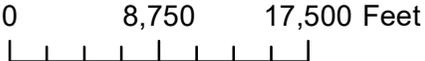


Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community

**Legend LSM122-2**

-  AREA OF INTEREST
-  LEE'S SUMMIT CITY BOUNDARY



INI	BY	DATE	DESCRIPTION
	CN	11/19/2021	PERMIT DRAWING



Lee's Summit, MO  
City Permit Plan View

ALL CONSTRUCTION IS PROPOSED AND SUBJECT TO RELOCATION WHEN OBJECTS ARE ENCOUNTERED THAT WILL INTERFERE WITH THE EXISTING INFRASTRUCTURE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD LOCATE ALL UNDERGROUND OBSTRUCTIONS AND UTILITIES PRIOR TO CONSTRUCTION. COMPLETE REPAIR OF ANY AND ALL DAMAGES INCURRED SHALL BE AT THE EXPENSE OF THE CONTRACTOR.



CREATED BY:  
BRET SIMONS  
EMAIL:  
bret.simons@aeg.cc



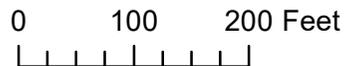
Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community



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EMAIL:  
bret.simons@aeg.cc

**Legend LSM122-2**

 AREA OF INTEREST



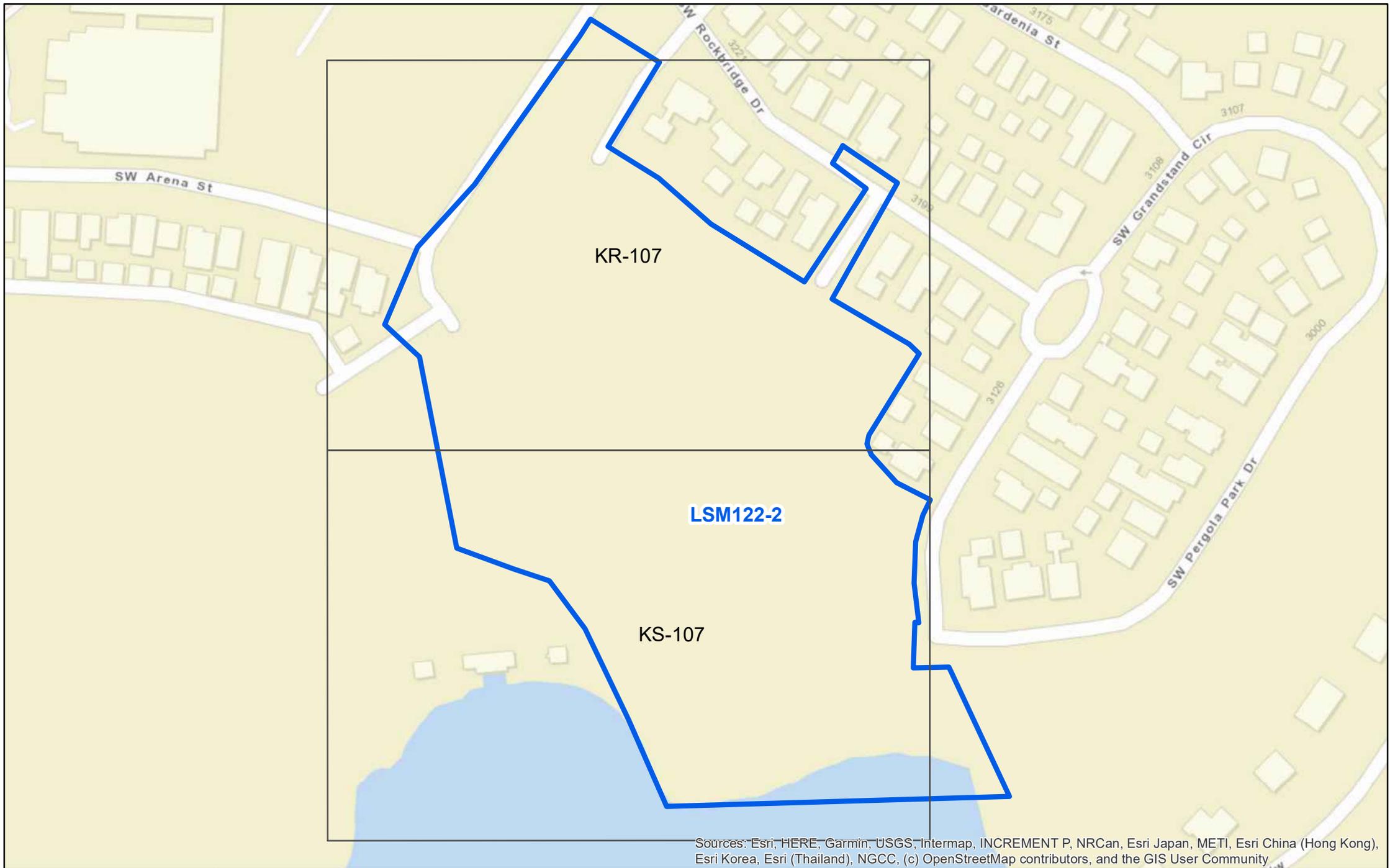
INI	BY	DATE	DESCRIPTION
	CN	11/19/2021	PERMIT DRAWING



Lee's Summit, MO  
City Permit Plan View

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Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community



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BRET SIMONS  
EMAIL:  
bret.simons@aeg.cc

**Legend LSM122-2**

 AREA OF INTEREST

 GRIDS

0 100 200 Feet



INI	BY	DATE	DESCRIPTION
	CN	11/19/2021	PERMIT DRAWING



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City Permit Plan View

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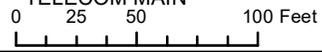




Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

**Legend LSM122-2**

- AREA OF INTEREST
- PROPOSED CAP LOCATION
- PROPOSED VAULT
- - - PROPOSED UG CONDUIT
- SPLICE CLOSURE
- EXISTING UTILITIES**
- ELECTRIC MAIN
- GAS MAIN
- TELECOM MAIN
- SEWER MAIN
- WATER MAIN



INI	BY	DATE	DESCRIPTION
	CN	11/19/2021	PERMIT DRAWING

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BRET SIMONS  
EMAIL:  
bret.simons@aeg.cc



Lee's Summit, MO  
City Permit Plan View

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Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

**Legend LSM122-2**

- AREA OF INTEREST
  - ⊙ PROPOSED CAP LOCATION
  - PROPOSED VAULT
  - - - PROPOSED UG CONDUIT
  - SPLICE CLOSURE
- EXISTING UTILITIES**
- ELECTRIC MAIN
  - GAS MAIN
  - TELECOM MAIN
  - SEWER MAIN
  - WATER MAIN
- 0 25 50 100 Feet



INI	BY	DATE	DESCRIPTION
	CN	11/19/2021	PERMIT DRAWING

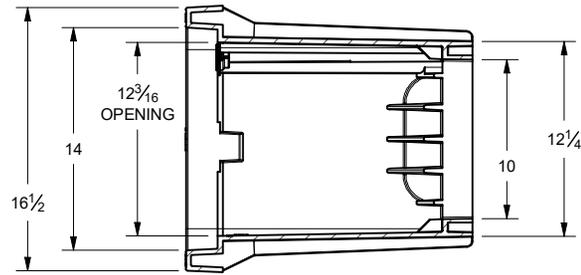
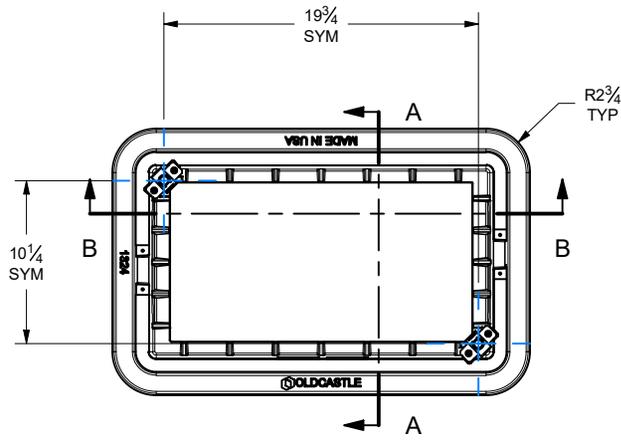
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BRET SIMONS  
EMAIL:  
bret.simons@aeg.cc



