

LEGEND:

- A/E

-

ACCESS EASEMENT
- BC

-

BACK OF CURB
- B/B

-

BACK TO BACK
- BM

-

BENCHMARK
- BL or B.L.

-

BUILDING LINE
- CO

-

CLEANOUT
- TJB

-

TELEPHONE JUNCTION BOX
- C&G

-

CURB AND GUTTER
- D/E

-

DRAINAGE EASEMENT
- E/E

-

ELECTRICAL EASEMENT
- EL

-

ELEVATION
- FL

-

FLOW LINE
- G/E

-

GAS LINE EASEMENT
- HDPE

-

HIGH-DENSITY POLYETHYLENE
- L/E

-

LANDSCAPE EASEMENT
- MSFE

-

MINIMUM SERVICEABLE FLOOR ELEVATION
- PVC

-

POLYVINYL CHLORIDE
- P/L

-

PROPERTY LINE
- PUB/E

-

PUBLIC EASEMENT
- RCP

-

REINFORCED CONCRETE PIPE
- ROW or R/W

-

RIGHT-OF-WAY
- S/E

-

SANITARY SEWER EASEMENT
- SL

-

SERVICE LINE
- SW

-

SIDEWALK
- TE

-

TOP ELEVATION
- U/E

-

UTILITY EASEMENT
- WSE

-

WATER SURFACE ELEVATION
- W/E

-

WATERLINE EASEMENT

- ASPHALT PAVEMENT - EXISTING
- ASPHALT PAVEMENT - PROPOSED
- CONCRETE PAVEMENT - EXISTING
- CONCRETE SIDEWALK - EXISTING
- CONCRETE SIDEWALK - PROPOSED
- CURB & GUTTER
- CURB & GUTTER - EXISTING
- TREELINE
- EXISTING LOT AND R/W LINES
- EXISTING PLAT LINES
- P/L

PROPERTY LINES
- ROW

RIGHT-OF-WAY
- STORM SEWER
- STORM SEWER - EXISTING
- CATV_x

FIBER OPTIC CABLE - EXISTING
- T_x

TELEPHONE LINE - EXIST.
- E_x

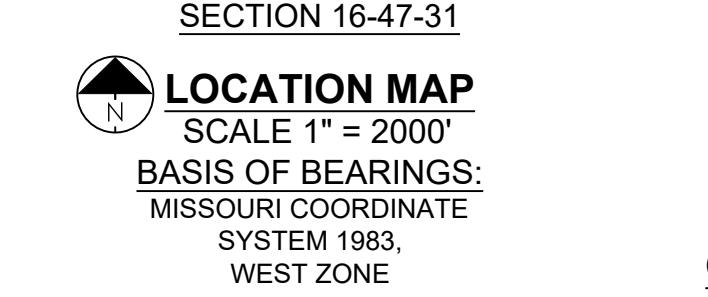
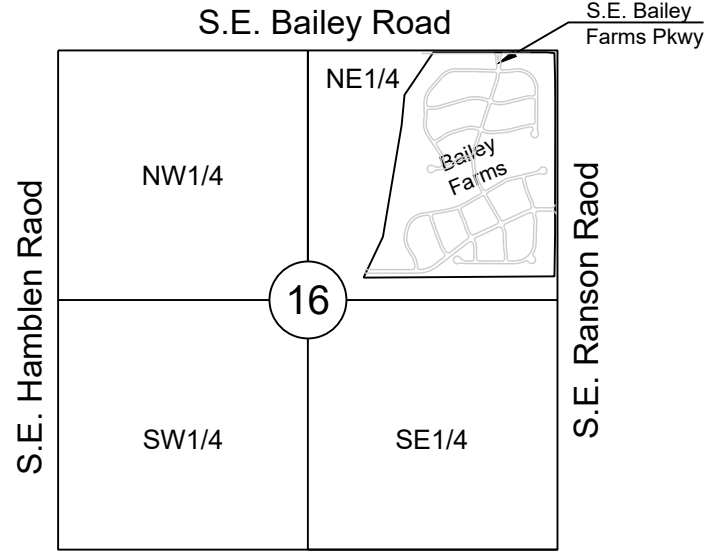
ELECTRIC LINE - EXISTING
- OH_P_x

OVERHEAD POWER LINE - EXIST.
- UG_E_x

UNDERGROUND ELECTRIC - EX.
- G_x

GAS LINE - EXISTING
- W_x

WATERLINE - EXISTING
- LIGHT - EXISTING
- EXISTING MANHOLE
- CLEANOUT
- EXISTING SANITARY MANHOLE
- PROPOSED SANITARY MANHOLE
- EXISTING AREA INLET
- EXISTING CURB INLET
- EXISTING GRATE INLET
- EXISTING JUNCTION BOX
- EXISTING STORM MANHOLE



UTILITY CONTACTS:

MISSOURI DEPARTMENT OF TRANSPORTATION (MODOT)
Steve Holloway
600 NE Colbern Road
Lee's Summit, MO 64086
(816) 399-9633

MISSOURI GAS ENERGY (MGE)
Brent Jones
3025 SE Clover Drive
Lee's Summit, MO 64082
(816) 399-9633
brent.jones@spireenergy.com

KANSAS CITY POWER & LIGHT COMPANY (KCP&L)
Ron Dejamette
1300 SE Hamblin Road
Lee's Summit, MO 64081
Office: (816) 347-4316
Cell: (816) 810-5234
ron.dejamette@kcpl.com

CITY OF LEES SUMMIT PUBLIC WORKS
Dena Mezger
220 SE Green Street
Lee's Summit, MO 64063
(816) 969-1800

AT&T
Mark Manion or Marty Loper
500 E. 8th Street, Room 370
Kansas City, MO 64106
(816) 275-2341 or (816) 275-1550

COMCAST CABLE
John Meadows
4700 Little Blue Parkway
Independence, MO 64057
(816) 795-2257

CITY OF LEE'S SUMMIT WATER UTILITIES
Mark Schaulter
1200 SE Hamblen Road
Lee's Summit, MO 64081
(816) 969-1900

GENERAL NOTES:

- ALL CONSTRUCTION TO FOLLOW THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL AS ADOPTED BY ORDINANCE 5813.
- ALL WORKMANSHIP AND MATERIALS SHALL BE SUBJECT TO THE INSPECTION AND APPROVAL OF THE ENGINEERING DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI.
- LINEAL FOOT MEASUREMENTS SHOWN ON THE PLANS ARE HORIZONTAL MEASUREMENTS, NOT SLOPE MEASUREMENTS. ALL PAYMENTS SHALL BE MADE ON HORIZONTAL MEASUREMENTS.
- NO GEOLOGICAL INVESTIGATION HAS BEEN PERFORMED ON THE SITE.
- THE UTILITY LOCATIONS SHOWN ON THESE PLANS ARE TAKEN FROM UTILITY COMPANY RECORDS AND APPARENT FIELD LOCATIONS. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF THE SENATE BILL NUMBER 583, 78TH GENERAL ASSEMBLY OF THE STATE OF MISSOURI. THE BILL REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT OF WAY DO SO ONLY AFTER GIVING NOTICE TO, AND OBTAINING INFORMATION FROM, UTILITY COMPANIES. STATE LAW REQUIRES 48 HOURS ADVANCE NOTICE. THE CONTRACTOR MAY ALSO UTILIZE THE FOLLOWING TOLL FREE PHONE NUMBER PROVIDED BY "MISSOURI ONE CALL SYSTEM, INC.": 1-800-DIG-RITE. THIS PHONE NUMBER IS APPLICABLE ANYWHERE WITHIN THE STATE OF MISSOURI. PRIOR TO COMMENCEMENT OF WORK, THE CONTRACTOR SHALL NOTIFY ALL THOSE COMPANIES WHICH HAVE FACILITIES IN THE NEAR VICINITY OF THE CONSTRUCTION TO BE PERFORMED.
- PRIOR TO ORDERING PRECAST STRUCTURES, SHOP DRAWING SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR APPROVAL. AFTER APPROVAL OF THE SHOP DRAWINGS, A COPY OF THE APPROVED AND SIGNED SHOP DRAWINGS SHALL BE PROVIDED TO THE CITY INSPECTOR UPON REQUEST.
- THE CONTRACTOR SHALL PROTECT ALL MAJOR TREES FROM DAMAGE. NO TREE SHALL BE REMOVED WITHOUT PERMISSION OF THE OWNER, UNLESS SHOWN OTHERWISE.
- CLEARING AND GRUBBING OPERATIONS AND DISPOSAL OF ALL DEBRIS THEREFROM SHALL BE PERFORMED BY THE CONTRACTOR IN STRICT ACCORDANCE WITH ALL LOCAL CODES AND ORDINANCES.
- ALL WASTE MATERIAL RESULTING FROM THE PROJECT SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR, OR AS DIRECTED BY THE OWNER.
- ALL EXCAVATIONS SHALL BE UNCLASSIFIED. NO SEPARATE PAYMENT WILL BE MADE FOR ROCK EXCAVATION.
- THE CONTRACTOR SHALL CONTROL THE EROSION AND SILTATION DURING ALL PHASED OF CONSTRUCTION, AND SHALL KEEP THE STREETS CLEAN OF MUD AND DEBRIS.
- ALL MANHOLES, CATCH BASINS, UTILITY VALVES AND METER PITS TO BE ADJUSTED OR REBUILT TO GRADE AS REQUIRED.
- THE CONTRACTOR SHALL CONTACT DEVELOPMENT SERVICES INSPECTIONS AT: 816-969-1200 TO OBTAIN A DEVELOPMENT SERVICES CONSTRUCTION PERMIT. A MINIMUM 48 HOUR NOTICE SHALL BE GIVEN PRIOR TO PERMIT ISSUANCE.
- THE CONTRACTOR SHALL CONTACT THE RIGHT OF WAY INSPECTOR AT 816-969-1800 PRIOR TO ANY LAND DISTURBANCE ACTIVITIES WITHIN THE RIGHT OF WAY. THESE ACTIVITIES MAY REQUIRE A PERMIT.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL TRAFFIC HANDLING MEASURES NECESSARY TO ENSURE THAT THE GENERAL PUBLIC IS PROTECTED AT ALL TIMES. TRAFFIC CONTROL SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD-LATEST EDITION).

STREET NOTES:

- ALL STREET CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL. ALL APPLICABLE AASHTO STANDARDS HAVE BEEN MET.
- ALL INSPECTION OF STREET CONSTRUCTION TO BE PERFORMED BY THE CITY OF LEE'S SUMMIT DEVELOPMENT ENGINEERING.
- CURB RETURN RADII SHALL BE 25' AT BACK OF CURB UNLESS OTHERWISE NOTED.
- SUBGRADE TO BE COMPACTED TO 95% STANDARD PROCTOR DENSITY.
- ASSUMED DESIGN SPEED = 25 MPH (COLLECTOR).
- MINIMUM STOPPING SIGHT DISTANCE = 155 FEET.
- MINIMUM K, SAG CURVE = 26 (14 WITH LIGHTING), CREST CURVE = 12.
- GRADE INTERSECTIONS TO DRAIN AS SHOWN.
- SSD = STOPPING SIGHT DISTANCE.
- ALL ADA SIDEWALK RAMPS SHALL BE CONSTRUCTED BY THE DEVELOPER WITH THE PUBLIC INFRASTRUCTURE.

EARTHWORK:

- It is recommended that a Geotechnical Engineer observe and document all earthwork activities.
- Contours have been shown at 1-foot or 2-foot intervals, as indicated. Grading shall consist of completing the earthwork required to bring the physical ground elevations of the existing site to the finished grade (or sub-grade) elevations provided on the plans as spot grades, contours or others means as indicated on the plans.
- The existing site topography depicted on the plans by contouring has been established by aerial photography and field verified by g.p.s. observation near 2-20-19. The contour elevations provided may not be exact ground elevations, but rather interpretations of such. Accuracy shall be considered to be such that not more than 10 percent of spot elevation checks shall be in error by more than one-half the contour interval provided, as defined by the National Map Accuracy Standards. Any quantities provided for earthwork volumes are established using this topography contour accuracy, and therefore the inherent accuracy of any earthwork quantity is assumed from the topography accuracy.
- Proposed contours are to approximate finished grade.
- Unless otherwise noted, payment for earthwork shall include backfilling of the curb and gutter, sidewalk and further manipulation of utility trench spoils. The site shall be left in a mowable condition and positive drainage maintained throughout.
- Unless otherwise noted, all earthwork is considered Unclassified. No additional compensation will be provided for rock or shale excavation, unless specifically stated otherwise.
- Prior to earthwork activities, pre-disturbance erosion and sediment control devices shall be in place per the Storm Water Pollution Prevention plan and/or the Erosion and Sediment Control Plan prepared for this site.
- All topsoil shall be stripped from all areas to be graded and stockpiled adjacent to the site at an area specified by the project owner or his appointed representative. Vegetation, trash, trees, brush, tree roots and limbs, rock fragments greater than 6-inches and other deleterious materials shall be removed and properly disposed of offsite or as directed by the owner or his appointed representative.
- Unless otherwise specified in the Geotechnical Report, all fills shall be placed in maximum 6-inch lifts and compacted to 95-percent of maximum density as defined using a standard proctor test (AASHTO T99/ASTM 698).
- Fill materials shall be per Geotechnical Report and shall not include organic matter, debris or topsoil. All fills placed on slopes greater than 6:1 shall be benched.
- The Contractor shall be responsible for redistributing the topsoil over proposed turf and landscaped areas to a minimum depth of 6-inches below final grade.
- All areas shall be graded for positive drainage. Unless noted otherwise the following grades shall apply:
 - Turf Areas - 2.5% Minimum, 4H:1V Maximum
 - Paved Areas - 1.2% Minimum, 5% Maximum
- All disturbed areas shall be fertilized, seeded and mulched immediately after earthwork activities have ceased. Seeding shall be per the Erosion and Sediment Control Plan and/or Landscape Plan. If not specified seeding shall be per APWA Section 2400, latest edition. Unless otherwise noted, seeding shall be subsidiary to the contract price for earthwork and grading activities.
- All disturbed areas in the right-of-way shall be sodded.
- Underdrains are recommended for all paved areas adjacent to irrigated turf and landscaped beds.
- Contractor shall adhere to the reporting requirements outlined in the Storm Water Pollution Prevention Plan (SWPPP) prepared for this project. Erosion and Sediment control devices shall be properly maintained and kept clean of silt and debris and in good working order. Additional erosion and sediment control measures shall be installed as required.

UTILITIES:

- Existing utilities have been shown to the greatest extent possible based upon information provided to the Engineer. The contractor is responsible for contacting the respective utility companies and field locating utilities prior to construction and identifying any potential conflicts. All conflicts shall immediately be brought to the attention of the Engineer.
- The contractor shall be responsible for coordinating any required utility relocations. Utilities damaged through the negligence of the contractor shall be repaired at the contractor's expense.
- Contractor shall verify flow-lines and structure tops prior to construction, and shall notify Engineer of any discrepancies. Provide shop drawings for all precast and manufactured utility structures for review by the Engineer prior to construction of the structures.
- Utility Separation: Watertines shall have a minimum of 10 feet horizontal and 2 feet vertical separation from all sanitary sewer lines, manholes, and sanitary sewer service laterals, as measured from edge to edge. If minimum separations can not be obtained, concrete encasement of the sanitary line shall be required 10 feet in each direction of the conflict.
- Payment for trenching, backfilling, pipe embedment, flowable fill, backfill materials, clean up, seeding, sodding and any other items necessary for the construction of the utility line shall be included in the contract price for the utility installation.
- The Contractor shall be responsible for contacting respective utility companies 48-hours in advance for the inspection of any proposed utility main extension or service line or service connection to any existing main.
- Trench spoils shall be neatly placed onsite adjacent to the trench, and compacted to prevent saturation and excess sediment runoff. Unsuitable materials, excess rock and shale, asphalt, concrete, trees, brush etc. shall be properly disposed of offsite. Materials may be wasted onsite at the direction of the Owner or his appointed representative.
- All excavation is considered unclassified, unless noted otherwise. Unclassified excavation for utility trenching is subsidiary to the unit price provided for the pipe. Any quantity provided for rock excavation is estimated based on the best information provided to the Project Engineer. The Engineer has the authority to identify and define the physical characteristics to determine the classification. Unit price quantities for rock excavation will be paid at a trench width of the nominal pipe diameter of the installed main plus 18 inches. Contractor is required to dispose of excess rock from their trenches by disposing it in areas as specified by the Project Engineer.

RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlagel and Associates.

"100-00 100.10", "1-00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlagel and Associates, P.A.

Sheet List Table	
Sheet Number	Sheet Title
1	COVER SHEET
2	DEMO PLAN
3	GENERAL LAYOUT
4	GRADING PLAN
5	EROSION CONTROL PLAN
6	TURN LANE PLAN (STA. 95+70 TO 101+50)
7	TURN LANE PLAN (STA. 101+50 TO END)
8	INTERSECTION DETAIL
9	PAVEMENT MARKING PLAN
10	STORM PLAN
11	CROSS SECTION STA. 96+00 - 100+75
12	CROSS SECTIONS STA. 101+00 - 106+75
13	CROSS SECTIONS STA. 107+00 - 107+25

APPROVED BY:

CITY ENGINEER
APPROVED FOR ONE YEAR FROM THIS DATE

DATE

OWNER/DEVELOPER:

CLAYTON PROPERTIES GROUP INC., DBA SUMMIT HOMES
BRADLEY KEMPF
120 SE 30TH STREET
LEE'S SUMMIT, MO 64082
p (816) 246-6700
BRADLEY@SUMMITHOMESKC.COM



MISSOURI GEOGRAPHIC REFERENCE SYSTEM BENCHMARK:

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25



PREPARED BY:



SCHLAGEL & ASSOCIATES, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS
SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

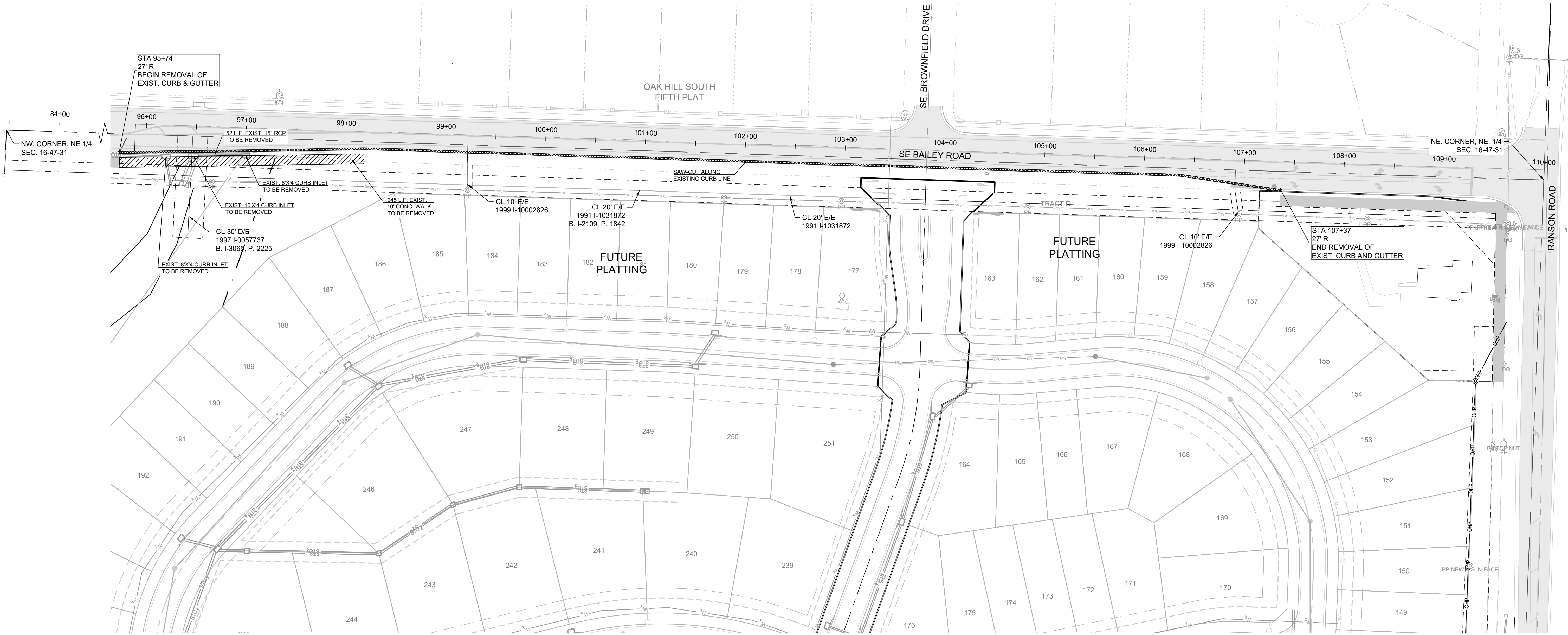
REVISION	DATE	DESCRIPTION
1	6/7/2024	per City Comments
2	10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
3	10/24/2024	Reviewed detail for thickened curb.
4	10/24/2024	
5	10/24/2024	
6	10/24/2024	
7	10/24/2024	
8	10/24/2024	

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/2/2024
PROJ. NUMBER:	21-136

COVER SHEET

SHEET

1



**MISSOURI GEOGRAPHIC REFERENCE SYSTEM
BENCHMARK:**

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25

PREPARED BY:



SCHLAGEL & ASSOCIATES, P.A.

