FINAL DEVELOPMENT PLANS

STREETS OF WEST PRYOR

LEE'S SUMMIT, MO

CONSTRUCTION AS NOTED ON PLANS REVIEW **DEVELOPMENT SERVICES** LEE'S SUMMIT, MISSOURI

06/01/2021

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SM Engineering

Manhattan Kansas, 66503 smcivilengr@gmail.com 785.341.9747

Revisions 4-2-21 CITY COMMENTS

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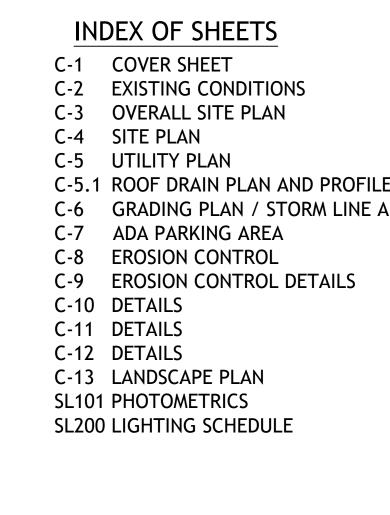
Site Improvement PLan

permit 11 MARCH 2021

C-11 DETAILS

C-13 LANDSCAPE PLAN

SL200 LIGHTING SCHEDULE



C-6 GRADING PLAN / STORM LINE A PLAN & PROFILE

C-9 EROSION CONTROL DETAILS

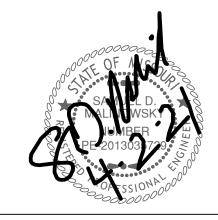
SL101 PHOTOMETRICS

DEVELOPER SWP III, LLC C/O DRAKE DEVELOPMENT, LLC 7200 W 132nd ST, SUITE 150 OVERLAND PARK, KS 66213

ENGINEER

913-662-2630

SM ENGINEERING SAM MALINOWSKY 5507 HIGH MEADOW CIRCLE MANHATTAN KANSAS, 66503 SMCIVILENGR@GMAIL.COM 785.341.9747



SAMUEL D. MALINOWSKY PROFESSIONAL ENGINEEER

Water Utilities Department 1200 SE Hamblen Road Lee's Summit, Mo 64081 Jeff Thorn 816-969-1900 jeff.thorn@cityofls.net **Communication Service**

Katie.darnell@spireenergy.com

Water/Sanitary Sewer

Nathan.michael@evergy.com

AT&T Carrie Cilke 816-703-4386 cc3527@att.com

UTILITIES

Evergy

Electric Service

Nathan Michael

913-347-4310

Gas Service

Katie Darnell 816-969-2247

Time Warner Cable Steve Baxter 913-643-1928 steve.baxter@charter.com

Comcast Ryan Alkire 816-795-2218 rvan.alkire@cable.comcast.com

Google Fiber **Becky Davis** 913-725-8745 rebeccadavis@google.com



UTILITY STATEMENT:

THE UNDERGROUND UTILITIES SHOWN HEREON ARE FROM FIELD SURVEY INFORMATION OF ONE-CALL LOCATED UTILITIES, FIELD SURVEY INFORMATION OF ABOVE GROUND OBSERVABLE EVIDENCE, AND/OR THE SCALING AND PLOTTING OF EXISTING UTILITY MAPS AND DRAWINGS AVAILABLE TO THE SURVEYOR AT THE TIME OF SURVEY. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. FURTHERMORE, THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES BY EXCAVATION UNLESS OTHERWISE NOTED ON THIS SURVEY.

SAFETY NOTICE TO CONTRACTOR

IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICE, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

WARRANTY/DISCLAIMER

THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENEDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER SM ENGINEERING NOR ITS PERSONNEL CAN OR DO WARRANTY THESE DESIGNS OR PLANS AS CONSTRUCTED, EXCEPT IN THE SPECIFIC CASES WHERE SM ENGINEERING PERSONNEL INSPECT AND CONTROL THE PHYSICAL CONSTRUCTION ON A CONTEMPORARY BASIS AT THE SITE.

CAUTION- NOTICE TO CONTRACTOR

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXPOSE EXISTING UTILITIES AT LOCATIONS OF POSSIBLE CONFLICTS PRIOR TO ANY CONSTRUCTION.

LOCATION MAP

THERE ARE NO OIL / GAS WELLS ON SITE PER ALTA SURVEY

LEGAL DESCRIPTION: LOT 1 &2 STREET OF WEST PRYOR LEE'S SUMMIT, MO, JACKSON COUNTY MISSOURI

ALL EXISTING TOPOGRAPHIC DATA AND INFRASTRUCTURE IMPROVEMENTS SHOWN BASED ON INFORMATION BY KAW VALLEY ENGINEERING

BENCHMARKS:

HWY 470

PROJECT LOCATION —

3rd STREFT

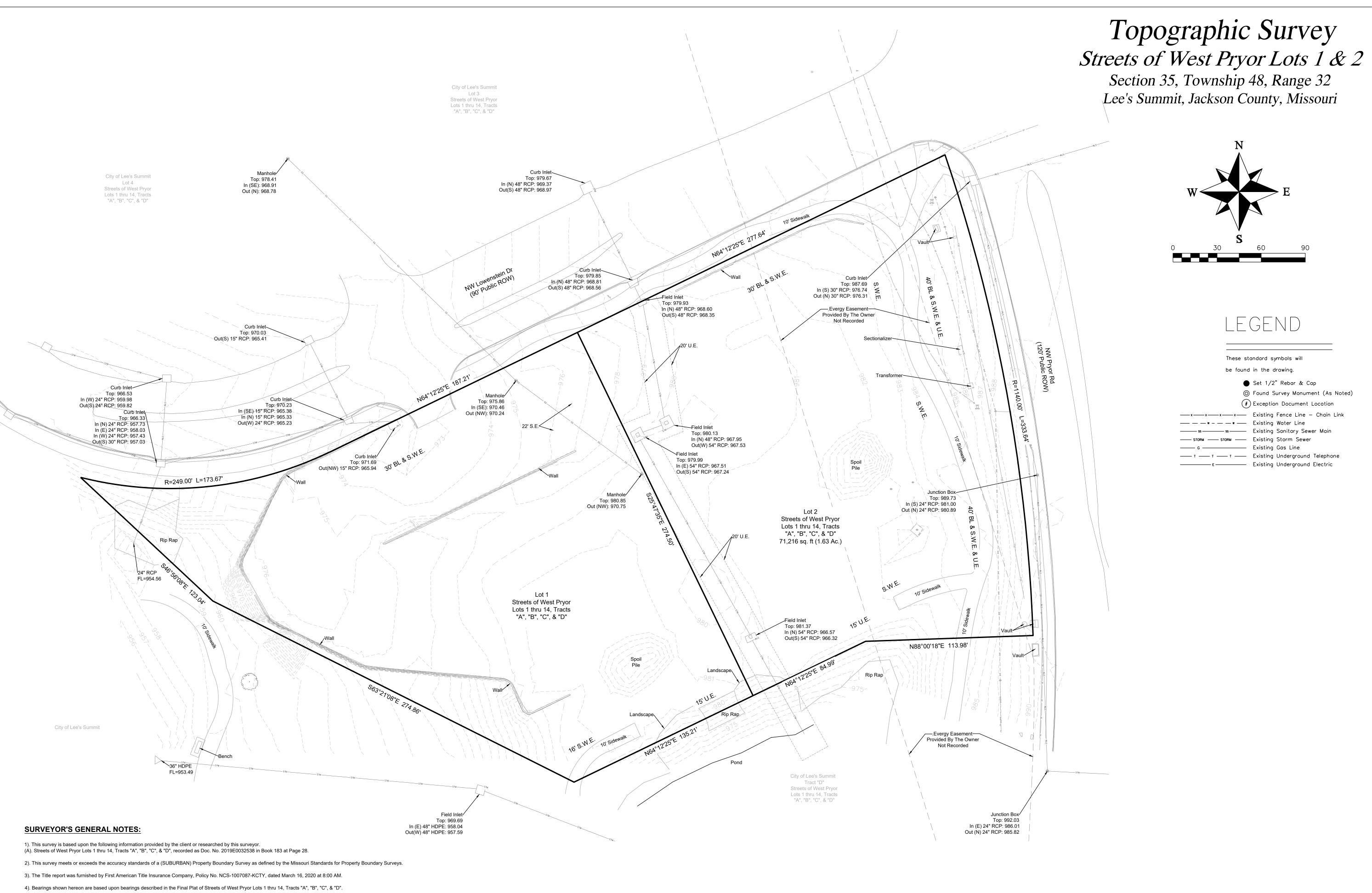
CHIPMAN ROAD

#1 CHISELED "SQUARE" ON TOP OF CURB POINT OF INTERSECTION OF WEST PARK PARKING LOT AT EAST DRIVE ENTRANCE ELEVATION 985.05

#2 CHISELED "SQUARE" ON NORTHWEST CORNER AREA INLET, 25' EAST OF CURB LINE AND ON-LINE WITH SOUTH CURB OF LOWENSTEIN DRIVE AT 90° BEND IN ROAD ELEVATION 971.06

FLOODPLAIN NOTE:

SUBJECT PROPERTY IS SHOWN TO BE LOCATED IN "OTHER AREAS ZONE X" ON THE FLOOD INSURANCE RATE MAP FOR JACKSON COUNTY, MISSOURI AND INCORPORATED AREAS. COMMUNITY PANEL NO. 29095C0416G, REVISED JANUARY 20, 2017. "OTHER AREAS ZONE X" IS DEFINED AS "AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUL CHANCE FLOODPLAIN". LOCATION DETERMINED BY A SCALED GRAPHICAL PLOT OF THE FLOOD INSURANCE RATE MAP.



Lee's Summit, Jackson County, Missouri

● Set 1/2" Rebar & Cap Found Survey Monument (As Noted)

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DEVELOPMENT SERVICES
LEE'S SUMMIT, MISSOURI

PROFESSIONAL SEAL

PROPERTY DESCRIPTION

5). This company assumes no responsibility in the location of existing utilities within the subject premises. This is an above-ground survey. The underground utilities, if shown, are based on information provided by the various utility companies and these locations should be considered approximate. There may be additional underground utilities not shown on this drawing.

