

AN ORDINANCE APPROVING REZONING FROM DISTRICT PI (PLANNED INDUSTRIAL DISTRICT) TO DISTRICT PMIX (PLANNED MIXED USE DISTRICT) AND PRELIMINARY DEVELOPMENT PLAN FOR EAST VILLAGE PHASE 2 ON APPROXIMATELY 50 ACRES OF LAND GENERALLY LOCATED AT THE SOUTHEAST INTERSECTION OF SE BAILEY RD AND SOUTH M-291 HWY, ALL IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 33, THE UNIFIED DEVELOPMENT ORDINANCE, OF THE CODE OF ORDINANCES FOR THE CITY OF LEE'S SUMMIT, MISSOURI.

WHEREAS, Application #PL2025-275 submitted by East Village Investors, LLC., requesting approval of a rezoning from PI (Planned Industrial District) to PMIX (Planned Mixed Use District) and preliminary development plan on land generally located at the southeast intersection of SE Bailey Rd and South M-291 Hwy was referred to the Planning Commission to hold a public hearing; and,

WHEREAS, the Unified Development Ordinance provides for the approval of a rezoning and preliminary development plan by the City following public hearings by the Planning Commission and City Council; and,

WHEREAS, after due public notice in the manner prescribed by law, the Planning Commission held a public hearing for the consideration of the rezoning and preliminary development plan on December 11, 2025, and rendered a report to the City Council recommending that the rezoning and development plan be approved; and,

WHEREAS, after due public notice in the manner prescribed by law, the City Council held a public hearing on January 6, 2026, and rendered a decision to approve the rezoning and preliminary development plan for said property.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF LEE'S SUMMIT, MISSOURI, as follows:

SECTION 1. That a rezoning and preliminary development plan is hereby approved on the following described property:

Rezoning:

Commencing at the North Quarter Corner of said Section 17; thence S32°03'09"E, a distance of 229.87 feet to the Point of Beginning; thence S29°26'48"E, a distance of 1323.66 feet; thence N87°32'36"W, a distance of 832.39 feet; thence N2°36'22"E, a distance of 358.00 feet; thence N87°50'43"W, a distance of 1507.83 feet; thence N26°54'08"W, a distance of 312.41 feet; thence N26°49'41"W, a distance of 241.77 feet; thence along a curve to the right tangent to the preceding course and having a radius of 1765.59 feet, an arc distance of 392.23 feet; thence N32°04'16"E, a distance of 61.73 feet; thence S87°49'40"E, a distance of 1158.47 feet; thence S78°33'50"E, a distance of 869.58 feet to the Point of Beginning.

Containing 2,079,263 Sq. Ft. or 47.73 Acres±; and

Preliminary Development Plan:

TRACT 1:

ALL THE PART OF THE NORTHWEST QUARTER, AND ALL THAT PART OF THE NORTHEAST QUARTER OF SECTION 17, TOWNSHIP 47, RANGE 31, IN LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF SAID NORTHEAST QUARTER; THENCE SOUTH 87 DEGREES 53 MINUTES 51 SECONDS EAST ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER, A DISTANCE OF 11.96 FEET TO THE SOUTHWESTERLY RIGHT OF WAY LINE OF THE UNION PACIFIC RAILROAD, AS NOW ESTABLISHED; THENCE SOUTH 29 DEGREES 25 MINUTES 41 SECONDS EAST ALONG SAID SOUTHWESTERLY RIGHT OF WAY LINE, A DISTANCE OF 223.09 FEET TO THE TRUE POINT OF BEGINNING OF THE TRACT OF LAND TO BE HEREIN DESCRIBED; THENCE SOUTH 29 DEGREES 25 MINUTES 41 SECONDS EAST, A DISTANCE OF 1323.59 FEET TO A POINT IN THE SOUTH LINE OF THE NORTHWEST QUARTER OF SAID NORTHEAST QUARTER; THENCE NORTH 87 DEGREES 32 MINUTES 55 SECONDS WEST, ALONG SAID SOUTH LINE, A DISTANCE OF 832.35 FEET TO THE SOUTHWEST CORNER OF SAID NORTHWEST QUARTER OF THE NORTHEAST QUARTER; THENCE NORTH 02 DEGREES 36 MINUTES 20 SECONDS EAST ALONG THE EAST LINE OF SAID NORTHWEST QUARTER AND ALONG THE EAST LINE OF MADDOX ACRES, A SUBDIVISION, A DISTANCE OF 358.00 FEET TO THE NORTHEAST CORNER OF LOT 12 OF SAID SUBDIVISION; THENCE NORTH 87 DEGREES 49 MINUTES 43 SECONDS WEST ALONG THE NORTH LINE OF SAID SUBDIVISION, A DISTANCE OF 1507.48 FEET TO A POINT ON THE EAST RIGHT OF WAY LINE OF MISSOURI STATE HIGHWAY NO. 291 AS NOW ESTABLISHED; THENCE NORTHWESTERLY ALONG SAID RIGHT OF WAY LINE, ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 2416.83 FEET, A CHORD BEARING OF NORTH 26 DEGREES 20 MINUTES 38 SECONDS WEST, A CENTRAL ANGLE OF 1 DEGREE 18 MINUTES 57 SECONDS, AN ARC LENGTH OF 55.50 FEET; THENCE NORTH 27 DEGREES 00 MINUTES 06 SECONDS WEST CONTINUING ALONG SAID RIGHT OF WAY LINE A DISTANCE OF 256.79 FEET; THENCE NORTH 26 DEGREES 49 MINUTES 41 SECONDS WEST CONTINUING ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 241.77 FEET; THENCE CONTINUING ALONG SAID RIGHT OF WAY LINE, ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 1784.56 FEET, A CHORD BEARING OF NORTH 18 DEGREES 00 MINUTES 47 SECONDS WEST, A CENTRAL ANGLE OF 12 DEGREES 35 MINUTES 16 SECONDS, AN ARC LENGTH OF 392.13 FEET; THENCE NORTH 32 DEGREES 04 MINUTES 12 SECONDS EAST CONTINUING ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 61.73 FEET; THENCE SOUTH 87 DEGREES 49 MINUTES 44 SECONDS EAST, A DISTANCE OF 1158.47 FEET; THENCE SOUTH 78 DEGREES 33 MINUTES 51 SECONDS EAST, A DISTANCE OF 869.58 FEET TO THE POINT OF BEGINNING.

TRACT 2:

A TRACT OF LAND BEING A PORTION OF SE BAILEY ROAD RIGHT-OF-WAY, IN THE NORTHEAST AND THE NORTHWEST QUARTERS OF SECTION 17, TOWNSHIP 47 NORTH, RANGE 31 WEST, OF THE FIFTH PRINCIPAL MERIDIAN, IN LEE'S SUMMIT, JACKSON COUNTY, MISSOURI, TO BE VACATED, AND MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTH QUARTER CORNER OF SECTION 17, TOWNSHIP 47 NORTH, RANGE 31 WEST, OF THE FIFTH PRINCIPAL

MERIDIAN, IN JACKSON COUNTY, LEE'S SUMMIT, MISSOURI, THENCE SOUTH 87° 43'34" EAST, 42.84 FEET, ON THE NORTH LINE OF SAID NORTHEAST QUARTER; THENCE SOUTH 2° 16'26" WEST, 49.97 FEET, TO A POINT ON THE WEST RIGHT-OF-WAY OF THE UNION PACIFIC RAILROAD; THENCE SOUTH 29° 25' 37" EAST, 164.35 FEET, ON SAID WEST RIGHT-OF-WAY LINE OF SAID UNION PACIFIC RAILROAD TO THE INTERSECTION OF SAID WEST RIGHT-OF-WAY LINE AND THE SOUTH RIGHT-OF-WAY LINE OF SAID SE BAILEY ROAD; THENCE NORTH 78° 33'50" WEST, 869.58 FEET, ON THE SAID SOUTH RIGHT-OF-WAY LINE OF SAID SE BAILEY ROAD TO A POINT OF DEFLECTION IN THE SOUTH RIGHT-OF-WAY LINE OF SE BAILEY ROAD; THENCE SOUTH 87° 49'40" EAST, 772.12 FEET, 50.00 FEET SOUTH OF, AND PARALLEL WITH THE NORTH LINE OF SAID NORTHWEST QUARTER TO THE POINT OF BEGINNING.

TRACT 3:

LOT THREE (3), MADDUX ACRES, A SUBDIVISION IN LEES SUMMIT, JACKSON COUNTY, MISSOURI, ACCORDING TO THE RECORDED PLAT THEREOF.

SECTION 2. That the following conditions of approval apply:

1. A modification shall be granted to the minimum 20' parking lot setback from the South M-291 Hwy right-of-way, to allow parking lot setbacks of 10' along South M-291 Hwy for Lots 14-18.
2. Development of East Village Phase 2 shall be in accordance with the preliminary development plan dated November 24, 2025; typical commercial building elevations dated November 24, 2025; apartment elevations dated August 8, 2025; and townhome elevations dated November 24, 2025.
3. Road improvements for East Village Phase 2 shall be constructed as recommended in the Transportation Impact Analysis prepared by City staff dated October 30, 2025.
4. To remain consistent with the associated East Village Phase 1, allowable land uses on the subject East Village Phase 2 property shall follow the EnVision LS permitted uses listed under UDO Section 5.560.A, 5.560.D (which include all office and retail uses permitted by right in the CP-2 zoning district under Table 6-1 of the UDO), and the following additional land uses for which an appeal to the EnVision LS land use regulations was previously granted by the City Council: automotive/truck-related uses; retail--big box in excess of 80,000 sq. ft. on one level; car washes; daycares; and drive-through restaurants.
5. Approval shall be granted for one (1) oversized, primary identification monument sign in East Village Phase 2 with an overall height of 30', 224 sq. ft. sign face area and 312 sq. ft. overall sign structure area to be located at the southwest project site boundary along South M-291 Hwy as depicted on the preliminary development plan November 24, 2025, and monument sign detail dated August 7, 2025.
6. No occupancy permits can be issued for any portion of East Village Phase 2 until downstream sanitary sewer capacity improvements have been completed, or alternate means to prevent downstream impact are approved by Water Utilities. The City has planned for an excess flow holding basin to be designed in 2028 depending on available funding to address these capacity issues.

SECTION 3. That rezoning of the property from PI to PMIX shall be as depicted on the rezoning exhibit dated September 23, 2025, and appended hereto as Attachment A. Development shall be in accordance with the preliminary development plan dated November 24, 2025, appended hereto as Attachment B; copy of UDO Sections 5.650.A and 5.650.D, appended hereto as Attachment C; commercial building elevations dated November 24, 2025, appended hereto as Attachment D; primary monument sign elevation detail dated August 7, 2025, appended hereto as Attachment E; apartment elevations dated August 8, 2025, appended hereto as Attachments F; townhome elevations dated November 24, 2025, appended hereto as Attachment G; and the Transportation Impact Analysis, dated October 30, 2025, appended hereto as Attachment H.

SECTION 4. Nonseverability. All provisions of this ordinance are so essentially and inseparably connected with, and so dependent upon, each other that no such provision would be enacted without all others. If a court of competent jurisdiction enters a final judgment on the merits that is not subject to appeal and that declares any provision or part of this ordinance void, unconstitutional, or unenforceable, then this ordinance, in its collective entirety, is invalid and shall have no legal effect as of the date of such judgment.

SECTION 5. That failure to comply with all of the provisions contained in this ordinance shall constitute violations of both this ordinance and Chapter 33, the City's Unified Development Ordinance, of the Code of Ordinances for the City of Lee's Summit.

SECTION 6. That this ordinance shall be in full force and effect from and after the date of its passage and adoption, and approval by the Mayor.

PASSED by the City Council of the City of Lee's Summit, Missouri, this 13<sup>th</sup> day of January, 2026.

ATTEST:

  
City Clerk *Trisha Fowler Arcuri*

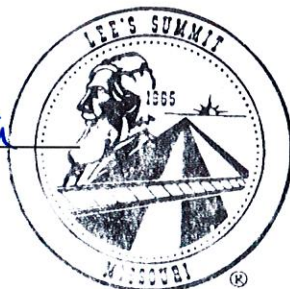


  
Mayor *William A. Baird*

APPROVED by the Mayor of said city this 14<sup>th</sup> day of January, 2026.

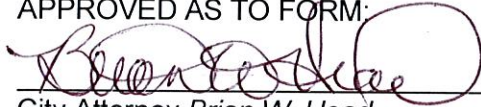
ATTEST:

  
City Clerk *Trisha Fowler Arcuri*



  
Mayor *William A. Baird*

APPROVED AS TO FORM:

  
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City Attorney *Brian W. Head*



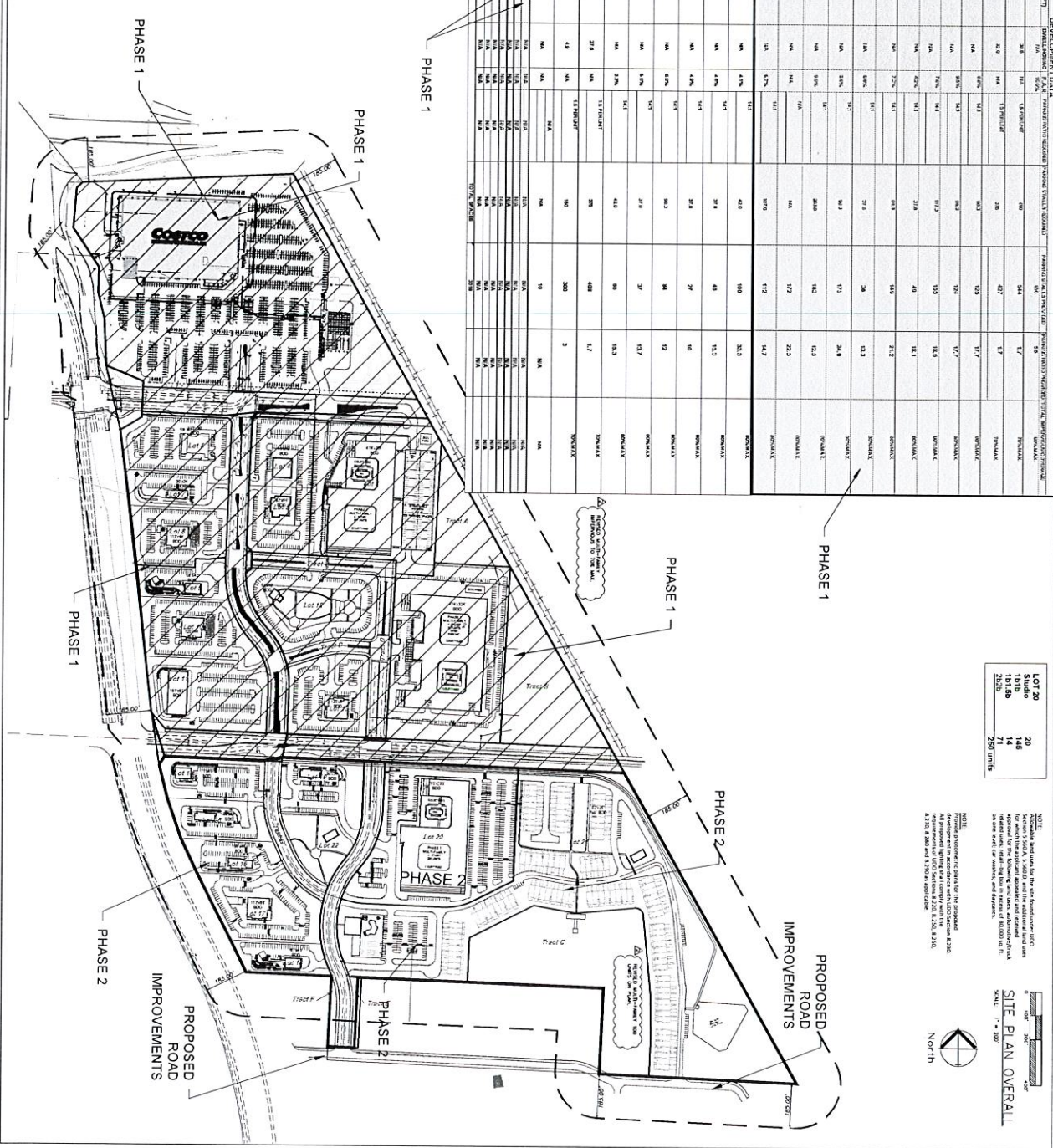
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**Sec. 5.650. Area development plan (ADP) permitted uses.**