**BAILEY ROAD AT BAYLEY FARMS
TURN LANE IMPROVEMENT PLANS**
SE BAYLEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
1 6/7/2024	per City Comments
2 10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
3 10/24/2024	Revised detail for thickened curb.
4	
5	
6	
7	
8	

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/22/2024
PROJ. NUMBER:	21-136

DEMO PLAN

SHEET

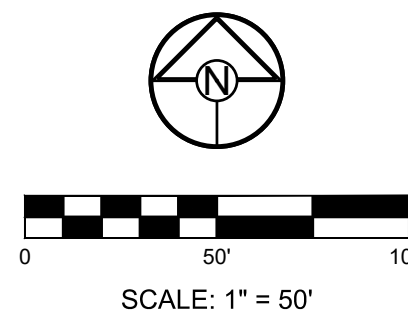
2

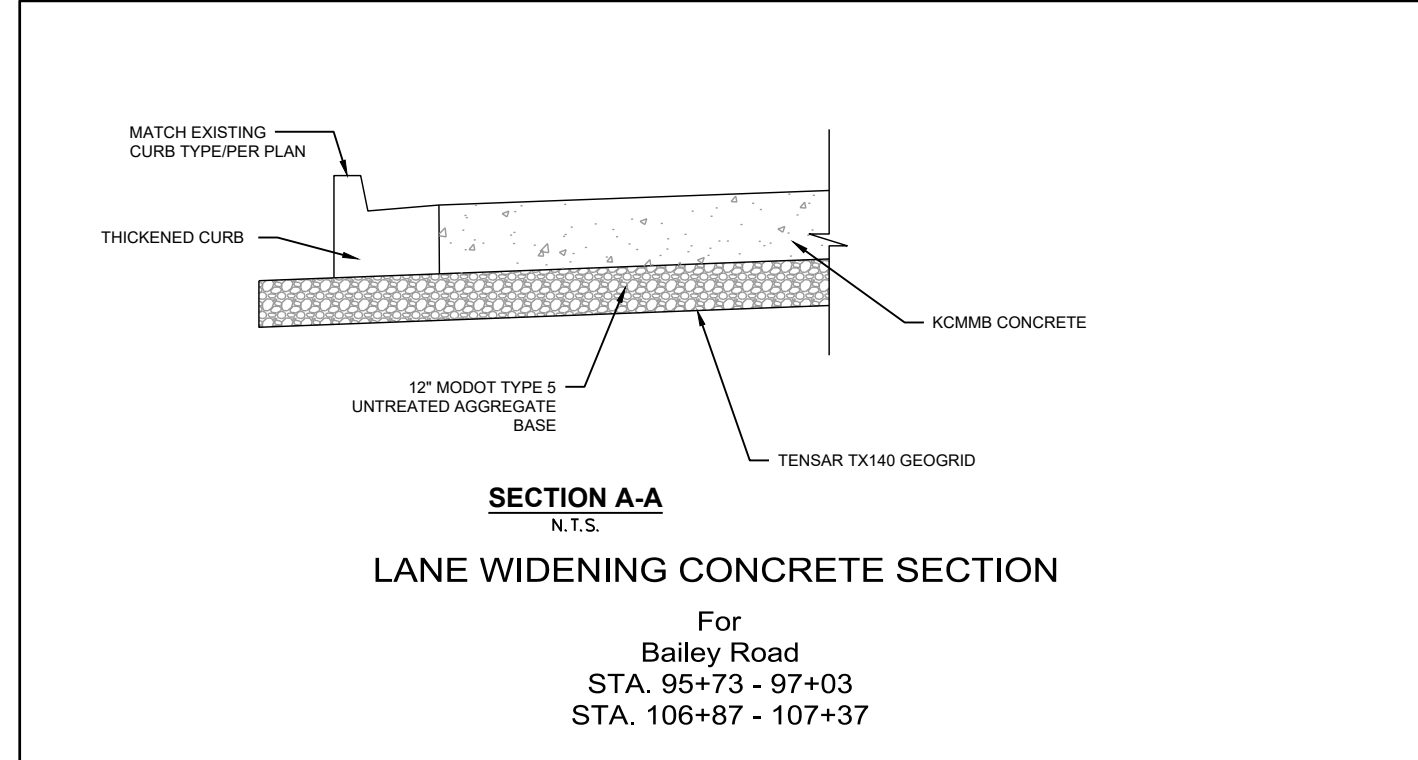
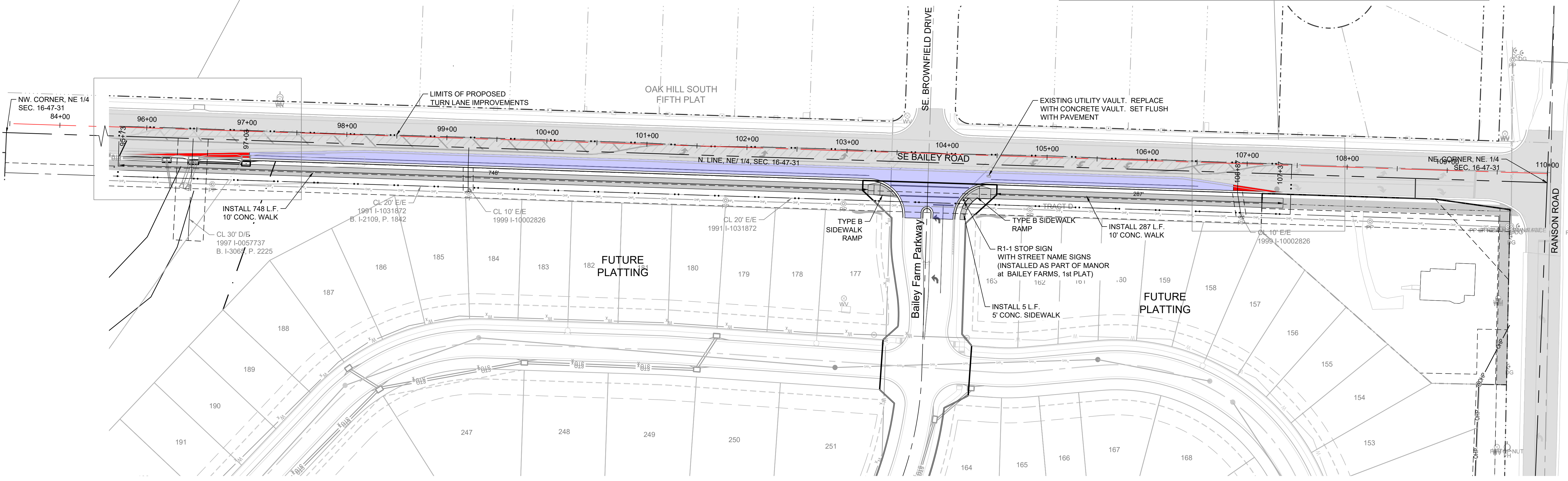
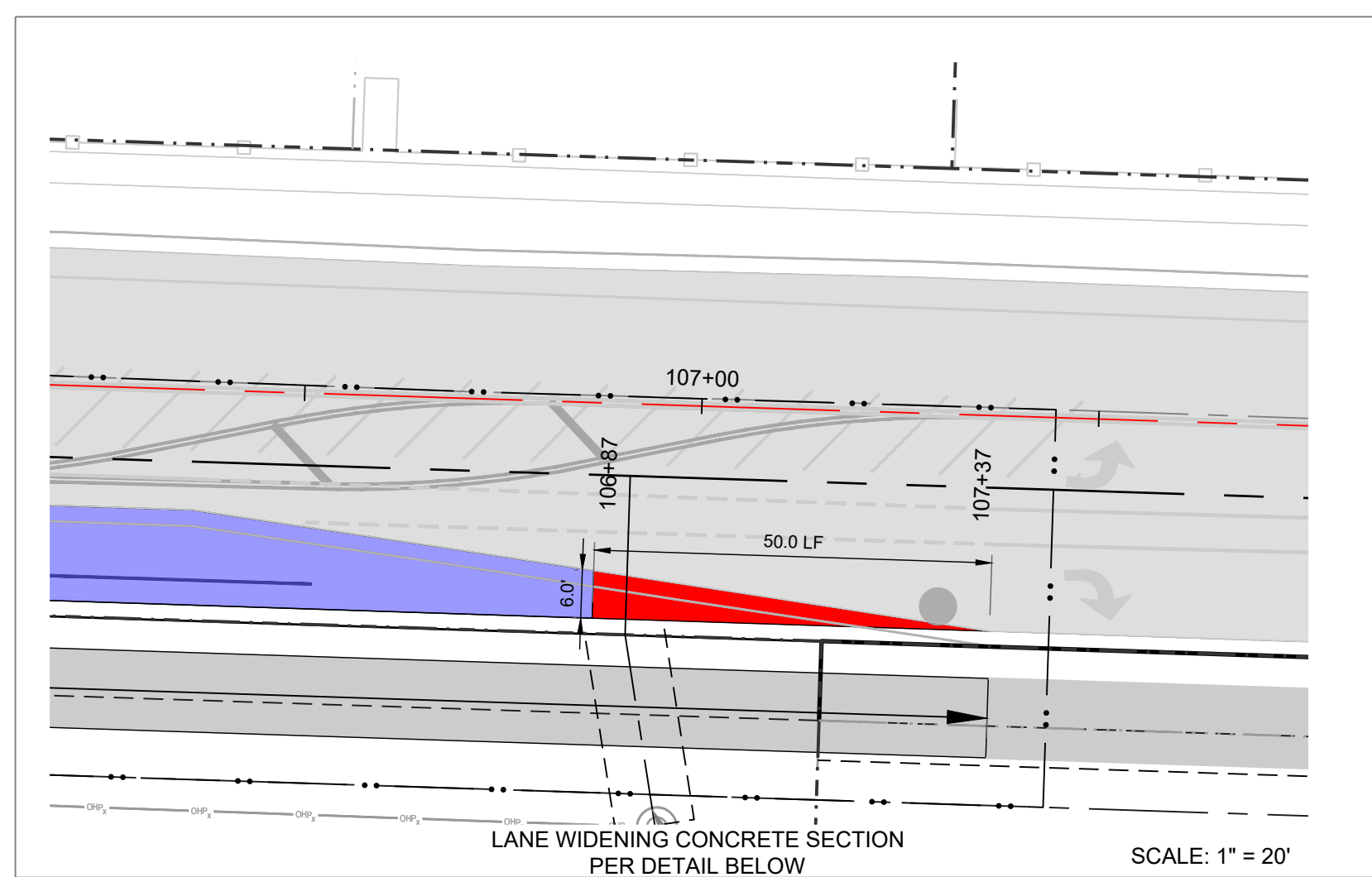
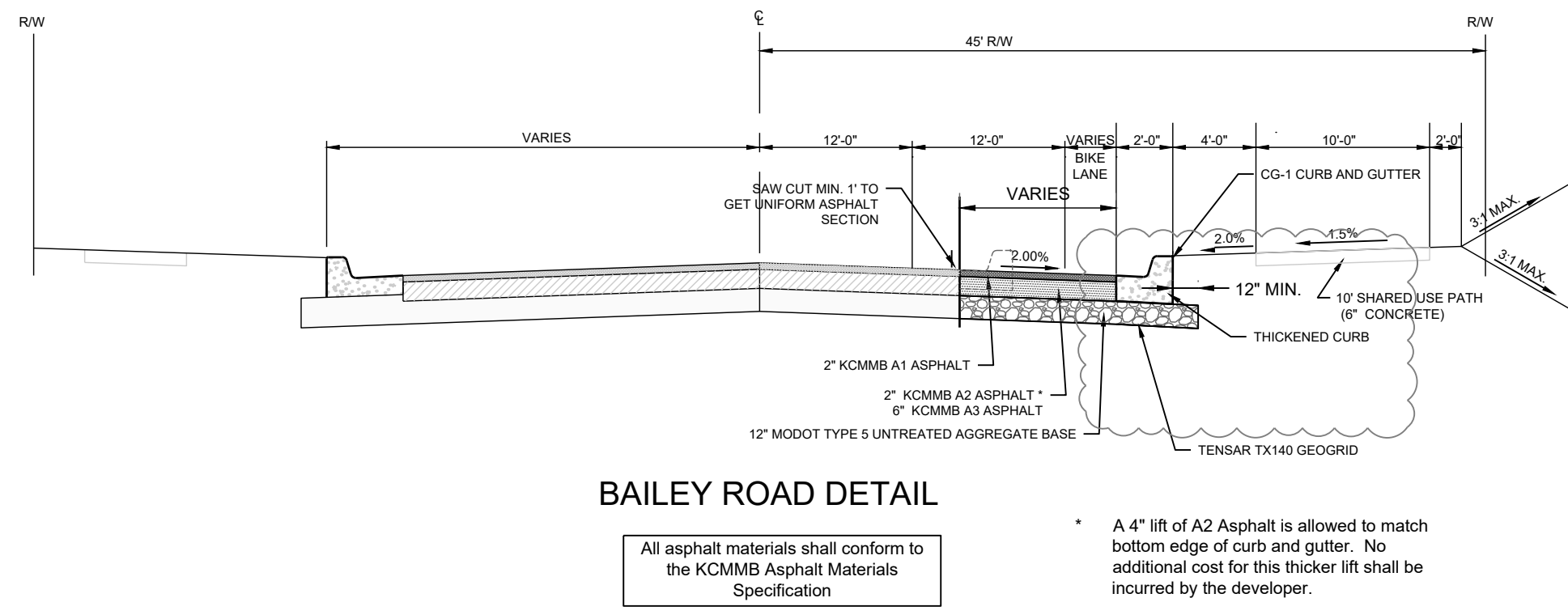
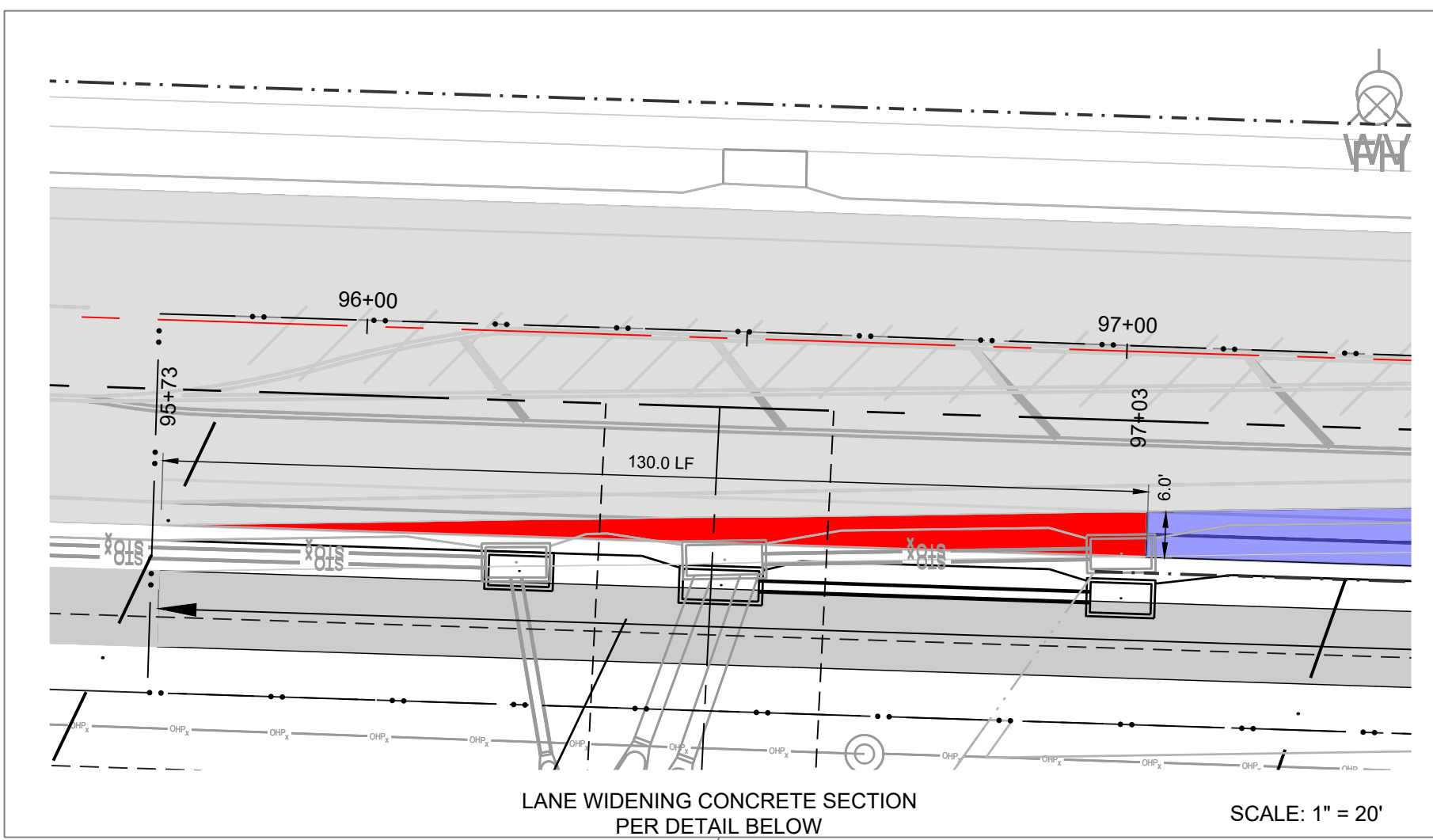
RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"400-00 100.10", "4-00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.





RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

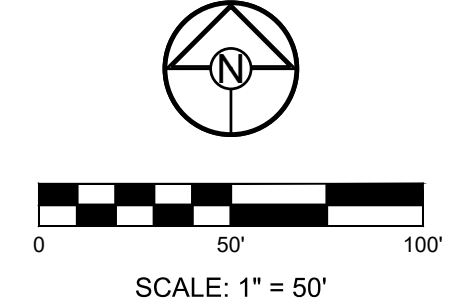
"100+00 100.10", "1.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.

MISSOURI GEOGRAPHIC REFERENCE SYSTEM BENCHMARK:

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25



BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
1 6/7/2024	per City Comments
2 10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
3 10/24/2024	Reviewed detail for thickened curb.
4 10/24/2024	Reviewed detail for thickened curb.
5 10/24/2024	Reviewed detail for thickened curb.
6 10/24/2024	Reviewed detail for thickened curb.
7 10/24/2024	Reviewed detail for thickened curb.
8 10/24/2024	Reviewed detail for thickened curb.
9 10/24/2024	Reviewed detail for thickened curb.
10 10/24/2024	Reviewed detail for thickened curb.

ELEV. = 1046.25

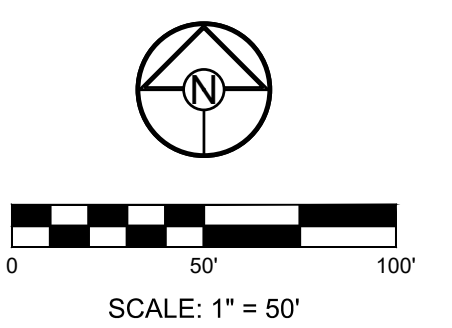
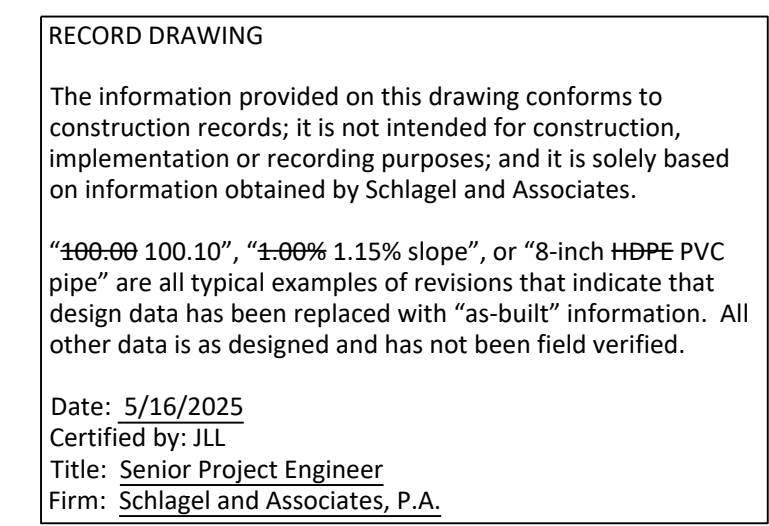
REPAIRED BY:



1	6/7/2024	per City Comments
2	10/8/2024	Added Lane Widening Detail
3	4	for Street Sections 6' or Less
4	10/24/2024	Revised detail for thickened curb
5		
6		
7		
8		
9		

SHEET

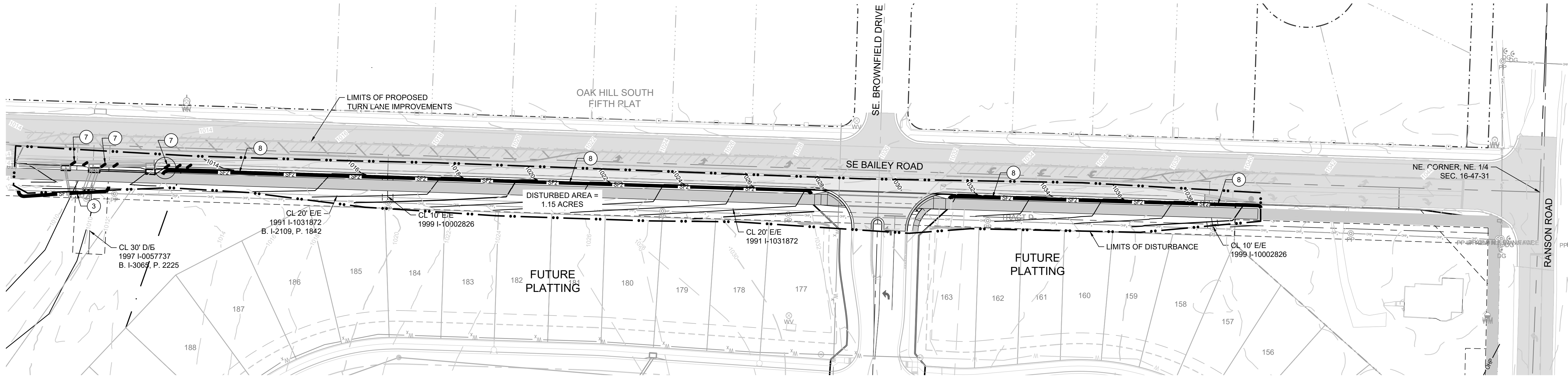
4



MISSOURI GEOGRAPHIC REFERENCE SYSTEM
BENCHMARK:

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25

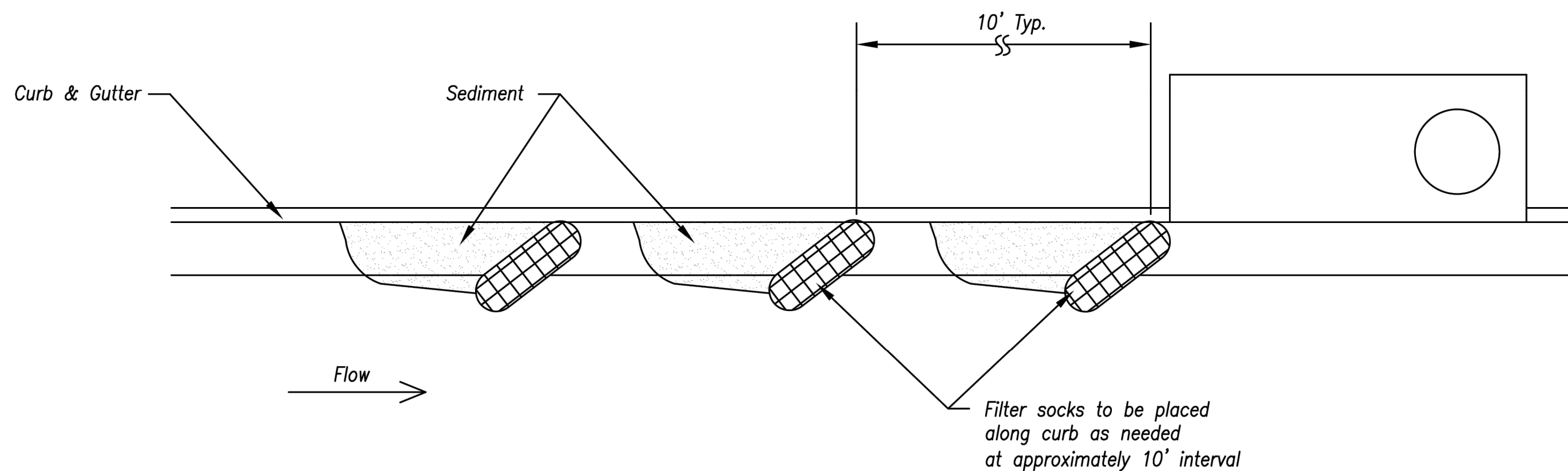


DISTURBED AREA = 1.15 A.C.

SITE SPECIFIC NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL EXISTING UTILITY LOCATIONS PRIOR TO EXCAVATION.
2. THERE ARE NO WETLANDS, NATURAL OR ARTIFICIAL WATER STORAGE DETENTION AREAS IN THE PROJECT AREA.
3. NO PART OF THE PROJECT LIES WITHIN THE 100 YEAR FLOOD PLAIN PER FEMA FLOOD INSURANCE RATE MAP NUMBER 29095C0438G & 29095C0439G DATED JANUARY 20, 2017.
4. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE IMPLEMENTED ACCORDING TO THE BMP STAGING CHART.
5. ADDITIONAL EROSION CONTROL MAY BE REQUIRED BY THE CITY ENGINEER AT ANY TIME EXISTING MEASURES ARE FOUND TO BE INEFFECTIVE OR PROBLEMATIC AREAS ARE

6. NOTED IN THE FIELD. STABILIZATION OF DISTURBED AREAS MUST, AT A MINIMUM, BE INITIATED IMMEDIATELY WHENEVER ANY CLEARING, GRADING, EXCAVATING, OR OTHER SOIL DISTURBING ACTIVITIES HAVE PERMANENTLY CEASED ON ANY PORTION OF THE SITE, OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. THE DISTURBED AREAS SHALL BE PROTECTED FROM EROSION BY STABILIZING THE AREA WITH MULCH OR OTHER SIMILARLY EFFECTIVE SOIL STABILIZING BMPs. INITIAL STABILIZATION ACTIVITIES MUST BE COMPLETED WITHIN 14 DAYS AFTER SOIL DISTURBING ACTIVITIES CEASE.
7. ALL PERIMETER SILT FENCE, EARTH DIKES, SEDIMENT BASINS, AND ROCK CONSTRUCTION ENTRANCES WILL BE INSTALLED BEFORE GRADING OPERATIONS BEGIN.
8. SILT FENCE AND EARTH DIKES THAT ARE PLACED BEFORE GRADING BEGINS WILL BE MAINTAINED BY THE GRADING CONTRACTOR.
9. AREAS WITHIN PUBLIC RIGHT-OF-WAY SHALL BE SODDED IMMEDIATELY AFTER CONSTRUCTION IS COMPLETE.