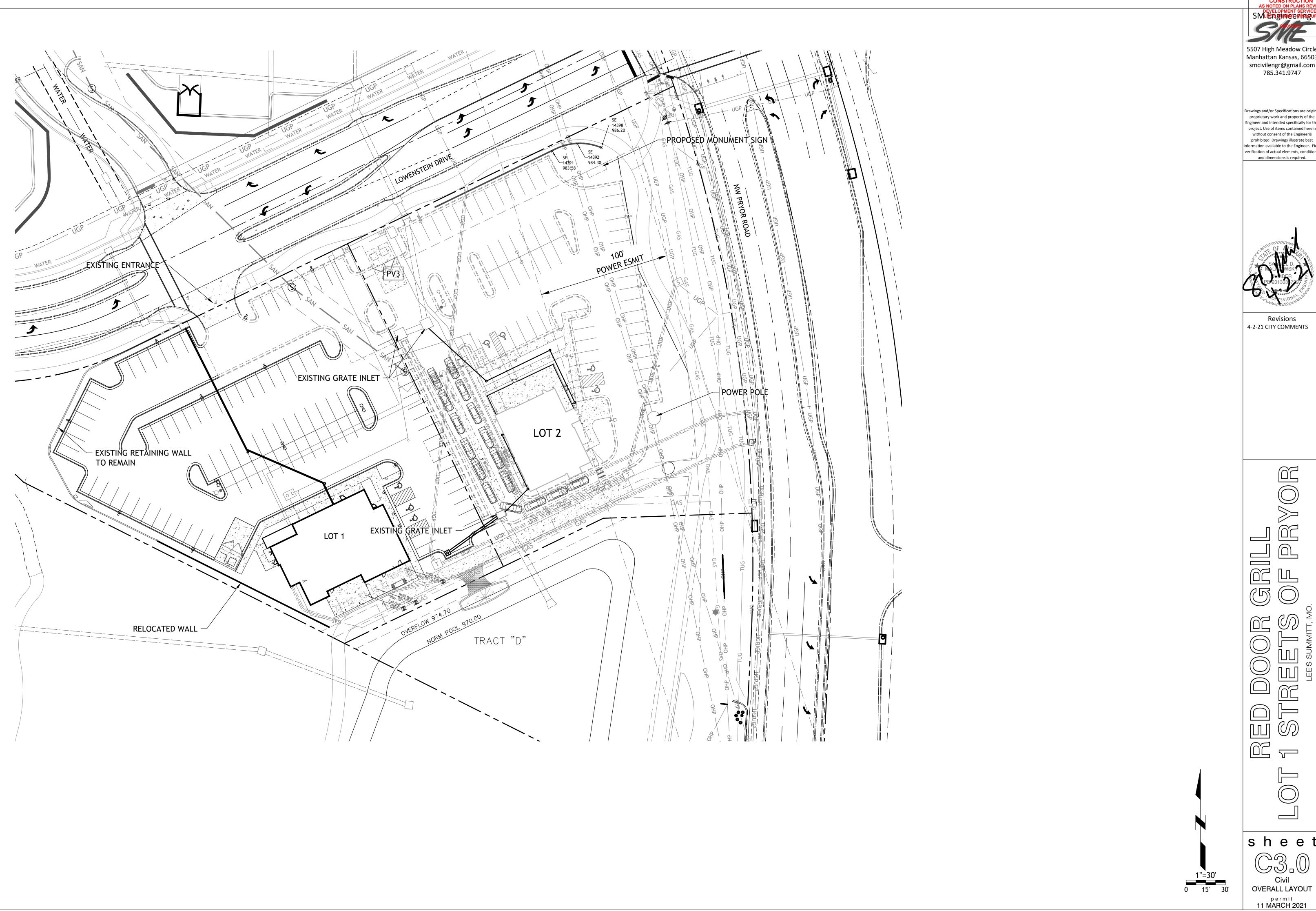
6). Subsurface and environmental conditions were not surveyed or examined or considered as a part of this survey. No evidence or statement is made concerning the existence or underground or overhead conditions, containers or facilities that may affect the use or development of this property. No attempt has been made to obtain or show data concerning

existence, size, depth, conditions, capacity or location of any utility existing on the site, whether private, municipal or public owned.

7) This property is located outside the 100 year flood plain, zone "x" as shown on the Firm panel 29095C0416G, dated January 20, 2017.

-Locate Ticket # 210210384

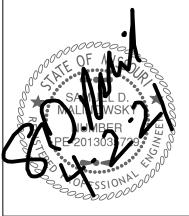
- Lot 1, Streets of West Pryor, Lots 1 thru 14, Tracts "A", "B", "C", & "D", A Subdivision in the City of Lee's Summit, Jackson County, Missouri.
- Lot 2, Streets of West Pryor, Lots 1 thru 14, Tracts "A", "B", "C", & "D", A Subdivision in the City of Lee's Summit, Jackson County, Missouri.



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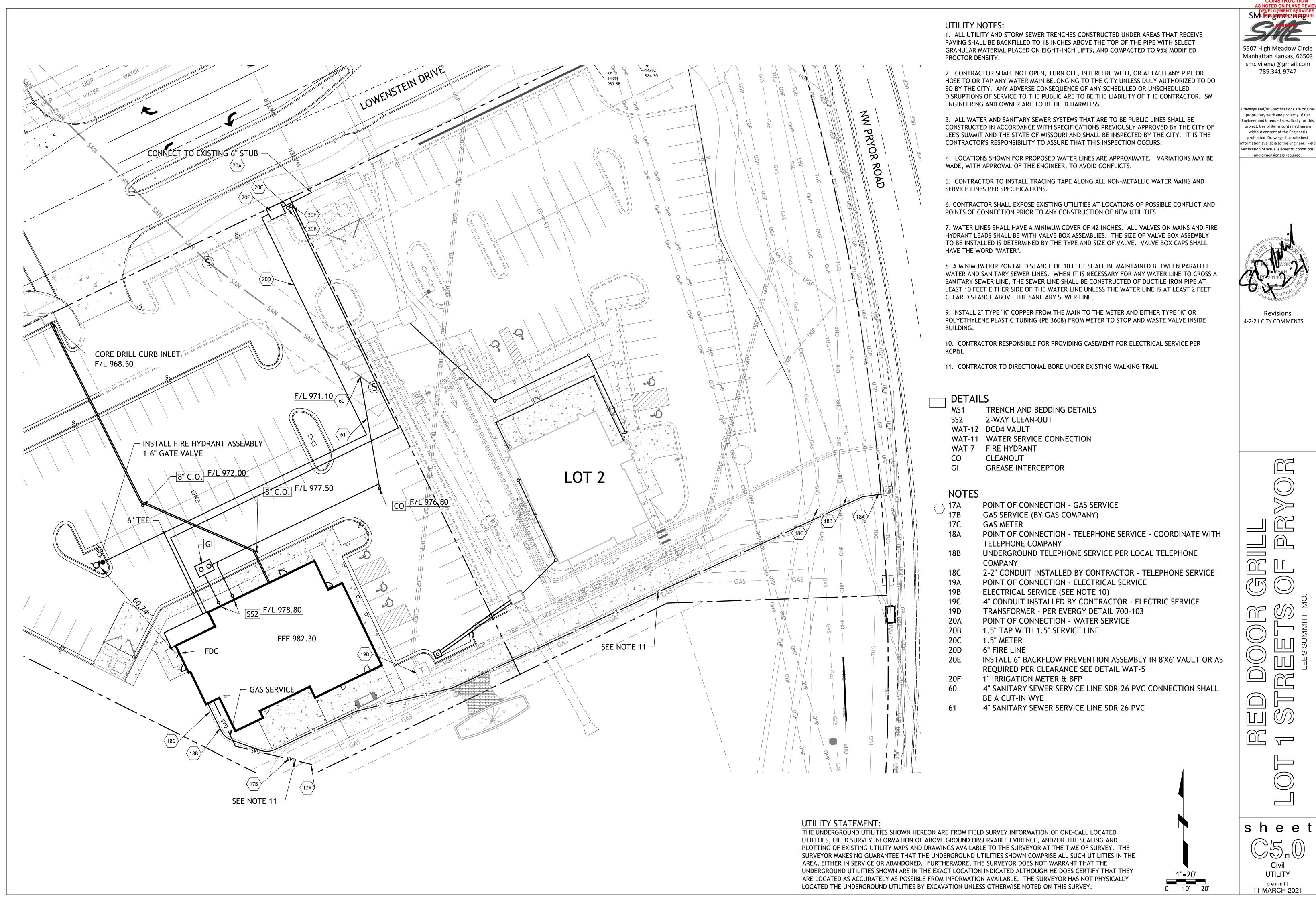
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													SITE DATA TOTAL SITE TOTAL IMPERVIOUS	·	AS NOTED ON PLANS DEVELOPMENT SEI SINLEEN SUMMER MIS
	OT AREA LO	OT AREA T	SITE DATA BLDG AREA SF	# OF	PARKING	PARKING	ADA SPACES	ADA SPACES	PRK LOT	TOTAL IMPERVIOUS	OPEN SPACE		OPEN SPACE TOTAL BUILDING	18,227sf (51.5%) 5,549sf	5507 High Meadow (Manhattan Kansas, 6 smcivilengr@gmail.
LOT USE		(ACRES)	(ENVELOPE)	FLOORS				PROV (VAN)	COVERAGE	COVERAGE	OT EN STACE		FAR PARKING REQUIRED	0.081TOTAL REQUIRED 78	785.341.9747
LOT 1 MIXED USE	67,758	1.56	5,549	1	78	105	1	2	43982	49,531 V DRIVE	18,227		PARKING PROVIDED	113	
									LOWENSTEIN					-UP AND ALL CONSTRUCTION ACTIVITIES WITH OW	VNER. without consent of the Engine prohibited. Drawings illustrate information available to the Engine verification of actual elements, co
													ARE TO MEET OR EXCE	THODS AND MATERIALS NOT SPECIFIED IN THESE PIED THE STANDARD SPECIFICATIONS.	
													BOUNDARIES SHALL BE	I WORK AND UTILITY WORK OUTSIDE OF PROPERTY PERFORMED IN COOPERATION WITH AND IN EGULATIONS OF THE AUTHORITIES CONCERNED.	
		EXIST	FING ENTRANCE $oldsymbol{ o}$		SW	LP)	9						WORK IN A MANNER TO TRA	CE AND SAFETY: THE CONTRACTOR SHALL CONDUCTION HAT WILL INSURE, AS FAR AS PRACTICABLE, THE LIGHT OF THE LIGHT	EAST E AND
							PV1		4				5. ALL DIMENSIONS SH NOTED.	OWN ARE TO THE BACK OF CURB UNLESS OTHERW	PE-2013031729
						S	EXISTING	G GRATE INLET						SHOWN WITH A "VAN" SHALL BE 16'-0" MIN. AND SI TING "VAN-ACCESSIBLE". SEE DETAIL102.	HALL Revisions 4-2-21 CITY COMMENT
		LP	· · · · · 7		(LP)		17	Say 19				· · · · · · · · · · · · · · · · · · ·	AND DIMENSIONS OF E	L REFER TO ARCHITECTURAL PLANS FOR EXACT LO NTRANCE. SLOPED PAVING, EXIT PORCHES AND RA ENSIONS AND EXACT BUILDING UTILITY ENTRANCE	MPS,
		R5.0'	PV2		·5.		R3.0	R10.0′		CANTAL MARINE MA			CONTRACT DRAWINGS OF ANY DISCREPANCIE	NOT BEEN VERIFIED WITH FINAL ARCHITECTURAL . CONTRACTOR SHALL VERIFY AND NOTIFY THE EN- S. CONTRACTOR IS FULLY RESPONSIBLE FOR REVIE L DRAWINGS AND CONTRACTOR DOCUMENTS.	GINEER
	7.0.53	CG1	LP	3		(1)	2K)	CG1			4 4 4 4		3. ALL DIMENSIONS AR	E PERPENDICULAR TO PROPERTY LINE.	
(LP)		P70.0.		9.0		2A.O.	LP 1	14			1.00		4. ACTUAL SIGN LOCA MANAGER.	TIONS TO BE COORDINATED WITH CONSTRUCTION	
P\	V1 24.0,		PV1		PV2			13 R _{10.0}	0'			LOT 2			
)}	R10.0'		R> 0. 7	0	PV	1		B	OO LP						
	DP5 0'	R6.0.	CG1 CG1	8", ROO	E DRAIN		(11)	4	PV2						
	/13/		PV2 R10.0'				io.		R5.0'	PV1 13.0					
			0.00	LP	PV2		PO.	PK2	PV1 PK1	14.0		TOP TOP			
			PV1		P\$:0/		\	RIB PK2 5.1	19.00	ALC CDATE IN ET		G			
(LP					PV3		cw2 LOT	- 1 PK2	EXISTI	R5.0	JUGP 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4	GAS			
							105 ST	ALLS CW1	5	O UGP A A A	GA			R THE FOLLOWING DETAILS: E & VAN ACCESSIBLE SPACE STRIPING SN	
			LP .						Flex	to do	GAS		CG-1 CURB AND GUT CW1 CURB WALK AT	TER BUILDING	
			TE	E \			(A)		4 99%				PV1 REGULAR DUTY PV2 HEAVY DUTY AS PV3 HEAVY DUTY CO	SPHALT PAVEMENT	
						3	0 0	Elev	Eley, 99"2 Arz" (SA SA S					P RAMP SEE GEN-3A DETAIL SHEET C9.0 AND APS SHEET C7.0	
							200	SV5					TE TRASH ENCLOS		
													NOTES: 8A DOOR (SEE ARC	H. PLANS) NG LOT STRIPING (SHERWIN-WILLIAMS	s h e e
													TM 2160 LEAD CO CLEAN-OUT (SE 11 PAINT CURB RE	FREE OR APPROVED EQUAL)	Civil SITE PLAN permit 11 MARCH 202

