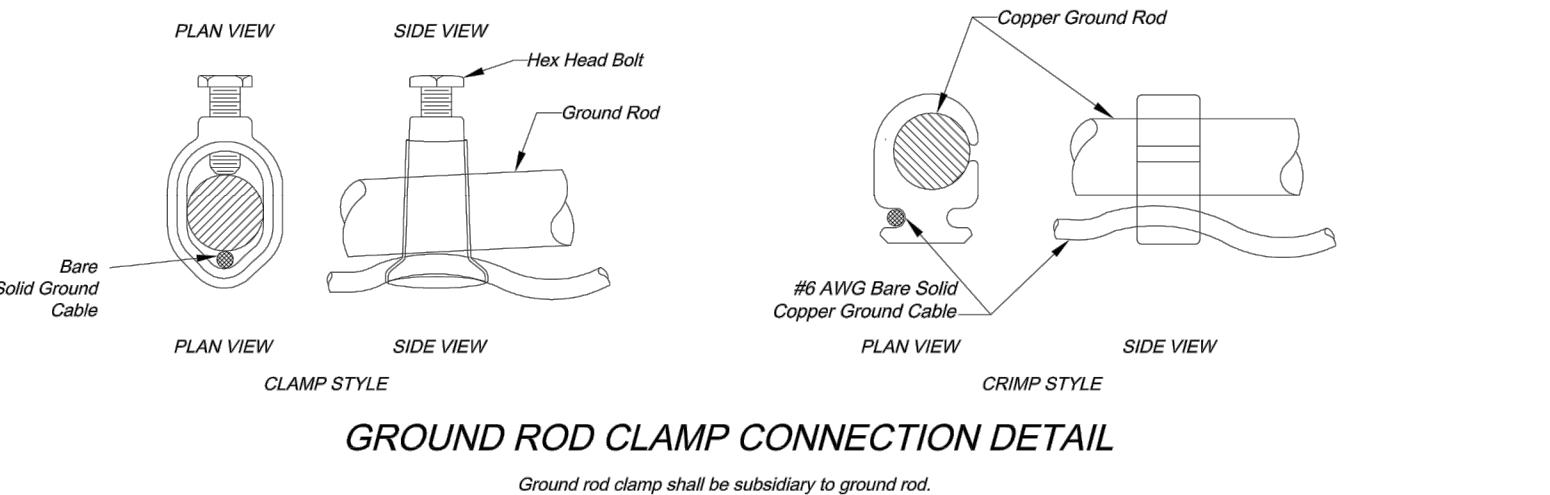
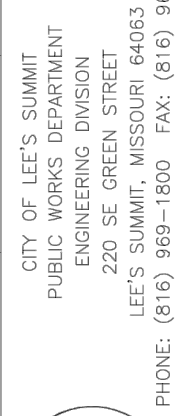
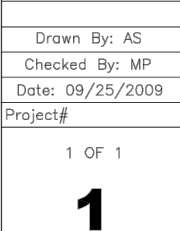
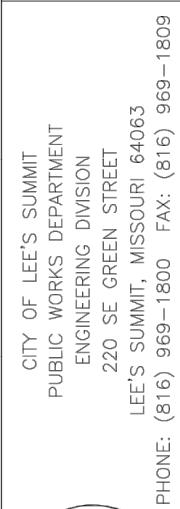








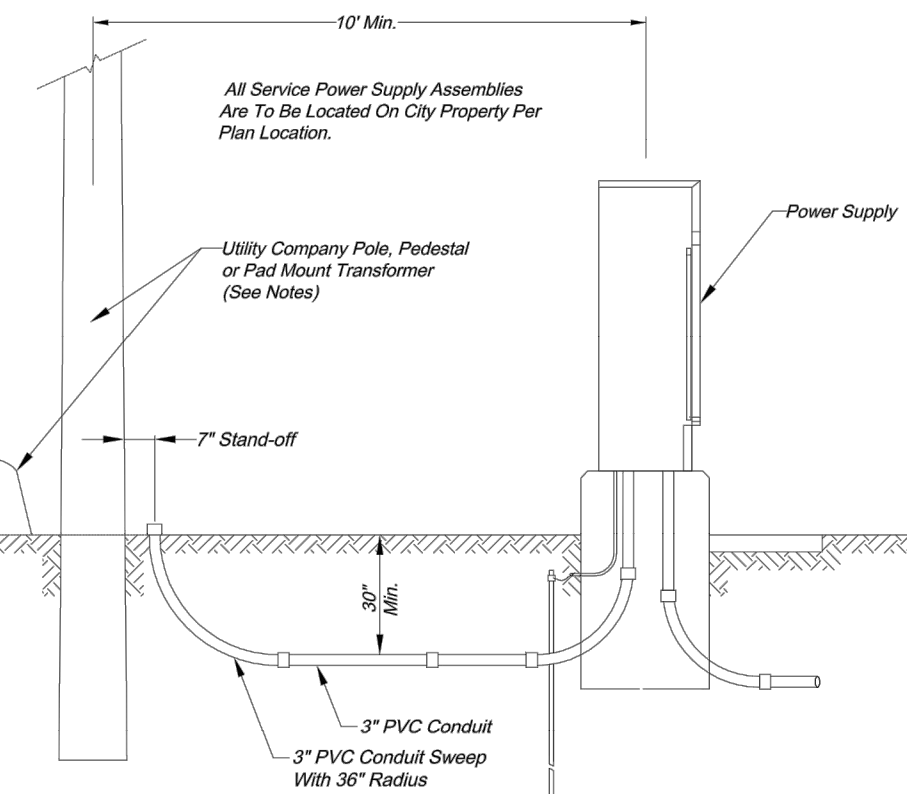
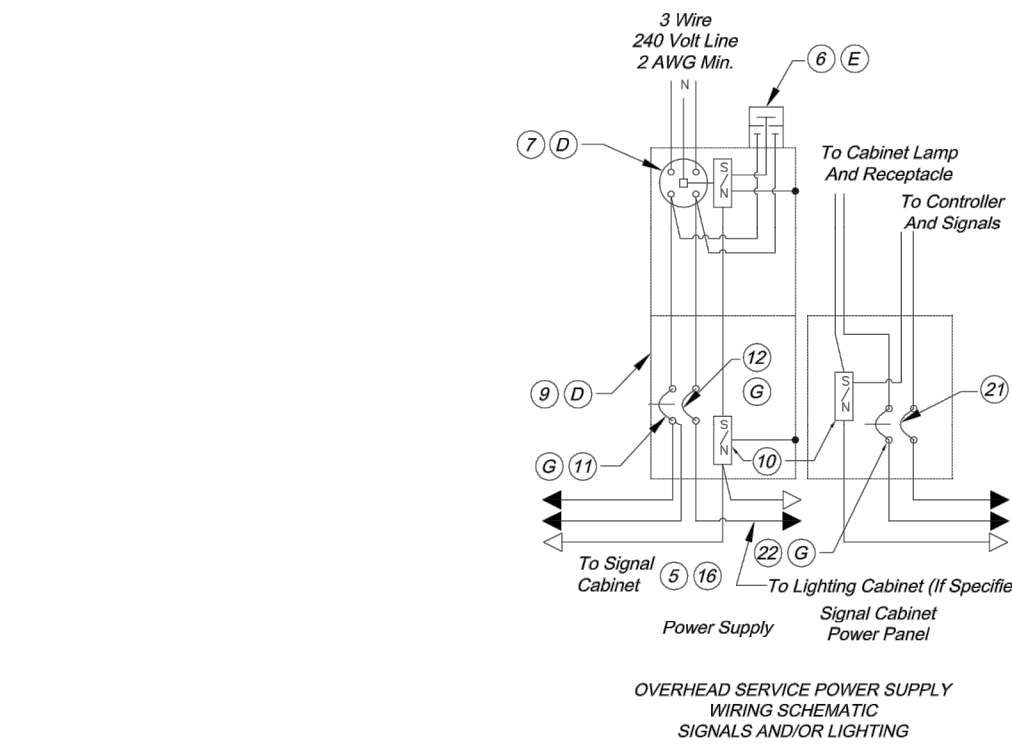




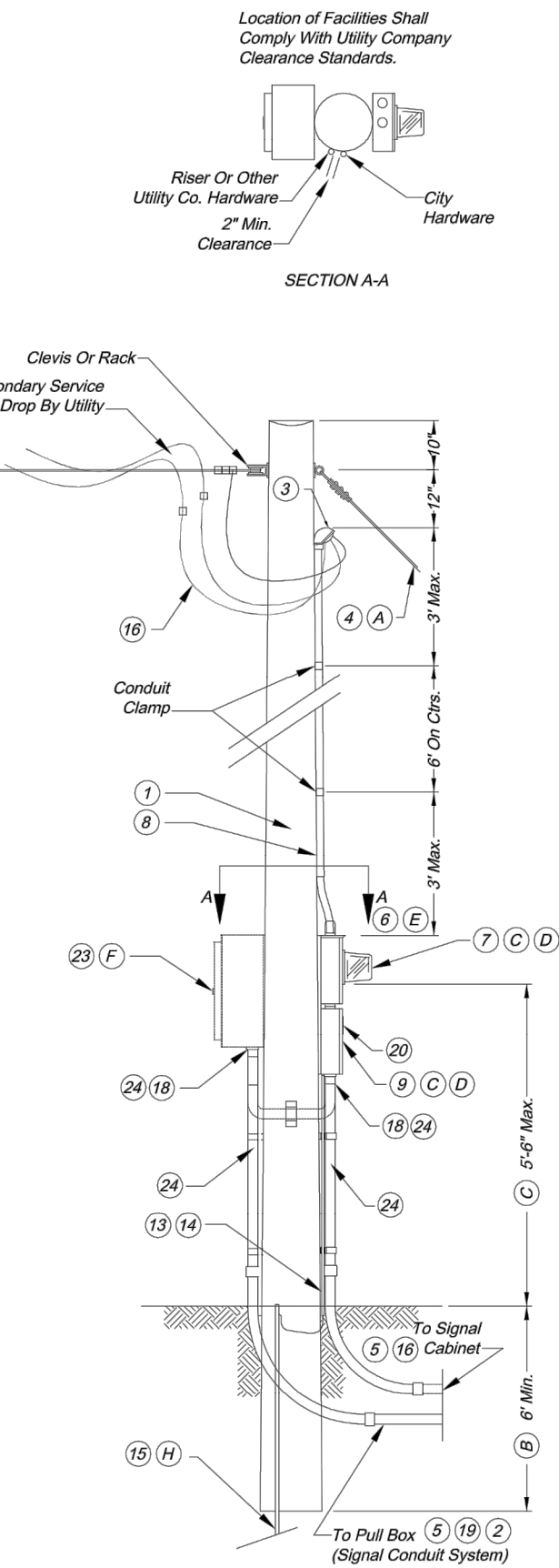








NOTES:  
1. Contractor shall install a conduit stub 24" to 6" above ground at utility poles. Conduit shall be stubbed to the side of the pole that will allow a direct run up the pole to the transformer without crossing other utility lines or cables. The end of the conduit shall be capped.  
2. Contractor shall install conduit in a trench to within 24" of pedestals or pad mount transformers and leave a 36" x 36" x 36" access hole in the ground. Contractor shall keep open trench covered and promptly backfill access hole when service is completed.



OVERHEAD SERVICE POWER SUPPLY  
TEMPORARY SIGNAL ONLY

LIST OF MATERIALS	
Item	Description
1	Service pole 30' min., Class IV wood, Contractor provided, City owned. *
2	#8 AWG Min. Cable, 600 volt *
3	Service entrance head
4	Guy cable, as required
5	2" min. rigid conduit with preformed elbows
6	Lightning arrester, Valve type, 2 pole, 650 volt
7	240 volt Meter socket, 100 amp for signals
8	2" min. rigid conduit
9	Service disconnect box, Locking, Rainlight, NEMA 4
10	Insulated, Groundable neutral, 300 amp minimum
11	Signal breaker, Single pole, 40 amp min., Type A or B
12	Lighting breaker, Single pole, 40 amp, Type A or B
13	Metal conduit, 1/2"
14	Ground wire, #2 AWG min.
15	Ground rod, 3/4" x 6' min.
16	#2 AWG min. cable, 600 volt
17	Reserved
18	Threaded conduit hub with sealing washers
19	Lighting cables *
20	Weatherproof adhesive label (signals) vinyl raised lettering
21	Type B controller and signal breaker, as specified.
22	Type B auxiliary breaker, 15 amp
23	Lighting control cabinet
24	2" Steel Conduit (minimum)

- Notes
- (A) Service pole shall be guyed when span of overhead wire exceeds 50'.  
(B) Increase 1 foot for each 5 feet above 50 feet.  
(C) Service disconnect boxes and meter boxes shall be aluminum or stainless steel. All hardware, hinges, catches, etc. shall be stainless steel. Meter socket and other equipment shall be U.L. approved, and conform to the requirements of the utility company providing power.  
(D) Schematic diagram shall be mounted on inside of door.  
(E) Utility company shall decide if lightning arresters are to be connected on the load or line side of the meter. The utility company shall also decide if the lightning arrester is terminated in the meter or disconnect cabinet. If terminated in the disconnect cabinet, it shall be installed on the connect cabinet.  
(F) If lighting is specified, install lighting control on power supply.  
(G) Breakers shall conform to the standard specifications.  
(H) If subsurface conditions exist which prohibit the placement of the ground rod in vertical position, the rod may be driven at an oblique angle not to exceed 45 degrees from vertical or buried in a trench at least 30 in. deep. Connection to ground rod shall be clamp type as detailed on standard drawing TS-2.

General Notes:  
For cable types and installation. See standard specifications.  
The type power supply assembly is shown on the plans or is designated on the contract.  
The utility company shall be notified 30 days prior to date service will be required.  
All openings in any utility enclosure, service box, or meter shall be covered and sealed with lifetime silicone caulk.  
Contractor to provide sufficient number of ground rod(s) as required for maximum of 25 Ohms resistance to ground.  
All materials required excluding reference items as shown on drawing shall be included in price bid for power supply assembly.

