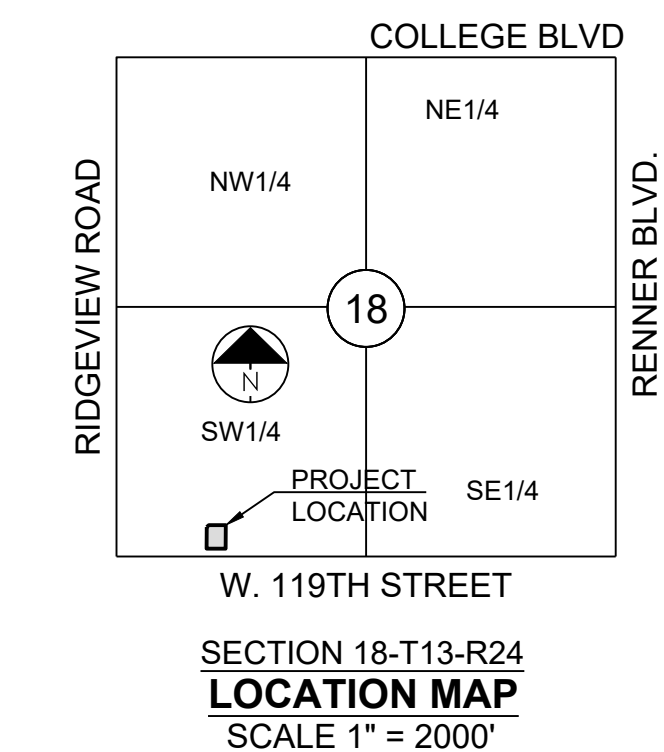
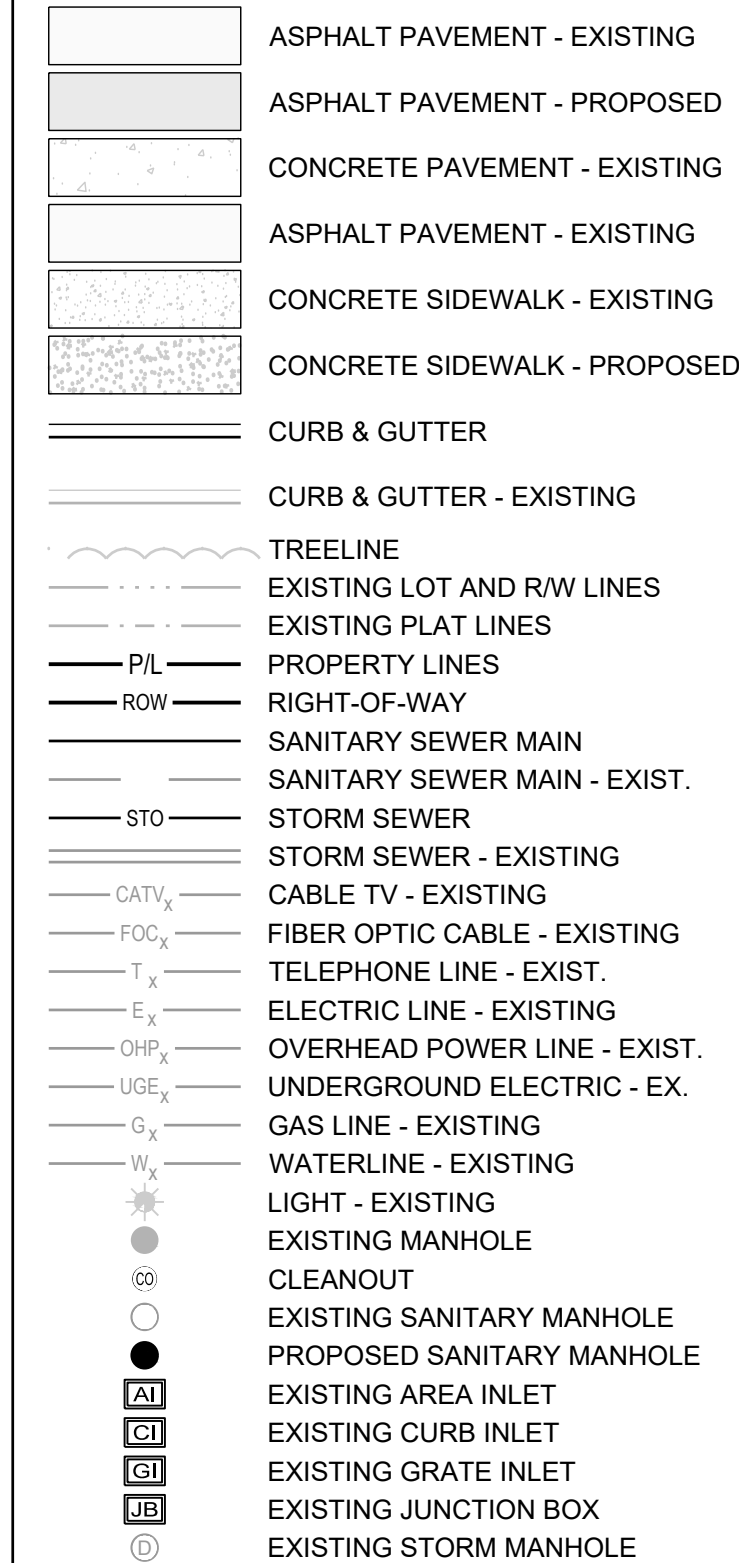


A/E	- ACCESS EASEMENT
BC	- BACK OF CURB
B/B	- BACK TO BACK
BM	- BENCHMARK
BL or B.L.	- BUILDING LINE
CO	- CLEANOUT
TJB	- TELEPHONE JUNCTION BOX
C&G	- CURB AND GUTTER
D/E	- DRAINAGE EASEMENT
E/E	- ELECTRICAL EASEMENT
EL	- ELEVATION
FL	- FLOW LINE
G/E	- GAS LINE EASEMENT
HDPE	- HIGH-DENSITY POLYETHYLENE
L/E	- LANDSCAPE EASEMENT
MSFE	- MINIMUM SERVICEABLE FLOOR ELEVATION
PVC	- POLYVINYL CHLORIDE
P/L	- PROPERTY LINE
PUB/E	- PUBLIC EASEMENT
RCP	- REINFORCED CONCRETE PIPE
ROW or R/W	- RIGHT-OF-WAY
S/E	- SANITARY SEWER EASEMENT
SL	- SERVICE LINE
S/W	- SIDEWALK
TE	- TOP ELEVATION
U/E	- UTILITY EASEMENT
WSE	- WATER SURFACE ELEVATION
W/E	- WATERLINE EASEMENT

MISSOURI DEPARTMENT OF
TRANSPORTATION (MODOT)

MISSOURI GAS ENERGY (MGE)
Brent Jones
3025 SE Clover Drive
Lee's Summit, MO 64082
(816) 399-9633
brent.jones@spireenergy.com

EVERGY (formerly KCP&L)
Ron DeJarnette
1300 SE Hamblin Road
Lee's Summit, MO 64081
Office: (816) 347-4316
Cell: (816) 810-5234
ron.dejarnette@evergy.com

Michael Park
220 SE Green Street
Lee's Summit, MO 64063
(816) 969-1800
michael.park@cityofs.net

Mark Manion or Marty Loper
500 E. 8th Street, Room 370
Kansas City, MO 64106
(816) 275-2341 or (816) 275-1550

PUBLIC WATER S
Mark Schaufler
220 SE Green Street
Lee's Summit, MO 64063
(816) 969-1900

1. ALL CONSTRUCTION TO FOLLOW THE CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL AS ADOPTED BY ORDINANCE 5813.
2. ALL WORKMANSHIP AND MATERIALS SHALL BE SUBJECT TO THE INSPECTION AND APPROVAL OF THE ENGINEERING DEPARTMENT OF THE CITY OF LEE'S SUMMIT, MISSOURI.
3. LINEAL FOOT MEASUREMENTS SHOWN ON THE PLANS ARE HORIZONTAL MEASUREMENTS, NOT SLOPE MEASUREMENTS. ALL PAYMENTS SHALL BE MADE ON HORIZONTAL MEASUREMENTS.
4. NO GEOLOGICAL INVESTIGATION HAS BEEN PERFORMED ON THE SITE.
5. THE UTILITY LOCATIONS SHOWN ON THESE PLANS ARE TAKEN FROM UTILITY COMPANY RECORDS AND APPARENT FIELD LOCATIONS. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION.
6. THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS OF THE SENATE BILL NUMBER 583, 78TH GENERAL ASSEMBLY OF THE STATE OF MISSOURI. THE BILL REQUIRES THAT ANY PERSON OR FIRM DOING EXCAVATION ON PUBLIC RIGHT OF WAY DO SO ONLY AFTER GIVING NOTICE TO, AND OBTAINING INFORMATION FROM, UTILITY COMPANIES. STATE LAW REQUIRES 48 HOURS ADVANCE NOTICE. THE CONTRACTOR MAY ALSO UTILIZE THE FOLLOWING TOLL FREE PHONE NUMBER PROVIDED BY "MISSOURI ONE CALL SYSTEM, INC.": 1-800-DIG-RITE. THIS PHONE NUMBER IS APPLICABLE ANYWHERE WITHIN THE STATE OF MISSOURI. PRIOR TO COMMENCEMENT OF WORK, THE CONTRACTOR SHALL NOTIFY ALL THOSE COMPANIES WHICH HAVE FACILITIES IN THE NEAR VICINITY OF THE CONSTRUCTION TO BE PERFORMED.
7. PRIOR TO ORDERING PRECAST STRUCTURES, SHOP DRAWING SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR APPROVAL.
8. THE CONTRACTOR SHALL PROTECT ALL MAJOR TREES FROM DAMAGE. NO TREE SHALL BE REMOVED WITHOUT THE PERMISSION OF THE OWNER, UNLESS SHOWN OTHERWISE.
9. CLEARING AND GRUBBING OPERATIONS AND DISPOSAL OF ALL DEBRIS THEREFROM SHALL BE PERFORMED BY THE CONTRACTOR IN STRICT ACCORDANCE WITH ALL LOCAL CODES AND ORDINANCES.
10. ALL WASTE MATERIAL RESULTING FROM THE PROJECT SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR, OR AS DIRECTED BY THE OWNER.
11. ALL EXCAVATIONS SHALL BE UNCLASSIFIED. NO SEPARATE PAYMENT WILL BE MADE FOR ROCK EXCAVATION.
12. THE CONTRACTOR SHALL CONTROL THE EROSION AND SILTATION DURING ALL PHASED OF CONSTRUCTION, AND SHALL KEEP THE STREETS CLEAN OF MUD AND DEBRIS.
13. ALL MANHOLES, CATCH BASINS, UTILITY VALVES AND METER PITS TO BE ADJUSTED OR REBUILT TO GRADE AS REQUIRED.
14. SUBGRADE SOIL FOR ALL CONCRETE STRUCTURES, REGARDLESS OF THE TYPE OR LOCATION, SHALL BE FIRM, DENSE AND THOROUGHLY COMPACTED AND CONSOLIDATED; SHALL BE FREE FROM MUCK AND MUD; SHALL BE SUFFICIENTLY STABLE TO REMAIN FIRM AND INTACT UNDER THE FEET OF THE WORKMEN OR MACHINERY ENGAGED IN SUBGRADE SURFACING, LAYING REINFORCING STEEL, AND DEPOSITING CONCRETE THEREON. IN ALL CASES WHERE SUBSOIL IS MUCKY OR WORKS INTO MUD OR MUCK DURING SUCH OPERATIONS, A SEAL COURSE OF EITHER CONCRETE OR ROCK SHALL BE PLACED BELOW SUBGRADE TO PROVIDE A FIRM BASE FOR WORKING AND FOR PLACING THE FLOOR SLAB.
15. THE CONTRACTOR SHALL CONTACT PUBLIC WORKS INSPECTIONS AT: 816-969-1800 TO OBTAIN A PUBLIC WORKS CONSTRUCTION PERMIT. A MINIMUM 48 HOUR NOTICE SHALL BE GIVEN PRIOR TO PERMIT ISSUANCE.
16. THE CONTRACTOR SHALL CONTACT THE CITY'S EROSION CONTROL SPECIALIST AT: 816-969-1800 PRIOR TO ANY LAND DISTURBANCE.
17. THE CONTRACTOR SHALL CONTACT THE RIGHT OF WAY INSPECTOR AT 816-969-1800 PRIOR TO ANY LAND DISTURBANCE ACTIVITIES WITHIN THE RIGHT OF WAY. THESE ACTIVITIES MAY REQUIRE A PERMIT.
18. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL TRAFFIC HANDLING MEASURES NECESSARY TO ENSURE THE CITY'S TRAFFIC CONTROL AT ALL TIMES. TRAFFIC CONTROL SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD-LATEST EDITION).
19. ALL SANITARY SEWER LATERALS SHALL HAVE A TRENCH CHECK, CONSISTING OF FLOWABLE BACKFILL, INSTALLED DURING CONSTRUCTION. TRENCH CHECK SHALL EXTEND TO BOTTOM OF TRENCH, TO WIDTH OF TRENCH, TO 12 INCHES ABOVE PIPE, FOR A MINIMUM LENGTH OF 12 INCHES. TRENCH CHECK SHALL BE LOCATED AT LEAST 5 FEET FROM SANITARY MAIN.

1. REFER TO GEOTECHNICAL REPORT FOR ALL COMPACTION REQUIREMENTS AND ASPHALT AND CONCRETE RECOMMENDED THICKNESS AND SUBGRADE TREATMENTS.
2. RECOMMEND A GEOTECHNICAL ENGINEER REVIEW ALL EARTHWORK ACTIVITY TO MAKE SURE RECOMMENDATIONS IN GEOTECHNICAL REPORT ARE FOLLOWED.
3. PRIOR TO PLACEMENT OF PAVEMENT, GEOTECHNICAL ENGINEER MUST APPROVE SUBGRADE IN WRITTEN FORM TO THE OWNER AND PROJECT ENGINEER.
4. ALL UTILITY INSTALLATIONS UNDER PAVED AREAS MUST BE COMPACTED AS PER THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER AND THE GEOTECHNICAL REPORT.
5. ALL CONSTRUCTION SHALL COMPLY WITH THE CITY OF CITY OF LEE'S SUMMIT DESIGN AND CONSTRUCTION MANUAL.