On Grade Curb Inlet Protection

		EROSION AND SEDIMENT CONTROL STAGING CHART				
		PROJECT STAGE	BMP PLAN REF. NO	BMP DESCRIPTION	REMOVE AFTER STAGE	NOTES:
PRE-CLEARING PHASE	A - PRIOR TO LAND DISTURBANCE	1		CONSTRUCTION ENTRANCE & STAGING AREA	D	MAINTAIN, REPAIR, OR REPLACE AS NECESSARY
		2		SUPER SEDIMENT FENCE AND ORANGE SAFETY FENCE	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
		3		SILT FENCE (PRIOR TO LAND DISTURBANCE)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
CONSTRUCTION PHASE	B - MASS GRADING	4		SILT FENCE (DURING CONSTRUCTION)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
	C - UTILITY CONSTRUCTION	5		CONCRETE WASHOUT AREA	E	MAINTAIN, REPAIR, OR REPLACE AS NECESSARY
		6		INLET PROTECTION (SILT FENCE)	D/E	PLACE SILT FENCE AROUND ALL STORM SEWER STRUCTURES / YARD AREA STORM STRUCTURES TO HAVE SILT FENCE REMOVED ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
FINAL STABILIZATION PHASE	D - AFTER PAVING OPERATIONS	7		INLET PROTECTION (GRAVEL FILTER BAGS)	E	INSTALL INLET PROTECTION PER APWA DETAIL SHEET ESC-06 FOR ON GRADE CURB INLET PROTECTION
		8		SILT FENCE (AFTER CURB CONSTRUCTION)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
		9		SEEDING AND MULCHING	E	ALL DISTURBED AREAS AFTER 14 DAYS OF CONSTRUCTION INACTIVITY
	E - UNTIL CLOSURE OF LAND DISTURBANCE PERMIT					ADDITIONAL SEDIMENT AND EROSION CONTROL MEASURES MAY BE REQUIRED ANY TIME CURRENT MEASURES ARE FOUND TO BE INEFFECTIVE.

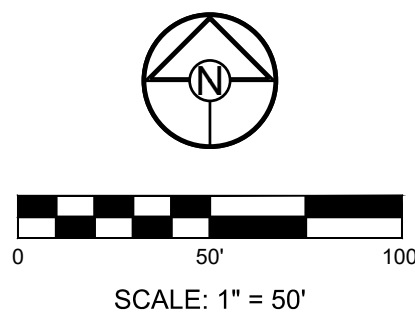
LEGEND	
	TEMPORARY STORAGE AREA FOR EXCESS MATERIAL
	TEMP. CONSTRUCTION ENTRANCE AND STAGING AREA
	CONCRETE WASHOUT AREA
	SILT FOAM DIKE - STAKED & INSTALL PER MFR'S RECOMMENDATIONS
	ROCK DITCH CHECK
	GRAVEL CURB INLET SEDIMENT TRAP
	SILT SOCK / ROCK SOCK / SOCK WATTLE
	BMP PLAN REF. NO.
	SILT FENCE (PRIOR TO LAND DISTURBANCE)
	SILT FENCE (DURING CONSTRUCTION)
	CONSTRUCTION FENCE
	LIMITS OF DISTURBANCE
	EXISTING CONTOURS
	PROPOSED CONTOURS
	STRAW BALE DITCH CHECK
	GRAVEL FILTER FOR STORM SEWER STRUCTURES ONLY

RECORD DRAWING

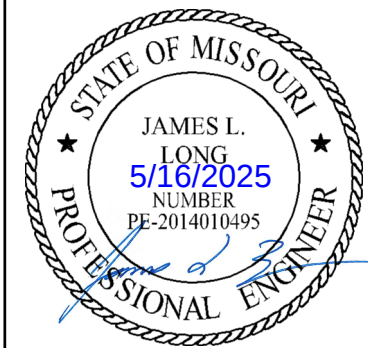
The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"100-00 100.10", "1-00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.



PREPARED BY:

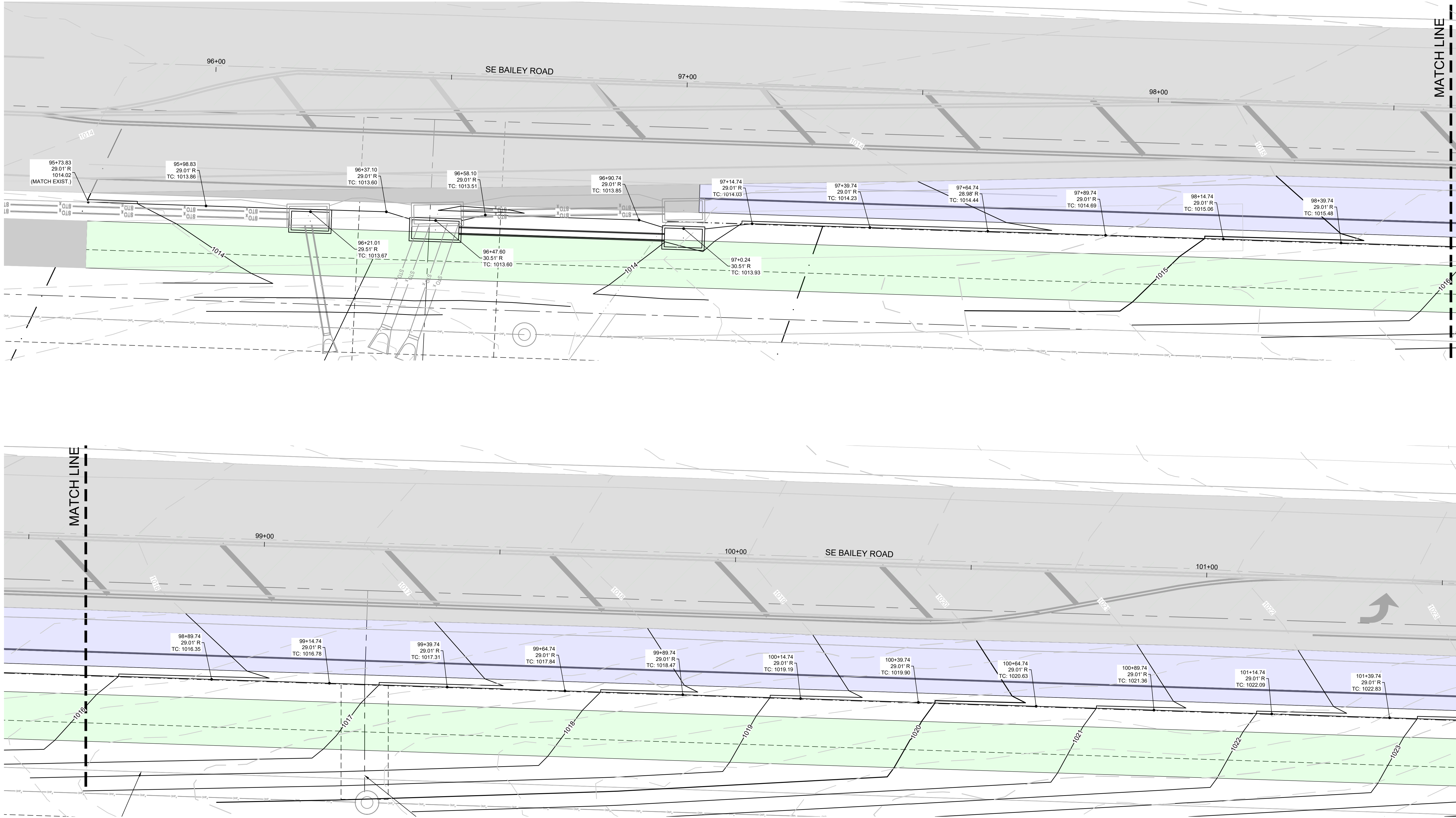


SCHLAGEL & ASSOCIATES, P.A.

BAILLEY ROAD AT BAILLEY FARMS
TURN LANE IMPROVEMENT PLANS
SE BAILLEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
6/7/2024	per City Comments
10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
10/24/2024	Reviewed detail for thickened curb.
2/2/2024	
2/1-136	

DRAWN BY: NCA	CHECKED BY: JLL	DATE PREPARED: 2/2/2024	PROJ. NUMBER: 21-136
EROSION CONTROL PLAN			
SHEET 5			



**MISSOURI GEOGRAPHIC REFERENCE SYSTEM
BENCHMARK:**

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25

SCHLAGEL
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS
14920 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
Missouri State Certificates of Authority
#E2002003600-F #LAC2001005237 #LS200200865-F

PREPARED BY:

JAMES L. LONG
5/16/2025
NUMBER
DE-2014010495
PROFESSIONAL ENGINEER

SCHLAGEL & ASSOCIATES, P.A.

SE BAILEY ROAD LEE'S SUMMIT, MISSOURI
BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

REVISION	DATE	DESCRIPTION
1	6/7/2024	per City Comments
2	10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
3	10/24/2024	Reviewed detail for thickened curb.
4	2/2/2024	
5	2/1-136	

TURN LANE PLAN
(STA. 95+70 TO 101+50)

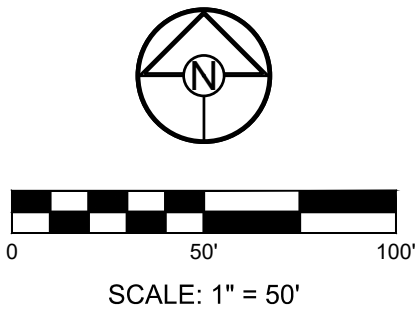
SHEET
6

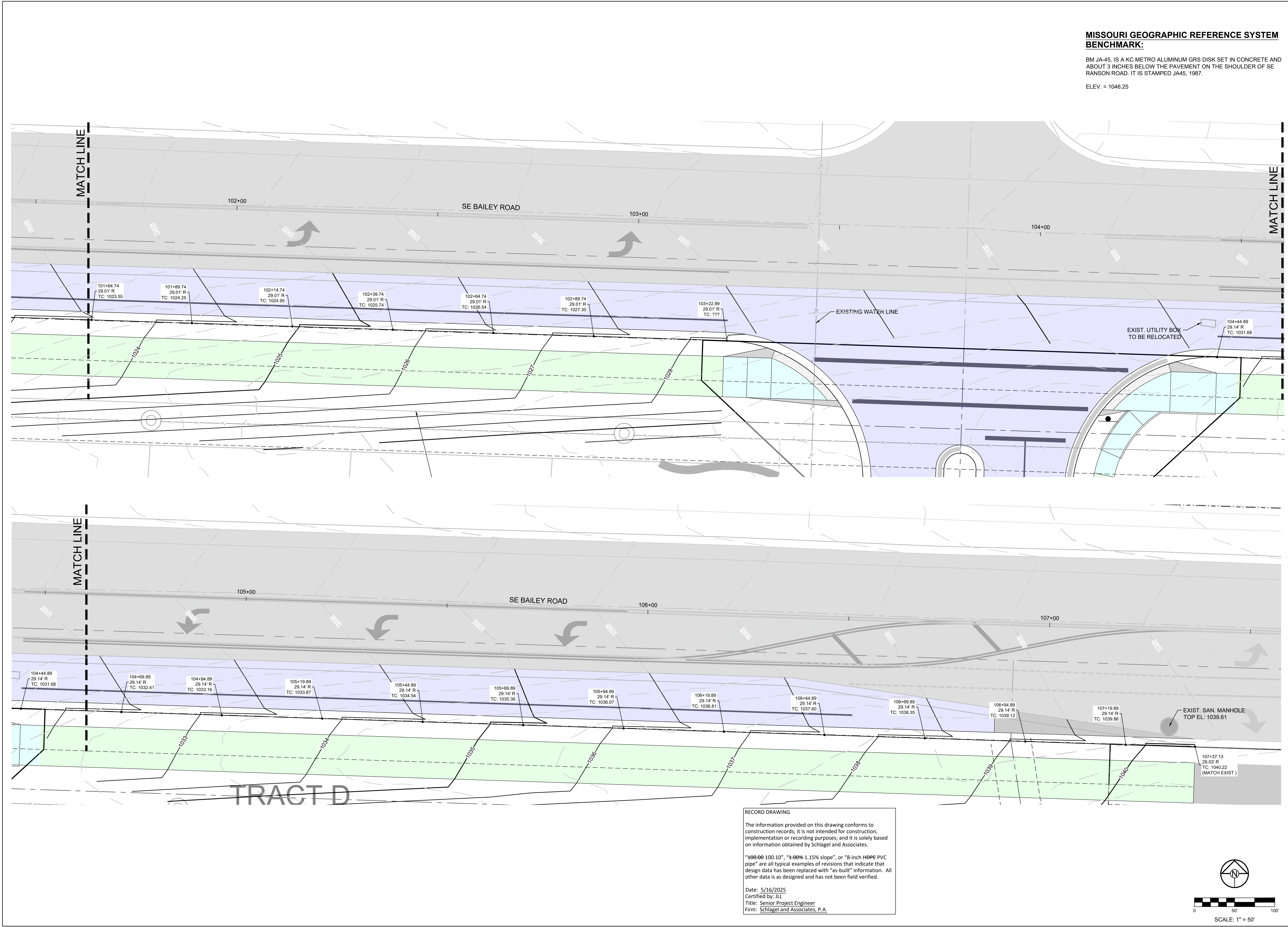
RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"100.00 100.10", "3.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.





MISSOURI GEOGRAPHIC REFERENCE SYSTEM BENCHMARK:

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25

SCHLAGEL

ENGINEERS

PLANNERS SURVEYORS LANDSCAPE ARCHITECTS

14920 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
Missouri State Certificates of Authority
#E2002003600-F #LAC2001005237 #LS2002008659-F

PREPARED BY:

STATE OF MISSOURI

JAMES L. LONG

5/16/2025

PROFESSIONAL ENGINEER

SCHLAGEL & ASSOCIATES, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
6/7/2024	per City Comments
10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
10/24/2024	Reviewed detail for thickened curb.
2/2/2024	
2/1/2024	

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/2/2024
PROJ. NUMBER:	21-136

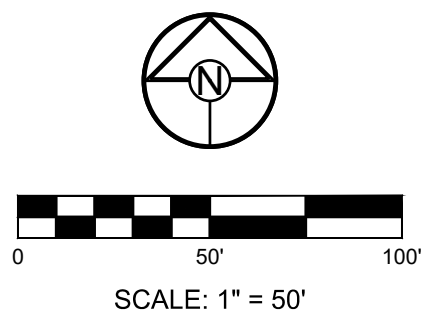
TURN LANE PLAN (STA. 101+50 TO END)
SHEET 7

RECORD DRAWING

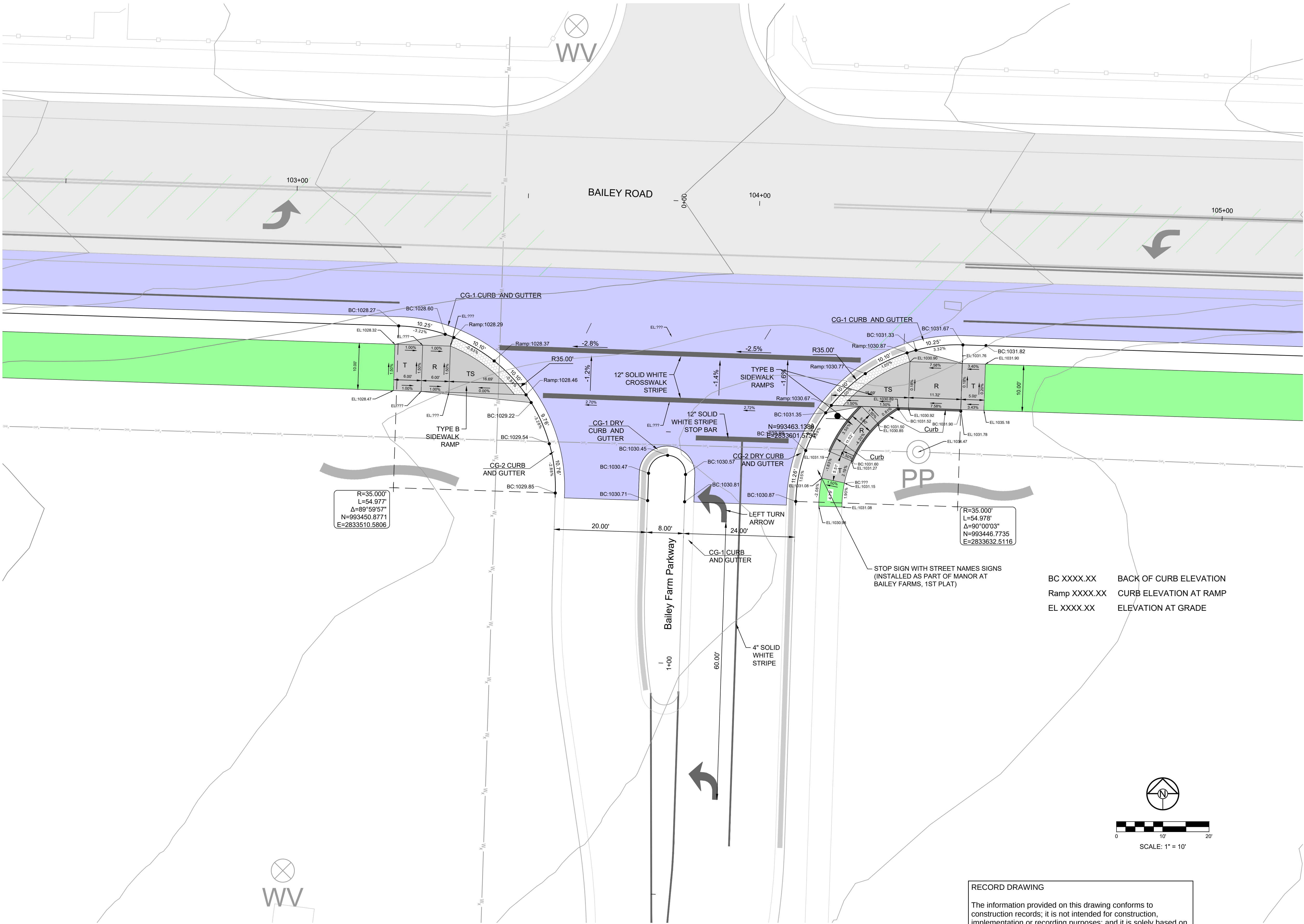
The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"~~100-00~~ 100.10", "±.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.



I:\PROJECTS\2021\21-133 0 Design\3 0 DWG Plans\6 0 SS\Bailey Road Widening\21-133-TL-Ramp Detail.dwg, Intersection Detail, 6/7/2024 11:13:38 AM, 1:1



RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlagel and Associates.

"100.00 100.10", "1.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlagel and Associates, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
1 6/7/2024	per City Comments
2	
3	
4	
5	
6	
7	
8	

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/2/2024
PROJ. NUMBER:	21-136

INTERSECTION
DETAIL

MISSOURI GEOGRAPHIC REFERENCE SYSTEM
BENCHMARK:

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

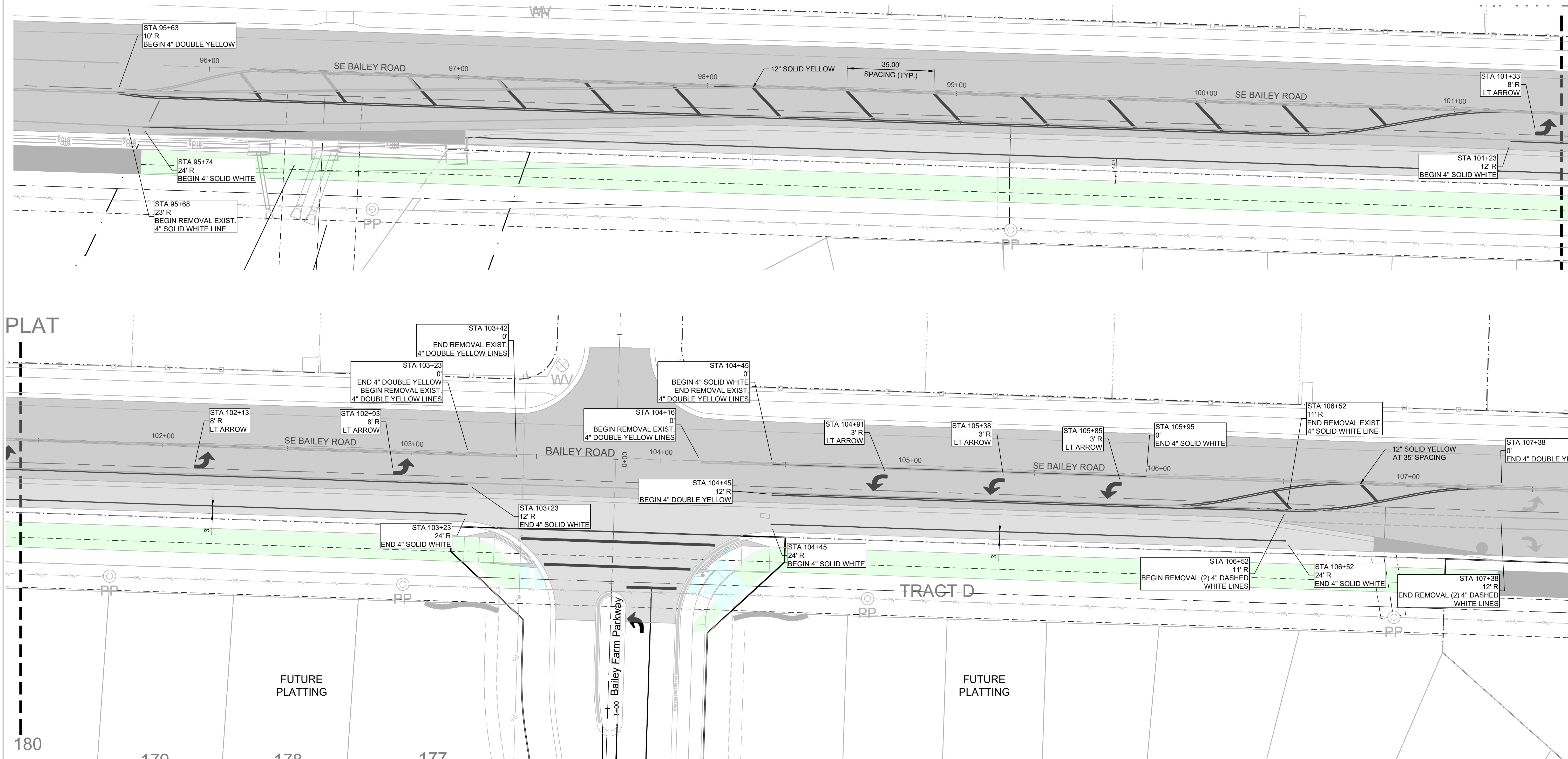
ELEV. = 1046.25

PREPARED BY:



SCHLAGEL & ASSOCIATES, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS
SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

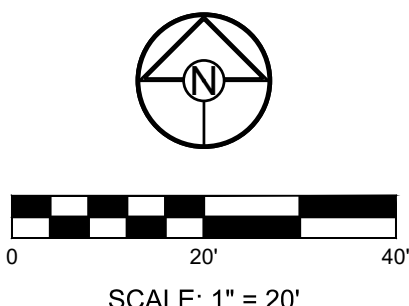


RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"100-00 100.10", "4-00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

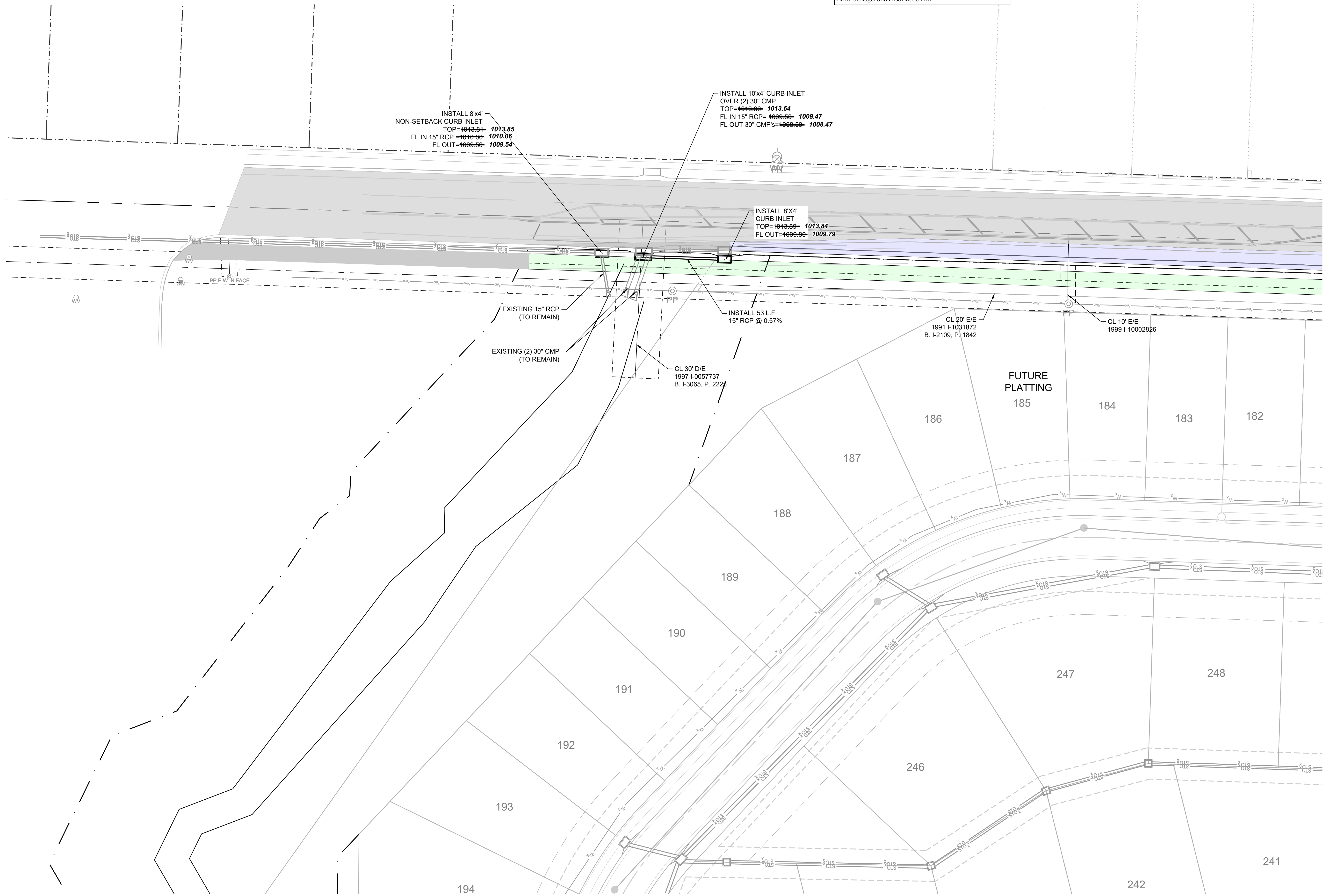
Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.