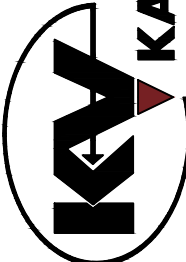
CITY OF LEE'S SUMMIT  
PLANNING DEPARTMENT  
ENGINEERING DIVISION  
220 SE GREEN STREET  
LEE'S SUMMIT, MISSOURI 64083  
PHONE: (816) 885-1600 FAX: (816) 885-1605

POWER SUPPLY ASSEMBLY  
240/120 VOLT SERVICE  
STANDARD DRAWING TS-9

Drawn By: AS  
Checked By: MP  
Date: 09/25/2008  
Project#:

1

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KAW VALLEY ENGINEERING  
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING  
DESIGN SERVICES UNDER MISSOURI STATE CERTIFICATE OF AUTHORITY # 000842.  
EXPIRES 12/31/19

STREETS OF WEST PRYOR  
NW/Q NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
TRAFFIC SIGNAL STANDARD CITY DETAILS

PROJ. NO. A14-7067-1  
DESIGNER LDO DRAWN BY JT/BKR  
CFN  
7067-1SIG\_DET  
SHEET 32 REV 1

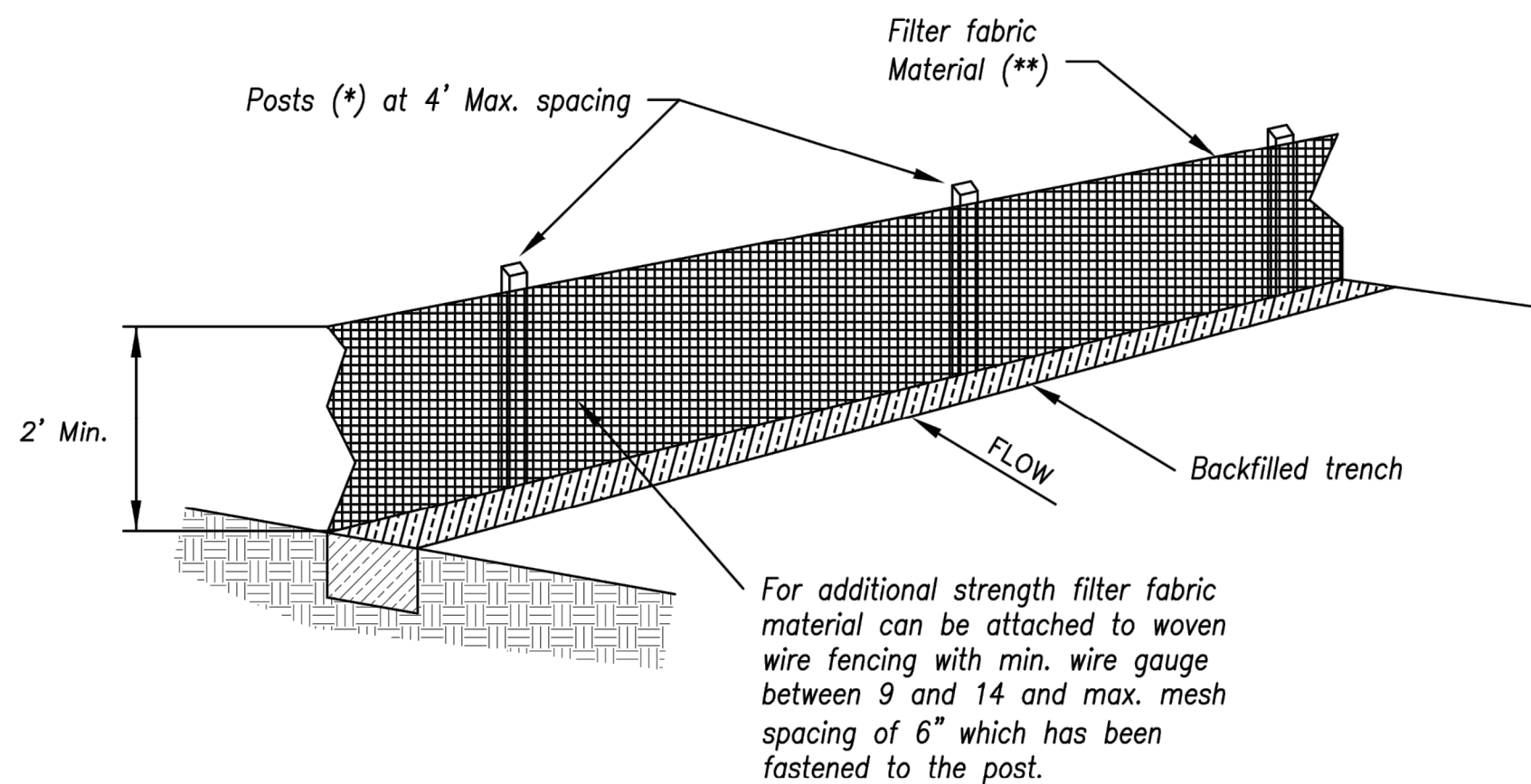


LEON D. OSBOURN  
ENGINEER  
MO # 021726

REV	DATE	DESCRIPTION	REV	DATE	DESCRIPTION
1	(DATE)	REVISED PER CITY COMMENTS	LDO	JT	LDO
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO

DSN DWN CHK





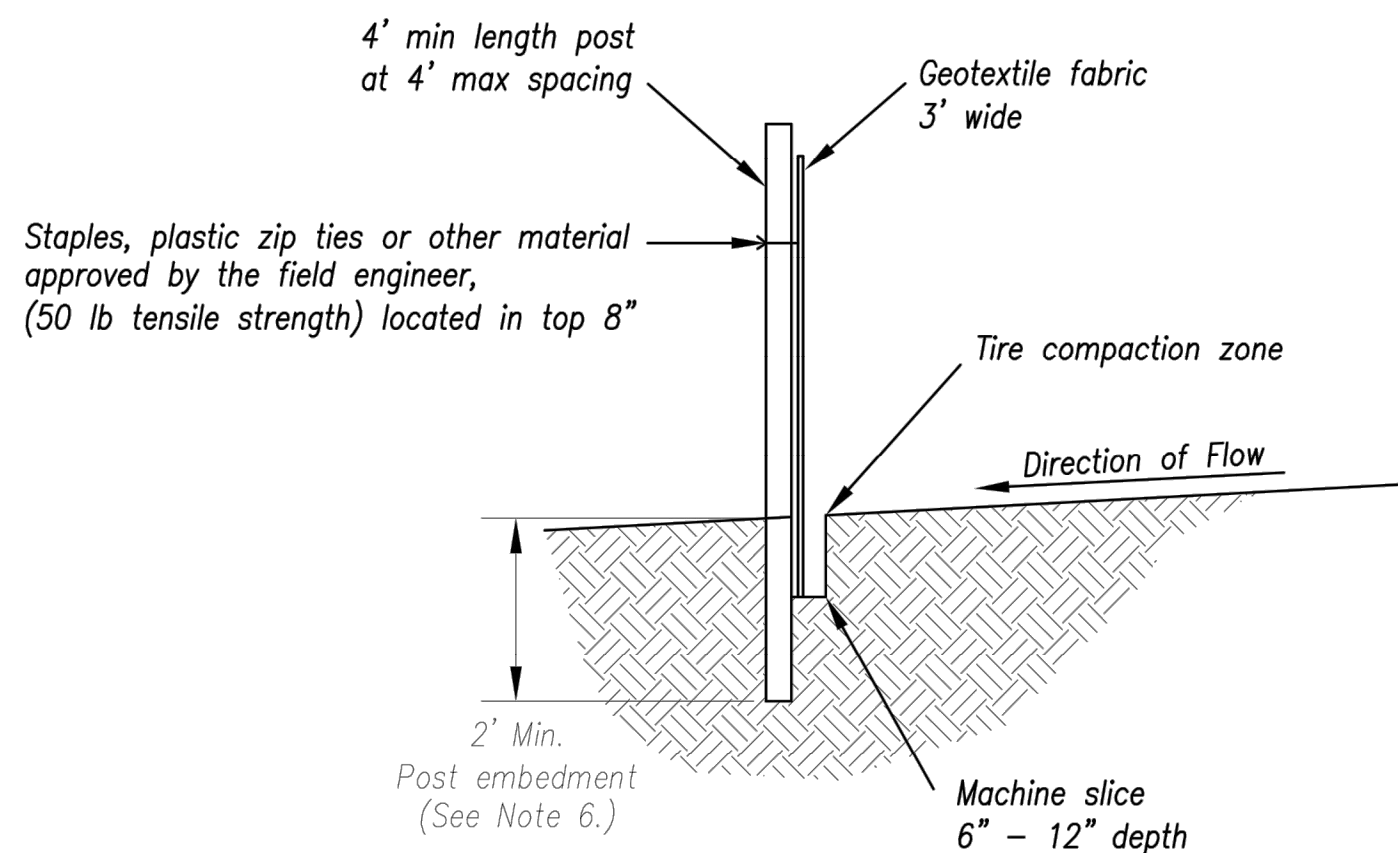
(\*) **POSTS**

- MIN. LENGTH 4'
- HARDWOOD 1 3/8" x 1 3/8"
- NO.2 SOUTHERN PINE 2 5/8" x 2 5/8"
- STEEL 1.33 LB/FT

(\*\*) - Geotextile Fabric shall meet the requirements of AASHTO M288

**SILT FENCE DETAILS**

Not to Scale



**Notes:**

1. In order to contain water, the ends of the silt fence must be turned uphill (Figure A).
2. Long perimeter runs of silt fence must be limited to 100'. Runs should be broken up into several smaller segments to minimize water concentrations (Figure A).
3. Long slopes should be broken up with intermediate rows of silt fence to slow runoff velocities.
4. Attach fabric to upstream side of post.
5. Install posts a minimum of 2' into the ground.
6. Trenching will only be allowed for small or difficult installation, where slicing machine cannot be reasonably used.

**Maintenance:**

1. Remove and dispose of sediment deposits when the deposit approaches 1/3 the height of silt fence.
2. Repair as necessary to maintain function and structure.