- A. ADP permitted uses. The following uses are permitted throughout the ADP unless further modified in each "Specific Area" below or listed under "E. ADP Prohibited Uses":
1. CP-2 office and retail uses permitted by right (P) in Table 6-1 of the Unified Development Ordinance.
  2. Loft dwellings.
  3. Multi-family residential apartments, market rate, age restricted and senior.
  4. Drug store including drive-up window.
  5. Financial services, including drive-up window and drive-through facility, as a "C" use such as banks and credit unions.
  6. Bars and taverns as a "C" use.
  7. Hotel.
  8. Massage therapy as a "C" use.
  9. Restaurant, general as a "C" use.
  10. Civic or fraternal organization as a "C" use.
  11. Research, design, marketing and production needs of the general business community.
  12. Other uses specifically approved as part of a Preliminary Development Plan or further modified from the "Specific Area Uses" or "Prohibited Uses".
    - Uses shown as "C" uses must comply with the conditions established in UDO Article 6, Division II unless further modified through the approval process.
- D. LS arts and entertainment center—Specific area uses.
1. Rooftop restaurants.
  2. Restaurants/coffee shops located within a larger building.
  3. Artist studio, video production labs.
  4. Performing arts.
  5. Hospital, medical clinic prohibited.
  6. Restaurant—Drive-up and drive-thru services ~~prohibited.~~

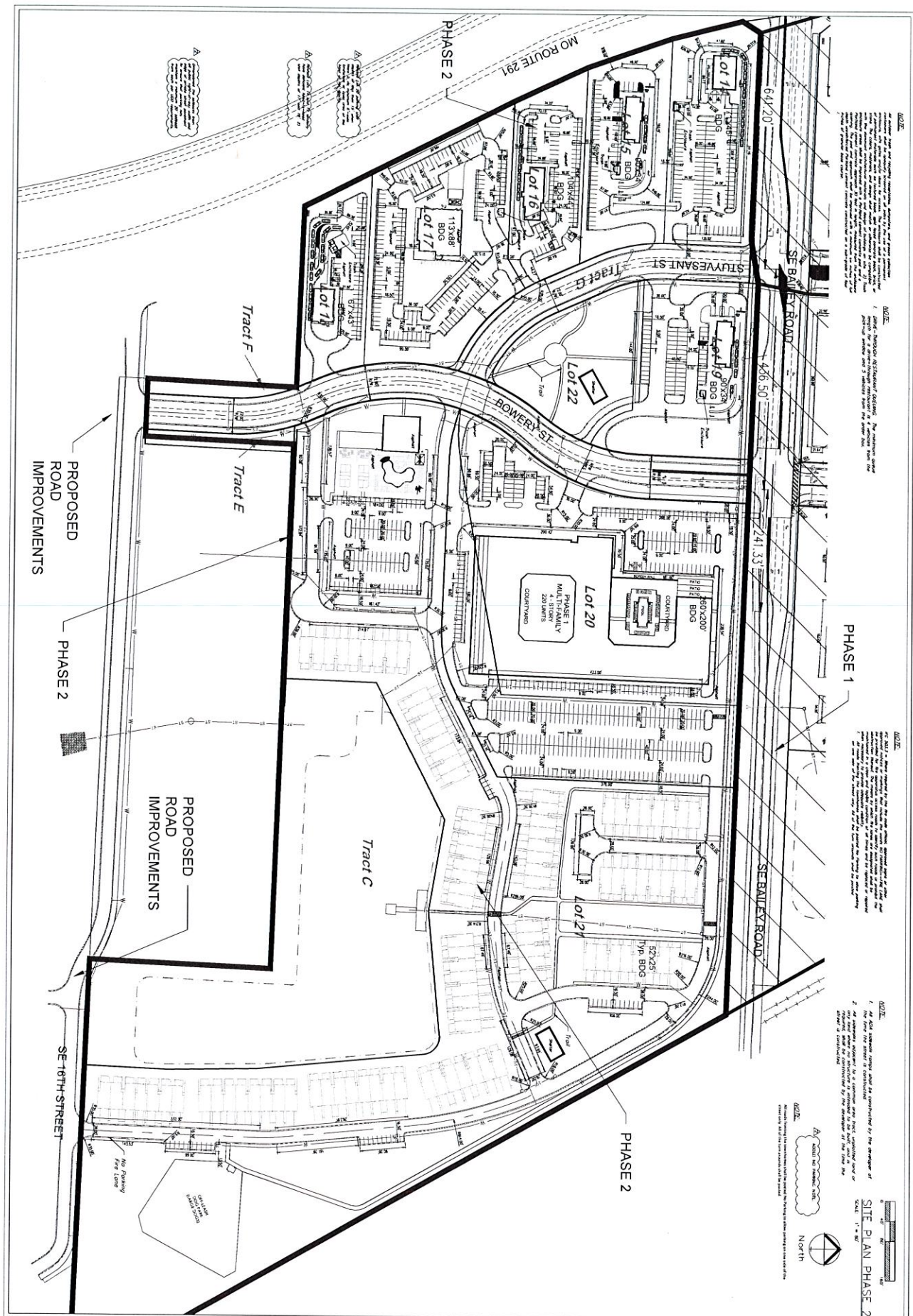


LOT #	ADDITION	LAND USE (BY AREA)	RESIDENTIAL	COMMERCIAL	INDUSTRIAL	NUMBER OF UNITS	BLDG. AREA (SQ. FT.)	COLLECTOR DRAIN	STORM DRAIN	SEWER	WATER	TRASH	RECYCLING	LANDSCAPE	OTHER	TOTAL
1	PARK	22.47					14,424	1A	1A	1A	1A	1A	1A	1A	1A	14,424
2	PARK	8.20					200,000	1A	1A	1A	1A	1A	1A	1A	1A	200,000
3	PARK	7.20					200,000	1A	1A	1A	1A	1A	1A	1A	1A	200,000
4	PARK	1.20					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
5	PARK	1.20					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
6	PARK	2.20					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
7	PARK	1.20					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
8	PARK	2.12					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
9	PARK	1.20					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
10	PARK	2.24					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
11	PARK	2.20					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
12	PARK	3.12					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
13	PARK	2.20					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
14	PARK	1.20					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
15	PARK	1.48					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
16	PARK	1.24					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
17	PARK	2.12					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
18	PARK	1.11					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
19	PARK	2.20					14,400	1A	1A	1A	1A	1A	1A	1A	1A	14,400
20	PARK	1.20					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200
21	PARK	18.43					144,000	1A	1A	1A	1A	1A	1A	1A	1A	144,000
22	PARK	1.21					7,200	1A	1A	1A	1A	1A	1A	1A	1A	7,200



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NOTES:  
1. All proposed improvements shall be in accordance with the provisions of the applicable codes and ordinances of the City of Jackson, Missouri.  
2. All proposed improvements shall be in accordance with the provisions of the applicable codes and ordinances of the State of Missouri.  
3. All proposed improvements shall be in accordance with the provisions of the applicable codes and ordinances of the County of Jackson, Missouri.  
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10. All proposed improvements shall be in accordance with the provisions of the applicable codes and ordinances of the State of Missouri.



NOTE: All dimensions are shown in feet and inches. All dimensions are to the centerline of the road unless otherwise noted. All dimensions are to the centerline of the road unless otherwise noted. All dimensions are to the centerline of the road unless otherwise noted.

NOTE: The proposed improvements shown on this plan are subject to the approval of the local government. The proposed improvements shown on this plan are subject to the approval of the local government. The proposed improvements shown on this plan are subject to the approval of the local government.

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**ENGINEERING SOLUTIONS**  
 ENGINEERING & SURVEYING  
 50 SE 30TH STREET  
 LEE'S SUMMIT, MISSOURI 64092  
 P: 816.634.0000 F: 816.634.0001

**SITE PLAN PHASE 2**  
 Preliminary Development Plans for  
 East Village  
 Lee's Summit, Jackson County, Missouri

Drawn By: [Name]  
 Date: [Date]  
 Scale: [Scale]

East Village  
 Lee's Summit, Jackson County, Missouri

Professional Engineer  
 License No. [Number]  
 State of Missouri

**ENGINEERING SOLUTIONS**  
 ENGINEERING & SURVEYING  
 50 SE 30TH STREET  
 LEE'S SUMMIT, MISSOURI 64092  
 P: 816.634.0000 F: 816.634.0001



AS SHOWN ON THIS PLAN

**NOTES**

1. INTERSECTION, LOT 13 SHALL PROVIDE CONNECTION TO PUBLIC SIDEWALK WITH PROPOSED, LOT 13 PARK PLAZA.

2. INTERSECTION, LOT 13 SHALL PROVIDE CONNECTION TO PUBLIC SIDEWALK WITH PROPOSED, LOT 13 PARK PLAZA.

3. INTERSECTION, LOT 13 SHALL PROVIDE CONNECTION TO PUBLIC SIDEWALK WITH PROPOSED, LOT 13 PARK PLAZA.

4. INTERSECTION, LOT 13 SHALL PROVIDE CONNECTION TO PUBLIC SIDEWALK WITH PROPOSED, LOT 13 PARK PLAZA.

5. INTERSECTION, LOT 13 SHALL PROVIDE CONNECTION TO PUBLIC SIDEWALK WITH PROPOSED, LOT 13 PARK PLAZA.



**SIDEWALK PLAN PHASE 2**

SCALE: 1" = 10'

**LEGEND**

Green Space

Sidewalk

C-103

Lee's Summit, Missouri  
 State of Missouri  
 License No. 117127220  
 Date: 11/17/2020

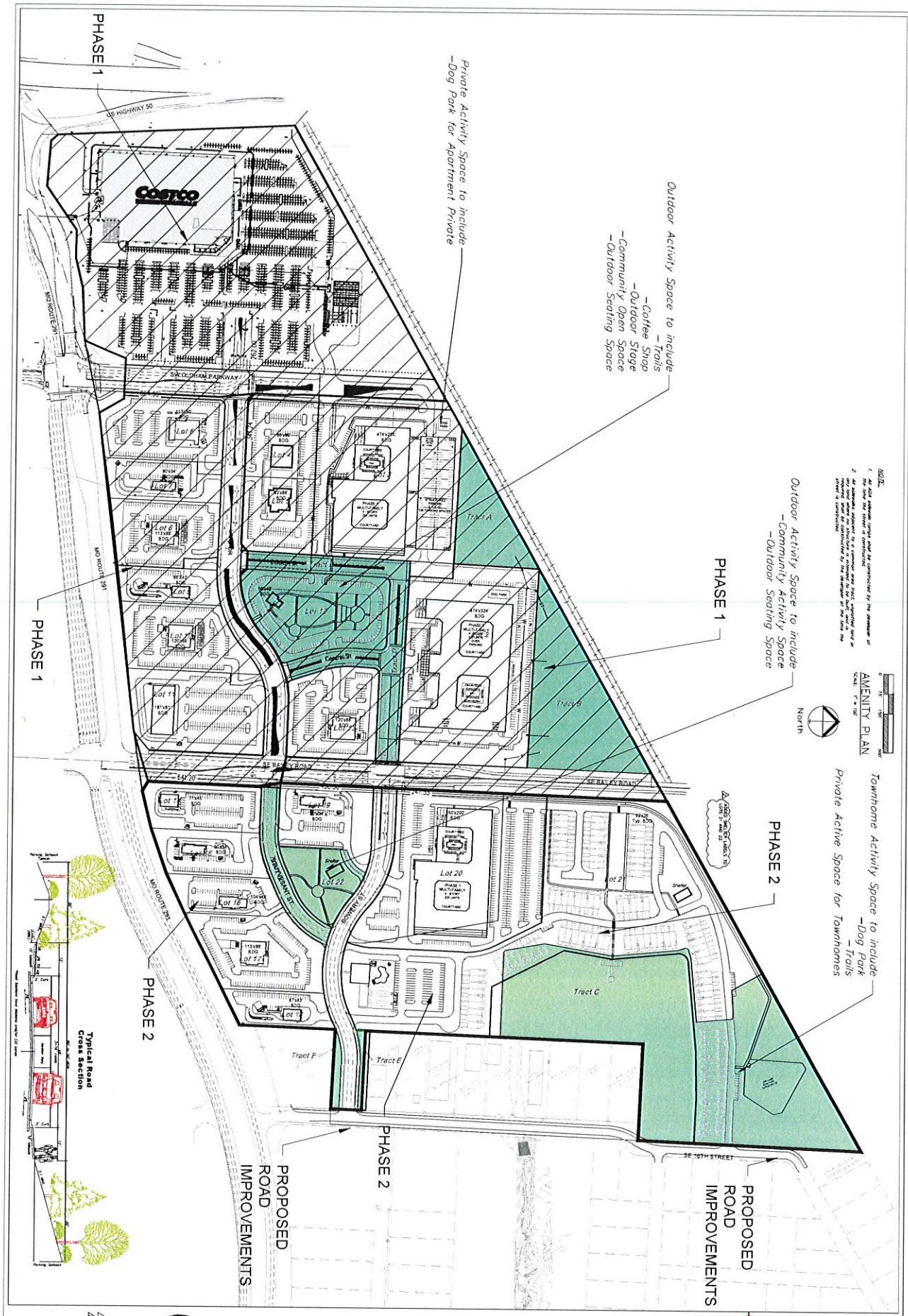
**SIDEWALK PLAN**  
 Preliminary Development Plans for:  
 East Village  
 Lee's Summit, Jackson County, Missouri

Project: 01-20-04 EAST  
 Date: 01-20-2020  
 October 8, 2020

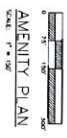
East Village  
 Lee's Summit, Jackson County, Missouri

ENGINEERING & SURVEYING SOLUTIONS  
 50 SE 10TH STREET  
 LEE'S SUMMIT, MO 64082  
 P: (816) 623-8888 F: (816) 623-8840

**ENGINEERING & SURVEYING SOLUTIONS**  
 50 SE 10TH STREET  
 LEE'S SUMMIT, MO 64082  
 P: (816) 623-8888 F: (816) 623-8840



**NOTE:**  
 1. All existing structures shall be demolished by the developer at the start of construction.  
 2. All existing structures to be removed shall be demolished and the site shall be returned to its original condition or better.