REVISION DATE	DESCRIPTION
6/7/2024	per City Comments
10/8/2024	Added Lane Widening Detail for Street Sections 8' or Less
10/24/2024	Reviewed detail for thickened curb.
2/2/2024	
2/1-136	

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/2/2024
PROJ. NUMBER:	21-136

PAVEMENT MARKING PLAN
SHEET
9



RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"300-09 100.10", "1-00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.

MISSOURI GEOGRAPHIC REFERENCE SYSTEM BENCHMARK:

BM JA-45, IS A KC METRO ALUMINUM GRS DISK SET IN CONCRETE AND ABOUT 3 INCHES BELOW THE PAVEMENT ON THE SHOULDER OF SE RANSON ROAD. IT IS STAMPED JA45, 1987.

ELEV. = 1046.25

SCHLAGEL

ENGINEERS

PLANNERS SURVEYORS LANDSCAPE ARCHITECTS

14920 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
Missouri State Certificates of Authority
#E2002003600-F #LAC2001005237 #LS2002008659-F

PREPARED BY:

STATE OF MISSOURI

JAMES L. LONG

5/16/2025

NUMBER

DE-20140101495

PROFESSIONAL ENGINEER

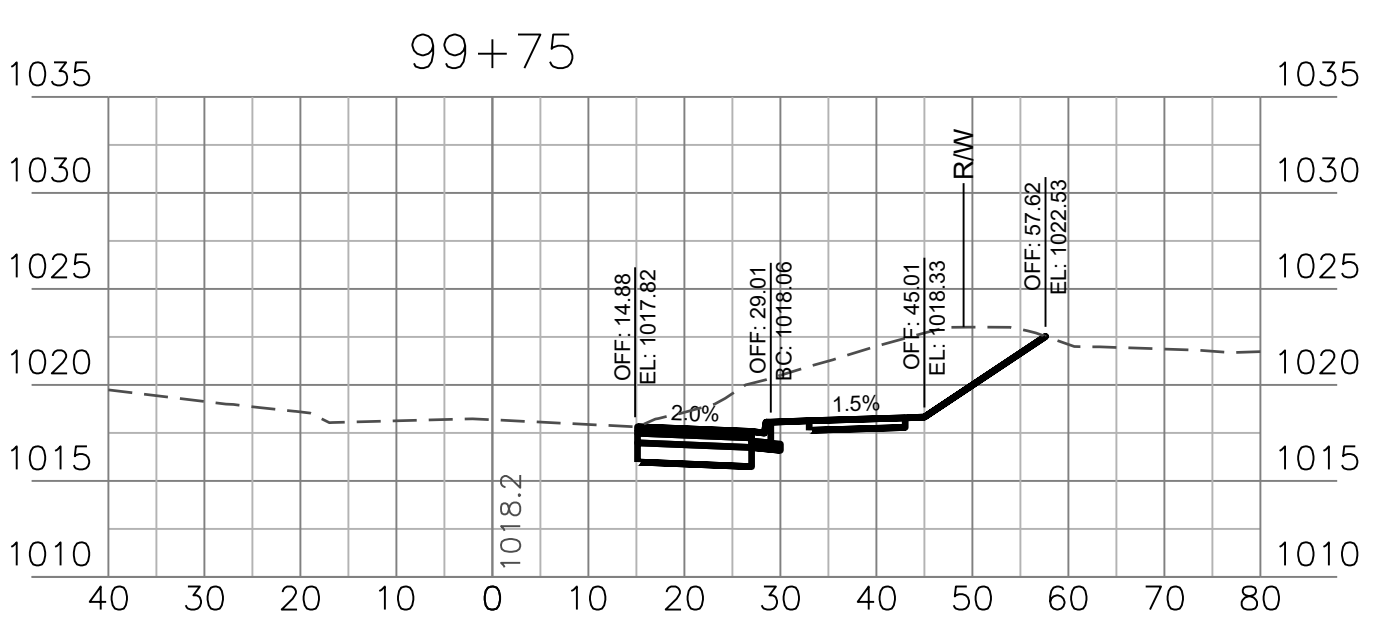