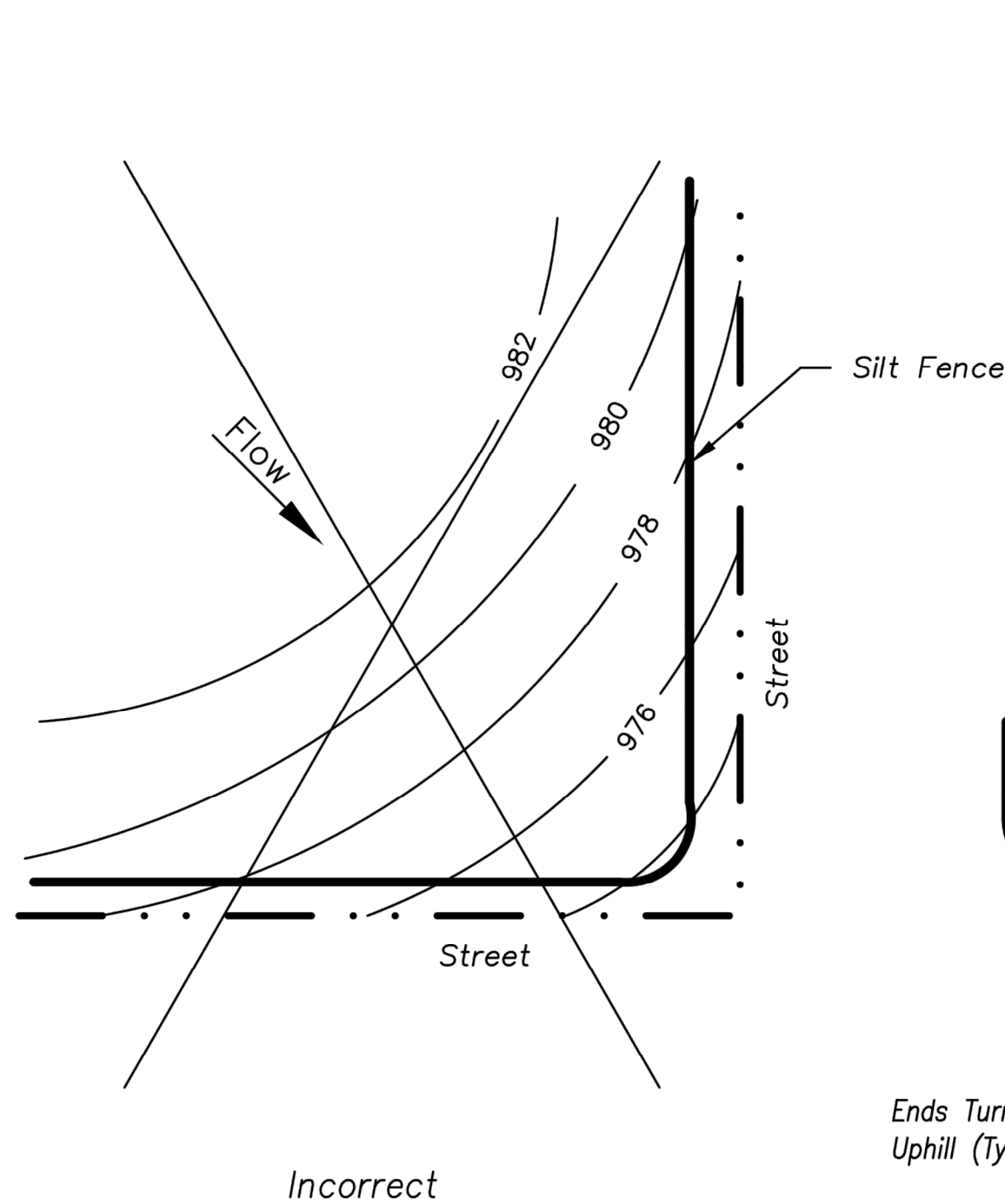
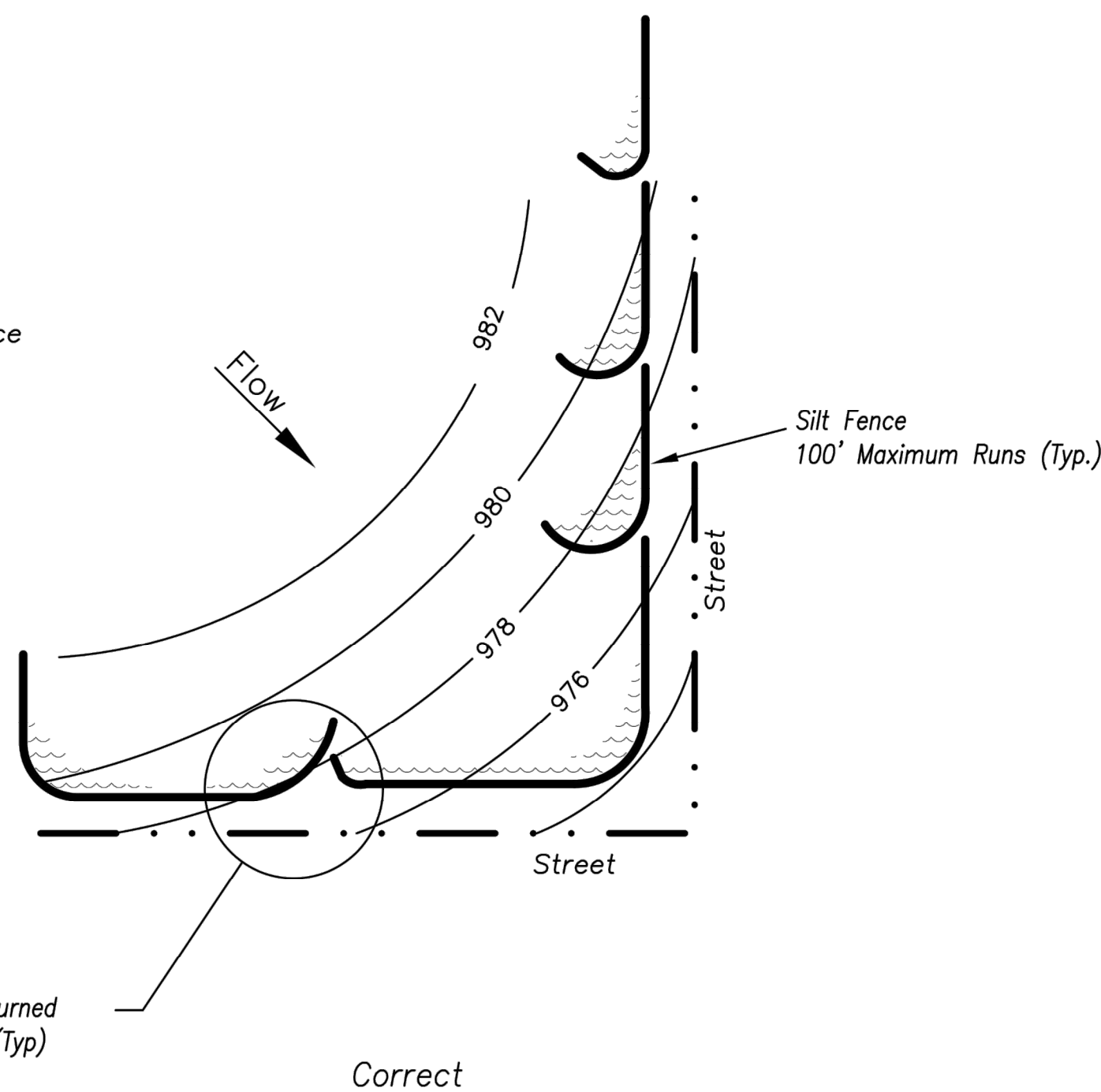
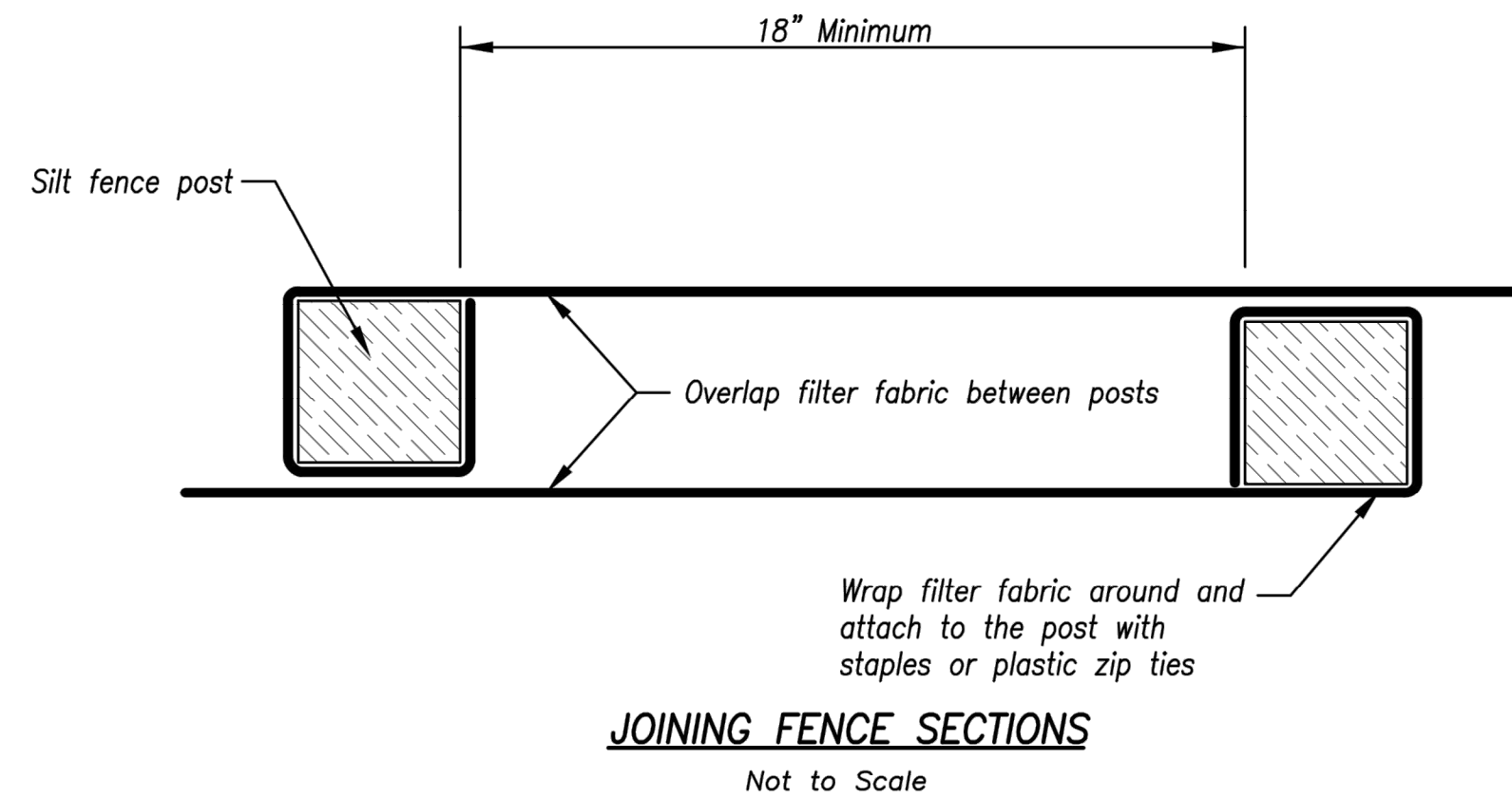
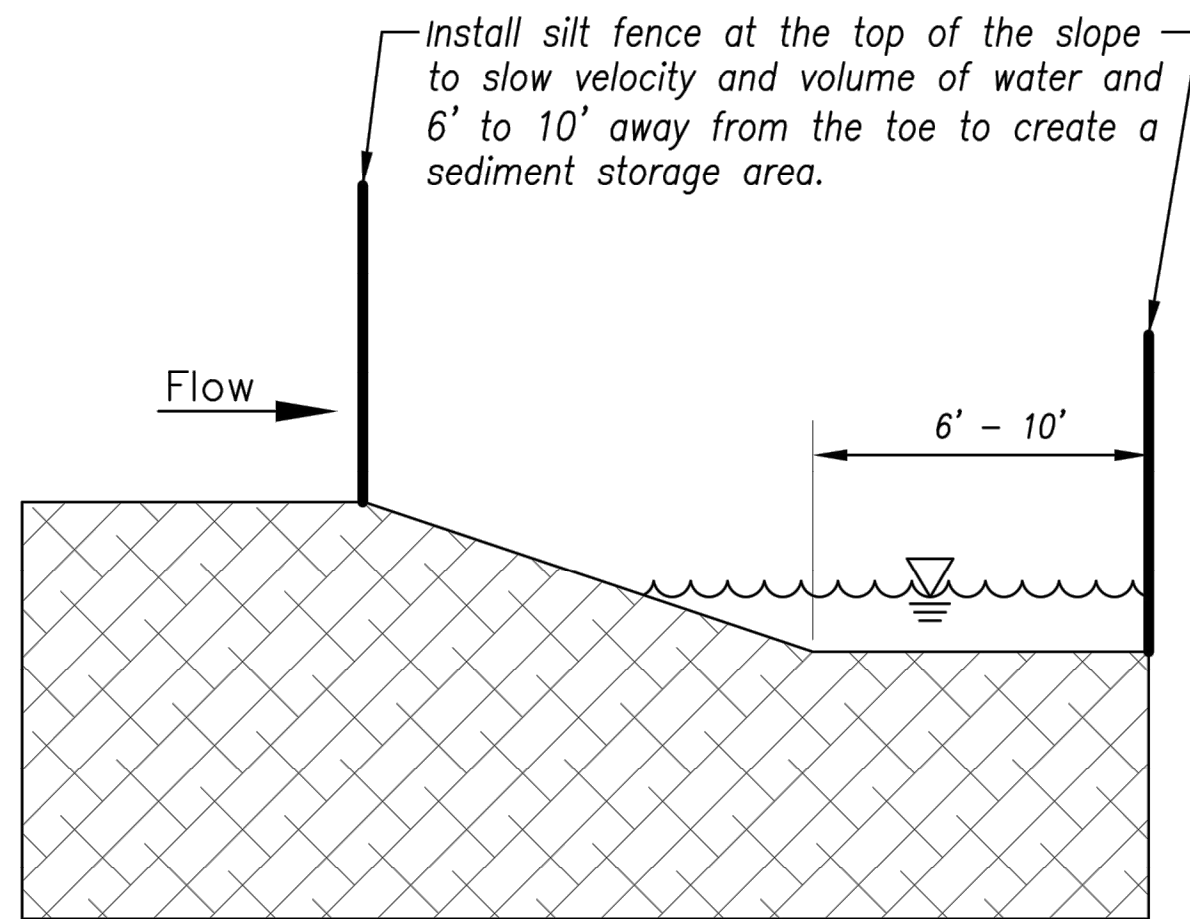


Figure A

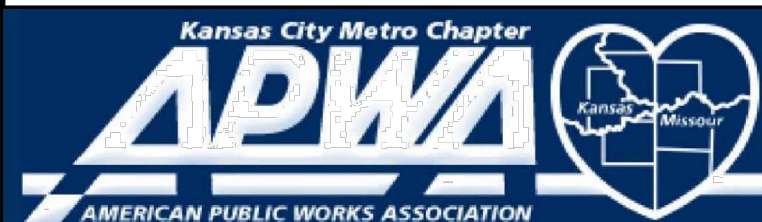


**SILT FENCE LAYOUT**

Not to Scale



**AMERICAN PUBLIC WORKS ASSOCIATION**



KANSAS CITY  
METRO CHAPTER

SILT FENCE

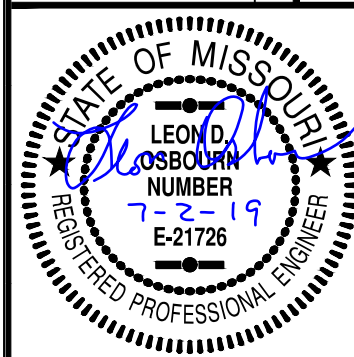
STANDARD DRAWING  
NUMBER ESC-03

ADOPTED:  
10/24/2016

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

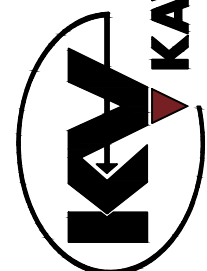
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
EROSION CONTROL DETAIL SHEET