1. IT IS RECOMMENDED THAT A GEOTECHNICAL ENGINEER OBSERVE AND DOCUMENT ALL EARTHWORK ACTIVITIES.
2. CONTOURS HAVE BEEN SHOWN AT 1-FOOT OR 2-FOOT INTERVALS, AS INDICATED. GRADING SHALL CONSIST OF COMPLETING THE EARTHWORK REQUIRED TO BRING THE PHYSICAL GROUND ELEVATIONS OF THE EXISTING SITE TO THE FINISHED GRADE (OR SUB-GRADE) ELEVATIONS PROVIDED ON THE PLANS AS SPOT GRADES, CONTOURS OR OTHERS MEANS AS INDICATED ON THE PLANS.
3. THE EXISTING SITE TOPOGRAPHY DEPICTED ON THE PLANS BY CONTOURING HAS BEEN ESTABLISHED BY AERIAL PHOTOGRAPHY AND FIELD VERIFICATION BY G.P.S. OBSERVATION NEAR JULY 18TH, 2016. THE CONTOUR ELEVATIONS PROVIDED MAY NOT BE EXACT GROUND ELEVATIONS, BUT RATHER INTERPRETATIONS OF SUCH ACCURACY SHALL BE CONSIDERED TO BE SUCH THAT NOT MORE THAN 10 PERCENT OF SPOT ELEVATION CHECKS SHALL BE IN ERROR BY MORE THAN ONE-HALF THE CONTOUR INTERVAL PROVIDED, AS DEFINED BY THE NATIONAL MAP ACCURACY STANDARDS. ANY QUANTITIES PROVIDED FOR EARTHWORK VOLUMES ARE ESTABLISHED USING THIS TOPOGRAPHY CONTOUR ACCURACY, AND THEREFORE THE INHERENT ACCURACY OF ANY EARTHWORK QUANTITY IS ASSUMED FROM THE TOPOGRAPHY ACCURACY.
4. PROPOSED CONTOURS ARE TO APPROXIMATE FINISHED GRADE.
5. UNLESS OTHERWISE NOTED, PAYMENT FOR EARTHWORK SHALL INCLUDE BACKFILLING OF THE CURB AND GUTTER, SIDEWALK AND FURTHER MANIPULATION OF UTILITY TRENCH SPOILS. THE SITE SHALL BE LEFT IN A MOVABLE CONDITION AND POSITIVE DRAINAGE MAINTAINED THROUGHOUT.
6. UNLESS OTHERWISE NOTED, ALL EARTHWORK IS CONSIDERED UNCLASSIFIED. NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR ROCK OR SHALE EXCAVATION, UNLESS SPECIFICALLY STATED OTHERWISE.
7. PRIOR TO EARTHWORK ACTIVITIES, PRE-DISTURBANCE EROSION AND SEDIMENT CONTROL DEVICES SHALL BE IN PLACE PER THE STORM WATER POLLUTION PREVENTION PLAN AND/OR THE EROSION AND SEDIMENT CONTROL PLAN PREPARED FOR THIS SITE.
8. ALL TOPSOIL SHALL BE STRIPPED FROM ALL AREAS TO BE GRADED AND STOCKPILED ADJACENT TO THE SITE AT AN AREA SPECIFIED BY THE PROJECT OWNER OR HIS APPOINTED REPRESENTATIVE. VEGETATION, TRASH, TREES, BRUSH, TREE ROOTS AND LIMBS, ROCK FRAGMENTS GREATER THAN 6-INCHES AND OTHER DELETERIOUS MATERIALS SHALL BE REMOVED AND PROPERLY DISPOSED OF OFFSITE OR AS DIRECTED BY THE OWNER OR HIS APPOINTED REPRESENTATIVE.
9. UNLESS OTHERWISE SPECIFIED IN THE GEOTECHNICAL REPORT, ALL FILLS SHALL BE PLACED IN MAXIMUM 6-INCH LIFTS AND COMPACTED TO 95-PERCENT OF MAXIMUM DENSITY AS DEFINED USING A STANDARD PROCTOR TEST (AASHTO T99/ASTM 698).
10. SUBGRADE FOR PAVEMENTS SHALL BE PROOF-ROLLED PRIOR TO PAVING OPERATIONS UTILIZING A FULLY LOADED TANDEM AXLE DUMP TRUCK. ALL AREAS EXHIBITING EXCESSIVE PUMPING AND HEAVING SHALL BE REMOVED, FILLED AND COMPACTED WITH SUITABLE MATERIALS AND RETESTED UNTIL ACCEPTABLE RESULTS ARE ACHIEVED AND FINAL APPROVAL HAS BEEN OBTAINED FROM THE GEOTECHNICAL ENGINEER.
11. SUBGRADE FOR BUILDING PAD SHALL INCLUDE A MINIMUM OF 18-INCHES OF LOW VOLUME CHANGE (LVC) MATERIAL, OR AS IDENTIFIED IN THE SITE SPECIFIC GEOTECHNICAL REPORT.
12. FILL MATERIALS SHALL BE PER GEOTECHNICAL REPORT AND SHALL NOT INCLUDE ORGANIC MATTER, DEBRIS OR TOPSOIL. ALL FILLS PLACED ON SLOPES GREATER THAN 6:1 SHALL BE BENCHED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REDISTRIBUTING THE TOPSOIL OVER PROPOSED TURF AND LANDSCAPED AREAS TO A MINIMUM DEPTH OF 6-INCHES BELOW FINAL GRADE.
14. ALL AREAS SHALL BE GRADED FOR POSITIVE DRAINAGE. UNLESS NOTED OTHERWISE THE FOLLOWING GRADES SHALL APPLY:
 - A. TURF AREAS – 2-5% MINIMUM, 4H:1V MAXIMUM
 - B. PAVED AREAS – 1-2% MINIMUM, 5% MAXIMUM
 - A.D.A. PARKING STALLS SHALL NOT BE SLOPED GREATER THEN 2% IN ANY DIRECTION AND CONSTRUCTED PER A.D.A. REQUIREMENTS
15. ALL DISTURBED AREAS SHALL BE FERTILIZED, SEEDED AND MULCHED IMMEDIATELY AFTER EARTHWORK ACTIVITIES HAVE CEASED. SEEDING SHALL BE PER THE EROSION AND SEDIMENT CONTROL PLAN AND/OR LANDSCAPE PLAN. IF NOT SPECIFIED SEEDING SHALL BE PER APWA SECTION 2400, LATEST EDITION, UNLESS OTHERWISE NOTED. SEEDING SHALL BE SUBSIDIARY TO THE CONTRACT PRICE FOR EARTHWORK AND GRADING ACTIVITIES.
16. ALL DISTURBED AREAS IN THE RIGHT-OF-WAY SHALL BE SODDED.
17. UNDERDRAINS ARE RECOMMENDED FOR ALL PAVED AREAS ADJACENT TO IRRIGATED TURF AND LANDSCAPED BEDS.
18. CONTRACTOR SHALL ADHERE TO THE REPORTING REQUIREMENTS OUTLINED IN THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PREPARED FOR THIS PROJECT. EROSION AND SEDIMENT CONTROL DEVICES SHALL BE PROPERLY MAINTAINED AND KEPT CLEAN OF SILT AND DEBRIS AND IN GOOD WORKING ORDER. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED AS REQUIRED.

1. EXISTING UTILITIES HAVE BEEN SHOWN TO THE GREATEST EXTENT POSSIBLE BASED UPON INFORMATION PROVIDED TO THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE RESPECTIVE UTILITY COMPANIES AND FIELD LOCATING UTILITIES PRIOR TO CONSTRUCTION AND IDENTIFYING ANY POTENTIAL CONFLICTS. ALL CONFLICTS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ANY REQUIRED UTILITY RELOCATIONS. UTILITIES DAMAGED THROUGH THE NEGLIGENCE OF THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
3. CONTRACTOR SHALL VERIFY FLOW-LINES AND STRUCTURE TOPS PRIOR TO CONSTRUCTION, AND SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES. PROVIDE SHOP DRAWINGS FOR ALL PRECAST AND MANUFACTURED UTILITY STRUCTURES FOR REVIEW BY THE ENGINEER PRIOR TO CONSTRUCTION OF THE STRUCTURES.
4. UTILITY SEPARATION: WATERLINES SHALL HAVE A MINIMUM OF 10 FEET HORIZONTAL AND 2 FEET VERTICAL SEPARATION FROM ALL SANITARY AND STORM SEWER LINES. IF MINIMUM SEPARATIONS CAN NOT BE OBTAINED, CONCRETE ENCASEMENT OF THE SANITARY OR STORM SEWER LINE SHALL BE REQUIRED 10 FEET IN EACH DIRECTION OF THE CONFLICT.
5. PAYMENT FOR TRENCHING, BACKFILLING, PIPE EMBEDMENT, FLOWABLE FILL, BACKFILL MATERIALS, CLEAN UP, STAKING, SOILING, ETC. IS THE NECESSARY FOR THE CONSTRUCTION OF THE UTILITY LINE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE UTILITY INSTALLATION.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING RESPECTIVE UTILITY COMPANIES 48-HOURS IN ADVANCE FOR THE INSPECTION OF ANY PROPOSED UTILITY MAIN EXTENSION OR SERVICE LINE OR SERVICE CONNECTION TO ANY EXISTING MAIN.
7. TRENCH SPOILS SHALL BE NEATLY PLACED ONSITE ADJACENT TO THE TRENCH, AND COMPACTED TO PREVENT SATURATION OF THE SOILS. SEDIMENT RUNOFF, UNSUITABLE MATERIALS, EXCESS ROCK AND SHALE, ASPHALT, CONCRETE, TREES, BRUSH ETC. SHALL BE PROPERLY DISPOSED OF OFFSITE. MATERIALS MAY BE WASTED ONSITE AT THE DIRECTION OF THE OWNER OR HIS APPOINTED REPRESENTATIVE.

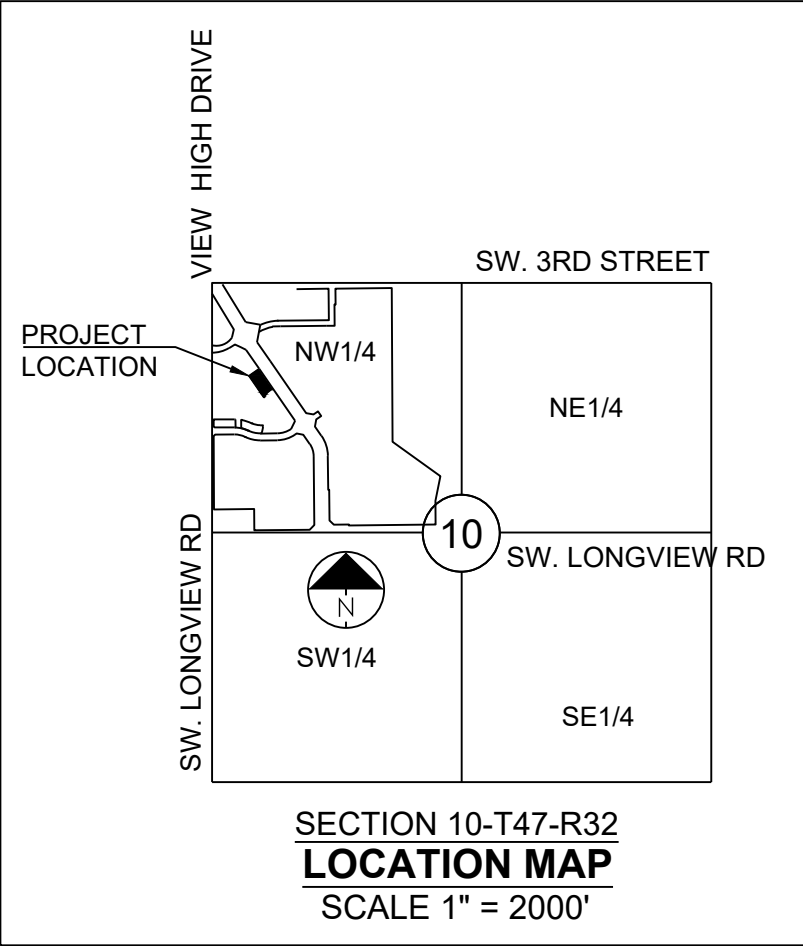
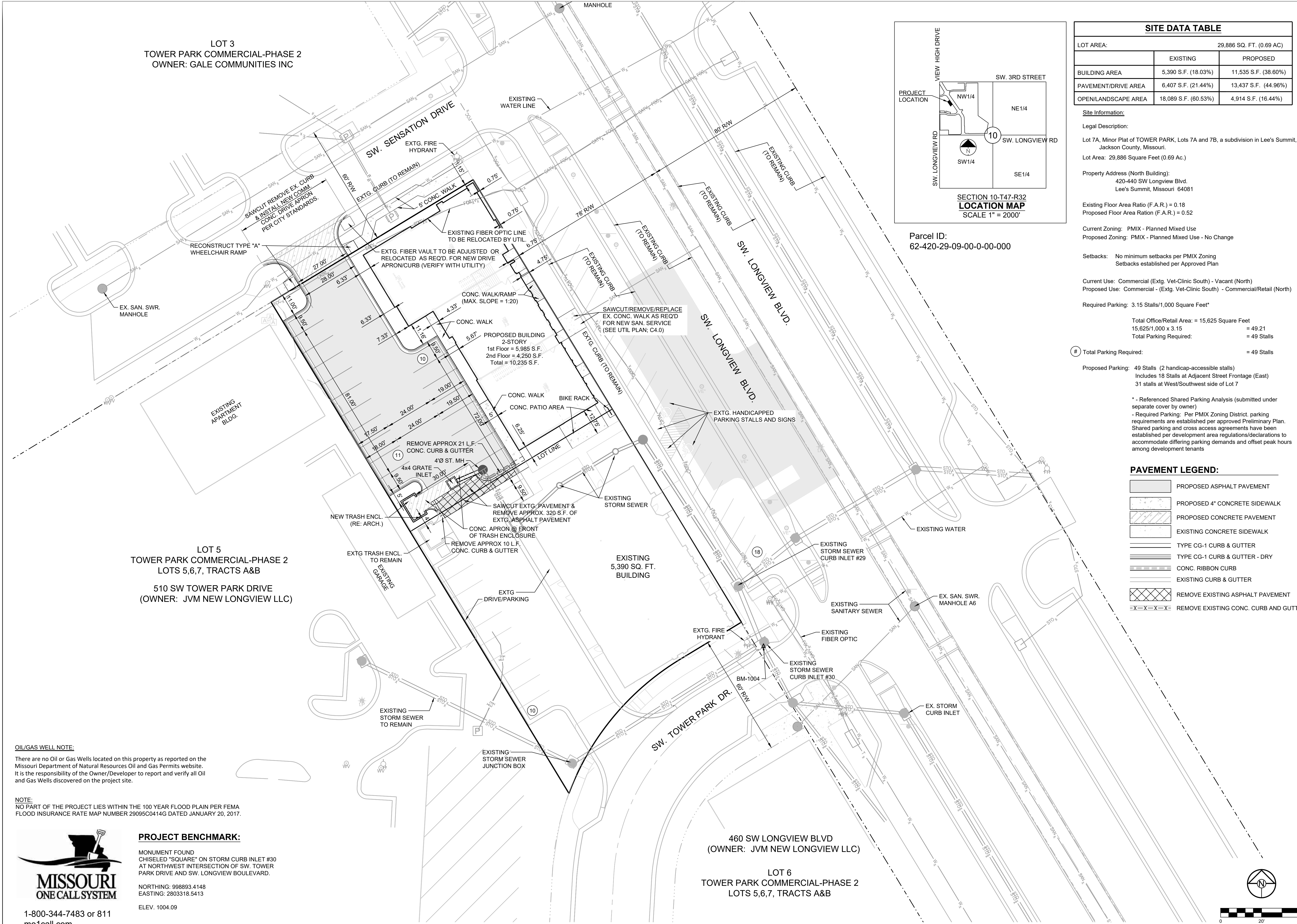
08/24/2021

Sheet List Table	
Sheet Number	Sheet Title
C0.0	COVER SHEET
C1.0	SITE PLAN
C2.0	GRADING PLAN
C3.0	STORM SEWER PLAN AND PROFILE
C4.0	SITE UTILITY PLAN
C5.0	EROSION CONTROL PLAN
C6.0	EROSION CONTROL DETAILS
C7.0	SIDEWALK & CURB DETAILS
C8.0	PAVEMENT DETAILS
C9.0	STORM SEWER DETAILS
C10.0	UTILITY DETAILS
L1.0	LANDSCAPE PLAN

NLVC, LLC/BOX REAL ESTATE DEV
RUSSEL G. PEARSON
3152 SW GRAND CIRCLE
LEE'S SUMMIT, MO 64081
p (816) 589-4415
RPEARSON@BOXCEVCO.COM



COVER SHEET	REVISION DATE	DESCRIPTION
	△ 6-29-2021	City Planning - FDP Review
	△ 8-17-2021	City Planning - FDP Review #2
	△	
	△	
	△	
	△	
	△	
	△	
	△	
DRAWN BY:	###	
	###	
	###	
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	###	
CHECKED BY:		
DATE PREPARED:		
	06/21/2021	
PROJ. NUMBER:	20-106	
SHEET	C0.0	



Parcel ID:
62-420-29-09-00-0-00-000

SITE DATA TABLE		
LOT AREA:		29,886 SQ. FT. (0.69 AC)
	EXISTING	PROPOSED
BUILDING AREA	5,390 S.F. (18.03%)	11,535 S.F. (38.60%)
PAVEMENT/DRIVE AREA	6,407 S.F. (21.44%)	13,437 S.F. (44.96%)
OPEN/LANDSCAPE AREA	18,089 S.F. (60.53%)	4,914 S.F. (16.44%)

Site Information:

Legal Description:

Lot 7A, Minor Plat of TOWER PARK, Lots 7A and 7B, a subdivision in Lee's Summit, Jackson County, Missouri.

Lot Area: 29,886 Square Feet (0.69 Ac.)

Property Address (North Building):
420-440 SW Longview Blvd.
Lee's Summit, Missouri 64081

Existing Floor Area Ratio (F.A.R.) = 0.18
Proposed Floor Area Ratio (F.A.R.) = 0.52

Current Zoning: PMIX - Planned Mixed Use
Proposed Zoning: PMIX - Planned Mixed Use - No Change

Setbacks: No minimum setbacks per PMIX Zoning
Setbacks established per Approved Plan

Current Use: Commercial (Extg. Vet-Clinic South) - Vacant (North)
Proposed Use: Commercial - (Extg. Vet-Clinic South) - Commercial/Retail (North)

Required Parking: 3.15 Stalls/1,000 Square Feet*

Total Office/Retail Area: = 15,625 Square Feet
15,625/1,000 x 3.15 = 49.21
Total Parking Required: = 49 Stalls

Total Parking Required: = 49 Stalls
Proposed Parking: 49 Stalls (2 handicap-accessible stalls)
Includes 18 Stalls at Adjacent Street Frontage (East)
31 stalls at West/Southwest side of Lot 7


* - Referenced Shared Parking Analysis (submitted under separate cover by owner)
- Required Parking: Per PMIX Zoning District, parking requirements are established per approved Preliminary Plan. Shared parking and cross access agreements have been established per development area regulations/declarations to accommodate differing parking demands and offset peak hours among development tenants

PAVEMENT LEGEND:

- PROPOSED ASPHALT PAVEMENT
- PROPOSED 4" CONCRETE SIDEWALK
- PROPOSED CONCRETE PAVEMENT
- EXISTING CONCRETE SIDEWALK
- TYPE CG-1 CURB & GUTTER
- TYPE CG-1 CURB & GUTTER - DRY
- CONC. RIBBON CURB
- EXISTING CURB & GUTTER
- REMOVE EXISTING ASPHALT PAVEMENT
- REMOVE EXISTING CONC. CURB AND GUTTER

OIL/GAS WELL NOTE:
There are no Oil or Gas Wells located on this property as reported on the Missouri Department of Natural Resources Oil and Gas Permits website. It is the responsibility of the Owner/Developer to report and verify all Oil and Gas Wells discovered on the project site.