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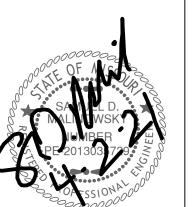


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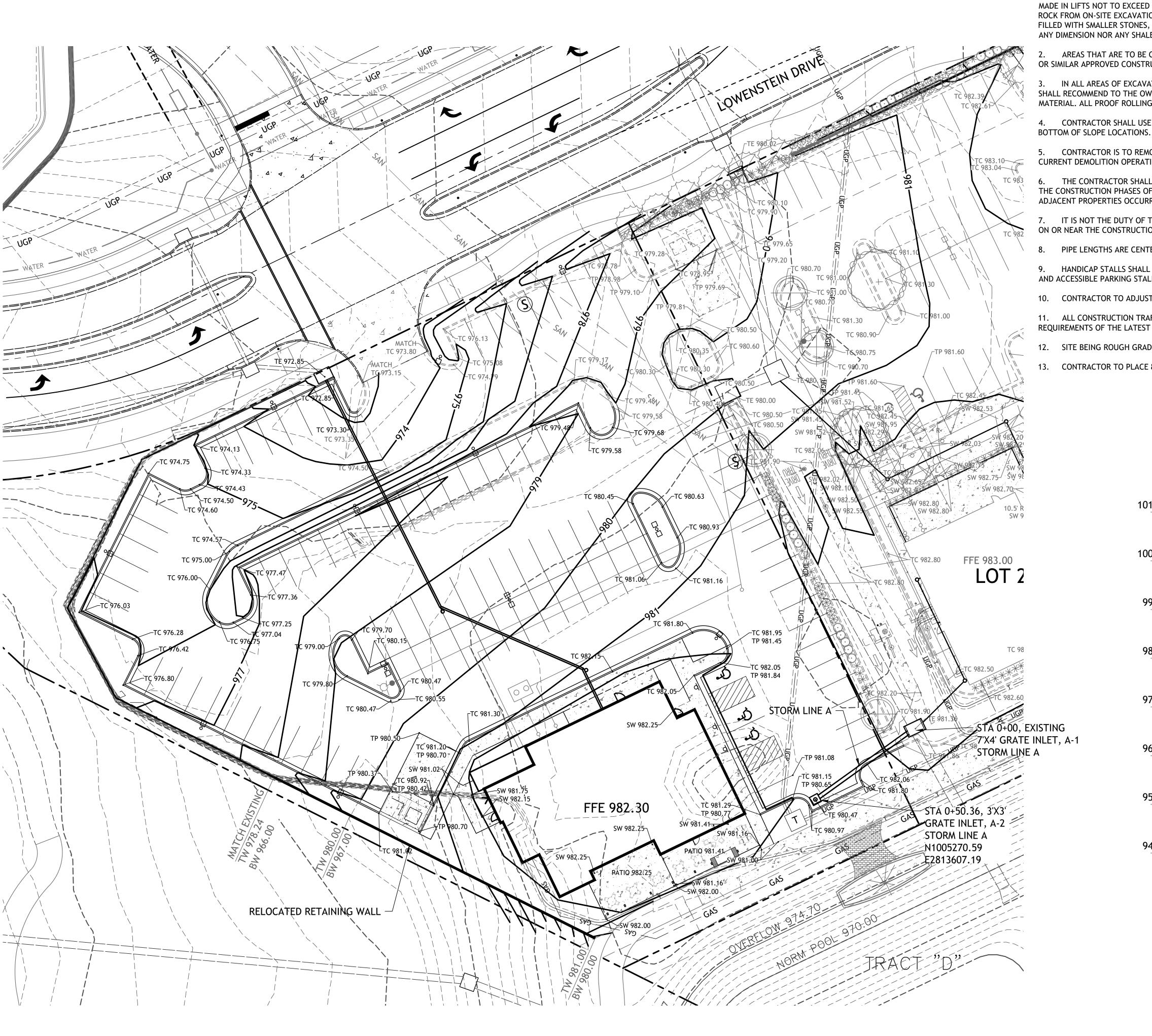




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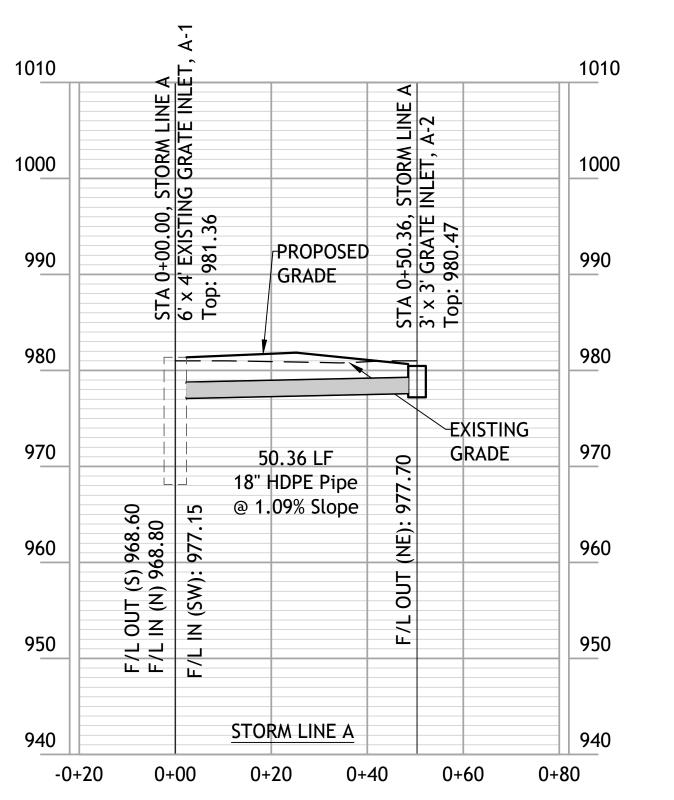


GRADING NOTES:

1. EARTHWORK UNDER THE BUILDING SHALL COMPLY WITH THE PROJECT ARCHITECTURAL PLANS. OTHER FILL MATERIAL SHALL BE MADE IN LIFTS NOT TO EXCEED EIGHT INCHES DEPTH COMPACTED TO 95% STANDARD PROCTOR DENSITY. FILL MATERIAL MAY INCLUDE ROCK FROM ON-SITE EXCAVATION IF CAREFULLY PLACED SO THAT LARGE STONES ARE WELL DISTRIBUTED AND VOIDS ARE COMPLETELY FILLED WITH SMALLER STONES, EARTH, SAND OR GRAVEL TO FURNISH A SOLID EMBANKMENT. NO ROCK LARGER THAN THREE INCHES IN ANY DIMENSION NOR ANY SHALE SHALL BE PLACED IN THE TOP 12 INCHES OF EMBANKMENT.

2. AREAS THAT ARE TO BE CUT TO SUBGRADE LEVELS SHALL BE PROOF ROLLED WITH A MODERATELY HEAVY LOADED DUMP TRUCK OR SIMILAR APPROVED CONSTRUCTION EQUIPMENT TO DETECT UNSUITABLE SOIL CONDITIONS.

- 3. IN ALL AREAS OF EXCAVATION, IF UNSUITABLE SOIL CONDITIONS ARE ENCOUNTERED. A QUALIFIED GEOTECHNICAL ENGINEER SHALL RECOMMEND TO THE OWNER THE METHODS OF UNDERCUTTING AND REPLACEMENT OF PROPERLY COMPACTED, APPROVED FILL MATERIAL. ALL PROOF ROLLING AND UNDERCUTTING SHOULD BE PERFORMED DURING A PERIOD OF DRY WEATHER.
- 4. CONTRACTOR SHALL USE SILT FENCE OR OTHER MEANS OF CONTROLLING EROSION ALONG THE EDGE OF THE PROPERTY OR OTHER
- 5. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS.
- 6. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- 7. IT IS NOT THE DUTY OF THE ENGINEER OR THE OWNER TO REVIEW THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE AT ANY TIME DURING CONSTRUCTION.
- 8. PIPE LENGTHS ARE CENTER TO CENTER OF STRUCTURE OR TO END OF END SECTIONS.
- 9. HANDICAP STALLS SHALL MEET ADA REQUIREMENTS AND SHALL NOT EXCEED 2% SLOPE IN ANY DIRECTION AT THE BUILDING ENTRY AND ACCESSIBLE PARKING STALLS. SLOPES EXCEEDING 2.0% WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- 10. CONTRACTOR TO ADJUST DEPTHS OF EXISTING SERVICE LINES AS NECESSARY
- 11. ALL CONSTRUCTION TRAFFIC, TEMPORARY TRAFFIC CONTROL DEVICES AND PAVEMENT MARKINGS SHALL CONFORM TO REQUIREMENTS OF THE LATEST MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- 12. SITE BEING ROUGH GRADED TO 12.5" BELOW FINISHED GRADE
- 13. CONTRACTOR TO PLACE 8" LOW PERMEABILITY LVC FOR BUILDING PAD