PROJ. NO.	A14-7067-1
DESIGNER	LDO
DRAWN BY	JT/BKR
CFN	7067-1SIG_DET
SHEET	33
REV	0



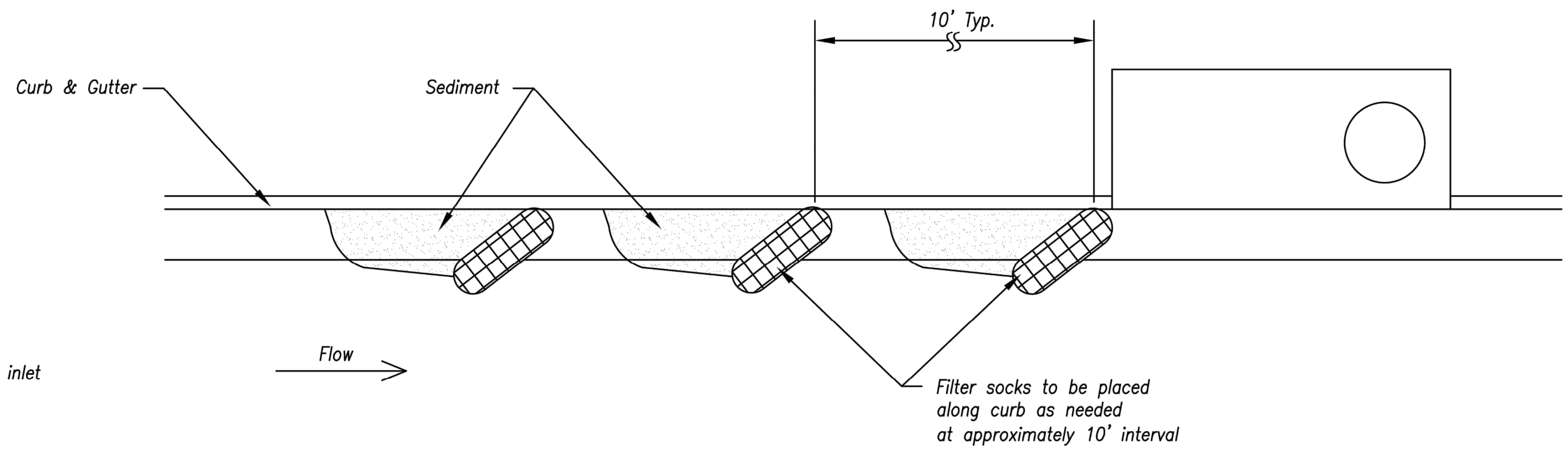
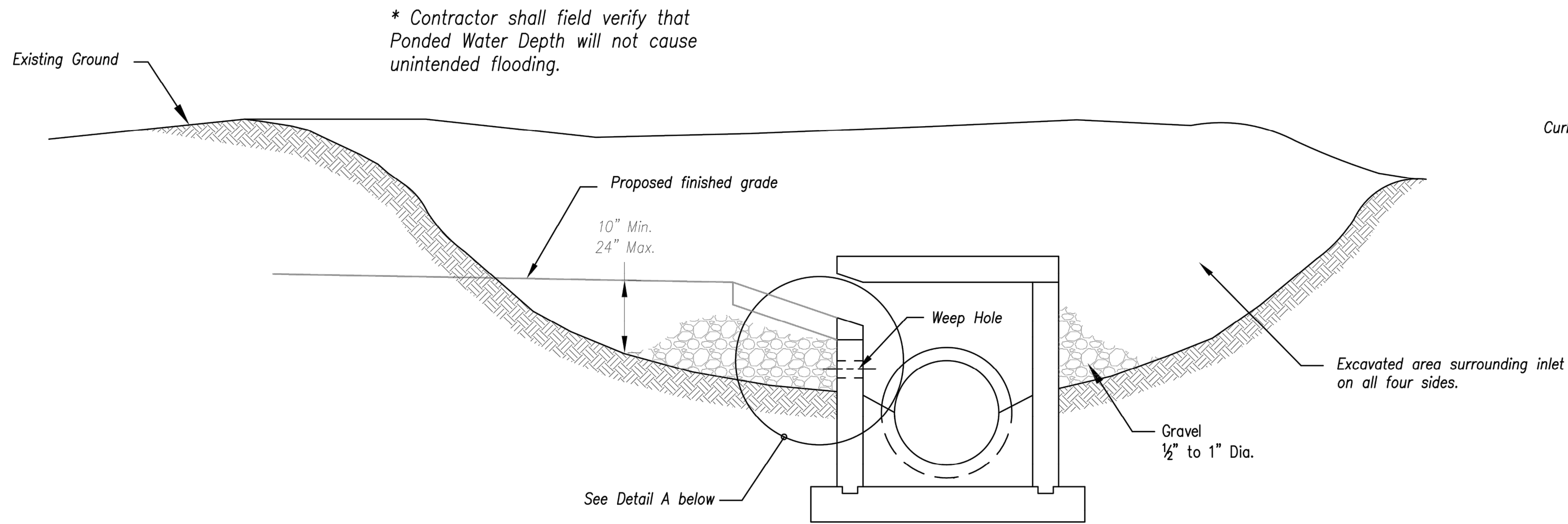
LEON D. OSBOURN  
ENGINEER  
MO # 021726

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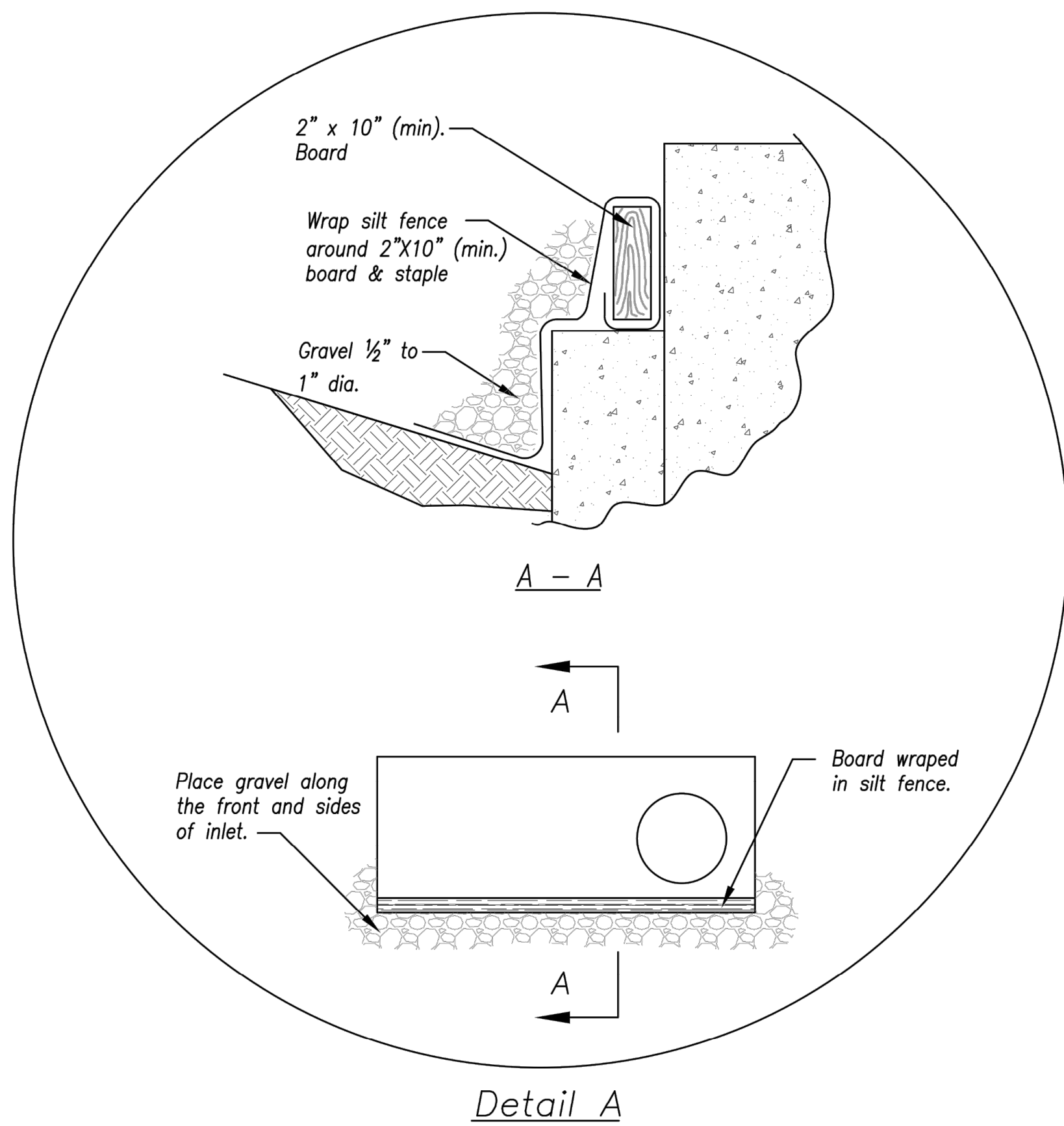


KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19





On Grade Curb Inlet Protection



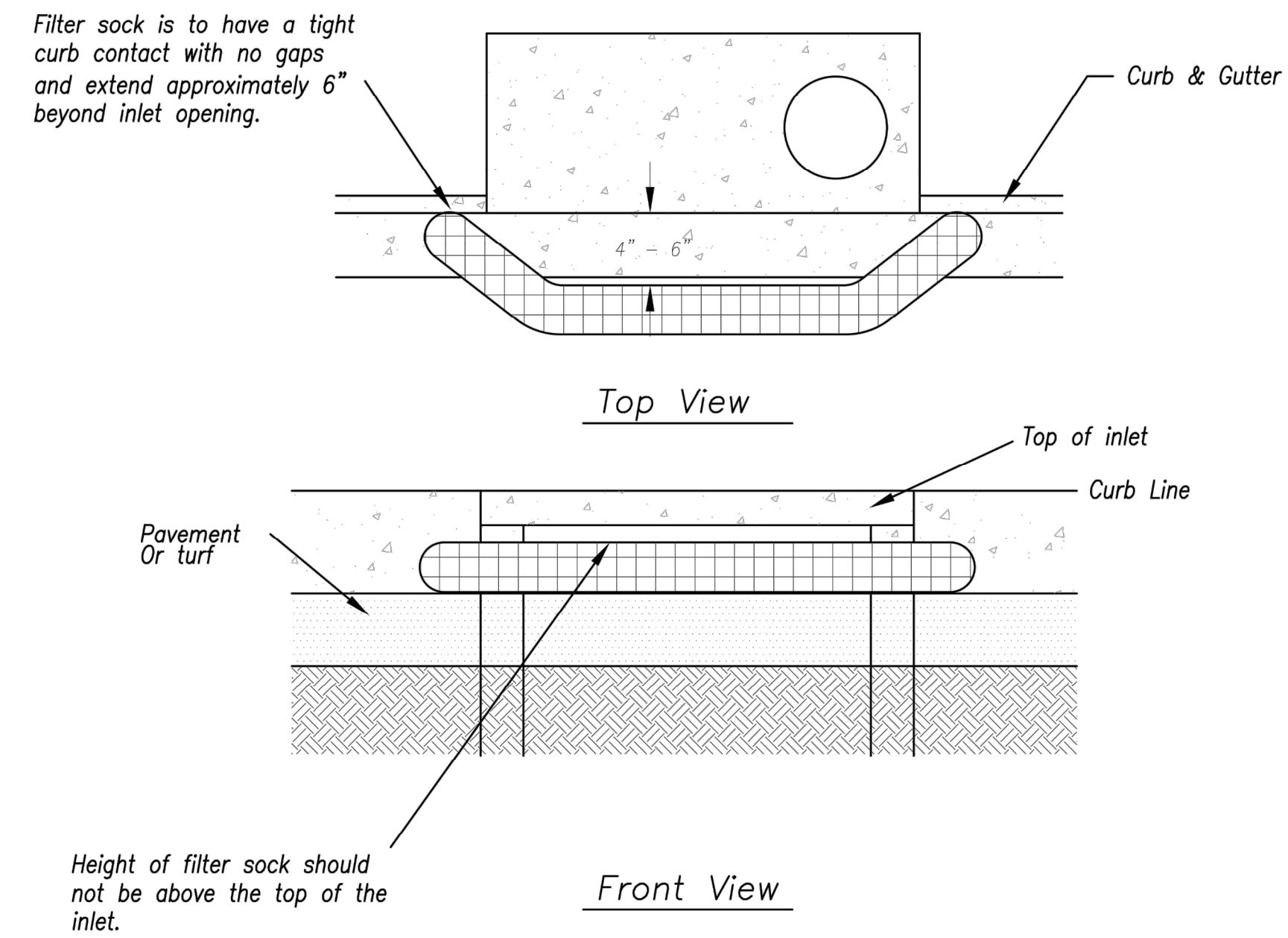
EARLY STAGE CURB INLET  
(Open Box and Prior to Pouring  
Curb and Inlet Throat)

Notes:

1. Immediately following inlet construction and prior to construction of curb and inlet throat, protect inlet opening by installing 2" X 10" (min.) board wrapped in silt fence. Structures shall have excavated storage area on all four sides to allow settling of sediment (Early Stage Curb Inlet).
2. When inlet is completed and curb poured, filter socks or approved equal should be used (Late Stage Curb Inlet). Straw wattles are not approved for curb inlet use.
3. Contractor to field verify ponding water shall not create a traffic hazard.


Maintenance:

1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
3. Repair or replace as necessary to maintain function and integrity of installation.