NOTE:
NO PART OF THE PROJECT LIES WITHIN THE 100 YEAR FLOOD PLAIN PER FEMA FLOOD INSURANCE RATE MAP NUMBER 29095C0414G DATED JANUARY 20, 2017.



MISSOURI
ONE CALL SYSTEM

1-800-344-7483 or 811
mo1call.com

PROJECT BENCHMARK:

MONUMENT FOUND
CHISELED "SQUARE" ON STORM CURB INLET #30
AT NORTHWEST INTERSECTION OF SW. TOWER
PARK DRIVE AND SW. LONGVIEW BOULEVARD.

NORTHING: 998893.4148
EASTING: 2803318.5413
ELEV. 1004.09

SCHLAGEL
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS
14920 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
Missouri State Certificates of Authority
#E2002003600-F #LAC001005237 #LS2002008659-F

RELEASE FOR CONSTRUCTION
AS NOTED ON PLANS REVIEW
DEVELOPMENT SERVICES
LEE'S SUMMIT, MISSOURI
8/24/2021

PREPARED BY:



TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS

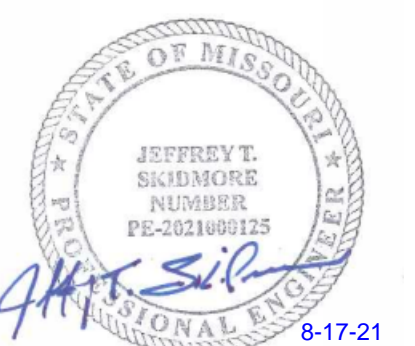
420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION	DRAWN BY	CHECKED BY	DATE PREPARED	PROJ. NUMBER
6-28-2021	City Planning - FDP Review	###	###	06/01/2021	20-106
8-17-2021	City Planning - FDP Review #2				

SITE PLAN

SHEET

C1.0



TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS

420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

REVISION DATE	DESCRIPTION	DRAWN BY
6-28-2021	City Planning - FDP Review	###
8-17-2021	City Planning - FDP Review #2	###
8-17-2021	DATE PREPARED	08/01/2021
20-106	PROJ. NUMBER	20-106

GRADING PLAN

SHEET

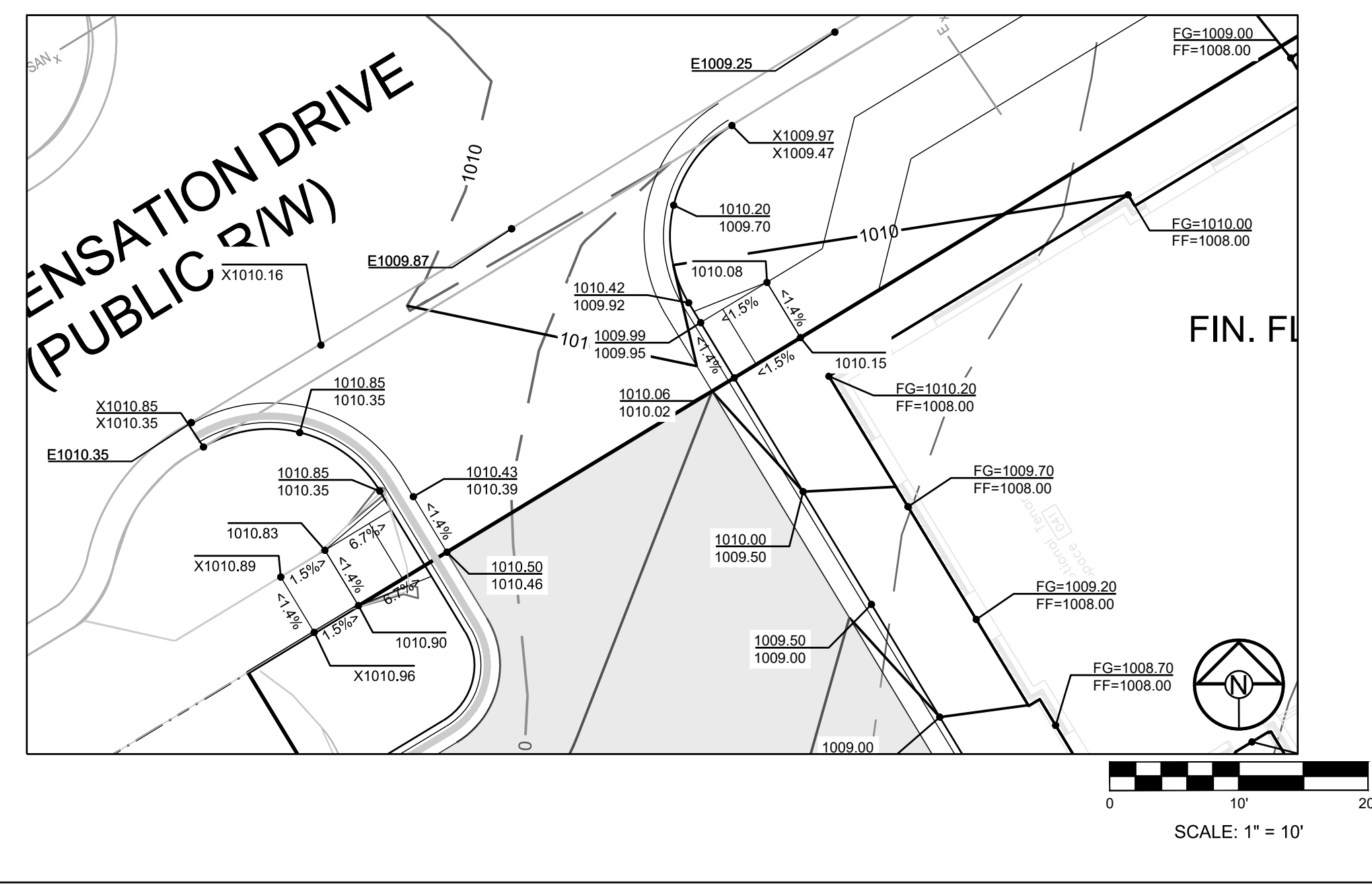
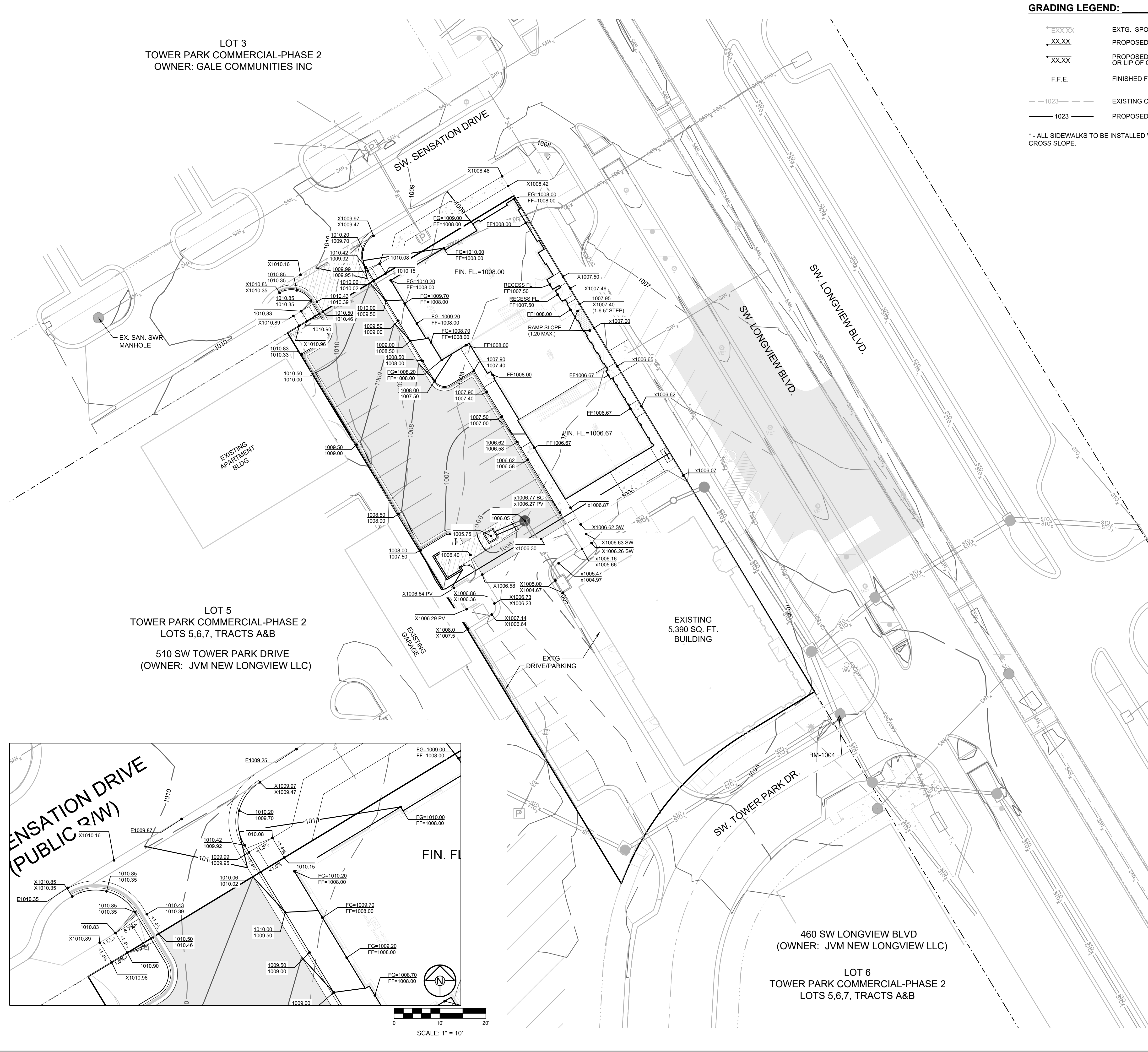
C2.0

- GRADING LEGEND:**
- EXTG. SPOT ELEVATION
 - PROPOSED TOP OF CURB ELEV.
 - PROPOSED PAVEMENT GRADE ELEV. OR LIP OF CURB ELEVATION
 - FINISHED FLOOR ELEVATION
 - EXISTING CONTOUR
 - PROPOSED CONTOUR
 - * - ALL SIDEWALKS TO BE INSTALLED WITH A 1.5% MAXIMUM CROSS SLOPE.
- PAVEMENT LEGEND:**
- PROPOSED ASPHALT PAVEMENT
 - EXISTING ASPHALT PAVEMENT
 - PROPOSED 4" CONCRETE SIDEWALK
 - PROPOSED CONCRETE PAVEMENT
 - EXISTING CONCRETE SIDEWALK
 - TYPE CG-1 CURB & GUTTER
 - TYPE CG-1 CURB & GUTTER - DRY
 - CONC. RIBBON CURB
 - EXISTING CURB & GUTTER
 - REMOVE EXISTING ASPHALT PAVEMENT
 - REMOVE EXISTING CONC. CURB AND GUTTER

- GRADING NOTES:**
- NO EDGE DRAINS OR UNDERDRAINS ARE INDICATED BEHIND THE CURBS. OWNER SHOULD CONSIDER THE INSTALLATION OF UNDERDRAINS DUE TO THE IRRIGATION OF GREENSPACE AREAS OF THE SITE.
 - RECOMMEND A GEOTECHNICAL ENGINEER REVIEW ALL EARTHWORK ACTIVITY TO MAKE SURE RECOMMENDATIONS IN GEOTECHNICAL REPORT ARE FOLLOWED.
 - PRIOR TO PLACEMENT OF CURB AND GUTTER AND PAVEMENT, GEOTECHNICAL ENGINEER MUST APPROVE SUBGRADE IN WRITTEN FORM TO THE OWNER AND PROJECT ENGINEER.
 - ALL UTILITY INSTALLATIONS UNDER PAVED AREAS MUST BE COMPACTED AS PER THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER AND/OR GEOTECHNICAL REPORT.
 - ALL CONSTRUCTION SHALL COMPLY WITH THE CITY OF LEE'S SUMMIT TECHNICAL SPECIFICATIONS.
 - EXISTING TOPOGRAPHY SHOWN AS ESTABLISHED FROM BASE SURVEY PREPARED BY SCHLAGEL AND ASSOCIATES, P.A. - VERIFY GRADES PRIOR TO COMMENCEMENT OF GRADING AND CONSTRUCTION ACTIVITIES. NO ADDITIONAL MONEY WILL BE PAID FOR HAUL-IN OR HAUL-OFF OF MATERIAL.
 - ALL ROCK, CONCRETE, ASPHALT, TREE, BRUSH, ETC. TO BE REMOVED AS A PART OF THE PROJECT CONSTRUCTION SHALL BE DISPOSED OF BY THE GRADING CONTRACTOR AND SHALL BE A SUBSIDIARY OBLIGATION OF THE CONTRACT. THE GRADING CONTRACTOR IS ALSO RESPONSIBLE FOR ALL GRADING ON THE SITE INCLUDING THE MANIPULATION OF THE EXCESS DIRT MATERIAL THAT WAS LEFT ALONG THE SEWER TRENCHES. THE COST FOR THIS WORK WILL BE INCLUDED IN THE LUMP SUM FEE FOR GRADING.
 - ENTIRE PROJECT SHALL BE LEFT IN A MOWABLE CONDITION. ALL DISTURBED AREAS SHALL BE SEEDED & MULCHED AS PER PROJECT REQUIREMENTS. ALL DISTURBED AREAS WITHIN THE PUBLIC STREET RIGHT-OF-WAY SHALL BE SODDED IN COMPLIANCE WITH THE CITY OF LEE'S SUMMIT TECHNICAL SPECIFICATIONS AND MUNICIPAL CODE.
 - THE CONTRACTOR SHALL PROVIDE FOR POSITIVE DRAINAGE AWAY FROM BUILDINGS AND SIDEWALKS AT ALL TIMES.
 - THE CONTRACTOR IS RESPONSIBLE FOR PROTECTION OF ALL PROPERTY CORNERS. ANY PROPERTY CORNERS DISTURBED OR DAMAGED BY GRADING ACTIVITIES SHALL BE RESET BY A PROFESSIONAL LAND SURVEYOR LICENSED IN THE STATE OF MISSOURI, AT THE CONTRACTOR'S EXPENSE.

NOTE:
NO PART OF THE PROJECT LIES WITHIN THE 100 YEAR FLOOD PLAIN PER FEMA FLOOD INSURANCE RATE MAP NUMBER 29095C0414G DATED JANUARY 20, 2017.

PROJECT BENCHMARK:
MONUMENT FOUND
CHISELED "SQUARE" ON STORM CURB INLET #30
AT NORTHWEST INTERSECTION OF SW. TOWER PARK DRIVE AND SW. LONGVIEW BOULEVARD.
NORTHING: 998893.4148
EASTING: 2803318.5413
ELEV. 1004.09



LOT 3
TOWER PARK COMMERCIAL-PHASE 2
OWNER: GALE COMMUNITIES INC

LOT 5
TOWER PARK COMMERCIAL-PHASE 2
LOTS 5,6,7, TRACTS A&B

510 SW TOWER PARK DRIVE
(OWNER: JVM NEW LONGVIEW LLC)

NOTE:
NO PART OF THE PROJECT LIES WITHIN THE 100 YEAR FLOOD PLAIN PER FEMA
FLOOD INSURANCE RATE MAP NUMBER 29095C0414G DATED JANUARY 20, 2017



1-800-344-7483 or 811
mo1call.com

PROJECT BENCHMARK:

MONUMENT FOUND
CHISELED "SQUARE" ON STORM CURB INLET #30
AT NORTHWEST INTERSECTION OF SW. TOWER
PARK DRIVE AND SW. LONGVIEW BOULEVARD.