**Tomhome Activity Space to include**  
 - Dog Park  
 - Trails

**Outdoor Activity Space to include**  
 - Community Activity Space  
 - Outdoor Seating Space

**Outdoor Activity Space to include**  
 - Coffee Shop  
 - Outdoor Stage  
 - Community Open Space  
 - Outdoor Seating Space

**Private Activity Space to include**  
 - Dog Park for Apartment Private

**PROPOSED ROAD IMPROVEMENTS**

**PROPOSED ROAD IMPROVEMENTS**

C104



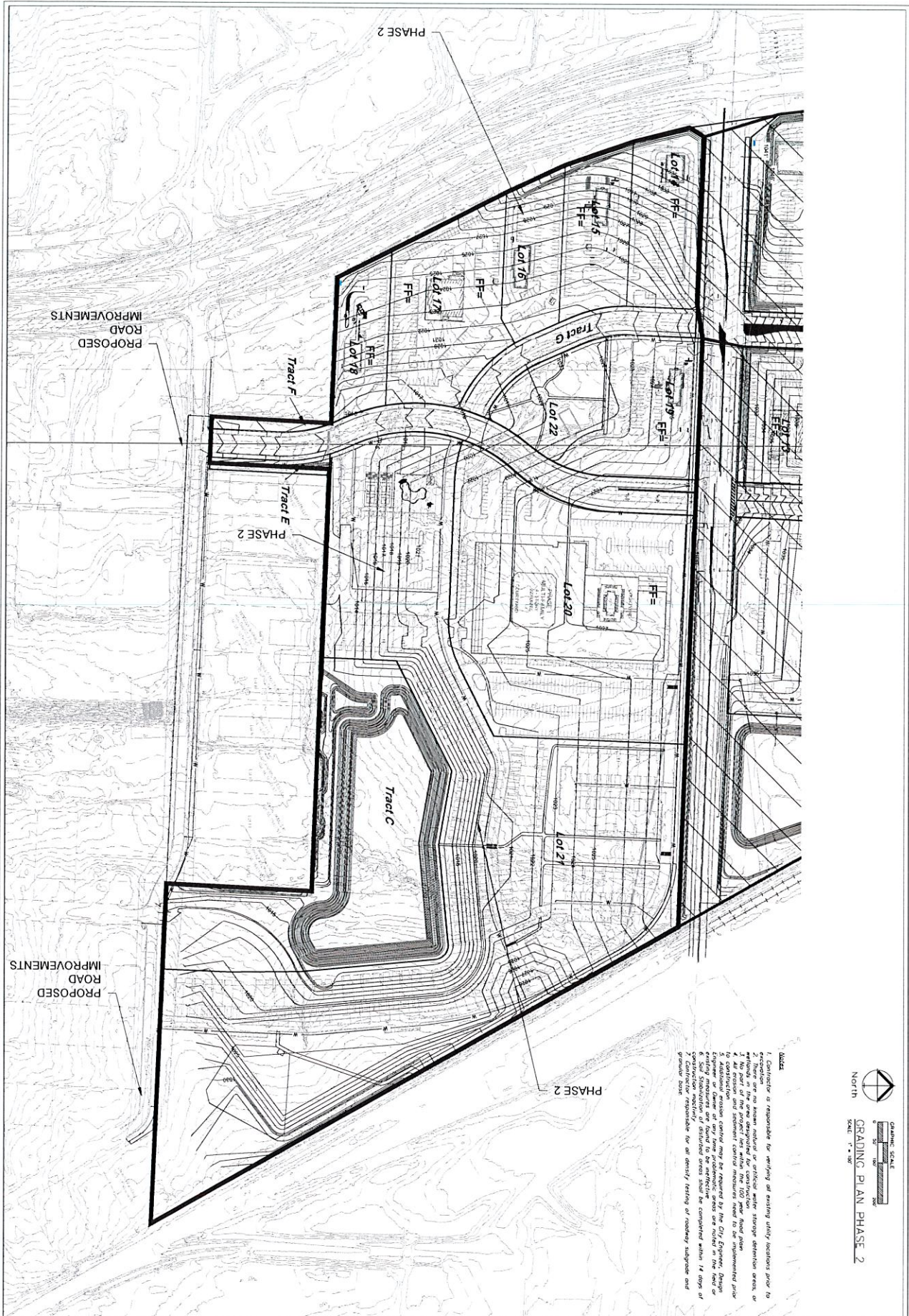
**AMENITY PLAN**  
 Preliminary Development Plans for  
 East Village  
 Lee's Summit, Jackson County, Missouri

Project: 20-020-001 EAST  
 Date: 01/11/2023

East Village  
 Lee's Summit, Jackson County, Missouri

**ENGINEERING & SURVEYING SOLUTIONS**  
 50 SE 16TH STREET  
 LEE'S SUMMIT, MO 64083  
 P: 816.325.9888 F: 816.325.9849





GRAPHIC SCALE  
 GRADING PLAN PHASE 2  
 SCALE: 1" = 30'

NOTES  
 1. Contractor is responsible for verifying all existing utility locations prior to excavation. The contractor shall be responsible for providing adequate storage diversion areas, or methods on the site designated for construction materials, within the 100' road frontage.  
 2. No part of the project lies within the 100' road frontage.  
 3. The contractor shall be responsible for providing adequate storage diversion areas, or methods on the site designated for construction materials, within the 100' road frontage.  
 4. The contractor shall be responsible for providing adequate storage diversion areas, or methods on the site designated for construction materials, within the 100' road frontage.  
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 10. The contractor shall be responsible for providing adequate storage diversion areas, or methods on the site designated for construction materials, within the 100' road frontage.

MISSOURI STATE BOARD OF SURVEYORS  
 LICENSE NO. 117242/2020  
 DATE: 11/24/2020



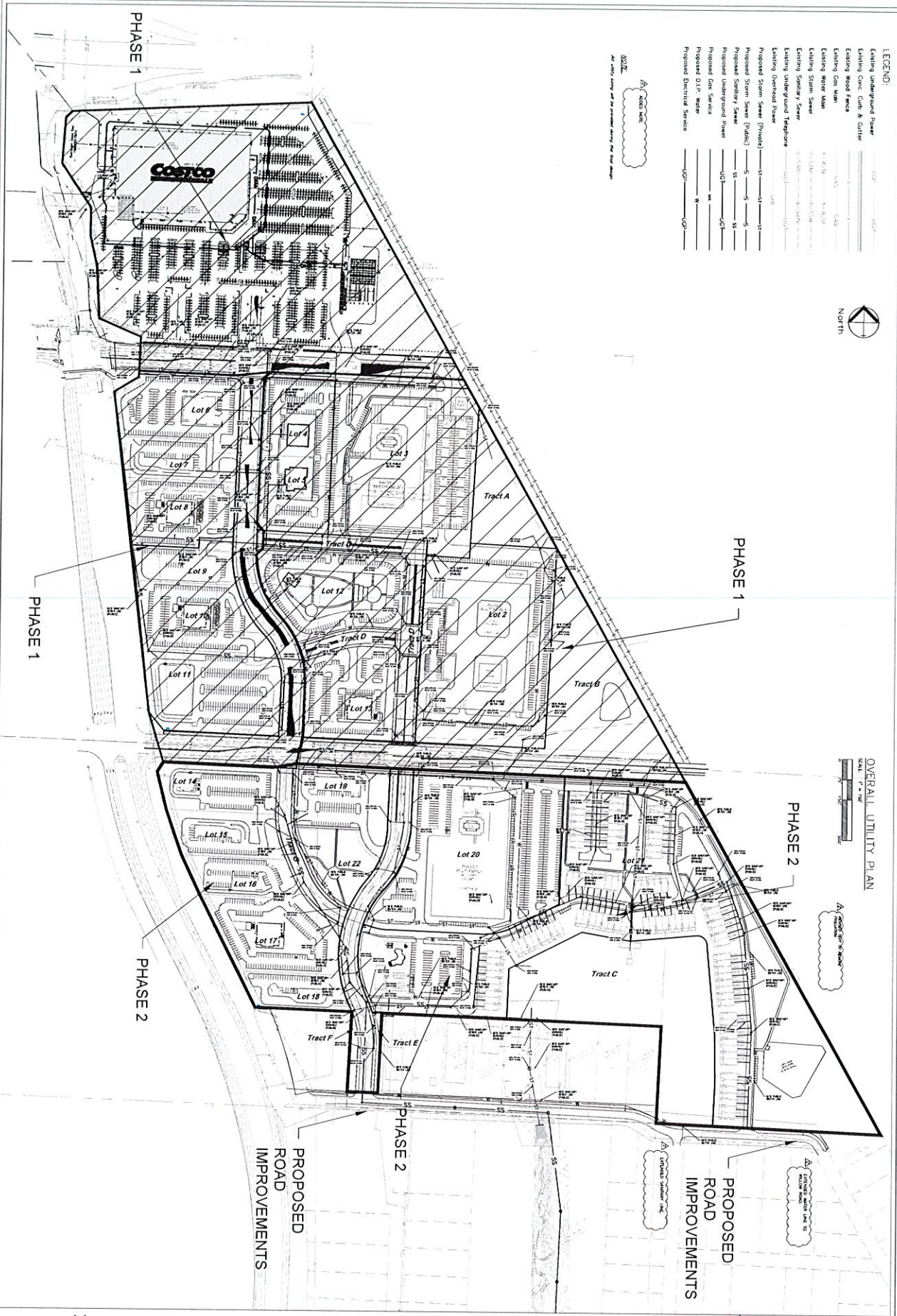
Grading Plan Phase 2  
 Preliminary Development Plans for:  
 East Village  
 Lee's Summit, Jackson County, Missouri

Project: EAST VILLAGE  
 Date: 9/2020

East Village  
 Lee's Summit, Jackson County, Missouri

DESIGNED BY: [Name]  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 APPROVED BY: [Name]

**ENGINEERING SOLUTIONS**  
 ENGINEERING & SURVEYING  
 50 SE 30TH STREET  
 LEE'S SUMMIT, MO 64082  
 P (816) 633-9888 F (816) 633-9849



**LEGEND:**

Existing Underground Power	---
Existing Power, Curb & Gutter	---
Existing Road Pavement	---
Existing Gas Water	---
Existing Water Main	---
Existing Storm Sewer	---
Existing Sanitary Sewer	---
Existing Underground Telephone	---
Existing Overhead Power	---
Proposed Storm Sewer (Private)	---
Proposed Storm Sewer (Public)	---
Proposed Sanitary Sewer	---
Proposed Underground Power	---
Proposed Gas Services	---
Proposed Oil, Water	---
Proposed Electrical Services	---



**OVERALL UTILITY PLAN**

Storm Sewer Plan  
 Preliminary Development Plans for:  
 East Village  
 Lee's Summit, Jackson County, Missouri

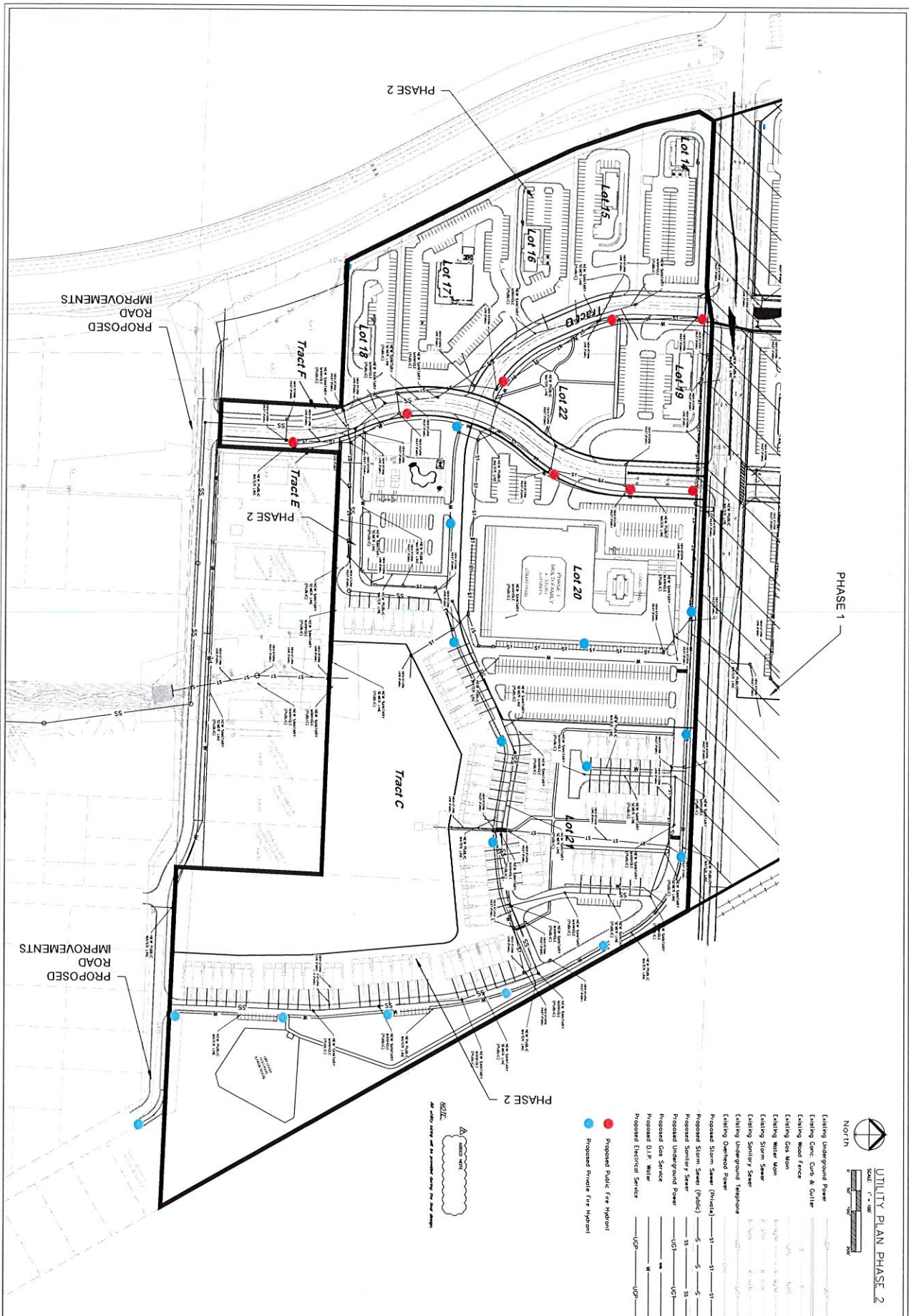


Project: D:\DAM EAST  
 Date: 8/2025

East Village  
 Lee's Summit, Jackson County, Missouri

Lee's Summit Engineering & Surveying Solutions  
 50 SE 10TH STREET  
 LEE'S SUMMIT, MO 64089  
 P: (816) 623-9888 F: (816) 623-7940