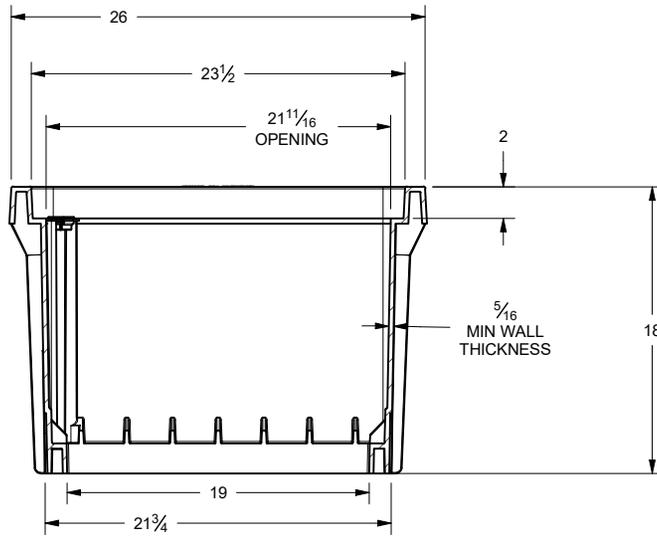
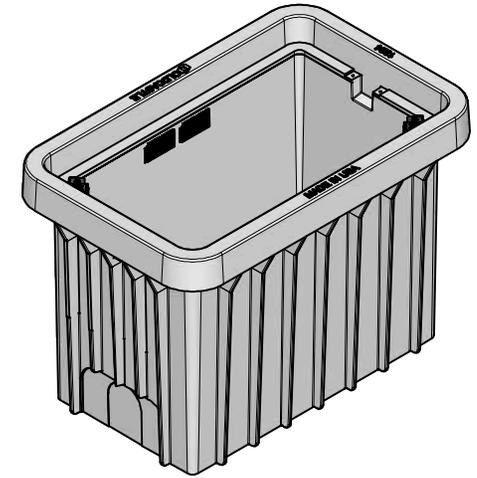
Lee's Summit, MO  
City Permit Plan View

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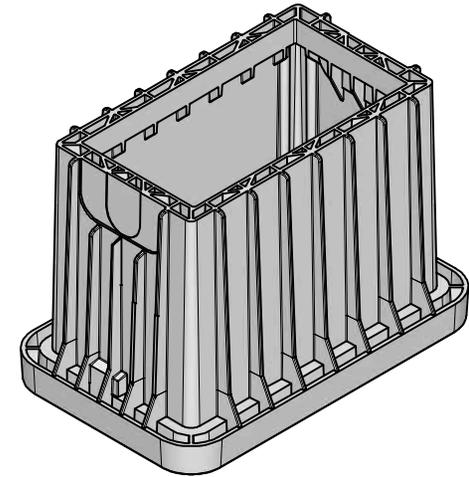




SECTION A-A



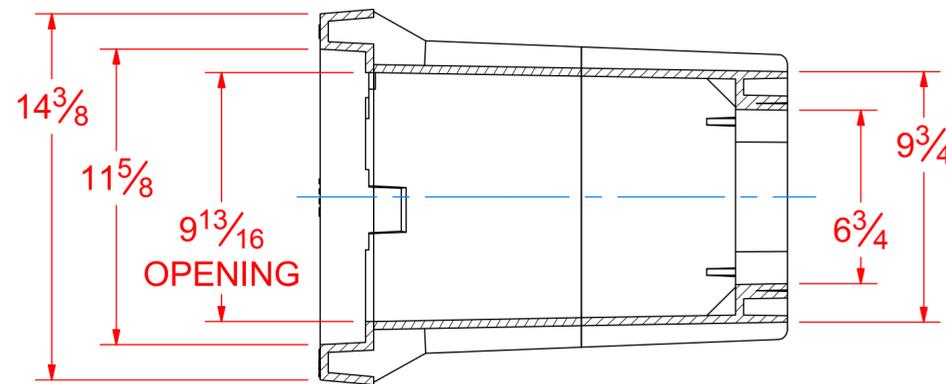
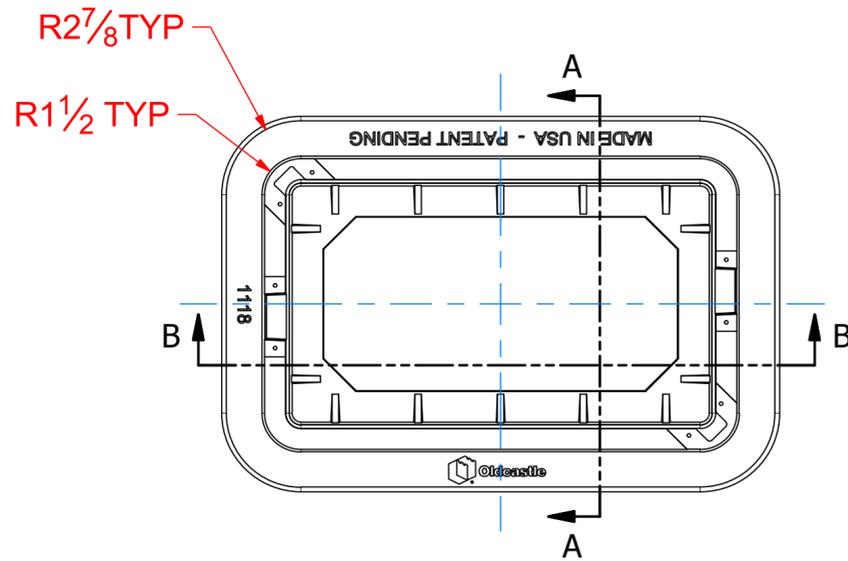
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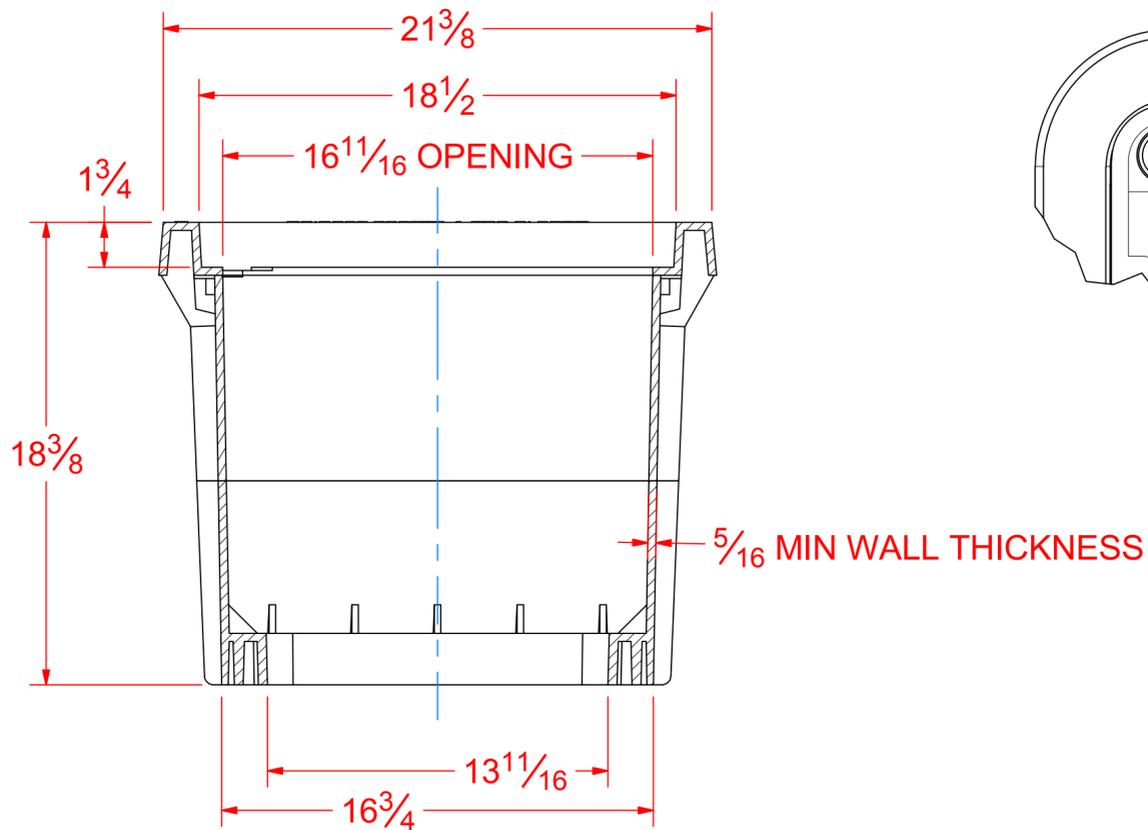
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 EST. LID WEIGHT: 15.79 LBS | EST. BODY WEIGHT: 25.9 LBS