NORTHING: 998893.4148
EASTING: 2803318.5413

ELEV. 1004.09

460 SW LONGVIEW BLVD
(OWNER: JVM NEW LONGVIEW LLC)

LOT 6
TOWER PARK COMMERCIAL-PHASE 2
LOTS 5,6,7, TRACTS A&B

UTILITY INFORMATION & CONTACTS:

Missouri Gas Energy
Attn: Lucas Walls
3025 Southeast Clover Drive
Lee's Summit, Missouri 64082
Phone: (816) 969-2218
Email: lucas.walls@sug.com

Kansas City Power & Light
Attn: Phillip Ingram
1300 Hamblen Road
Lee's Summit, Missouri 64081
Phone: (816) 347-4339
Email: phillip.ingram@kcpl.com

Sewer and Water - City of Lee's Summit
220 SE Green Street
Lee's Summit, Missouri 64063
Phone: (816) 969-1900
Email: publicworks@cityofls.net

AT&T
Attn: Herb Upshaw
9444 Nall Avenue
Overland Park, Kansas 66207
Phone: (913) 383-4929
Email: hu4112@att.com

811
Missouri One Call - 1-800-344-7483

UTILITY STATEMENT:

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDON. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS

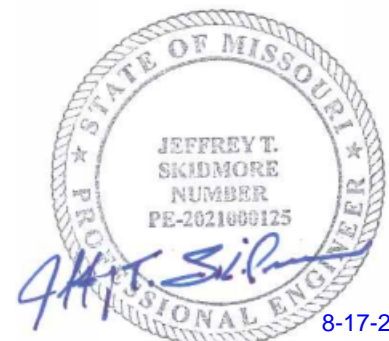
DRAWN BY:	REVISION DATE	DESCRIPTION
###	6-28-2021	City Planning - FDP Review
###	8-7-2021	City Planning - FDP Review #2
###	###	
###	###	
###	###	
DATE PREPARED:	###	
06/1/2021	###	
PROJ. NUMBER:	###	
20.005		

SITE UTILITY PLAN

SHEET

C4.0

SCHLAGEL
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS
14820 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
#E2002003800F #LAC2001005237 #L5200200859-F



LOT 3
TOWER PARK COMMERCIAL-PHASE 2
OWNER: GALE COMMUNITIES INC

LOT 5
TOWER PARK COMMERCIAL-PHASE 2
LOTS 5,6,7, TRACTS A&B
510 SW TOWER PARK DRIVE
(OWNER: JVM NEW LONGVIEW LLC)

LOT 6
TOWER PARK COMMERCIAL-PHASE 2
LOTS 5,6,7, TRACTS A&B
460 SW LONGVIEW BLVD
(OWNER: JVM NEW LONGVIEW LLC)

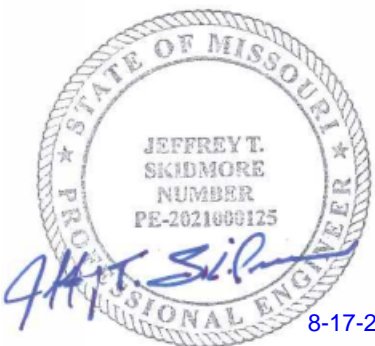
NOTE:
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FLOOD INSURANCE RATE MAP NUMBER 29095C0414G DATED JANUARY 20, 2017.

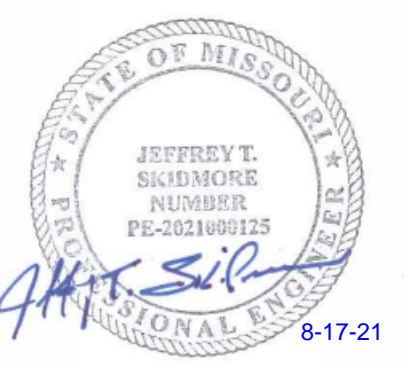
PROJECT BENCHMARK:
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AT NORTHWEST INTERSECTION OF SW. TOWER
PARK DRIVE AND SW. LONGVIEW BOULEVARD.
NORTHING: 998893.4148
EASTING: 2803318.5413
ELEV. 1004.09

SW. SENSATION DRIVE
SW. LONGVIEW BLVD.
SW. TOWER PARK DR.
EXISTING APARTMENT BLDG.
EXISTING GARAGE
EXISTING 5,390 SQ. FT. BUILDING
CONSTRUCTION FENCE
EXTG. DRIVE/PARKING
APPROX. LIMITS OF DISTURBANCE (0.40 AC.)
FIN. FL.=1008.00
FIN. FL.=1006.67
EX. SAN. SWR. MANHOLE
BM-1004

PROJECT STAGE	BMP PLAN REF. NO	BMP DESCRIPTION	REMOVE/ AFTER STAGE	NOTES:
A - PRIOR TO LAND DISTURBANCE	1	CONSTRUCTION ENTRANCE & STAGING AREA	D	MAINTAIN, REPAIR, OR REPLACE AS NECESSARY
	2	SILT FENCE (PRIOR TO LAND DISTURBANCE)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
	3	EXISTING INLET PROTECTION (GRAVEL CURB INLET SEDIMENT TRAP)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
B - MASS GRADING	4	SILT FENCE (DURING CONSTRUCTION)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
C - UTILITY CONSTRUCTION	5	CONCRETE WASHOUT AREA	E	MAINTAIN, REPAIR, OR REPLACE AS NECESSARY
	6	INLET PROTECTION (SILT FENCE)	D/E	PLACE SILT FENCE AROUND ALL STORM SEWER STRUCTURES / YARD AREA STORM STRUCTURES PRIOR TO TOPS BEING PLACED SILT FENCE REMOVED & REPLACE WITH #7 BELOW WITH PLACEMENT OF TOPS AND/OR STABILIZATION OF DRAINAGE AREAS.
D - AFTER PAVING OPERATIONS	7	INLET PROTECTION (GRAVEL FILTER BAGS)	E	BOARDS SHALL BE PLACED IN FRONT OF INLET OPENING FROM THE TIME SILT FENCE IS REMOVED UNTIL SUCH TIME THAT THE CURB / THROAT IS POURED. PLACE GRAVEL FILTER BAGS AT THE OPENING OF ALL CURB INLETS IMMEDIATELY AFTER THE INLET THROATS ARE POURED
	8	SILT FENCE (AFTER CURB CONSTRUCTION)	E	PLACE WHERE INDICATED, REPAIR OR REPLACE AS NECESSARY AND REMOVE ONLY WHEN GRADED AREAS HAVE SUFFICIENT GROUND COVER ESTABLISHED
	9	SEEDING AND MULCHING	E	ALL DISTURBED AREAS AFTER 14 DAYS OF CONSTRUCTION INACTIVITY
E - UNTIL CLOSURE OF LAND DISTURBANCE PERMIT	10		E	ADDITIONAL SEDIMENT AND EROSION CONTROL MEASURES MAY BE REQUIRED ANY TIME CURRENT MEASURES ARE FOUND TO BE INEFFECTIVE.

<div style="display: flex; justify-content: space-between;"> <div> <p>EROSION CONTROL PLAN</p> <p>SHEET</p> <p>C5.0</p> </div> <div> <p>2015.006</p> </div> </div>	<p>DRAWN BY:</p> <p>####</p>	<p>DESCRIPTION</p> <p>City Planning - FPD Review</p>	
	<p>CHECKED BY:</p> <p>####</p>	<p>DATE</p> <p>8-7-2021</p>	<p>DESCRIPTION</p> <p>City Planning - FPD Review #2</p>
	<p>DATE PREPARED:</p> <p>06/01/2021</p>	<p>DATE</p> <p>6-9-2021</p>	<p>DESCRIPTION</p> <p>City Planning - FPD Review</p>
	<p>PROJ. NUMBER:</p> <p>2015.006</p>	<p>DATE</p> <p>8-7-2021</p>	<p>DESCRIPTION</p> <p>City Planning - FPD Review #2</p>
	<p>PROJ. NUMBER:</p> <p>2015.006</p>	<p>DATE</p> <p>8-7-2021</p>	<p>DESCRIPTION</p> <p>City Planning - FPD Review #2</p>





TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS
420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

REVISION	DATE	DESCRIPTION
1	6-28-2021	City Planning - FDP Review
2	8-17-2021	City Planning - FDP Review #2
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Plan View
Not to Scale

Side Elevation
Not to Scale

Section A-A
Not to Scale

Notes for Concrete Washout:

- Concrete washout areas shall be installed prior to any concrete placement on site.
- Concrete washout area shall include a flat subsurface pit sized relative to the amount of concrete to be placed on site. The slopes leading out of the subsurface pit shall be 3:1. The vehicle tracking pit shall be sloped towards the concrete washout area.
- Vehicle tracking control is required at the access point to all concrete washout areas.
- Signs shall be placed at the construction site entrance, washout area and elsewhere as necessary to clearly indicate the location(s) of the concrete washout area(s) to operators of concrete truck and pump rigs.
- A one-way stop sign may be required along the bottom and sides of the subsurface pit in sandy or gravelly soils.

Maintenance for Concrete Washout:

- Concrete washout materials shall be removed once the materials have filled the washout to approximately 75% full.
- Concrete washout areas shall be enlarged as necessary to maintain capacity for washed concrete.
- Concrete washout water, washed pieces of concrete and all other debris in the subsurface pit shall be transported from the job site in a water-tight container and disposed of properly.
- Concrete washout areas shall remain in place until all concrete for the project is placed.
- When concrete washout areas are removed, excavations shall be filled with suitable compacted backfill and finished, any disturbed areas associated with the installation, maintenance, and/or removal of the concrete washout areas shall be stabilized.

CONCRETE WASHOUT

Notes for Construction Entrance:

- Avoid locating on steep slopes, at curves on public roads, or over/under disturbed area.
- Remove all vegetation and other unsuitable material from the foundation area, grade, and crown for positive drainage.
- If slope towards the public road exceeds 3%, construct a 6- to 8-inch high ridge with 34:1V side slopes across the foundation approximately 15 feet from the edge of the public road to divert runoff from it.
- Install pipe under the entrance if needed to maintain drainage ditches along public roads.
- Place stone to dimensions and grade as shown on plans. Leave surface sloped for drainage.
- Divert all surface runoff and drainage from the entrance to a sediment control device.
- If conditions warrant, place geotextile fabric on the graded foundation to improve stability.

Maintenance for Construction Entrance:

- Reshape entrance as needed to maintain function and integrity of installation. Top dress with clean aggregate as needed.

CONSTRUCTION ENTRANCE

Construction Entrance modified from 2015 Overland Park Standard Details for Erosion and Sediment Control. Concrete Washout modified from 2009 City of Great Bend Standard Drawings.

AMERICAN PUBLIC WORKS ASSOCIATION
Kansas City Metro Chapter
METRO CHAPTER
CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT
STANDARD DRAWING NUMBER ESC-01
ADOPTED: 10/24/2016

Early Stage Curb Inlet
(Open Box and Prior to Pouring Curb and Inlet Throat)

Detail A

Notes:

- Immediately following inlet construction and prior to construction of curb and inlet throat, protect inlet opening by installing 2\"/>
- When inlet is completed and curbs poured, filter socks or approved equal should be used (Late Stage Curb Inlet). Stone weaves are not approved for curb inlet use.
- Contractor to field verify ponding water shall not create a traffic hazard.

Maintenance:

- Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
- Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
- Repair or replace as necessary to maintain function and integrity of installation.

On Grade Curb Inlet Protection

Sump Inlet Sediment Filter

Front View

LATE STAGE CURB INLET
(After Pouring Curb and Inlet Throat)

AMERICAN PUBLIC WORKS ASSOCIATION
Kansas City Metro Chapter
METRO CHAPTER
CURB INLET PROTECTION
STANDARD DRAWING NUMBER ESC-06
ADOPTED: 10/24/2016

SILT FENCE DETAILS
Not to Scale

SILT FENCE LAYOUT
Not to Scale

Notes:

- In order to contain water, the ends of the silt fence must be turned up (Figure A).
- Long perimeter runs of silt fence must be limited to 100'. Runs should be broken up into several smaller segments to minimize water concentrations (Figure A).
- Long slopes should be broken up with intermediate rows of silt fence to slow runoff velocities.
- Attach fabric to upstream side of post.
- Install posts a minimum of 2' into the ground.
- Trenching will only be allowed for small or difficult installation, where slicing machine cannot be reasonably used.

Maintenance:

- Remove and dispose of sediment deposits when the deposit approaches 1/2 the height of silt fence.
- Repair as necessary to maintain function and structure.

JOINING FENCE SECTIONS
Not to Scale

AMERICAN PUBLIC WORKS ASSOCIATION
Kansas City Metro Chapter
METRO CHAPTER
SILT FENCE
STANDARD DRAWING NUMBER ESC-03
ADOPTED: 10/24/2016

Section A-A
Not to Scale

Plan
Not to Scale

Front View

Notes:

- Early Stage Area Inlet Sediment Barrier to be installed immediately after inlet or junction box is constructed.
- Silt fence shall remain in place until excavated area is removed and Late Stage Area Inlet is being installed.
- Backfill excavated area ONLY after final grading of the site. Stabilization of the site is to immediately follow.
- Wire reinforced silt fence may be used in place of silt fence attached to wood frame.

Early Stage Area Inlet
(All open boxes and inlets not at final grade)

Plan
Not to Scale

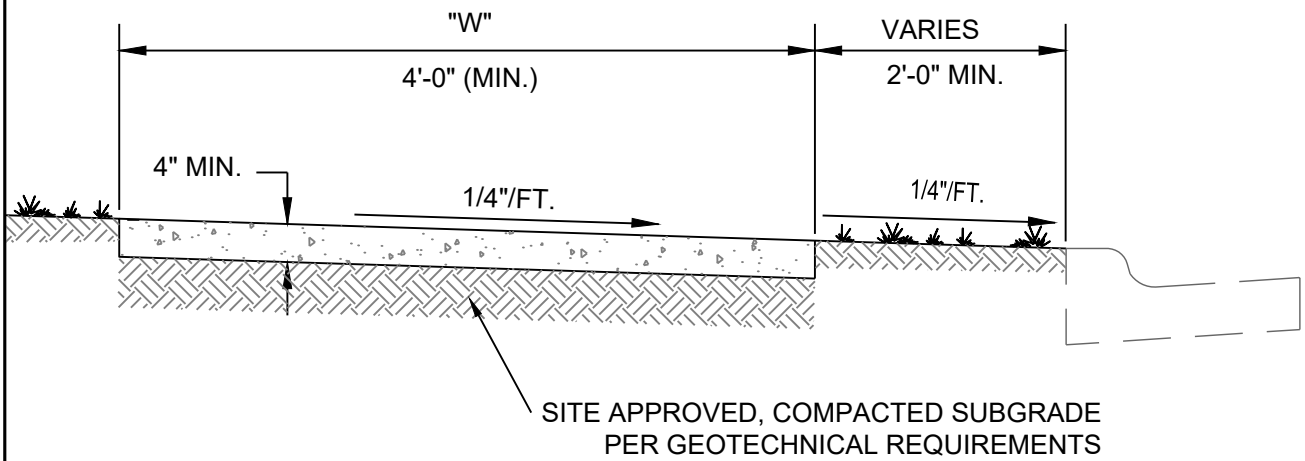
Front View

LATE STAGE AREA INLET
(Area inlets at final grade and existing inlets)

Maintenance:

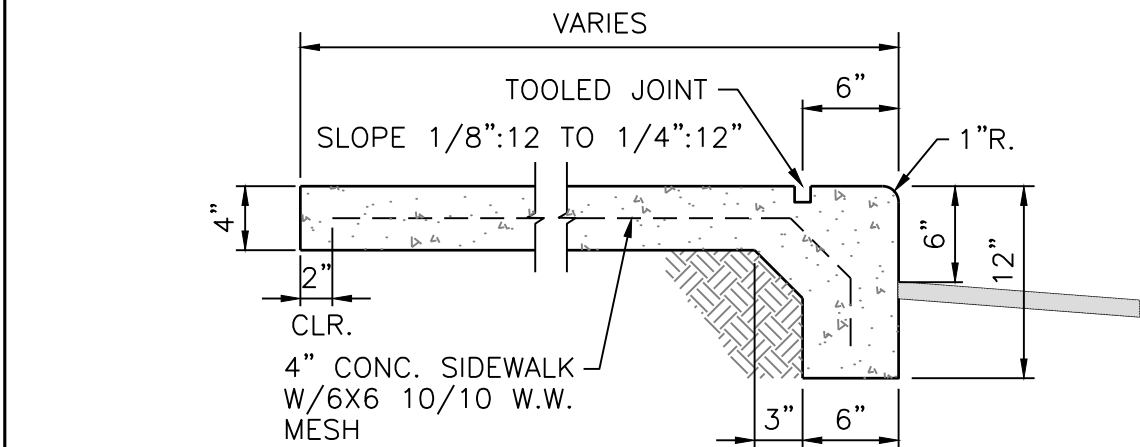
- Remove deposited sediment from excavated storage areas when available storage has been reduced by 20%.
- Remove deposited sediment from filter socks or similar when any accumulation of sediment is visible.
- Repair or replace as necessary to maintain function and integrity of installation.