Sump Inlet Sediment Filter

LATE STAGE CURB INLET  
(After Pouring Curb and Inlet Throat)

AMERICAN PUBLIC WORKS ASSOCIATION	
	KANSAS CITY METRO CHAPTER
CURB INLET PROTECTION	STANDARD DRAWING NUMBER ESC-06 ADOPTED: 10/24/2016



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**KAW VALLEY ENGINEERING**

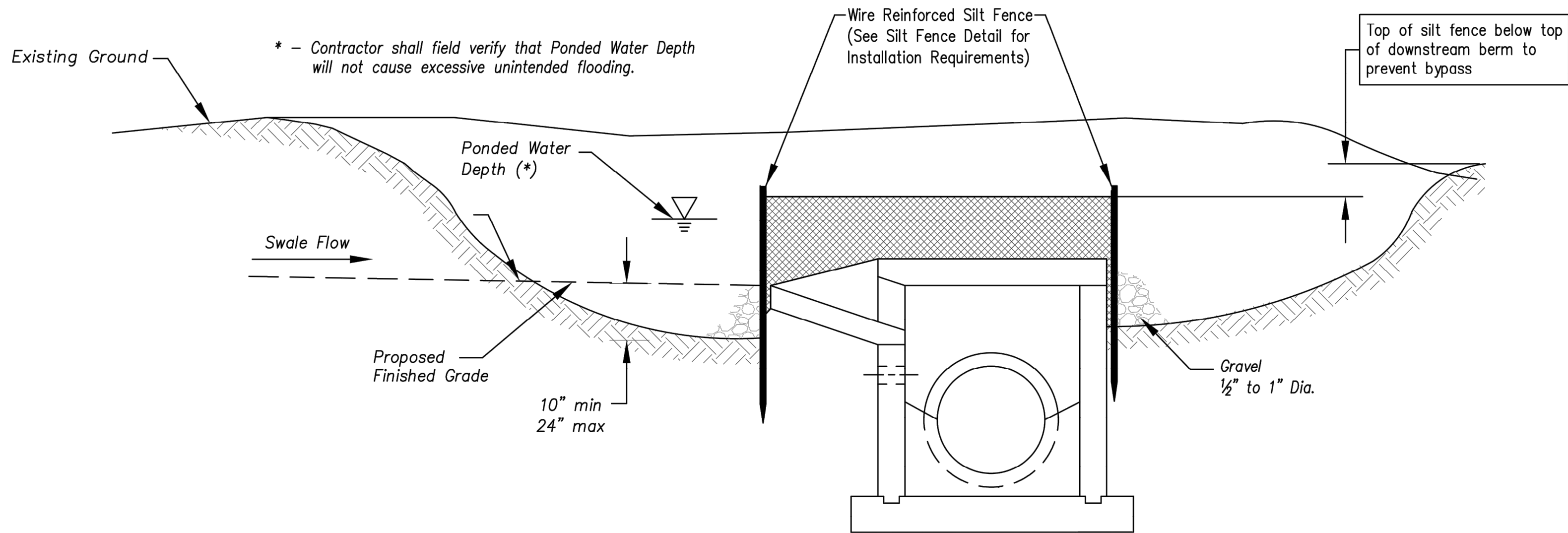
KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES IN THE STATE OF MISSOURI. CERTIFICATE OF AUTHORITY # 000842. EXPIRES 12/31/19

STREETS OF WEST PRYOR  
NWQ NW PRYOR RD & NW LOWENSTEIN DR  
LEE'S SUMMIT, MISSOURI

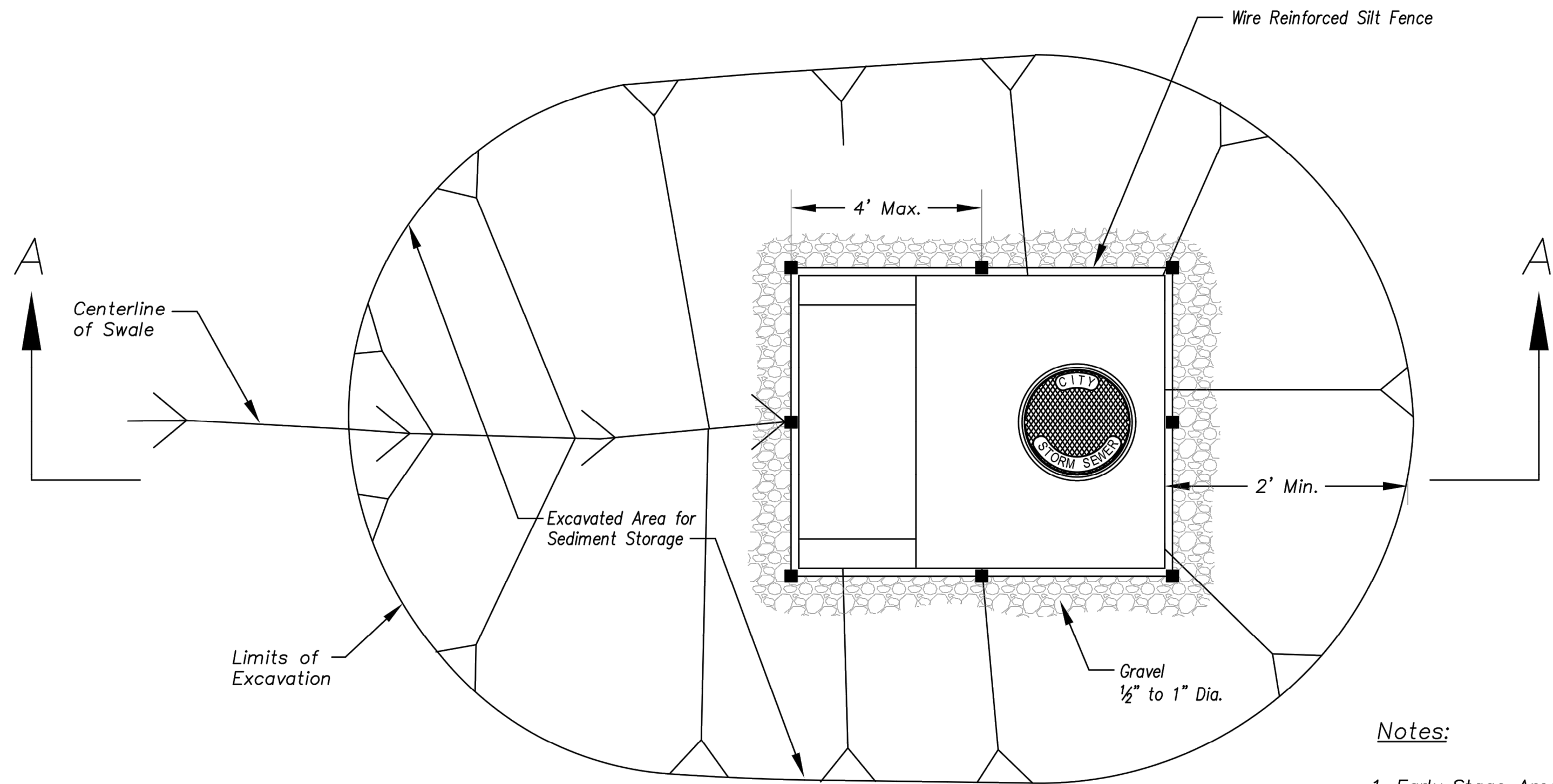
NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING  
EROSION CONTROL DETAIL SHEET

PROJ. NO.	A14_7067-1
DESIGNER	LDO
DRAWN BY	JT/BKR
CFN	7067-1SIG_DET
SHEET	34
REV	0





Section A-A  
Not to Scale

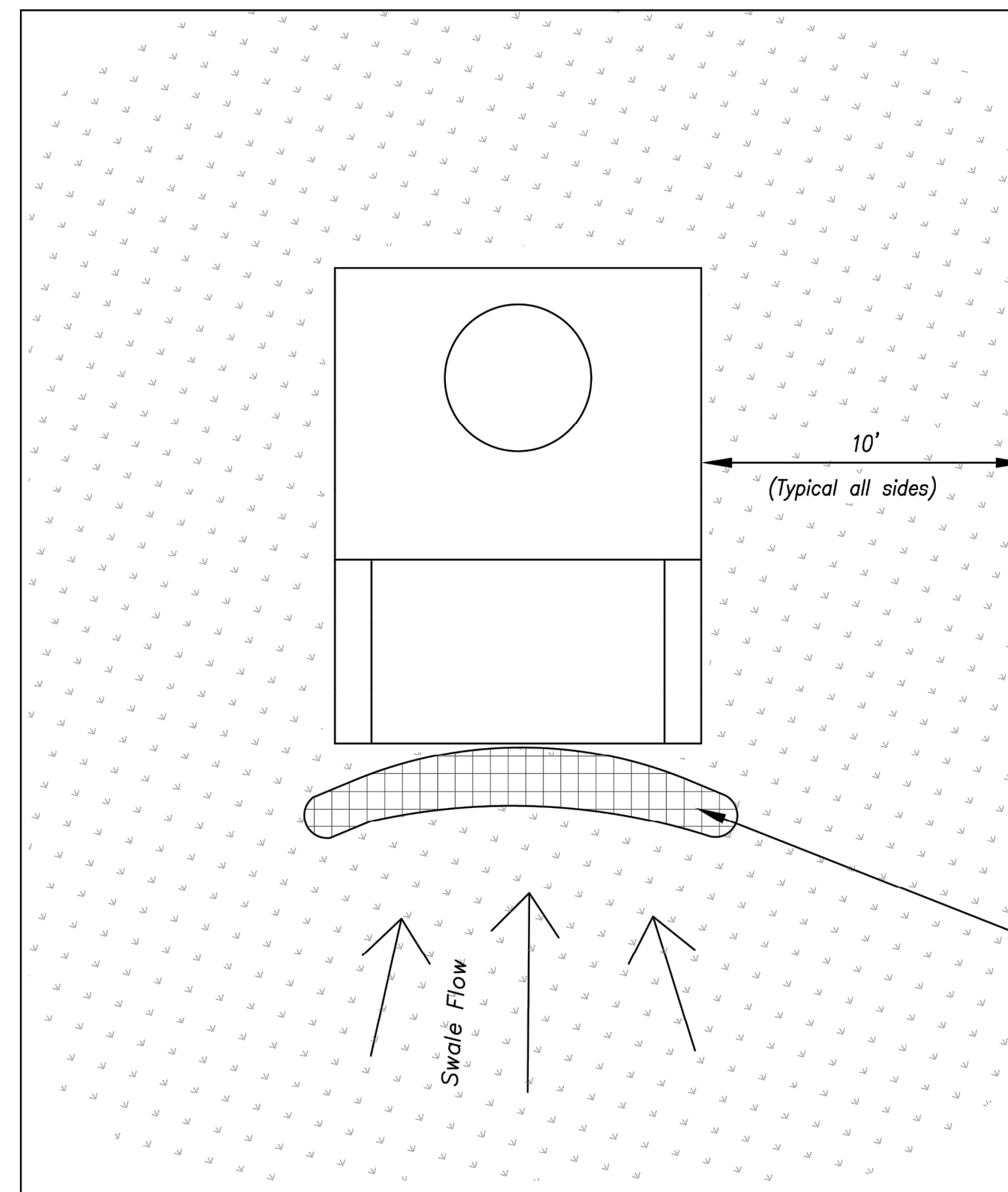


Plan  
Not to Scale

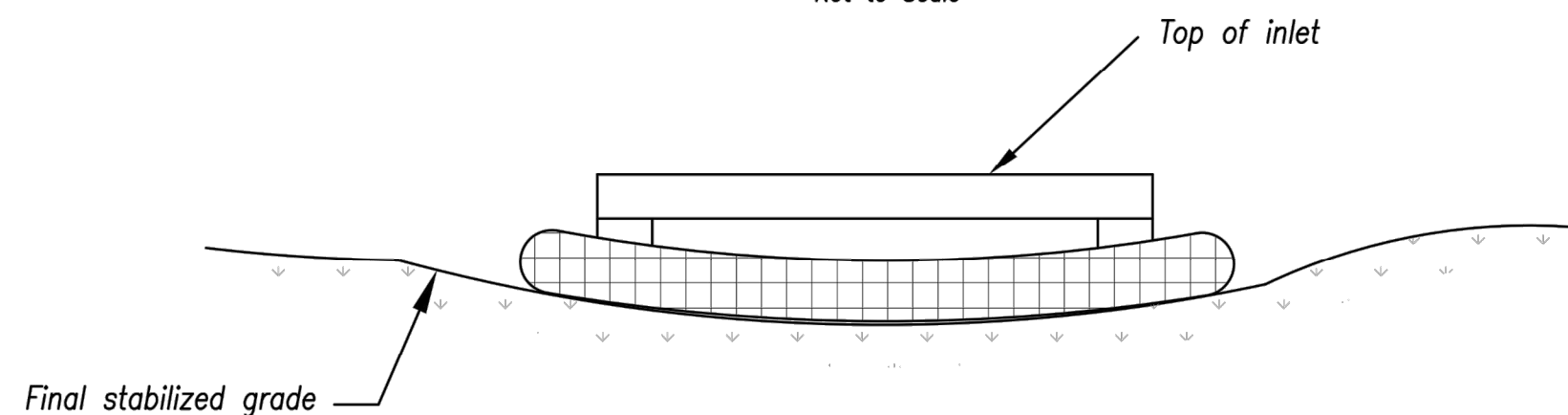
**EARLY STAGE AREA INLET**  
(All open boxes and inlets not at final grade)

Notes:

1. Early Stage Area Inlet Sediment Barrier to be installed immediately after inlet or junction box is constructed.
2. Silt fence shall remain in place until excavated area is removed and Late Stage Area Inlet is being installed.
3. Backfill excavated area ONLY after final grading of the site. Stabilization of the site is to immediately follow.
4. Wire reinforced silt fence may be used in place of silt fence attached to wood frame.