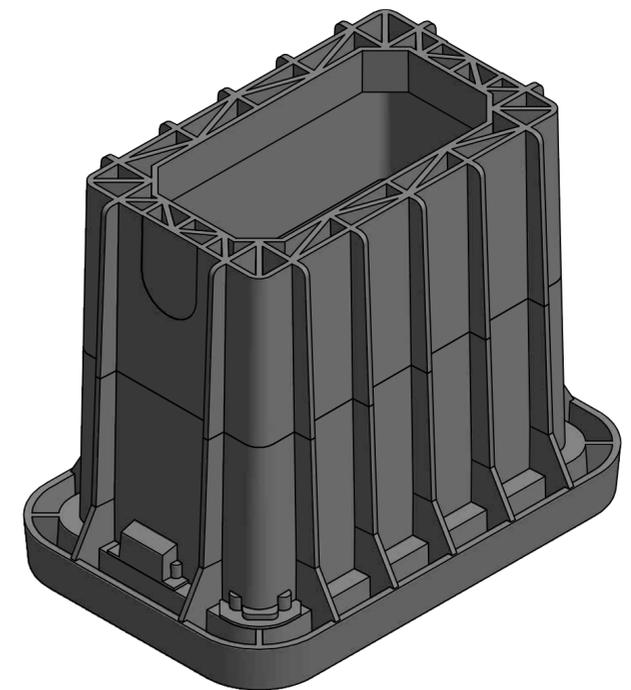
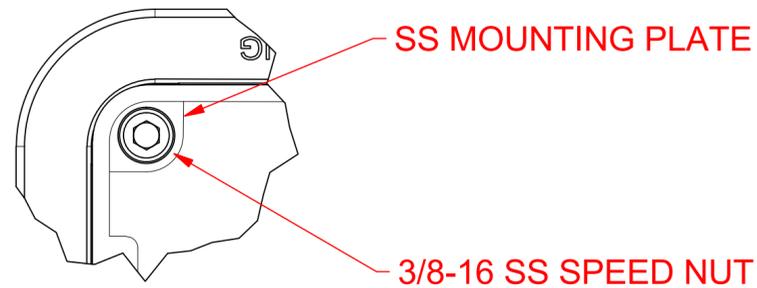
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OES_TREATMENT/COATING	PROJECTION	 <b>Oldcastle Infrastructure</b> <small>A CRH COMPANY</small>												
OES_DRAWING_UNITS														
<b>GENERAL TOLERANCES</b> <table border="0"> <tr> <td>IMPERIAL</td> <td>METRIC</td> <td rowspan="5"> <ul style="list-style-type: none"> <li>ITEMS NOT DIMENSIONED CAN BE MANUFACTURED WITH 3D FILE USING THE FINER GENERAL TOLERANCE</li> <li>DO NOT MEASURE OR CHANGE SCALE</li> <li>REMOVE BURRS AND SHARP EDGES</li> </ul> </td> </tr> <tr> <td>X.XXX = ± .005</td> <td>X.XX = ± 0.10</td> </tr> <tr> <td>X.XX = ± 0.01</td> <td>X.X = ± 0.5</td> </tr> <tr> <td>X/X = ± 1/16</td> <td>X = ± 1</td> </tr> <tr> <td>X° = ± 1°</td> <td>X° = ± 1°</td> </tr> </table>		IMPERIAL	METRIC	<ul style="list-style-type: none"> <li>ITEMS NOT DIMENSIONED CAN BE MANUFACTURED WITH 3D FILE USING THE FINER GENERAL TOLERANCE</li> <li>DO NOT MEASURE OR CHANGE SCALE</li> <li>REMOVE BURRS AND SHARP EDGES</li> </ul>	X.XXX = ± .005	X.XX = ± 0.10	X.XX = ± 0.01	X.X = ± 0.5	X/X = ± 1/16	X = ± 1	X° = ± 1°	X° = ± 1°	<b>OES_DESCRIPTION</b> <b>1324-18 DL/LW UNIT, FLUSH COVER T15 GRAY, 2X 1/2-6 HEX AUGER CPBLT, 'GFBR'</b>	
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X.XXX = ± .005	X.XX = ± 0.10													
X.XX = ± 0.01	X.X = ± 0.5													
X/X = ± 1/16	X = ± 1													
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AUTHOR K LEITENBERGER	CHECKED BY E LONG	SHEET SIZE A	OES_SKU 13246018											
11/20/2020	11/20/2020	SCALE: 1/12	FILE NAME: 0033599.idw											
		REVISION NUMBER												
		SHEET 2 OF 3												



SECTION A-A



SECTION B-B



MATERIAL: BLENDED POLYPROPYLENE, GRAY

SKU SHOWN: 11182580

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**Oldcastle** Enclosure Solutions  
 1675 INDUSTRIAL DR.  
 NAPOLEON, OH 43545  
 419-592-2309

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CHECKED	E LONG	12/22/10
QA		
MFG		
APPROVED		