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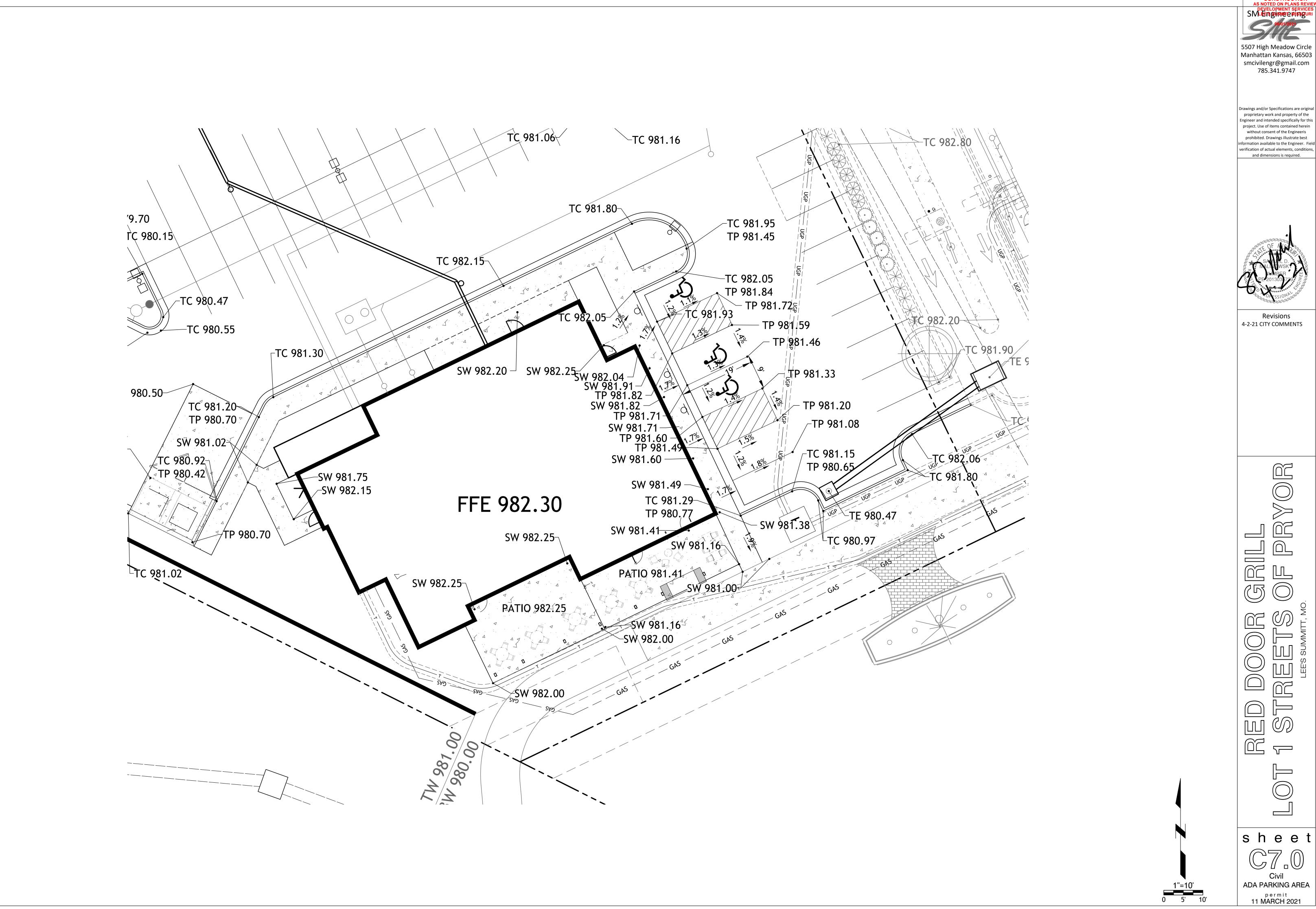
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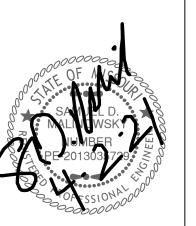
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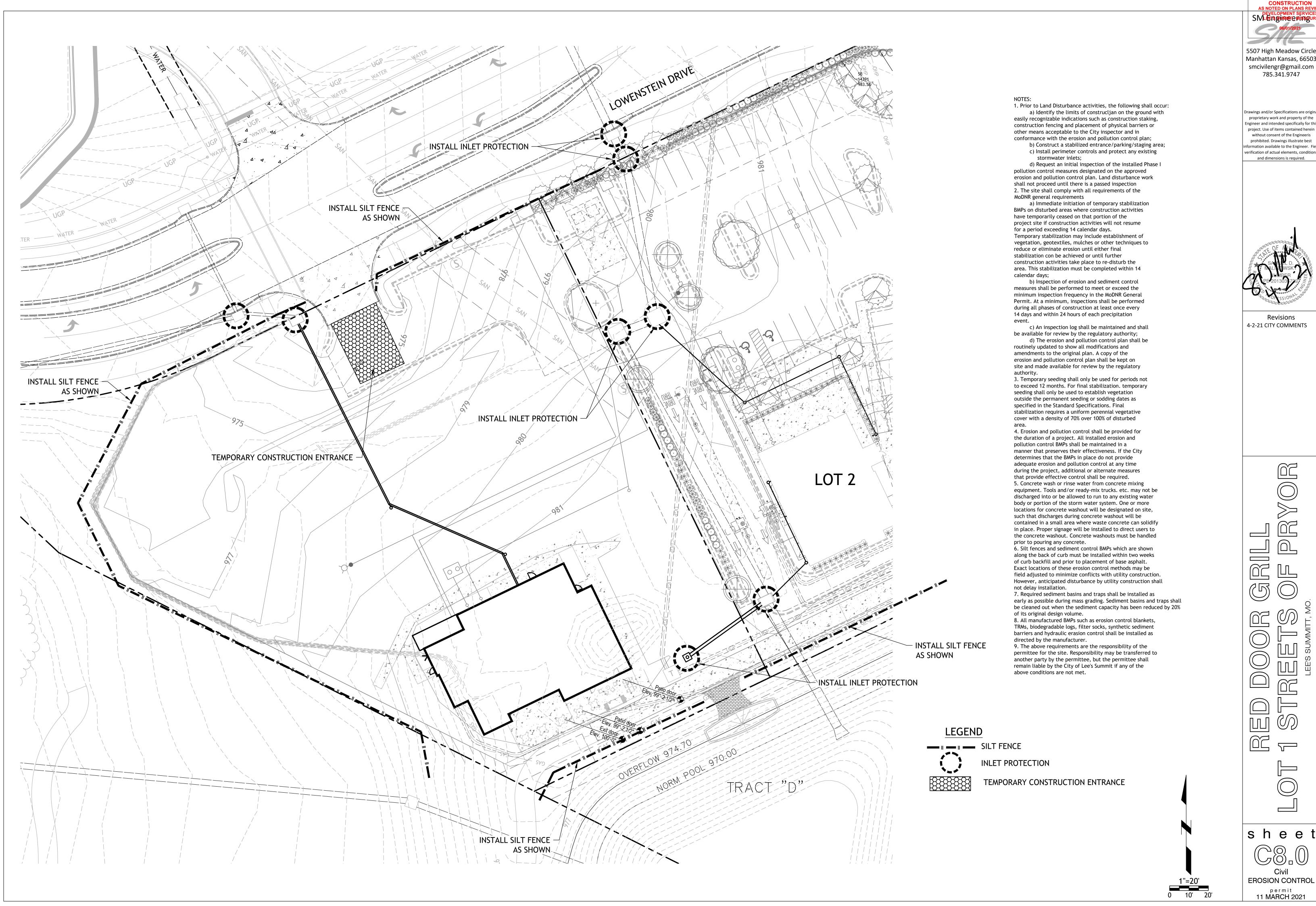


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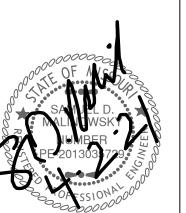