Plan  
Not to Scale




Front View

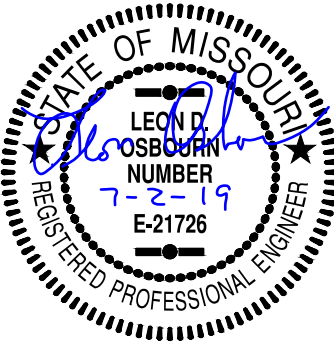
**LATE STAGE AREA INLET**  
(Area inlets at final grade and existing inlets)

Maintenance:

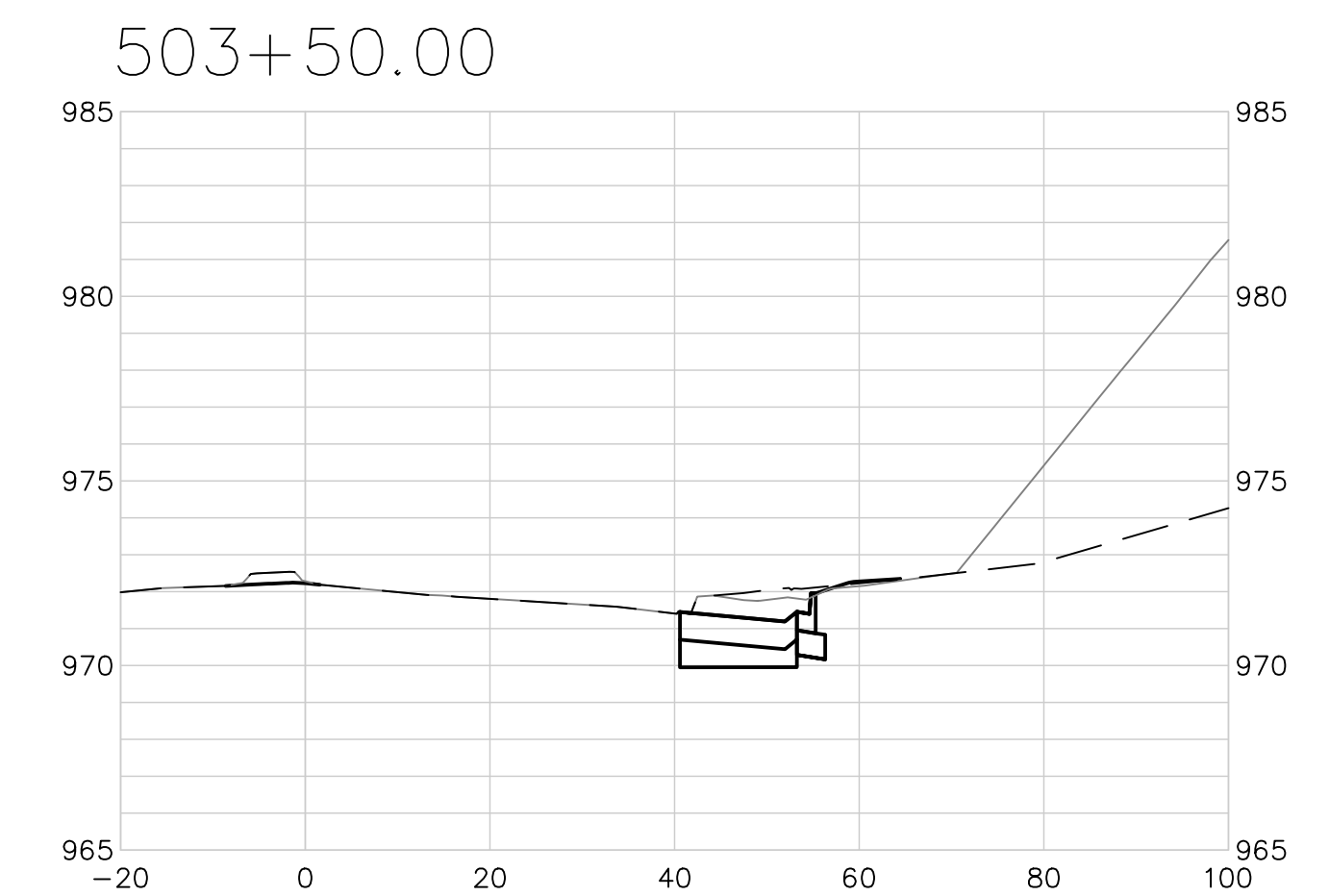
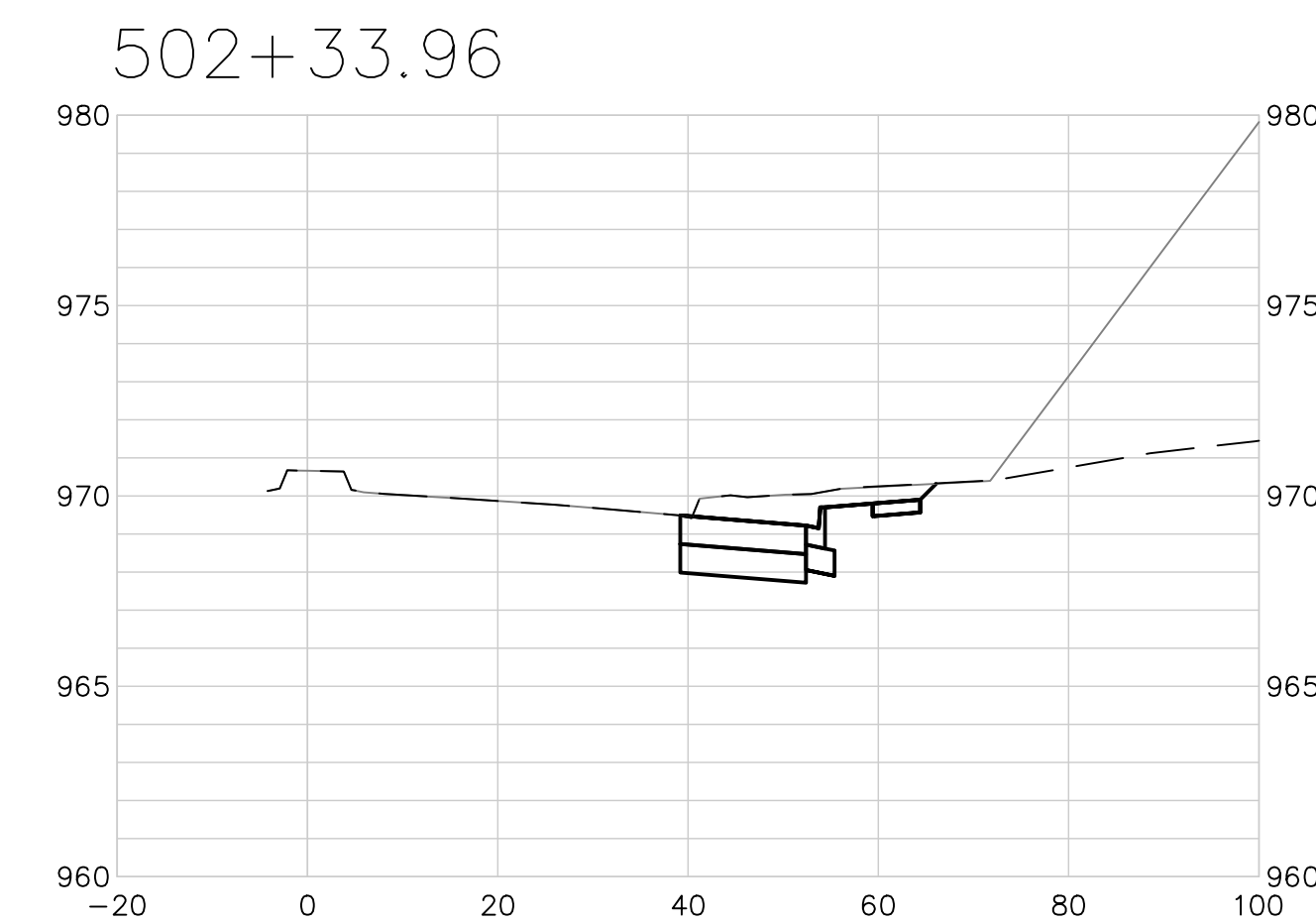
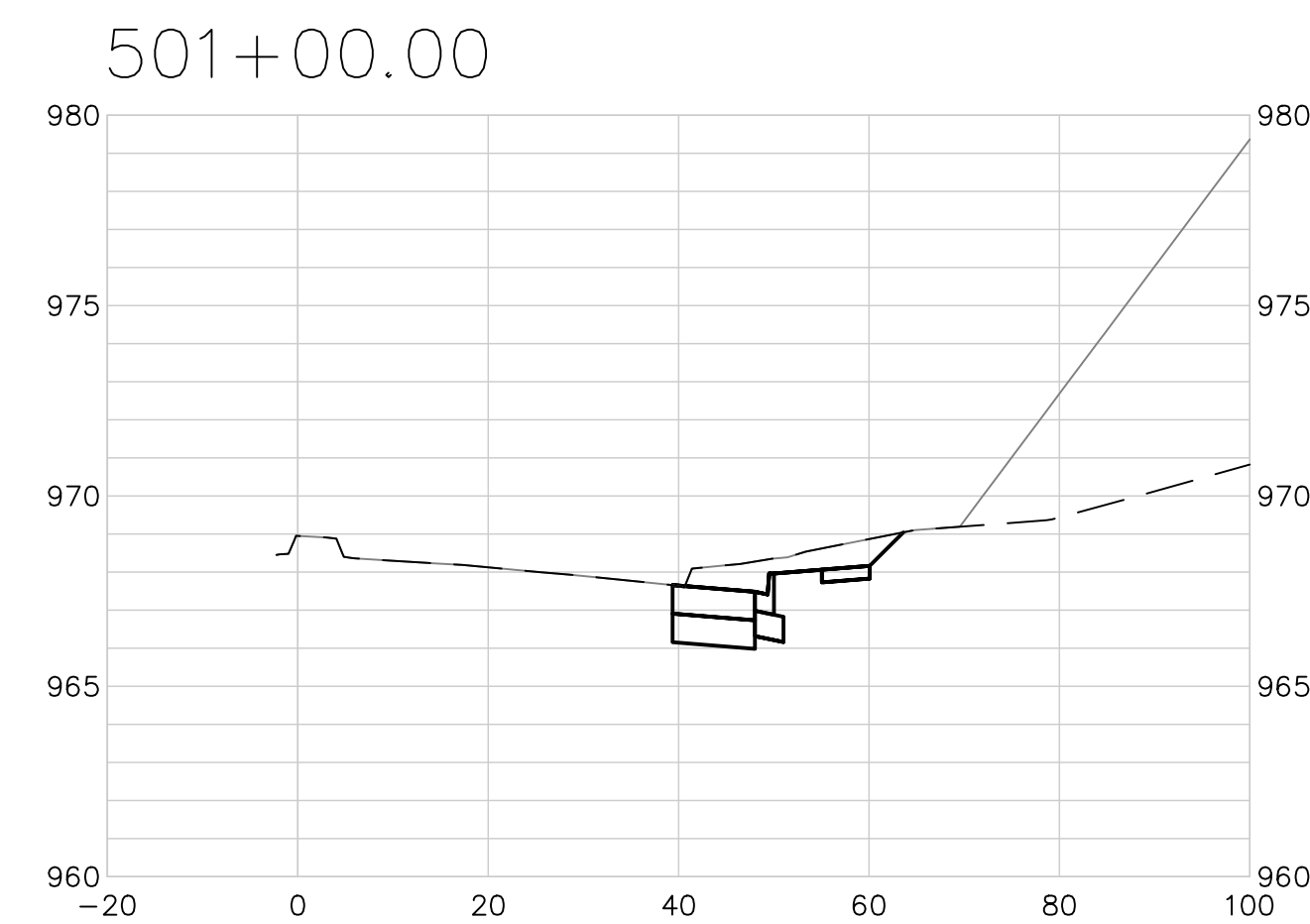
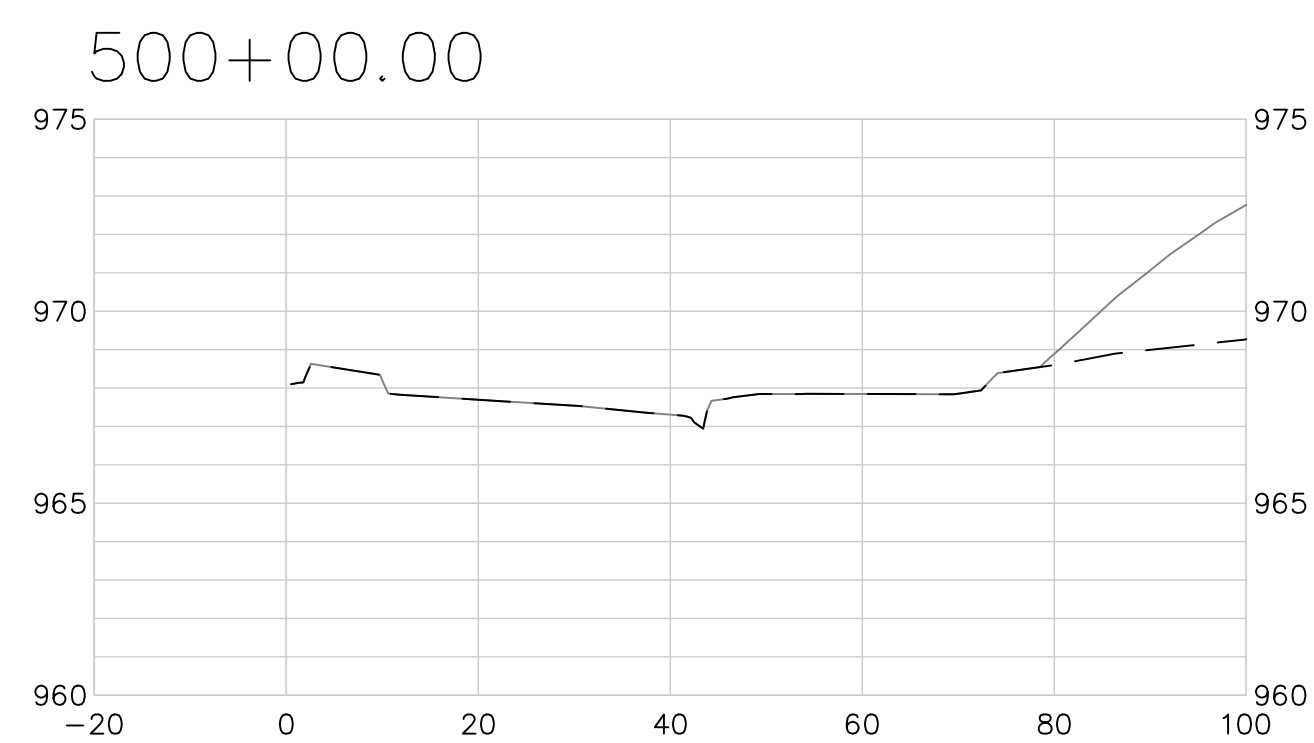
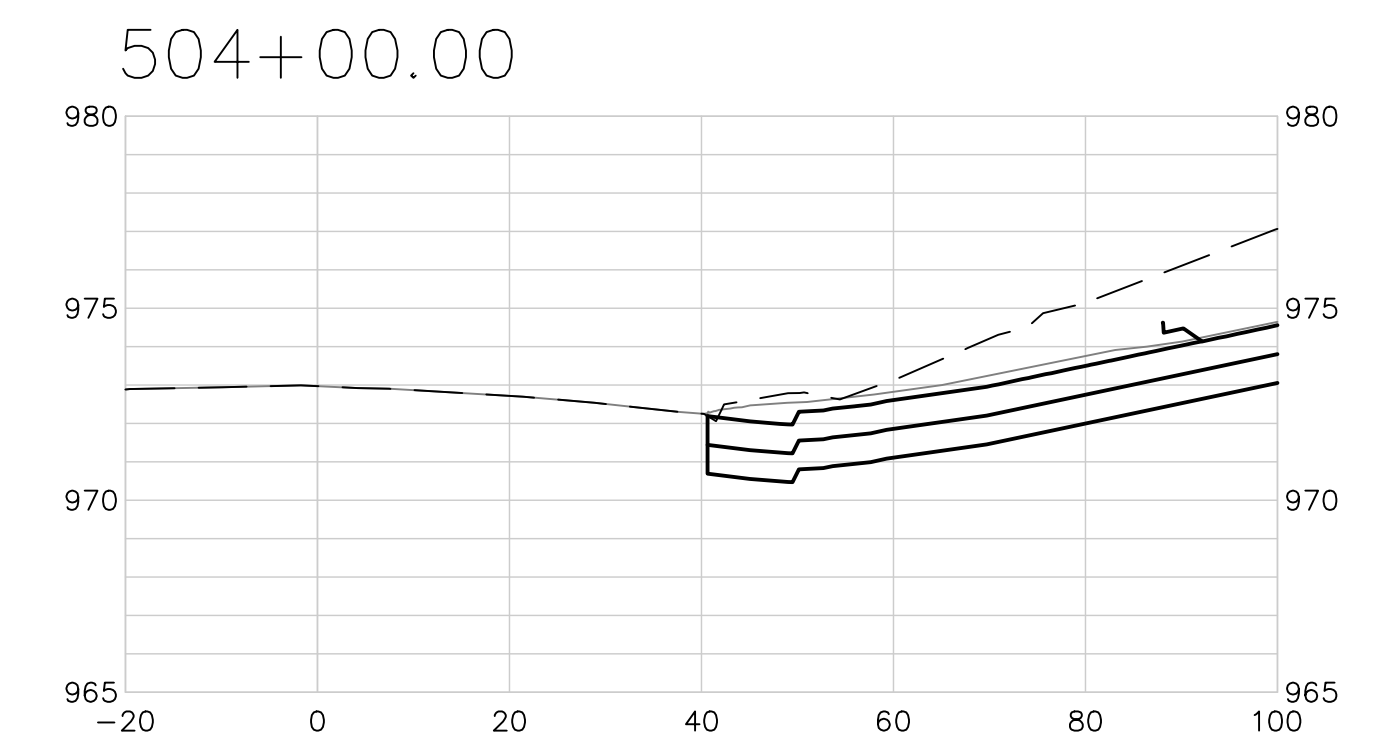
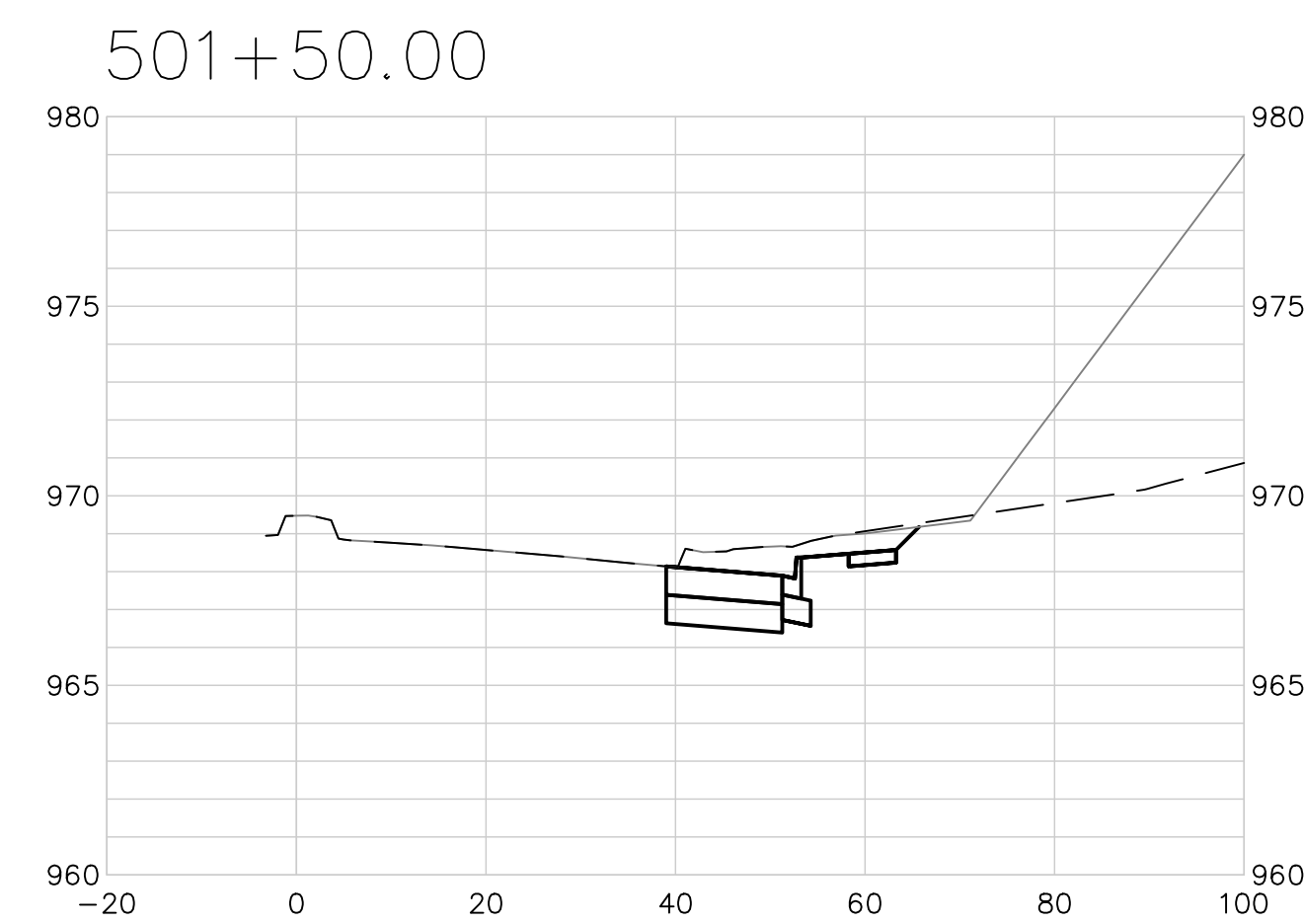