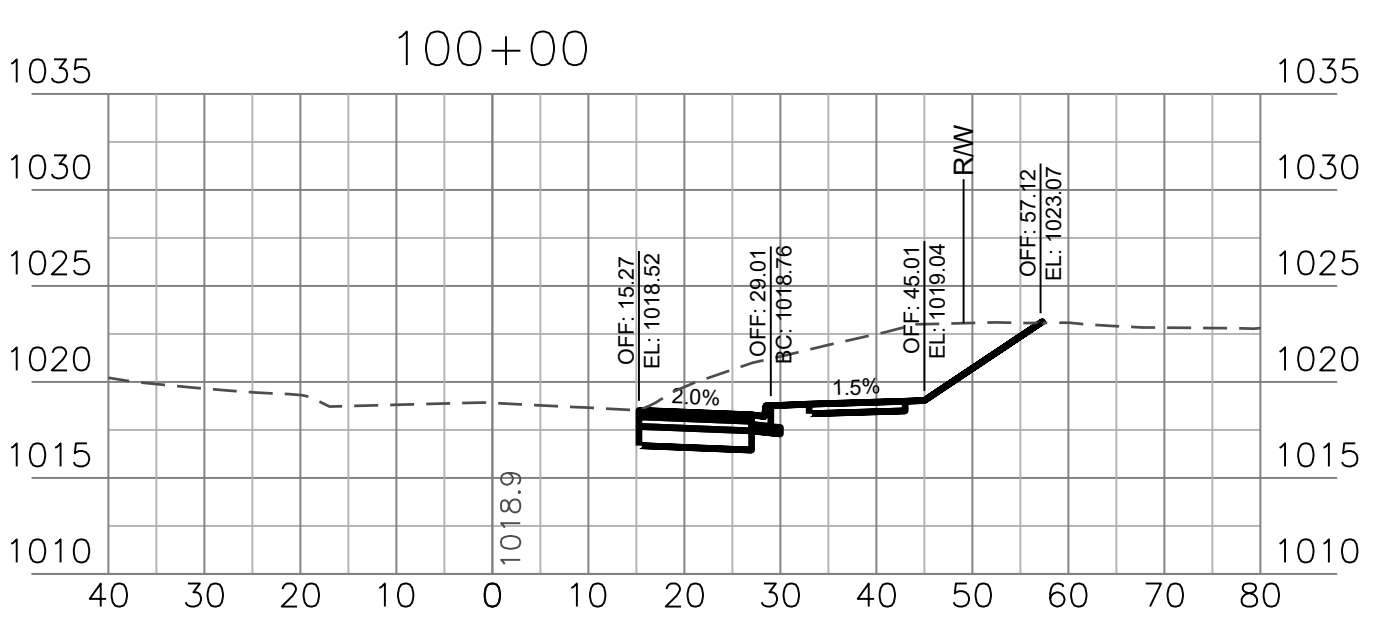
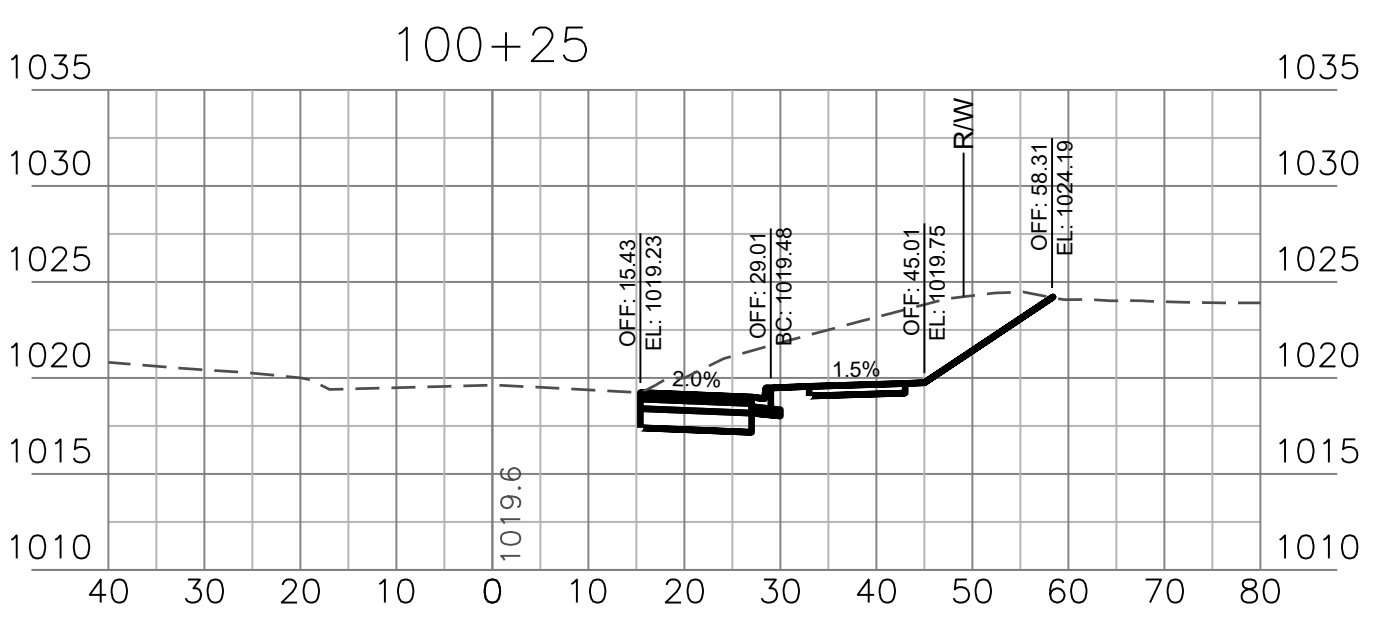
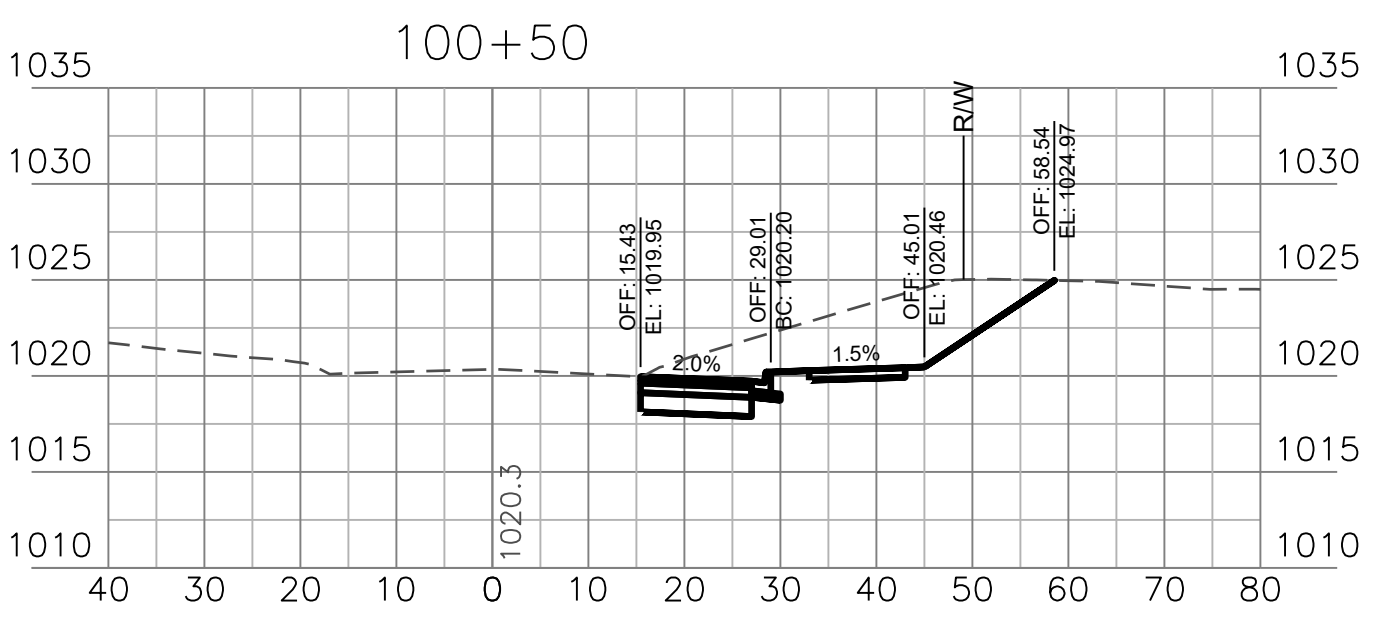
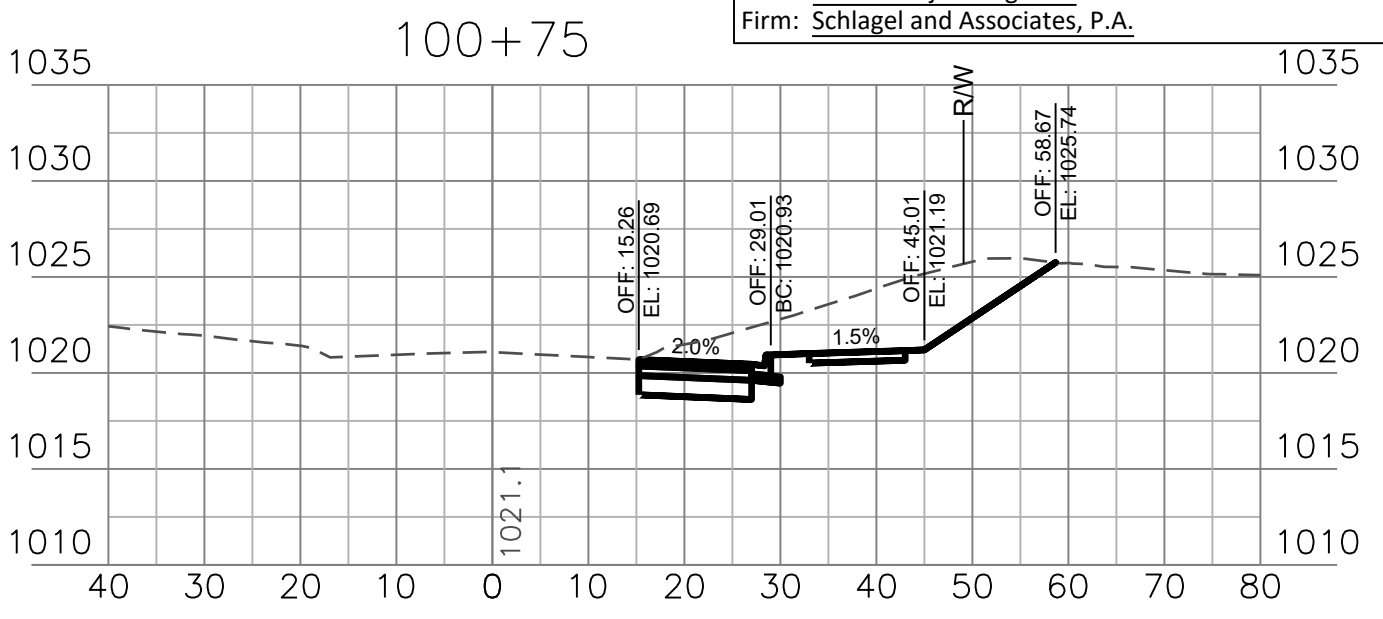
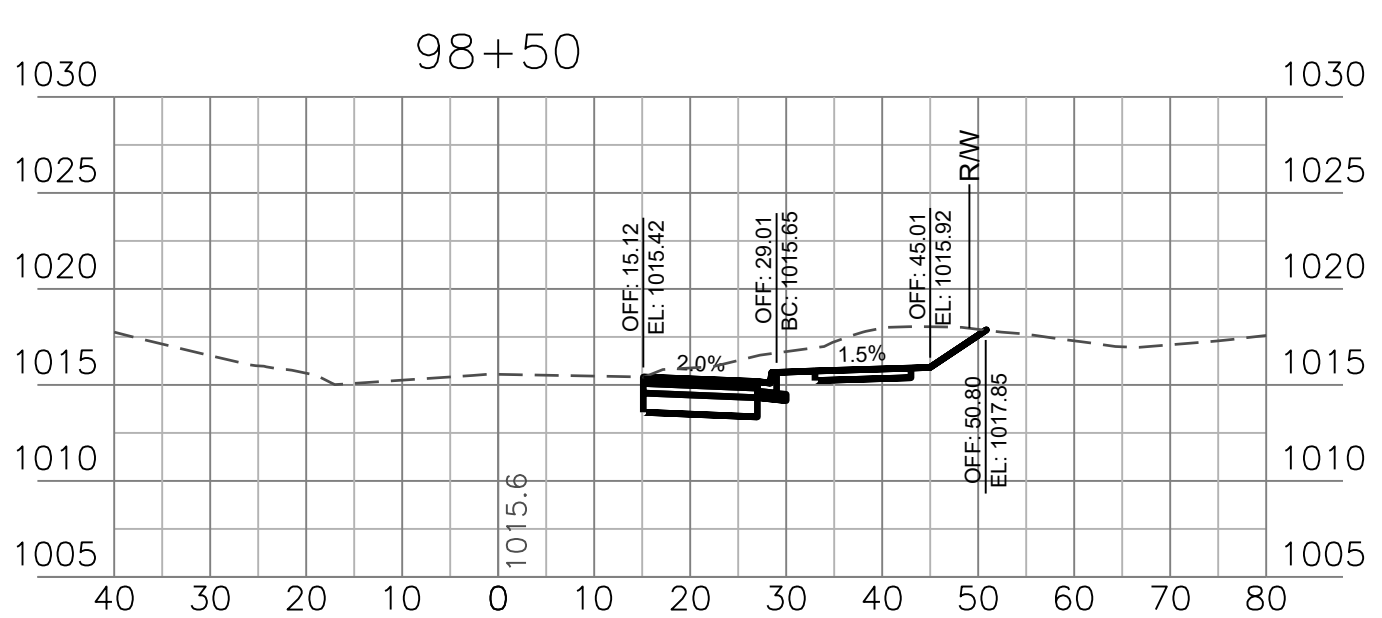
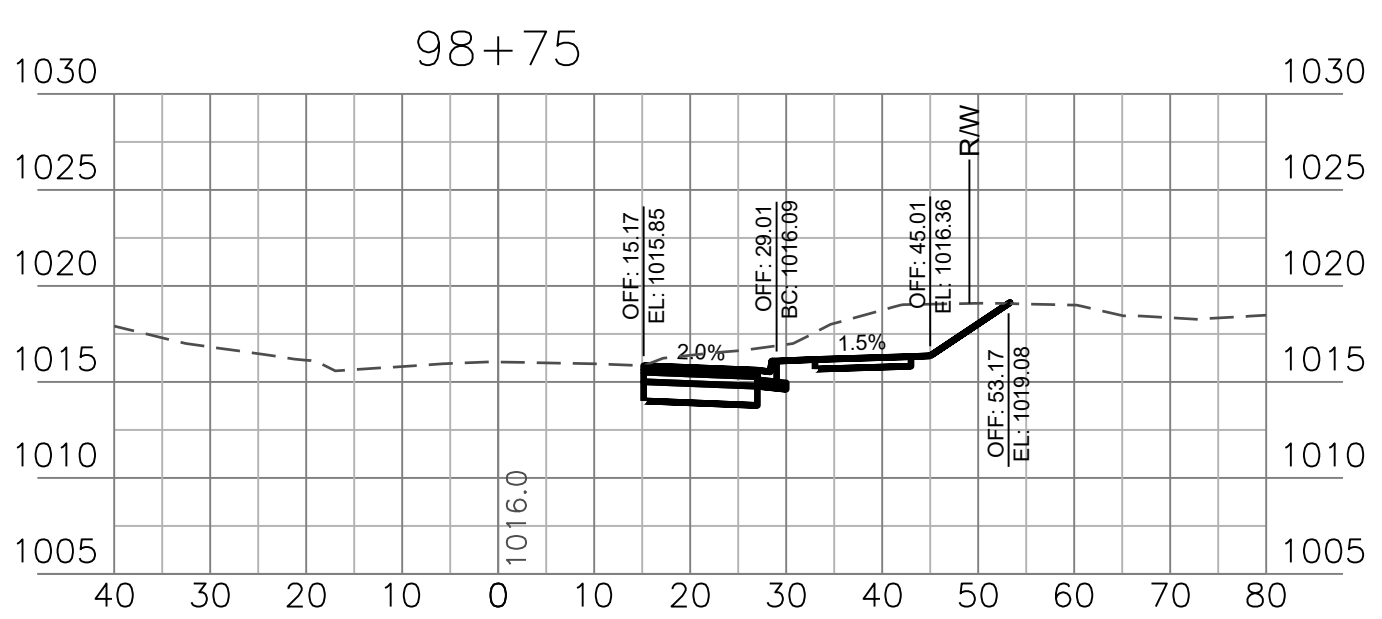
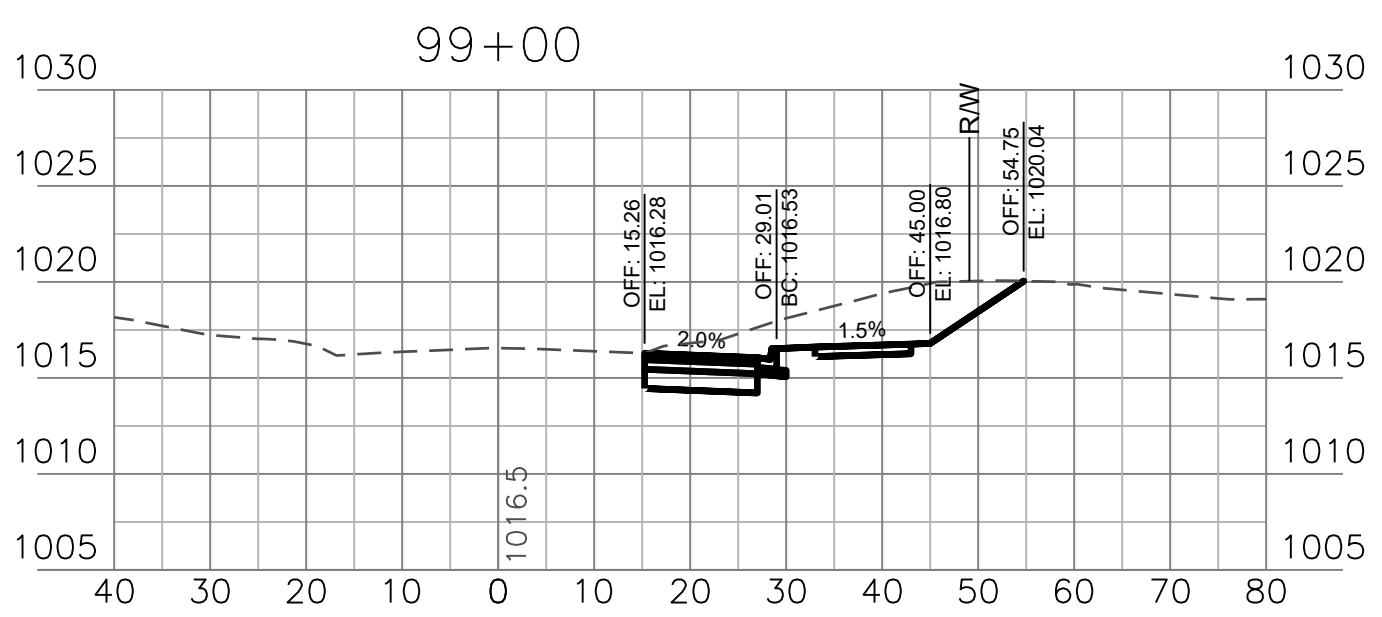
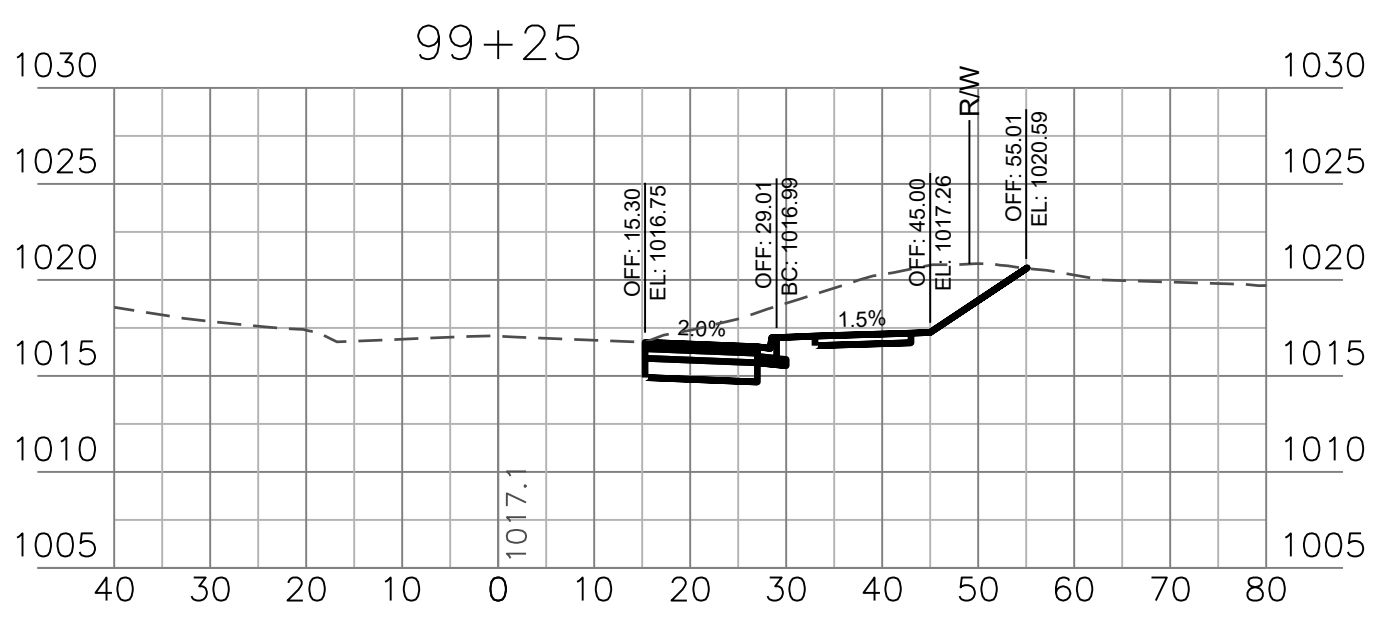
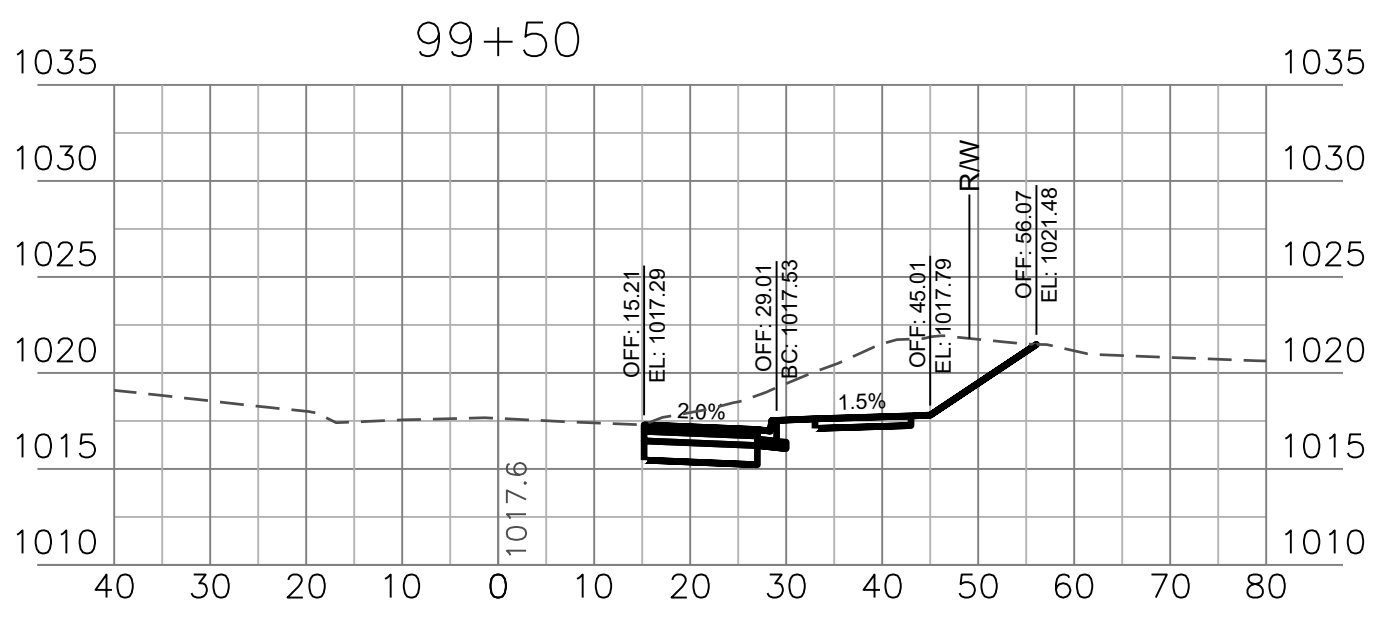
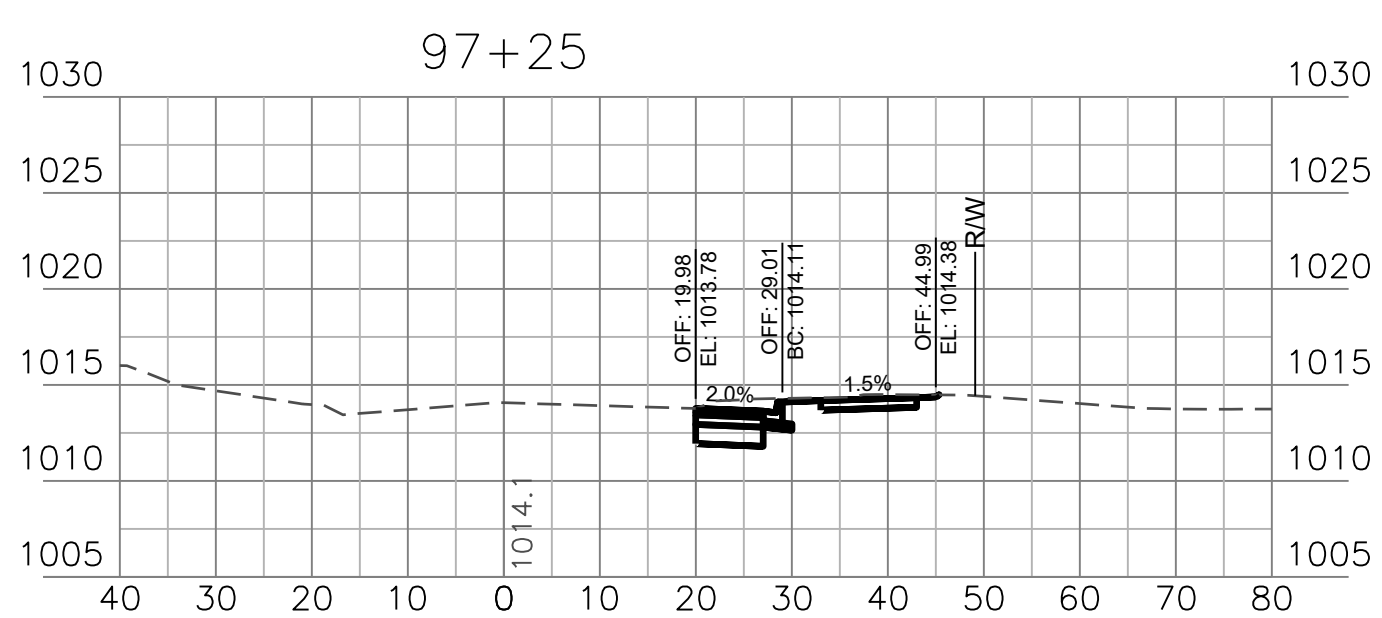
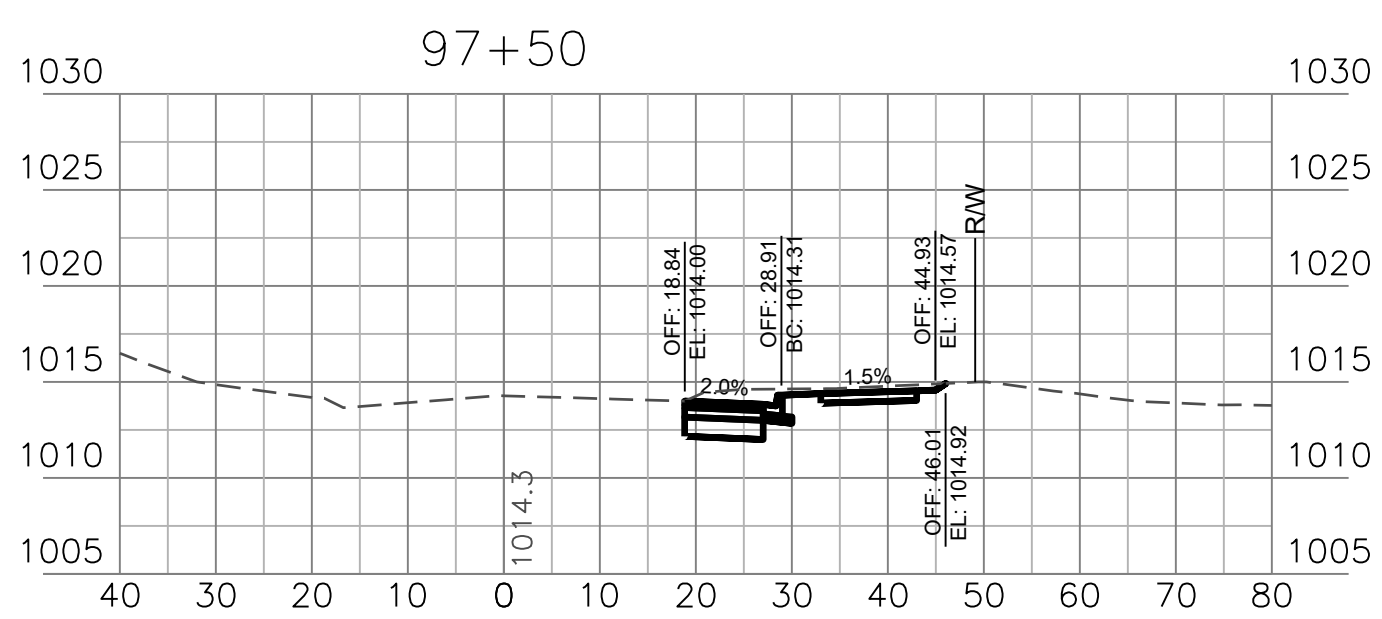
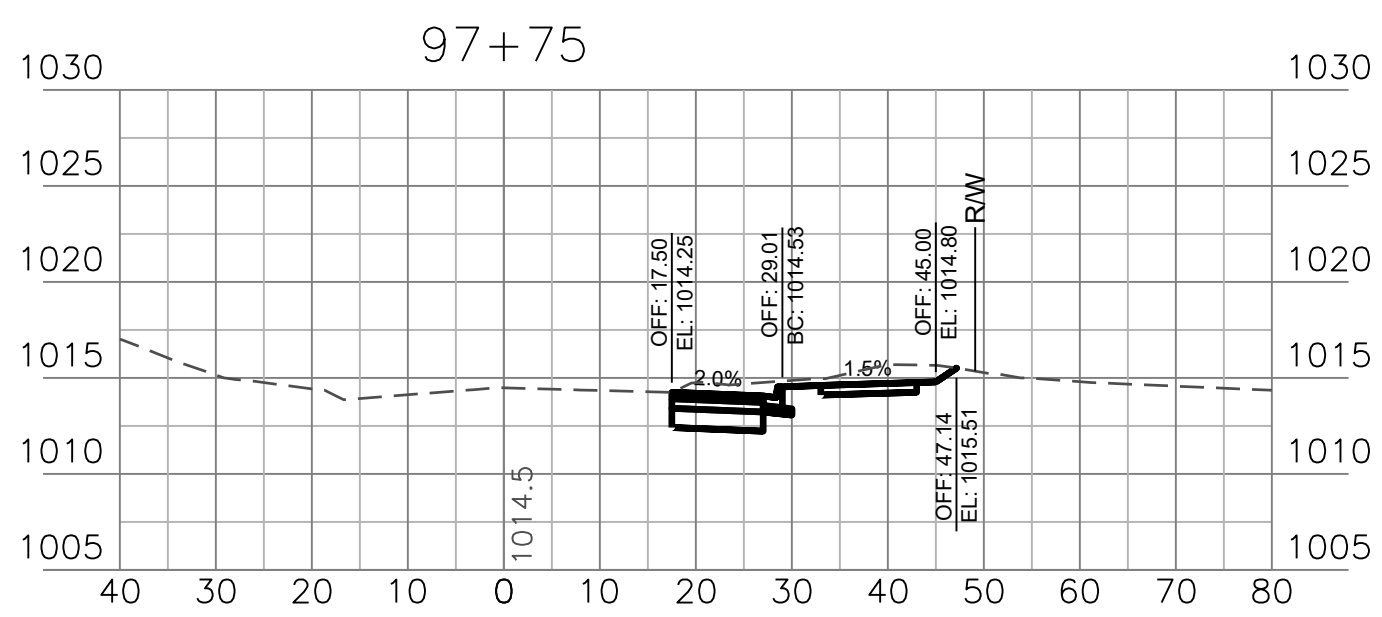
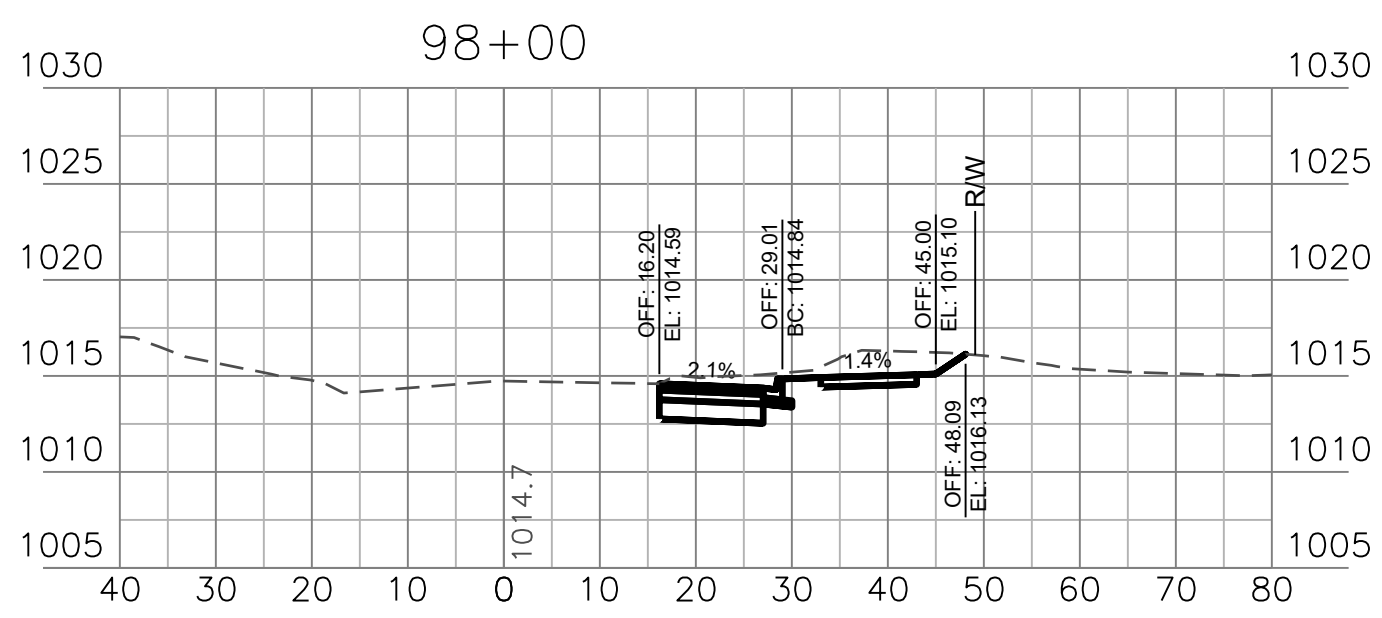
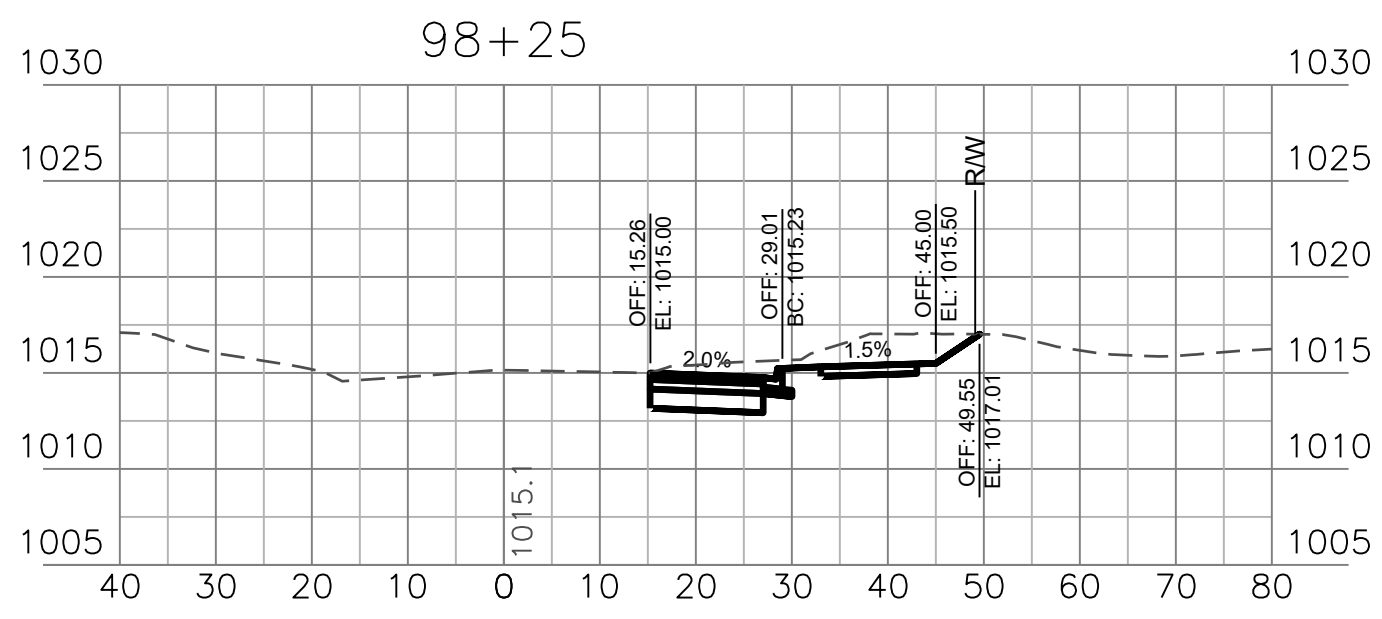
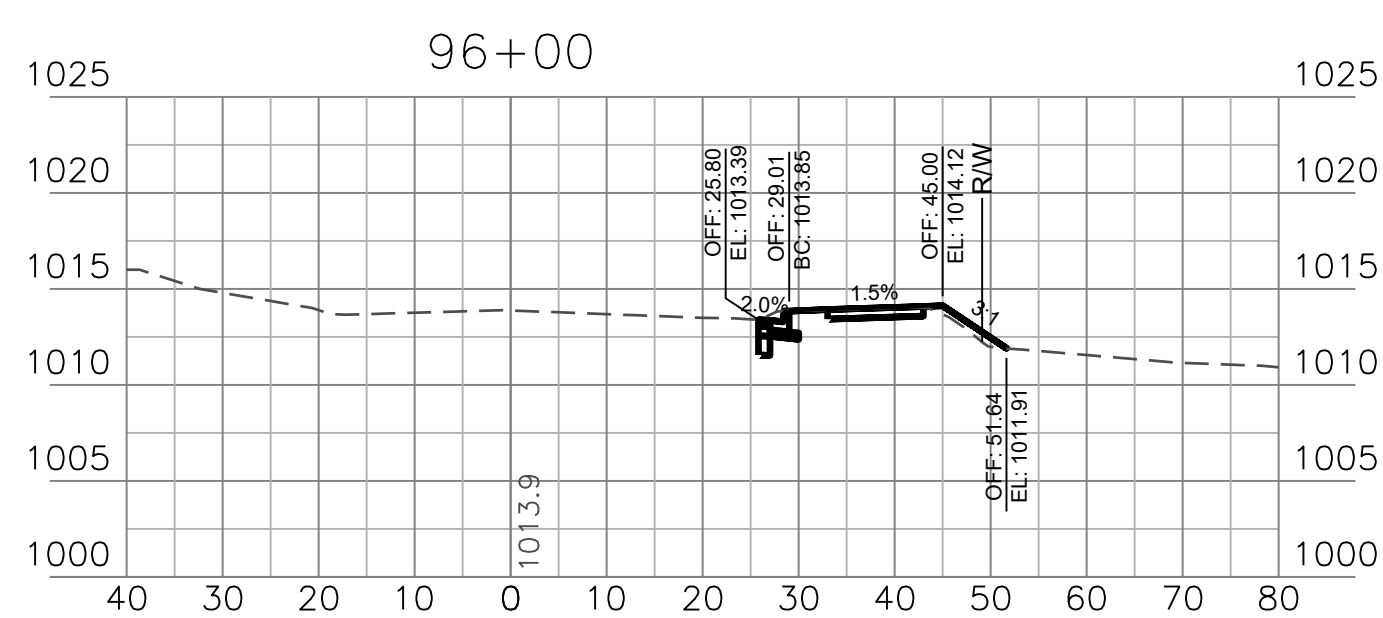
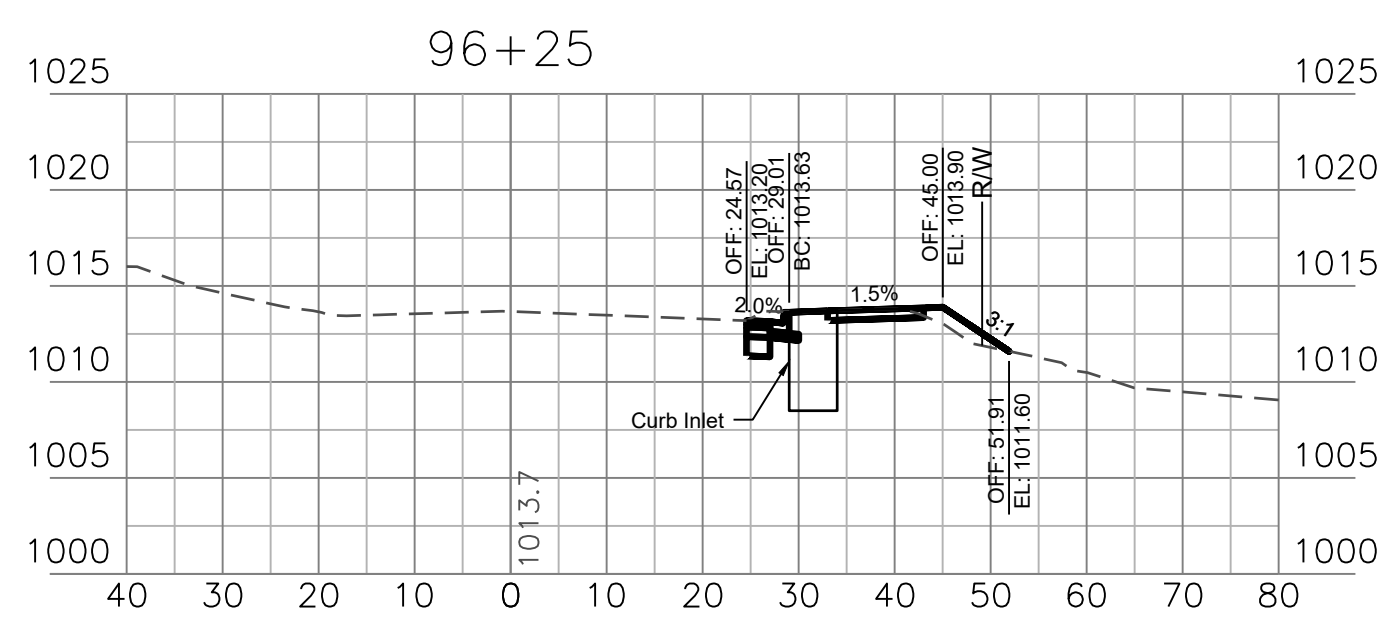
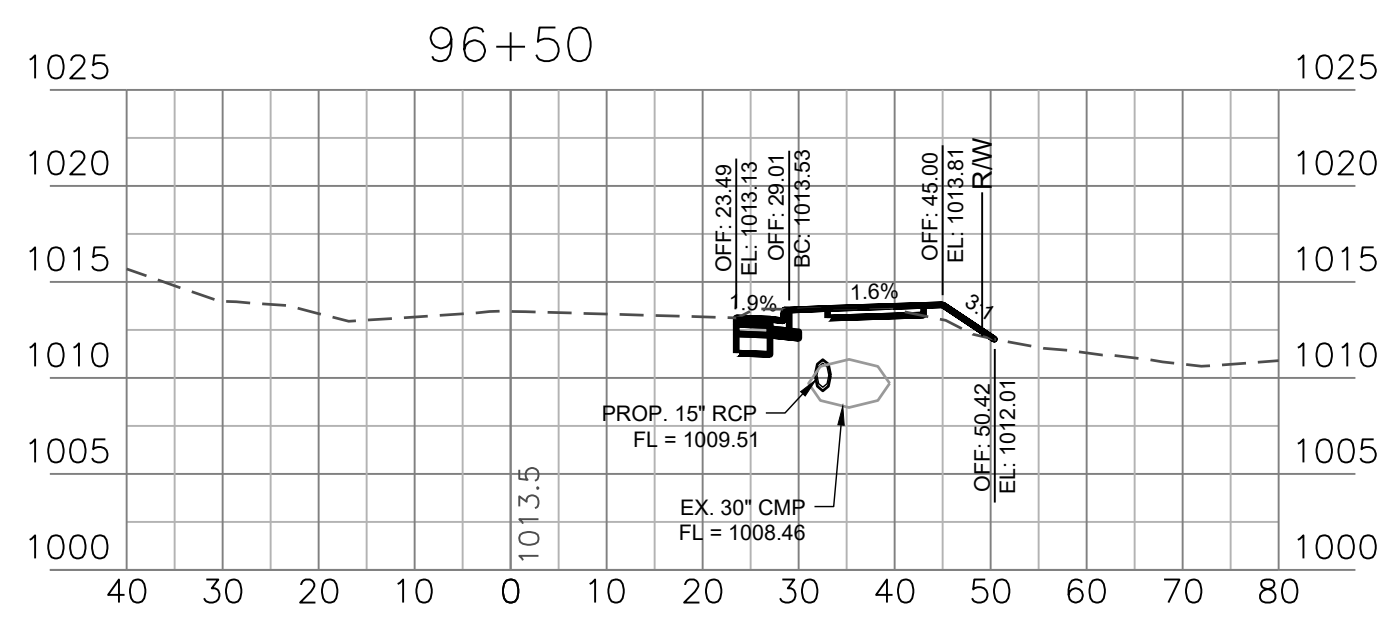
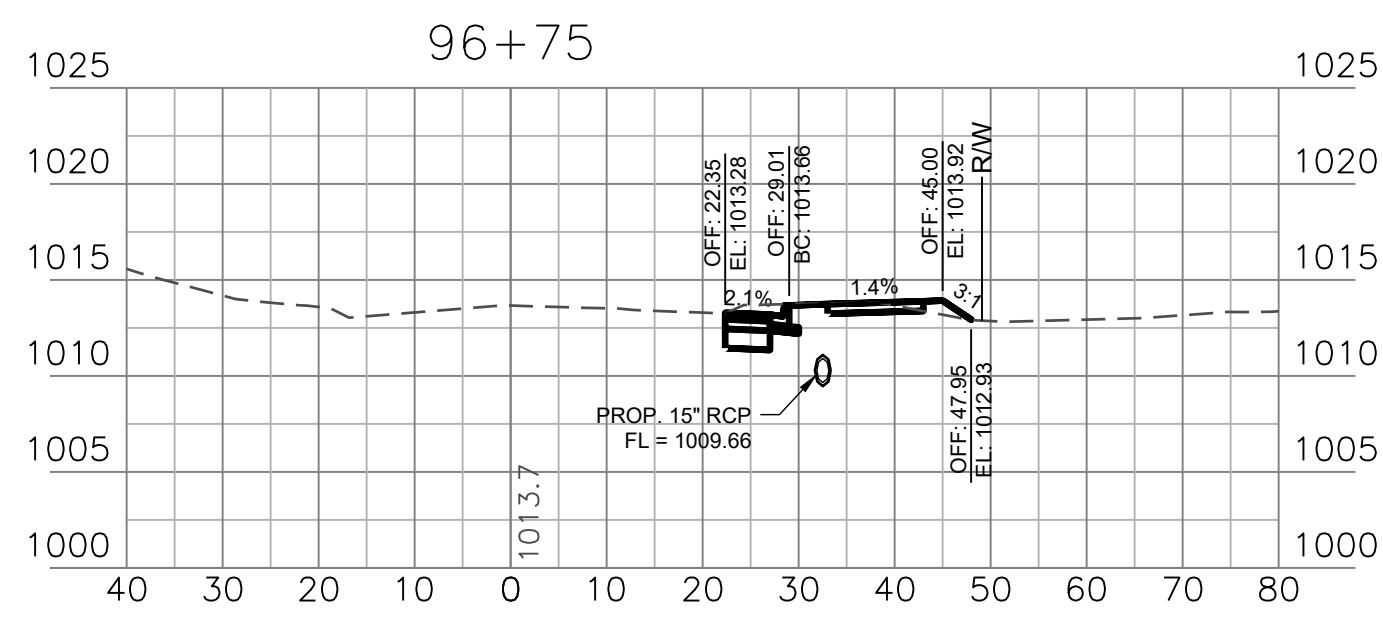
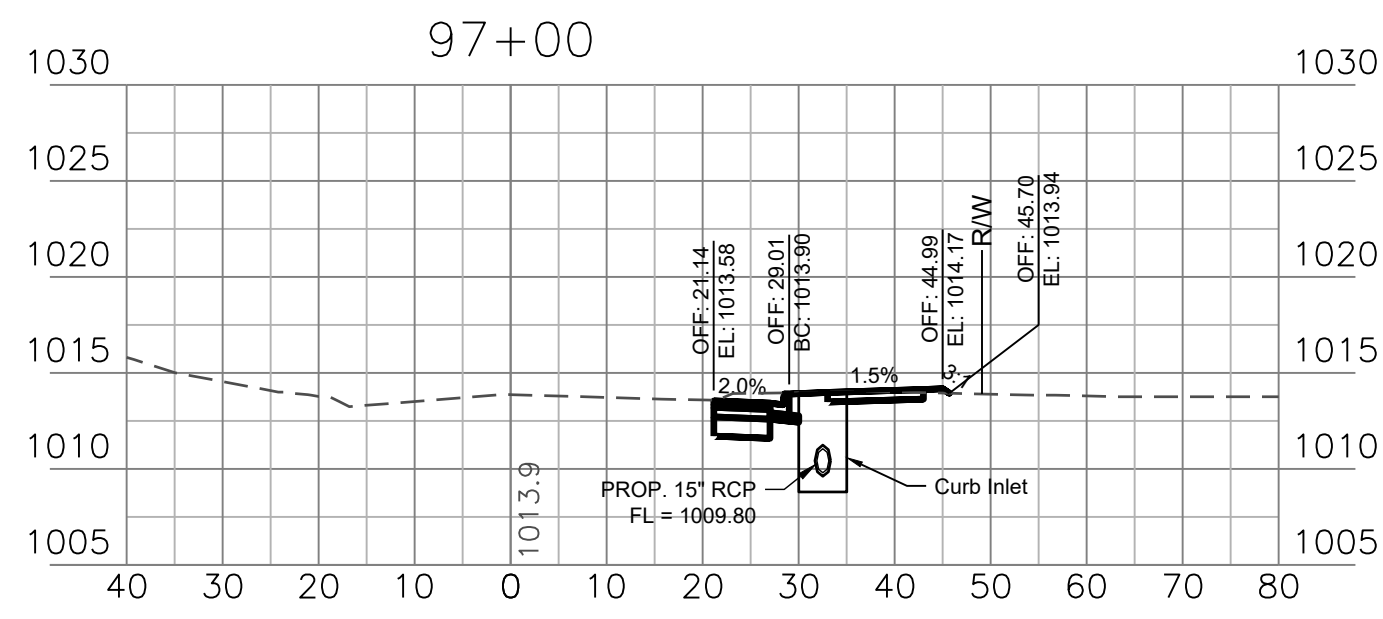
SCHLAGEL & ASSOCIATES, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
1 6/7/2024	per City Comments
2 10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
3 10/24/2024	Reviewed detail for thickened curb.
4	
5	
6	
7	
8	