**ENGINEERING SOLUTIONS**  
 ENGINEERING & SURVEYING



PHASE 1

PHASE 2

PROPOSED ROAD IMPROVEMENTS

PROPOSED ROAD IMPROVEMENTS

UTILITY PLAN PHASE 2  
 SCALE: 1" = 50'  
 NORTH

- Existing Underground Power
- Existing Concrete Curb & Gutter
- Existing Mowed Areas
- Existing Gas Main
- Existing Water Main
- Existing Storm Sewer
- Existing Sanitary Sewer
- Existing Underground Telephone
- Proposed Storm Sewer (Private)
- Proposed Storm Sewer (Public)
- Proposed Sanitary Sewer
- Proposed Underground Power
- Proposed Gas Service
- Proposed D.I.P. Meter
- Proposed Electrical Service
- Proposed Public Fire Hydrant
- Proposed Private Fire Hydrant



ENGINEERING & SURVEYING SOLUTIONS  
 50 SE NINTH STREET  
 LEE'S SUMMIT, MO 64082  
 P (816) 625-8887 F (816) 625-9949

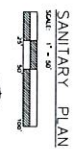
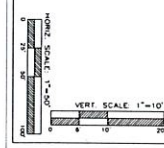
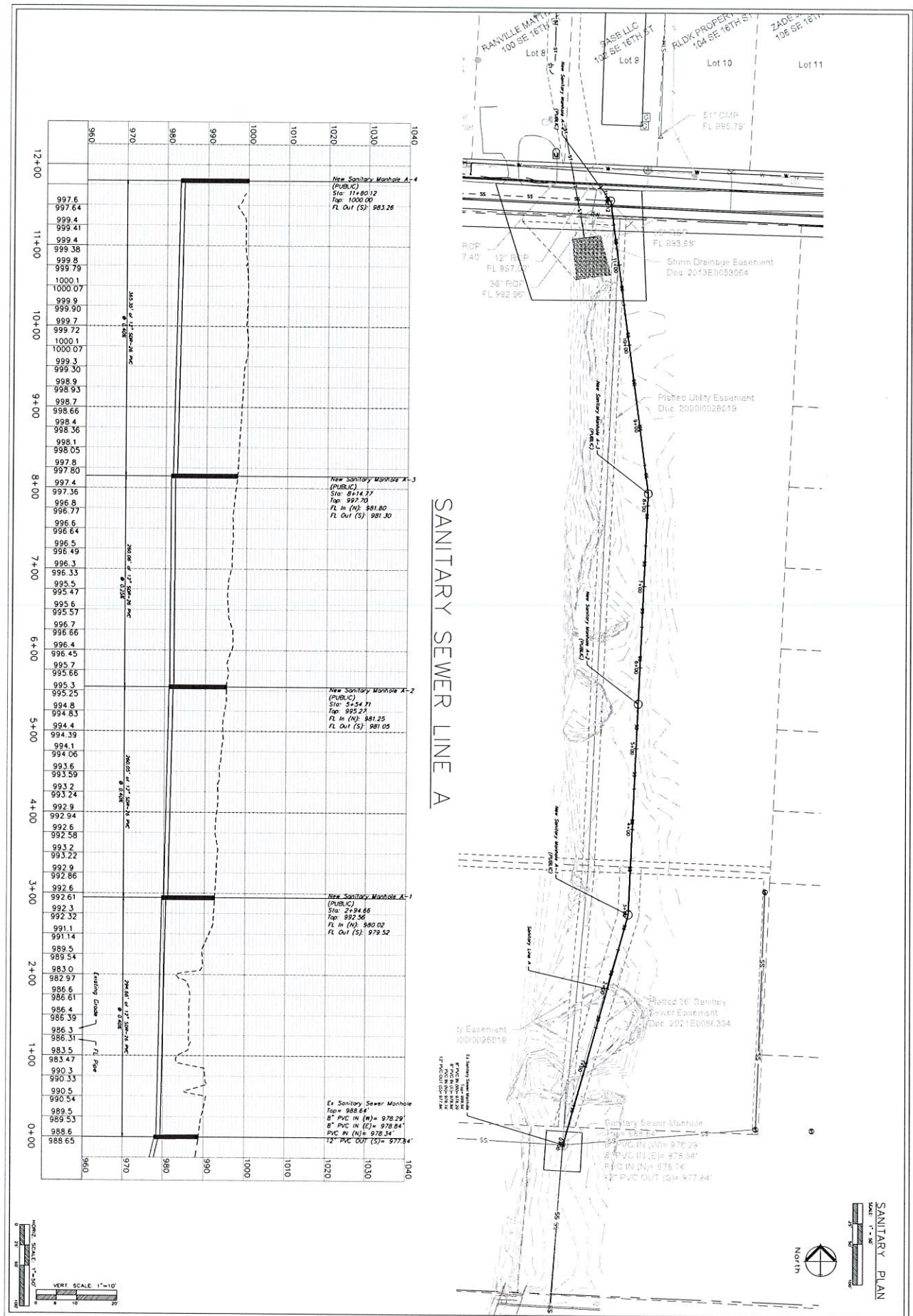


UTILITY PLAN PHASE 2  
 Preliminary Development Plans for  
 East Village  
 Lee's Summit, Jackson County, Missouri

East Village  
 Lee's Summit, Jackson County, Missouri

PROJECT: East Village  
 DATE: 11/26/2023  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]







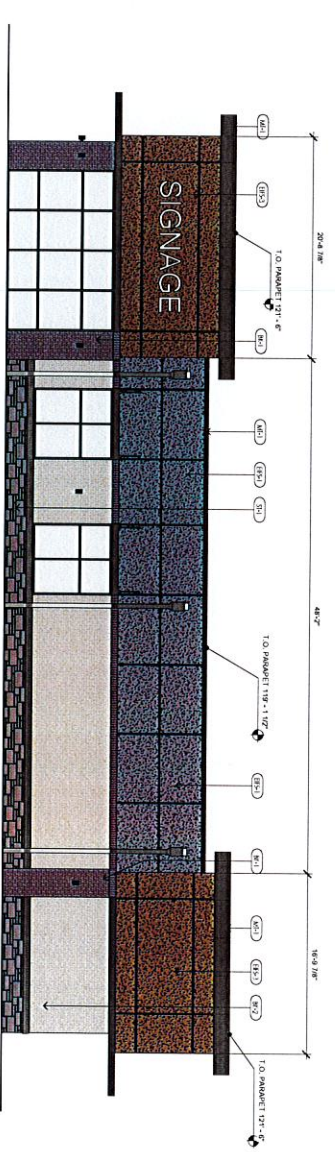
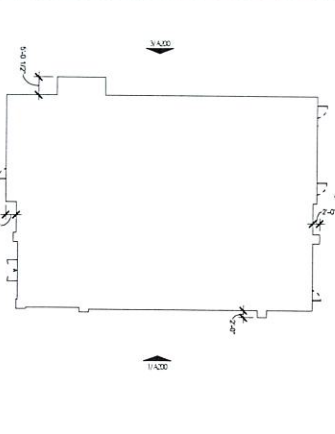
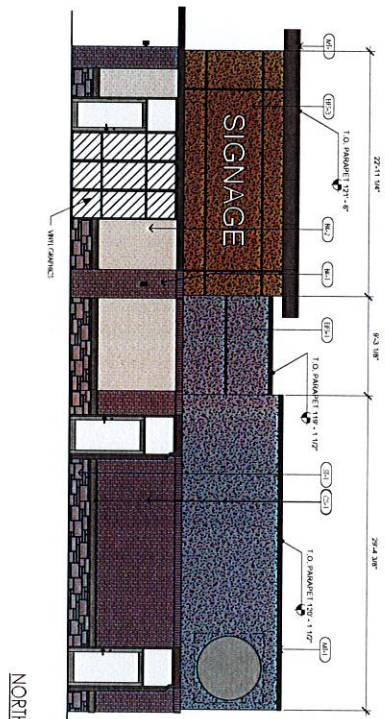
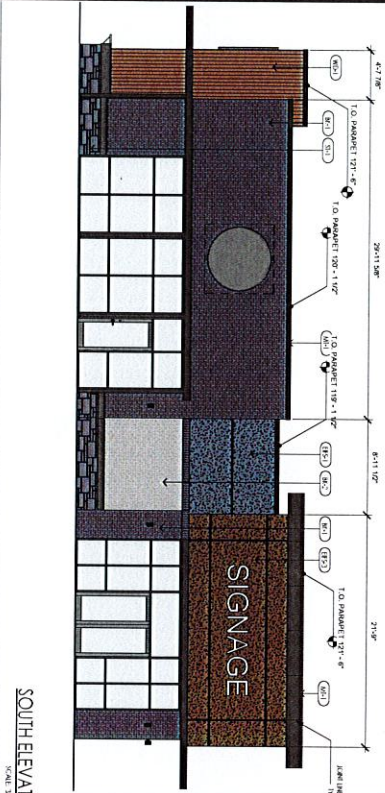
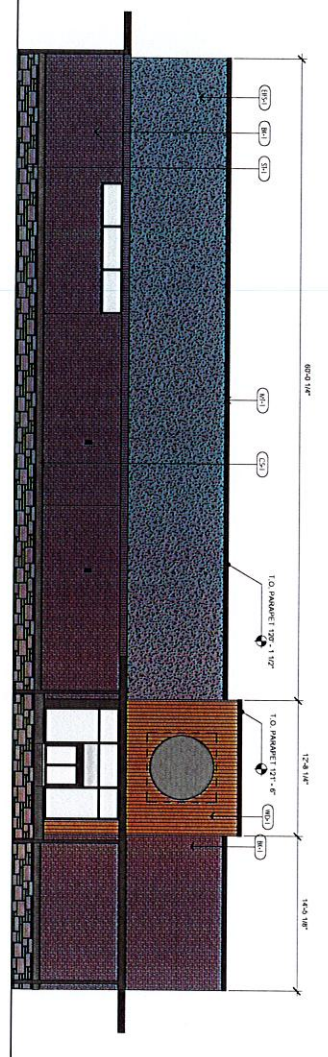


**EXTERIOR FINISH LEGEND**

NO.	DESCRIPTION	FINISH	NOTES
1	CONCRETE	CONCRETE	
2	BRICK	BRICK	
3	STONE	STONE	
4	WOOD	WOOD	
5	GLASS	GLASS	
6	PAINT	PAINT	
7	EIFS	EIFS	
8	EIFS	EIFS	
9	EIFS	EIFS	
10	EIFS	EIFS	
11	EIFS	EIFS	
12	EIFS	EIFS	
13	EIFS	EIFS	
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96	EIFS	EIFS	
97	EIFS	EIFS	
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99	EIFS	EIFS	
100	EIFS	EIFS	

**FRONT PORCH ELEVATION CLASS 1 & 2 MATERIAL CALCULATIONS**

CLASS	AREA	UNIT	QUANTITY
CLASS 1 MATERIAL TOTAL	132.23	SQ. FT.	132.23
CLASS 2 MATERIAL TOTAL	44.25	SQ. FT.	44.25
CLASS 3 MATERIAL TOTAL	381.51	SQ. FT.	381.51
CLASS 4 MATERIAL TOTAL	29.25	SQ. FT.	29.25
CLASS 5 MATERIAL TOTAL	14.25	SQ. FT.	14.25
CLASS 6 MATERIAL TOTAL	13.25	SQ. FT.	13.25
CLASS 7 MATERIAL TOTAL	4.25	SQ. FT.	4.25
CLASS 8 MATERIAL TOTAL	1.25	SQ. FT.	1.25
CLASS 9 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 10 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 11 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 12 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 13 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 14 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 15 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 16 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 17 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 18 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 19 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 20 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 21 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 22 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 23 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 24 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 25 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 26 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 27 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 28 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 29 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 30 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 31 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 32 MATERIAL TOTAL	0.25	SQ. FT.	0.25
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CLASS 34 MATERIAL TOTAL	0.25	SQ. FT.	0.25
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CLASS 36 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 37 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 38 MATERIAL TOTAL	0.25	SQ. FT.	0.25
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CLASS 40 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 41 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 42 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 43 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 44 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 45 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 46 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 47 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 48 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 49 MATERIAL TOTAL	0.25	SQ. FT.	0.25
CLASS 50 MATERIAL TOTAL	0.25	SQ. FT.	0.25



drawing number: **AP200**

DATE: 10/15/2014

PROJECT: DRAKE DEVELOPMENT EAST VILLAGE PHASE 2 LEE'S SUMMIT, MO

**DRAKE DEVELOPMENT EAST VILLAGE PHASE 2 LEE'S SUMMIT, MO**

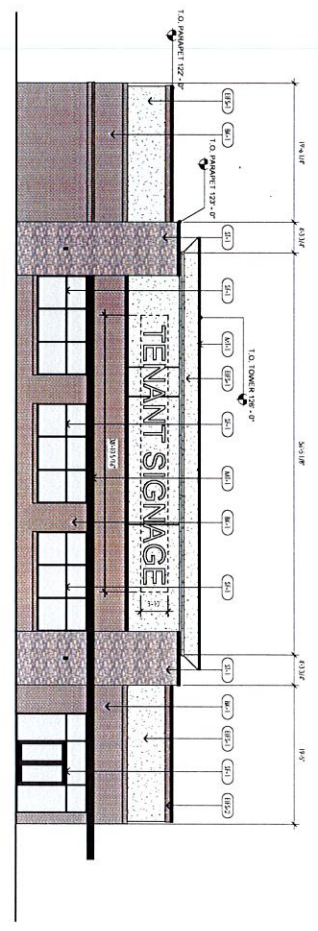
**Kloverarchitects**

8613 PENROSE LANE, SUITE 400 • LENEXA, KS 66219  
ph: 913.649.6161 • f: 913.649.1275 • www.klover.net

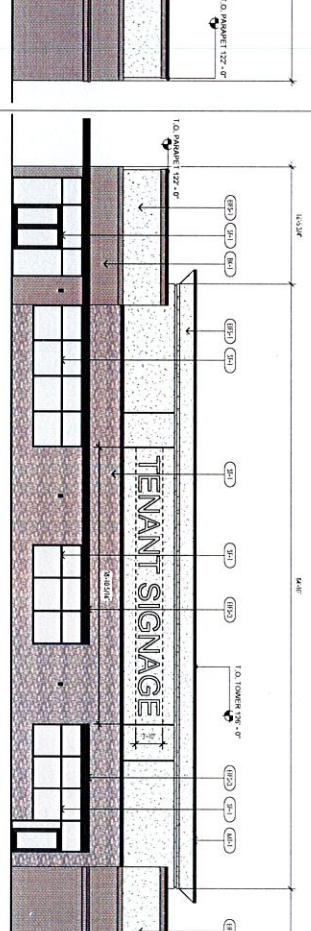


EXTENSION FINISH SCHEDULE			
TAC	MATERIAL	MANUFACTURER	COMMENTS
MS-1	FULL BRICK	BELTON BRICK	STANDARD GRAY GROUP
EP-1	EPDM	SPRINT	—
EP-2	EPDM	SPRINT	—
EP-3	EPDM	SPRINT	—
MS-1	METAL	TRB	—
MS-2	STAINLESS	KAMMER OREG	CLAM GLAZING
MS-3	STAINLESS	KAMMER OREG	STANDARD GRAY GROUP
MS-4	STONE	INDUSTRIAL BRICK	—
MS-5	STONE	INDUSTRIAL BRICK	—