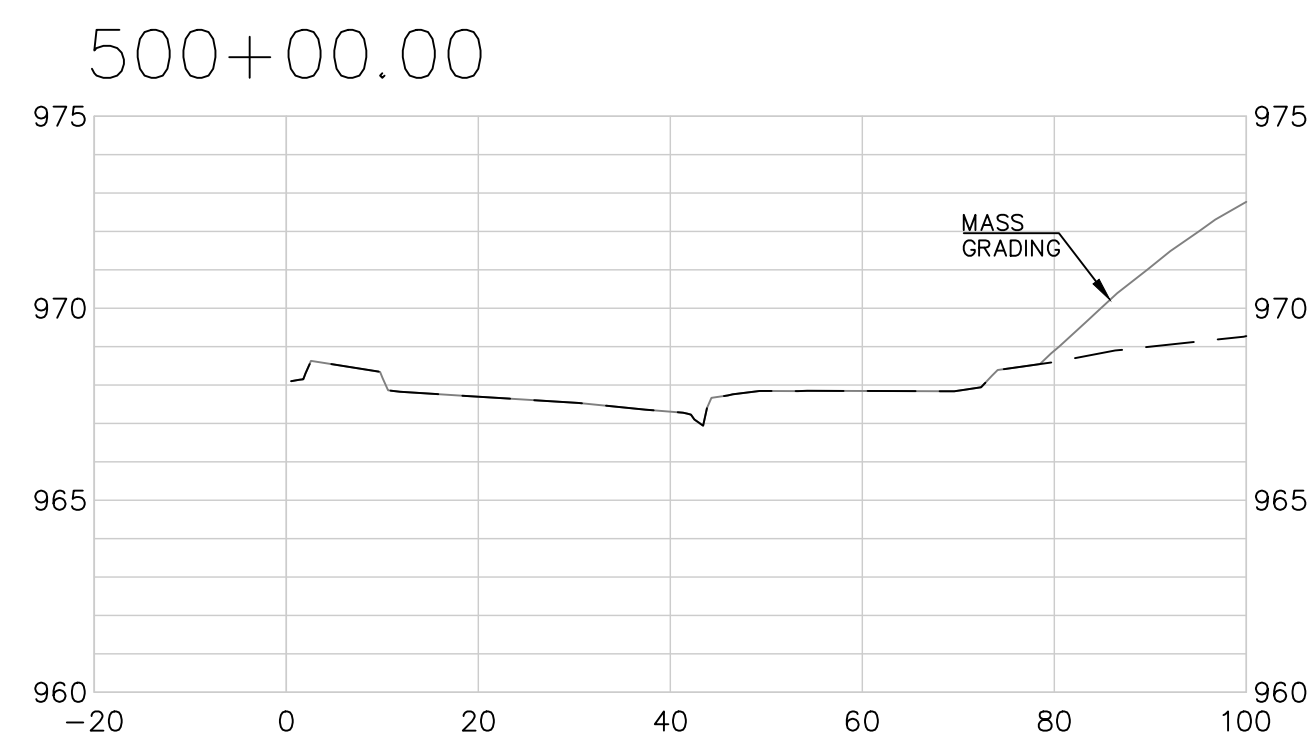
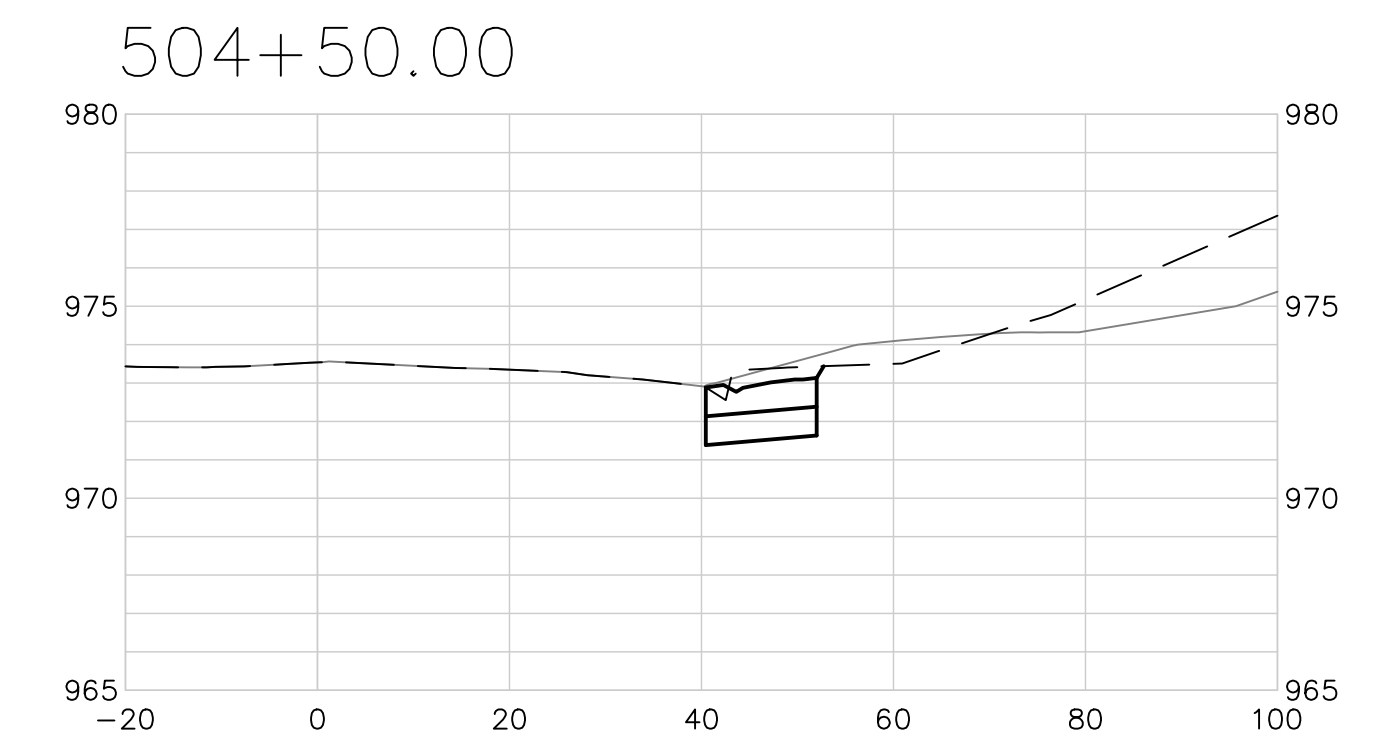
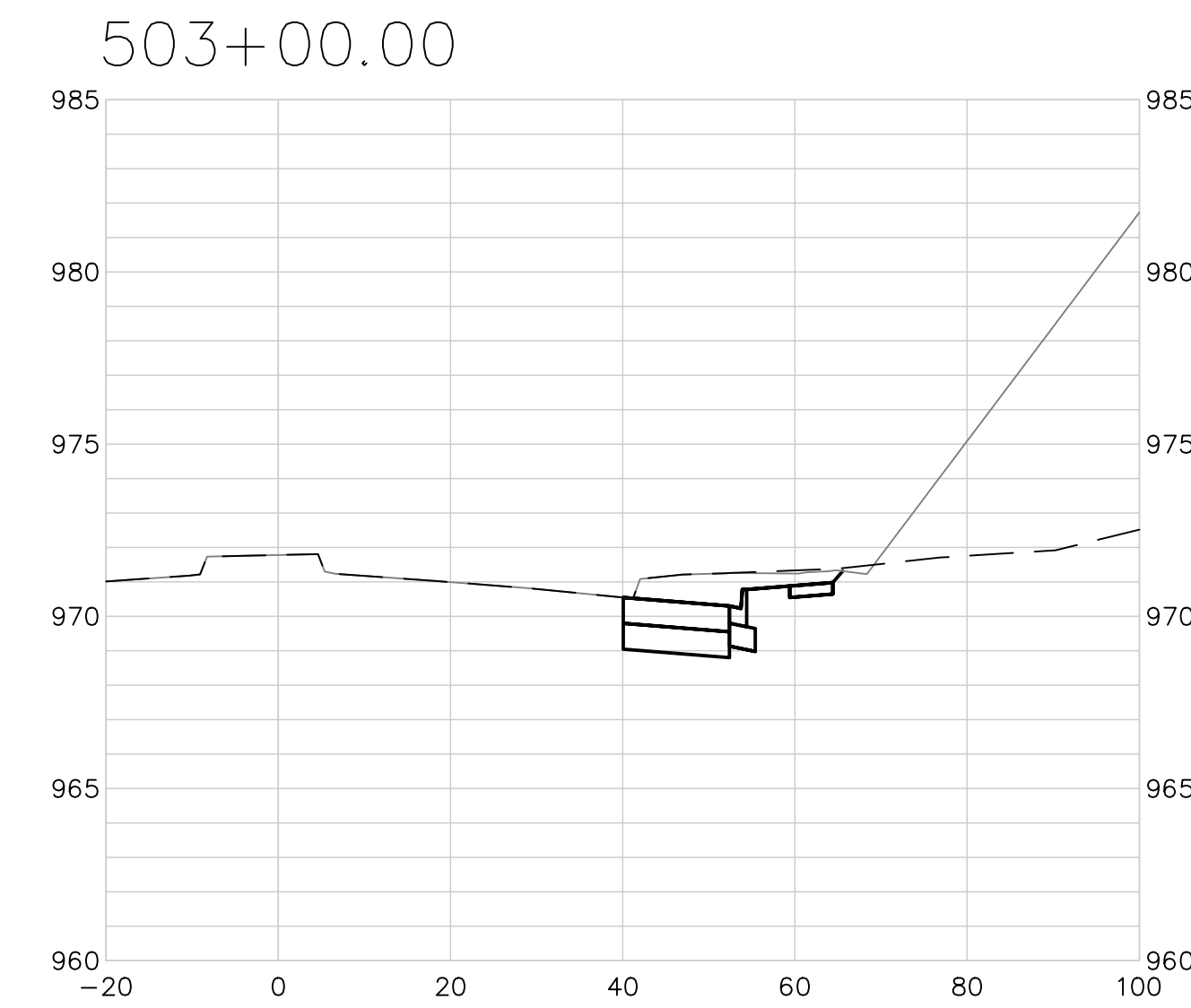
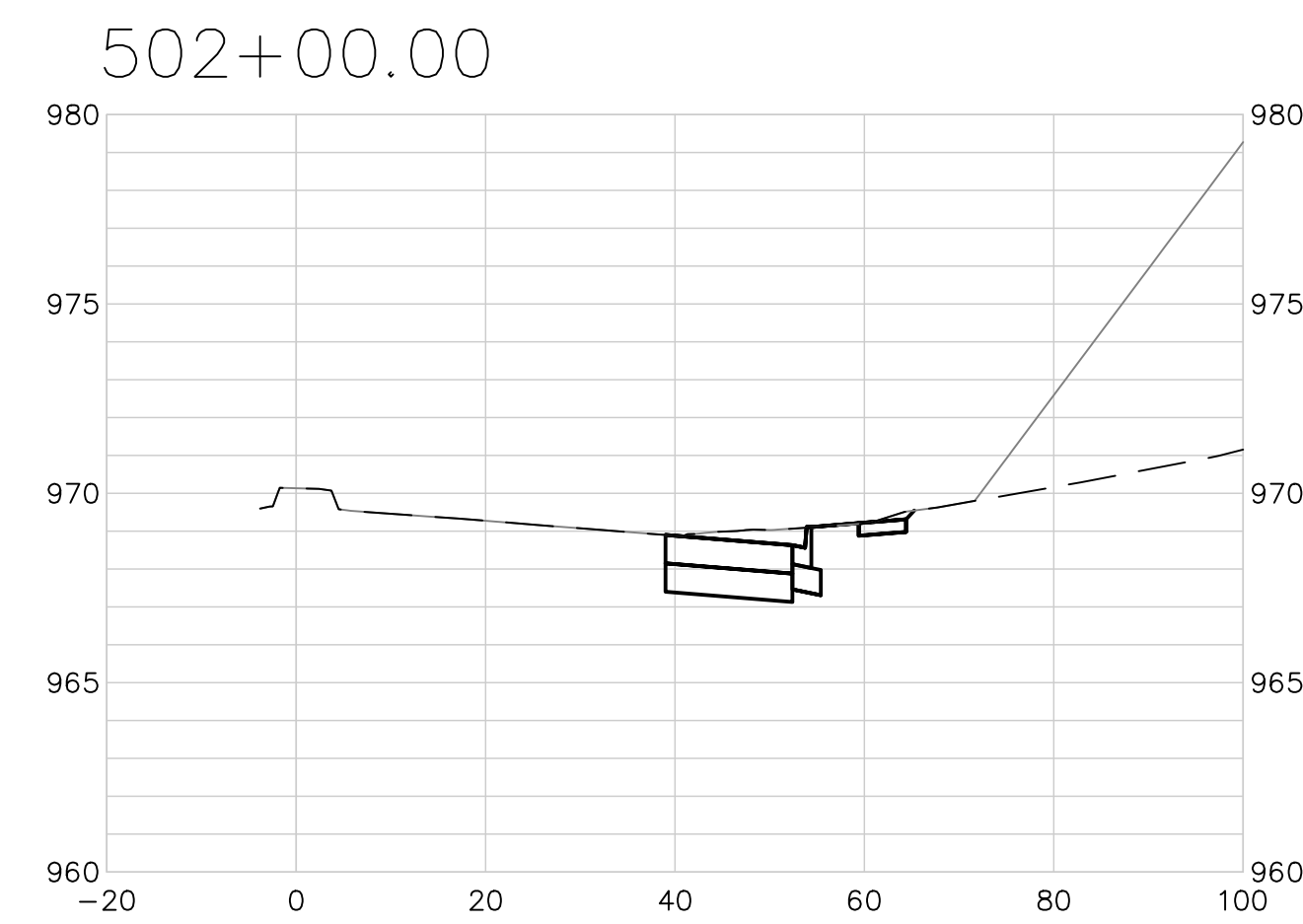
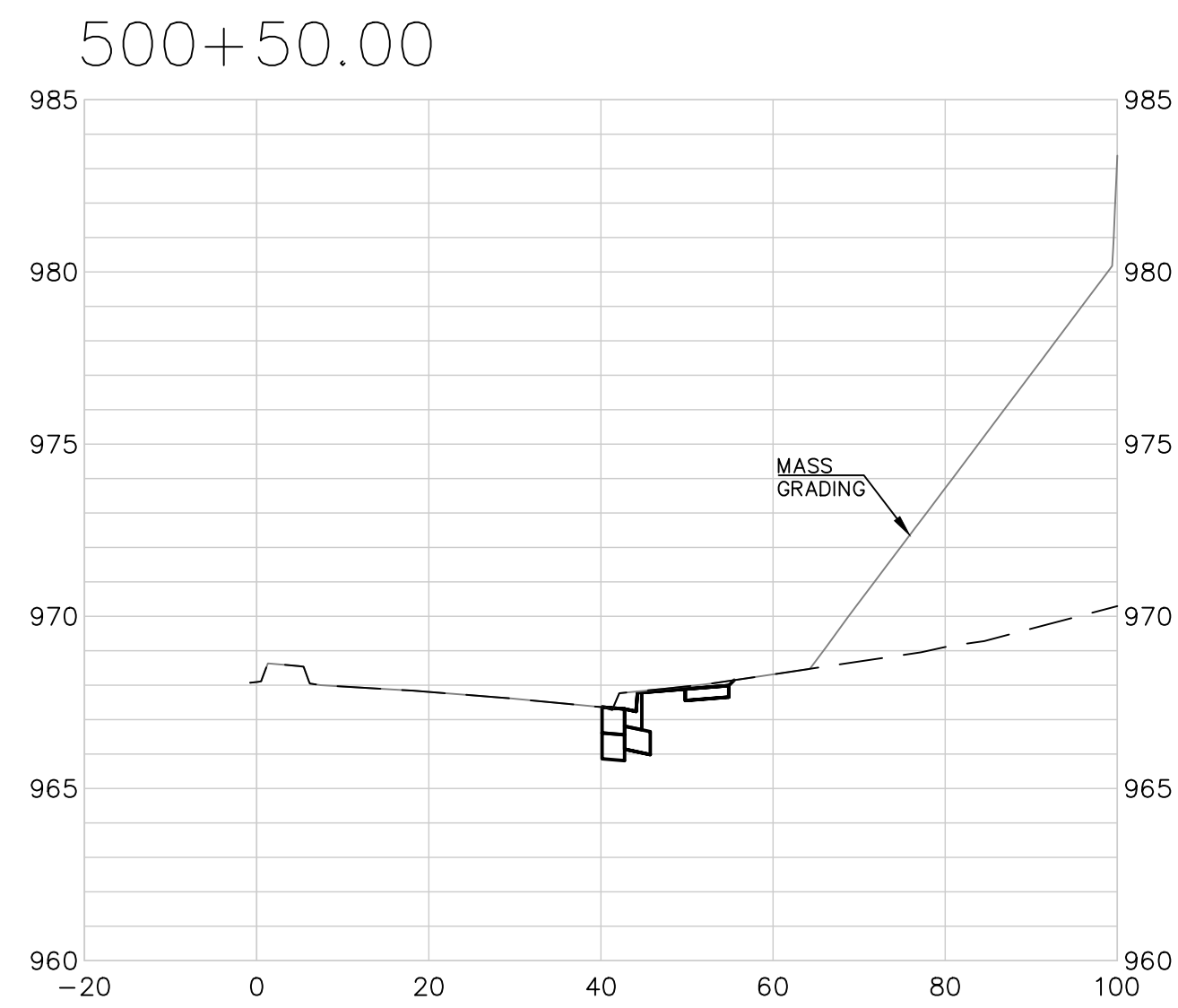
1. Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
2. Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
3. Repair or replace as necessary to maintain function and integrity of installation.