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/22/2024
PROJ. NUMBER:	21-136



RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

100-00 100.10", 1.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.



PREPARED BY:

JAMES L. LONG
5/16/2025
PROFESSIONAL ENGINEER

SCHLAGER & ASSOCIATES, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

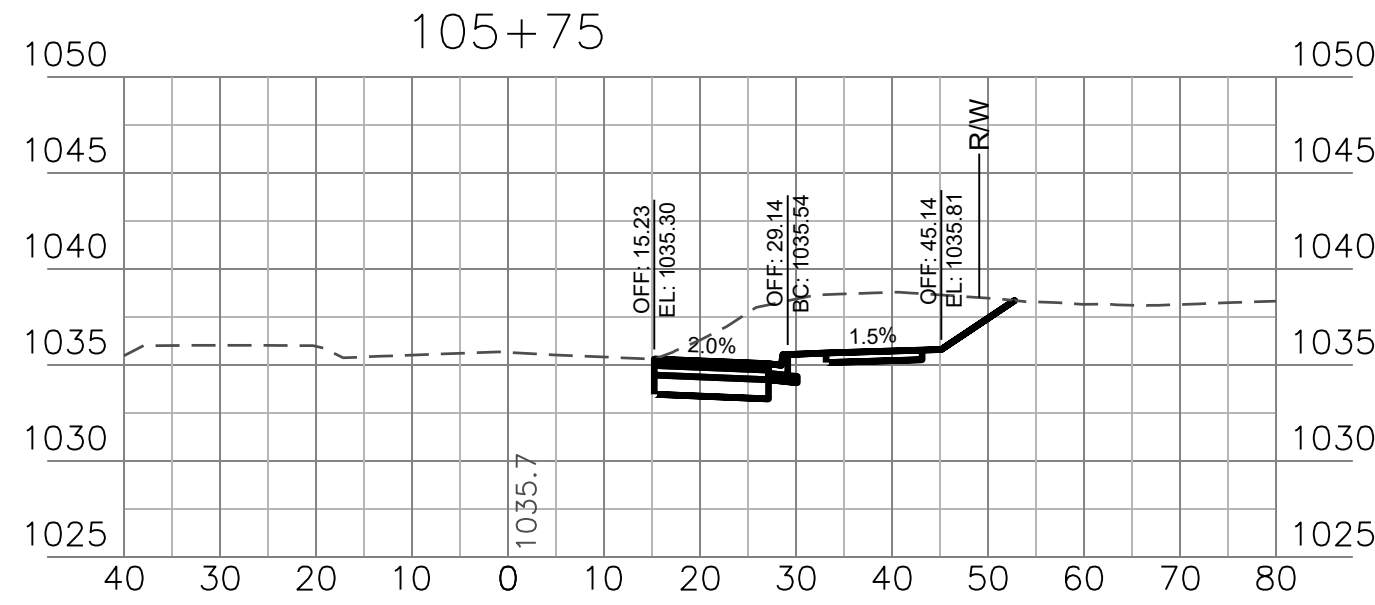
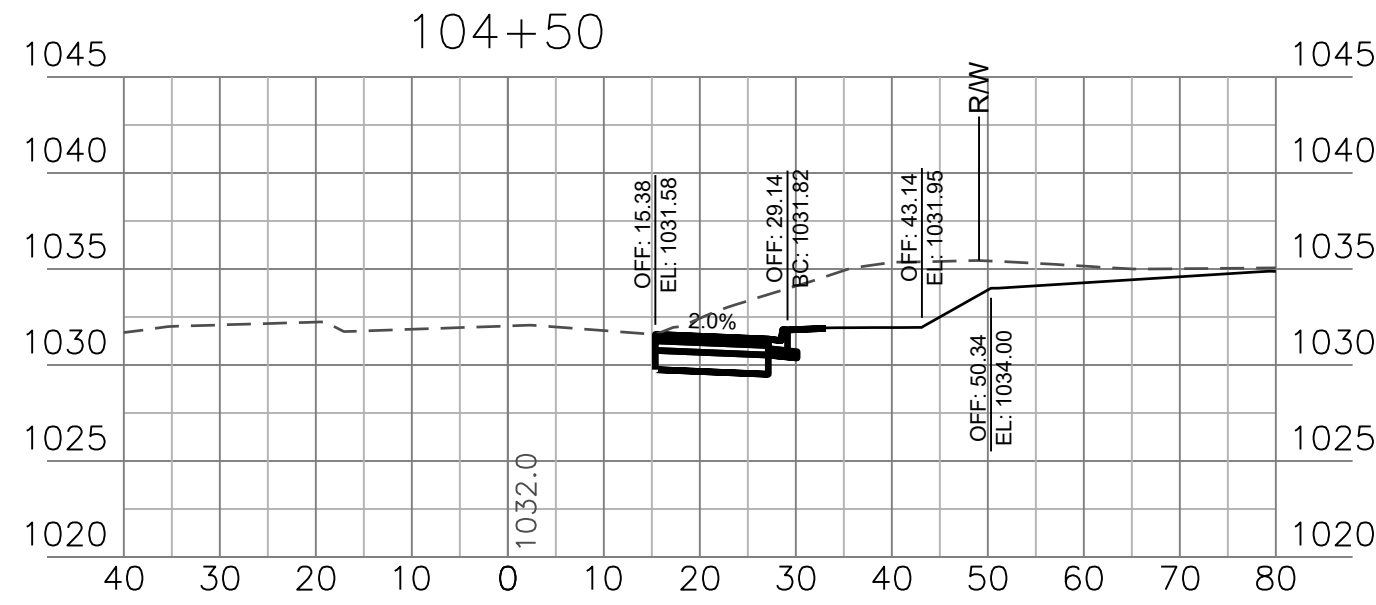
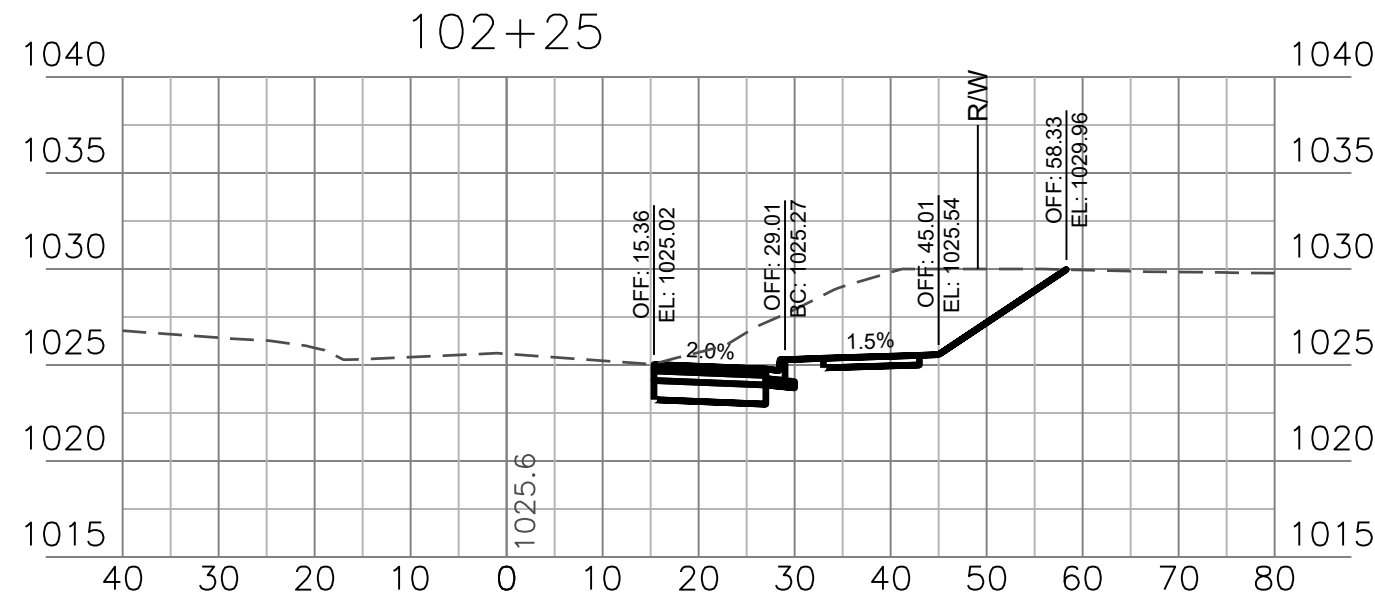
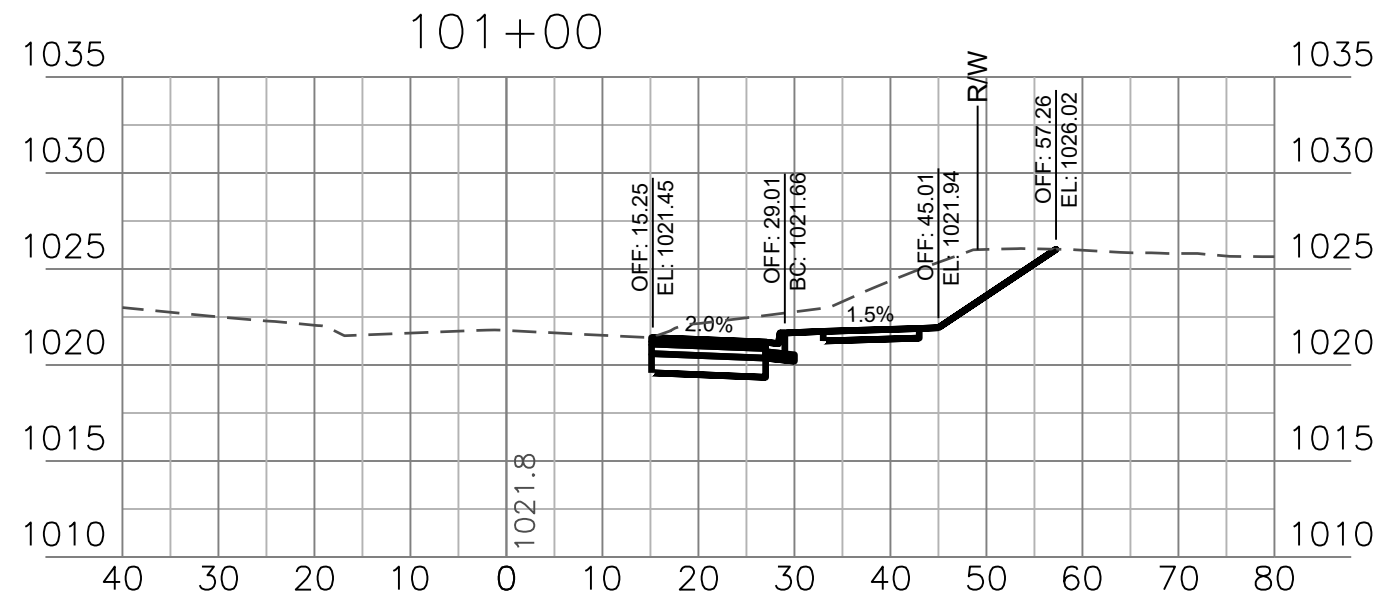
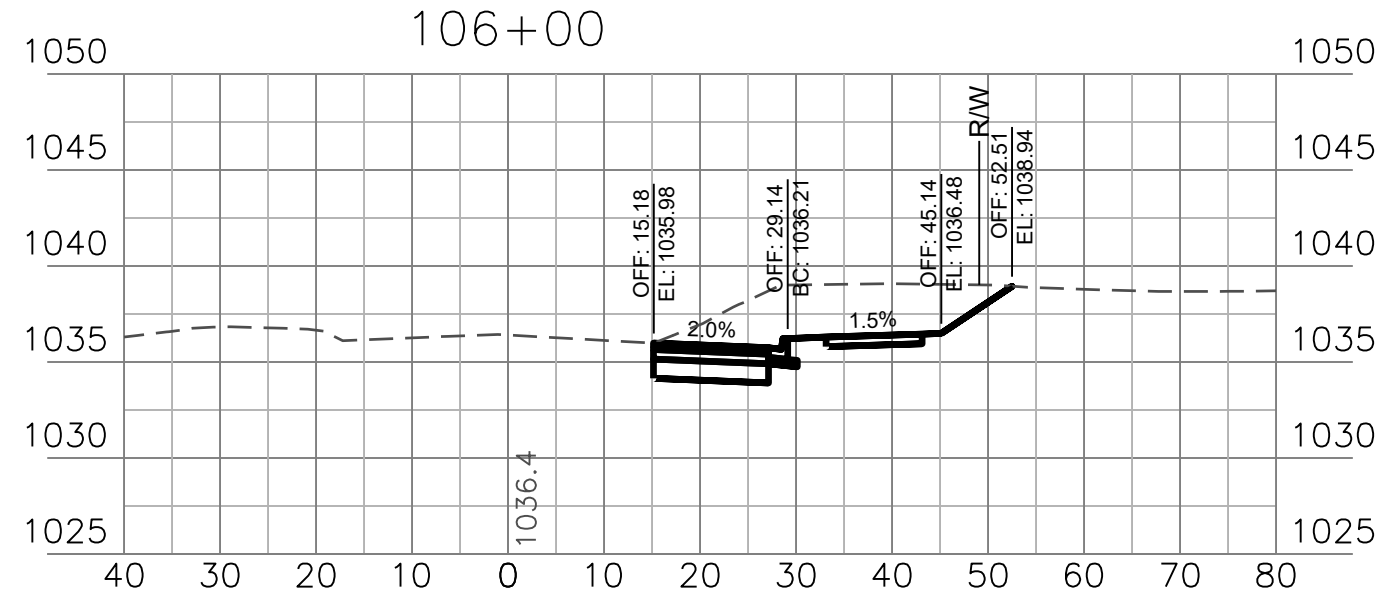
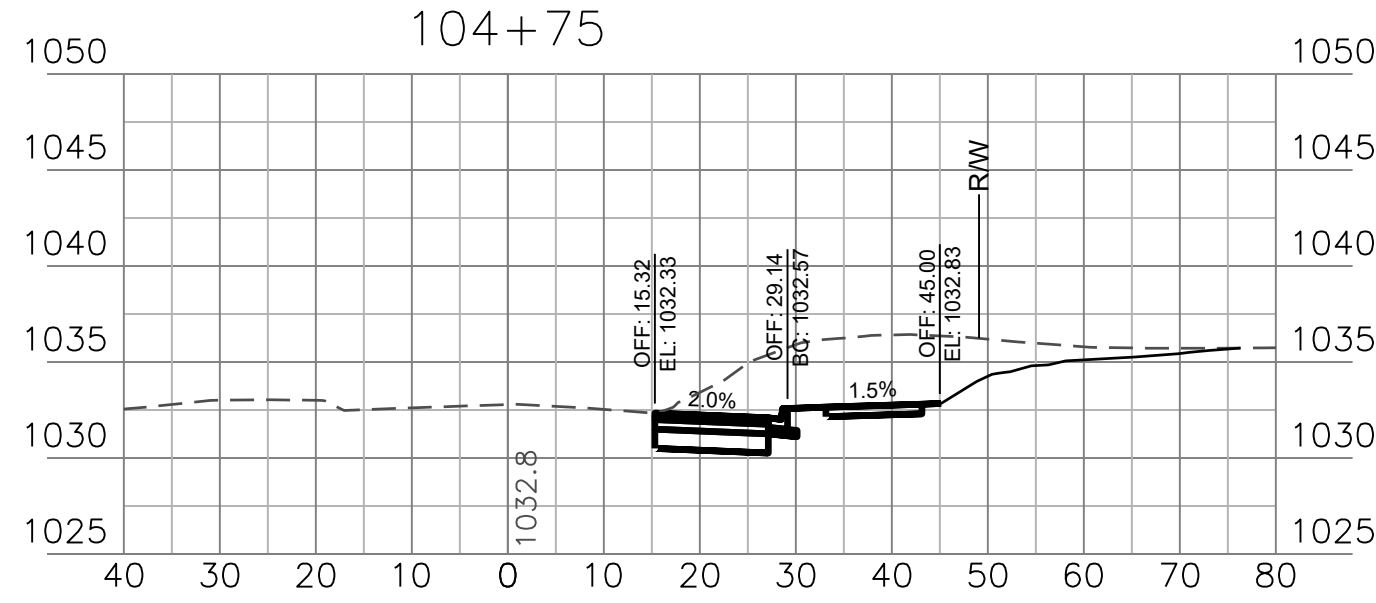
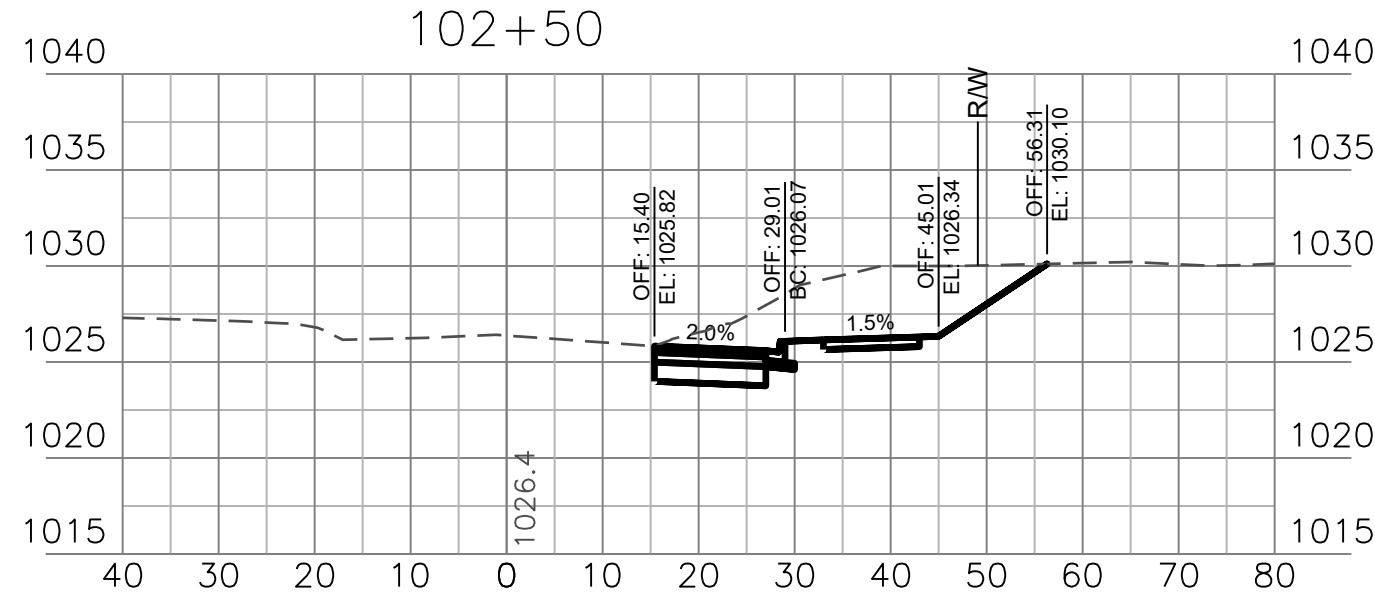
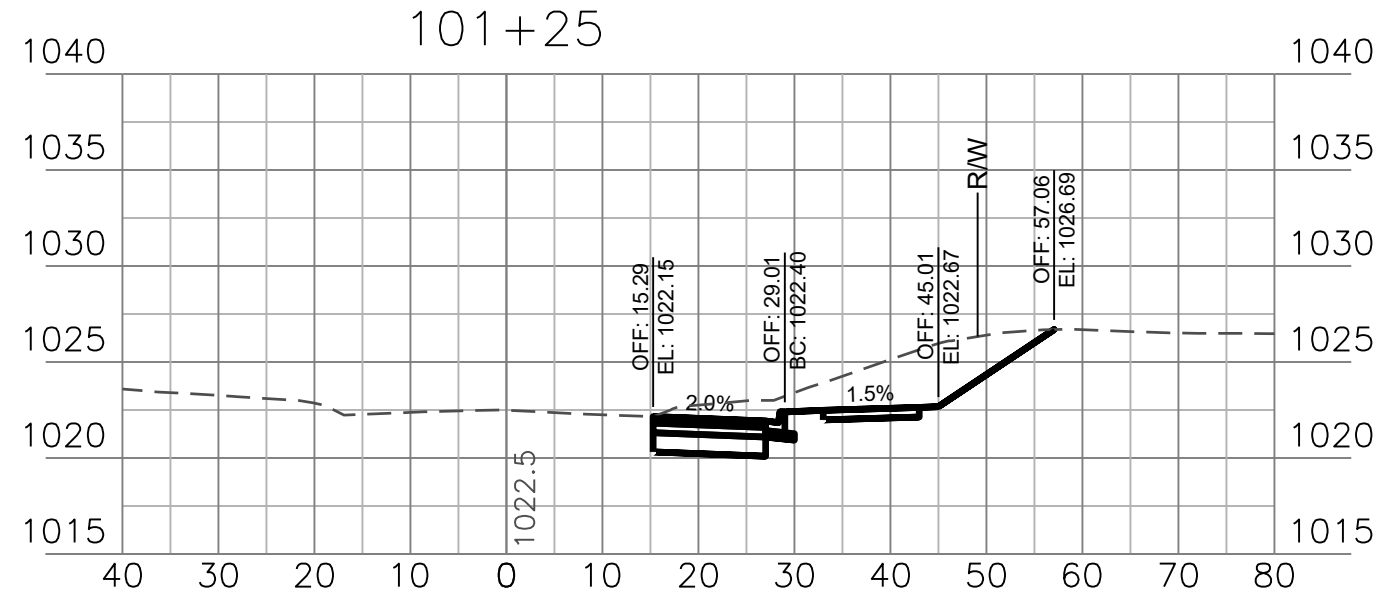