AMERICAN PUBLIC WORKS ASSOCIATION
Kansas City Metro Chapter
METRO CHAPTER
AREA INLET AND JUNCTION BOX PROTECTION
STANDARD DRAWING NUMBER ESC-07
ADOPTED: 10/24/2016

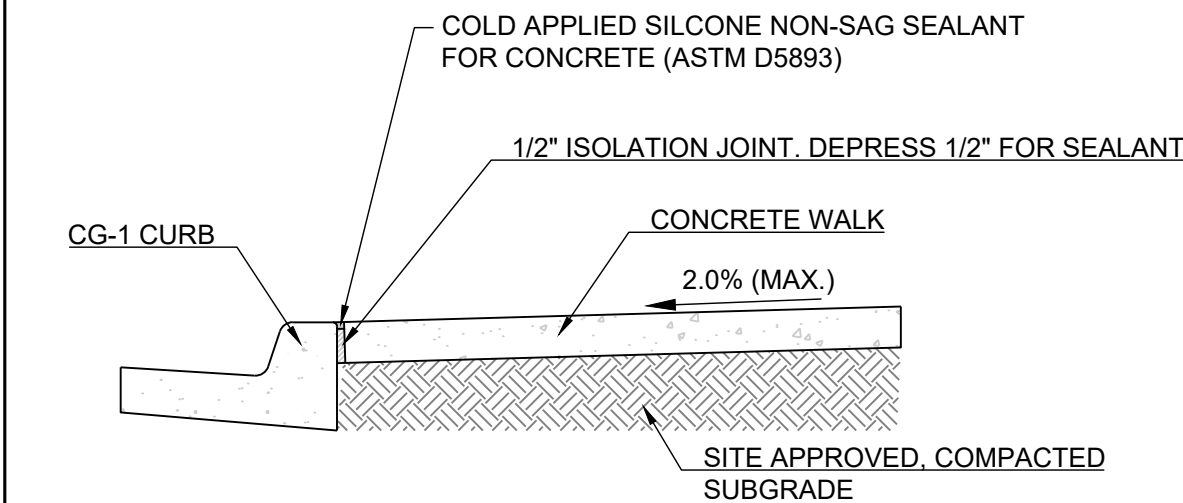


- NOTES:
1. TYPE 1 JOINTS SHALL BE PLACED "W" CENTERS.
 2. TYPE THREE JOINTS SHALL BE PLACED AT 250' CENTERS AND WHERE WALK ABUTS EXISTING CONCRETE.
 3. ALL CONCRETE SHALL BE KCMMB-4K.
 4. WHEN UTILITY SERVICE BOXES, METER BOXES, ETC. WHICH MEASURE LESS THAN 1 FT. SQUARE MUST BE PLACED IN THE SIDEWALK, THE UTILITY SERVICE BOXES, ETC. SHALL BE NO CLOSER TO ANY EDGE OF THE SIDEWALK PANEL THAN 1 FT.
 5. WHEN UTILITY BOXES, METER BOXES, ETC. GREATER THAN 1 FT. IN ANY DIMENSION MUST BE PLACED IN THE SIDEWALK, THEY SHALL BE PLACED IN THE CORNER OF THE SIDEWALK PANEL.
 6. AN ISOLATION JOINT SHALL BE PLACED BETWEEN THE CONCRETE AND ANY UTILITY BOX, ETC. WHICH IS PLACED IN THE SIDEWALK.
 7. NO SECTION OF SIDEWALK LESS THEN 12" IN ANY DIMENSION. (HORIZONTAL)
 8. AB-3 MAY BE USED AS A LEVELING COURSE. AB-3 MUST BE MOIST (MIN. 5% MOISTURE) AND COMPACTED. DEPTH OF LEVELING COURSE SHALL NOT EXCEED 6". CLEAN ROCK WILL NOT BE ALLOWED.

TYPICAL CONCRETE SIDEWALK DETAILS

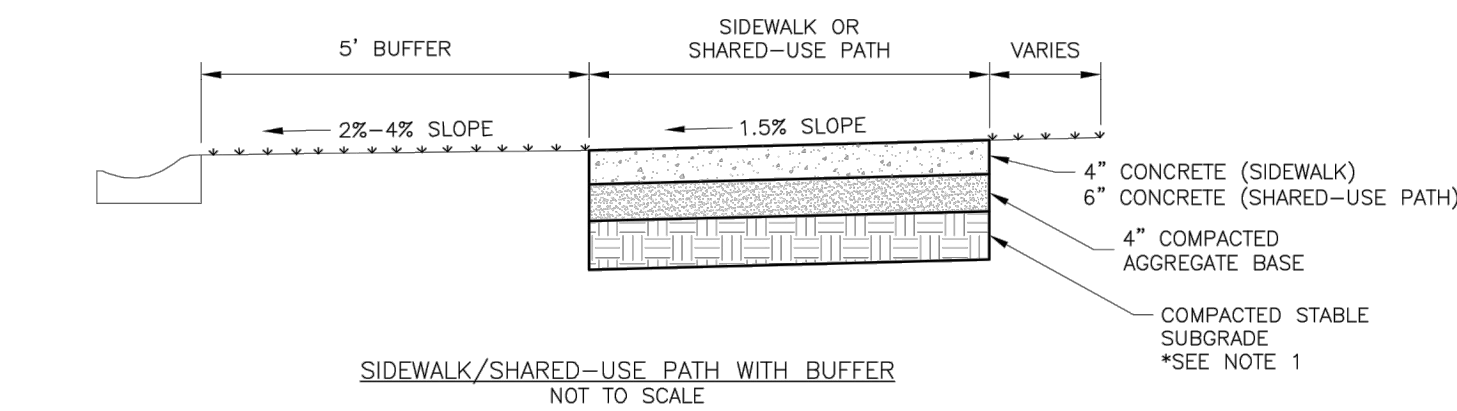
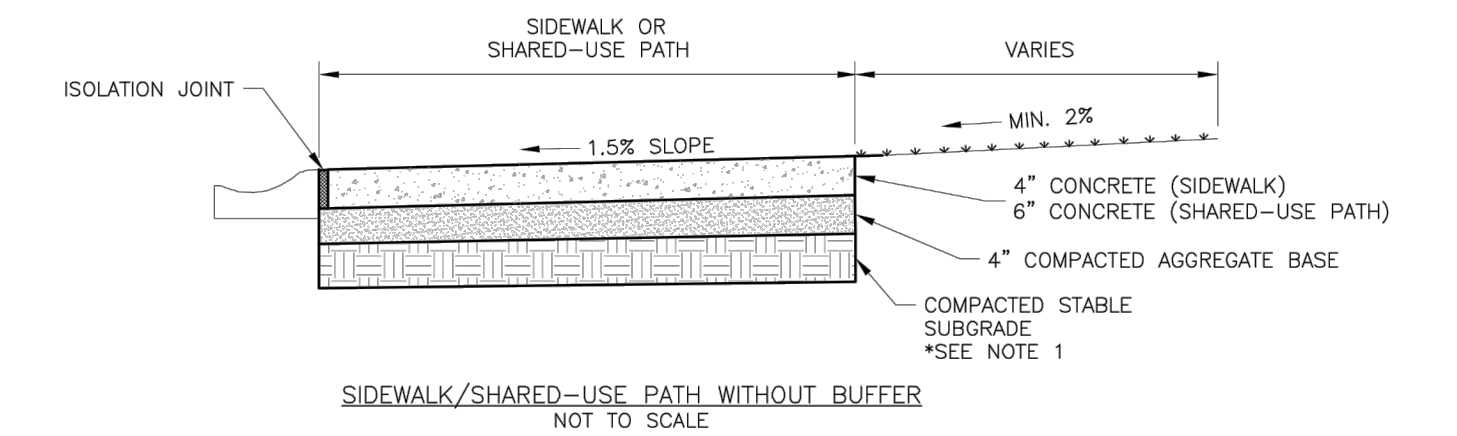


INTEGRAL CURB/SIDEWALK DETAIL



- A. Expansion- and Isolation-Joint-Filler Strips: ASTM D994, D1751 or D1752.
- B. Cold Applied Joint Sealant : Single Component, Nonsag, Silicone Joint Sealant for Concrete: ASTM D5893, Type NS

SIDEWALK @ BACK OF CURB DETAIL



- GENERAL NOTES:
1. SUBGRADE MUST BE OF STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
 2. 1.5% CROSS SLOPE MUST BE MAINTAINED THROUGH DRIVEWAYS.
 3. KCMMB 4K CONCRETE MIX SHALL BE REQUIRED FOR ALL SIDEWALKS AND SHARED-USE PATHS OR AS APPROVED BY THE CITY INSPECTOR.
 4. ALL SIDEWALKS SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG).
 5. AN EXPANSION JOINT SHALL BE PLACED AT A MAXIMUM OF 150 FT. CONSTRUCTION JOINTS SHALL BE PLACED THE SAME WIDTH OF SIDEWALK, BUT NO GREATER THAN 10 FT.
 6. SHARED-USE PATH WIDTH SHALL BE 10 FT. WIDE.
 7. SIDEWALK FINISHING (NO PICTURE FRAMING) AS DIRECTED BY CITY INSPECTOR.
 8. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.

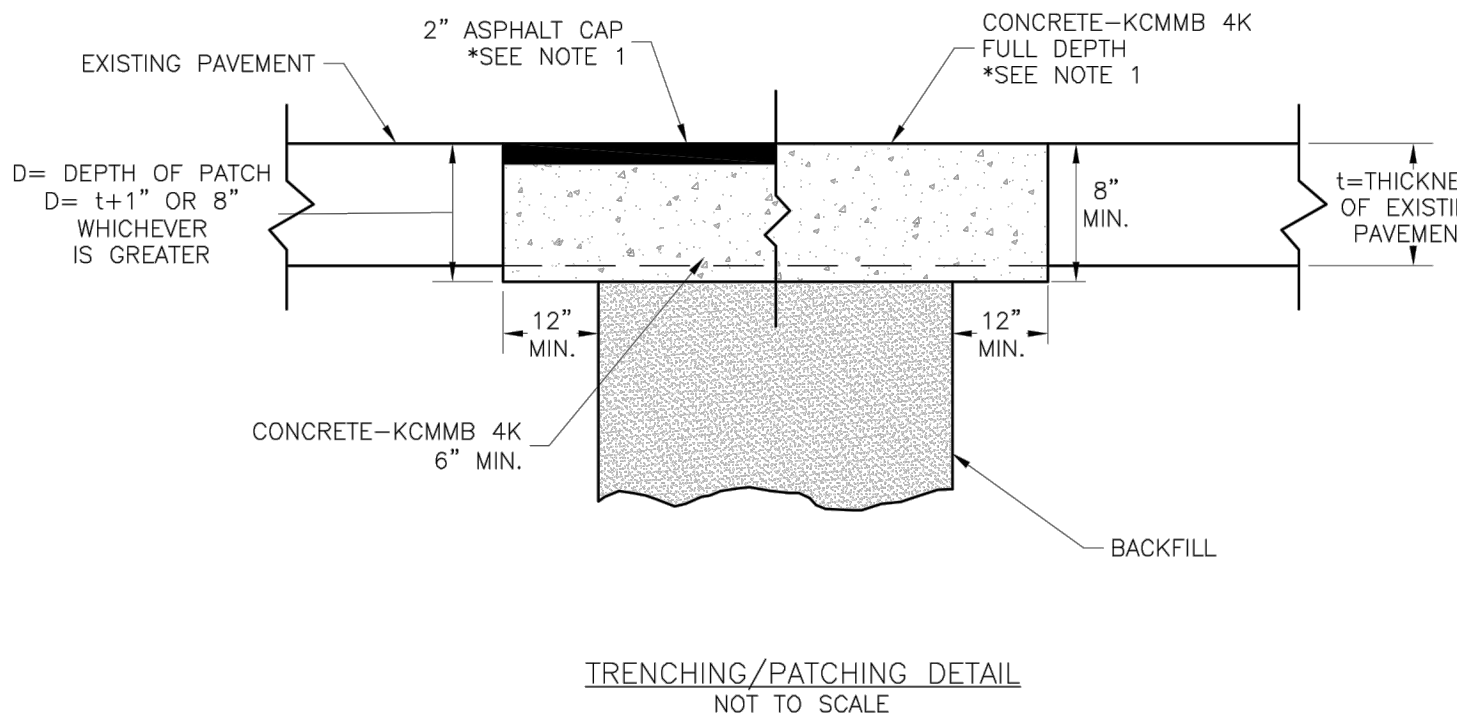
LS

LEE'S SUMMIT
MISSOURI

PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

DATE: 04/17
DRAWN BY: MJF
CHECKED BY: DL

GEN-2



- NOTE:
1. ASPHALT CAP OR FULL DEPTH CONCRETE SHALL BE DETERMINED BY CITY INSPECTOR.

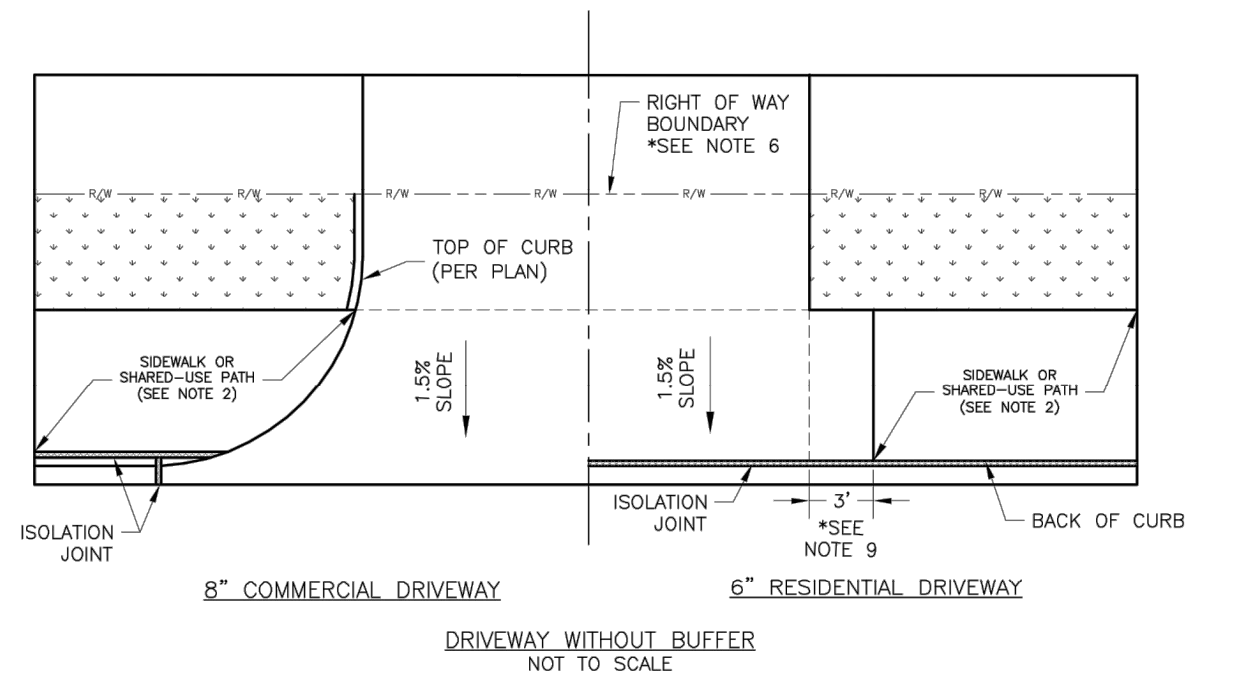
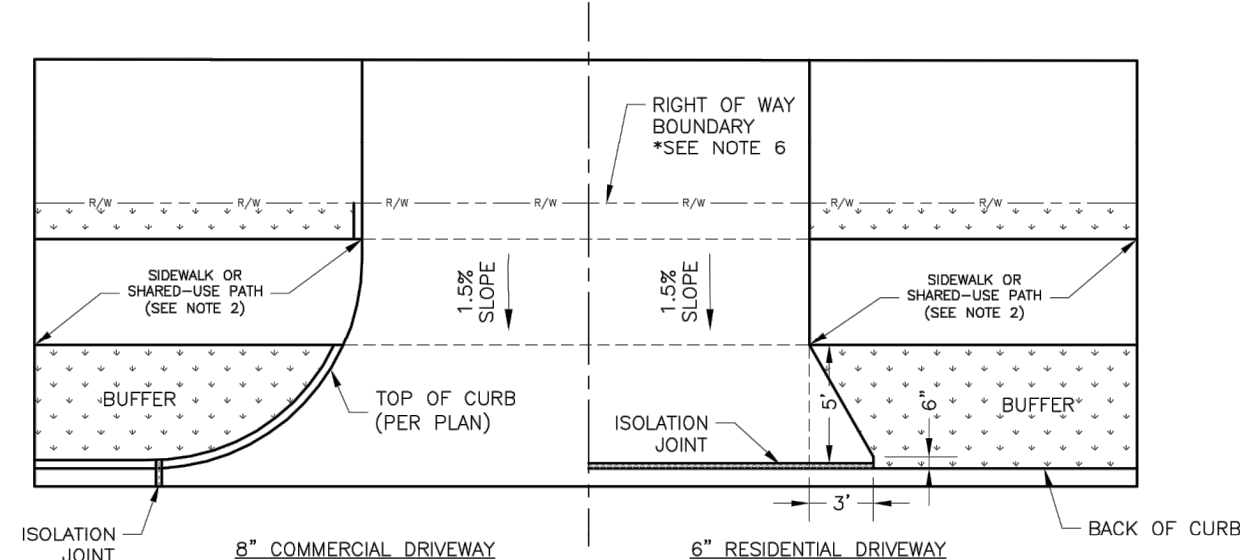
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LEE'S SUMMIT
MISSOURI

PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

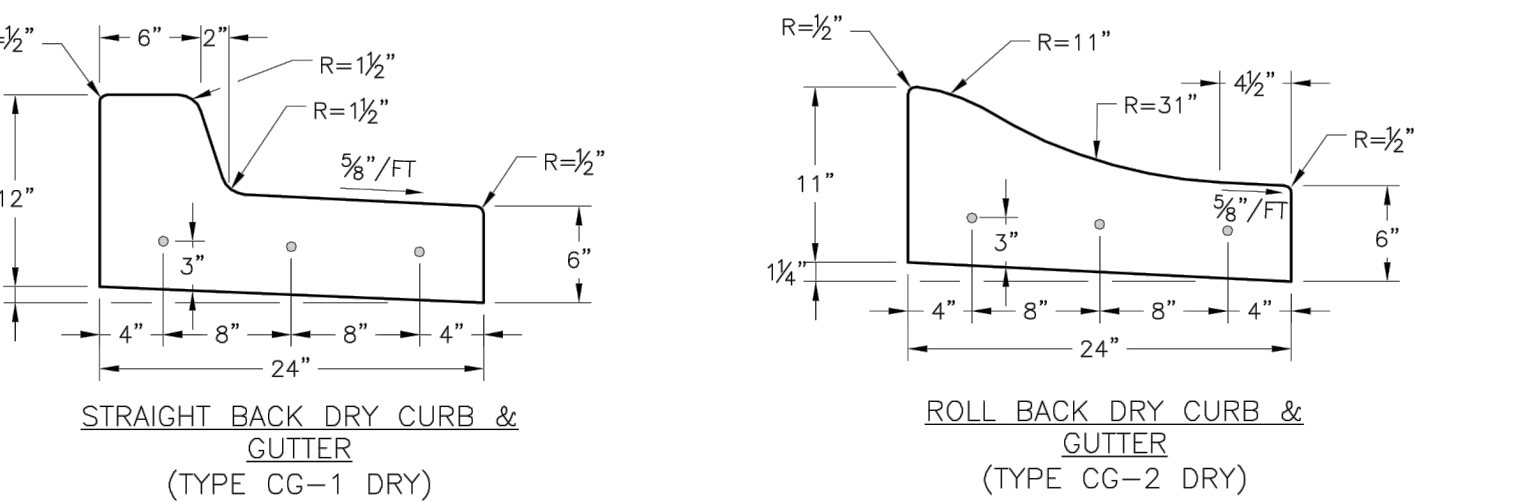
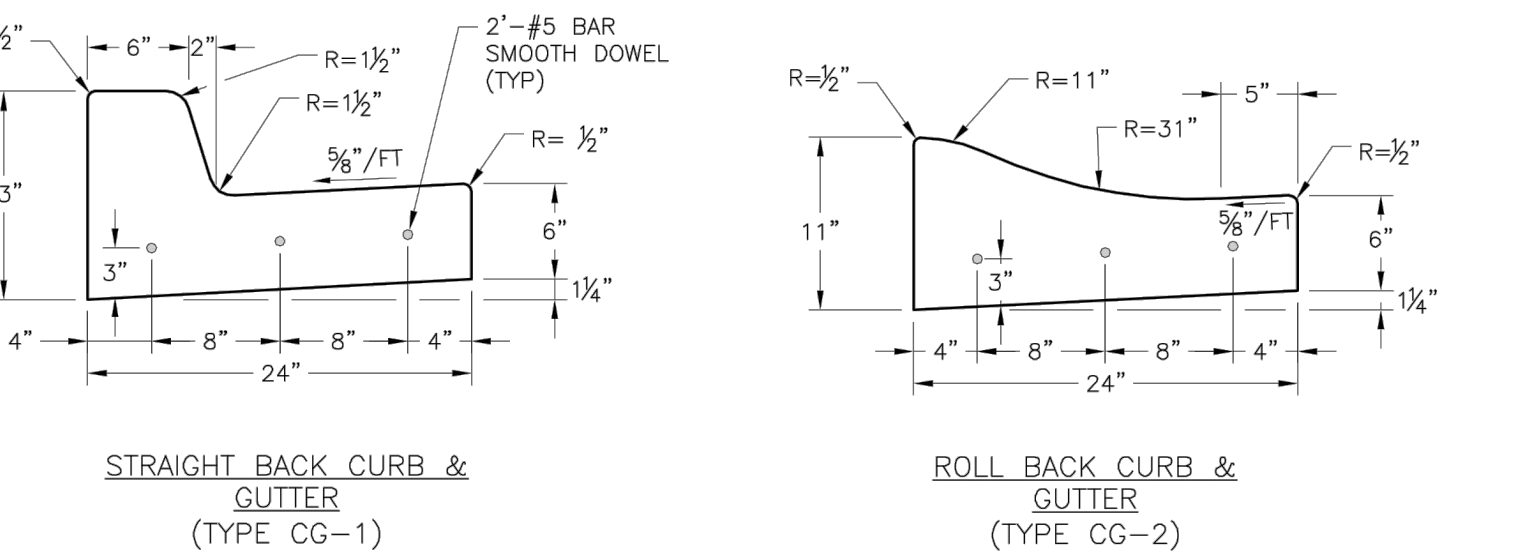
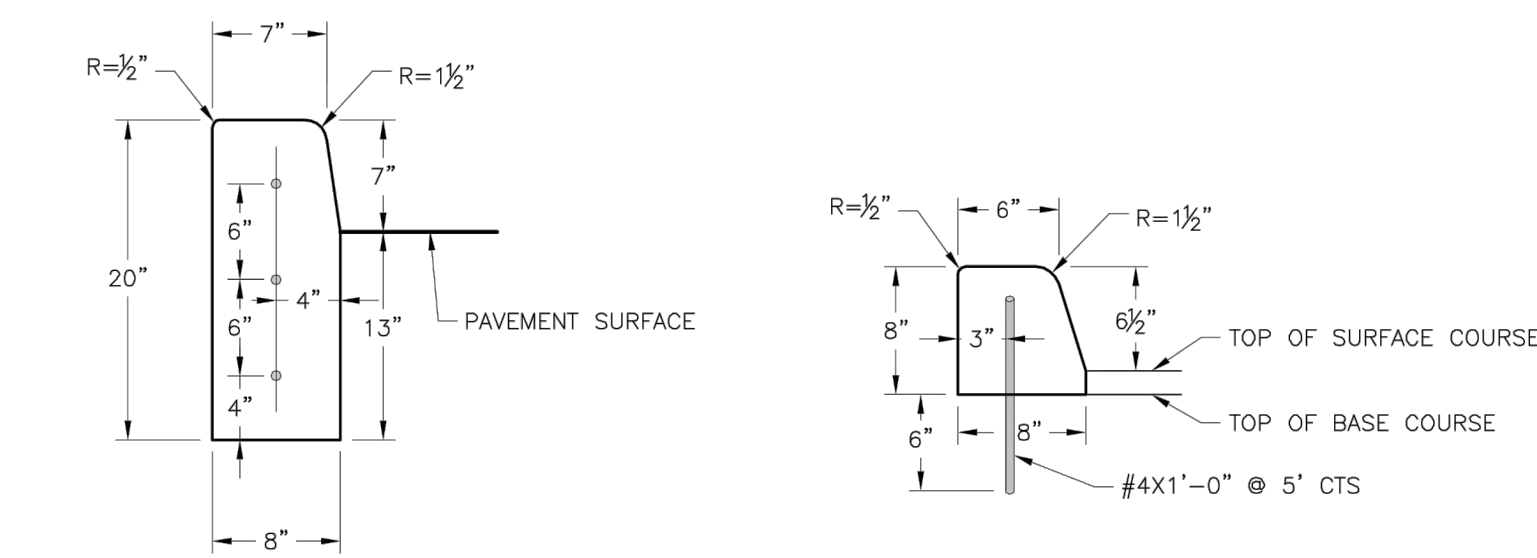
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DRAWN BY: MJF
CHECKED BY: DL

GEN-5

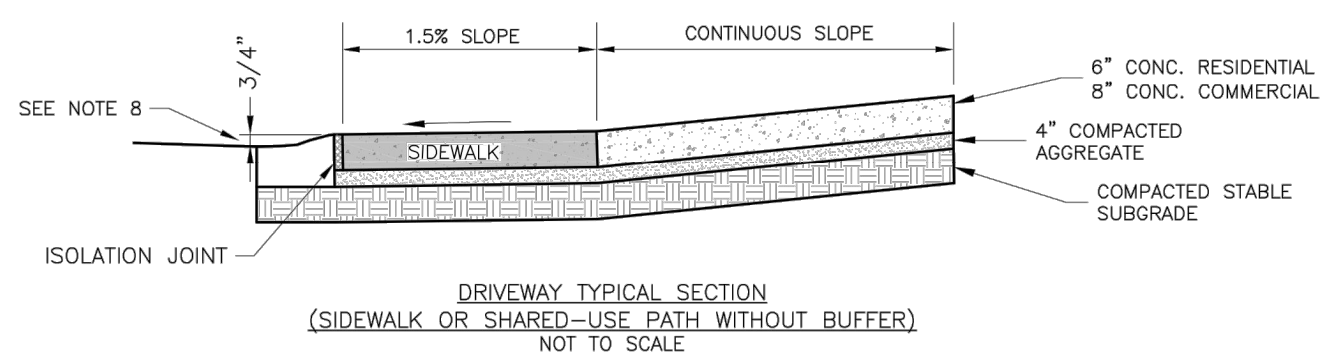
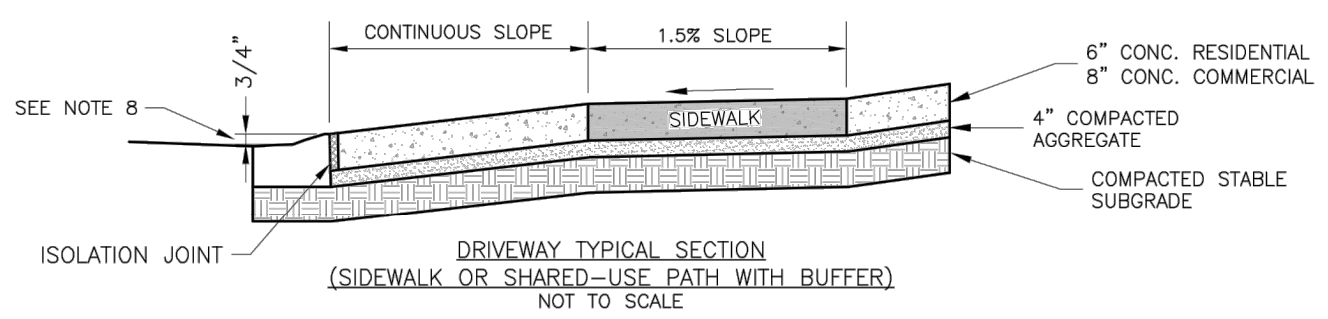
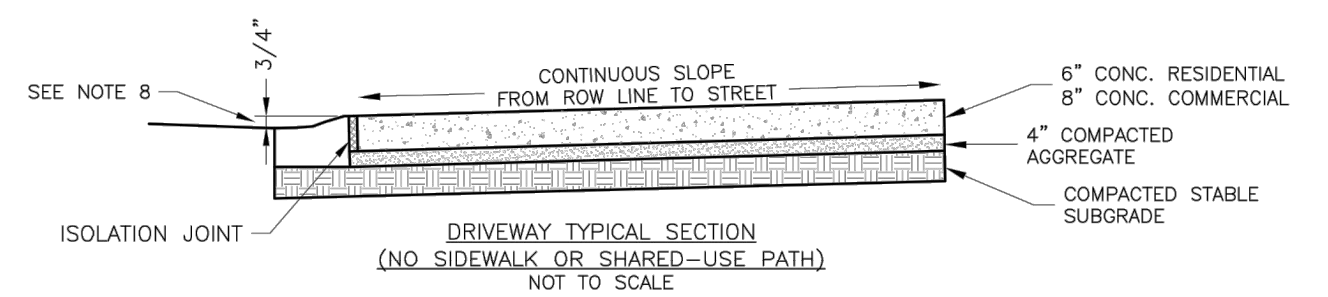


- GENERAL NOTES:
1. SUBGRADE SHALL BE STABLE, COMPACTED EARTH AND SHALL BE OVERLAYED WITH 4" COMPACTED DENSE GRADED AGGREGATE BASE.
 2. ALL DRIVE APPROACHES SHALL MEET CURRENT PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG) FOR SLOPE REQUIREMENTS WHEN SIDEWALK IS REQUIRED (SEE ADA RAMP RETROFIT DETAIL).
 3. JOINT AT BACK OF CURB LINE SHALL BE AN ISOLATION JOINT FOR RESIDENTIAL DRIVEWAYS.
 4. KCMMB 4K CONCRETE MIX IS REQUIRED FOR ALL CURBS.
 5. COMMERCIAL DRIVEWAYS AND DRIVEWAY APPROACHES, IN THE PUBLIC RIGHT OF WAY, SHALL BE KCMMB 4K CONCRETE MIX.
 6. A JOINT MUST BE INSTALLED AT THE RIGHT OF WAY BOUNDARY FOR PROPERTY DELINEATION.
 7. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.
 8. 3/4" FROM TOP OF CURB TO FLOWLINE AT DRIVEWAY (TYPE CG-1 CURB ONLY). MUST MAINTAIN ORIGINAL FLOWLINE OF CURB.
 9. SIDEWALK ADJOINING CURB SHALL BE 6" THICK, EXTENDING 3' FROM THE DRIVEWAY.
 10. THE MAXIMUM WIDTH OF A RESIDENTIAL DRIVEWAY IS 36 FEET WITHIN THE RIGHT OF WAY.

- GENERAL NOTE:
- ALL NEW CURB AND GUTTER SHALL BE INSTALLED ON AN AGGREGATE BASE, A STABILIZED SUBGRADE OR GEOGRID EXTENDING A MINIMUM OF 1 FOOT (1.0') BEHIND THE BACK OF CURB.



- GENERAL NOTES:
1. 3/4" ISOLATION JOINTS WITH 3 (2'-#5 BAR) SMOOTH DOWELS SHALL BE PLACED AT RADIUS POINTS AND AT 150' INTERVALS. THESE DOWEL BARS SHALL BE GREASED AND WRAPPED ON ONE END WITH EXPANSION TUBES.
 2. 3" DEEP CONTRACTION JOINTS SHALL BE INSTALLED AT APPROXIMATELY 10' INTERVALS. THESE JOINTS SHALL PASS ACROSS THE ENTIRE CURB SECTION.
 3. CONCRETE FILL SHALL HAVE UNIFORM AND SMOOTH FINISH
 4. KCMMB 4K CONCRETE SHALL BE USED FOR ALL CURB.
 5. ASPHALTIC CONCRETE SURFACE COURSE SHALL CONFORM TO STANDARD SPECIFICATIONS SECTION 2205.2.
 6. CURBS FOR NEW STREETS SHALL BE BUILT ON ASPHALT OR AGGREGATE BASE AS SHOWN IN TYPICAL SECTION DETAIL.
 7. WHITE CURING COMPOUND MUST BE APPLIED UNIFORMLY TO THE CONCRETE SURFACE IMMEDIATELY AFTER FINAL FINISHING.



STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

DATE: 04/17
DRAWN BY: MJF
CHECKED BY: DL
PROJ. #: 20-106

GEN-1

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

DATE: 04/17
DRAWN BY: MJF
CHECKED BY: DL
PROJ. #: 20-106

GEN-1

GEN-1

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

DATE: 04/17
DRAWN BY: MJF
CHECKED BY: DL
PROJ. #: 20-106

GEN-4

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO

DATE: 04/17
DRAWN BY: MJF
CHECKED BY: DL
PROJ. #: 20-106

GEN-4

GEN-4

CONSTRUCTION
AS NOTED ON PLANS REVIEW
DEVELOPMENT SERVICES
LEE'S SUMMIT, MISSOURI

DATE: 04/17
DRAWN BY: MJF
CHECKED BY: DL
PROJ. #: 20-106

GEN-4

JEFFREY T. SCHMIDT
REGISTERED PROFESSIONAL ENGINEER
PE-201600125
8-17-21

PREPARED BY:

TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS

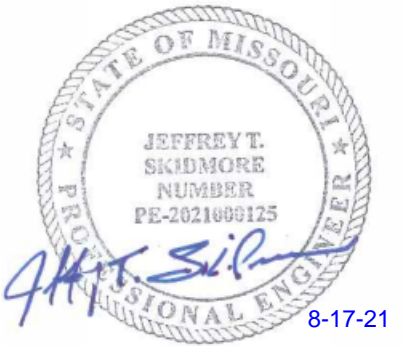
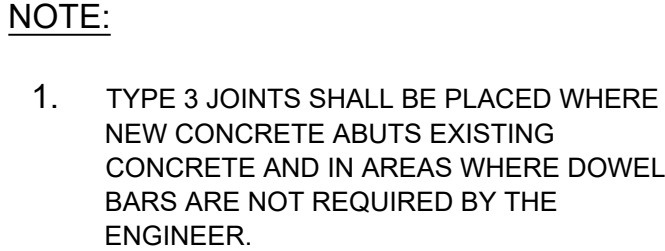
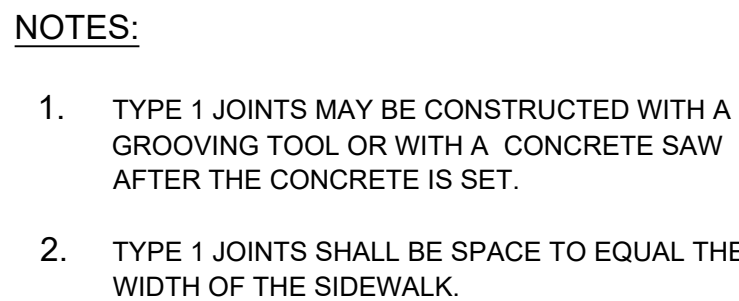
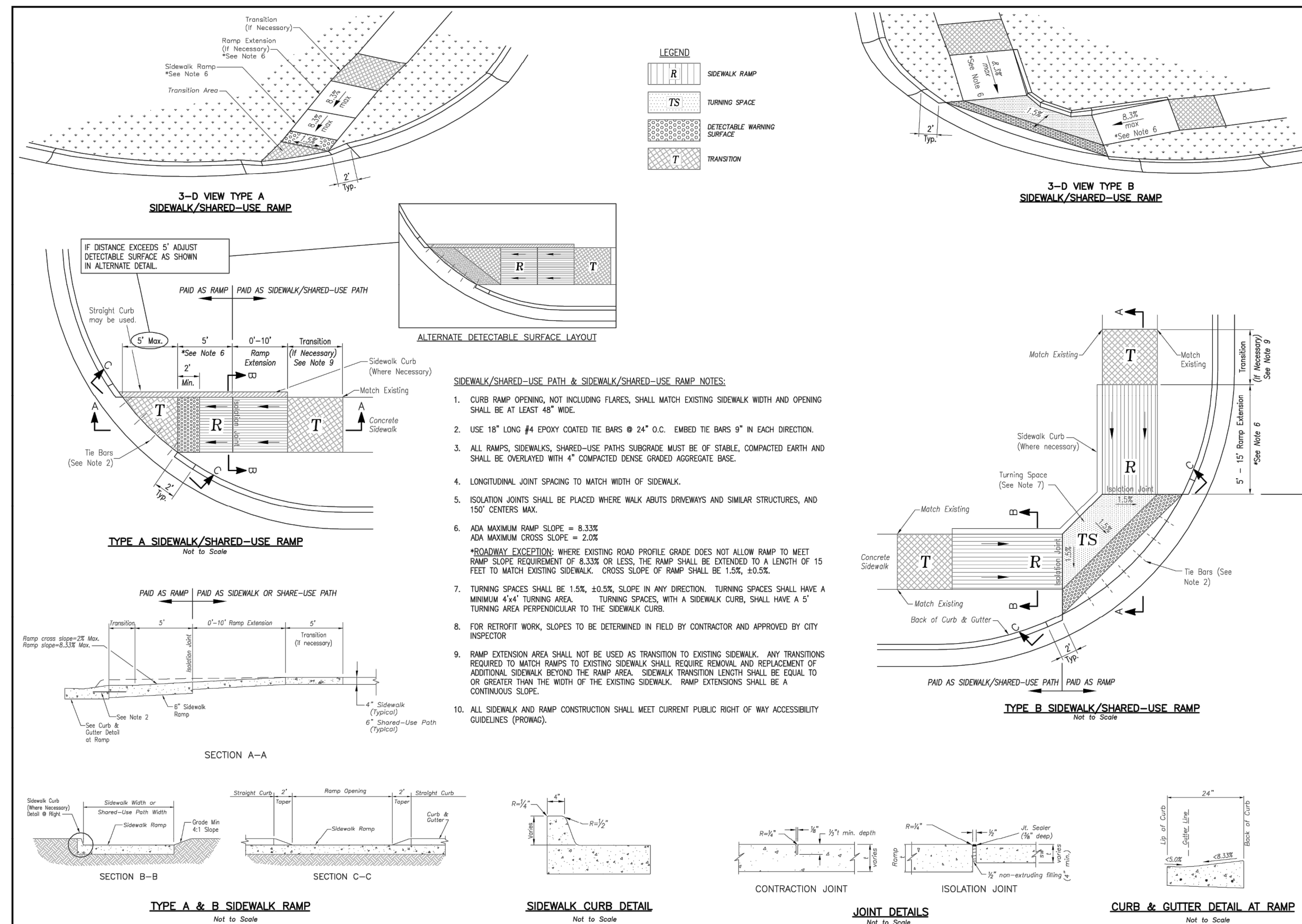
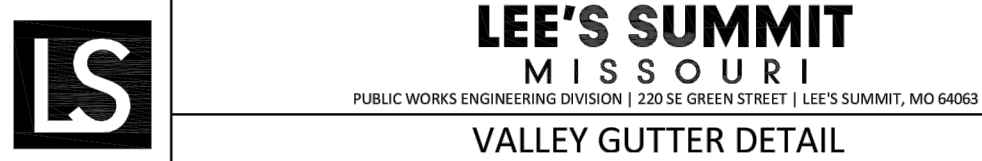
420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

REVISION DATE
1 6-28-2021 City Planning - FDP Review
2 8-17-2021 City Planning - FDP Review #2
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DRAWN BY: ###
CHECKED BY: ###
DATE PREPARED: 06/01/2021
PROJ. NUMBER: 20-106

SIDEWALK & CURB DETAILS

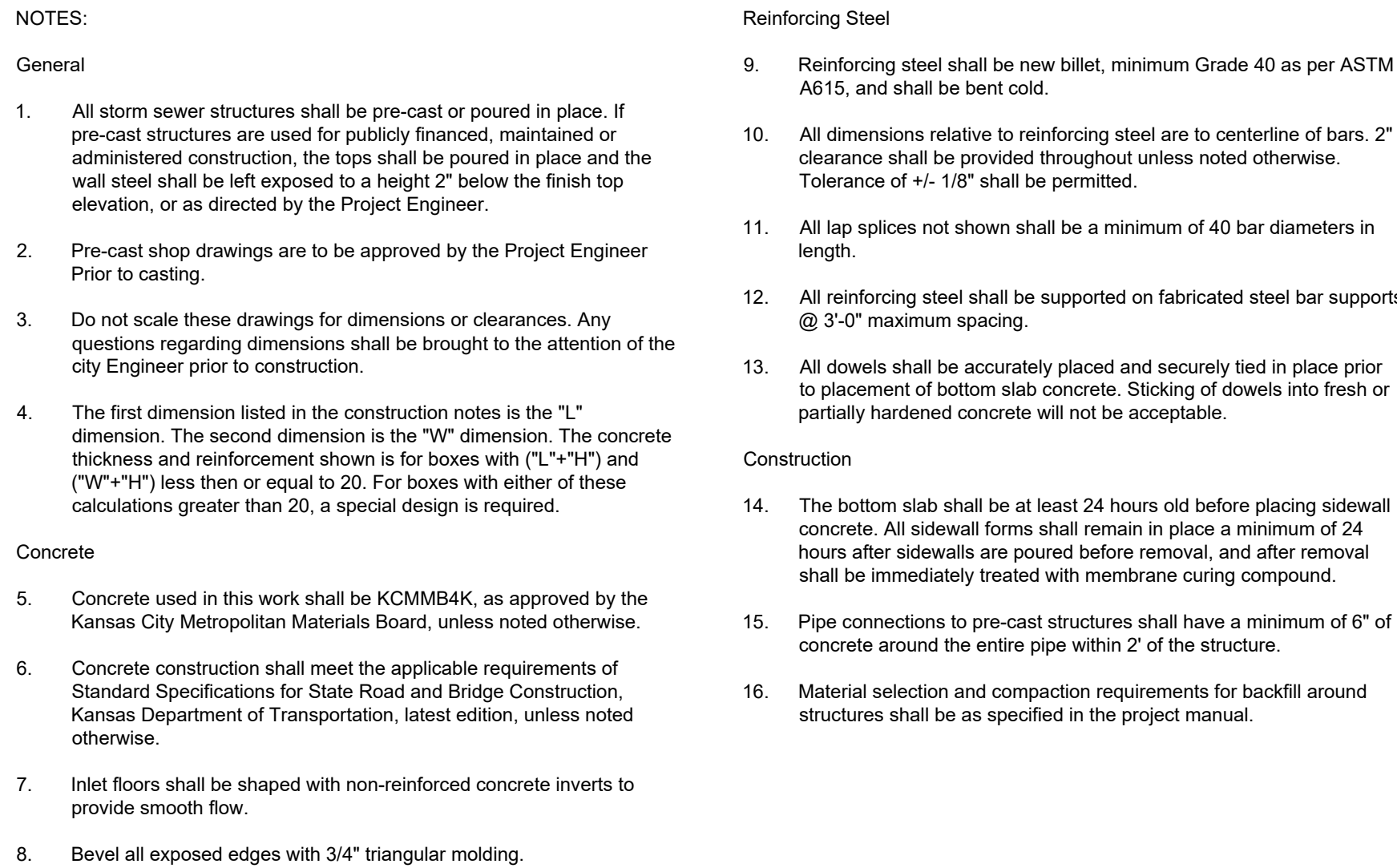
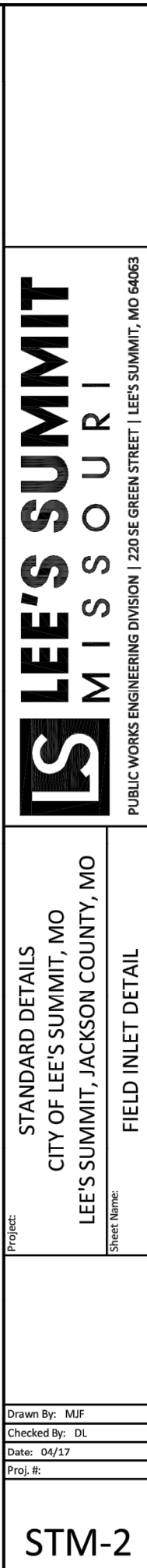
SHEET
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TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS

420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

<div> <div>PAVEMENT DETAILS</div> <div> <div>SHEET</div> <div>C8.0</div> </div> </div>	DRAWN BY:	####	REVISION DATE	DESCRIPTION
	CHECKED BY:	####	6-28-2021	City Planning - FDP Review #1
	DATE PREPARED:	06/01/2021	8-17-2021	City Planning - FDP Review #2
	PROJ. NUMBER:	20-106		



GENERAL NOTES

All manhole rings shall be set in a minimum of two (2) rows of 3/4 to 1 inch pre-formed bitumastic joint sealer.

All manhole rings to be placed in pavement or in areas to be subsequently paved shall have "Machined Horizontal Bearing Surfaces" and shall comply with Class #25 as established in ASTM A-48.

The inside diameter of the manhole shall be 4'-0" for pipe diameters from 8" thru 24" and shall be 5'-0" for pipe diameters from 27" thru 36".

All manhole bases (pre-cast or poured-in-place) shall have No. 4 reinforcing bars placed on 12" centers both ways.

All standard manhole rings and covers to be Deeter 1315-jcs
Neenah NF-15360009/B (frame) and NF-15360010/B (cover)
approved equal.

All manhole rings and covers shown in plans to be "bolt-down" to be Clay & Bailey Manufacturing Co. No. 20140R, Neenah R-1915-F2 or approved equal. An extra payment for furnishing "bolt-down" ring and cover as shown in plans will not be made, but shall be considered as subsidiary to the item, "Standard Manhole -6".

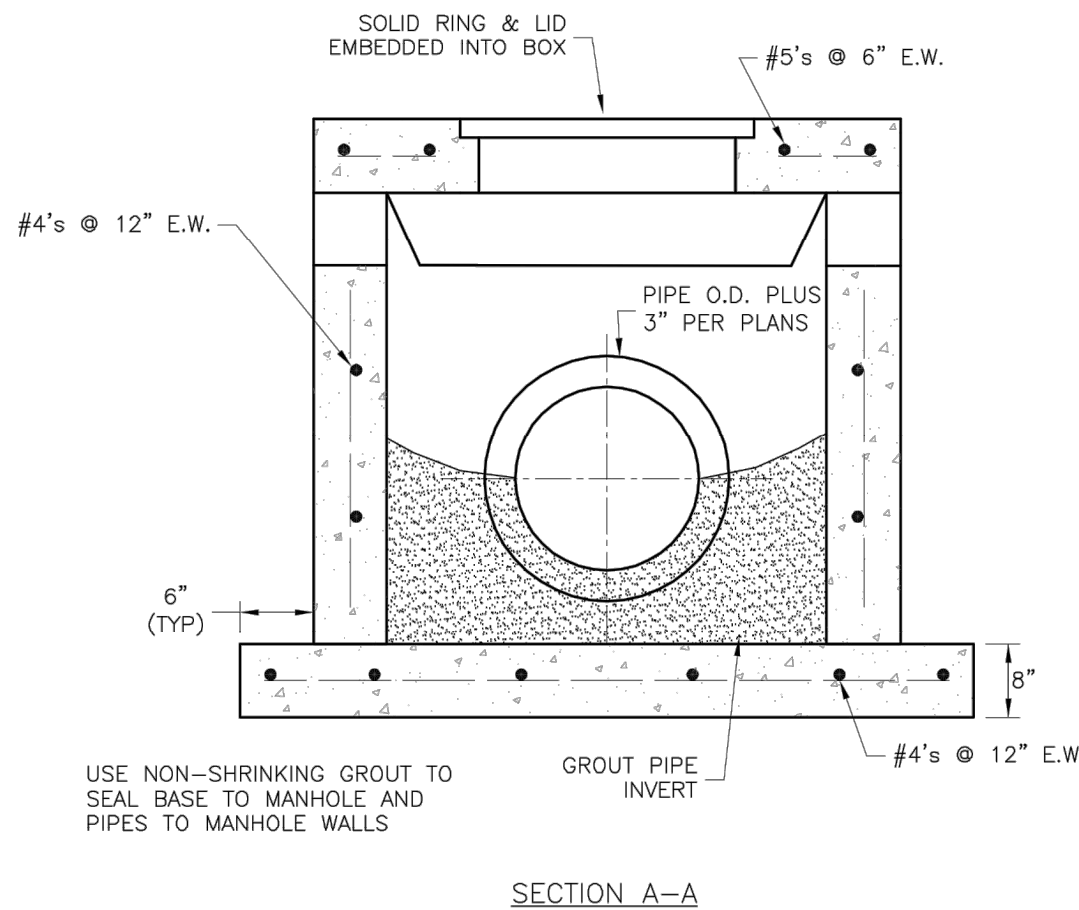
Standard cast iron manhole steps to be Clay & Bailey Manufacturing Co. No. 2102 or approved equal. Steel core, plastic coated steps may be used (M.A. Ind., Inc. No. PS1-PF, PS2-PF, or approved equal).

Maximum grade adjustment allowable is 12". Minimum allowable vertical thickness for precast concrete grade adjustment ring is 2"

Concrete used in this work shall be KCMMB4K, as approved by the Kansas City Metropolitan Materials Board.

GENERAL NOTES FOR PRE-CAST MANHOLES

1. Reinforcement in all sections shall equal or exceed A.S.T.M. C-478 specifications.
2. Mastic material to be used at all sections joints. O-Rings may be used for joints below the cone section, but the cone section itself shall not have O-ring joints.
3. Approved grout and concrete mortar to be used around pipe in knock-outs.



GENERAL NOTES

1. LOCATE RING AND COVER OVER OUTLET ON BLANK WALL.
2. USE $\frac{3}{8}$ " CHAMFER ON ALL EXPOSED CONCRETE CORNERS.
3. FLOOR OF INLET GROUTED AND SHAPED TO MATCH PIPE INVERT TO PROVIDE SMOOTH FLOW.
4. STEPS REQUIRED AT 16" O.C. WHEN DEPTH FROM TOP OF CASTING TO INVERT EXCEEDS 3' ON BLANK WALL IF POSSIBLE.
5. BOXOUTS WILL NOT BE ALLOWED TO PROJECT THROUGH THE CORNERS OF THE STRUCTURE.
6. THE MINIMUM REINFORCING SHALL BE 1 H-BAR OVER A CAST-IN-PLACE PIPE AND 2 H-BARS OVER A PRECAST BOXOUT.
7. SHOW FIELD INLET ORIENTATION ON PLANS PLUS NUMBER AND SIDE OF OPENINGS.
8. PRECAST LIDS SHALL BE PINNED, SEALED WITH NON-SHINKING GROUT AND REMOVABLE FOR FUTURE MAINTENANCE.
9. FOR RING AND COVER SEE THE STORMWATER APPROVED PRODUCT LIST.



*COVER AND FRAME MODEL INFORMATION REFER TO THE STORMWATER APPROVED PRODUCTS LIST.



*COVER AND FRAME MODEL INFORMATION REFER TO THE STORMWATER APPROVED PRODUCT LIST.