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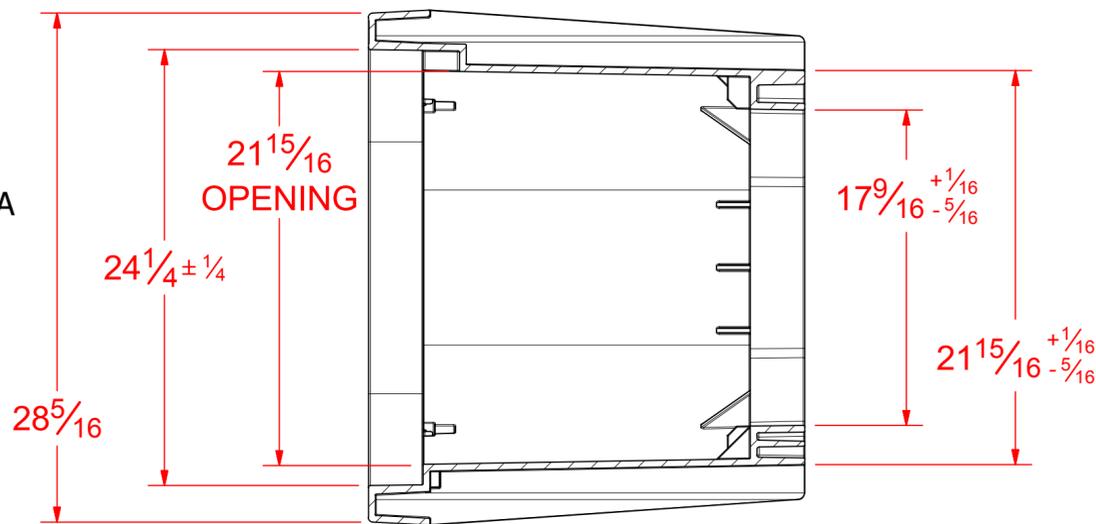
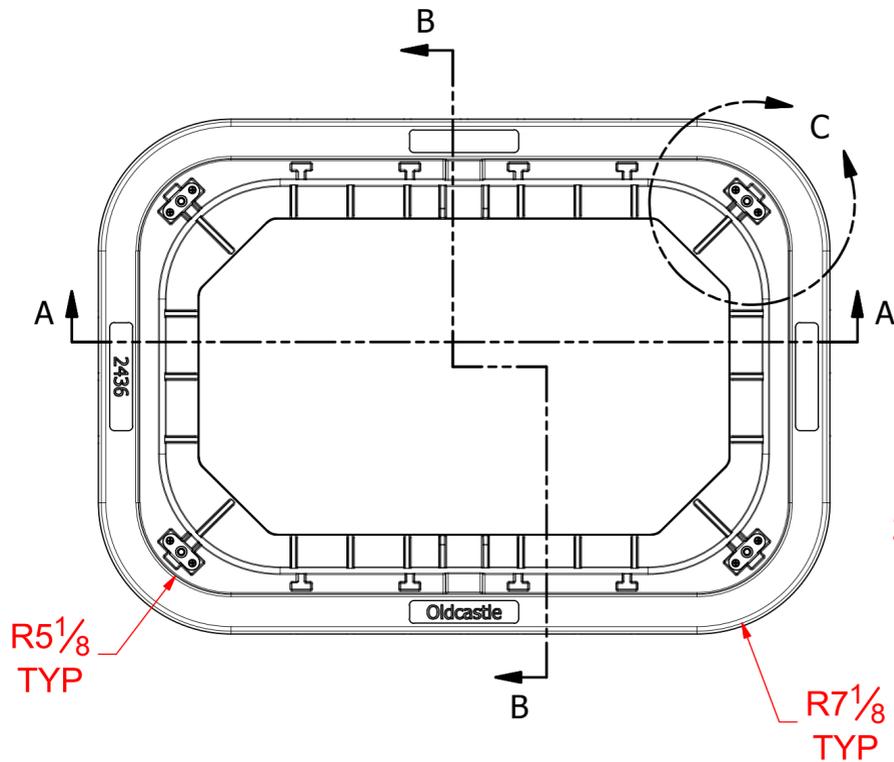
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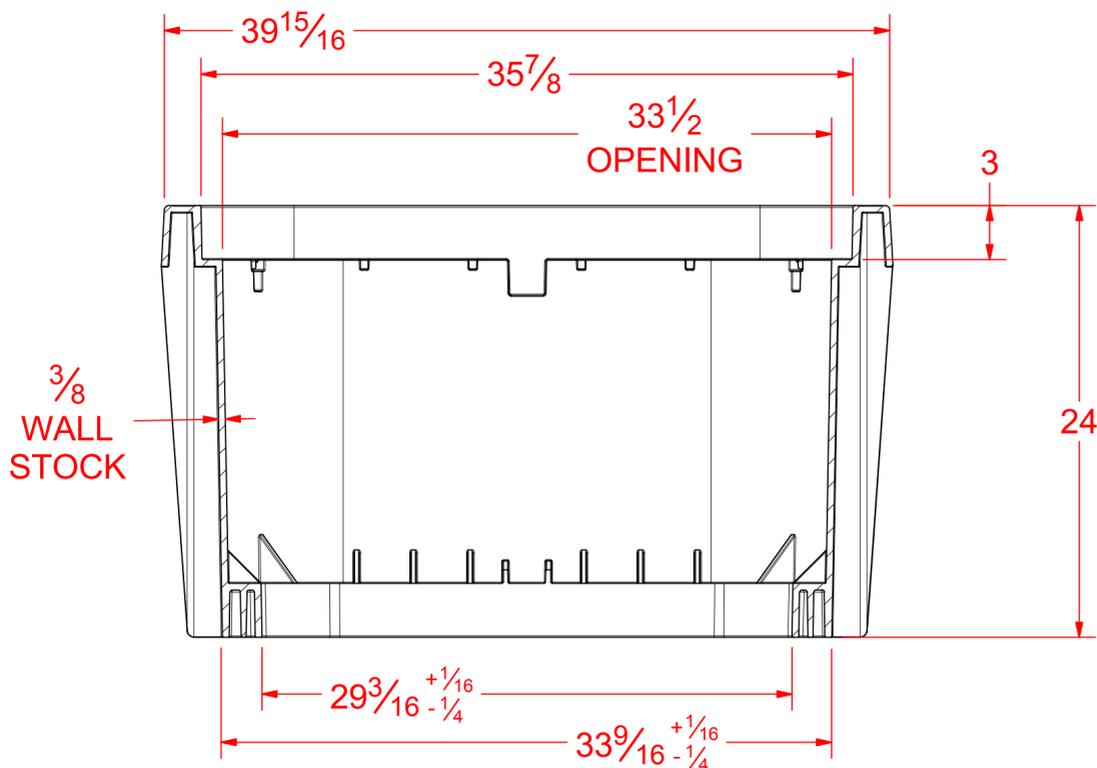
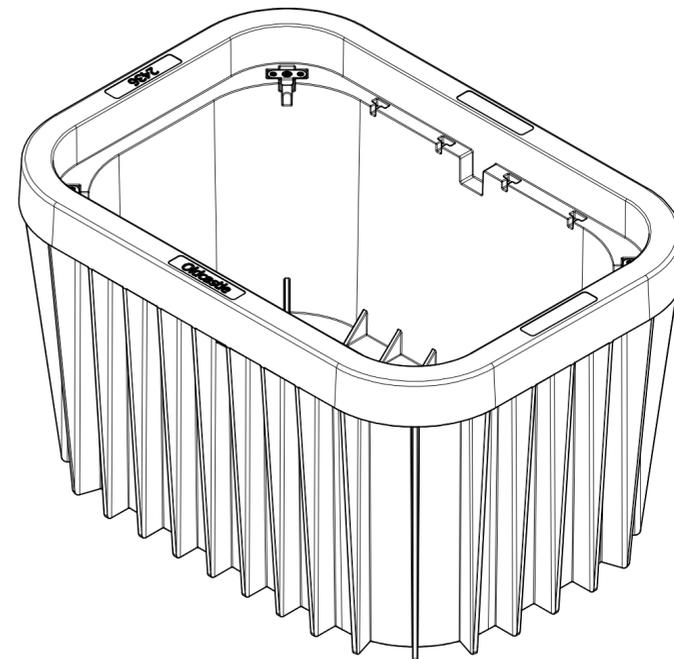
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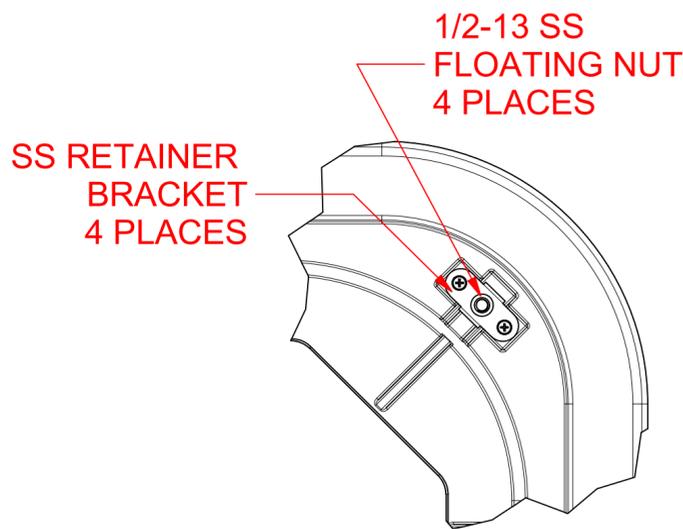
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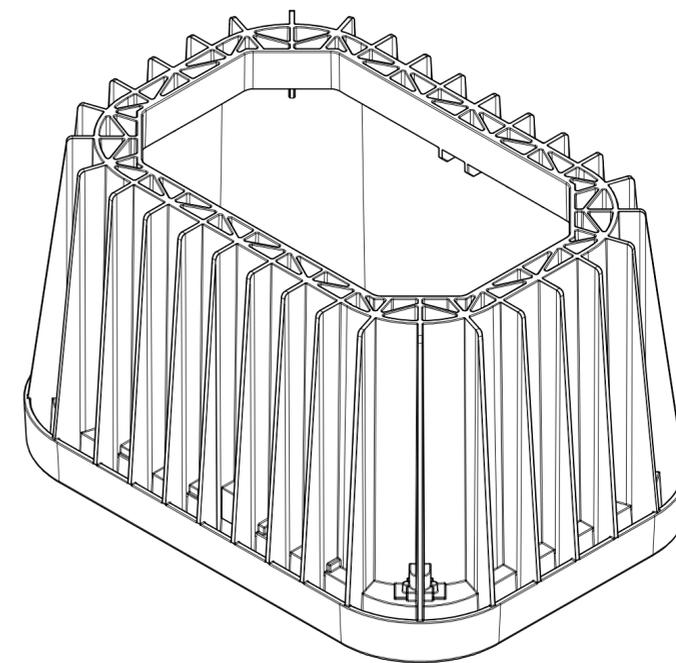
SECTION B-B