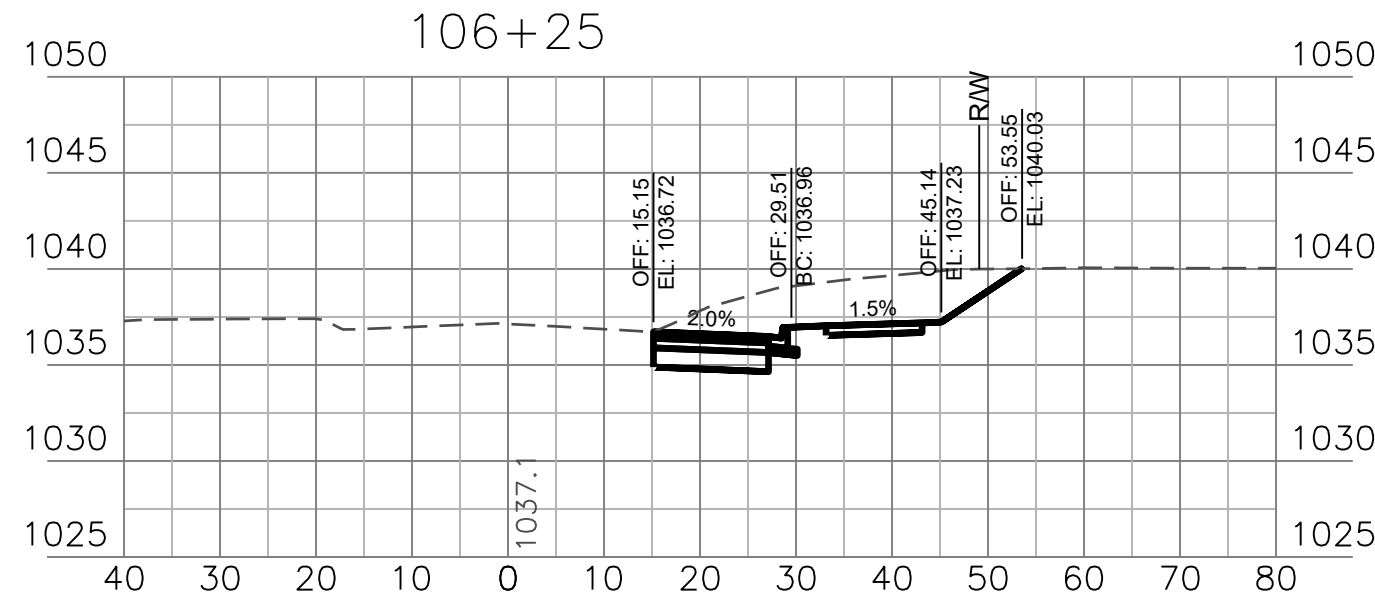
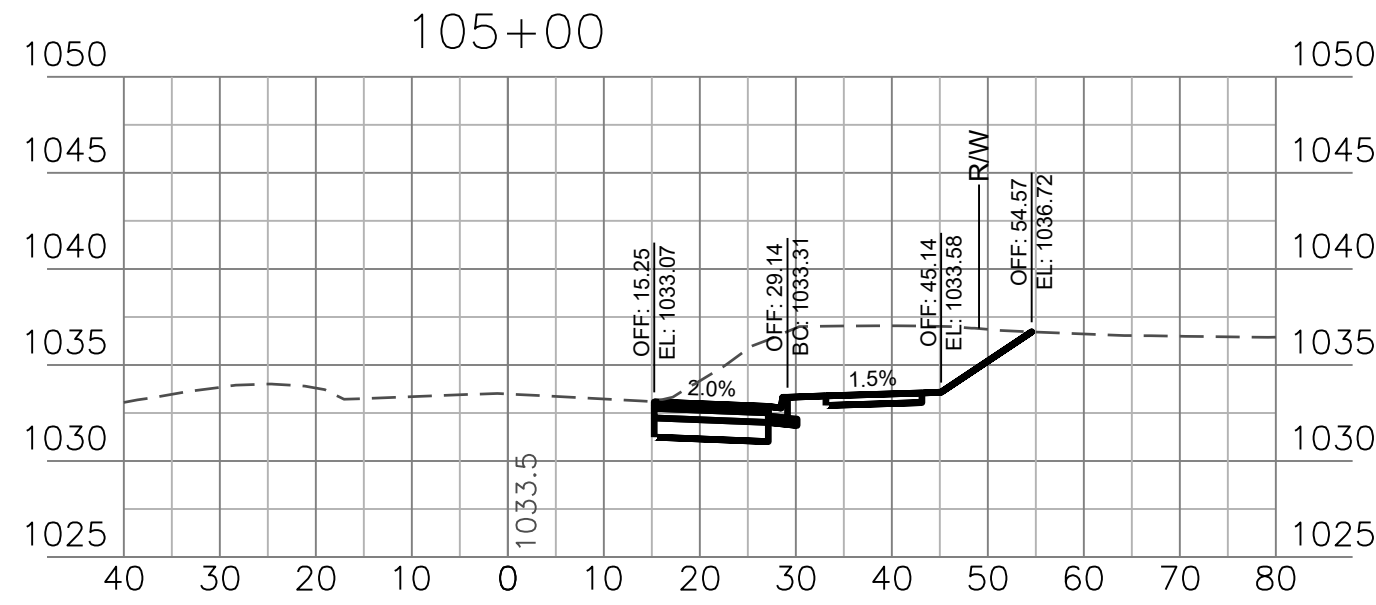
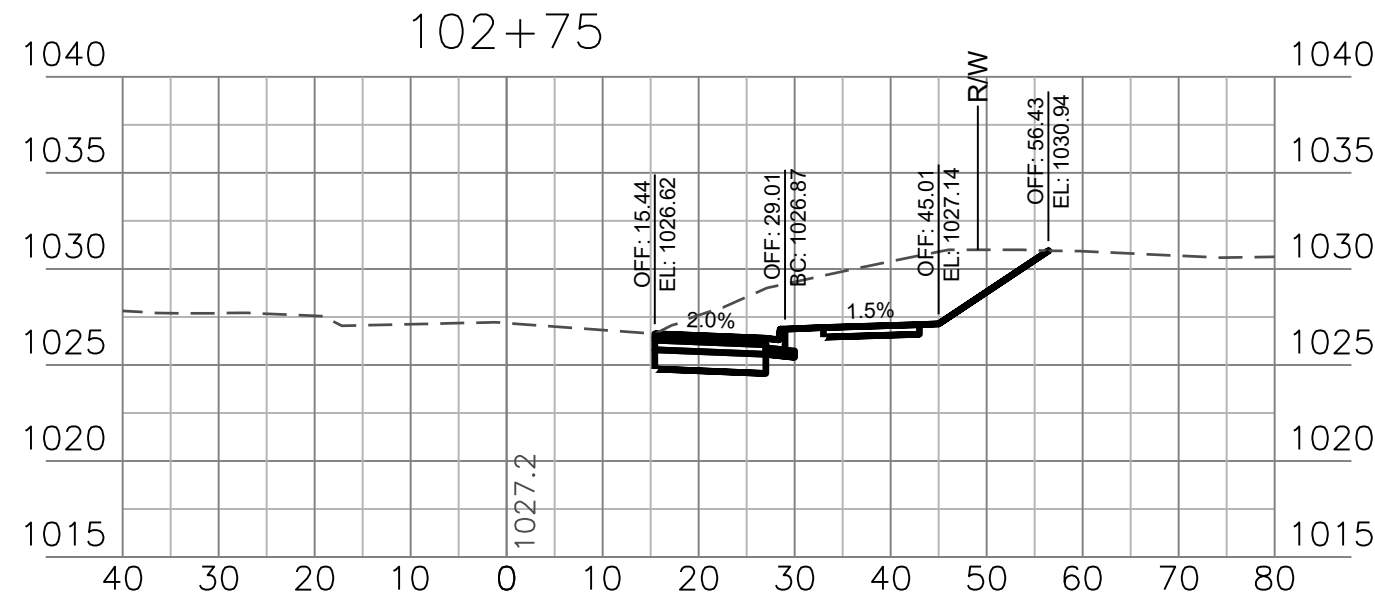
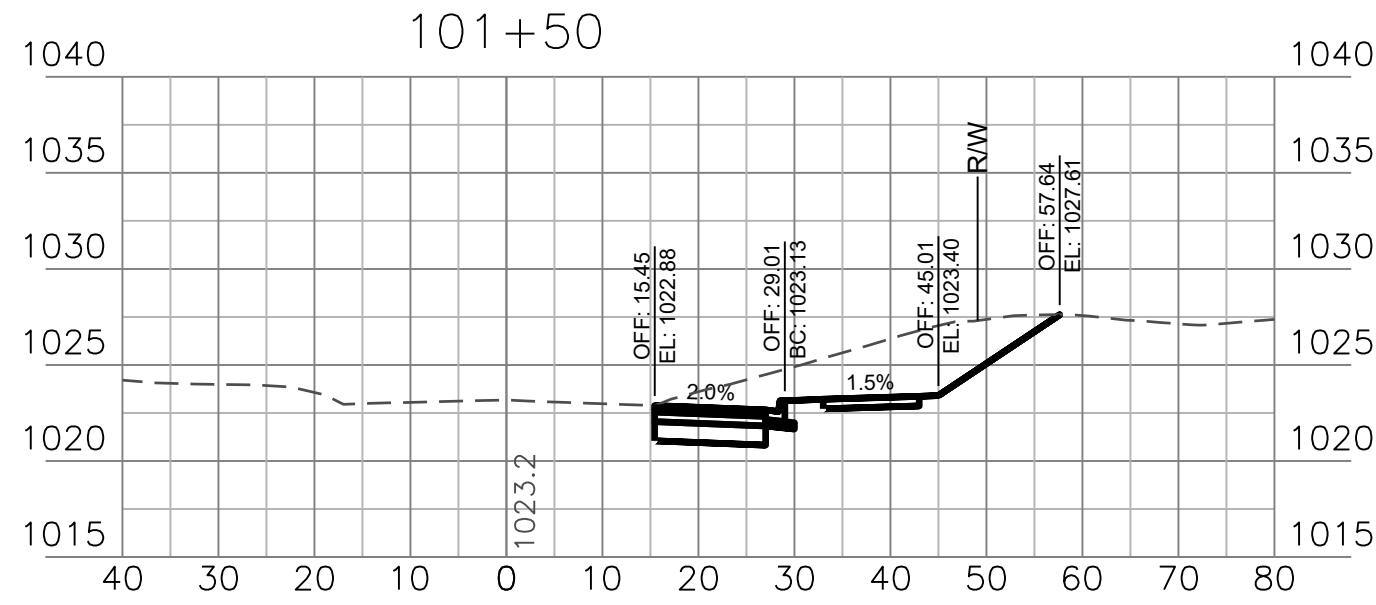
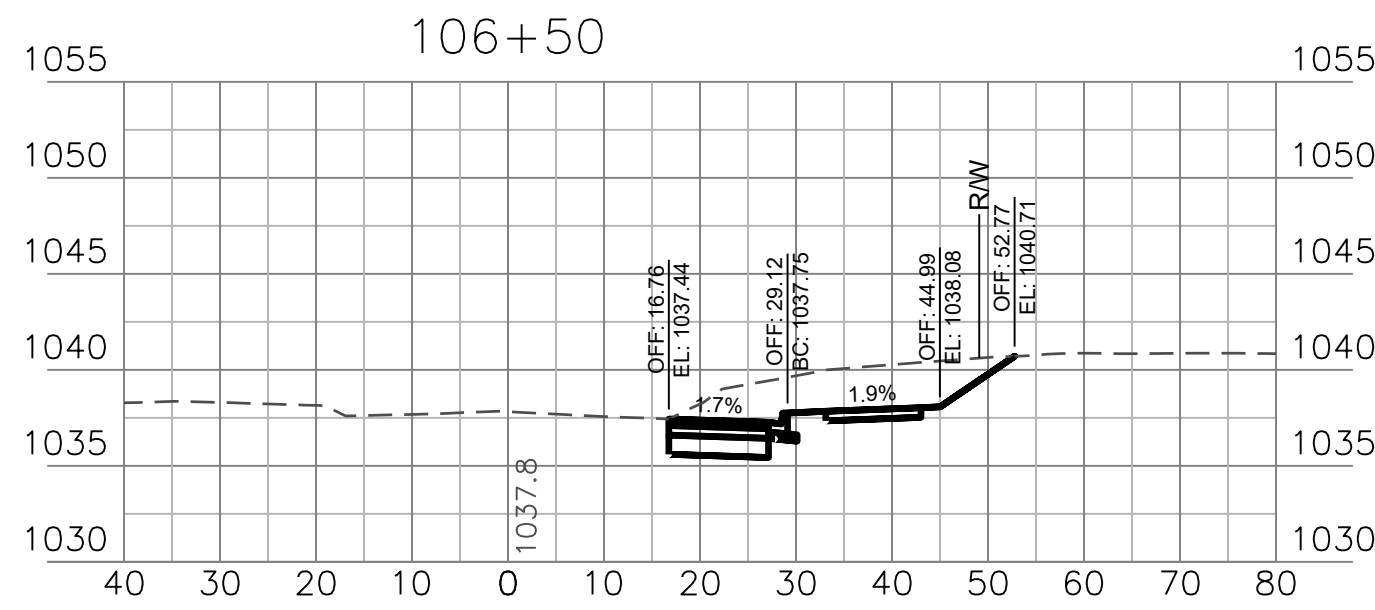
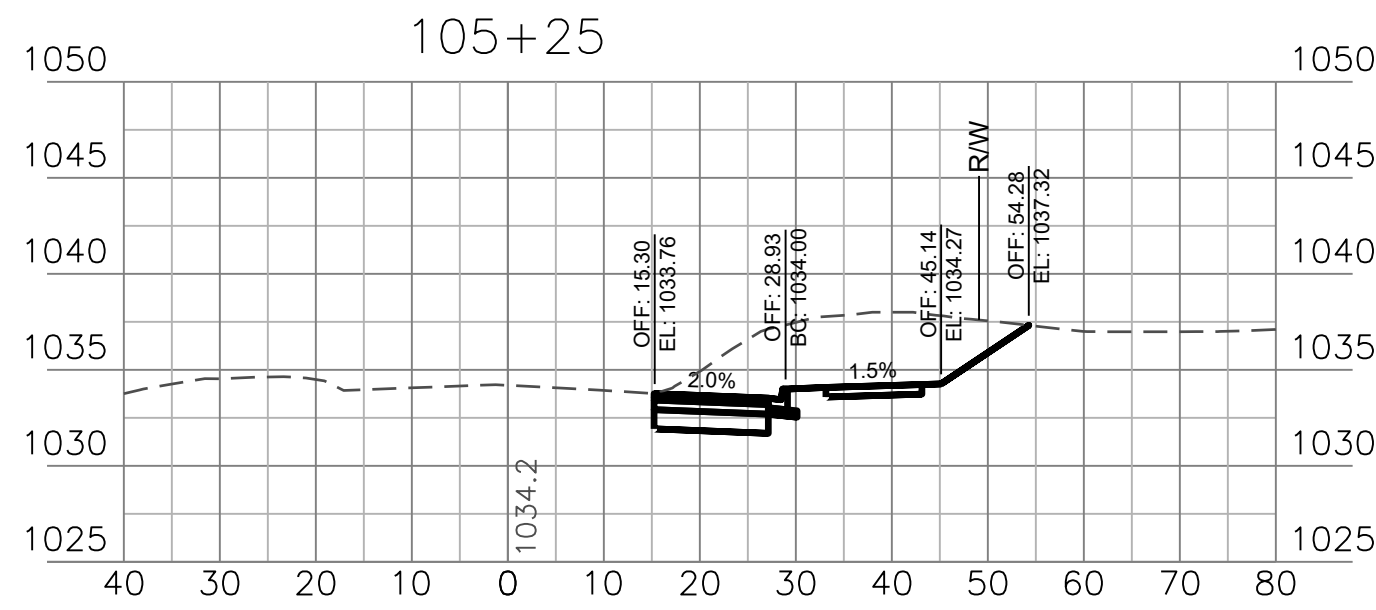
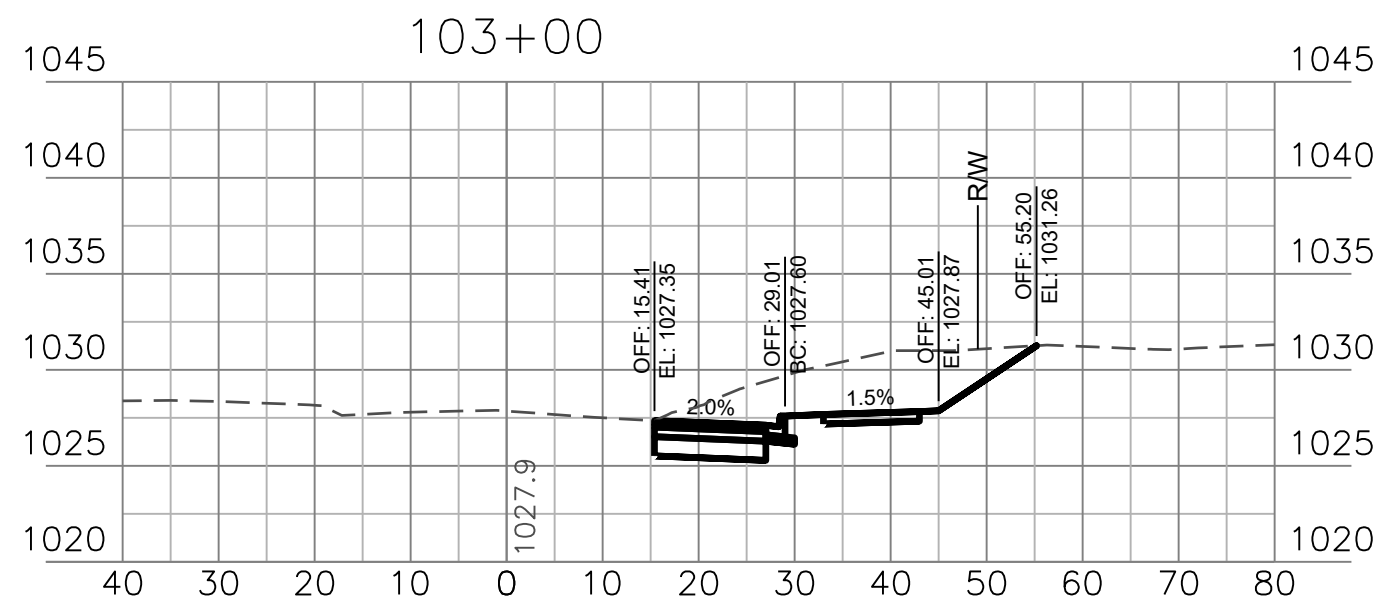
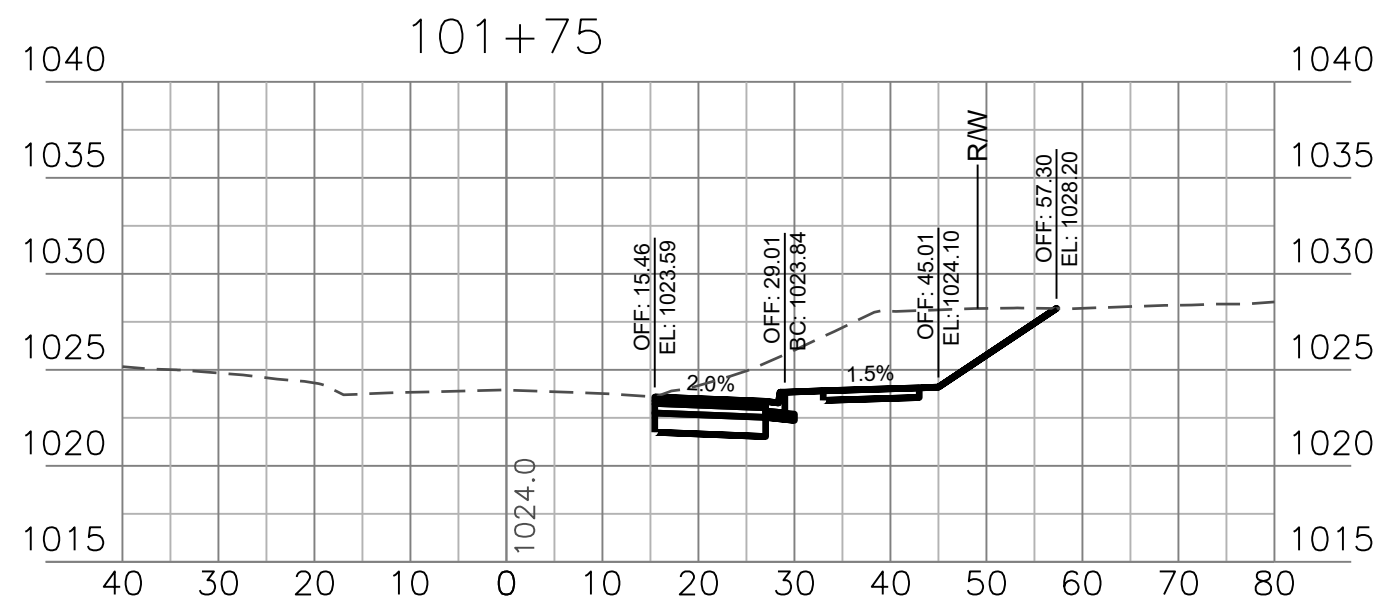
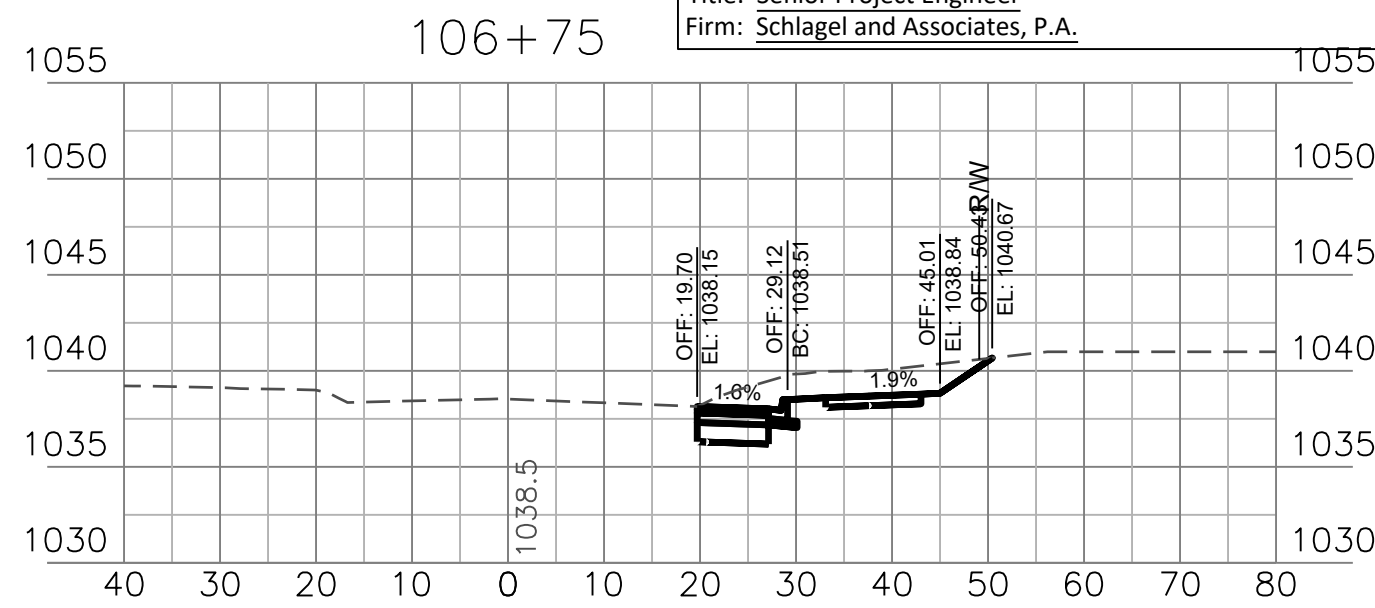
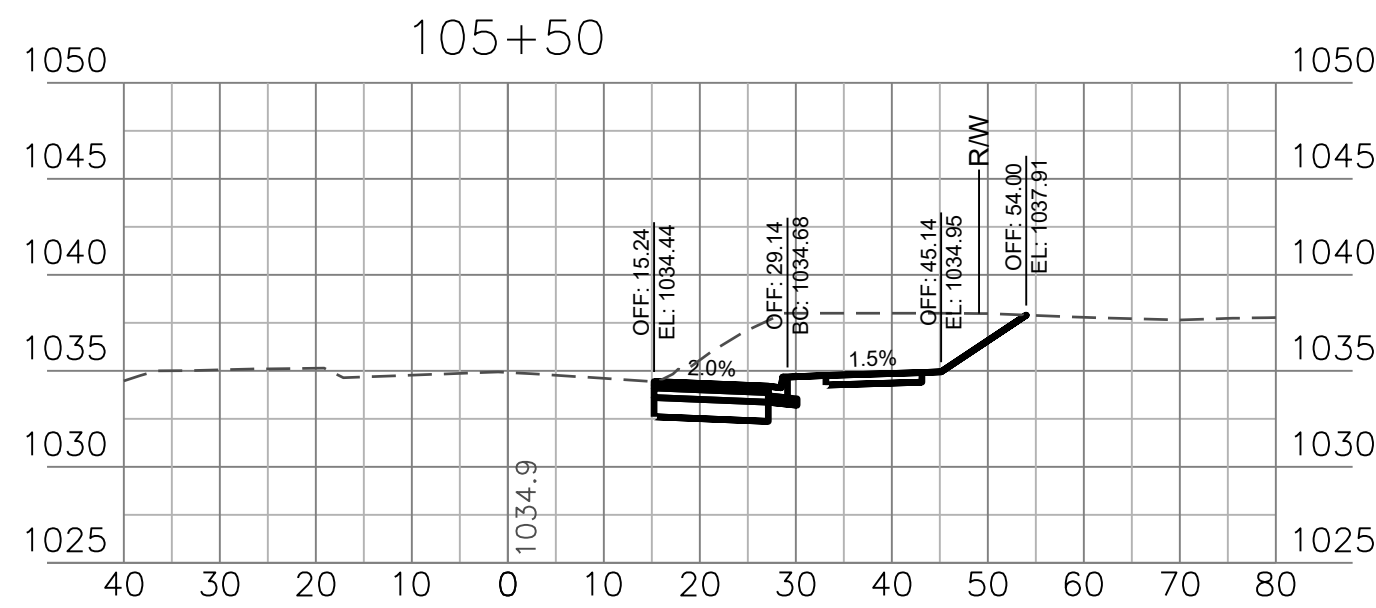
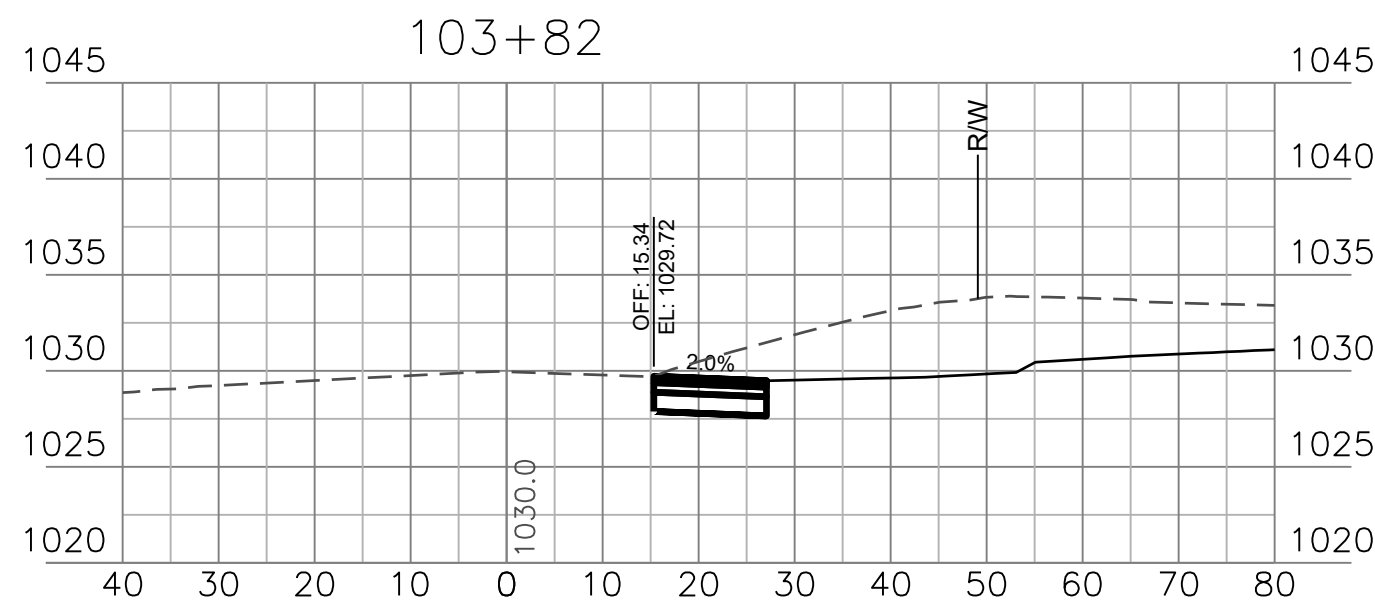
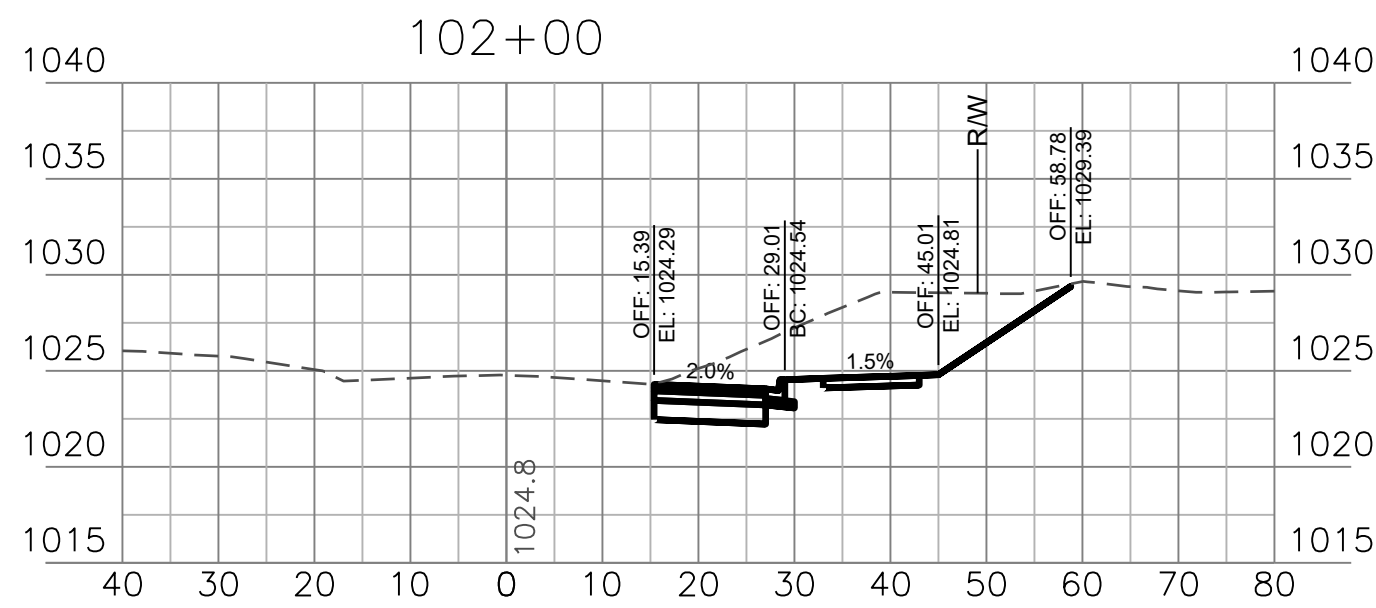
SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
6/7/2024	per City Comments
10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
10/24/2024	Revised detail for thickened curb.