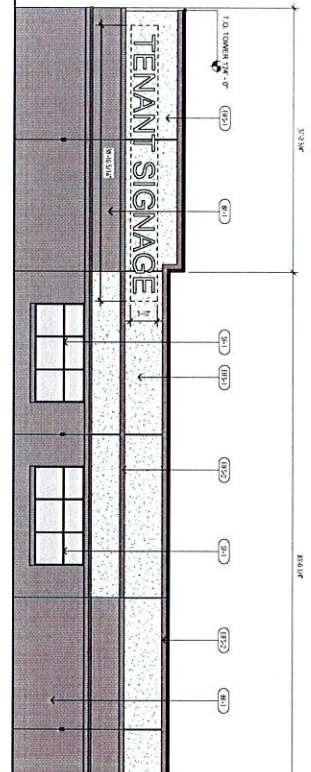
NON-FACADE CLASS 1 & 3 MATERIAL CALCULATIONS	
FACADE TOTAL	2,287 SF
CLASS 1 MATERIAL TOTAL	1,247 SF
CLASS 2 MATERIAL TOTAL	344 SF
CLASS 3 MATERIAL TOTAL	696 SF
STONE FINISH TOTAL	146 SF
CLASS 1 & 3 MATERIAL TOTAL	1,421 SF



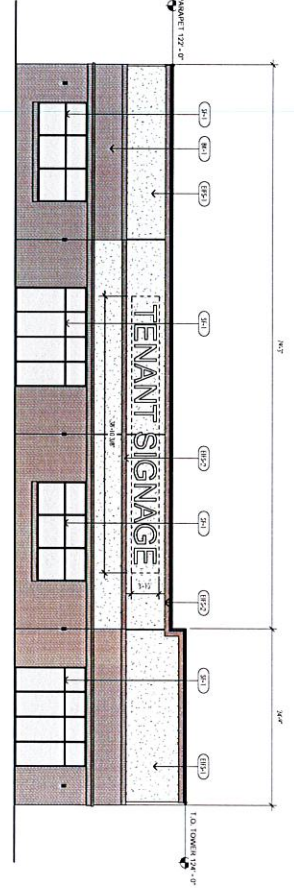
NORTH ELEVATION 3  
SCALE: 1/8" = 1'-0"



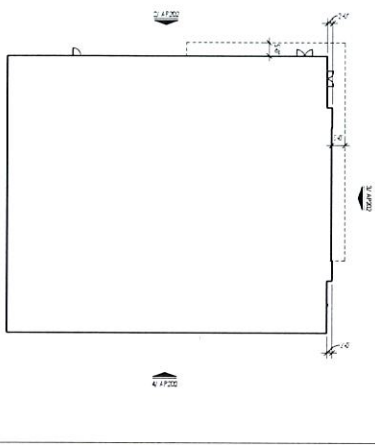
WEST ELEVATION 2  
SCALE: 1/8" = 1'-0"



EAST ELEVATION 4  
SCALE: 1/8" = 1'-0"



SOUTH ELEVATION 1  
SCALE: 1/8" = 1'-0"



KEY PLAN 5  
SCALE: 3/4" = 1'-0"

DRAKE DEVELOPMENT  
EAST VILLAGE  
PHASE 2  
LEE'S SUMMIT, MO

**kloverarchitects**  
8813 PENROSE LANE, SUITE 400 • LENEXA, KS 66219  
ph. 913.649.8161 • fx. 913.649.1275 • www.klover.net

project number: AP202  
drawing discipline: ARCHITECTURE  
drawing revision: 01  
drawing title: ARCHITECTURAL ELEVATIONS  
DATE: 10/20/2020  
DRAWN BY: JACOB  
CHECKED BY: JACOB  
DESIGNED BY: JACOB  
DATE: 10/20/2020

not for construction

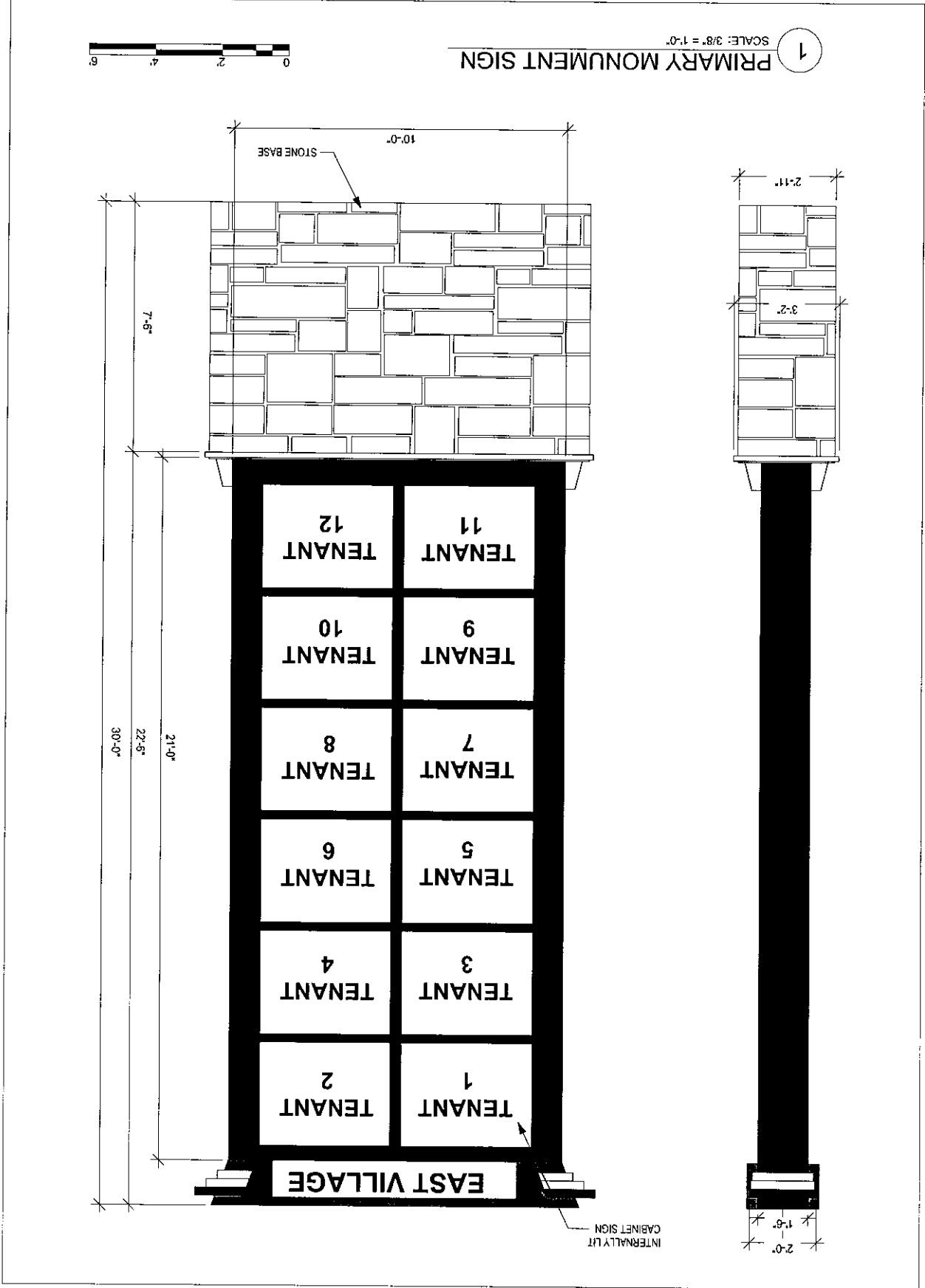


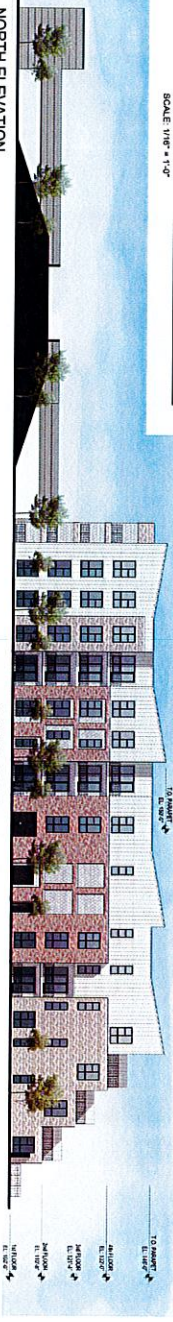
**MONUMENT  
EAST VILLAGE  
LEES SUMMIT,**

SUBMISSION  
DATES  
8/7/2025

SHEET TITLE  
PRIMARY  
MONUMENT SIGN

SHEET NUMBER  
**A-101**



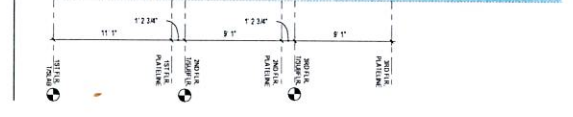


**EXTERIOR MATERIAL LIST**

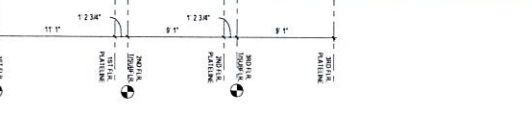
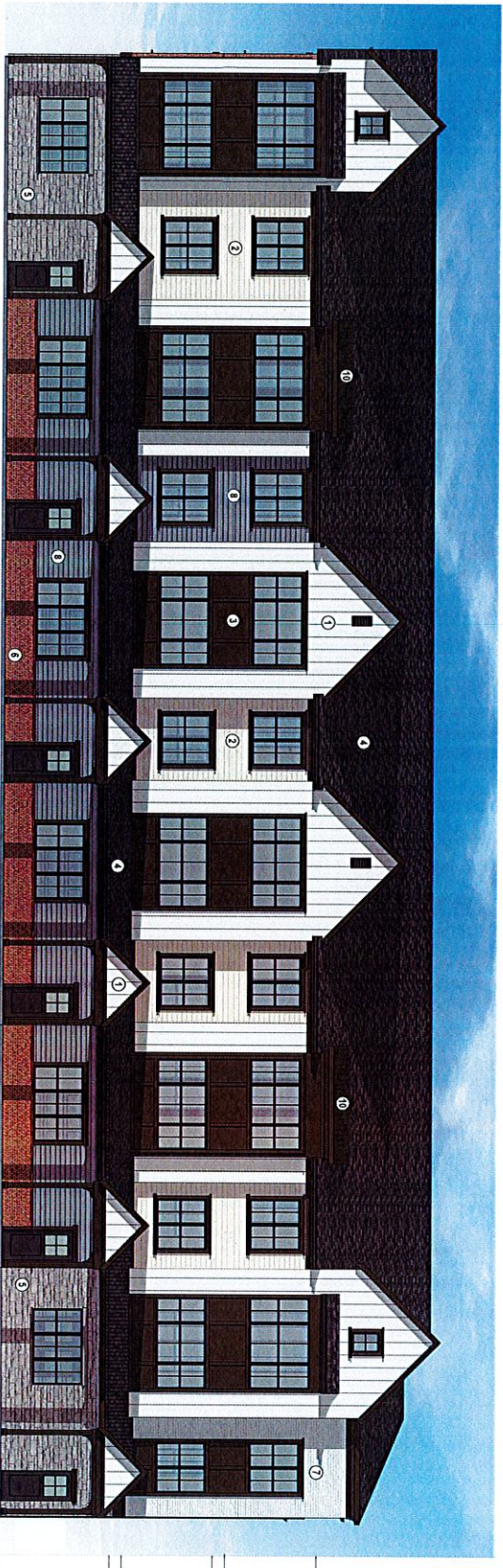
001	BRICK	1/2" x 4" x 8" (COMMON BRICK)
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003	BRICK	1/2" x 4" x 8" (COMMON BRICK)
004	BRICK	1/2" x 4" x 8" (COMMON BRICK)
005	BRICK	1/2" x 4" x 8" (COMMON BRICK)
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020	BRICK	1/2" x 4" x 8" (COMMON BRICK)
021	BRICK	1/2" x 4" x 8" (COMMON BRICK)
022	BRICK	1/2" x 4" x 8" (COMMON BRICK)
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026	BRICK	1/2" x 4" x 8" (COMMON BRICK)
027	BRICK	1/2" x 4" x 8" (COMMON BRICK)
028	BRICK	1/2" x 4" x 8" (COMMON BRICK)
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047	BRICK	1/2" x 4" x 8" (COMMON BRICK)
048	BRICK	1/2" x 4" x 8" (COMMON BRICK)
049	BRICK	1/2" x 4" x 8" (COMMON BRICK)
050	BRICK	1/2" x 4" x 8" (COMMON BRICK)