Modified from 2015 Overland Park Standard Details  
for Erosion and Sediment Control.

<b>AMERICAN PUBLIC WORKS ASSOCIATION</b> 		<b>KANSAS CITY METRO CHAPTER</b>	
<b>AREA INLET AND JUNCTION BOX PROTECTION</b>		<b>STANDARD DRAWING</b> <b>NUMBER ESC-07</b> <b>ADOPTED:</b> 10/24/2016	

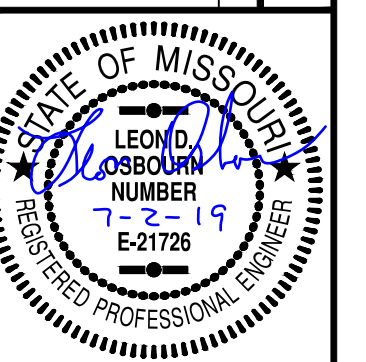
<b>STREETS OF WEST PRYOR</b> NWQ NW PRYOR RD & NW LOWENSTEIN DR LEE'S SUMMIT, MISSOURI  NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING <b>EROSION CONTROL DETAIL SHEET</b>		<b>LEON D. OSBOURN</b> ENGINEER MO # 021726	<b>KAW VALLEY ENGINEERING</b> 2319 N. JACKSON   P.O. BOX 1304 JUNCTION CITY, KANSAS 66441 PH. (785) 762-5040   FAX (785) 762-7744 joe@kveeng.com   www.kveeng.com KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING KANSAS STATE CERTIFICATE OF AUTHORITY # 000842 EXPIRES 12/31/19	<table border="1"> <tr> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> <tr> <td>0</td> <td>5-23-19</td> <td>INITIAL ISSUE</td> </tr> </table>	REV	DATE	DESCRIPTION	0	5-23-19	INITIAL ISSUE	<table border="1"> <tr> <td>LDO</td> <td>JT</td> <td>LDO</td> <td>JT</td> </tr> <tr> <td>DSN</td> <td>DWN</td> <td>CHK</td> <td>CHK</td> </tr> </table>	LDO	JT	LDO	JT	DSN	DWN	CHK	CHK
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DSN	DWN	CHK	CHK																
<b>PROJ. NO. A14_7067-1</b>																			
<table border="1"> <tr> <td>DESIGNER</td> <td>LDO</td> <td>DRAWN BY</td> <td>JT/BKR</td> </tr> <tr> <td>CFN</td> <td>7067-1SIG_DET</td> <td>SHEET</td> <td>35</td> </tr> <tr> <td>REV</td> <td>0</td> <td colspan="2"></td> </tr> </table>				DESIGNER	LDO	DRAWN BY	JT/BKR	CFN	7067-1SIG_DET	SHEET	35	REV	0						
DESIGNER	LDO	DRAWN BY	JT/BKR																
CFN	7067-1SIG_DET	SHEET	35																
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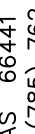


SCALE:  
1"=20' HORIZ.  
1"=5' VERT.

REV	DATE	DESCRIPTION	DSN	DWN	CHK
0	5-23-19	INITIAL ISSUE	LDO	JT	LDO



LEON D. OSBOURN  
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**STREETS OF WEST PRYOR**  
**NWQW NW PRYOR RD & NW LOWENSTEIN DR**  
**LEE'S SUMMIT, MISSOURI**

**NW PRYOR RD SIGNAL IMPROVEMENTS AND LANE WIDENING**  
**NW PRYOR ROAD - CROSS-SECTIONS**

PROJ. NO.	
A14-7067-1	
DESIGNER	DRAWN BY
LDO	JT/BKR
CFN	
067-1SIG_STPP	
SHEET	REV
36	0