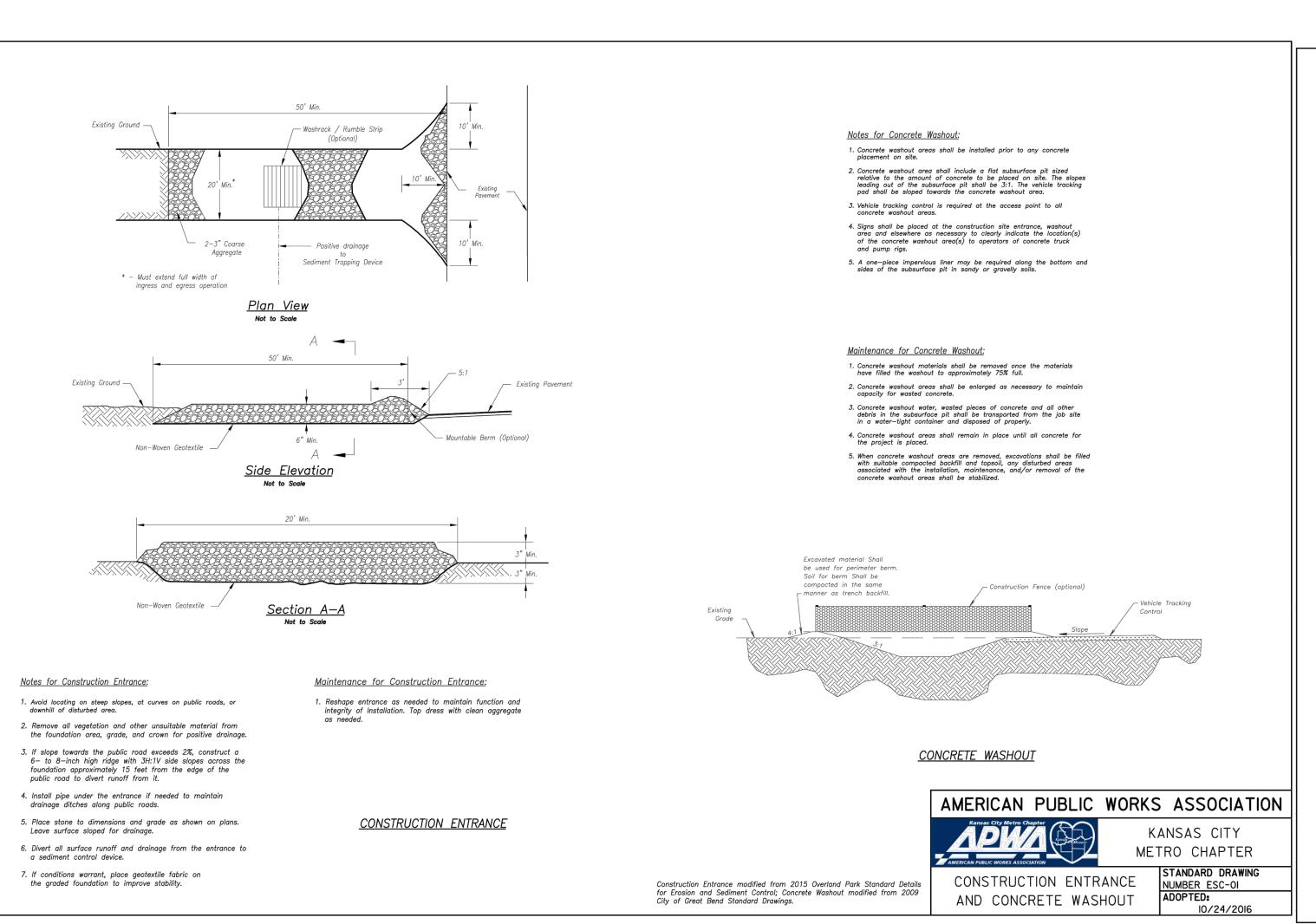


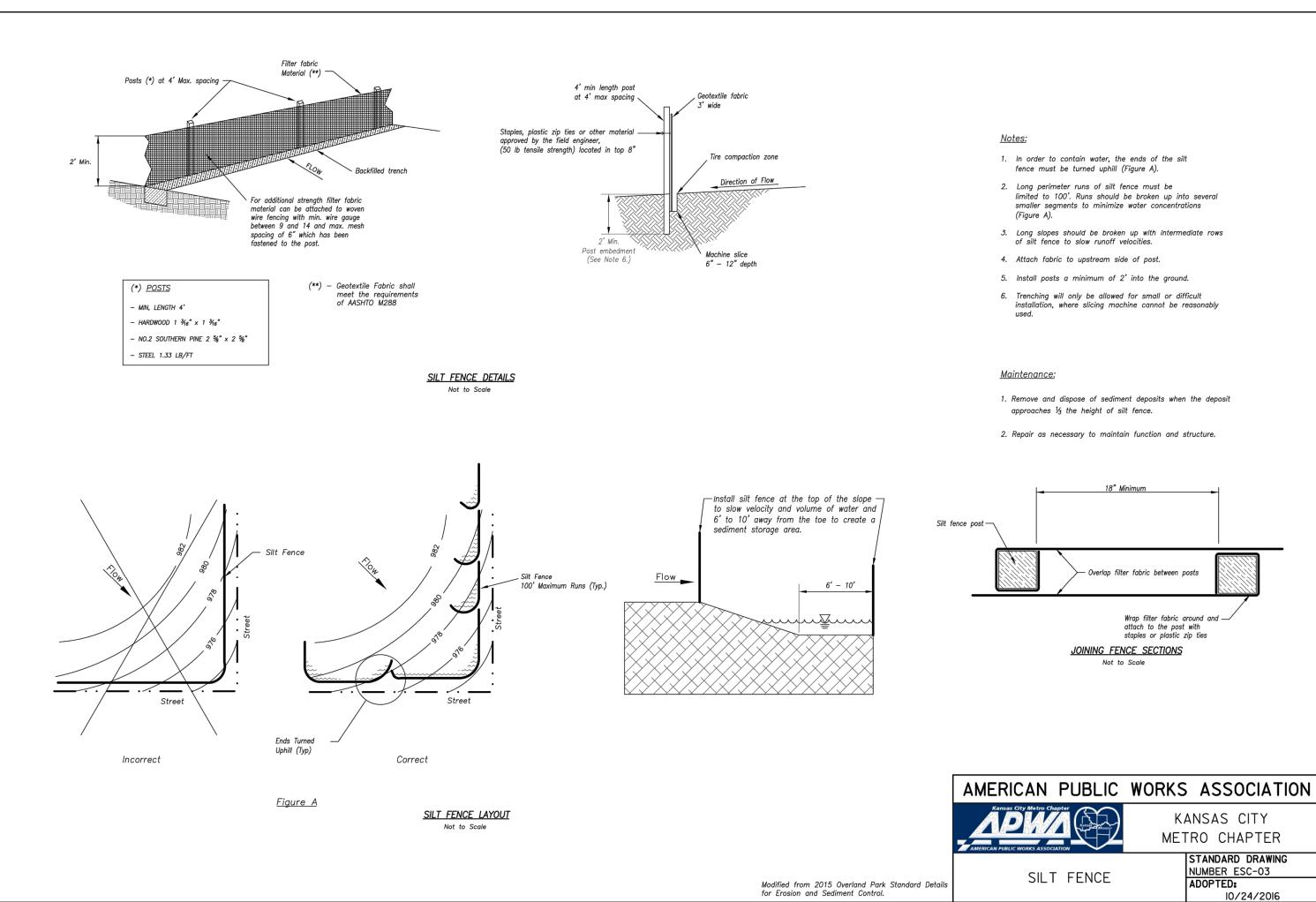
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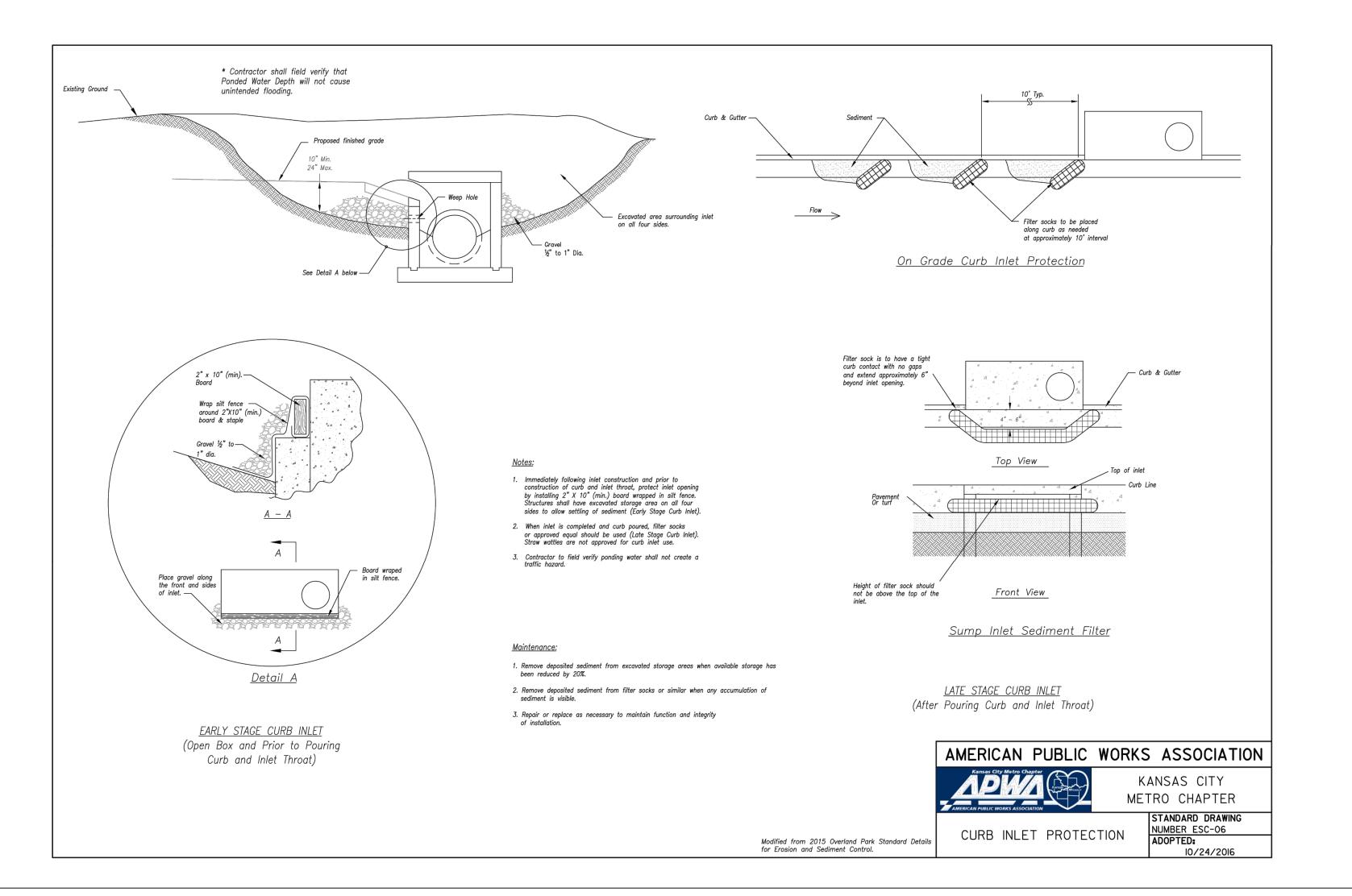
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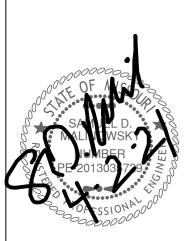


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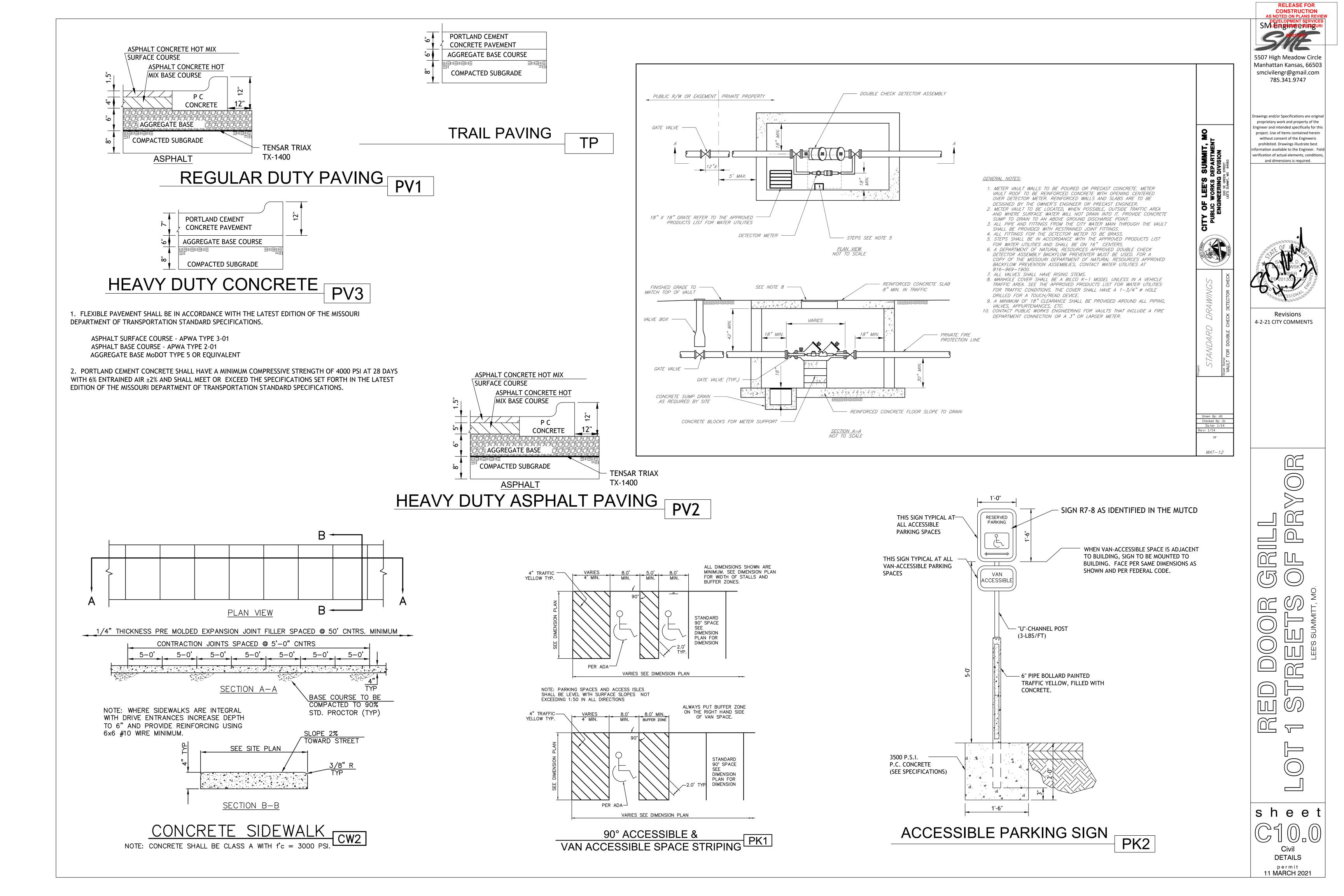
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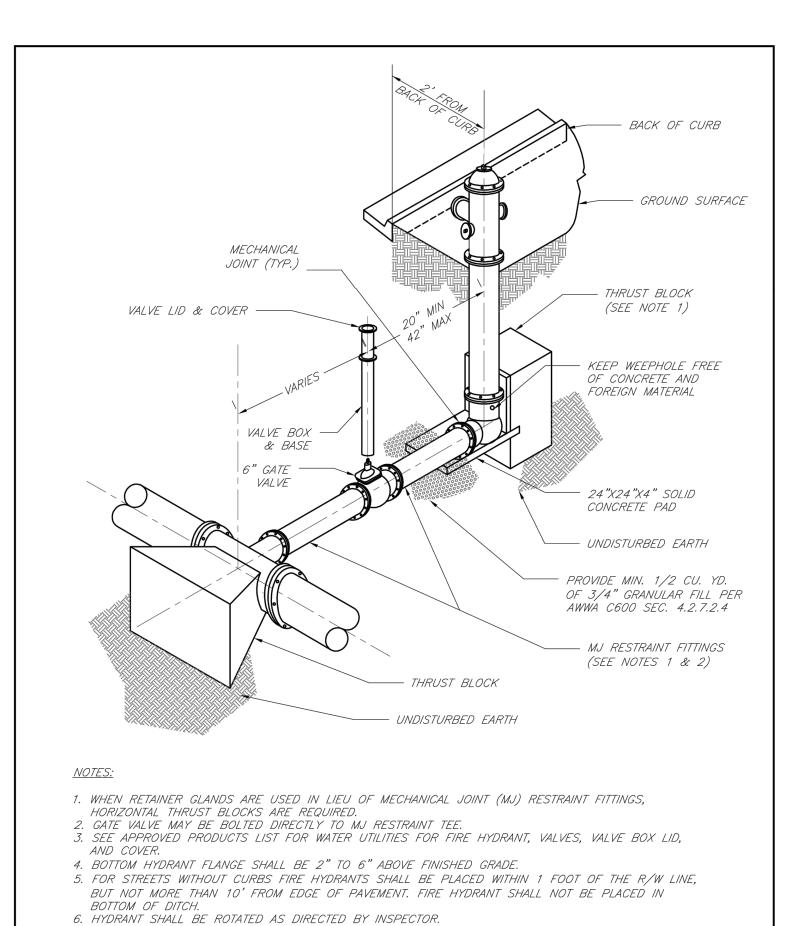
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11 MARCH 2021