ENGINEERS

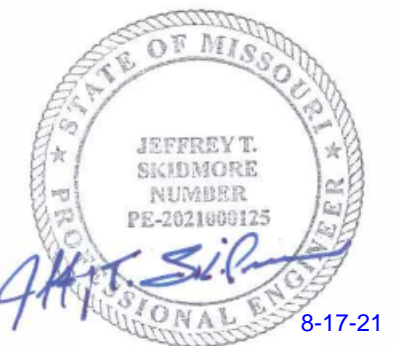
SCHLAGEL

LANDSCAPE ARCHITECTS

14320 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
Missouri State Certificate of Authority
#E2002036867 • Professional Seal #2002036867-F

RELEASE FOR
CONSTRUCTION
AS NOTED ON PLANS REVIEWED
DEVELOPMENT SERVICES
LAWRENCE, MISSOURI

06/24/2021



TOWER PARK COMM. - LOT 7 - NORTH BLDG.

420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

REVISION	DATE	DESCRIPTION
1	6-8-2021	City Planning - FDP Review
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###	06/01/2021
CHECKED BY:	PROJ. NUMBER:
###	20-106

STORM SEV DETAILS

SHEET

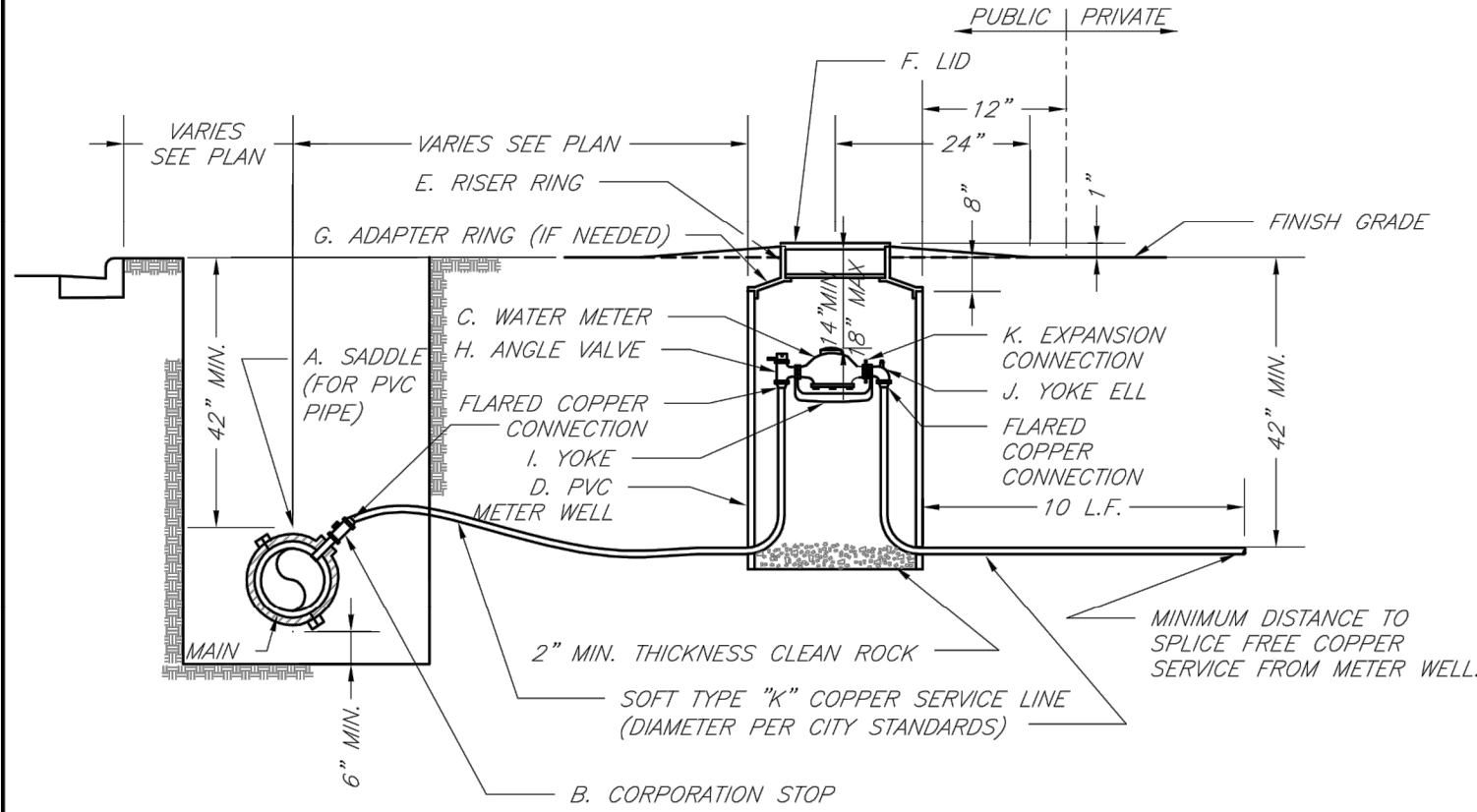
C9.0

SCHLAGEL

ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS

14920 West 107th Street • Lenexa, Kansas 66215
(913) 492-5158 • Fax: (913) 492-8400
WWW.SCHLAGELASSOCIATES.COM
Missouri State Certificates of Authority
#E20020036090-F #LAC001005237 #LS2002008659-F

PREPARED BY:



TYPICAL METER INSTALLATION — 2" AND SMALLER
NOT TO SCALE

NOTES:

1. METER INSTALLATION SHALL NOT BE LOCATED IN AREAS SUBJECT TO VEHICULAR TRAFFIC OR IN CONCRETE PAVEMENT WITHOUT CITY APPROVAL.
2. IF METER IS TO BE LOCATED OTHER THAN IN FRONT OF PROPERTY LINE, CITY APPROVAL SHALL BE OBTAINED.
3. CITY TO FURNISH ITEMS A-K.
4. NO OTHER EQUIPMENT SHALL BE INSTALLED IN THIS PIT.
5. 42" MINIMUM BURY DEPTH FOR ALL SERVICE LINES.
6. EXCAVATION FOR TAP TO EXPOSE 4' LINEAR FEET OF MAIN.
7. NO SPLICES ALLOWED BETWEEN METER AND MAIN.
8. SERVICE CONNECTION TAP AT APPROXIMATELY 45 DEGREES.
9. LID AND RISER RING SHALL BE SET SO THAT GROUND WATER WILL DRAIN AWAY FROM THE WELL.
10. CONTACT WATER UTILITIES, 816-969-1900, FOR REQUIREMENTS OF A METER LARGER THAN 2"



LEE'S SUMMIT
MISSOURI

PUBLIC WORKS ENGINEERING DIVISION | 220 SE GREEN STREET | LEE'S SUMMIT, MO 64063

SERVICE CONNECTION/METER WELL

Date: 02/13

Drawn By: JN

Checked By: DL

FILE: WAT-11

Rev: 1/14

Rev:

TOWER PARK COMM. - LOT 7 - NORTH BLDG.

FINAL DEVELOPMENT PLANS

420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

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UTILITY DETAILS

SHEET

C10.0

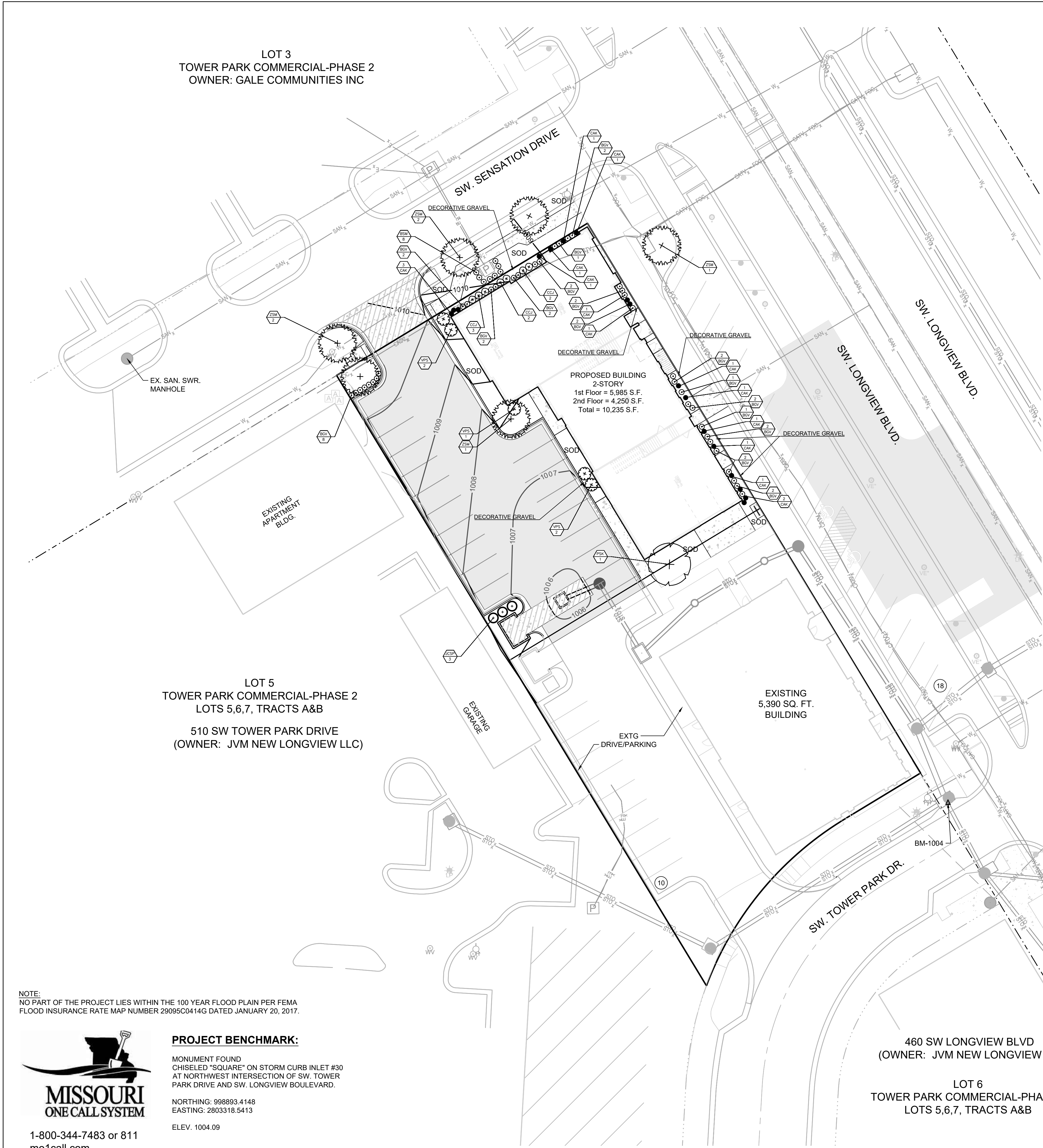
SCHLAGEL & ASSOCIATES, P.A.

TOWER PARK COMM. - LOT 7 - NORTH BLDG.
FINAL DEVELOPMENT PLANS

420 SW LONGVIEW LEE'S SUMMIT, MISSOURI

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NOTE:
NO PART OF THE PROJECT LIES WITHIN THE 100 YEAR FLOOD PLAIN PER FEMA
FLOOD INSURANCE RATE MAP NUMBER 29095C0414G DATED JANUARY 20, 2017.



PROJECT BENCHMARK:
MONUMENT FOUND
CHISELED "SQUARE" ON STORM CURB INLET #30
AT NORTHWEST INTERSECTION OF SW. TOWER
PARK DRIVE AND SW. LONGVIEW BOULEVARD.
NORTHING: 998893.4148
EASTING: 2803318.5413
ELEV. 1004.09

SHADE TREES					
	ZSM	6 EA.	Zelkova serrata 'Musashino'	Musashino Columnar Zelkova	3" Cal. B&B
ORNAMENTAL TREES					
	PSK	1 EA.	Prunus serrulata 'Kwansan'	Kwansan Flowering Cherry	1.5" Cal. B&B
EVERGREEN TREES					
	JCSP	3 EA.	Juniperus chinensis 'Spartan'	Spartan Juniper	8' ht. B&B
SHRUBS					
	BGV	35 EA.	Buxus x 'Green Velvet'	Green Velvet Boxwood	5 gal. Cont.
	BSM	8 EA.	Buxus sempervirens 'Monrue' Plant Patent #15,243	Green Tower Boxwood	5 gal. Cont.
	CCJ	7 EA.	Caryopteris x clandonensis 'Janice' PPAF	Lil Miss Sunshine™ Bluebeard	5 gal. Cont.
	VPS	5 EA.	Viburnum plicatum tomentosum 'Summer Snowflake'	Summer Snowflake Viburnum	5 gal. Cont.
GRASSES					
	CAK	16 EA.	Calamagrostis x acutifolia 'Karl Foerster'	Karl Foerster Grass	2 gal. Cont.

- NOTES:**
- UTILITY INFORMATION SHOWN IS DESIGNED LOCATION OR LOCATIONS BASED ON UTILITY LOCATES. AS BUILT LOCATIONS MAY VARY. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMMENCING LANDSCAPE INSTALLATION. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS OR OBSTRUCTIONS.
 - QUANTITIES INDICATED ON THE PLAN ARE FOR CONVENIENCE ONLY. CONTRACTOR SHALL VERIFY ALL PLANT QUANTITIES PRIOR TO PLANTING. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES. THE PLAN QUANTITIES AND NUMBER OF SYMBOLS SHALL SUPERSEDE QUANTITIES IN THE SCHEDULE.
 - ALL PLANT MATERIAL SHALL COMPLY WITH THE CITY OF LEE'S SUMMIT STANDARDS AND ANSI A60.1 THE AMERICAN STANDARD FOR NURSERY STOCK.
 - ALL TREES SHALL MEET THE SIZE REQUIREMENTS OF THE LEE'S SUMMIT ORDINANCE. ALL TREES SHALL BE CALLIPERED AND UNDERSIZED TREES SHALL BE REJECTED.
 - ALL SHRUBS TO BE UTILIZED FOR SCREENING SHALL BE 24" HEIGHT AT TIME OF PLANTING.
 - ALL PLANTING BEDS CONTAINING SHRUBS, GROUND COVER, PERENNIALS, ANNUALS SHALL BE IN A PLANTING BED WITH 3" MIN. DEPTH OF MULCH AND A "V-CUT" EDGE.
 - ALL TREES SHALL HAVE A MIN. 3 FT. DIA. AREA THAT HAS 3" MIN. DEPTH OF WOOD MULCH.
 - ALL TURF AREAS SHALL BE SODDED UNLESS INDICATED ON THE PLANS.
 - ANY DEVIATION FROM THE APPROVED LANDSCAPE PLAN SHALL REQUIRE WRITTEN APPROVAL OF THE LANDSCAPE ARCHITECT AND THE CITY OF LEE'S SUMMIT, PRIOR TO INSTALLATION.
 - THE LANDSCAPE ARCHITECT AND OWNER SHALL APPROVE GRADES AND CONDITION OF SITE PRIOR TO SODDING OPERATIONS.
 - INSTALLATION AND MAINTENANCE OF LANDSCAPING SHALL COMPLY WITH THE CITY OF LEE'S SUMMIT STANDARDS.
 - ALL PLANT MATERIAL SHALL BE INSTALLED TO ALLOW A MINIMUM CLEARANCE BETWEEN PLANT AND ADJACENT PAVEMENT OF 1 FT. FOR PERENNIALS AND GROUND COVER AND 1.5 FT. FOR SHRUBS. A 2 FT. CLEARANCE (4 FEET FROM BACK OF CURB TO THE CENTER OF SHRUB) FOR CAR OVERHANG IS REQUIRED AT ALL PARKING ISLANDS AND PERIMETERS.
 - AFTER COMPLETE INSTALLATION OF ALL PLANT MATERIAL AND SOD THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT THAT THE WORK IS COMPLETE AND READY FOR REVIEW. THE LANDSCAPE ARCHITECT SHALL REVIEW THE LANDSCAPE INSTALLATION TO DETERMINE COMPLIANCE WITH THE APPROVED PLANS. WHEN THE LANDSCAPE INSTALLATION MEETS THE REQUIREMENTS OF THE APPROVED PLAN, THE LANDSCAPE ARCHITECT SHALL PROVIDE A SIGNED AND SEALED LETTER TO THE CITY STATING THAT ALL LANDSCAPE PLANTINGS HAVE BEEN INSTALLED PER THE APPROVED PLAN.
 - ALL EXTERIOR GROUND OR BUILDING MOUNTED EQUIPMENT (MECHANICAL, ELECTRICAL AND/OR TELEPHONE CABINETS), TRANSFORMERS, AIR CONDITIONING UNITS, ETC. SHALL BE SCREENED FROM PUBLIC VIEW BY INSTALLING FIVE SEA GREEN JUNIPERS EVENLY SPACED AROUND THE PERIMETER. FINAL LOCATION OF ANY EQUIPMENT SHALL BE DETERMINED AND VERIFIED WITH THE FINAL DESIGN AND PERMITTING OF THE PROJECT.
 - ALL ROOFTOP EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW WITH AN ARCHITECTURAL TREATMENT COMPATIBLE WITH THE BUILDING AND INTEGRAL TO THE OVERALL APPEARANCE OF THE BUILDING.