- MATERIAL LEGEND**  
 MATERIALS TO BE USED FOR INTERIORS AND EXTERIORS  
 NUMBERED TO CORRELATE WITH THE COLOR SCHEMES
- 1 FIBRE CEMENT BRICKS (MANTLE)
  - 2 WHITE
  - 3 FIBRE CEMENT LAP SIDING
  - 4 HORIZONTAL SIDING
  - 5 ASPHALT SHINGLES (DARK BROWN)
  - 6 FIBRE CEMENT SIDING
  - 7 FIBRE CEMENT SIDING
  - 8 FIBRE CEMENT SIDING
  - 9 FIBRE CEMENT SIDING
  - 10 FIBRE CEMENT SIDING
  - 11 CONCRETE BLOCK WALL (EXTERIOR)

- COLOR SCHEME A (COOL TONES)**
- 1 FIBRE CEMENT BRICKS (MANTLE)
  - 2 WHITE
  - 3 FIBRE CEMENT LAP SIDING
  - 4 HORIZONTAL SIDING
  - 5 ASPHALT SHINGLES (DARK BROWN)
  - 6 FIBRE CEMENT SIDING
  - 7 FIBRE CEMENT SIDING
  - 8 FIBRE CEMENT SIDING
  - 9 FIBRE CEMENT SIDING
  - 10 FIBRE CEMENT SIDING
  - 11 CONCRETE BLOCK WALL (EXTERIOR)



2 REAR ELEVATION  
 SCALE: 3/16" = 1'-0"

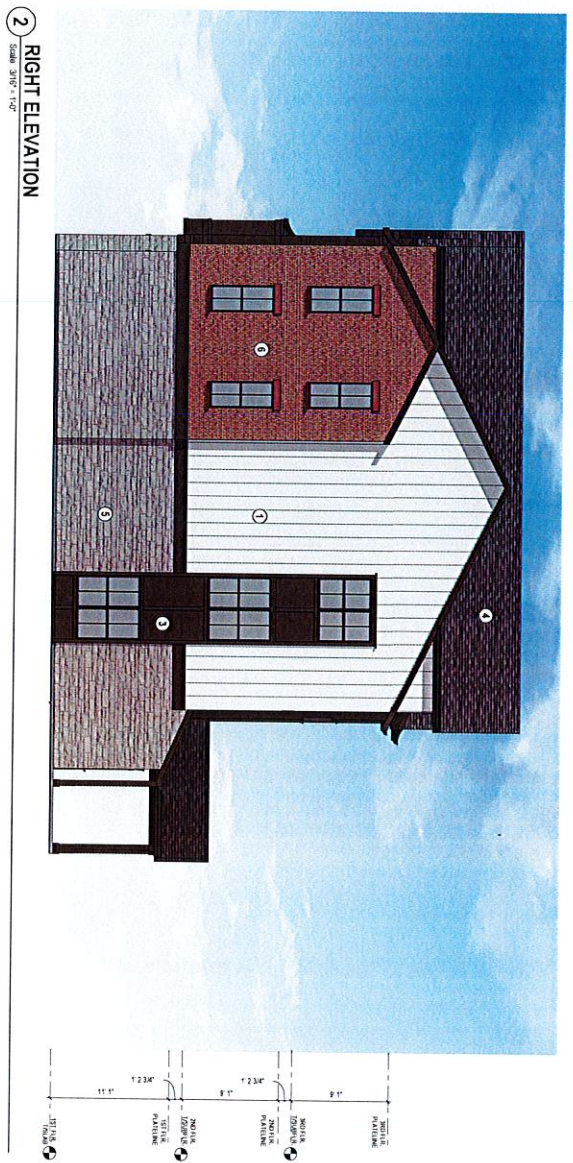


1 FRONT ELEVATION  
 SCALE: 3/16" = 1'-0"

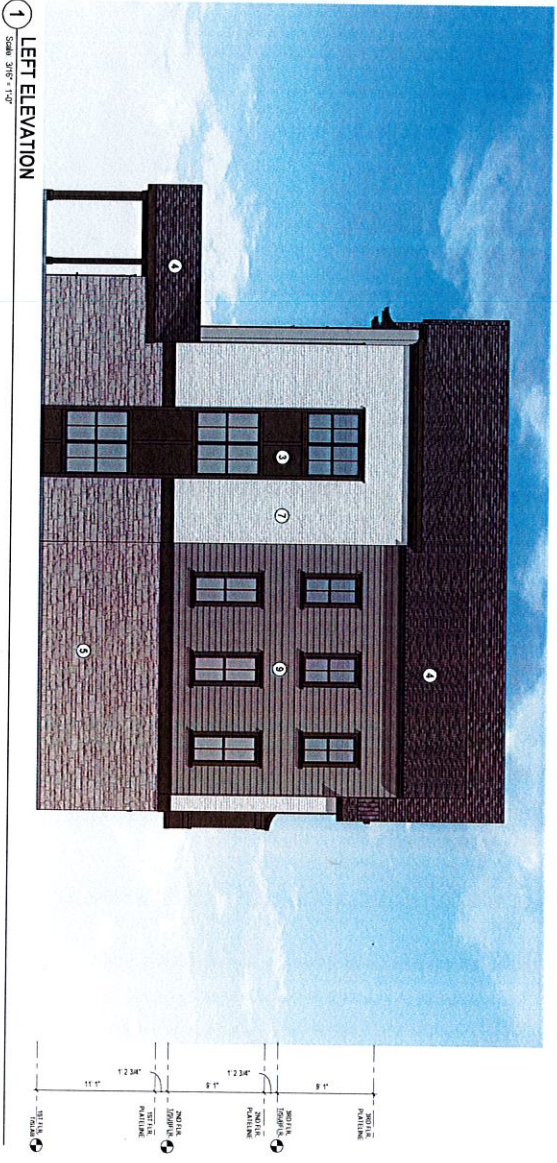
**MATERIAL LEGEND**  
APPLY TO ALL EXTERIOR SURFACES UNLESS OTHERWISE NOTED

- COLOR SCHEME A (POOL TONES)**
- 1 FINE GRAY
  - 2 FINE GRAY
  - 3 FINE GRAY
  - 4 FINE GRAY
  - 5 FINE GRAY
  - 6 FINE GRAY
  - 7 FINE GRAY
  - 8 FINE GRAY
  - 9 FINE GRAY
  - 10 FINE GRAY
  - 11 FINE GRAY

- COLOR SCHEME B (MARM TONES)**
- 1 FINE GRAY
  - 2 FINE GRAY
  - 3 FINE GRAY
  - 4 FINE GRAY
  - 5 FINE GRAY
  - 6 FINE GRAY
  - 7 FINE GRAY
  - 8 FINE GRAY
  - 9 FINE GRAY
  - 10 FINE GRAY
  - 11 FINE GRAY



2 RIGHT ELEVATION  
 Scale: 3/16" = 1'-0"



1 LEFT ELEVATION  
 Scale: 3/16" = 1'-0"

- MATERIAL LEGEND**  
 MATERIALS TO BE USED IN THIS PROJECT  
 (MATERIALS TO BE USED IN THIS PROJECT)
- 1 HERRING BONE BRICKS (MATTEN)
  - 2 WHITE
  - 3 HERRING BONE BRICKS (MATTEN)
  - 4 HERRING BONE BRICKS (MATTEN)
  - 5 HERRING BONE BRICKS (MATTEN)
  - 6 HERRING BONE BRICKS (MATTEN)
  - 7 HERRING BONE BRICKS (MATTEN)
  - 8 HERRING BONE BRICKS (MATTEN)
  - 9 HERRING BONE BRICKS (MATTEN)
  - 10 HERRING BONE BRICKS (MATTEN)
  - 11 HERRING BONE BRICKS (MATTEN)

- COLOR SCHEME B (WARM TONES)**
- 1 HERRING BONE BRICKS (MATTEN)
  - 2 HERRING BONE BRICKS (MATTEN)
  - 3 HERRING BONE BRICKS (MATTEN)
  - 4 HERRING BONE BRICKS (MATTEN)
  - 5 HERRING BONE BRICKS (MATTEN)
  - 6 HERRING BONE BRICKS (MATTEN)
  - 7 HERRING BONE BRICKS (MATTEN)
  - 8 HERRING BONE BRICKS (MATTEN)
  - 9 HERRING BONE BRICKS (MATTEN)
  - 10 HERRING BONE BRICKS (MATTEN)
  - 11 HERRING BONE BRICKS (MATTEN)



2 REAR ELEVATION  
 Scale: 3/16" = 1'-0"



1 FRONT ELEVATION  
 Scale: 3/16" = 1'-0"

**PRELIMINARY ELEVATIONS - 2-STORY END CAP**

OLDHAM VILLAGE EAST / DRAKE DEVELOPMENT

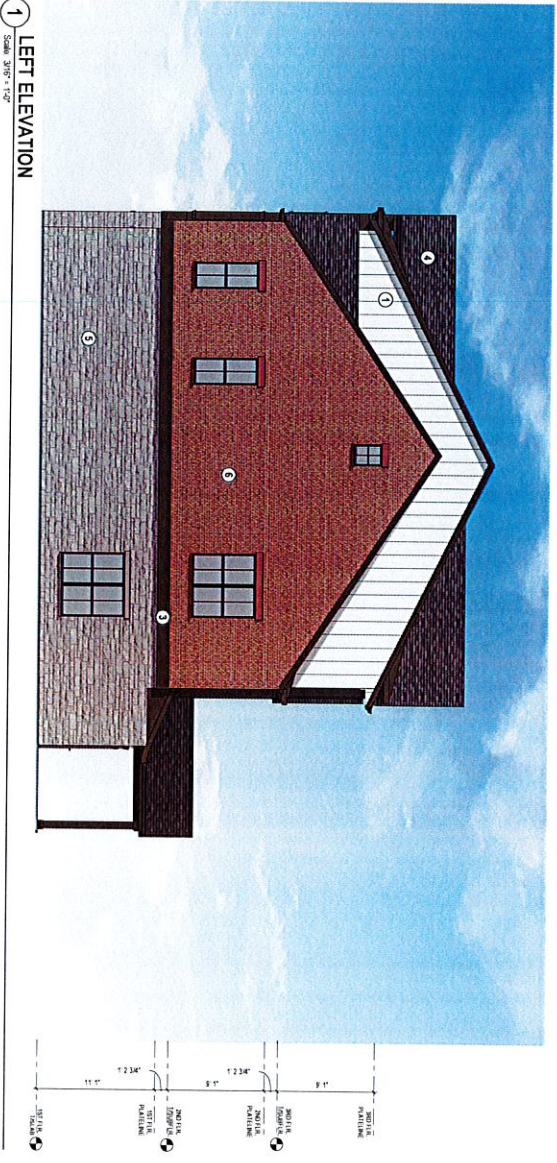
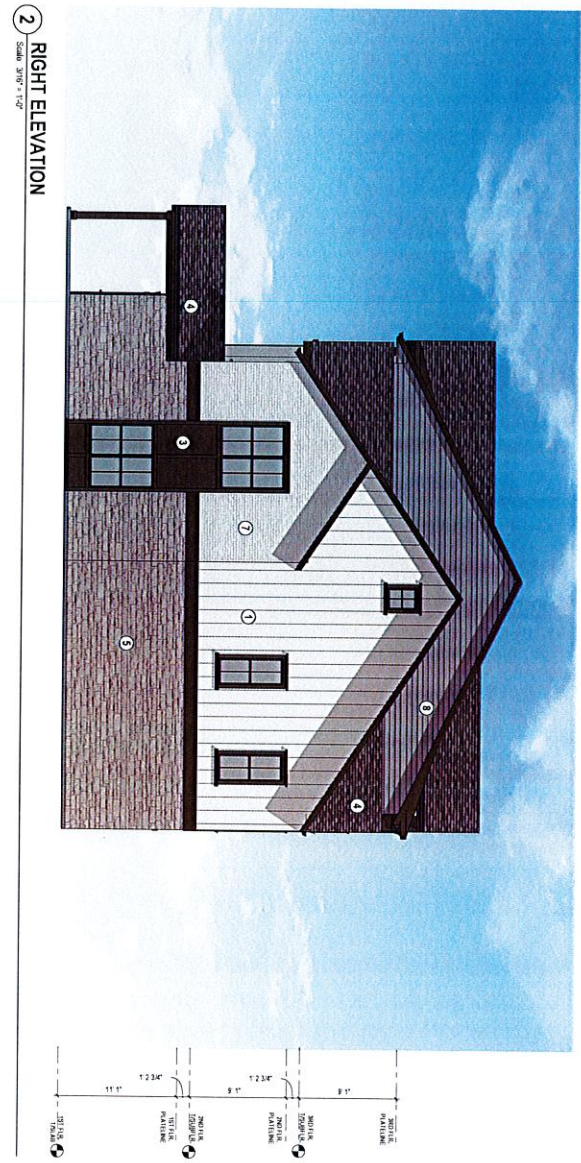
#####  
 November 24, 2025



**MATERIAL LEGEND**  
 MATERIALS TO BE USED IN THIS PROJECT ARE LISTED BELOW. THE MATERIALS LISTED ARE SUBJECT TO CHANGE WITHOUT NOTICE.