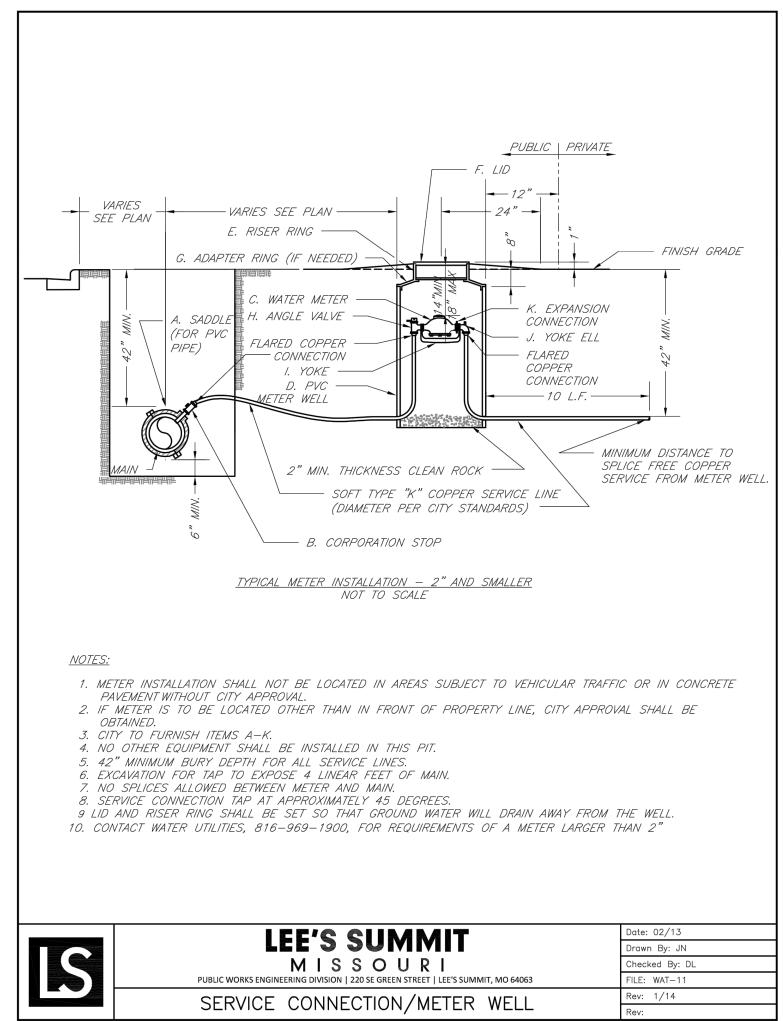


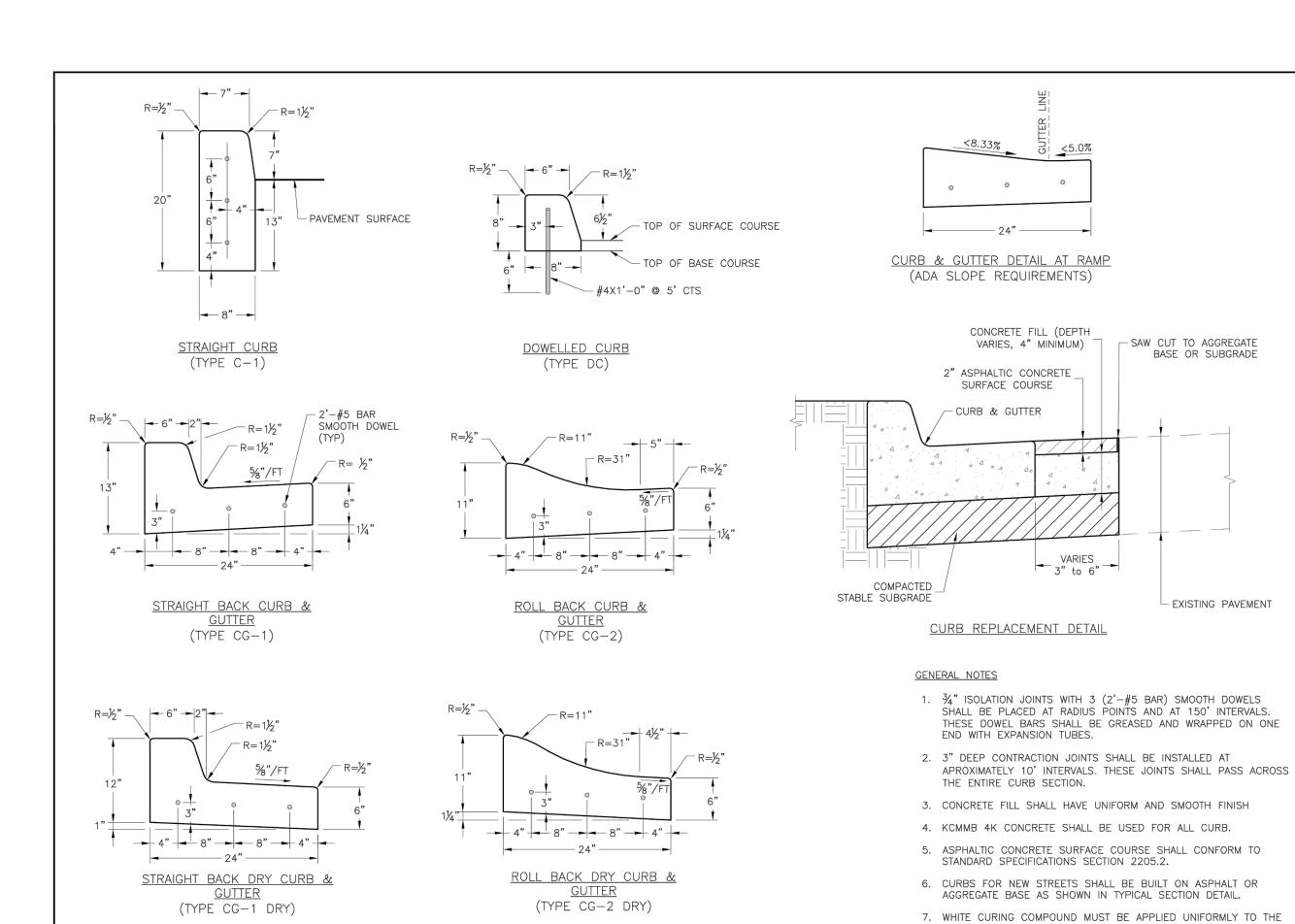


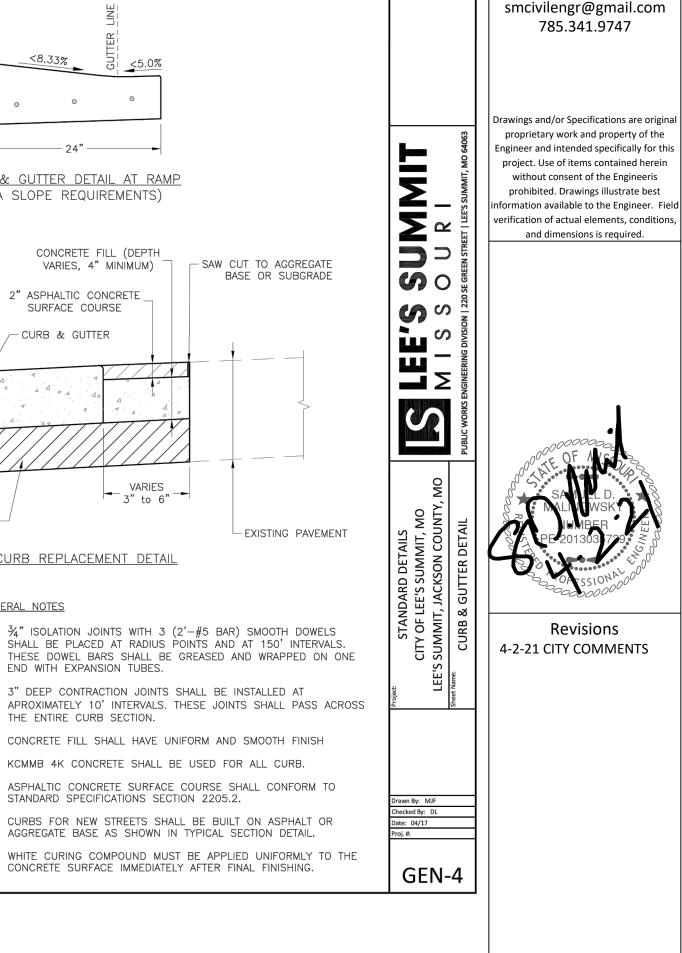
LEE'S SUMMIT

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HYDRANT INSTALLATION - STRAIGHT SET