SECTION A-A



DETAIL C



MATERIAL: GLASS FILLED POLYPROPYLENE, BLACK

SKU SHOWN: 24362070

**CUSTOMER FORMAT DRAWING**

BODY EST. WEIGHT: 70 LBS

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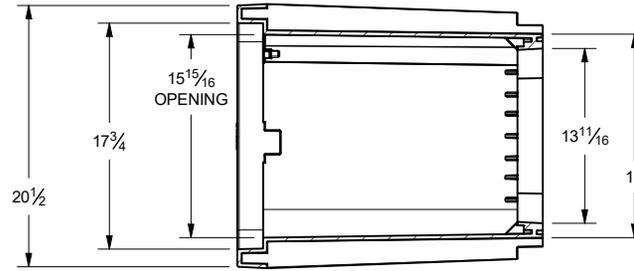
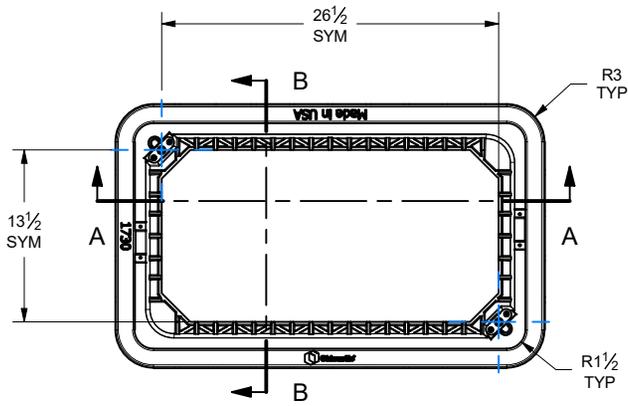
**Oldcastle® Enclosure Solutions**

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NAPOLEON, OH 43545  
419-592-2309

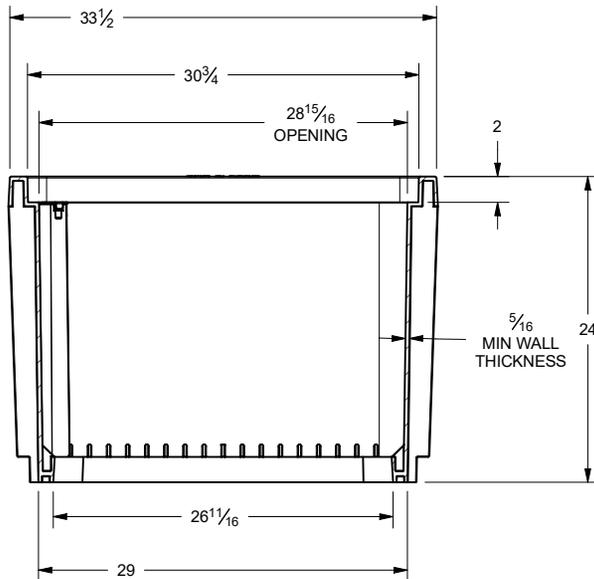
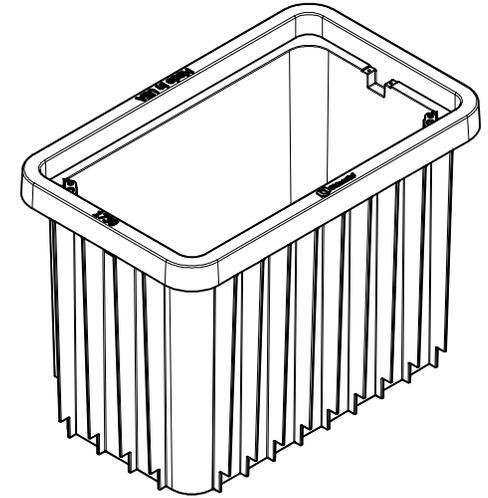
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CHECKED	E LONG	4/29/2016
QA	D LITTLE	8/18/2016
MFG	P MASTERLASO	8/16/2016
APPROVED	T DANG	10/20/2016

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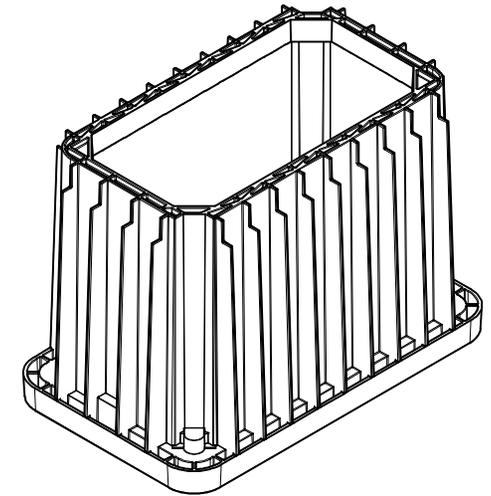
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.XX = ±.010  
.X = ±.030  
FRACTION = ±1/16  
ANGLE = ±1°



SECTION B-B



SECTION A-A



MATERIAL: BLENDED POLYPROPYLENE

# CUSTOMER FORMAT DRAWING

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 EST. LID WEIGHT: 20.87 LBS | EST. BODY WEIGHT: 41.3 LBS

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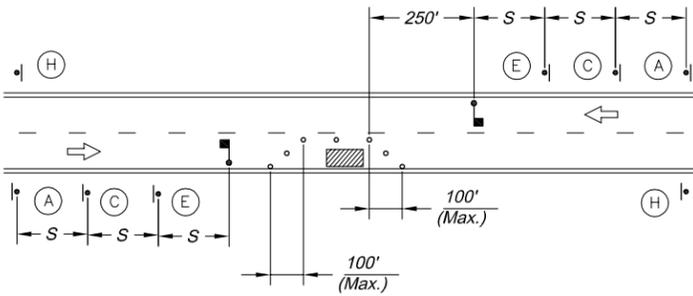
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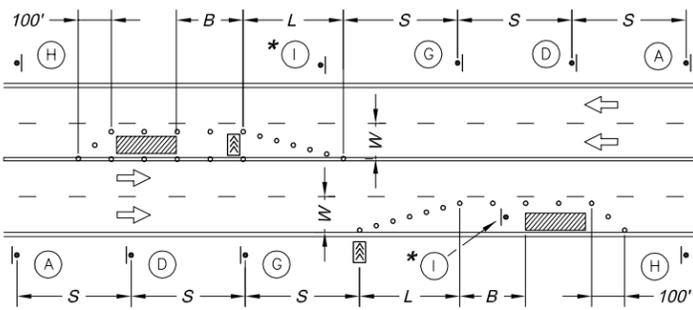
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CHECKED BY E LONG	4/30/2020	SCALE: 1/15	FILE NAME: 0030984.idw	SHEET 5 OF 6

### SYMBOL LEGEND

- Work Area
- Channelizer
- Sign
- Arrow Panel
- Barricade
- Flagger
- Direction of Travel