- COLOR SCHEME A (COOL TONES)**
- 1 FRESH CLAMNET DORMER MATERIAL
  - 2 WHITE
  - 3 FRESH CLAMNET LAP SIDING
  - 4 FRESH CLAMNET PANEL SIDING
  - 5 FRESH CLAMNET PANEL SIDING
  - 6 ASPHALT SHINGLES
  - 7 DARK BRICK
  - 8 FINE STONE SHINGLES
  - 9 DARK GRAY
  - 10 FRESH BRICK
  - 11 WHITE
  - 12 FRESH CLAMNET LAP SIDING
  - 13 FRESH CLAMNET LAP SIDING
  - 14 FRESH CLAMNET LAP SIDING
  - 15 FRESH CLAMNET LAP SIDING
  - 16 FRESH CLAMNET LAP SIDING
  - 17 STAINLESS STEEL ROOFING
  - 18 STAINLESS STEEL ROOFING
  - 19 STAINLESS STEEL ROOFING
  - 20 STAINLESS STEEL ROOFING
  - 21 STAINLESS STEEL ROOFING

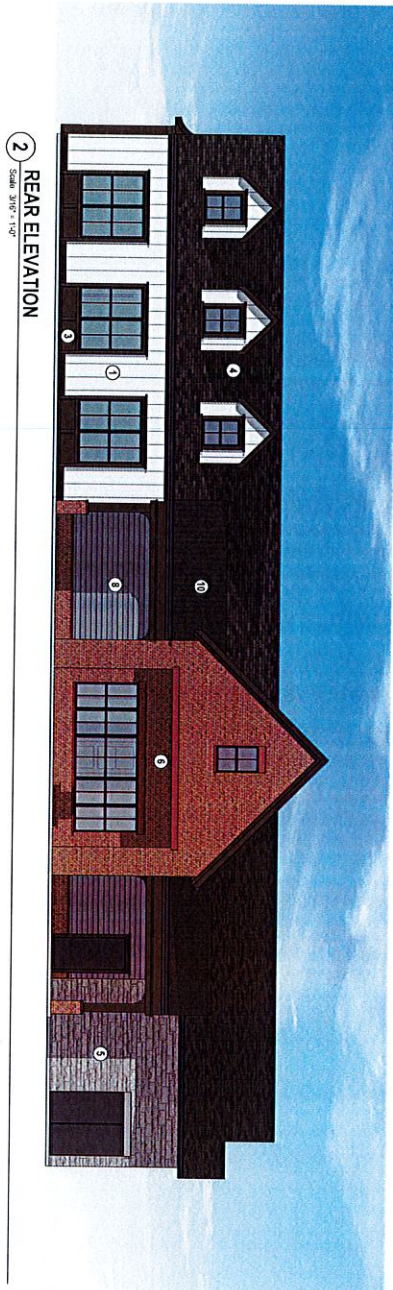
- COLOR SCHEME B (WARM TONES)**
- 1 FRESH CLAMNET DORMER MATERIAL
  - 2 FRESH CLAMNET LAP SIDING
  - 3 FRESH CLAMNET LAP SIDING
  - 4 FRESH CLAMNET LAP SIDING
  - 5 FRESH CLAMNET LAP SIDING
  - 6 FRESH CLAMNET LAP SIDING
  - 7 FRESH CLAMNET LAP SIDING
  - 8 FRESH CLAMNET LAP SIDING
  - 9 FRESH CLAMNET LAP SIDING
  - 10 FRESH CLAMNET LAP SIDING
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  - 20 FRESH CLAMNET LAP SIDING
  - 21 FRESH CLAMNET LAP SIDING



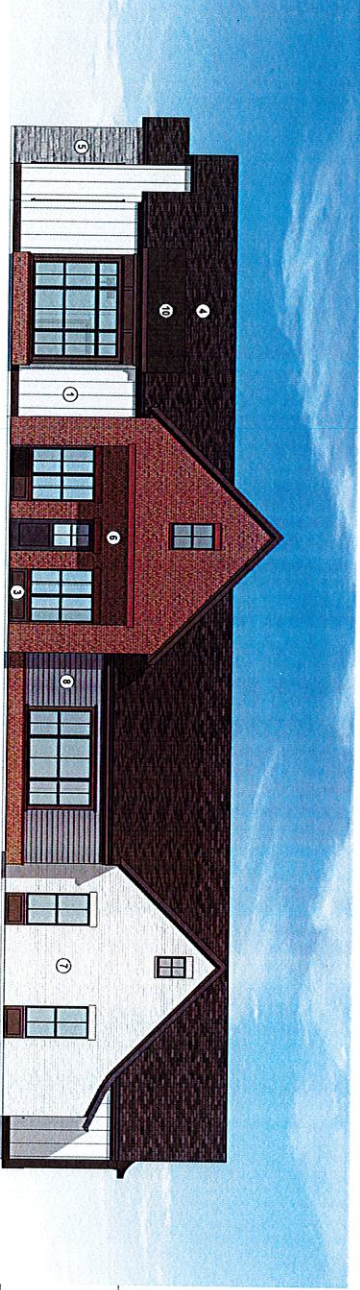
**MATERIAL LEGEND**

- COLOR SCHEME A**
- 1 DARK CHARCOAL BRICK & BROWN SPLIT-TILE
  - 2 DARK CHARCOAL BRICK
  - 3 DARK CHARCOAL BRICK
  - 4 DARK CHARCOAL BRICK
  - 5 DARK CHARCOAL BRICK
  - 6 DARK CHARCOAL BRICK
  - 7 DARK CHARCOAL BRICK
  - 8 DARK CHARCOAL BRICK
  - 9 DARK CHARCOAL BRICK
  - 10 DARK CHARCOAL BRICK
  - 11 DARK CHARCOAL BRICK

- COLOR SCHEME B**
- 1 DARK CHARCOAL BRICK & BROWN SPLIT-TILE
  - 2 DARK CHARCOAL BRICK
  - 3 DARK CHARCOAL BRICK
  - 4 DARK CHARCOAL BRICK
  - 5 DARK CHARCOAL BRICK
  - 6 DARK CHARCOAL BRICK
  - 7 DARK CHARCOAL BRICK
  - 8 DARK CHARCOAL BRICK
  - 9 DARK CHARCOAL BRICK
  - 10 DARK CHARCOAL BRICK
  - 11 DARK CHARCOAL BRICK



**2 REAR ELEVATION**  
Scale: 3/16" = 1'-0"



**1 FRONT ELEVATION**  
Scale: 3/16" = 1'-0"



**MATERIAL LEGEND**

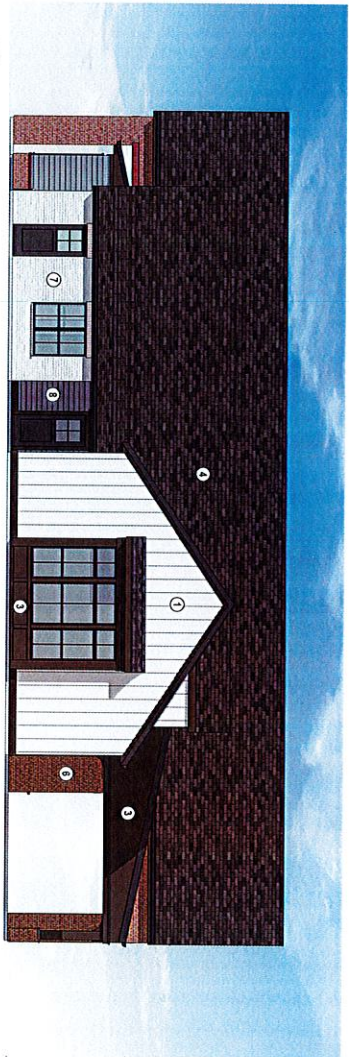
**COLOR SCHEME A**

- 1 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 2 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 3 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 4 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 5 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 6 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 7 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 8 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 9 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 10 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 11 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)

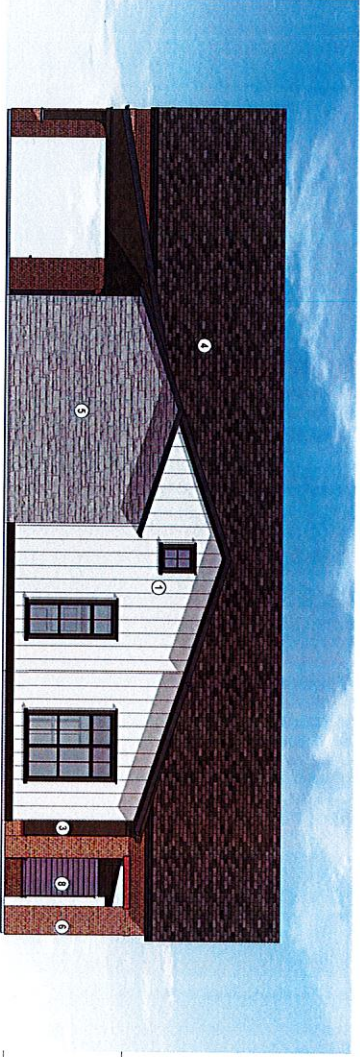
**COLOR SCHEME B**

- 1 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 2 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 3 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 4 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 5 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 6 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 7 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 8 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 9 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 10 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)
- 11 HERRINGBONE BRICK (1:1) (SEE PLAN FOR LOCATION)

**2 RIGHT ELEVATION**  
Scale: 3/16" = 1'-0"



**1 LEFT ELEVATION**  
Scale: 3/16" = 1'-0"





**LEE'S SUMMIT**  
MISSOURI

**DEVELOPMENT REVIEW FORM**  
**TRANSPORTATION IMPACT**

<b>DATE:</b>	October 30, 2025	<b>CONDUCTED BY:</b>	Erin Ralovo, PE, PTOE
<b>SUBMITTAL DATE:</b>	July 14, 2023	<b>PHONE:</b>	816.969.1800
<b>APPLICATION #:</b>	PL2025207	<b>EMAIL:</b>	Erin.Ralovo@cityofls.net
<b>PROJECT NAME:</b>	EAST VILLAGE	<b>PROJECT TYPE:</b>	Prel Dev Plan (PDP)

**SURROUNDING ENVIRONMENT** (*Streets, Developments*)

The proposed development is located on the southeast corner of the US 50 and M291 interchange. It is bordered by M291 on the west, US 50 Highway on the north and 16<sup>th</sup> Street on the south. The area to the west is a large planned commercial development, Oldham Village, with a mix of uses. East of the development is a largely commercial/industrial area and to the south of 16<sup>th</sup> Street is mixed residential and commercial area. Phase 1 is north of the existing intersection of M291 and Oldham Parkway and will be built north of a newly constructed extension of Oldham Parkway. Phase 2 is south of Oldham Parkway and north of Bailey Road. Phase 3 is south of Bailey Road and north of 16th Street.

**ALLOWABLE ACCESS**

Phase 1 will be accessed from M291 via Oldham Parkway. Oldham Parkway will be constructed from M291, east to the Railroad tracks.

- Access 1 will be a Right Out only, approximately 350 feet east of M291.
- Access 2 will be a full access signalized intersection, constructed at approximately 660 feet east of M291.
- Access 3 will be at a full access on the far east end of the newly constructed Oldham Parkway, approximately 1,000 feet from M291, leading to the fuel station.

Phase 2 will be accessed from M291 via Oldham Parkway on the north and Bailey Road on the South. Phase 3 may also be accessed from Hamblen Road via Bailey Road.

- Access 1 on Oldham Parkway, referred to as the north/south collector, will be at the full access signalized intersection across from the main entrance to Phase 1 (also identified as Access 2 in Phase 1).
- Access 2, the most eastern access on the north of Phase 2 along Oldham Parkway, at the end of the new extension of Oldham Parkway and will turn south into a parking lot for the Multi-family development.

To the south, Phase 2 will be accessed along Bailey Road.

- Access 1, the north/south collector is proposed to be a Right-In Right-Out Left-In access point approximately 640 feet from the centerline of M291. This access was specified by staff to be a Right In Right Out only by staff and the Left In is not supported based on Access Management Code, anticipated vehicle queues, and the interest of public safety along a minor arterial such as Bailey Road in close proximity to M291 Highway with merging lanes of traffic.
- Access 2, labeled Tract D, along Bailey Road is a full access signalized intersection, approximately 1,065 feet east of M291. The intersection cannot be located farther east for better signal separation due to the physical constraints of the Bailey Road bridge and necessity for minimum left turn lane capacity.



The Access Management Code includes requirements for minimum spacing between street connections depending on street classifications, turn lanes, volume/capacity and queuing, intersection function area, and the type of access. Along Commercial Collector streets, such as Oldham Parkway, the minimum spacing for full access intersections is 300 feet, measured between centerlines. Other minimum conditions may extend the 300 feet including for sight distance, vehicle queuing and turn lane design, intersection functional area, and traffic signal spacing. The access points in Phase 1 meet this basic commercial collector minimum spacing, but not the minimum spacing required for other criteria. The minimum spacing for signalized intersections along a city roadway is at least 1/4 of a mile (1,320 feet) but shall not be less than 1/8 of a mile where extraordinary circumstances exist. The signalized intersection at Oldham Parkway and the warehouse entrance (Access 2)/north-south collector (Access 1), Node 6 in the traffic study, is 660 feet, 1/8 of a mile, from the signal at M291. Due to the extreme volumes expected at this intersection, and the high number of expected left turns, staff would consider this an extraordinary circumstance in which the intersection meets traffic signal warrants. Given the extremely close proximity to adjacent traffic signal at M291, congestion should be expected due to challenging signal coordination and vehicle queues may be significant at peak hours. For these reasons, any other access between the intersection and M291 or near the signal shall not allow left turn movements and access shall be minimized. The impact of these signals in close proximity should not affect M291 and there are no other public streets in the vicinity besides those which service the development. That too was considered in the determination of an extraordinary circumstance. Alternatives were not viable given the surrounding development bound by M291 to the west, US50 to the north and the railroad to the east.

Bailey Road is classified as a Minor Arterial, requiring the spacing between intersections to be a minimum of 400 feet. However, the 400 foot minimum may require additional separation as previously noted and described in the Access Management Code. The access points along Bailey meet this minimum spacing for a Minor Arterial, but not the minimum spacing required for other criteria, such as intersection functional area, turn lane design, and traffic signal separation. The full access signalized intersection on Bailey Road, Node 20 in the traffic study, shows a maximum PM Peak hour left turn queue length of 331 feet. This exceeds the 250 foot left turn lane length. The Right- In-Right-Out-Left-In access at Bailey and the north/south collector, Node 7 in the traffic study, prohibits the lengthening of this left-turn lane at the traffic signal to meet the vehicle queue length. If the Left-In were eliminated at Node 20, the left turn lane at the proposed traffic signal could comply with code requirements. Additionally, the peak left turn volume at Node 7 is 202 vehicles and the opposing movements have a total of 993 vehicles. This leads staff to believe that there will be insufficient breaks in traffic to provide opportunity for left turns at Node 7 and reasonable cause for safety concerns. The proposed traffic signal along Bailey Road is 1065 feet from M291, less than the 1/2 Mile spacing criteria. However, the location is as planned by the City when Bailey Road was constructed and cannot be any farther east without impacting the Bailey Road bridge. Staff supports the proposed traffic signal location on Bailey Road.

There are several driveways shown along the proposed north-south collector that do not have minimum throat lengths to avoid access conflicts. These conflicts are typically limited to impacting property circulation and do not affect the public roadways. However, where those conflicts and minimum lengths could impact public roadways, there are opportunities to revise the design to cause additional throat length (such as reducing adjacent parking). Those driveway modifications and site plan modifications can be done in preparation of the Final Development Plan at the direction of Staff without substantially impacting the Preliminary Development Plan.

Other than the aforementioned driveway throat lengths, which can be revised to substantially comply with code, and Node 7, which could be code compliant if it were limited to Right-In Right-Out, and which would also address the eastbound left-turn lane code issue (inadequate storage capacity) associated with the proposed traffic signal along Bailey Road, all other intersections and turn lanes comply with the Access Management Code as shown on the Preliminary Development Plan (subject to PDP conditions of approval, final development plan revisions and engineering design per Access Management Code and Design and Construction Manual).