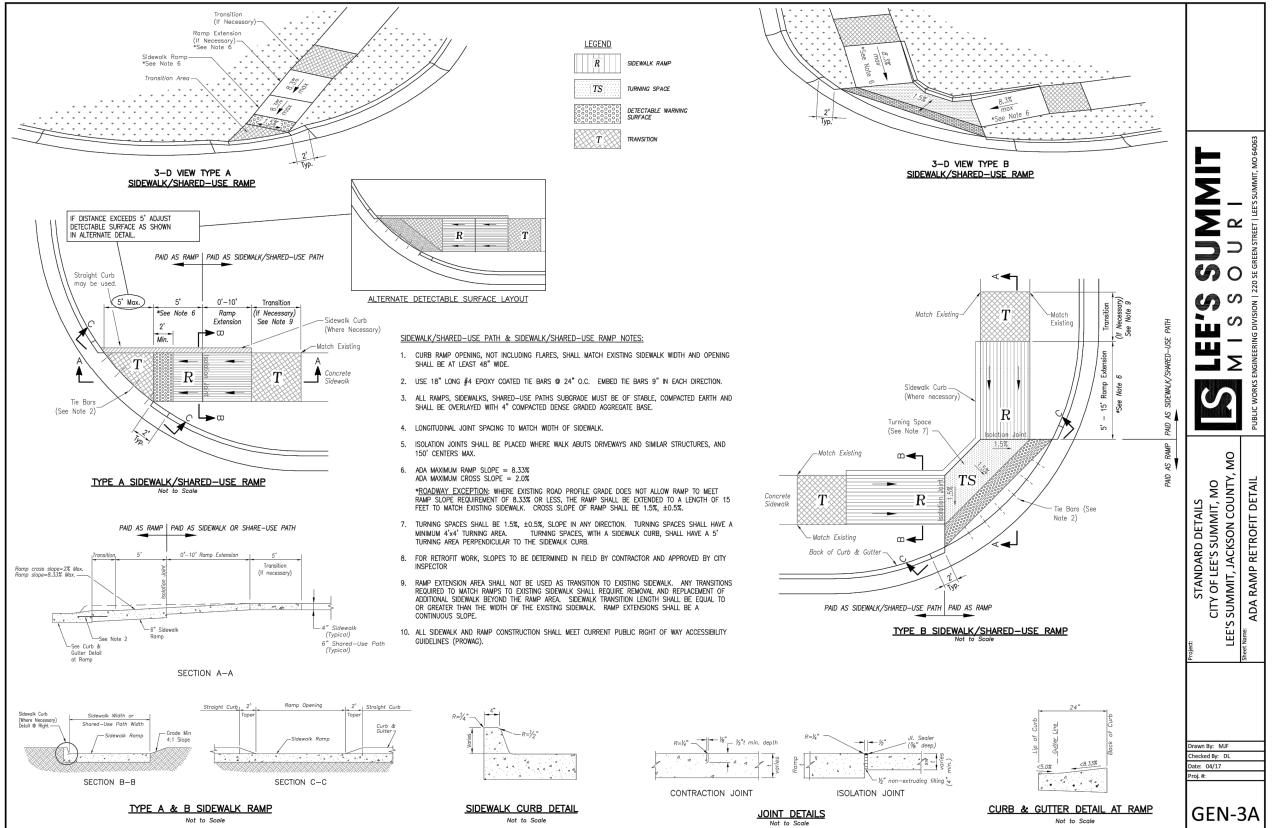






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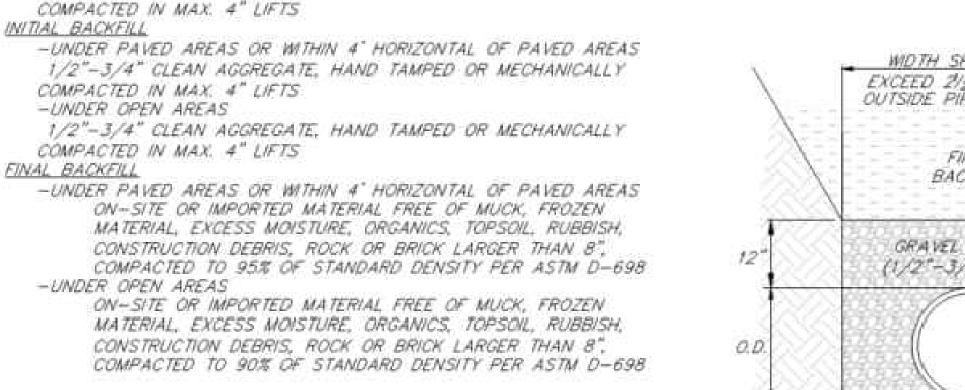
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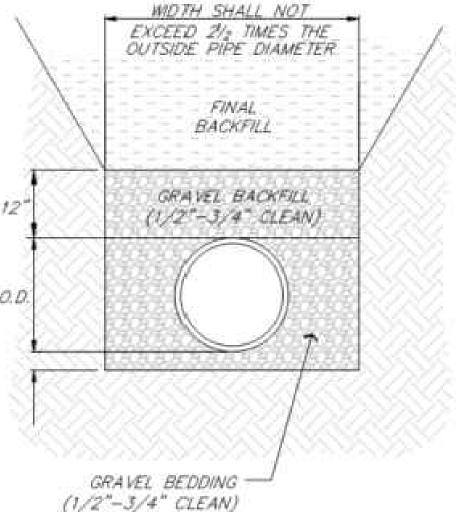
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BEDDING DEF	TH BELOW PI	PE"
PIPE DIAMETER	IN SOIL	IN ROCK
24" AND LESS	4"	6"
27" THRU 60"	- 4	0"

1/2"-3/4" CLEAN AGGREGATE, HAND TAMPED OR MECHANICALLY



PIPE BEDDING DETAIL

NOT TO SCALE

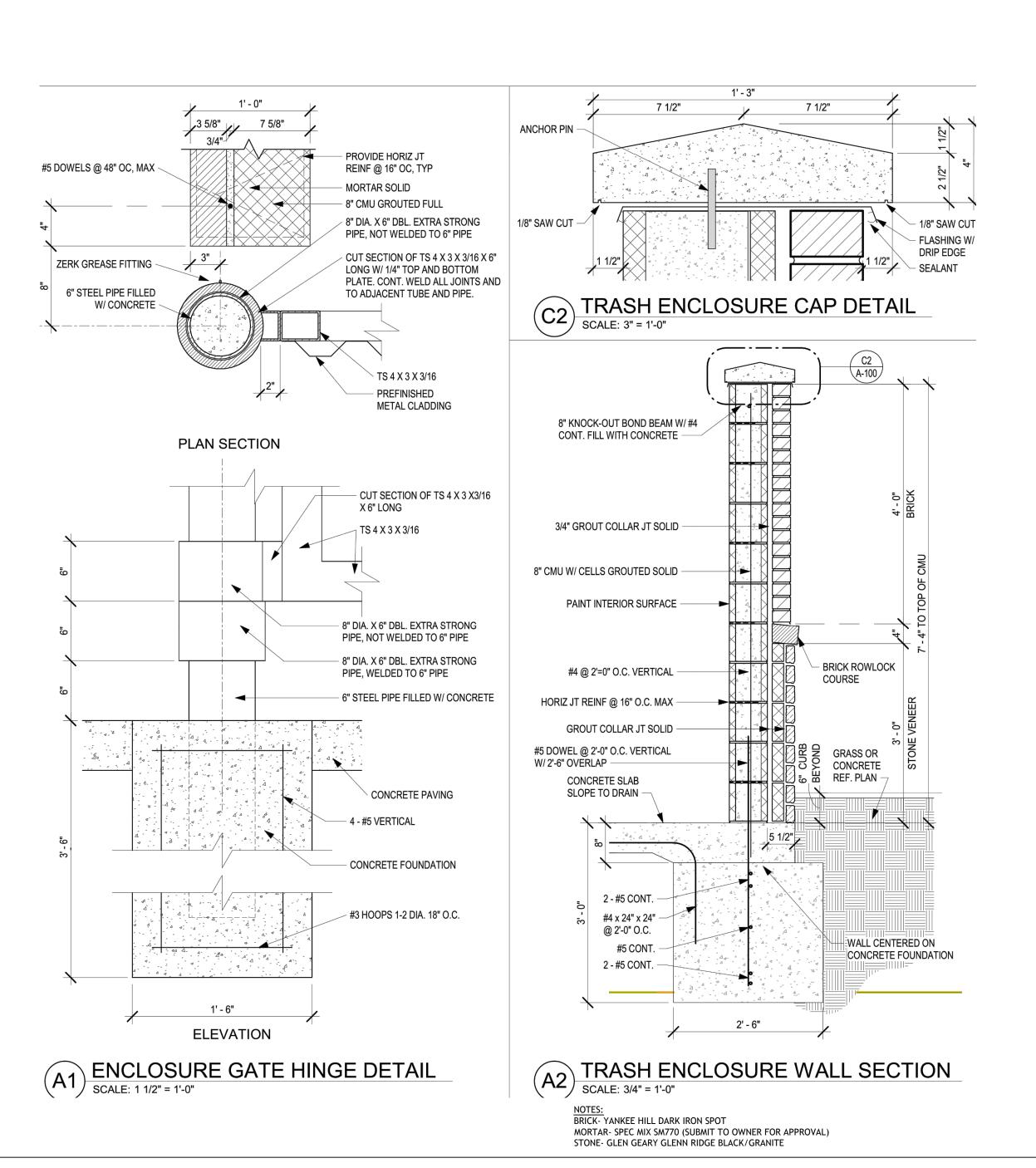
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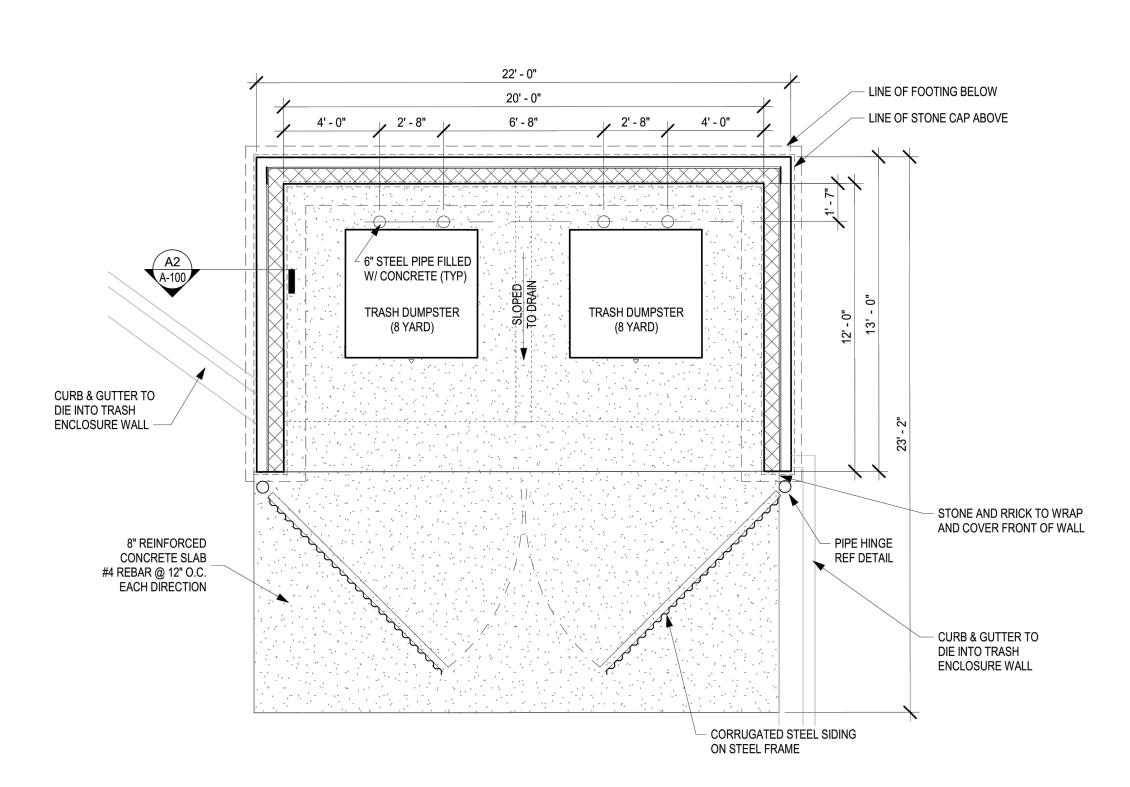
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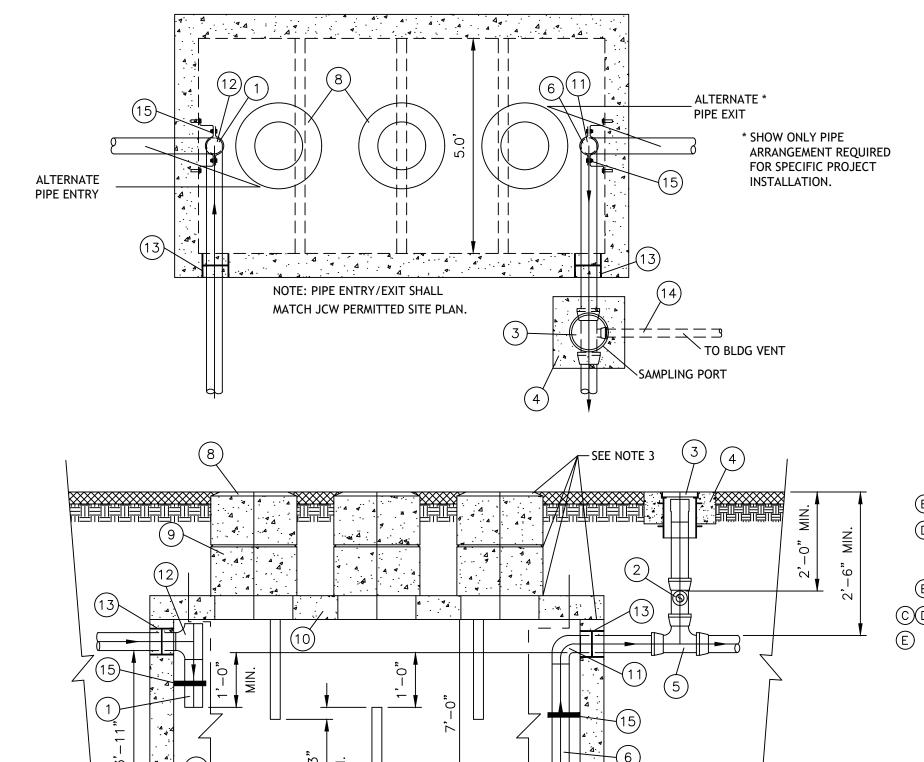
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TRASH ENCLOSURE GATE ELEVATION SCALE: 1/4" = 1'-0"