DRAWN BY:	NCA
CHECKED BY:	JLL
DATE PREPARED:	2/22/2024
PROJ. NUMBER:	21-136

CROSS SECTION
STA. 96+00 - 100+75

SHEET
11



RECORD DRAWING

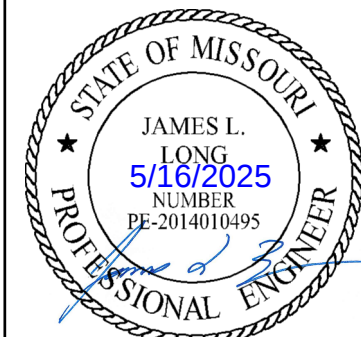
The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes, and it is solely based on information obtained by Schlagel and Associates.

~~100.00~~ 100.10", "1.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlagel and Associates, P.A.



PREPARED BY:



SCHLAGEL & ASSOCIATES, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

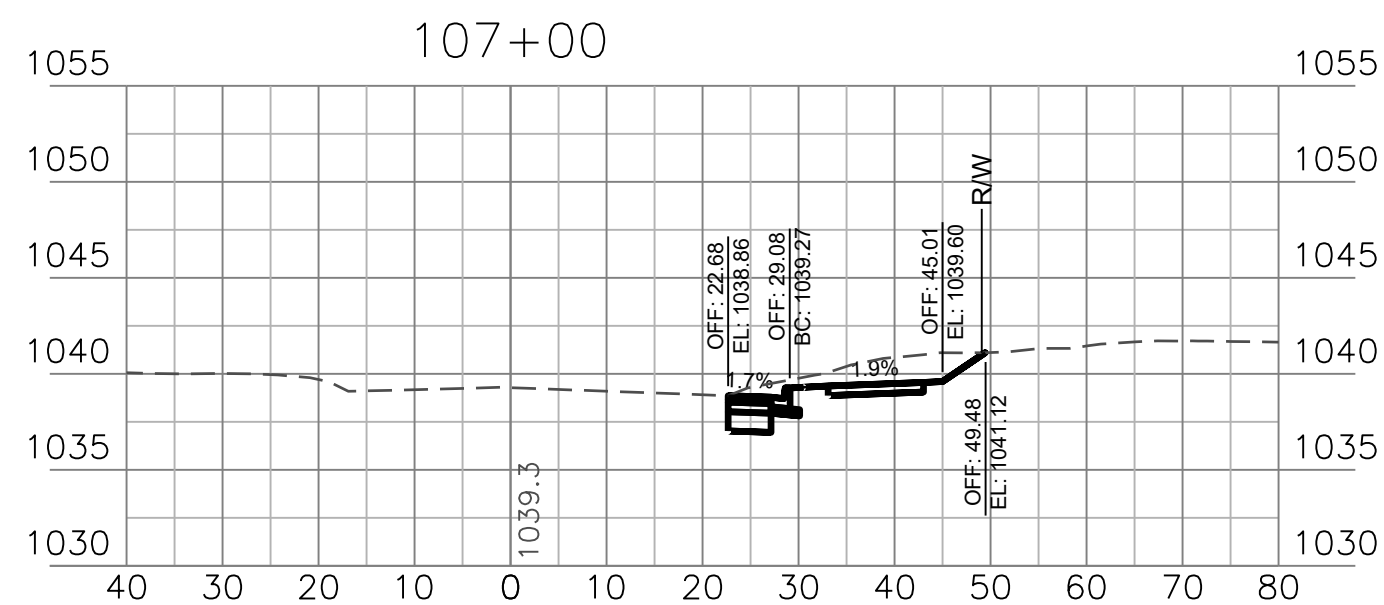
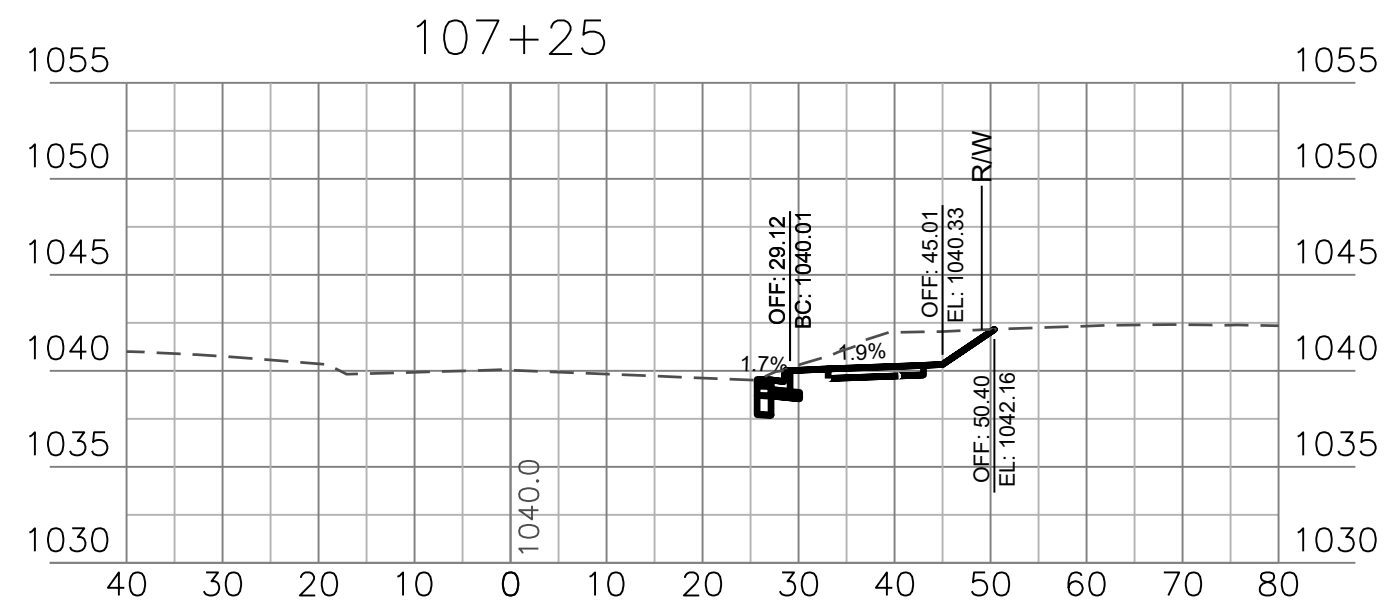
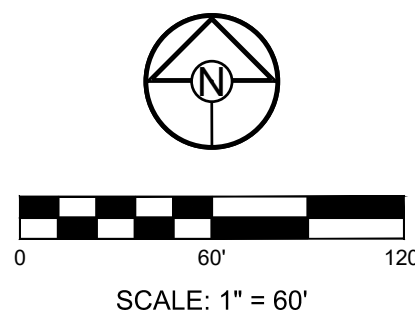
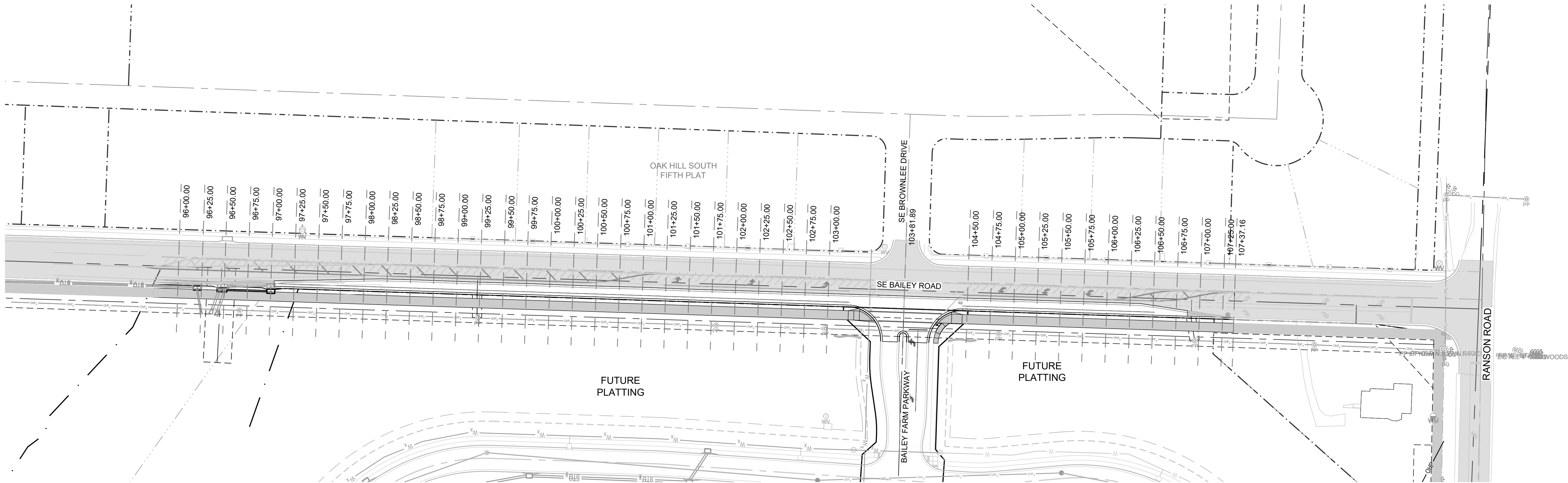
SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

DRAWN BY:	REVISION DATE	DESCRIPTION
NCA	6/7/2024	per City Comments
	10/8/2024	Added Lane Widening Detail for Street Sections 6' or Less
JLL	10/24/2024	Reviewed detail for thickened curb.
DATE PREPARED:		
2/22/2024		
PROJ. NUMBER:		
21-136		

CROSS
SECTIONS STA.
101+00 - 106+75

SHEET

12



RECORD DRAWING

The information provided on this drawing conforms to construction records; it is not intended for construction, implementation or recording purposes; and it is solely based on information obtained by Schlager and Associates.

"400-00 100.10", "4.00% 1.15% slope", or "8-inch HDPE PVC pipe" are all typical examples of revisions that indicate that design data has been replaced with "as-built" information. All other data is as designed and has not been field verified.

Date: 5/16/2025
Certified by: JLL
Title: Senior Project Engineer
Firm: Schlager and Associates, P.A.

BAILEY ROAD AT BAILEY FARMS
TURN LANE IMPROVEMENT PLANS

SE BAILEY ROAD LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION
6/7/2024	per City Comments
10/8/2024	Added Lane Widening Detail for Street Sections 8' or Less
10/24/2024	Reviewed detail for thickened curb.
DATE PREPARED:	
2/2/2024	
PROJ. NUMBER:	
21-136	

CROSS
SECTIONS STA.
107+00 - 107+25