**TRIP GENERATION**

<b>Time Period</b>	<b>Total</b>	<b>In</b>	<b>Out</b>
Weekday	32,669	N/A	N/A
A.M. Peak Hour	1,070	466	604
P.M. Peak Hour	1,781	963	819
Saturday Peak Hour	3,664	1,892	1,772

The values shown above are for the overall development, Phases 1 through 3. A breakdown of trips by phase is shown below.

**Phase 1 Trip Generation:**

Weekday Total: 11,704 vehicles per day (vpd)  
 AM Peak Hour: 322, 179 In, 143 Out  
 PM Peak Hour: 776, 373 In, 404 Out  
 SATURDAY Peak Hour: 1,207, 599 In, 608 Out

Phase 1 trips were generated using historical data provided by Costco from a Trip Generation Technical Memorandum prepared by Kittleson Associates dated July 9, 2025. Kittleson maintains a continually updated database for the warehouse retailer based on 40 similar sites. The ITE code of 857 - Discount Club provides a much lower trip generation with a weekday total of only 6,836 vehicles per day. Because the Kittleson numbers are significantly more conservative, staff feels comfortable with this generation. The trip generation data prepared by Kittleson Associations adhered to the ITE Trip Generation Study requirements for establishing trip rates.

**Phase 2 Trip Generation:**

Weekday Total: 11,147 vpd  
 AM Peak Hour: 448, 175 In, 273 Out  
 PM Peak Hour: 614, 365 In, 249 Out  
 SATURDAY: 1,272, 681 In, 591 Out

Phase 2 trips were generated using ITE Codes in the ITE Trip Generation Manual 11<sup>th</sup> Edition using ITE codes 220 - Multifamily Housing (Low-Rise), 931 - Fine Dining Restaurant, 932 - High-Turnover (Sit-Down) Restaurant, 930 - Fast Casual Restaurant, 934 - Fast-Food with Drive Thru Window, 822 - Strip Retail Plaza (<40K), 936 - Coffee/Donut Shot w/o Drive Thru Window, and 411 - Public Park. The total trips were determined based on the anticipated use for each pad site. The total daily trips were not separated into Trips In and Trips Out. Staff does not agree with the high number of Fine Dining Restaurants included in the study as shown in this phase of the project and feel that a more conservative look at the trip generation is needed. This would mean that it is very likely that some of the studied land uses, specifically Fine Dining, could

change to High-Turnover Sit-Down Restaurant or Fast Casual. Fine Dining has a significantly lower trip generation than High-Turnover Sit-Down Restaurant or Fast Casual. Staff is requesting an updated Traffic Impact Study after Phase 1 is completed and as Phase 2 sites are more determined so that conditions of approval that require additional turn lanes, turn lane capacity or limited access can be identified and imposed as part of the development project rather than future mitigating taxpayer capital improvements. As restaurants come forward for Final Development Plan and permitting, staff will determine the most appropriate category for the proposed site and a revised Traffic Impact Study may be required if the number of trips increased substantially due to the change in category.

Phase 3 Trip Generation:

Weekday Total: 9,818 vpd

AM Peak Hour: 300, 112 In, 188 Out

PM Peak Hour: 391, 225 In, 166 Out

SATURDAY Peak Hour: 1,185, 612 In, 573 Out

Phase 3 trips were generated using ITE Codes in the ITE Trip Generation Manual 11th Edition using ITE codes 220 - Multifamily Housing (Low-Rise), 215 - Single-Family Attached Housing, 930 - Fast Casual Restaurant, and 934 - Fast-Food with Drive Thru Window. The total trips were determined based on the anticipated use for each pad site. The total daily trips were not separated into Trips In and Trips Out.

**TRANSPORTATION IMPACT STUDY REQUIRED?**      **Yes**       **No**

The proposed development will likely generate more than 100 vehicle trips to the surrounding street system during a peak hour based on industry standard methods for trip generation estimates, a minimum requirement for a traffic impact study in the Access Management Code. A traffic impact study was prepared by BHC, dated October 24, 2025. The traffic study was prepared to assess traffic impacts associated with the development and to provide public improvement recommendations or waivers requested by the development that mitigate delay and/or meet minimum standards defined by City and/or MoDOT policies.

The traffic study evaluated existing conditions and proposed development conditions of the subject development. The existing conditions included forecasted traffic volumes from the approved Oldham Village Development (previous traffic impact study). The analysis included morning and evening commuter peak hours at the intersections of M291 and Oldham Parkway, M291 and Persels Road/Bailey Road, M291 and SE 16<sup>th</sup> Street, M291 and Scherer Road, and at Bailey Road and Hamblen Road. The proposed intersections associated with the development along Oldham Parkway, Bailey Road, and internal to the site were also studied. The study considered several scenarios: Existing Conditions, Existing plus Phase 1 (Costco), Existing plus Phase 1-2 (Development between Oldham Parkway and Bailey Road), Existing plus full development (Development between Bailey Road and SE 16<sup>th</sup> Street), and Future Conditions for year 2045.

The traffic study looked at each intersection for turn lane requirements based on roadway classification, operational capacity/queues, projected turning movements, and minimum standards in the Access Management Code. The available space and storage lengths were reviewed in coordination with the required turn lane lengths to provide recommendations for the subject development. The study found that the following the following improvements would be required:

## PHASE 1

1. Oldham Parkway and M291
  - Extend the existing southbound dual left-turn lanes to provide a minimum of 430 FT of storage, or as much as feasible within existing geometrics.
2. Oldham Parkway and North/South Collector
  - Construct a full intersection with traffic signal installation, including the future south leg and pedestrian facilities.
3. Lot 1 Access Drive
  - Retail tenant could post signage at the western Right-Out restricting use to northbound M291 patrons to discourage Oldham Parkway weaving movements.

## PHASE 2

1. M291 corridor Improvements
  - Construct a third southbound through-lane from south of Oldham Parkway through Persels and continue to (or through) Scherer Road. This should include removal of existing shoulder, new paved lane and new paved shoulder.
  - Include associated shoulder, lane, and signal modifications primarily along the western half of M291.
2. Traffic Impact Study Update
  - Upon completion of Phase 1, update the TIA to confirm southbound widening limits and validate prior recommendations.
3. Bailey Road at North/South collector
  - Implement center median and lane improvements to accommodate future Phase 3 traffic and establish this intersection as a westbound right-in/right-out with eastbound left-turn in capability.
4. Bailey Road at Drive 8
  - Implement lane improvements to accommodate future Phase 3 traffic and the signalization of this intersection.

## PHASE 3 (Full Development)

1. M291 and Persels/Bailey
  - Add a second southbound left-turn lane (380 FT storage) and improve the westbound right-turn configuration. Westbound left-turn storage should strive for 320 FT of storage pending an updated TIS.
  - Include shoulder, lane, and signal improvements along the eastern half of M291 at Bailey Road.
2. Bailey Road and Drive 8
  - Install a traffic signal to provide protected northbound left-turn and eastbound U-turn movements for Phase 3 traffic to provide full access to M291.
3. 16<sup>th</sup> Street
  - In accordance with the Unimproved Road Policy (URP), SW 16<sup>th</sup> Street shall be updated to full urban standards in include, but not limited to, widening to a minimum of 12 foot lanes in each direction, stormwater and gutter improvements, and sidewalk. SW 16<sup>th</sup> Street improvements are associated with Phase 3 of the TIS, but will be provided in the future Phase 2 PDP for East Village.

### Other Intersection-Specific Recommendations

-M291 and Scherer - Continue monitoring corridor traffic growth; an additional southbound through-lane may be necessary in Phase 2 or Phase 3 or as traffic grows from other developments in the region.

-North /South collector Intersections - Provide southbound right-turn lanes (minimum 100 FT + taper) and revise lot access throat lengths to accommodate projected Saturday peak-hour queues.

-Private Street Option: The City and the Developer may consider classifying the North/South collector from Bailey Road to 16<sup>th</sup> Street as a private street, which could alleviate spacing and access control issues under current AMC requirements.

The traffic study looked at each noted intersection to be analyzed for traffic operations and assigned a Level of Service (LOS) associated with their delay. Level of Service (LOS) is an industry accepted performance measure for traffic operations based on delay represented by the A to F lettered scale, with A the best and F the worst. City policy has established a LOS goal C for traffic signal operations and LOS D (where LOS E and F may be acceptable) for stop controlled movements. MoDOT has a similar performance standard, but LOS D is acceptable for signal operations. These LOS targets indicate acceptable operational performance or adequate operational conditions for the transportation network.

In addition to measured vehicle delay, vehicle queues were analyzed. With exception of a couple movements, all of the overall LOS meet the City's goals and queues were maintained in the planned turn lane capacities. This will be re-evaluated by staff when a new TIS is received after Phase 1 is open to traffic.

### LIVABLE STREETS (*Resolution 10-17*)

COMPLIANT

EXCEPTIONS

The proposed development plan will provide required sidewalks, shared use path/trail, on-road bicycle accommodations and all elements otherwise required by ordinances and standards and comprehensive plan (i.e. Bicycle Transportation Plan and Greenway Master Plan) including but not limited to property landscaping, lighting, parking, and ADA accessibility. No exceptions to the Livable Streets Policy adopted by Resolution 10-17 are requested. The proposed development as presented intends to incorporate "place making" elements with multi-modal complete streets providing access to/from and within the mixed land use project, including open spaces, recreational areas, and other non-motorized accommodations and an active environment with purposeful connections to surrounding developments, networks and downtown Lee's Summit less than 1 mile to the north. Furthermore, the Oldham Village project will be interconnected with the Fieldhouse less than 1/2 mile to the west.

### RECOMMENDATION:

APPROVAL

DENIAL

N/A

STIPULATIONS

*Recommendations for Approval refer only to the transportation impact and do not constitute an endorsement from City Staff.*

Staff recommends approval of the proposed preliminary development plan subject to the conditions noted below and any such public improvements shall be substantially completed prior to any issued occupancy permit associated with the phase of development described therein:

1. Construct all public improvements recommended in the Traffic Impact Study and other public improvements as stated above and shown on the plans, including but not limited to those restated below (in case of differing conditions the condition whichever has the most capacity shall apply):

## PHASE 1 (North of Oldham Pkwy)

### A. Oldham Parkway and M291

- Extend the existing southbound dual left-turn lanes to provide a minimum of 430 FT of storage, or as much as feasible within existing geometrics.

### B. Oldham Parkway and North/South Collector

- Construct a full intersection with traffic signal installation, including the future south leg and pedestrian facilities. The traffic signal shall include fiber optic network interconnect to the adjacent traffic signals owned by the City of Lee's Summit and MoDOT.

### C. Lot 1 Access Drive

- Access shall be Right-Out only. There is a discrepancy between the PDP and the TIS; the PDP shows RIRO and TIS shows RIRO-Left-In eastbound.
- Retail tenant could post signage at the western Right-Out restricting use to northbound M291 patrons to discourage Oldham Parkway weaving movements.

## PHASE 2 (Area Between Oldham Parkway and Bailey Road)

### D. M291 corridor Improvements

- Construct a third southbound through-lane from south of Oldham Parkway through Persels and continue to (or through) Scherer Road. This should include removal of existing shoulder, new paved lane, and new paved shoulder.
- Include associated shoulder, lane, and signal modifications primarily along the western half of M291.

### E. Traffic Impact Study Update

- Upon completion of Phase 1, update the TIS (including land uses in Phase 2) to confirm southbound widening limits and validate prior recommendations. Any additional public improvements or modifications described in the updated TIS or at the direction of the City Traffic Engineer shall become conditions of approval the same as any other listed condition of approval herein applicable to Phase 2.

### F. Bailey Road at North/South collector

- Implement center median and lane improvements to accommodate future Phase 3 traffic and establish this intersection as a westbound right-in/right-out with eastbound left-turn-in capability (refer to Condition #4 regarding the left-in). The eastbound left-turn may be omitted from initial construction as it is not required or it may be removed in the future.

### G. Bailey Road at Drive 8

- Implement lane improvements to accommodate future Phase 3 traffic and install a traffic signal with fiber optic network interconnect to adjacent traffic signals owned by the City of Lee's Summit and MoDOT.

## PHASE 3 (South of Bailey Rd)

### H. M291 and Persels/Bailey

- Add a second southbound left-turn lane with 380 feet of storage and improve the westbound right-turn configuration. Westbound left-turn storage shall have 320 feet minimum of storage plus taper pending an updated TIS.
- Shoulder, lane, and signal improvements along the eastern half of M291 at Bailey Road.

### I. Bailey Road and Drive 8

- Install a traffic signal with modifications to provide protected northbound left-turn and eastbound U-turn movements.

2. The public improvements shall include preservation or construction of new shared use paths and on-road bicycle facilities consistent with the Greenway Master Plan and Bicycle Transportation Plan, respectively, including but not limited to Bailey Road, wherever such roadways are directly impacted by or adjacent to the proposed development or related public improvements.

3. At the time this memo was written MoDOT had not completed their review of the traffic study and the improvements recommended within. The developer will be required to construct any improvements required by MoDOT. MoDOT may add or waive any public improvements tied to the MoDOT facilities or MoDOT right-of-way as owner of the state transportation facilities and state (MHTC) right-of-way.

4. The Developer shall construct the roadway as required, full pavement and median doweled on top of the pavement so as to minimize any structural disruption work, public impact, etc. This would allow the City, within its inherent and absolute right, to manage City right-of-way, to easily remove the island and close the left lane if it is determined that this turn lane is a safety or operational concern or it is determined that there is a need for safety or operational improvement at adjacent intersections (e.g. a longer left turn lane is needed at nearby full access traffic signal).

a. If the developer moves forward with constructing the left-turn lane along Bailey Road at the unsignalized North/South Collector (Right-In-Right-Out-Left-In), the Developer shall escrow \$100,000 for the future reconstruction of the median in the subject area to limit the intersection to Right-In-Right-Out only. The decision to limit access and cause expenditure of said escrow for reconstruction shall exclusively reside with the City Traffic Engineer. Escrow shall be required prior to any occupancy permit within Phase 2 (the area between Oldham Pkwy and Bailey Road). Escrow payment may be released when the development reaches 75% of occupancy of total square footage of non-residential area of Phases 2 and 3 (the area between Oldham Pkwy and 16<sup>th</sup> St.) but not less than 10 years.

b. If the Developer moves forward as recommended, without a left-turn lane, updated plans shall be submitted at the time of permit review.

5. The Developer is required to improve 16<sup>th</sup> Street from M291 Highway to the easternmost connection with the proposed development to meet full urban standards in accordance with the Unimproved Road Policy.

6. The Developer shall re-evaluate minimum throat lengths ahead of Final Development Plan submittals. Staff will review and determine the impact of a substandard throat length at that time and require changes as needed.