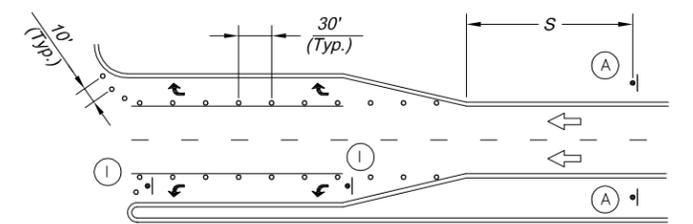


LANE CLOSURE - TWO LANE STREET



LANE CLOSURE - FOUR LANE STREET

\* Install Signs Every 200 Feet Throughout the Closed Lane or As Needed



TURN LANE CLOSURE

Sign Spacing "S"	
Speed Limit (mph)	Spacing (Feet)
25	100
30 - 35	250
≥ 40	350

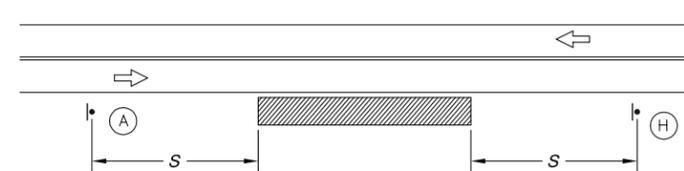
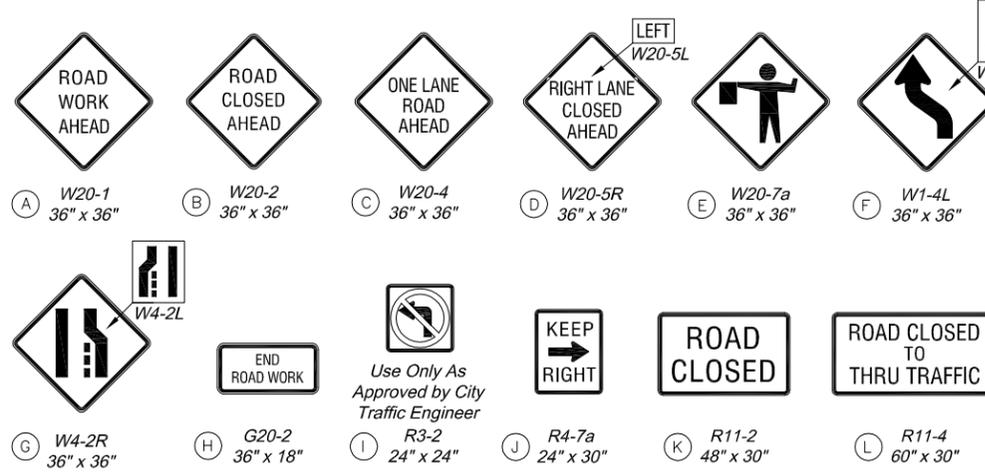
  

Taper Dimensions (Feet)				
Speed Limit (mph)	Minimum Taper Length "L", per Lane Width "W"			Minimum Number of Channelizers
	10	11	12	
25	105	115	125	6
30	150	165	180	7
35	205	225	245	8
40	270	295	320	9
45	450	495	540	13

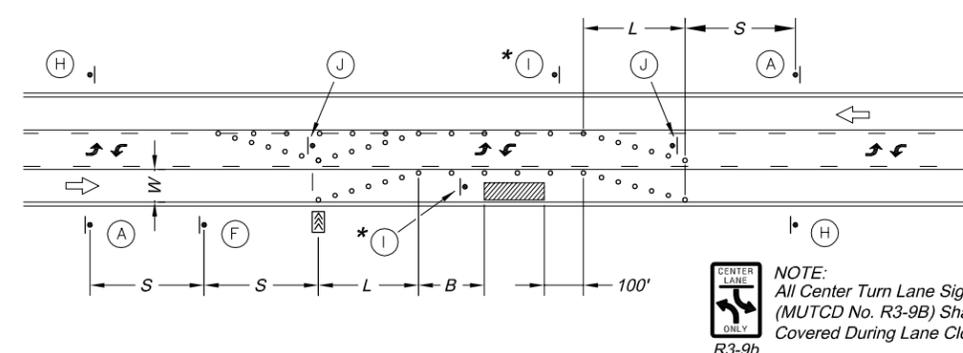
Guidelines for Length of Longitudinal Buffer Space "B"	
Speed Limit (mph)	Length (Feet)
25	35
30	55
35	85
40	120
45	170

Maximum Channelizer Spacing		
Speed Limit (mph)	Within Taper (Feet)	
	Inside	Outside
25	25	50
30	30	60
35	35	70
40	40	80
45	45	90

### SIGN LEGEND

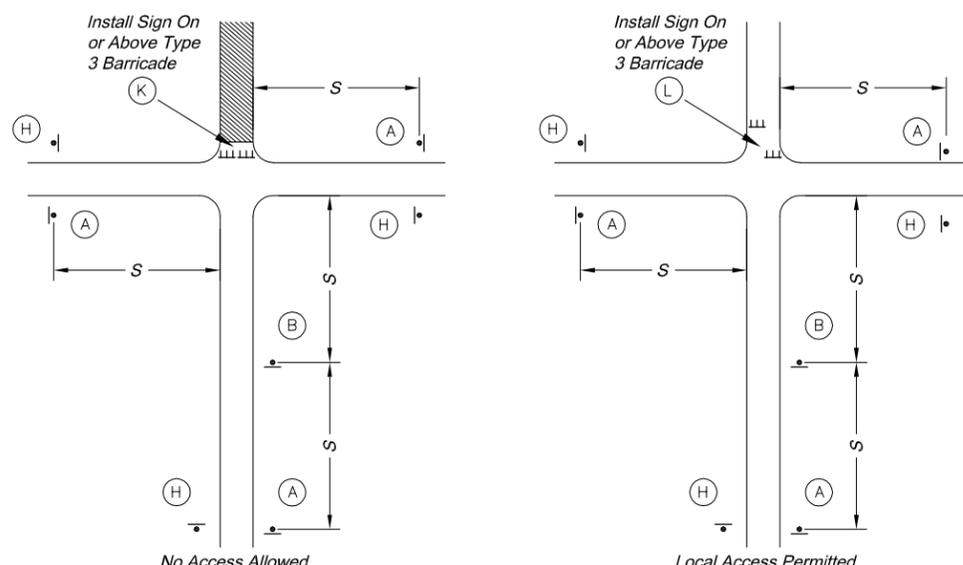


TYPICAL SIGNING FOR WORK ADJACENT TO THE STREET



LANE CLOSURE - THREE LANE STREET

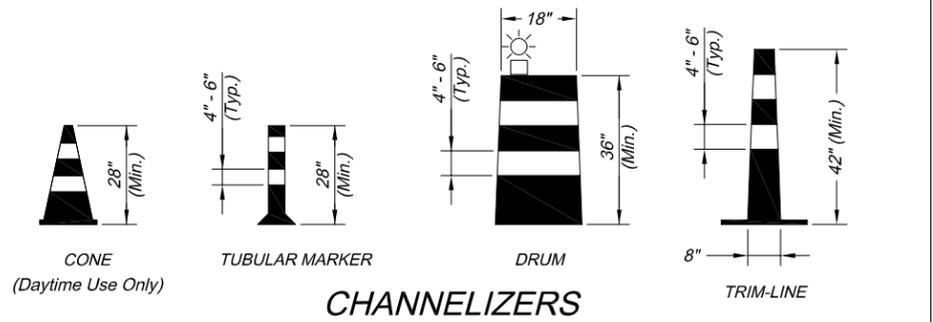
\* Install Signs Every 200 Feet Throughout the Closed Lane or As Needed



TYPICAL STREET CLOSURE

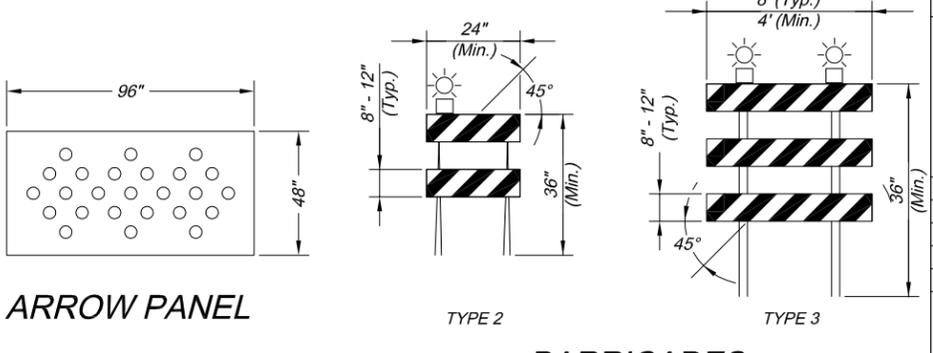
### GENERAL NOTES:

- All signs, barricades, channelizers, markings and other traffic control devices shall conform to the latest edition of the Manual on Uniform Traffic Control Devices (MUTCD).
- All traffic control devices shall be standard in size, shape, color, and message, in good condition, and retro-reflectORIZED. All signs shall be securely mounted with height and lateral location as described in the MUTCD.
- Warning lights shall be used on barricades in place at night and on warning signs which alert drivers about a change in alignment, traffic control, lane closure, or road closure.
- Flaggers shall be used where indicated on the plans, where construction vehicles interact with normal traffic, or where construction activities impose a restriction on traffic, as directed by the City Traffic Engineer. Where flaggers are used, advance signing shall be erected as shown in the details or as specified in the MUTCD. Flaggers shall meet the requirements in the MUTCD in regard to character, training, attire, and behavior.
- Trim-lines are the City's preferred channelizing device. Cones may not be used at nighttime.
- Traffic control devices not in use or not applicable shall be either covered or removed from the work area.
- The Contractor shall use barricades, street plates, or fencing as needed to effectively shield pedestrian and vehicular traffic from exposed objects, excavations, and construction activities.
- Access shall be maintained to all driveways and side streets unless noted otherwise on the plans.
- No street shall be closed without the approval of the City Traffic Engineer. The Contractor shall notify the City Traffic Engineer at least 7 days in advance of any street closure. If a detour route around the closure is to be provided, all detour signing shall be as shown on a plan approved by the City Traffic Engineer.
- Construction vehicles parked along streets shall be located within the work area (traffic control) or where otherwise normally permitted. Construction materials, including traffic control and vehicles shall not restrict sight distance for vehicles exiting at streets or drives.
- Construction materials shall be kept off of sidewalks, consolidated in one location within City right-of-way, and removed daily unless otherwise approved by the Inspector. Dirt, mud, and other construction debris on streets and sidewalks shall be removed immediately.
- The Contractor shall not perform any work that will restrict vehicular traffic in any way between the hours of 7:00 a.m. and 9:00 a.m. or 4:00 p.m. and 6:00 p.m. Monday through Friday unless otherwise indicated in the specifications.
- All travel lanes should be at least 11 feet wide unless otherwise authorized by the City Traffic Engineer. A "Narrow Lanes" sign shall be installed in advance of a lane width reduction to less than 11 feet.
- All edge drop-offs of more than 2 inches and less than 4 inches should be protected by a wedge or barrier and all edge drop-offs greater than 4 inches shall have edge protection (see Traffic Control Specifications for edge treatment requirements).
- The "Workers" symbolic sign (MUTCD No. W21-1a) may be used instead of the "Road Work Ahead" sign for work with a duration of 12 hours or less. The "End Road Work" sign is not required to be installed after the "Workers" sign.
- No traffic signal shall be altered or modified in any way without a plan approved by the City Traffic Engineer.
- The Contractor shall be responsible for maintaining all traffic control devices on an around-the-clock basis, whether or not work is actively being pursued and any deficiencies noted shall be corrected immediately.
- The traffic control requirements shown on these plans are minimum requirements only and do not attempt to address in depth the variety of situations that may occur once construction has started. In no way do the requirements shown on these plans relieve the Contractor of his responsibility for selecting the proper traffic control devices and implementation procedures that will assure the safety of drivers, pedestrians, and workers at all times.
- Should the contractor fail to enforce the traffic control plan or fail to clean, replace or otherwise maintain the traffic control devices when directed to do so by the City Traffic Engineer or representative, the City may take one or more of the following actions:
  - Employ another agency to correct deficiencies in traffic control devices and deduct the cost from the Contractor's pay estimate,
  - Stop the work until deficiencies are corrected,
  - Suspend all pay estimates until deficiencies are corrected, or
  - Place the Contractor in default.



CHANNELIZERS

NOTE: White Bands On Barricades and Channelizers Shall Be Made From High Intensity Sheeting Material.



ARROW PANEL

TYPE 2

TYPE 3

BARRICADES

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Project: **TRAFFIC CONTROL DETAILS**  
Sheet Name: **STANDARD DRAWING TC-1**

Drawn By: JJW  
Checked By: MP  
Date: 12-10-2008  
Project No.: X

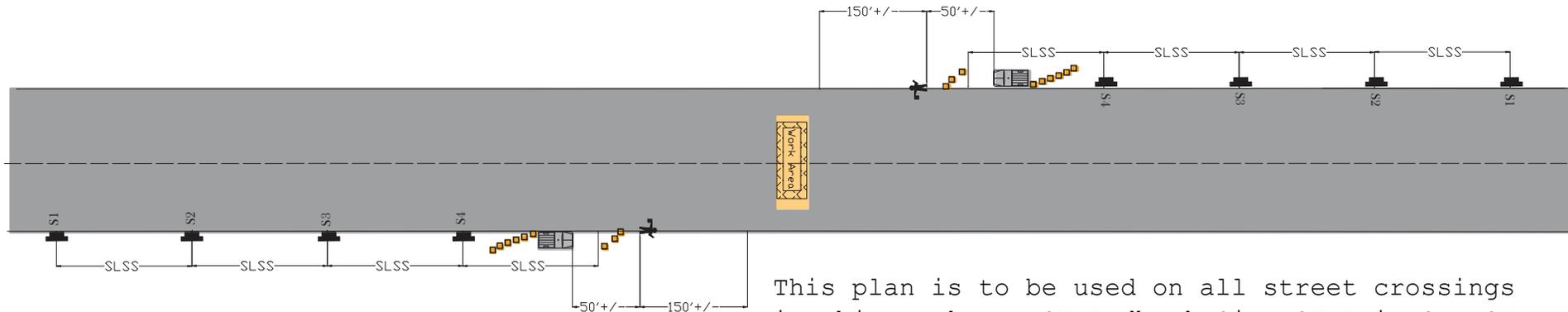


# TRAFFIC CONTROL FOR UNDERGROUND INSTALLATION ACROSS ROADS

S1	UTILITY WORK AHEAD (W21-7)
S2	MEN WORKING
S3	BE PREPARED TO STOP (W20-7B)
S4	FLAGGER AHEAD (W20-7A)
	FLAGGER/SPOTTER STATION
	PROTECTIVE VEHICLE

- NOTES:**
- DAYLIGHT USE ONLY
  - FLAGGER COMMUNICATION SHALL BE REINFORCED WITH RADIOS
  - 24" FLAGGING PADDLES REQUIRED ON THE HIGHWAY
  - FOR USE ON WET OR DRY PAVEMENT ONLY
  - NOT FOR USE IN FOGGY CONDITIONS
  - GOOGLE FIBER / ATLANTIC ENGINEERING GROUP TO PULL NEW FIBER OPTIC CABLE THROUGH CONDUIT
  - CABLE WILL BE PULLED THROUGH EXISTING CONDUIT OR NEW CONDUIT WILL BE INSTALLED. TRAFFIC WILL BE STOPPED ONLY AS NEEDED.
  - FLASHERS TO BE USED ON PROTECTIVE VEHICLE
  - TO INSTALL UNDERGROUND CONDUIT AND FIBER USE SPOTTERS AND SIGNS TO BE PLACED ON RESPECTIVE SIDES OF ROADS
  - TRAFFIC DELAYS TO BE KEPT TO A MINIMUM, +/- 3 MINUTES MAX

SLSS	
SPEED LIMIT	SIGN SPACING
60/70 MPH	1000' +/-
50/55 MPH	500' +/-
40/45 MPH	350' +/-
0/35 MPH	200' +/-



This plan is to be used on all street crossings in this package. SW Redbuck Cir, SW Amie St, SW Mary St, SW Gale Dr, SW Pergola Park Dr, SW Rockbridge Dr.  
See attached maps.