TRASH ENCLOSURE



7'-6"

ITEM	DESCRIPTION						
1	4" ABS INLET PIPE*						
2	4"x4"x2" TEE WITH 2" PIPE TO BUILDING VENT*						
3	THREADED C/O CAP JOSAM 58860 OR APP EQUAL**						
4	CONCRETE PAD						
5	4"x4"x4" TWO-WAY CLEANOUT TEE*						
6	4" ABS OUTLET*						
7	4" - 6" GRAVEL BEDDING						
8	HEAVY-DUTY CAST IRON FRAME AND COVER ***						
9	CONCRETE ADJUSTMENT RINGS						
10	REINFORCE AS REQUIRED FOR SERVICE CONDITIONS						
11	4" ABS 90° ELBOW*						
12	4" ABS TEE*						
13	A-LOK OR PRESS SEAL PSX PIPE/WALL CONNECTOR						
14	2" VENT PIPE (IDENTIFY PIPE TYPE, CLASS & JOINT						
14	AS REQUIRED FOR PROJECT)						
15	STAINLESS STEEL PIPE SUPPORT CLAMP ****						

* 6" PIPE MAY BE SUBSTITUDED TO MATCH UPSTREAM PIPE DIAMETER ** REFER TO CLEAN OUT DETAIL(S) ON STANDARD DETAIL SHEET. *** CLAY & BAILEY 2008 BV OR EQUAL (FROST PROOF COVERS OPTIONAL) ****FM STAINLESS FASTNERS #63 OR EQUAL. 1/2"x2-1/2" SS BRACKET W/ 1/2"x1-1/2" FULLY THREADED SS HEX BOLT WITH 1/2" SS WASHER AND 1/2"X1-3/4" SS ANCHORS. CLAMP TO BE FACTORY INSTALLED.

THREE COVERS AND RISERS SHOWN. TWO COVERS AND RISERS CENTERED OVER UPPER TWO BAFFLES ARE INTERCEPTOR SIZE - 1000 GAL MINIMUM (REVISE THE SIZE DIMENSIONS, AS NEEDED, FOR LARGER CAPACITY

INTERCEPTORS) ALL JOINTS AT THE FRAME & COVER*, CONCRETE ADJUSTMENT RINGS AND THE LID OF THE INTERCEPTOR SHALL BE SEALED WITH A MINIMUM OF TWO (2) ROWS OF 3/4 TO 1 INCH PREFORMED BUTYL JOINT SEALER AND A 6" BUTYL JOINT WRAP AROUND SLEEVE (EZ WRAP). THE ENDS OF THE 6" EZ WRAP SHALL OVERLAP BY 12". PIPING ON THE INTERIOR OF THE INTERCEPTOR SHALL BE ABS WITH SOLVENT-CEMENTED JOINTS. GREASE INTERCEPTOR INCLUDING ADJUSTMENT RINGS AND CASTINGS SHALL BE VACUUM TESTED FOR WATER TIGHTNESS AFTER THE BACKFILL OPERATIONS HAVE BEEN COMPLETED IN ACCORDANCE WITH JCW TECHNICAL

SPECIFICATIONS. A VACUUM OF 10 INCHES OF MERCURY SHALL BE DRAWN AND WITH THE VACUUM PUMP SHUT OFF THE MERCURY SHALL NOT DROP BELOW 9 INCHES WITHIN 1 MINUTE OR BELOW 5 INCHES WITHIN 5 MINUTES.

GREASE INTERCEPTOR

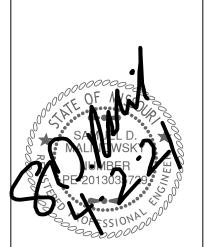
GREASE INTERCEPTOR SCHEDULE									
MANUFACTURER	MODEL	CAPACITY FULL		LENGTH WIDTH		HEIGHT	INLET	OUTLET	
	NO.	US gal.	WT (LBS)	L	W	Н	FL	FL	
OLD CASTLE	Q-1500	1500	20,255	90"	60"	84"	978.3	978.3	

NOTE: REINFORCED TANK WITH MESH THROUGHOUT. REINFORCED LID FOR DRIVE AREA. 4000 LB CONCRETE

5507 High Meadow Circle Manhattan Kansas, 66503 smcivilengr@gmail.com 785.341.9747

CONSTRUCTION

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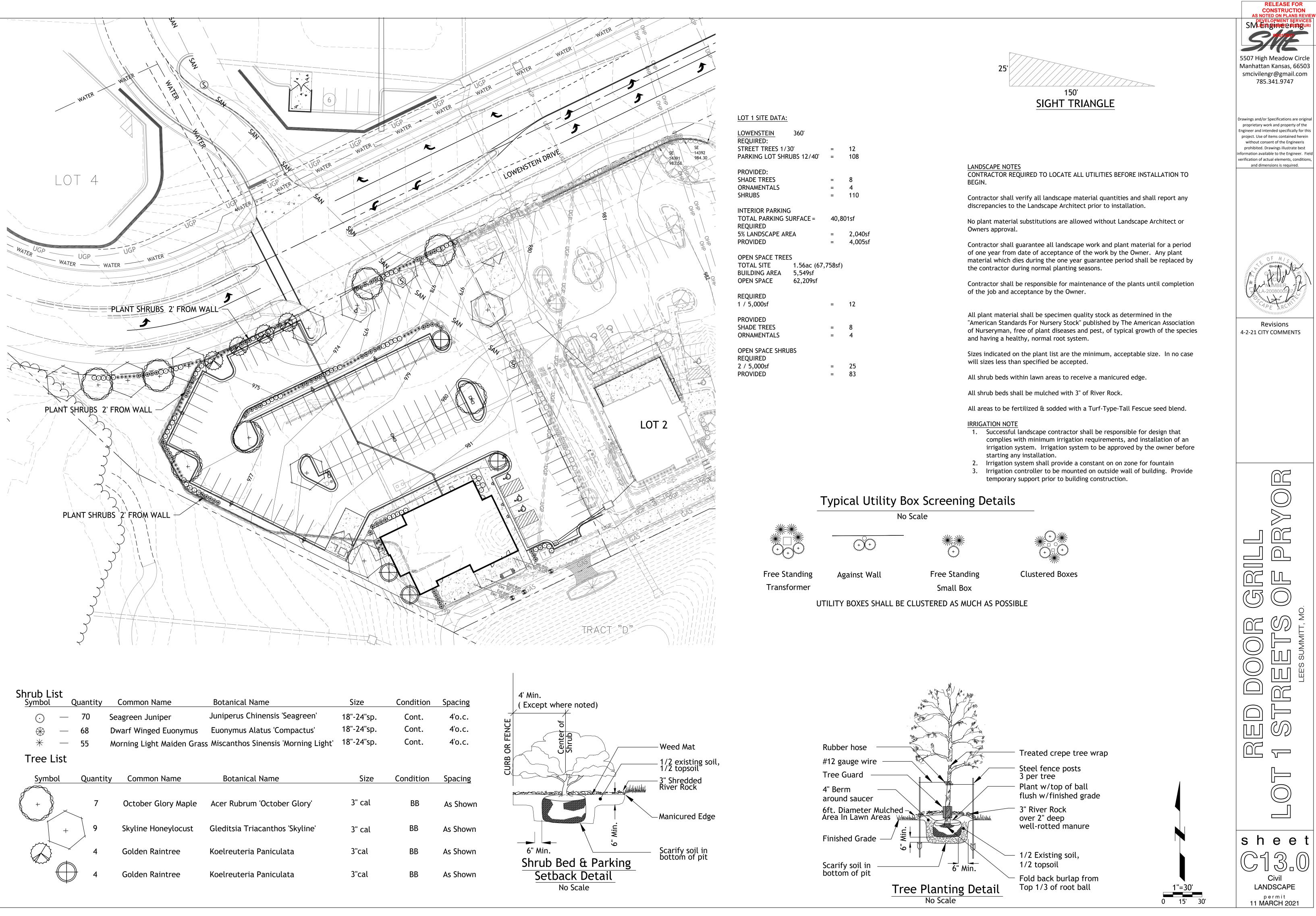


Revisions 4-2-21 CITY COMMENTS

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DETAILS

permit 11 MARCH 2021



5507 High Meadow Circle

Manhattan Kansas, 66503 smcivilengr@gmail.com 785.341.9747

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Revisions

4-2-21 CITY COMMENTS

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LANDSCAPE