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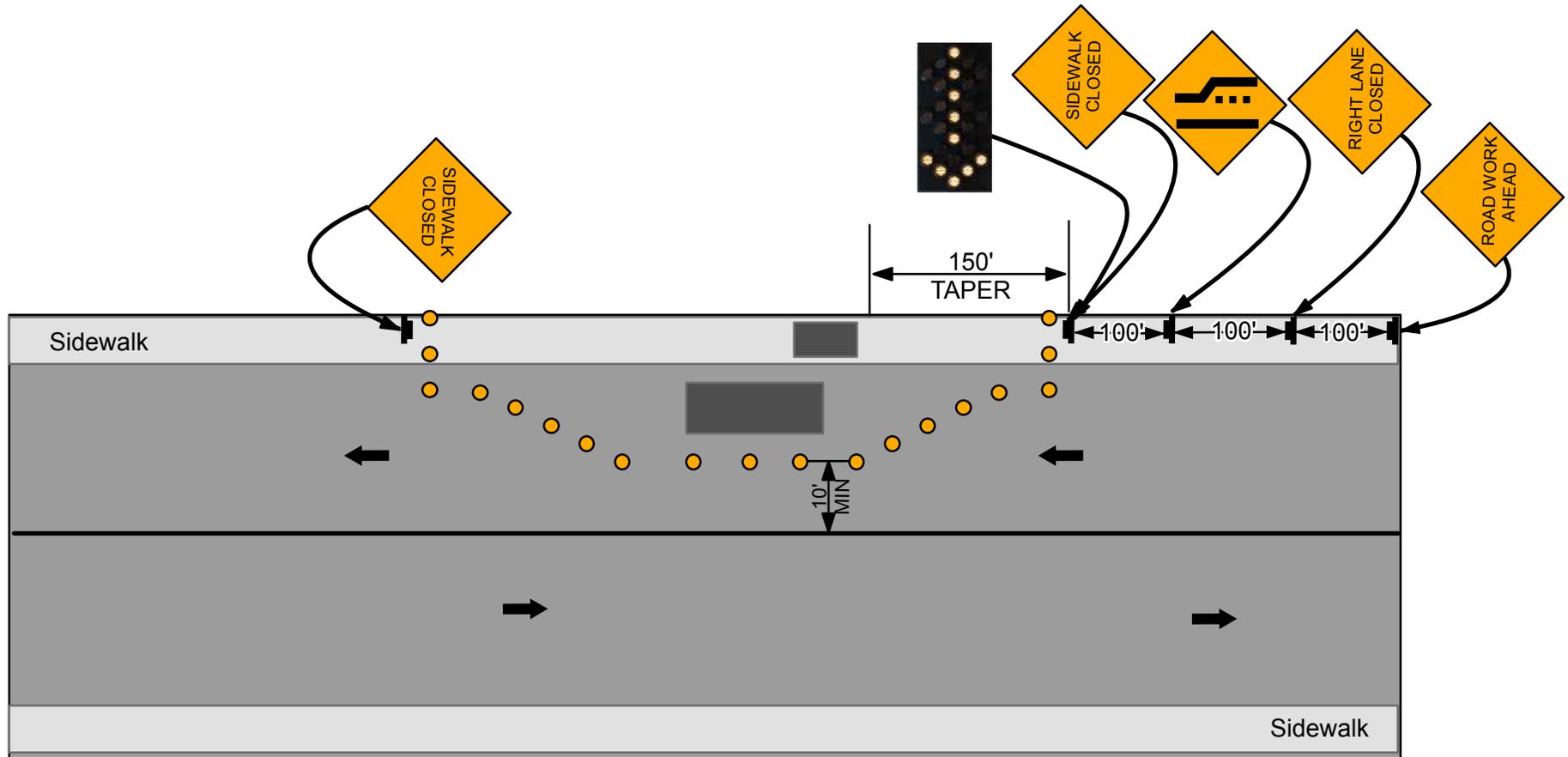
### Legend

-  CONE PLACEMENT
-  SIGNAGE PLACEMENT

SIGNS AND CONES  
MOVE AS WORK  
PROGRESSES



# TRAFFIC CONTROL ALONG ROADS WITH SIDEWALKS



This plan is to be used on all 2 lane roads with sidewalks in this package. SW Redbuck Cir, SW Amie St, SW Mary St, SW Gale Dr, SW Pergola Park Dr, SW Rockbridge Dr.  
See attached maps.



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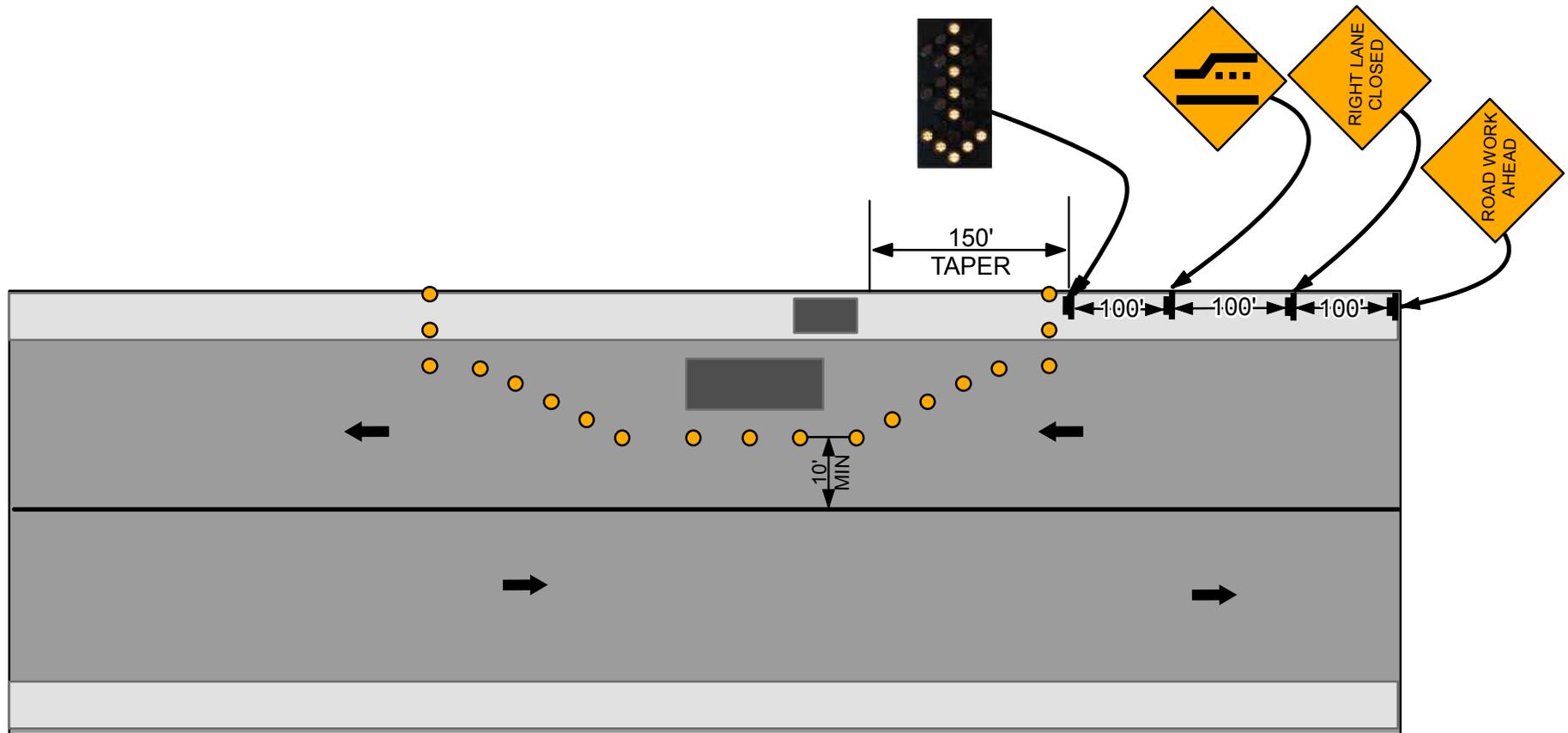
## Legend

-  CONSTRUCTION VEHICLE
-  CONE PLACEMENT
-  SIGNAGE PLACEMENT

SIGNS AND CONES  
MOVE AS WORK  
PROGRESSES



# TRAFFIC CONTROL ALONG ROADS



This plan is to be used on all 2 lane roads in this package. SW Redbuck Cir, SW Amie St, SW Mary St, SW Gale Dr, SW Pergola Park Dr, SW Rockbridge Dr. See attached maps.

## Legend

-  CONSTRUCTION VEHICLE
-  CONE PLACEMENT
-  SIGNAGE PLACEMENT

SIGNS AND CONES  
MOVE AS WORK  
PROGRESSES



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